

**CITY OF ONTARIO  
PLANNING COMMISSION/  
HISTORIC PRESERVATION  
MEETING AGENDA**

**May 26, 2020**

**Ontario City Hall  
303 East "B" Street, Ontario, California 91764**

**6:30 PM**

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**SPECIAL AND URGENT NOTICE ELIMINATING IN-PERSON PUBLIC PARTICIPATION AT CITY OF ONTARIO PLANNING COMMISSION MEETINGS**

In accordance with the Governor's Declarations of Emergency for the State of California (Executive Orders N-25-20 and N-29-20) and the Governor's Stay at Home Order (Executive Order N-33-20), the Ontario Planning Commission Meetings are being conducted via teleconference to limit in-person attendance at the upcoming meeting of the City of Ontario Planning / Historic Preservation Commission.

Members of the public may utilize alternative measures established by the City of Ontario to view the Planning Commission meetings and/or to address the Commissioners.

The meeting will be internet live streamed:  
[www.ontarioca.gov/Agendas/PlanningCommission](http://www.ontarioca.gov/Agendas/PlanningCommission)

We appreciate your understanding during this unprecedented time of social distancing under the Stay at Home Order. These procedures may be modified in the future as social and public gathering protocols change.

**WELCOME to a meeting of the Ontario Planning / Historic Preservation Commission.**

- All documents for public review are on file with the Records Management/City Clerk's Department located at 303 East B Street, Ontario, CA 91764.
- Anyone wishing to provide public comment or to address the Commission have been provided alternative measures including U.S. mail, email, a website comment form, and the ability to dial in and record a 5 minute voicemail. All public comments received by the established deadline for this meeting will be included as part of the official meeting record.
- In accordance with State Law, remarks during public comment are to be limited to subjects within the Planning Commission's jurisdiction. Remarks on other agenda items will be limited to those items.

**ROLL CALL**

DeDiemar \_\_ Downs \_\_ Gage \_\_ Gregorek \_\_ Reyes \_\_ Ricci \_\_ Willoughby \_\_

**PLEDGE OF ALLEGIANCE TO THE FLAG**

**ANNOUNCEMENTS**

- 1) Agenda Items
- 2) Commissioner Items

**PUBLIC COMMENTS**

*Citizens wishing to address the Planning/Historic Preservation Commission on any matter that is not on the agenda may do so at this time. Please state your name and address clearly for the record and limit your remarks to five minutes.*

*Please note that while the Planning/Historic Preservation Commission values your comments, the Commission cannot respond nor take action until such time as the matter may appear on the forthcoming agenda.*

**CONSENT CALENDAR ITEMS**

*All matters listed under CONSENT CALENDAR will be enacted by one summary motion in the order listed below. There will be no separate discussion on these items prior to the time the Commission votes on them, unless a member of the Commission or public requests a specific item be removed from the Consent Calendar for a separate vote. In that case, the balance of the items on the Consent Calendar will be voted on in summary motion and then those items removed for separate vote will be heard.*

**A-01. MINUTES APPROVAL**

Planning/Historic Preservation Commission Minutes of April 28, 2020, approved as written.

**A-02. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV19-006:**

A Development Plan to construct a 35,737 square foot industrial building on 1.48-acres of land located at 1533 West Holt Boulevard, within the IP (Industrial Park) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 01011-111-10) **submitted by United Trust Realty Corporation.**

- A-03. ADOPTION OF VEHICLE MILES TRAVELED (VMT) THRESHOLDS:** Adoption of Vehicle Miles Traveled Thresholds (VMT) for determining significance of transportation impacts through the California Environmental Quality Act in conformance with SB 743. **City Initiated City Council action is required.**

### **PUBLIC HEARING ITEMS**

*For each of the items listed under PUBLIC HEARING ITEMS, the public will be provided an opportunity to speak. After a staff report is provided, the chairperson will open the public hearing. At that time the applicant will be allowed five (5) minutes to make a presentation on the case. Members of the public will then be allowed five (5) minutes each to speak. The Planning/Historic Preservation Commission may ask the speakers questions relative to the case and the testimony provided. The question period will not count against your time limit. After all persons have spoken, the applicant will be allowed three minutes to summarize or rebut any public testimony. The chairperson will then close the public hearing portion of the hearing and deliberate the matter.*

### **HISTORIC PRESERVATION COMMISSION ITEMS**

- B. ENVIRONMENTAL ASSESSMENT AND CERTIFICATE OF APPROPRIATENESS REVIEW FOR FILE NO. PHP20-003:** A Certificate of Appropriateness for the adaptive reuse of the 3,658 square foot William B. Fallis House (Local Landmark No. 1), to a multi-modal mobility hub on 0.41 acres of land located at 122 South Vine Avenue, within the MU-1 (Mixed Use-Downtown) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15331 (Historical Resource Restoration/Rehabilitation) of the CEQA Guidelines. (APN: 1049-021-19) **submitted by the City of Ontario.**

**1. CEQA Determination**

No action necessary – Exempt: CEQA Guidelines Section § 15331

**2. File No. PHP20-003 (Certificate of Appropriateness)**

Motion to Approve/Deny

### **PLANNING COMMISSION ITEMS**

- C. ENVIRONMENTAL ASSESSMENT, DEVELOPMENT PLAN, AND CONDITIONAL USE PERMIT REVIEW FOR FILE NOS. PDEV18-022 AND PCUP18-021:** A Development Plan (File No. PDEV18-022) to construct a 6,870 square-foot industrial building in conjunction with a Conditional Use Permit (File No. PCUP18-021) to establish and operate a towing service on 3.1 acres of land located at 580 East Belmont Street, within the IL (Light Industrial) zoning district. Staff has determined that the project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan

(ALUCP); (APNs: 1049-491-01, 1049-491-02 and 1049-491-03) **submitted by Four Sisters Enterprises LLC.**

**1. CEQA Determination**

No action necessary – Exempt: CEQA Guidelines Section § 15332

**2. File No. PCUP18-021 (Conditional Use Permit)**

Motion to Approve/Deny

**3. File No. PDEV18-022 (Development Plan)**

Motion to Approve/Deny

**D. ENVIRONMENTAL ASSESSMENT, TENTATIVE TRACT MAP, AND DEVELOPMENT PLAN REVIEW FOR FILE NOS. PMTT19-011 (TT 20291) AND PDEV19-043:** A Tentative Tract Map (File No. PMTT19-011 / TT 20291) to subdivide 0.73-acre of land into a single lot for condominium purposes in conjunction with a Development Plan (File No. PDEV19-043) to construct 18 multiple-family dwellings located at 1005 and 1023 East Sixth Street, within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). (APNs: 1047-172-17 and 1047-172-22) **submitted by TAVK Holding LLC / T&S Allied Investment LLC.**

**1. CEQA Determination**

No action necessary – Exempt: CEQA Guidelines Section § 15332

**2. File No. PMTT19-011 (Tract Map)**

Motion to Approve/Deny

**3. File No. PDEV19-043 (Development Plan)**

Motion to Approve/Deny

**E. ENVIRONMENTAL ASSESSMENT AND REVIEW FOR FILE NO. PSPA19-007:** An Amendment to the Parkside Specific Plan (File No. PSPA19-007) to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4]

Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). The project site is bounded by Ontario Ranch Road to the north, Eucalyptus Avenue to south, Archibald Avenue to the east and Carpenter Avenue. The environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. This application is consistent with the previously adopted EIR and introduces no new significant environmental impacts. All previously adopted mitigation measures shall be a condition of project approval and are incorporated herein by reference. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). (APN(s): 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, and 0218-221-10); **submitted by SC Ontario Development Company, LLC. City Council action is required. This Item was continued from the April 28, 2020 Planning Commission meeting**

**1. CEQA Determination**

Motion to recommend Approval/Denial of use of an Addendum to a previous EIR

**2. File No. PSPA19-007 (Specific Plan Amendment)**

Motion to recommend Approval/Denial

**F. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT AGREEMENT AMENDMENT REVIEW FOR FILE NO. PDA05-002:**

A Fourth Amendment to the Development Agreement (File No. PDA05-002) to modify the commencement of certain specific infrastructure associated with the development of Tentative Tract Map 20316 (File No. PMTT19-020), and conform to the revised development standards established by the Parkside Specific Plan Amendment (File No. PSPA19-007), located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning Areas 1 through 4 of the Parkside Specific Plan. The environmental impacts of this project were previously analyzed in an addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008), certified by the City Council on September 5, 2006. This application is consistent with the previously adopted EIR and introduces no new significant environmental impacts. All previously adopted mitigation measures shall be a condition of project approval and are incorporated herein by reference. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). (APN(s): 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, and 0218-221-10); **submitted by SC Ontario**

**Development Company, LLC. City Council action is required.**

**1. CEQA Determination**

No action necessary – use of previous addendum to an EIR

**2. File No. PDA05-002 (Development Agreement Amendment)**

Motion to recommend Approval/Denial

**G. ENVIRONMENTAL ASSESSMENT AND TENTATIVE TRACT MAP REVIEW FOR FILE NO. PMTT19-020 (TT 20316):** A Tentative Tract Map (File No. PMTT19-20 / TT 20316) for condominium purposes to subdivide 56.99 acres of land into 4 numbered residential lots and 12 lettered lots for common areas, portions of the Great Park, private streets, public utility easements and neighborhood landscape edges. The project is located at the northwest corner of Archibald Avenue and Eucalyptus Avenue, within Planning areas 1 thru 4 (Conventionally-Loaded Lane Homes) of the Parkside Specific Plan. The environmental impacts of this project were previously analyzed in an addendum to the Parkside Specific Plan File (No. PSP03-002) EIR (SCH# 2004011008) that was certified by the City Council on September 5, 2006. This application is consistent with the previously adopted EIR and introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 0218-231-12, 0218-231-14, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-28, 0218-231-29, 0218-231-30, 0218-231-33, 0218-231-35 and 0218-231-38) **submitted by SC Ontario Development Company, LLC.**

**1. CEQA Determination**

No action necessary – use of previous addendum to an EIR

**2. File No. PMTT19-020 (Tract Map)**

Motion to Approve/Deny

**H. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV19-064:** A Development Plan to construct 540 single-family homes and a private recreational center on approximately 34.3 acres of land located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning Areas 1-4 and Planning Area 23 of the Parkside Specific Plan. The environmental impacts of this project were previously analyzed in an addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures shall be a condition of project approval and are incorporated herein by reference. The proposed project is

located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 0218-231-35, 0218-231-33, 0218-231-30, 0218-231-29, 0218-231-28, 0218-231-22, 0218-231-21, 0218-231-20, 0218-231-19, 0218-231-18, 0218-231-17, 0218-231-14, and 0218-231-12 ) **submitted by The New Home Company.**

**1. CEQA Determination**

No action necessary – use of previous addendum to an EIR

**2. File No. PDEV19-064 (Development Plan)**

Motion to Approve/Deny

**I. ENVIRONMENTAL ASSESSMENT AND SPECIFIC PLAN AMENDMENT REVIEW FOR FILE NO. PSPA20-004:**

An Amendment to the Grove Avenue Specific Plan for the following: 1) To establish consistency with the Ontario International Airport Land Use Compatibility Plan (ONT ALUCP); 2) To change the land use designation for 11.1 acres of land located at 1935 South Grove Avenue from Office/Commercial to Business Park to be consistent with TOP Policy Plan (Exhibit LU-01 Land Use Plan); and 3) Changes to the Development Standards to be consistent with the Business Park zoning district. The environmental impacts of this project were previously analyzed with The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140) certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts. State Law requires Specific Plans must be made consistent with adopted airport compatibility plans. The ONT ALUCP environmental impacts of this project were previously reviewed in conjunction with File No. PADV07-008, for which a Negative Declaration (State Clearinghouse No. 2011011081) was adopted by the Ontario City Council on April 19, 2011. This project introduces no new significant environmental impacts; (APN: 113-451-14) **City Initiated. City Council action is required.**

**1. CEQA Determination**

No action necessary – use of previous EIR

**2. File No. PSPA20-004 (Tract Map)**

Motion to Approve/Deny

**MATTERS FROM THE PLANNING/HISTORIC PRESERVATION COMMISSION**

- 1) Old Business
  - Reports From Subcommittees

- Historic Preservation (Standing): Met on May 14, 2020

- 2) New Business
  - Chairperson and Vice-Chairperson appointments
- 3) Nominations for Special Recognition

**DIRECTOR'S REPORT**

- 1) Monthly Activity Report

*If you wish to appeal any decision of the Planning/Historic Preservation Commission, you must do so within ten (10) days of the Commission action. Please contact the Planning Department for information regarding the appeal process.*

*If you challenge any action of the Planning/Historic Preservation Commission in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the Planning/Historic Preservation Commission at, or prior to, the public hearing.*



I, Gwen Berendsen, Administrative Assistant, of the City of Ontario, or my designee, hereby certify that a true, accurate copy of the foregoing agenda was posted on **May 22, 2020**, at least 72 hours prior to the meeting per Government Code Section 54954.2 at 303 East "B" Street, Ontario.

  
\_\_\_\_\_  
Gwen Berendsen, Secretary Pro Tempore

  
\_\_\_\_\_  
Cathy Wahlstrom, Planning Director  
Planning/Historic Preservation  
Commission Secretary



**CITY OF ONTARIO PLANNING COMMISSION/  
HISTORIC PRESERVATION MEETING**

**MINUTES**

**April 28, 2020**

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**CITY OF ONTARIO PLANNING COMMISSION/  
HISTORIC PRESERVATION MEETING**

(Presented to public via Zoom)

**MINUTES**

**April 28, 2020**

**REGULAR MEETING:** City Hall, 303 East B Street  
Called to order via zoom, by Chairman Willoughby at 6:30 PM

**COMMISSIONERS**

**Present:** Chairman Willoughby, Vice-Chairman DeDiemar, Downs, Gage, Gregorek, Reyes, and Ricci

**Absent:** None

**OTHERS PRESENT:** Planning Director Wahlstrom, City Attorney Rice, Principal Planner Mercier, Senior Planner Batres, Senior Planner Mejia, Assistant Planner Vaughn, Transportation Manager Bautista, and Planning Secretary Berendsen

**PLEDGE OF ALLEGIANCE TO THE FLAG**

The Pledge of Allegiance was led by Commissioner Ricci.

**ANNOUNCEMENTS**

Ms. Wahlstrom stated that the applicant for Item G is asking that it be continued to the next regular meeting. She also stated that we had received an electronic public comment letter and card regarding Item F, the Toyota Ontario Business Park Specific Plan Amendment, from Lozeau Drury, representing the Supporters Alliance for Environmental Responsibility, stating concerns regarding the environmental addendum, and that the Commissioners have been given a copy.

**PUBLIC COMMENTS**

No public comments, other than those noted in the announcements, were received.

**CONSENT CALENDAR ITEMS**

**A-01. MINUTES APPROVAL**

Planning/Historic Preservation Commission Minutes of March 26, 2020, approved as written.

**A-02. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV19-040: A Development Plan to construct one industrial building**

totaling 211,358 square feet on 9.34 acres of land located at 1610 and 1612 South Cucamonga Avenue, within the IG (General Industrial) zoning district. The environmental impacts of this project were previously analyzed with The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140) certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1050-201-02) **submitted by Alere Property Group LLC.**

### **PLANNING COMMISSION ACTION**

*It was moved by Gregorek, seconded by Downs, to approve the Planning Commission Minutes of March 26, 2016, as written and the Development Plan, File No., PDEV19-040, subject to the conditions of approval. The motion was carried 7 to 0.*

### **PUBLIC HEARING ITEMS**

- B. TWENTIETH ANNUAL MODEL COLONY AWARDS FILE NO. PHP20-004:** A request for the Historic Preservation Commission to accept the nominations for the Twentieth Annual Model Colony Awards; **submitted by City of Ontario. City Council presentation of Awards.**

Associate Planner Antuna, presented the staff report. She described the history of the Chaffey High School Campus and explained how the campus was designated as Local Landmark No. 58. She explained the improvements made to the Gardiner W. Spring Auditorium and its nomination for the Restoration Award and the completion of the Math and Science Building and its nomination for the Rehabilitation Award. She stated that staff is recommending the Planning Commission approve File No. PHP20-004, pursuant to the facts and reasons contained in the staff report, and to the City Council for presentation.

No one responded.

### **PUBLIC TESTIMONY**

No one responded.

As there was no one else wishing to speak, Chairman Willoughby closed the public testimony

Mr. Willoughby stated these are two ideal nominees for the awards, as they are such landmarks within our community, and they did such a good job with the remodeling and improvements. He stated he attended Chaffey High School one year in 1969.

Ms. DeDiemar stated she was in the Chaffey High School class of 1963.

Mr. Gregorek stated these are excellent choices for the awards, especially the auditorium and

both these nominations are well deserving.

Mr. Gage stated he was in the class of 1967 and at his 50<sup>th</sup> class reunion he was able to tour the Math and Science building and it is just beautiful and fits in the campus. He stated the auditorium renovations are just gorgeous. He stated these are worthy assets within the city and he thanked the people who gave to the restoration and rehabilitation.

### **PLANNING COMMISSION ACTION**

*It was moved by DeDiemar, seconded by Downs, to approve the nominations for the Twentieth Annual Model Colony Awards, File No., PHP20-004, and recommend City Council present the awards. Roll call vote: AYES, DeDiemar, Downs, Gage, Gregorek, Reyes, Ricci and Willoughby; NOES, none; RECUSE, none; ABSENT, none. The motion was carried 7 to 0.*

- C. **ENVIRONMENTAL ASSESSMENT, TENTATIVE PARCEL MAP, AND DEVELOPMENT PLAN REVIEW FOR FILE NOS. PMTT19-013 (PM 20157) AND PDEV19-050:** A Tentative Parcel Map (File No. PMTT19-013) to subdivide 5 acres of land into a single parcel, in conjunction with a Development Plan (File No. PDEV19-050) to construct a 104,993 square-foot industrial building, located at the northwest corner of Sunkist Street and Campus Avenue, at 617 East Sunkist Street, within the IL (Light Industrial) zoning district. The environmental impacts of this project were previously analyzed with The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140) certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1049-232-21) **submitted by Herdman Architecture and Design.**

Assistant Planner Vaughn, presented the staff report. She described the surrounding area, the cleanup of the lot-lines, proposed site plan, parking, landscaping, elevations, and the health risk assessment that was completed. She stated that staff is recommending the Planning Commission approve File Nos. PMTT19-013 and PDEV19-050, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval.

Mr. Reyes wanted clarification regarding the existing landscaping along the east Sunkist, if they would be protected or all new landscaping.

Ms. Vaughn stated the majority will be protected as well as some new.

Mr. Zeledon stated there was an Arborist report done on the Camphor trees and what healthy trees should remain and those that need to be replaced.

Mr. Reyes wanted clarification regarding trucks exiting out of westerly side and if they will be able to go west on Sunkist or move east to Campus to avoid the residential.

Mr. Zeledon stated they are to go east or north, but if the Commission would like we can add a

condition that trucks will exit and go east to Campus then north to Mission.

Mr. Downs wanted to know if the blue lighting would be on at night.

Mr. Vaughn referred to the applicant.

Mr. Zeledon stated as these are LED lights, they would be an accent at night to add illumination and an artsy feel to the building. He stated the applicant is available to speak on the item.

### **PUBLIC TESTIMONY**

Mr. Kevin Rice spoke and stated he appreciates the staff pulling this together and the city for being willing to develop the project, especially during the current conditions.

Mr. Willoughby asked Mr. Rice to clarify the LED lighting on the building.

Mr. Rice stated this is a feature they are trying to bring more modern to industrial and highlight the architecture at night. He stated this would be a subtle soft glow to highlight the building.

Mr. Willoughby asked Mr. Rice about the added condition regarding the truck traffic exiting east to Campus Ave.

Mr. Rice stated he agrees to this additional condition.

As there was no one else wishing to speak, Chairman Willoughby closed the public testimony

Mr. Reyes stated he was satisfied with the added condition to the Conditions of Approval, regarding trucks exiting towards Campus Ave.

Mr. Willoughby stated he agrees and requests that staff include the condition.

### **PLANNING COMMISSION ACTION**

*It was moved by Gregorek, seconded by Ricci, to adopt a resolution to approve the Tentative Parcel Map, File No., PMTT19-013, and the Development Plan, File No., PDEV19-050, subject to conditions of approval, including the additional condition regarding truck traffic exiting the SW corner going east to Campus Ave.. Roll call vote: AYES, DeDiamar, Downs, Gage, Gregorek, Reyes, Ricci, and Willoughby; NOES, none; RECUSE, none; ABSENT, none. The motion was carried 7 to 0.*

- D. ENVIRONMENTAL ASSESSMENT AND SPECIFIC PLAN AMENDMENT REVIEW FOR FILE NO. PSPA19-009:** An Amendment (File No. PSPA19-009) to the Piemonte Overlay of the Ontario Center Specific Plan, to modify the Minimum Parking Requirements (Section 3.3.5.1) to allow tandem parking up to a maximum of 50% of the required parking. The Ontario Center Specific Plan-Piemonte Overlay encompasses 84.43 acres of land, generally located north of Concours Street, south of Fourth Street, west of Via Alba, and east of Haven Avenue. The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA16-003, for which a

Mitigated Negative Declaration was adopted by the City Council on May 16, 2017. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 0210-204-26, 0210-204-37, 0210-204-40, 0210-531-15, 0210-531-16) **submitted by LCD Residential at Ontario, LLC. City Council action is required.**

- E. **ENVIRONMENTAL ASSESSMENT, TENTATIVE TRACT MAP AND DEVELOPMENT PLAN REVIEW FOR FILE NOS. PMTT19-016 (TT 20308) AND PDEV19-054:** A Tentative Tract Map (File No. PMTT19-016) to subdivide 3.02 acres of land into one numbered lot and two lettered lots for condominium purposes in conjunction with a Development Plan (File No. PDEV19-054) to construct 72 multi-family residential units (Townhomes). The project is located at the southwest corner of Via Alba and Via Villagio, within the Residential land use district of the Ontario Center Specific Plan-Piemonte Overlay. The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA16-003, for which a Mitigated Negative Declaration was adopted by the City Council on May 16, 2017. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0210-204-40) **submitted by The New Home Company Southern California LLC.**

Senior Planner Batres, presented the staff report. He described the Piemonte Overlay area and the specific site area. He described the need for the amendment regarding tandem parking, the proposed tract map including landscaping, parking, amenities, architectural design, elevations, and floor plans. He stated that staff is recommending the Planning Commission recommend approval for File No. PSPA19-009, and approve File Nos. PMTT19-016 and PDEV19-054, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval.

Ms. Wahlstrom gave some context regarding the Ontario Center Mixed Use area. She described the intensity of this area within the city, with the mixed uses and density. She stated this project is within a prime area that is urban and the transit, BRT will be coming to the area, which makes the increase in tandem parking more appropriate.

Mr. Willoughby clarified this tandem parking increase is only within this project and this specific area and no other area in the city.

Ms. Wahlstrom stated that is correct, it is only in this planning area, within this specific plan.

Mr. Gage wanted to clarify the tandem parking from 12 to 50 percent and that the applicant is doing the full 50 percent, and if in plan 2 the stairways hinder the parking space.

Mr. Batres stated no it's designed to go above, entering from north side of the plan and the stairs will move up.

Mr. Zeledon clarified on the slide the position on the first car and second car and that it is out of the distance from the staircase. He also stated it is a gated community and parking will be strictly enforced by the HOA.

Mr. Gage wanted to make sure it would accommodate a standard length of car in both spaces.

Mr. Batres stated yes.

Ms. Wahlstrom stated the stairways offer plenty of clearance in terms of height.

Mr. Gage wanted to know of any storage cabinets provided in the garage design.

Mr. Batres stated yes at the front of the garage there will be overhead storage.

Mr. Gage wanted to know if this was for each garage.

Mr. Batres stated yes.

Mr. Gage wanted to clarify the increase was for this urban area and be assured it wasn't setting a precedent.

Ms. Wahlstrom stated that would have to be a policy decision that would need to come back to the commission for action.

Mr. Gage wanted to clarify it would be looked at case by case.

Ms. Wahlstrom stated yes it would need to be looked at case by case.

Mr. Downs wanted to know where the elevators are located.

Mr. Batres stated none are proposed as it's only a three-story project.

Mr. Willoughby clarified that these are for sale units and not a complex.

Mr. Batres stated that is correct and that CC&Rs will be recorded with the project.

Mr. Reyes stated the amenities in the main area were good and wanted clarification regarding the paseo in middle of project with benches and shade trees, are there any other amenities and will it terminate at the west end or connect to future projects.

Mr. Batres stated the they have proposed benches within the paseo and pedestrian access along the east and two points of access along the main gates, southern portion of the project, but no gate to the west, but that is something that could be requested.

Mr. Reyes wanted clarification about paseo connectivity within the specific plan.

Mr. Zeledon stated the paseo system throughout the Ontario Center specific plan goes from Milliken over to Haven with connecting sidewalks, from the arena area to the office buildings to

the east and west, but this ends because this leads to commercial, but could be punched out at another time depending what is developed in the future.

Mr. Reyes stated that is why he brought it up so it could be linked through the commercial to future design, if we make a design now with maybe a block wall and gate, or have staff work with the applicant to design something that could be punched out at a later date.

Mr. Zeledon stated with the commercial there are safety issues they have to think about, and he pointed out the paseo connections walkway that comes out the gates and leads to the sidewalk that leads to the hotel and arena.

Mr. Reyes stated the parking lots at Victoria Gardens have paseo that goes right through the structures and we need to think about future development with a possible link.

Mr. Reyes wanted clarification regarding the garage areas elevations is there landscape pockets between the garage doors.

Mr. Batres stated yes, they are providing that.

Mr. Willoughby wanted clarification regarding the paseo goes east to west and to the west is the future entertainment, retail, restaurant area that has already been approved.

Mr. Zeledon stated no to the northwest of the site is the entertainment area, to the west will be future commercial, but the paseo will lead to the entertainment core that runs from 4<sup>th</sup> Street to the arena.

Mr. Willoughby stated that would be the little strip that is planned for future entertainment and this would be a way to take them there.

Mr. Zeledon stated yes potentially it would.

Mr. Willoughby wanted to clarify that the project was to the east is Big AL's.

Mr. Zeledon stated yes that is correct, you would go north and over to the Target center and Big AL's area.

### **PUBLIC TESTIMONY**

Mr. Sage McCleve with LCD Residential at Ontario, and Ms. Holly Slevcove with New Home Company, spoke. Mr. McCleve thanked staff and stated they did a great job, and this is a tight site to come up with a site plan that we will all be happy with. He stated they did reach out to the west commercial site and the optional plans he had seen showed commercial backing up to the paseo, but he could work with staff to make it accessible to the site to provide to continuation, but would need to see more of what the site will hold before they made that decision on a paseo connection. He stated he could work with staff to see about maybe pilasters or something that could be punched out and connected to easily. He also pointed out that the sidewalks to the south are on a shared drive isle to the Element hotel which provides east-west connectivity.

Mr. Willoughby stated he appreciates the idea of future connectivity to the west.



Mr. Gage increase tandem parking are you ok with staff recommendation to have storage cabinets at the full back of the garage, a strict HOA to regulate parking and planted landscape between the garages.

Mr. McCleve stated a strict association will be critical for the site, that will have strict parking regulations, due to the close proximity to neighbors. He stated there will be an element of landscaping between driveways, but have not finalized if it's potted plants or ground landscaping, and the tandem parking is a little over 40 feet, which leaves little bit of storage in the front and a nook in each garage for trash cans.

Ms. Slevcove stated each garage has enough air clearance for the racks installed in the front by the homeowner and there is room for a little front storage. She stated the landscape area between the garages will be planted with vine growth within the drive isles and that all landscaping will be HOA maintained.

Mr. McCleve stated they wanted uniform design and maintenance throughout the site.

Mr. Willoughby stated the entire project will look good all the time, which is critical.

Mr. Reyes wanted to know if any landscape lighting is proposed.

Ms. Slevcove stated yes there are bollards planned for the paseo areas and proposed site lighting plan with typical street lighting.

Mr. Reyes wanted to know of any proposed bollards along the perimeter, beyond the regular streetlights.

Ms. Slevcove stated not at this time, they are working within our site alone, which would be adequate enough with street lighting and the surrounding area.

Mr. Reyes stated maybe staff will need to work on the future lighting for the area as a whole.

Mr. Willoughby thanked the applicant and stated he likes what he sees and looks forward to the project.

Ms. Berendsen stated there are no public comments on this item.

Ms. Wahlstrom stated staff should continue to look at lighting as we move forward.

As there was no one else wishing to speak, Chairman Willoughby closed the public testimony

Mr. Gage stated he was not happy to increase tandem parking by 36 which is an inconvenience to homeowners. He stated he is aware special intense and urban area and we need to be very aware of walkability with proper lighting and cautioned that we make this an urban environment and get public transportation in there. He stated that the overhead storage in the garage helps.

Mr. Willoughby clarified that this change is only for the Piemonte Specific Plan and he thinks it will work in this area and requested a strict HOA and that requirements are met.

## PLANNING COMMISSION ACTION

*It was moved by Ricci, seconded by Downs, to recommend adoption of a resolution to approve the Specific Plan Amendment, File No., PSPA19-009. Roll call vote: AYES, DeDiemar, Downs, Gage, Gregorek, Reyes, Ricci, and Willoughby; NOES, none; RECUSE, none; ABSENT, none. The motion was carried 7 to 0.*

Mr. Zeledon described the decorative lighting plan within the connectivity throughout the specific plan, with low bollards throughout.

Mr. Reyes stated he would like staff work with applicant to work on the lighting plan and maybe pilasters at west end of paseo, to punch them out in the future with possible development and that he is happy with landscape and benches in the paseo. He stated that overall this is a great project.

*It was moved by Reyes, seconded by Ricci, to adopt a resolution to approve the Tentative Tract Map, File No., PMTT19-016, and the Development Plan, File No., PDEV19-054, subject to conditions of approval. Roll call vote: AYES, DeDiemar, Downs, Gage, Gregorek, Reyes, Ricci, and Willoughby; NOES, none; RECUSE, none; ABSENT, none. The motion was carried 7 to 0.*

- F. **ENVIRONMENTAL ASSESSMENT AND SPECIFIC PLAN AMENDMENT REVIEW FOR FILE NO. PSPA19-004:** An Amendment to the 95.35-acre Toyota Ontario Business Park Specific Plan, revising the current land use district covering Planning Area 1, from Office/Research and Development (“Office/R&D”) to Industrial Mixed Use, allowing for warehouse, distribution, and manufacturing land uses on the site in conjunction with the currently allowed Office/R&D land uses. Additionally, the Amendment will update the Specific Plan’s landscape palette to conform to current California friendly landscape practices. The Specific Plan area is generally located south of Jurupa Street, east of Milliken Avenue, north of Francis Street, and west of the I-15 freeway. The environmental impacts of this project were analyzed in an Addendum to The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140), certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APN: 0238-121-75) **submitted by MIG. This item was continued from the March 26, 2020 Planning Commission meeting. City Council action is required.**

Principal Planner Mercier presented the staff report. He described the location and surrounding area and the existing uses in each area. He described the change and what uses would be allowed and the landscape palette changes. He stated that staff is recommending the Planning Commission recommend approval of File No. PSPA19-004, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval.

Mr. Gage wanted to verify this landscape overlay doesn’t remove any of the existing date palms.

Mr. Mercier stated that is correct, it does not affect the existing palms.

Mr. Ricci wanted to know the NE corner land designation.

Mr. Mercier stated it is light industrial.

Mr. Ricci wanted to know the area to the west land designation.

Mr. Mercier stated the area to the west is the same and office/research is included as a use, but the land use designation is light industrial.

Mr. Willoughby stated that Planning area 1 is similar to the California Commerce Center that is along Jurupa and Milliken.

Mr. Mercier stated that is correct.

### **PUBLIC TESTIMONY**

Mr. Clint McMorris with Toyota spoke and thanked staff for their work and commissioners for their consideration, and the value they find in the longstanding relationship they have with the city.

Ms. Pam Steele with MIG, echoed Mr. McMorris's comments and stated the staff are a great bunch to work with.

Mr. Willoughby stated they have a long relationship with the city, and we appreciate that.

Mr. Gage wanted to verify that they are not taking down the existing date palms.

Mr. McMorris stated no.

Ms. Steele stated in reviewing the landscape palette, several plants in the palette were no longer desirable within the city.

Mr. Willoughby stated the change in the landscape palette is quite needed.

Mr. Gage asked if the applicant agrees with the conditions of approval.

Mr. McMorris stated yes.

Ms. Steele stated yes.

Mr. Reyes wanted to know the intentions of the land in the future, are they planning to use it or parcel it out.

Mr. McMorris stated they plan to sell the property.

Mr. Reyes stated he noticed expansion to the west with trucking and wanted to know if Mr.

McMorris felt they were at their max growth.

Mr. McMorris stated they have looked at site plans and doesn't see them increasing the footprint of the building at this time.

Mr. Willoughby asked regarding Planning area 3 expansion.

Mr. McMorris stated it could be a possible trailer storage expansion.

Ms. Berendsen stated public comments were received by Lozeau Drury for this item, as previously stated.

As there was no one else wishing to speak, Chairman Willoughby closed the public testimony

Mr. Reyes stated this proposal will match what is across the street and it will be a good use and wants to comment on landscape for industrial, that technically there are some things that could still be done, and staff should look at future California friendly landscape guidelines for industrial.

#### **PLANNING COMMISSION ACTION**

*It was moved by Reyes, seconded by Downs, to recommend adoption of a resolution to approve the Addendum and Specific Plan Amendment, File No., PSPA19-004, subject to conditions of approval. Roll call vote: AYES, DeDiemar, Downs, Gage, Gregorek, Reyes, Ricci, and Willoughby; NOES, none; RECUSE, none; ABSENT, none. The motion was carried 7 to 0.*

- G. ENVIRONMENTAL ASSESSMENT AND REVIEW FOR FILE NO. PSPA19-007:**  
An Amendment to the Parkside Specific Plan (File No. PSPA19-007) to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). The environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. This application is consistent with the previously adopted EIR and introduces no new significant environmental impacts. All previously adopted mitigation measures shall be a condition of project approval and are incorporated herein by reference. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). (APN(s): 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, and 0218-221-10); **submitted by SC Ontario Development Company, LLC. City Council action is required.**

This Item is being requested to be continued to the May 26, 2020 meeting.

Chairman Willoughby opened the public testimony

**PUBLIC TESTIMONY**

No one responded.

**PLANNING COMMISSION ACTION**

*It was moved by Downs to continue the Specific Plan Amendment, File No. PSPA19-007, to the May 26, 2020 meeting. It was carried unanimously, 7-0.*

**MATTERS FROM THE PLANNING COMMISSION**

**Old Business Reports From Subcommittees**

**Historic Preservation (Standing):** This subcommittee did not meet

**Development Code Review (Ad-hoc):** This subcommittee did not meet.

**Zoning General Plan Consistency (Ad-hoc):** This subcommittee did not meet.

**New Business**

None at this time.

**NOMINATIONS FOR SPECIAL RECOGNITION**

None at this time.

**DIRECTOR'S REPORT**

Ms. Wahlstrom stated the Monthly Activity Reports are in their packets. She also stated that the California Preservation Conference is May 18 – 20 and will be online this year.

**ADJOURNMENT**

Mr. Gregorek motioned to adjourn, carried unanimously. The meeting was adjourned at 8:26 PM.

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Secretary Pro Tempore

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Chairman, Planning Commission



# PLANNING COMMISSION STAFF REPORT

May 26, 2020

**FILE NO.:** PDEV19-006

**SUBJECT:** A Development Plan (File No. PDEV19-006) to construct a 35,737 square foot industrial building on 1.48-acres of land located at 1533 West Holt Boulevard, within the IP (Industrial Park) zoning district (APN: 1011-111-10); **submitted by United Trust Realty Corporation.**

**PROPERTY OWNER:** The Beacon Group, Inc.

**RECOMMENDED ACTION:** That the Planning Commission consider and approve File No. PDEV19-006, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval contained in the attached departmental reports.

**PROJECT SETTING:** The Project site is comprised of 1.48-acres of land located at the southeast corner of Holt Boulevard and Benson Avenue, at 1533 West Holt Boulevard, within the IP (Industrial Park) zoning district, and is depicted in Figure 1: Project Location, below. The site is relatively flat, with a gentle north to south slope of just over one percent. The property to the north of the Project site, across Holt Boulevard, is within the HDR-45 (25.1 to 45.0 Units/Acre) zoning district and is developed with a restaurant and liquor store. The property to the east is within the IP (Industrial Park) zoning district and is developed with industrial uses. The property to the south is within the IP (Industrial Park) zoning district and consists of vacant land and industrial land uses. The property to the west, across Benson Avenue, is within the City of Montclair and is developed with a used car dealership. The surrounding existing land uses, Policy Plan (General Plan), and zoning information are tabulated in the Technical Appendix of this report.



**Figure 1: Project Location**

Case Planner:	Luis E. Batres
Planning Director Approval:	
Submittal Date:	1/17/19

Hearing Body	Date	Decision	Action
DAB	5/18/20	Approval	Recommend
PC	5/26/20		Final
CC			

## **PROJECT ANALYSIS:**

[1] Background — On May 18, 2020, the Development Advisory Board (DAB) conducted a hearing to consider the subject Development Plan and concluded the hearing voting to recommend that the Planning Commission approve the Application subject to conditions of approval, which are included with the attached Planning Commission resolution.

[2] Site Design/Building Layout — The Applicant is requesting Development Plan approval to construct an industrial building totaling 35,737 square feet and having a floor area ratio (FAR) of 0.55. The proposed building is situated at the northwest corner of the site and is set back 20 feet from the Holt Boulevard (front) property line (10 feet will be dedicated to the City for the future widening of Holt Boulevard), 23 feet from the Benson Avenue (street side) property line, 0-feet from the south (rear) property line, and 135 feet from the east property line. Off-street parking is located on the east side of the building, adjacent to the front office entrance and fronting onto Holt Boulevard (see Exhibit B-Site Plan, attached).

An enclosed yard area is located on the east side of the building, which is screened from public view by a combination of the building and a decorative 10-foot high screen wall that has been designed to match the building architecture. A tubular steel fence, 10-feet in height, is proposed along the south and east interior property lines.

[3] Parking —The Project has provided off-street parking pursuant to the “Warehouse and Distribution Speculative” parking standards specified in the Ontario Development Code. The building requires a total of 28 parking spaces and one truck parking space, and 28 parking spaces and one truck parking space has been provided, in compliance with the minimum parking requirements; therefore, no parking shortage is anticipated.

[4] Architecture —The proposed industrial warehouse building will be of concrete tilt-up construction and designed in a contemporary architectural style that exemplifies the high-quality architecture promoted by the Ontario Development Code and The Ontario Plan (“TOP”). Special attention has been given to the use of color, massing, building form, materials, and architectural details (see Figure 2: Building Perspective, and Exhibits E and F—Exterior Elevations, attached).

This is exemplified through the use of:

- Extensive use of glazing on storefront and along the architectural tower facing Holt Boulevard and Benson Avenue;
- Articulation in building footprint and building roof lines;
- Incorporation of playful horizontal and vertical reveal lines over windows and on all architectural towers around the building;



- Incorporation of architectural towers along the north, east and west elevations;
- Incorporation of decorative steel black canopies; and
- Incorporation of five different building colors to add more interest.



***Figure 2: Building Perspective***

[5] Landscaping —The Ontario Development Code requires a minimum 15 percent landscape coverage for corner parcels, which the Project meets. The Project provides a 23-foot landscaped setback along Benson Avenue, 20-foot landscape setback along Holt Boulevard (10 feet will be dedicated to the City for the future widening of Holt Boulevard), a 5-foot landscaped setback along the east property line, and a 7-foot landscape setback along the south property line. In addition, landscaping will be provided within the interior area of the parking lot. The proposed landscape plan incorporates a combination of 48-inch, 36-inch, and 24-inch box trees, in addition to a variety of shrubs and ground cover. Proposed trees include Chitalpa Decentness (Pink Dawn), Pistachia Chinensis, Magnolia (Little Gem), Cercidium X (Desert Museum), Cercis Occidentalis (Western Redbud), Podocarpus Gracilior (Fern Pine), and Rhus Lancea (African Sumac) (see Exhibit C: Landscape Plan, attached).

In addition, the front office entry area and the entry driveway have been designed with decorative paving. A 22-foot by 22-foot outdoor employee area has also been incorporated at the southeast corner of the Project site. The employee area design includes outdoor furniture (2 pre-cast concrete tables with seats), enhanced paving, a shade tree, and accent ground cover.

[6] Utilities (drainage, sewer)—Public utilities (water and sewer) are available to serve the Project. Additionally, the applicant has submitted a Preliminary Water Quality Management Plan (PWQMP), which establishes the Project’s compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development (LID) best management practices (BMPs), such as retention and infiltration, bio treatment, and evapotranspiration. The PWQMP proposes catch basins and underground infiltration chambers located beneath the truck yard area, which are designed to accept runoff from building roofs, parking lots and Project drive aisles.

**COMPLIANCE WITH THE ONTARIO PLAN:** The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed Project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City’s Economy
- Operate in a Businesslike Manner
- Invest in the City’s Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)

[2] Vision.

**Distinctive Development:**

- Commercial and Residential Development
  - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

[3] Governance.

**Decision Making:**

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
  - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

[4] Policy Plan (General Plan)

**Land Use Element:**

➤ LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.

➤ LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).

▪ Goal LU2: Compatibility between a wide range of uses.

➤ LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

**Community Economics Element:**

▪ Goal CE1: A complete community that provides for all incomes and stages of life.

▪ Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

➤ CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

➤ CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

➤ CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

➤ CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

**Safety Element:**

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.
  - S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

**Community Design Element:**

- Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.
  - CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.
  - CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.
- Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.
  - CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:
    - Building volume, massing, and height to provide appropriate scale and proportion;
    - A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
    - Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.
  - CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.
  - CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways,

corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-12 Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

▪ Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

➤ CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

➤ CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.

➤ CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.

➤ CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.

➤ CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

- Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

- CD5-1 Maintenance of Buildings and Property. We require all public and privately-owned buildings and property (including trails and easements) to be properly and consistently maintained.

- CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

**HOUSING ELEMENT COMPLIANCE:** The Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

**AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE:** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed Project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

**ENVIRONMENTAL REVIEW:** The Project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, Infill Development Projects) of the CEQA Guidelines, which consists of projects characterized as infill development, and meets all of the following conditions: (a) the Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations; (b) the proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses; (c) the Project site has no value as habitat for endangered, rare, or threatened species; (d) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and (e) the Project site can be adequately served by all required utilities and public services.

**CONDITIONS OF APPROVAL:** See attached department reports.

**TECHNICAL APPENDIX:**

**Surrounding Zoning and Land Use:**

	<i>Existing Land Use</i>	<i>General Plan Designation</i>	<i>Zoning Designation</i>	<i>Specific Plan Land Use</i>
<i>Site:</i>	Vacant	Business Park	IP (Industrial Park)	n/a
<i>North:</i>	Commercial (Playa Sol Restaurant & Liquor Store)	HDR (High Density Residential)	HDR-45 (25.1 to 45.0 Units/Acre)	n/a
<i>South:</i>	Vacant & Industrial	Business Park	IP (Industrial Park)	n/a
<i>East:</i>	Commercial/Industrial	Business Park	IP (Industrial Park)	n/a
<i>West:</i>	Use Car Dealership	City of Montclair	City of Montclair	n/a

**General Site & Building Statistics**

<i>Item</i>	<i>Proposed</i>	<i>Min./Max. Standard</i>	<i>Meets Y/N</i>
<i>Lot/Parcel Size:</i>	1.48 Acres	1-Acre (Min.)	Y
<i>Building Area:</i>	35,737 sq. ft.	N/A	Y
<i>Floor Area Ratio:</i>	0.55	0.55 (Max.)	Y
<i>Building Height:</i>	39-Feet	45 (Max.)	Y

**Off-Street Parking:**

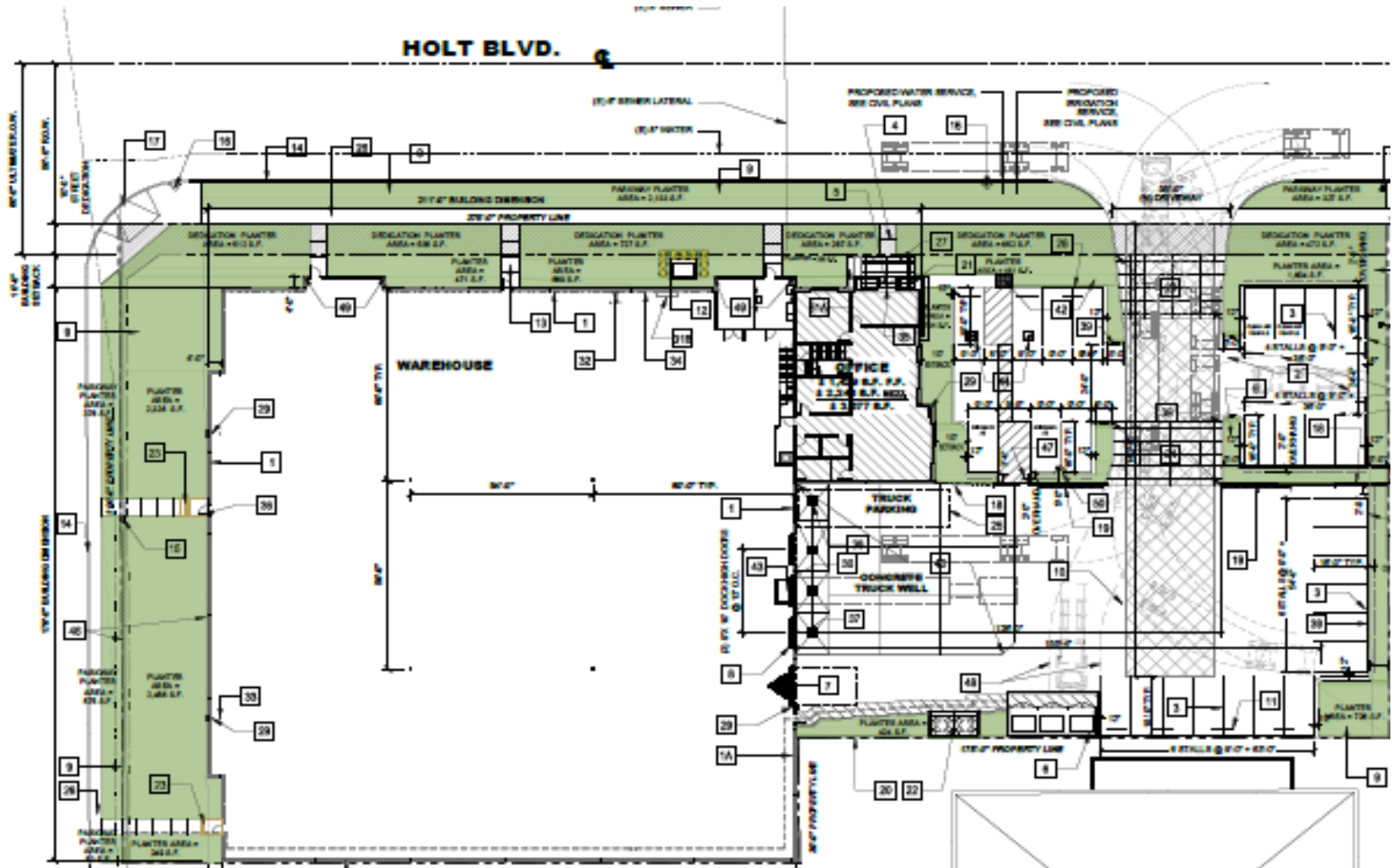
<i>Type of Use</i>	<i>Building Area</i>	<i>Parking Ratio</i>	<i>Spaces Required</i>	<i>Spaces Provided</i>
<i>Warehouse</i>	32,060 Sq. Ft.	One space per 1,000 SF for first 20,000 GFA and 0.5 per 1,000 SF for building GFA greater than 20,000 SF; plus 1 tractor-trailer parking space per 4 dock-high loading doors; plus required parking for "general business offices" when those uses exceed 10% of building GFA (Gross Floor Area).	26	26
<i>Office (portion of building exceeding 10% of warehouse GFA)</i>	3,677 Sq. Ft. (471 Sq. Ft. over 10% Allowed)	4 spaces per 1,000 SF of GFA	2	2
<b>TOTAL</b>	35,737		28	28

**Exhibit A—PROJECT AERIAL MAP**

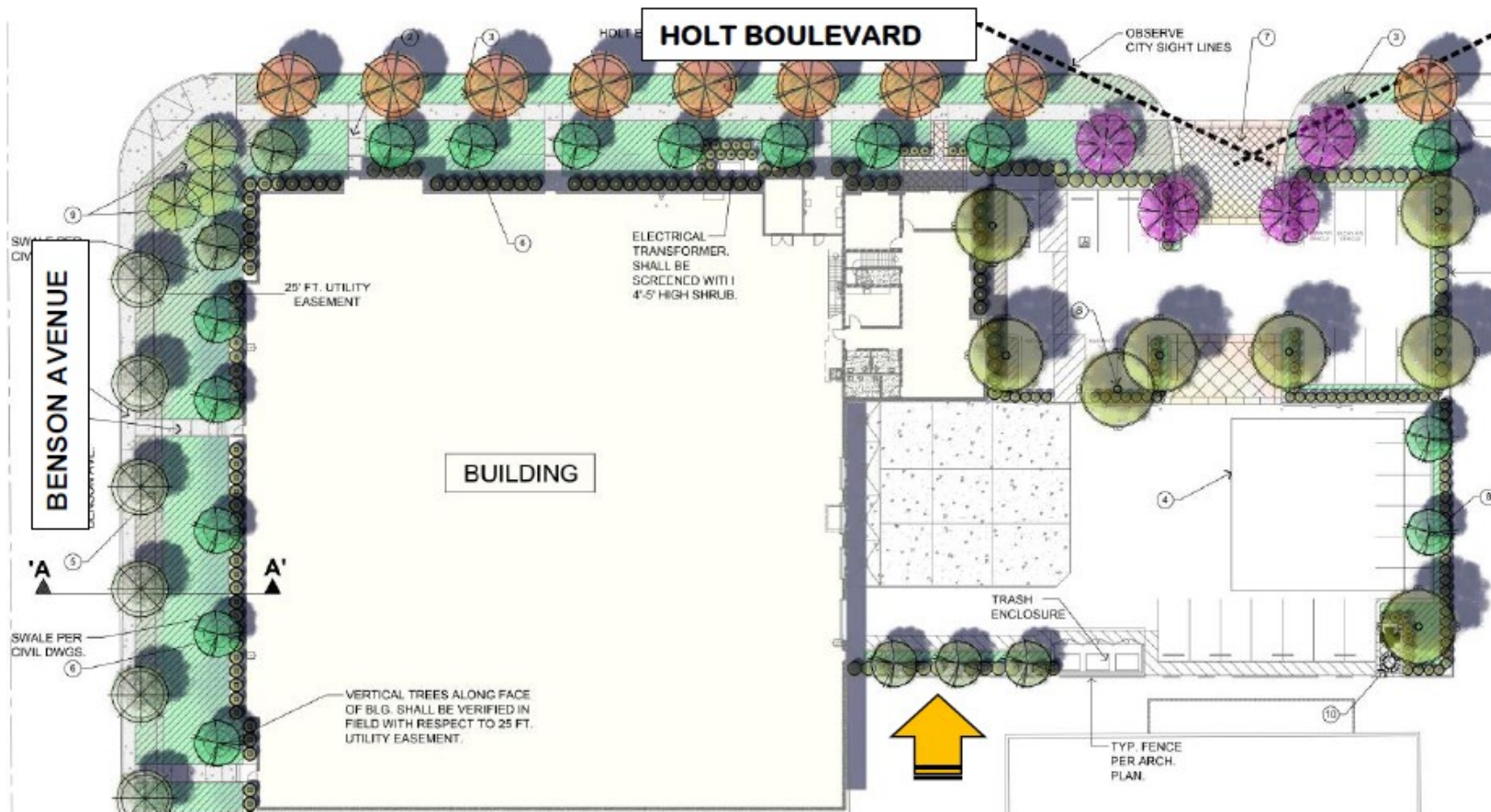




**Exhibit B—SITE PLAN**



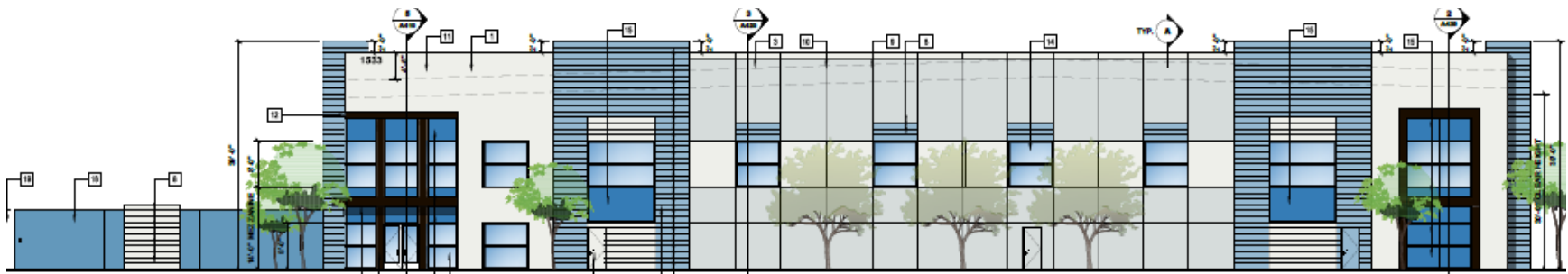
**Exhibit C—LANDSCAPE PLAN**



***Exhibit D—COLOR PERSPECTIVE***



**Exhibit E—EXTERIOR ELEVATIONS**

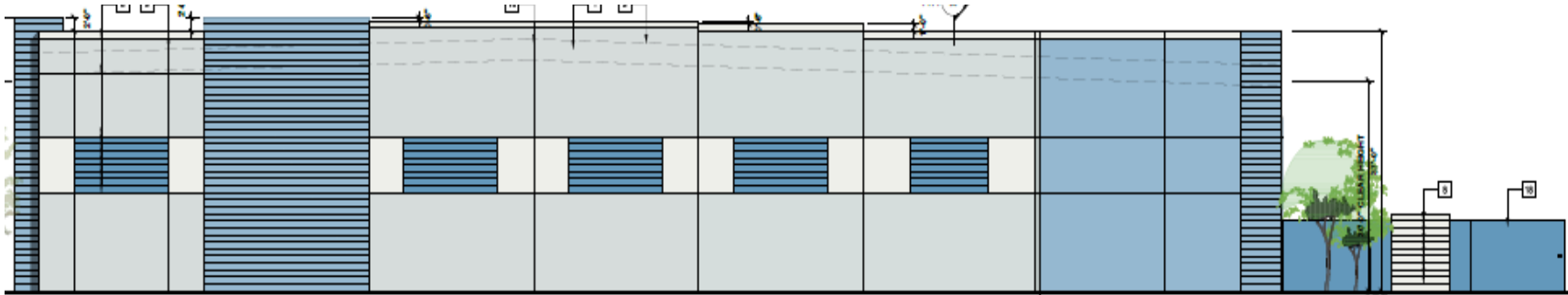


**NORTH ELVATION (HOLT BOULEVARD)**

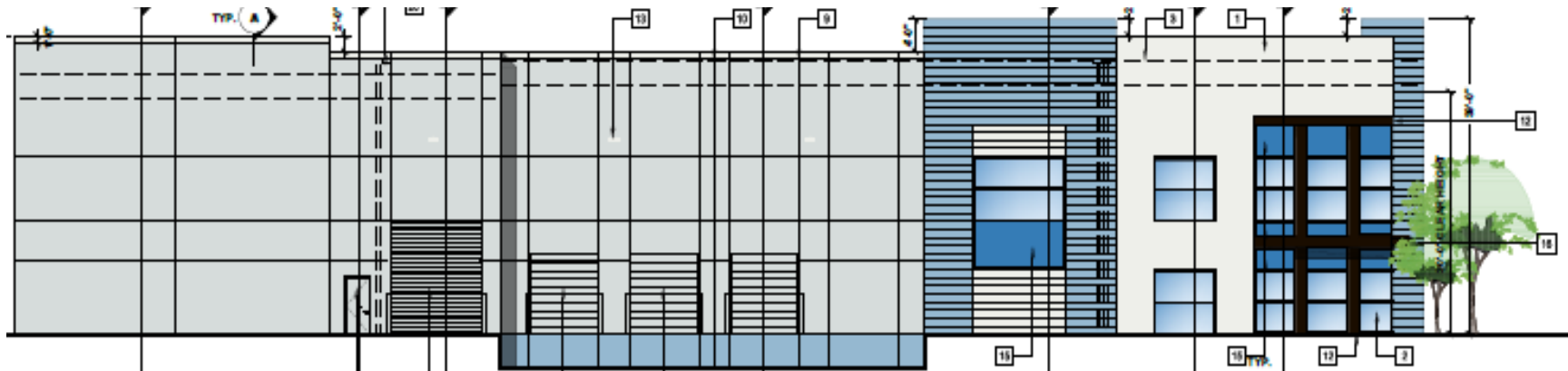


**WEST ELVATION (BENSON AVENUE)**

**Exhibit F—EXTERIOR ELEVATIONS**



**SOUTH ELEVATION (REAR)**



**EAST ELEVATION**

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV19-006, A DEVELOPMENT PLAN TO CONSTRUCT A 35,737 SQUARE FOOT INDUSTRIAL BUILDING ON 1.48-ACES OF LAND LOCATED AT 1533 WEST HOLT BOULEVARD, WITHIN THE IP (INDUSTRIAL PARK) ZONING DISTRICT AND MAKING FINDINGS IN SUPPORT THEREOF— APN: 1011-111-10.

WHEREAS, UNITED TRUST REALTY CORPORATION (hereinafter referred to as "Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV19-006, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 1.48-acres of land generally located along the southeast corner of Holt Boulevard and Benson Avenue, at 1533 West Holt Boulevard within the IP (Industrial Park) zone, and is presently vacant; and

WHEREAS, the property to the north of the Project site, (across Holt Boulevard) is within the HDR-45 (25.1 to 45.0 Units/Acre) zoning district and is developed with a restaurant and liquor store. The property to the east is within the IP (Industrial Park) zoning district and is developed with industrial land uses. The property to the south is within the IP (Industrial Park) zoning district and is developed with industrial uses. The property to the west (across Benson Avenue) is within the City of Montclair and is developed with a used car dealership; and

WHEREAS, the Applicant is requesting Development Plan approval to construct an industrial building totaling 35,737 square feet and having a floor area ratio (FAR) of 0.55. The proposed building is situated at the northwest corner of the site and is setback 20 feet from the Holt Boulevard (front) property line (10 feet will be dedicated to the City for the future widening of Holt Boulevard), 23 feet from the Benson Avenue (street side) property line, 0-feet from the south (rear) property line, and 135 feet from the east property line; and

WHEREAS, the Project is proposed with one point of vehicular access along the Holt Boulevard frontage. The proposed driveway provides full access; however, because Holt Boulevard is designated as a divided arterial in the City's Master Plan of Streets and Highways, the driveway will be limited to right-in/right-out access when the future median is installed; and

WHEREAS, the Project has provided off-street parking pursuant to the "Warehouse and Distribution Speculative" parking standards specified in the Ontario Development Code. The building requires a total of 28 parking spaces and one truck

parking space, and 28 parking spaces and one truck parking space has been provided, in compliance with the minimum parking requirements. Therefore, no parking shortage is anticipated; and

WHEREAS, the proposed industrial warehouse building will be of concrete tilt-up construction. The Project is proposing a contemporary architectural style that exemplifies the high-quality architecture promoted by the Ontario Development Code and The Ontario Plan ("TOP"). Special attention has been given to the use of color, massing, building form, materials, and architectural details; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act, commencing with Public Resources Code Section 21000 (hereinafter referred to as "CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 18, 2020, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB20-025, recommending that the Planning Commission approve the Application; and

WHEREAS, on May 26, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

**SECTION 1: Environmental Determination and Findings.** As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from environmental review pursuant to Section 15332 (Class-32, In-Fill Development Projects) of the CEQA Guidelines, which consists of projects characterized as infill development, and meets all of the following conditions: (a) the Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations; (b) the proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses; (c) the Project site has no value as habitat for endangered, rare, or threatened species; (d) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and (e) the Project site can be adequately served by all required utilities and public services; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.



**SECTION 2: *Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

**SECTION 3: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

**SECTION 4: *Concluding Facts and Reasons.*** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the Business Park district of the Policy Plan Land Use Map, and the IP (Industrial Park) zoning district. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. The proposed 35,737 square foot industrial

building development is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the Project will contribute to the establishment of a dynamic, progressive city containing distinct neighborhoods and districts that foster a positive sense of identity and belonging among residents, visitors, and businesses (Goal CD1). Furthermore, the Project will promote the City's policy to take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods (Policy CD1-1); and

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the IP (Industrial Park) zoning district, including standards relative to the particular land use proposed (35,737 square foot industrial building on 1.48-acres), as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions. The Project is proposing a FAR of 0.55, which is compatible with other similar industrial projects that have been reviewed, and approved by the Development Advisory Board and/or Planning Commission; and

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Planning Commission has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Development Code are maintained; [ii] the Project will not endanger the public health, safety or general welfare; [iii] the Project will not result in any significant environmental impacts; [iv] the Project will be in harmony with the area in which it is located; and [v] the Project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan; and

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.*** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Development Code that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (Industrial Building). As a result of this review, the Planning Commission has determined that the Project, when implemented in conjunction with the conditions of approval, will be

consistent with the development standards and guidelines described in the Development Code.

**SECTION 5: Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

**SECTION 6: Indemnification.** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

**SECTION 7: Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

**SECTION 8: Certification to Adoption.** The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of May 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

---

Jim Willoughby  
Planning Commission Chairman

ATTEST:

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Cathy Wahlstrom  
Planning Director and  
Secretary to the Planning Commission

STATE OF CALIFORNIA                    )  
COUNTY OF SAN BERNARDINO        )  
CITY OF ONTARIO                        )

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. \_\_\_\_\_ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 26, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

---

Gwen Berendsen  
Secretary Pro Tempore

**ATTACHMENT A:**

**File No. PDEV19-006  
Departmental Conditions of Approval**

*(Departmental conditions of approval to follow this page)*



City of Ontario  
Planning Department  
303 East B Street  
Ontario, California 91764  
Phone: 909.395.2036  
Fax: 909.395.2420

## ***Planning Department Land Development Division Conditions of Approval***

**Meeting Date:** May 26, 2020

**File No:** PDEV19-006

**Related Files:** n/a

**Project Description:** A Development Plan to construct a 35,737 square foot industrial building on 1.48-acres of land located at 1533 W. Holt Boulevard, within the Industrial Park (IP) zoning district (APN: 01011-111-10); **submitted by United Trust Realty Corporation.**

**Prepared By:** Luis E. Batres, Senior Planner  
Phone: 909.395.2431 (direct)  
Email: [Lbatres@ontarioca.gov](mailto:Lbatres@ontarioca.gov)

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

**1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

**2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

**2.1 Time Limits.**

(a) Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

**2.2 General Requirements.** The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

**2.3**     Landscaping.

(a)     The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b)     Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c)     Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d)     Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

**2.4**     Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

**2.5**     Parking, Circulation and Access.

(a)     The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b)     All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c)     Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d)     The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(e)     Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(f)     Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

**2.6**     Outdoor Loading and Storage Areas.

(a)     Loading facilities shall be designed and constructed pursuant to Development Code Division 6.03 (Off-Street Parking and Loading).

(b)     Areas designated for off-street parking, loading, and vehicular circulation and maneuvering, shall not be used for the outdoor storage of materials or equipment.



(c) Outdoor loading and storage areas, and loading doors, shall be screened from public view pursuant to the requirements of Development Code Paragraph 6.02.025.A.2 (Screening of Outdoor Loading and Storage Areas, and Loading Doors) Et Seq.

(d) Outdoor loading and storage areas shall be provided with gates that are view-obstructing by one of the following methods:

(i) Construct gates with a perforated metal sheet affixed to the inside of the gate surface (50 percent screen); or

(ii) Construct gates with minimum one-inch square tube steel pickets spaced at maximum 2-inches apart.

(e) The minimum gate height for screen wall openings shall be established based upon the corresponding wall height, as follows:

<b>Screen Wall Height</b>	<b>Minimum Gate Height</b>
14 feet:	10 feet
12 feet:	9 feet
10 feet:	8 feet
8 feet:	8 feet
6 feet:	6 feet

## 2.7 Site Lighting.

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

(c) Applicant shall work with staff during the plan check process to incorporate some decorative up and down lighting at key architectural and landscape areas of the project.

## 2.8 Mechanical and Rooftop Equipment.

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

2.9 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

**2.10** Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

**2.11** Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

**2.12** Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements.

(a) CC&Rs shall be prepared for the Project and shall be recorded prior to the issuance of a building permit.

(b) The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owners association and the CC&Rs shall be reviewed and approved by the City.

(c) CC&Rs shall ensure reciprocal parking and access between parcels.

(d) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:

(i) Landscaping and irrigation systems within common areas;

(ii) Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02;

(iii) Shared parking facilities and access drives; and

(iv) Utility and drainage easements.

(e) CC&Rs shall include authorization for the City's local law enforcement officers to enforce City and State traffic and penal codes within the project area.

(f) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.

(g) A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

**2.13** Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

(ii) Some of the property adjacent to this tract is zoned for agricultural uses and there could be fly, odor, or related problems due to the proximity of animals.

(iii) The area south of Riverside Drive lies within the San Bernardino County Agricultural Preserve. Dairies currently existing in that area are likely to remain for the foreseeable future.

(iv) This tract is part of a Landscape Maintenance District. The homeowner(s) will be assessed through their property taxes for the continuing maintenance of the district.

**2.14** Environmental Review.

(a) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(b) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

**2.15** Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

**2.16** Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

**2.17** Additional Requirements.

(a) Applicant shall work with staff during the plan check process to pop-out or inset portions of the building along the Benson Avenue frontage, so that the two architectural towers project out and or are inset into the building footprint to match the north architectural towers.

(b) Decorative steel canopies shall be incorporated on all architectural towers along Holt Boulevard and Benson Avenue to match the color perspective that was submitted with the project.

(c) Decorative paving shall be provided outside of the office plaza area and shall extend out to the public sidewalk and adjacent parking lot. Paving shall incorporate an attractive design.

(d) All architectural towers shall be enclosed on all four sides if the back sides will be visible from public views. Treatment shall also be provided on all sides.



**ENGINEERING DEPARTMENT  
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario  
Municipal Utilities Company and Information Technology & Management Services Department Conditions incorporated)

<input checked="" type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input type="checkbox"/> TRACT MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES
<b>PROJECT FILE NO. PDEV19-006</b>  <b>RELATED FILE NO(S). LLA19-001</b>	
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: ___/___/___	

**CITY PROJECT ENGINEER & PHONE NO:** Michael Bhatanawin, P.E. (909) 395-2130

**CITY PROJECT PLANNER & PHONE NO:** Luis Batres (909) 395-2431

**DAB MEETING DATE:** May 18, 2020

**PROJECT NAME / DESCRIPTION:** A Development Plan to construct a 33,489 square foot industrial building, on 1.51 acres of land within the IP (Industrial park) zoning district

**LOCATION:** Southeast corner of Holt Boulevard and Benson Avenue

**APPLICANT:** United Trust Realty Corporation

**REVIEWED BY:**  4/28/20  
 Bryan Lirley, P.E.      Date  
 Principal Engineer

**APPROVED BY:**  4/28/20  
 Raymond Lee, P.E.      Date  
 Assistant City Engineer



**THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.**

**1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL:** Check When Complete

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:   
\_\_\_\_\_ feet on \_\_\_\_\_  
Property line corner 'cut-back' required at the intersection of \_\_\_\_\_  
and \_\_\_\_\_.
- 1.02 Dedicate to the City of Ontario, the following easement(s): \_\_\_\_\_   
\_\_\_\_\_
- 1.03 Restrict vehicular access to the site as follows: \_\_\_\_\_
- 1.04 Vacate the following street(s) and/or easement(s): \_\_\_\_\_
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.
- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.
- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).
- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Management Services Department at (909) 395-2124 regarding this requirement.   
(1) \_\_\_\_\_  
(2) \_\_\_\_\_
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.



- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: [www.ci.ontario.ca.us](http://www.ci.ontario.ca.us)) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Management Services at (909) 395-2353 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments: 
  - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
  - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
  - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: \_\_\_\_\_

**2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:**

**A. GENERAL  
 ( Permits Includes Grading, Building, Demolition and Encroachment )**

- 2.01 Record Parcel Map/Tract Map No. \_\_\_\_\_ pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a duplicate photo mylar of the recorded map to the City Engineer's office.
- 2.03 **Note that the subject parcel is a recognized parcel in the City of Ontario per the Nemaha Tract per map recorded in Book 11, Page 47 of maps, in the office of the County Recorder of the County of San Bernardino.**
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of \_\_\_\_\_ .
- 2.05 Apply for a:  Certificate of Compliance with a Record of Survey;  Lot Line Adjustment 
  - Make a Dedication of Easement.
- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan ( WQMP), as applicable to the project.



- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).
- 2.08 **Submit a soils/geology report.**
- 2.09 Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:
- State of California Department of Transportation (Caltrans)
  - San Bernardino County Road Department (SBCRD)
  - San Bernardino County Flood Control District (SBCFCD)
  - Federal Emergency Management Agency (FEMA)
  - Cucamonga Valley Water District (CVWD) for sewer/water service
  - United States Army Corps of Engineers (USACE)
  - California Department of Fish & Game
  - Inland Empire Utilities Agency (IEUA)
  - Other: \_\_\_\_\_
- 2.10 **Dedicate to the City of Ontario the right-of-way described below:**
1. **Holt Boulevard to the half ultimate right-of-way width of 60 feet along the project frontage**
  2. **Benson Avenue to the ultimate right-of-way width of 44 feet along the project frontage**
  3. **Property line corner 'cut-back' required at the intersection of Holt Boulevard and Benson Avenue**
- 2.11 Dedicate to the City of Ontario the following easement(s): \_\_\_\_\_
- \_\_\_\_\_
- 2.12 **New Model Colony (NMC) Developments:**
- 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
  - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.
  - 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall).
- 2.13 **Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.**



- 2.14 The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.
- 2.15 Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately \$33,809, shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.
- 2.16 Other conditions: \_\_\_\_\_





**B. PUBLIC IMPROVEMENTS**  
 (See attached Exhibit 'A' for plan check submittal requirements.)

**2.17** Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Holt Boulevard	Benson Avenue	Street 3	Street 4
<b>Curb and Gutter</b>	<input checked="" type="checkbox"/> New; 36 ft. from C/L (A) <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
<b>AC Pavement</b>	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
<b>PCC Pavement (Truck Route Only)</b>	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Drive Approach</b>	<input checked="" type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace (A)	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Sidewalk</b>	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace (A, B)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>ADA Access Ramp</b>	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace (C)	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace (C)	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Parkway</b>	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
<b>Raised Landscaped Median</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Fire Hydrant</b>	<input checked="" type="checkbox"/> Upgrade (see Sec. 2.D) <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation



<b>Sewer</b> (see Sec. 2.C)	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
<b>Water</b> (see Sec. 2.D)	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Street Light</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2.G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
<b>Fiber Optics</b> (see Sec. 2.K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____	_____	_____
Other Improvements	_____	_____	_____	_____

**Specific notes for Improvements listed in item no. 2.17, above:**

- A. Remove existing drive approach on Holt Blvd and replace with curb, gutter and sidewalk per City standards.
- B. Remove existing asphalt sidewalk on Holt Blvd and replace with concrete sidewalk per City standards.
- C. Remove and replace the existing sidewalk ramp at the SEC of Holt Blvd & Benson Ave to meet current City standards. This includes but is not limited to relocating traffic signal appurtenances (e.g. pullbox) outside the ramp to accommodate the new ramp configuration.



- 2.18 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s): \_\_\_\_\_
- 2.19 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.
- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide  water service  sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 **Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately \$63,798, for undergrounding of utilities in Benson Avenue along the project frontage in accordance with Section 7-7.303.e of the City's Municipal Code (\$350 / LF and for 182.78 LF Benson Ave frontage).**
- 2.22 **Other conditions:** 
  - A. Slurry seal pavement on Holt Boulevard and Benson Avenue from centerline to curb and gutter along project frontage

**C. SEWER**

- 2.23 **A 8 inch sewer main is available for connection by this project in Holt Boulevard. (Ref: Sewer plan bar code: S11220)**
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 Other conditions: \_\_\_\_\_

**D. WATER**

- 2.27 **A 10 inch water main is available for connection by this project in Holt Boulevard (Ref: Water plan bar code: W12028)**
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.
- 2.29 **Other conditions:** 
  - A. Relocate existing water main to be outside of the proposed sidewalk and into the street, and upgrade it from the 14" to a 16" water main in Benson Avenue from the tee at the intersection of Benson and Holt to the southerly project property line. Also, use two-45 degrees elbows on each end to transition back to the tee and the southerly transition to the existing 14" water main OR Developer to pay an in-lieu fee for the work aforementioned directly above in the amount of \$36,000 which were agreed upon between the developer and OMUC.
  - B. Upgrade the existing two fire hydrants in Holt Blvd with a break-off check valve per City standards.

**E. RECYCLED WATER**

- 2.30 A \_\_\_\_\_ inch recycled water main is available for connection by this project in \_\_\_\_\_. (Ref: Recycled Water plan bar code: \_\_\_\_\_)
- 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.



- 2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.
  - 2.33 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.
- Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.
- 2.34 Other conditions: \_\_\_\_\_

**F. TRAFFIC / TRANSPORTATION**

- 2.35 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer: 
  - 1. On-site and off-site circulation
  - 2. Traffic level of service (LOS) at 'build-out' and future years
  - 3. Impact at specific intersections as selected by the City Engineer
- 2.36 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.
- 2.37 Other conditions: 
  - A. Holt Boulevard is designated as a divided arterial in the City's Master Plan of Streets and Highways. The proposed driveway on Holt Blvd shall be limited to right-turn access only when the future median is installed.
  - B. Design and construct in-fill public street lights along project frontages of Holt Blvd and Benson Ave. Street lighting shall be LED-type and in accordance with City's Traffic and Transportation Design Guidelines.
  - C. Design and construct proposed driveways in accordance with City Std. No. 1204.
  - D. Holt Blvd and Benson Ave shall be signed "No Parking Anytime".
  - E. All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City Std. No. 1309.
  - F. The Applicant/Developer's engineer-of-record shall meet with City Engineering staff prior to starting street lighting design plans.

**G. DRAINAGE / HYDROLOGY**

- 2.38 A \_\_\_\_\_ inch storm drain main is available to accept flows from this project in \_\_\_\_\_ (Ref: Storm Drain plan bar code: \_\_\_\_\_)
- 2.39 Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.
- 2.40 An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.
- 2.41 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.



- 2.42 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.43 Other conditions: 
  - A. Pay an in-lieu fee, approximately \$159,000, for the construction of the proposed 96" storm drain line in Benson Avenue along the project frontage.

**H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)**

- 2.44 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.  
If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.  
Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.45 Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.
- 2.46 Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.
- 2.47 Other conditions: \_\_\_\_\_

**J. SPECIAL DISTRICTS**

- 2.48 File an application, together with an initial payment deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community facilities District Act of 1982. The application and fee shall be submitted a minimum four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact the Financial Services Department at (909) 395-2353 to initiate the CFD application process.
- 2.49 Other conditions: \_\_\_\_\_

**K. FIBER OPTIC**

- 2.50 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole.



- 2.51 Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Information Technology Department at (909) 395-2000, regarding this requirement.

#### L. SOLID WASTE

- 2.52 Onsite solid waste shall be designed in accordance with the City's Solid Waste Manual location at:

<http://www.ontarioca.gov/municipal-utilities-company/solid-waste>

- 2.53 Other conditions:

- A. Prior to approval of the any building permits, the Conceptual Solid Waste Handling Plan needs to be updated and converted in to a Final Solid Waste Handling Plan Sheet and shall be submitted accompanying the Precise Grading Plan Submittal to the City/OMUC for review and approval. See "Solid Waste Handling Plan (SWHP) Requirements" document for details.
- B. The applicant shall submit an Integrated Waste Management Report for review and approval with the Precise Grading Plan. This report shall address the management of all integrated waste (Refuse, Recycling, Organics, etc.). The IWMR shall demonstrate compliance with the "Integrated Waste Management Report Requirements" document.

#### 3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.

- 3.02 Complete all requirements for recycled water usage.

1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.

2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.

3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.

- 3.03 The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.

- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.

- 3.05 Confirm payment of all Development Impact Fees (DIF) to the Building Department.

- 3.06 Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).

#### 4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 Complete all Conditions of Approval listed under Sections 1-3 above.



- 4.02 Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.
- 4.03 The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.



**EXHIBIT 'A'**

**ENGINEERING DEPARTMENT  
First Plan Check Submittal Checklist**

---

Project Number: PDEV19-006, and/or Parcel Map/Tract Map No. \_\_\_\_\_

**The following items are required to be included with the first plan check submittal:**

1.  **A copy of this check list**
2.  **Payment of fee for Plan Checking**
3.  **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4.  **One (1) copy of project Conditions of Approval**
5.  **Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).**
6.  **Three (3) sets of Public Street Improvement plan with street cross-sections**
7.  **Three (3) sets of Private Street improvement plan with street cross-sections**
8.  **Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
9.  **Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)**
10.  **Four (4) sets of Public Sewer Improvement plan**
11.  **Five (5) sets of Public Storm Drain improvement plan**
12.  **Three (3) sets of Public Street Light improvement plan**
13.  **Three (3) sets of Signing and Striping improvement plan**
14.  **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15.  **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
16.  **Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.**
17.  **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
18.  **One (1) copy of Hydrology/Drainage study**
19.  **One (1) copy of Soils/Geology report**
20.  **Payment for Final Map/Parcel Map processing fee**
21.  **Three (3) copies of Final Map/Parcel Map**






- 22.  One (1) copy of approved Tentative Map
- 23.  One (1) copy of Preliminary Title Report (current within 30 days)
- 24.  One (1) copy of Traverse Closure Calculations
- 25.  One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.
- 26.  Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 27.  Other: \_\_\_\_\_

**CITY OF ONTARIO**  
**LANDSCAPE PLANNING DIVISION**  
 303 East "B" Street, Ontario, CA 91764

**CONDITIONS OF APPROVAL**

Sign Off

  
 Jamie Richardson, Sr. Landscape Planner

07/02/2019  
 Date

Reviewer's Name:  
**Jamie Richardson, Sr. Landscape Planner**

Phone:  
**(909) 395-2615**

D.A.B. File No.:  
 PDEV19-006

Case Planner:  
 Luis Batres

Project Name and Location:  
 Holt and Benson Industrial  
 1533 W Holt Blvd  
 Applicant/Representative:  
 United Trust Realty Henry Hong/ Ignacio Crespo  
 371 Evergreen Dr.  
 Brea Ca 92821

<input checked="" type="checkbox"/>	<b>A Preliminary Landscape Plan (dated 2/5/2020) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.</b>
<input type="checkbox"/>	<b>A Preliminary Landscape Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.</b>

**A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE**

1. Call out utilities on grading and landscape plans
2. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council. Fees are:
  - Plan Check—less than 5 acres .....\$1,301.00
  - Inspection—Construction (up to 3 inspections per phase).....\$278.00
  - Total.....\$1,579.00
  - Inspection—Field – any additional..... \$83.00

Landscape construction plans with building permit number for plan check may be emailed to:  
[landscapeplancheck@ontarioca.gov](mailto:landscapeplancheck@ontarioca.gov)

**Civil/Site Plans**

3. Civil plan: Show planter areas per site plan (east of office building).
4. Show backflow devices set back 4' from paving all sides. Locate on level grade. Show backflows on private property not in the right of way.
5. Locate utilities including light standards, fire hydrants, and water, drain and sewer lines to not conflict with required tree locations; provide utility plan or show on site /civil plans.
6. Dimension all planters to have a minimum 5' wide inside dimension.
7. Identify what type of utilities are or will be located within the easement along Benson Ave.

**Landscape Plans**

8. Western perimeter calls out for swale per civil, however civil plans show a slope *not* a swale. Coordinate with civil to configure western perimeter.
9. If swale, outline edges, utilize arrows to demonstrate water flow, and ensure it is less than 40% of the perimeter's landscape area.
10. Show backflow devices with 36" high strappy leaf shrub screening and trash enclosures and transformers, a 4'-5' high evergreen hedge screening. Screen backflows and utilities with shrub masses. Continue the areas of shrub massing in the adjacent landscape areas.
11. Show all utilities on the landscape plans. Coordinate so utilities are clear of tree locations. Show light stds. 15' clear of required tree locations. Provide utility plan or show on site/civil plans.

12. Add small tree at parking row ends near building (do not use Rhus), consider *Podocarpus gracilior* near building.
13. Provide broad canopy trees in planters adjacent to parking spaces, consider *Quercus ilex*.
14. Provide larger accent trees at entry (consider *Quercus agrifolia* or *suber*). Replace *Magnolia* 'Little Gem' with *Cercis canadensis* 'Oklahoma' or *Tristania laurina*.
15. Show parkway landscape max 18" high: such as *Baccharis*, *Lonicera*, *Kurapia* etc. and street trees spaced 30' apart. Alternate background trees so future mature tree growth does not interfere with adjacent canopies.
16. Street trees for this project are: *Fraxinus oxycarpa* 'Raywood' on Holt and *Cercis Canadensis* 'Oklahoma' on Benson or *Tristania laurina*
17. Show 8' diameter of mulch only at new trees. Detail irrigation dripline outside of root zone.
18. Call out all fences and walls, materials and heights. Show mowstrips where fences end at PL.
19. Landscape construction plans shall meet the requirements of the Landscape Development Guidelines. See <http://www.ontarioca.gov/landscape-planning/standards>



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Luis Batres, Senior Planner  
Planning Department

**FROM:** Paul Ehrman, Deputy Fire Chief/Fire Marshal  
Fire Department

**DATE:** February 5, 2019

**SUBJECT:** PDEV19-006 - A Development Plan to construct a 35,435 square foot concrete tilt-up industrial building on 1.51 acres of land within the IP, Industrial Park zone located at the south east corner of Holt Boulevard and Benson Avenue at 1533 W. Holt Boulevard. APN: 1011-111-10 Related File: LLA19-001

- 
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.

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### **SITE AND BUILDING FEATURES:**

- A. 2016 CBC Type of Construction: Tyoe II B
- B. Type of Roof Materials: Panelized
- C. Ground Floor Area(s): 33,323 Sq. Ft.
- D. Number of Stories: 2
- E. Total Square Footage: 35,435 Sq. Ft.
- F. 2016 CBC Occupancy Classification(s): S, B

## **CONDITIONS OF APPROVAL:**

### **1.0 GENERAL**

- 1.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at [www.ontarioca.gov](http://www.ontarioca.gov), click on "Fire Department" and then on "Standards and Forms."
- 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

### **2.0 FIRE DEPARTMENT ACCESS**

- 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25') inside and forty-five feet (45') outside turning radius per Standard #B-005.
- 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150') in length shall have an approved turn-around per Standard #B-002.
- 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-six (26) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services..

### 3.0 WATER SUPPLY

- 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 2000 gallons per minute (g.p.m.) for 4 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

### 4.0 FIRE PROTECTION SYSTEMS

- 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard ~~Choose an item.~~ All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- 4.5 Fire Department Connections (FDC) shall be located on the address side of the building within one hundred fifty feet (150') of a public fire hydrant on the same side of the street. Provide identification for all fire sprinkler control valves and fire department connections per Standard #D-007. Raised curbs adjacent to Fire Department connection(s) shall be painted red, five feet either side, per City standards.
- 4.6 A fire alarm system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 72. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- 4.7 Portable fire extinguishers are required to be installed prior to occupancy per Standard #C-001. Please contact the Fire Prevention Bureau to determine the exact number, type and placement required.

## 5.0 BUILDING CONSTRUCTION FEATURES

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.
- ☒ 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.

## 6.0 OTHER SPECIAL USES

- ☒ 6.1 The storage, use, dispensing, or handling of any hazardous materials shall be approved by the Fire Department, and adequate fire protection features shall be required. If hazardous materials are proposed, a Fire Department Hazardous Materials Information Packet, including Disclosure Form and Information Worksheet, shall be completed and submitted with Material Safety Data Sheets to the Fire Department along with building construction plans.
- ☒ 6.2 Any High Piled Storage, or storage of combustible materials greater than twelve (12') feet in height for ordinary (Class I-IV) commodities or storage greater than six feet (6') in height of high hazard (Group A plastics, rubber tires, flammable liquids, etc.) shall be approved by the Fire Department, and adequate fire protection features shall be required. If High Piled Storage is proposed, a Fire Department High Piled Storage Worksheet shall be completed and detailed racking plans or floor plans submitted prior to occupancy of the building.
- ☒ 6.3 Underground fuel tanks, their associated piping and dispensers shall be reviewed, approved, and permitted by Ontario Building Department, Ontario Fire Department, and San Bernardino County Fire Department Hazardous Materials Division. In fueling facilities, an exterior emergency pump shut-off switch shall be provided.



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Luis Batres, Planning Department

**FROM:** Douglas Sorel, Police Department

**DATE:** February 4, 2019

**SUBJECT:** PDEV19-006 – A DEVELOPMENT PLAN TO CONSTRUCT AN INDUSTRIAL BUILDING AT THE SOUTHEAST CORNER OF BENSON AVE. AND HOLT BLVD.

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The “Standard Conditions of Approval” contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including, but not limited to, the requirements below.

- Required lighting for walkways, driveways, doorways, parking lots, hallways and other areas used by the public shall be provided. Lights shall operate via photosensor. Photometrics shall be provided to the Police Department and include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the buildings as stated in the Standard Conditions. The numbers shall be at a minimum 3 feet tall and 1 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.

The Applicant is invited to contact Douglas Sorel at (909) 408-1873 with any questions or concerns regarding these conditions.



# AIRPORT LAND USE COMPATIBILITY PLANNING

## CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV19-006  
 Address: 1533 W Holt Blvd  
 APN: 1011-111-10  
 Existing Land Use: Vacant  
 Proposed Land Use: Construct a 35,435 SF Industrial Building  
 Site Acreage: 1.51 acres Proposed Structure Height: 39 ft  
 ONT-IAC Project Review: n/a  
 Airport Influence Area: ONT

Reviewed By: Lorena Mejia  
 Contact Info: 909-395-2276  
 Project Planner: Luis Batres  
 Date: 3/4/19  
 CD No.: 2019-002  
 PALU No.: n/a

### The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="radio"/> High Terrain Zone	<input type="radio"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input checked="" type="checkbox"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input type="radio"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input type="radio"/> Real Estate Transaction Disclosure
<input type="radio"/> Zone 3	<input checked="" type="checkbox"/> 60 - 65 dB CNEL	<input type="radio"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: 200 ft plus	
<input type="radio"/> Zone 5			

### The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1   
  Zone 2   
  Zone 3   
  Zone 4   
  Zone 5   
  Zone 6  
 Allowable Height: \_\_\_\_\_

## CONSISTENCY DETERMINATION

This proposed Project is:  Exempt from the ALUCP     Consistent     Consistent with Conditions     Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Airport Planner Signature: \_\_\_\_\_

# CITY OF ONTARIO MEMORANDUM

**TO:** PLANNING DEPARTMENT, Luis Batres  
**FROM:** BUILDING DEPARTMENT, Kevin Shear  
**DATE:** January 23, 2019  
**SUBJECT:** PDEV19-006

- 
- The plan **does** adequately address the departmental concerns at this time.  
 No comments  
 Report below.

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### Conditions of Approval

1. Standard Conditions of Approval apply.
2. The site address will be 1533 W Holt Blvd

KS:lm



# PLANNING COMMISSION STAFF REPORT

May 26, 2020

**FILE NO.:** SB 743 VMT Thresholds

**SUBJECT:** Adoption of Vehicle Miles Traveled Thresholds (VMT) for determining significance of transportation impacts through the California Environmental Quality Act in conformance with SB 743; **City Initiated. City Council action is required.**

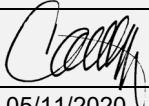
**RECOMMENDED ACTION:** That the Planning Commission consider and recommend City Council adoption of Vehicle Miles Traveled (VMT) Thresholds for Implementing the California Environmental Quality Act (CEQA) pursuant to SB 743, pursuant to the facts and reasons contained in the staff report and attached resolution.

## PROJECT ANALYSIS:

(1) Background - The California Environmental Quality Act (CEQA) is California's comprehensive environmental law. It requires public agencies to evaluate the environmental effects of a project before action is taken. CEQA also aims to prevent significant environmental effects from occurring as a result of agency actions by requiring agencies avoid or reduce, when feasible, the significant environmental impacts of their decisions.

With the adoption of SB 743 in 2013, Office of Planning and Research (OPR) was directed to examine alternative ways to analyze transportation impacts of projects during the CEQA process. OPR determined that use of Vehicle Miles Traveled (VMT) rather than Level of Service (LOS) is most suitable when analyzing the environmental impacts of a project since LOS assesses automobile delay and roadway efficiency rather than environmental impacts on transportation from projects. The use of VMT thresholds for passenger vehicle and light duty trucks was incorporated into revised state CEQA Guidelines that were adopted in December 2018. In addition, OPR adopted guidelines for use of VMT as the transportation metric to assist local governments when preparing their VMT thresholds. This VMT analysis does not apply to large trucks typically used for transporting of goods.

All cities in the State of California are required to adopt VMT thresholds for use during the CEQA process no later than July 1, 2020. The thresholds will determine what non-exempt projects may have a significant impact upon transportation. The decision on what the thresholds shall be is at the sole discretion of each jurisdiction. City staff participated in a collaborative study led by San Bernardino County Transportation Authority (SBCTA) which evaluated the tools, thresholds, and mitigation options appropriate for San

Case Planner:	Melanie Mullis, Principal Planner
Planning Director Approval:	
Submittal Date:	05/11/2020

Hearing Body	Date	Decision	Action
DAB			
PC	05/26/2020		Recommend
CC			Final

Bernardino County. This study allowed for the unique characteristics of our region to be examined and provide broad guidance to the local jurisdictions within it.

While LOS will no longer be used to assess the impacts of a project during the CEQA process, it does not preclude the City from using LOS analysis to comply with Congestion Management Plan requirements, assess a project's consistency with The Ontario Plan or to conduct project specific transportation analysis.

#### Local CEQA Guidelines

In July 2019, City Council adopted the current City CEQA Guidelines which includes provisions for using VMT thresholds. This proposed action would establish the thresholds that the City of Ontario will use during the review of projects for their environmental impacts to transportation.

These VMT Thresholds will become effective upon adoption. New projects or projects that have not circulated CEQA documents for public review before the effective date must comply with the City's new VMT Thresholds.

[2] Analysis — The proposed VMT Impact Thresholds contained in Exhibit A attached to the draft resolution identify the thresholds being proposed. The proposed VMT thresholds are based on the City's average VMT for Service Population (Residents and Employees) and includes trips both within the County and between the City and locations outside the County. The combination of the high number of jobs in Ontario, reasonable housing costs (compared to counties to the west), the presence of various regional uses such as Ontario International Airport, Ontario Mills, Toyota Arena and various large distribution facilities, and its proximity to Los Angeles, Riverside and Orange counties, make Ontario's VMT characteristics unique. Residents who work in Los Angeles or Orange counties can find more affordable housing in Ontario than locations to the west yet are within a commutable distance. Conversely, Ontario is within a reasonable commute distance from locations outside of Ontario for the more than 100,000 people who work in Ontario. This results in an average Service Population VMT higher than both the County and SCAG region because of these unique characteristics.

While Ontario's VMT for Service Population rate is higher than the County or SCAG region, The Ontario Plan land use and mobility plans concentrates balanced growth in strategic areas resulting in an overall Ontario and regional reduction in both VMT (0.2%) and GHG (0.2%). In addition, The Ontario Plan land uses have been incorporated into the County transportation model (SBTAM) which have been incorporated into SCAG's regional transportation model. As such, projects consistent with The Ontario Plan are consistent with the Regional Transportation Plan (RTP) and Sustainable Communities Strategy (SCS). Therefore, with the incorporation of the VMT reduction within The Ontario Plan land use analysis, staff is recommending that the City maintain TOP VMT reduction goal of 0.2%. Future projects consistent with TOP and in conformance with the VMT Impact Thresholds (Exhibit A) are determined to have less than significant impacts for transportation. Projects that exceed the TOP VMT Citywide average for Service

Population under General Plan Buildout conditions and/or RTP/SCS are determined to be significant and require additional CEQA analysis.

**COMPLIANCE WITH THE ONTARIO PLAN:** The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals.

- Operate in a Businesslike Manner
- Pursue City's Goals and Objectives by Working with Other Governmental Agencies

[2] Policy Plan (General Plan)

**Mobility Element:**

- Goal M1: A system of roadways that meets the mobility needs of a dynamic and prosperous Ontario.
  - M1-2 Mitigation of Impacts. We require development to mitigate its traffic impacts.

**HOUSING ELEMENT COMPLIANCE:** The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

**AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE:** The project site is located within the Airport Influence Area of the Ontario International Airport and has been found to be consistent with the policies and criteria set forth within the Ontario International Airport Land Use Compatibility Plan.

**ENVIRONMENTAL REVIEW:** The project is exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to § 15061(b)(3) (General Rule) of the CEQA Guidelines based on the fact that there is no possibility that adoption of VMT Thresholds will result in a significant effect on the environment.

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING CITY COUNCIL ADOPT “VEHICLES MILES TRAVELED” THRESHOLDS OF SIGNIFICANCE FOR PURPOSES OF ANALYZING TRANSPORTATION IMPACTS UNDER THE CALIFRONIA ENVIRONMENTAL QUALITY ACT (CEQA).

WHEREAS, the California Environmental Quality Act Guidelines (“CEQA Guidelines”) encourage public agencies to develop and publish generally applicable “thresholds of significance” to be used in determining the significance of a project’s environmental effects; and

WHEREAS, CEQA Guidelines section 15064.7(a) defines a threshold of significance as “an identifiable quantitative, qualitative or performance level of a particular environmental effect, noncompliance with which means the effect will normally be determined to be significant by the agency and compliance with which means the effect normally will be determined to be less than significant”; and

WHEREAS, CEQA Guidelines section 15064.7(b) requires that thresholds of significance must be adopted by ordinance, resolution, rule, or regulations, developed through a public review process, and be supported by substantial evidence; and

WHEREAS, pursuant to CEQA Guidelines section 15064.7(c), when adopting thresholds of significance, a public agency may consider thresholds of significance adopted or recommended by other public agencies provided that the decision of the agency is supported by substantial evidence; and

WHEREAS, Senate Bill 743, enacted in 2013 and codified in Public Resources Code section 21099, required changes to the CEQA Guidelines regarding the criteria for determining the significance of transportation impacts of projects; and

WHEREAS, in 2018, the Governor’s Office of Planning and Research (“OPR”) proposed, and the California Natural Resources Agency certified and adopted, new CEQA Guidelines section 15064.3 that identifies vehicle miles traveled (“VMT”) – meaning the amount and distance of automobile travel attributable to a project – as the most appropriate metric to evaluate a project’s transportation impacts; and

WHEREAS, as a result, automobile delay, as measured by “level of service” (“LOS”) and other similar metrics, will generally no longer constitute a significant environmental effect under CEQA; and

WHEREAS, CEQA Guidelines section 15064.3 requires agencies to stop treating automobile delay/LOS as an environmental impact effective on July 1, 2020, though public agencies may elect to be governed by this section immediately; and

WHEREAS, the City of Ontario, following a public review process consisting of staff presentations before the Planning Commission, wishes to adopt the VMT thresholds of significance for determining the significance of transportation impacts that are recommended in an analysis conducted by the San Bernardino County Transportation Authority on behalf of its member jurisdictions; and

WHEREAS, the project is exempt from the requirements of the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA") pursuant to § 15061(b)(3) (General Rule) of the CEQA Guidelines based on the fact that there is no possibility that adoption of VMT Thresholds will result in a significant effect on the environment; and

WHEREAS, on May 26, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

**SECTION 1. Environmental Determination and Findings.** As the recommending body for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

a. The Project is exempt from environmental review pursuant to Section 15601(b)(3) (General Rule) of the CEQA Guidelines; and

b. The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

c. The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

**SECTION 2. Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing,

and upon the specific findings set forth in Section 1, above, the Planning Commission hereby concludes as follows:

a. Adoption of the Vehicle Miles Traveled (VMT) Thresholds is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan; and

b. The Vehicle Miles Traveled (VMT) Thresholds would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City.

**SECTION 3. *Planning Commission Action.*** Based upon the findings and conclusions set forth in Sections 1 through 2, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL APPROVE

1. Utilizing the San Bernardino County Travel Demand Model (SBTAM) as its preferred methodology to measure VMT.
2. Utilizing the San Bernardino County Travel Demand Model (SBTAM) as its preferred method to analyze a project's VMT impact.
3. Utilizing a threshold consistent with Table 1 in Exhibit A

**SECTION 4: *Indemnification.*** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

**SECTION 5: *Custodian of Records.*** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

**SECTION 6: *Certification to Adoption.*** The Secretary shall certify to the adoption of the Resolution.

-----



The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26<sup>th</sup> day of May, 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

---

Jim Willoughby  
Planning Commission Chairman

ATTEST:

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Cathy Wahlstrom  
Planning Director and  
Secretary to the Planning Commission

STATE OF CALIFORNIA                    )  
COUNTY OF SAN BERNARDINO        )  
CITY OF ONTARIO                        )

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. PC20-\_\_\_, was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 26, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

---

Gwen Berendsen  
Secretary Pro Tempore

**Exhibit A**

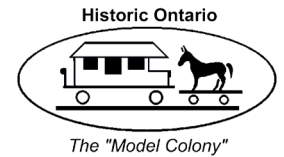
<b><u>VMT Impact Thresholds</u></b>		
<b><u>Methods</u></b>	<b><u>Project Threshold</u></b>	<b><u>Cumulative Threshold</u></b>
<b><u>Land Use Plans (such as General Plans and Specific Plans)</u></b>		
<ul style="list-style-type: none"> <li>• <b>San Bernardino Traffic Analysis Model (SBTAM) forecast of total daily VMT/SP.</b> To capture project effect, the same cumulative year population and employment growth totals should be used. The 'project' only influences land use allocation.</li> </ul>	A significant impact would occur if the project VMT/SP (for the land use plan) exceeds the Citywide average for Service Population under General Plan Buildout Conditions.	A significant impact would occur if the project caused total daily VMT within the City to be higher than the no project alternative under cumulative conditions.
<ul style="list-style-type: none"> <li>• <b>Consistency check with SCAG RTP/SCS.</b> Is the proposed project within the growth projections in the RTP/SCS?</li> </ul>	NA	A significant impact would occur if the project is determined to be inconsistent with the RTP/SCS.
<b><u>Land Use Projects</u></b>		
<ul style="list-style-type: none"> <li>• <b>Transit Priority Area (TPA) screening.</b></li> </ul>	Presumed less than significant VMT impact for projects located in TPAs if the project: <ul style="list-style-type: none"> <li>○ Has a Floor Area Ratio 0.75 or higher;</li> <li>○ Includes no more parking for residents, customers or employees than the City Development Code mandates;</li> <li>○ Is consistent with the Sustainable Communities Strategy (SCS) as determined by the City with input from SCAG or SBCTA; and</li> <li>○ If there are existing affordable residential units, replaces those residential units with at least as many similarly affordable units and not with a smaller number of moderate or high-income residential units.</li> </ul>	Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.
<ul style="list-style-type: none"> <li>• <b>Low VMT area screening.</b></li> </ul>	Presumed less than significant VMT impact for projects located in low VMT generating model traffic analysis zones (TAZs). These TAZs generate total daily VMT/SP that is 15% less than the baseline level for the County.	Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.
<ul style="list-style-type: none"> <li>• <b>Low Trip Generating Uses</b></li> </ul>	Projects below 110 Average Daily Trips (ADT) are presumed to be less than significant, such as: <ul style="list-style-type: none"> <li>○ 11 single family housing units;</li> <li>○ 16 multi-family, condominiums or townhouse housing units;</li> <li>○ 10,000 sq. ft. of office;</li> <li>○ 15,000 sq. ft. of light industrial;</li> </ul>	Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.

<b>VMT Impact Thresholds</b>		
<b><u>Methods</u></b>	<b><u>Project Threshold</u></b>	<b><u>Cumulative Threshold</u></b>
	<ul style="list-style-type: none"> <li>o 63,000 sq. ft. of warehousing; and 79,000 sq. ft. of high cube transload and short-term storage warehouse.</li> </ul>	
<ul style="list-style-type: none"> <li>• <b>Project type screening by land use type.</b></li> </ul>	<p>Projects that meet the criteria describe below can be screened from further VMT review during the CEQA process and are presumed to have less than significant impact upon transportation (determination to be made by City staff).</p> <ol style="list-style-type: none"> <li>1. Residential, office, retail or mix of these uses within ½ mile of an existing major transit stop or a stop along a high-quality transit corridor as long as the use:             <ol style="list-style-type: none"> <li>a. Has at least 0.75 FAR;</li> <li>b. Provides no more parking than the Development Code requires; and</li> <li>c. Is consistent with the RTP/SCS and The Ontario Plan.</li> </ol> </li> <li>2. Local serving retail:             <ol style="list-style-type: none"> <li>a. Not greater than 50,000 sq. ft. in size</li> <li>b. Projects with a Neighborhood Commercial TOP Land Use designation since this designation is intended to be local serving (Neighborhood Commercial is defined as “Local serving retail, personal service, office, and dining uses, typically located within a predominantly residential neighborhood”).</li> </ol> </li> <li>3. Certain Transportation projects that do not add vehicle capacity such as:             <ol style="list-style-type: none"> <li>a. Roadway maintenance;</li> <li>b. Right or left turn lanes (no through traffic);</li> <li>c. Addition of transit only lanes;</li> <li>d. Traffic signal timing to optimize vehicle, bicycle or pedestrian flow;</li> <li>e. Installation of roundabouts or traffic circles;</li> <li>f. Installation of traffic calming devices; and</li> <li>g. Installation of bicycle or pedestrian facilities and other active transportation projects.</li> </ol> </li> <li>4. Local Serving K – 12 Public Schools (not magnet or charter schools);</li> <li>5. Local/Neighborhood Parks;</li> <li>6. Daycare / Childcare/Pre-Kindergarten;</li> <li>7. Affordable or Supportive Housing;</li> <li>8. Student Housing on or adjacent to college campuses;</li> <li>9. Community Institutions (e.g. Public Library, Fire Station, local government facilities);</li> <li>10. Senior Housing (as defined by HUD) or Assisted Living facilities; HUD defines Senior Housing as projects that are:</li> </ol>	<p>Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.</p>

<b>VMT Impact Thresholds</b>		
<b><u>Methods</u></b>	<b><u>Project Threshold</u></b>	<b><u>Cumulative Threshold</u></b>
	a. Provided under any state or federal program that the Secretary of HUD has determined to be specifically designed and operated to assist elderly persons (as defined in the state or federal program); b. Intended for, and solely occupied by persons 62 years of age or older; or c. Intended and operated for occupancy by persons 55 years of age or older. (most commonly used)  11. Redevelopment of a site to residential or office that would generate fewer VMT than the existing use; and 12. Non-Destination Small Hotels (with 150 or fewer rooms and no Banquet Facilities). This is appropriate given locating small hotels in Ontario will serve the airport and local businesses and reduce the VMT of these customers from having to travel further from the airport or their business destination.	
<ul style="list-style-type: none"> <li>VMT analysis using SBTAM forecast of total daily VMT/SP.</li> </ul>	A significant impact would occur if the project VMT/SP exceeds the Citywide average for Service Population under General Plan Buildout Conditions.	A significant impact would occur if the project is determined to be inconsistent with the RTP/SCS.  A significant impact would occur if the project causes total daily VMT within the City to be higher than the no project alternative under cumulative conditions. This analysis should be performed using the 'project effect' or 'boundary' method.
<b>Transportation Projects (thresholds may apply for SB 743 or GHG purposes)</b>		
<ul style="list-style-type: none"> <li>SBTAM forecast of total citywide daily VMT<sup>1</sup></li> </ul>	A significant impact would occur if the project increased the baseline VMT within the City for Service Population.	A significant impact would occur if the project caused total daily VMT within the City to be higher than the no build alternative under cumulative conditions.
<ul style="list-style-type: none"> <li>Consistency check with SCAG RTP/SCS</li> </ul>	NA	A significant impact would occur if the project is determined to be inconsistent with the RTP/SCS.

<sup>1</sup> It is recommended that SBTAM is used to develop VMT estimates for transportation project impact assessment. However, the analyst must verify the model results for sensitivity to changes in VMT. Alternatively, if the model is not deemed appropriate, Robert Cevero's research on lane-mile elasticity and its relationship to VMT can be referenced.

# PLANNING / HISTORIC PRESERVATION COMMISSION STAFF REPORT MAY 26, 2020



**FILE NO.:** PHP20-003

**SUBJECT:** A Certificate of Appropriateness for the adaptive reuse of the William B. Fallis House (Local Historic Landmark No. 1), a 3,658 square-foot single family residential building, to a multi-modal mobility hub on 0.41 acres of land located at 122 South Vine Avenue, within the MU-1 (Downtown Mixed Use) zoning district (APN: 1049-021-19).

**PROPERTY OWNER/APPLICANT:** The City of Ontario

**RECOMMENDATION:** That the Planning/Historic Preservation Commission consider and approve File No. PHP20-003, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval contained in the attached departmental report.

**PROJECT SETTING:** The project site is comprised of 0.41 acres of land located at 122 South Vine Avenue and is depicted in Figure 1: Project Location. The site is located at the northwest corner of Vine Avenue and Brooks Street, one block south of Holt Boulevard, a future Bus Rapid Transit (BRT) route. The site is surrounded by a mix of residential, commercial and light industrial uses. To the east of the project site is Emporia Place, a 75-unit, three-story apartment complex, where the Casa Blanca Hotel and



**Figure 1: Project Location**

Case Planner:	Diane Ayala
Planning Director Approval:	
Submittal Date:	3/3/2020

Hearing Body	Date	Decision	Action
HPSC	5/14/2020	Approve	Recommend
PC/HPC	5/26/2020		Final
CC			

Developer's Row of grand Victorian styled homes previously stood. On May 18, 1993, the Ontario City Council designated the William B. Fallis House Ontario's first local historic landmark.

**ARCHITECTURAL DESCRIPTION/HISTORY:** Built in 1892, the William Barton Fallis House is a one-story, single-family home built for the Fallis family, early town settlers and well-known business owners of the Fallis Department store. As the family grew and needed more space, a second story was added in 1914, changing the roofline to a Mansard which is representative of the French Second Empire architectural style. This resulted in a truly one-of-a-kind eclectic style house in Ontario.



*The Fallis House and family, Circa 1910s*



*West Elevation (enclosed porch and mansard roof 2016)*

The house has a bell-cast mansard capped by tiled hip roof at the front part of the structure, with side shed roofs and other mansard roofs on the sides. The house has a variation of Queen Anne ornamentation and detailing, such as spool and spindle and brackets at the front porch, a five-sided window bay with cut away corners, pendants, and roof fish scale shingling design. On the interior, it retains the angled fireplace, and much of the floor plan integrity. The house remained in the Fallis family until about the 1940s when it sold to a new owner. As a response to the housing demand during the 1950s, the house was converted into multiple units and rented. The City of Ontario purchased the property in 1988 with Redevelopment Agency funds. The building was modified in the late 1990s to accommodate an office use and housed Code Enforcement operations from 2006 until 2008, at which time they moved to their current location on Emporia Street. The property has been vacant since.

**PROJECT BACKGROUND:** In 2018, the City of Ontario was awarded a \$33.25 million grant by the California Strategic Growth Council. The highly competitive Transformative Climate Communities (TCC) grant was awarded to Ontario to support the City's plans to create new economic opportunities and improve the health and well-being of residents. The plan is comprised of several projects that include modern affordable housing, multi-modal transportation, an urban greening program, an expansive rollout of solar energy, a small business incubator, and workforce and career training. The TCC project area

includes the area in and around the historic downtown core. Among the projects included in the TCC downtown plan is a network of bicycle and pedestrian facilities throughout the area.

The Historic Preservation Subcommittee reviewed this item on May 14, 2020 and recommended approval to the Planning/Historic Preservation Commission.

**PROJECT DESCRIPTION:** The Applicant is requesting a Certificate of Appropriateness (File No. PHP20-003) to allow for the adaptive reuse of the Fallis House as a multi-modal mobility hub. The hub is intended to provide a transit access point for pedestrians, bicyclists, and Omnitrans riders. The hub will include space for bike rental, maintenance/repair, retail, bike lockers and racks, and waiting areas equipped with real time messaging boards for upcoming bus routes as depicted in Exhibit A: Site Plan, attached. Staff will have access to the existing kitchen and dining room. The building and site will be brought into compliance with current American with Disabilities Act (ADA) standards, the parking lot will be improved with new pervious paving and have a new EV charging station, a new trash enclosure will be constructed, and the site will be landscaped with new California friendly drought-tolerant planting and a new drip irrigation system will be installed. All the hub's activities will occur on the first floor of the building and the exterior grounds as depicted in Exhibit B: Floor Plan, attached. A small storage area will be located on the second floor, the remainder of the second floor will not be occupied.

The Project proposes both interior and exterior alterations to the William B. Fallis House and property. The existing pool and barbeque on the southern and western portions of the site will be removed and the ground will be covered with pervious pavers to accommodate the bike rental display area, bike lockers, and offer an open space for any programmed class, training, or event. The existing deck and trellis on the south elevation of the house will be demolished and replaced with a new ADA accessible ramp and covered deck. On the west elevation, the exterior staircase leading to the second floor will be removed and the doorway will be restored to a window. A new ADA accessible ramp will be constructed, connecting the parking lot to the rear of the building and yard.

Existing large tree specimens such as Pinus Halenpensis, Jacaranda, and Strawberry Tree will remain on site and the existing Holly Oak tree will be relocated on site as well. The existing stone and tubular steel fence on the south property line will be removed and replaced with a new 6-foot high decorative concrete block and tubular steel combination wall. An existing tubular steel sliding gate will remain along the alley and will provide access to a parking lot.

Proposed rehabilitation to the building includes repainting with the existing color scheme, repairs or replacement with like-in-kind for the roof, siding, fascia, and doors and windows, where needed. The window porch enclosure on the primary (east) elevation will be removed (Exhibit C: Proposed Elevations, attached) restoring the original open porch. The removed windows will be refurbished and reused on the front elevation. The floor



plan will be slightly modified to accommodate the new use and to meet ADA standards by widening certain doorways. A new fire sprinkler system and floor will be installed and all electrical, plumbing, and heating and cooling systems will be upgraded including insulation of the attic.

**PROJECT ANALYSIS:** Section 4.02.050 (Historic Preservation Certificate of Appropriateness and Demolition of Historic Resources) of the Ontario Development Code, requires approval of a Certificate of Appropriateness for any infill or alteration to a historic resource. The intent is to recognize and protect identified historic resources within the community, from inappropriate alterations that may render the historic resource ineligible for listing on the local, state, and/or the national registers. The Secretary of the Interior Standards for the Treatment of Historic Properties, also referred to as the “Standards,” provide a critical part of the framework of the national preservation program and are widely used at the federal, state, and local levels to guide work on historic resources and have been adopted and incorporated into the Ontario Development Code.

The preservation treatment proposed for this project is the Standard for Rehabilitation. Rehabilitation is the process of returning a property to a state of utility, through repair or alteration, which makes possible a contemporary use, while preserving those portions and features of the property which are significant to its historic, architectural, and cultural values. The Standards state that alterations will not destroy or alter historic features that characterize the historic resource and that new work will be differentiated from old and will be compatible with historic materials, features, sizes, scale and proportion, and massing, to protect the integrity of the resource.

The adaptive reuse project will retain character-defining architectural features, details, and materials, and will not detrimentally affect the historical value of the resource. The project includes exterior restorations, such as repainting, roofing, window and siding repair, and removing the enclosed porch that will highlight further, the distinctive features of the building.

**COMPLIANCE WITH THE ONTARIO PLAN COMPLIANCE WITH THE ONTARIO PLAN:** The Project is consistent with the principles, goals, and policies contained within the Vision, Governance, Policy Plan (General Plan), and the City Council Priorities components of The Ontario Plan (“TOP”). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals

- Invest in the Growth and Evolution of the City’s Economy
- Focus Resources in Ontario’s Commercial and Residential Neighborhoods

[2] Vision

**Dynamic Balance**

- An appreciation for the "personality and charm" of this community, preserving important characteristics and values even as growth and change occur, all the while retaining a distinctive local feel where people love to be.

[3] Governance

**Decision Making**

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices
  - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

[4] Policy Plan

**Community Design Element**

- Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.
  - CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.
  - CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.
- Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.
  - CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.
- Goal CD4: Historic buildings, streets, landscapes and neighborhoods, as well as the story of Ontario's people, businesses, and social and community

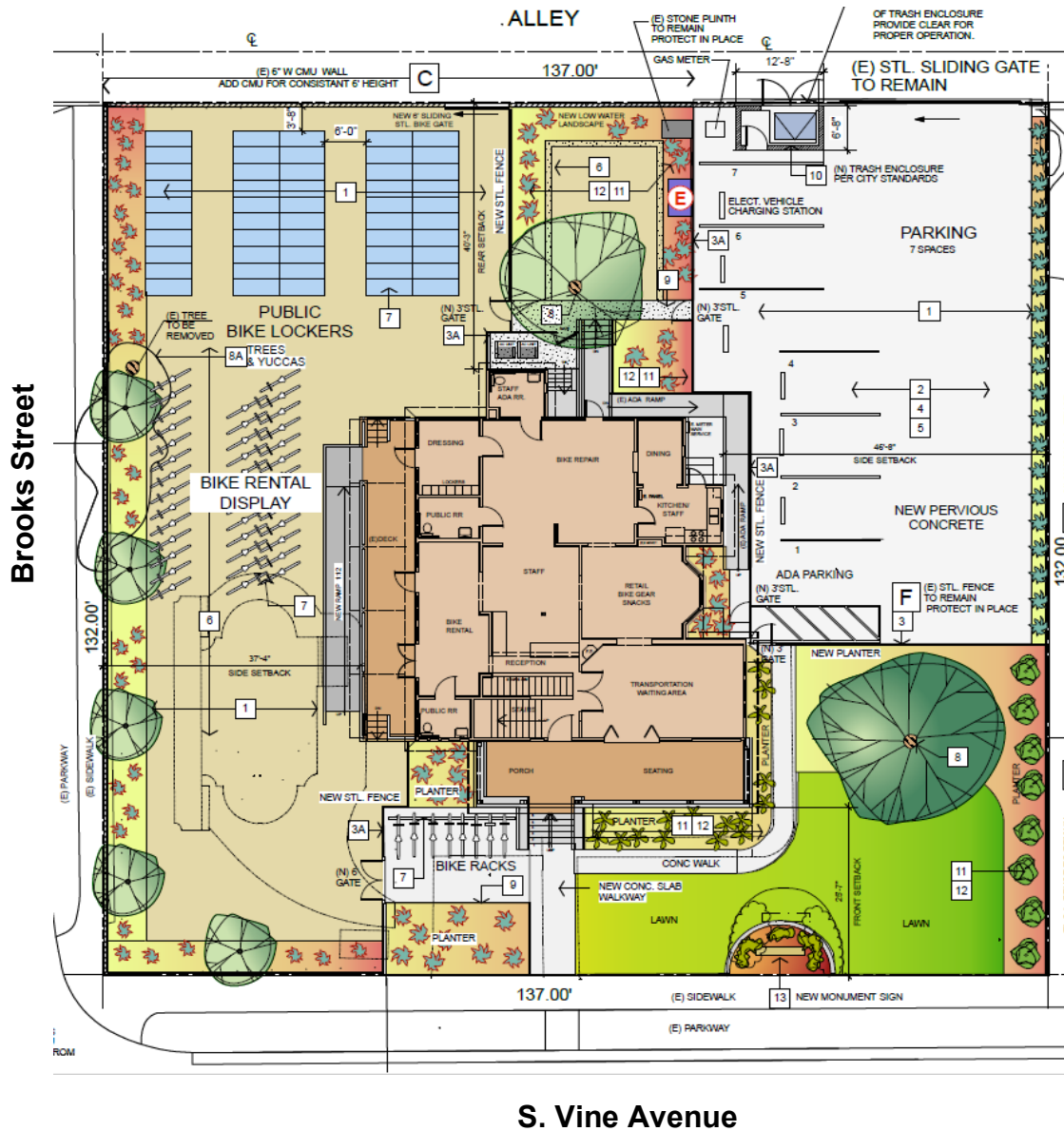
organizations, that have been preserved and serve as a focal point for civic pride and identity.

- CD4-2 Collaboration with Outside Agencies. We pursue opportunities to team with other agencies, local organizations and non-profits in order to preserve and promote Ontario's heritage.

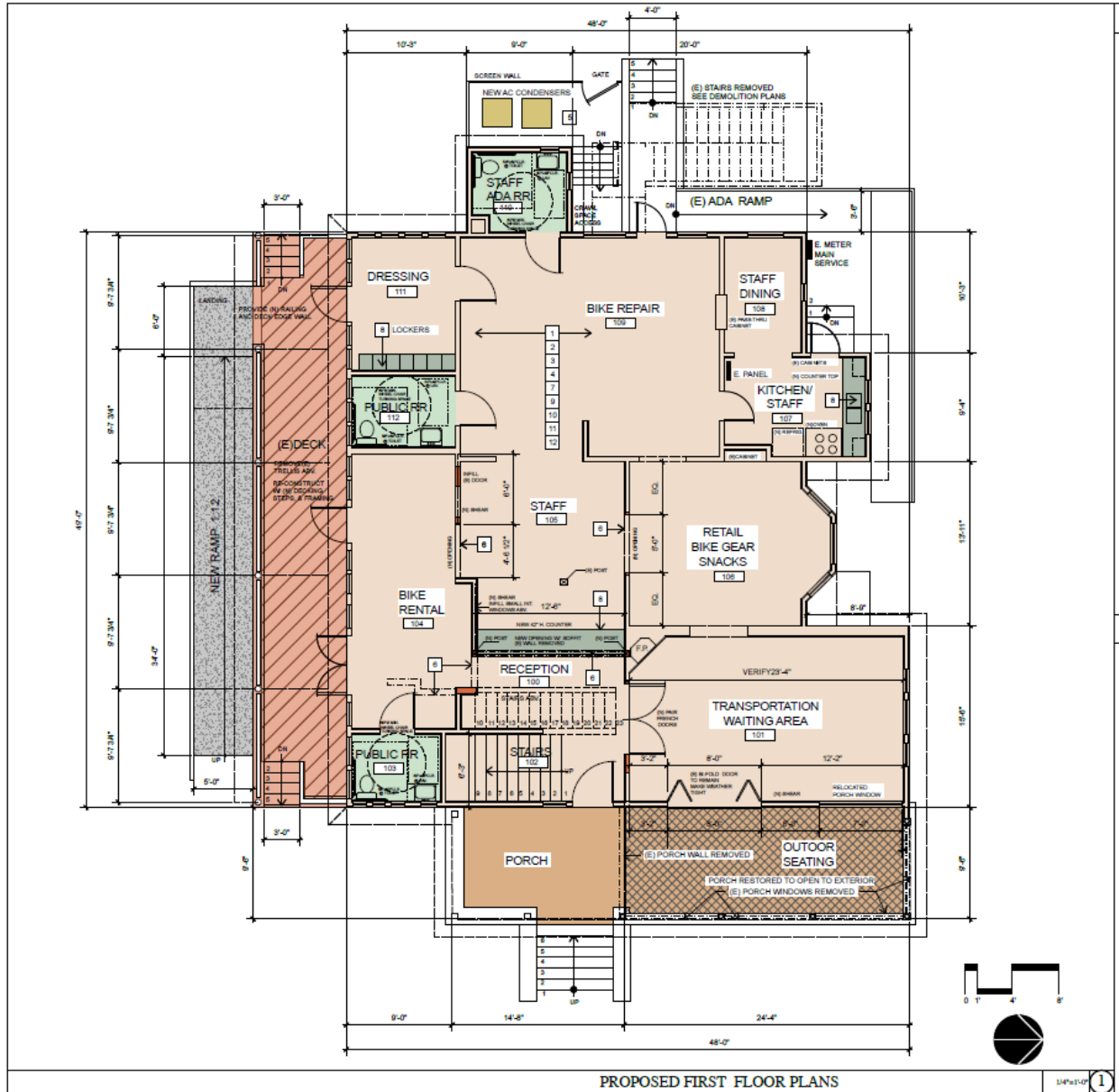
**ENVIRONMENTAL REVIEW:** The Project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15331 (Historical Resource Restoration/Rehabilitation) of the CEQA Guidelines, which consists of maintenance, repairs, stabilization, rehabilitation, restoration, preservation, conservation, or reconstruction of historical resources in a manner consistent with the Secretary of the Interior's Standards for the Treatment of Historic Properties with Guidelines for Preserving, Rehabilitating, Restoring, and Reconstructing Historic Buildings (1995), Weeks and Grimmer.

**CONDITIONS OF APPROVAL:** See attached department report.

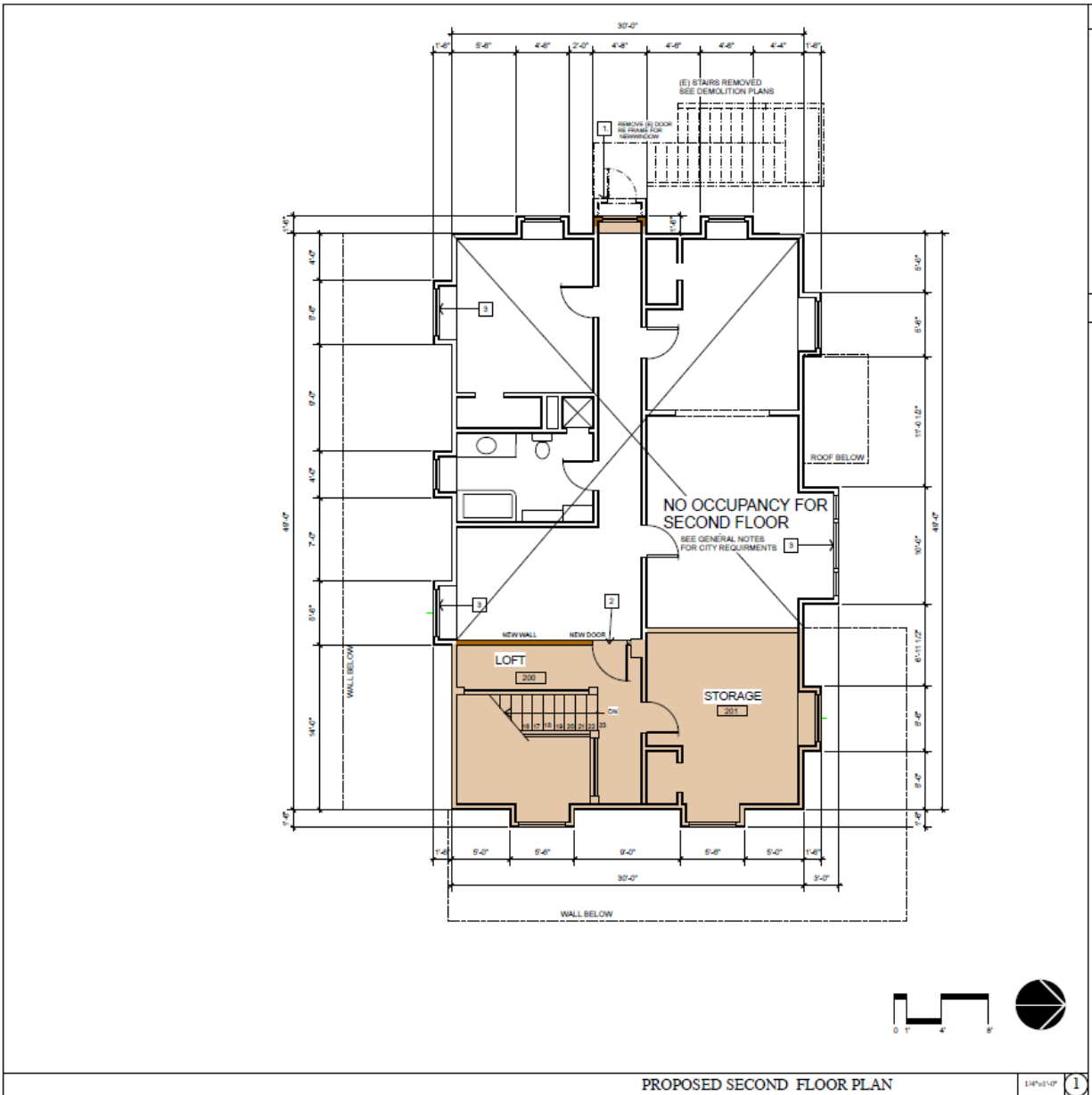
**Exhibit A: Site Plan**



**Exhibit B: Floor Plan**



**Exhibit B: Floor Plan (continued)**



**Exhibit C: Elevations**



**EAST ELEVATION**



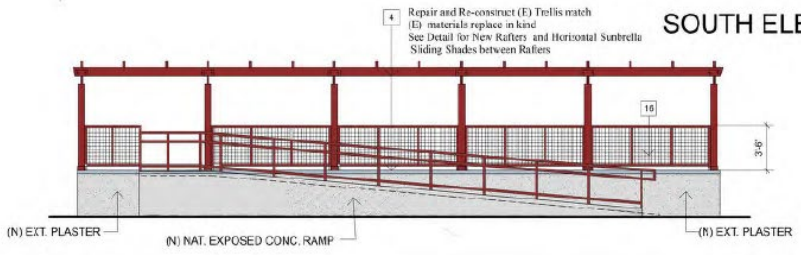
**NORTH ELEVATION**

**Exhibit C: Elevations (continued)**



17 Add 2x3 Galv. WW Mesh & Add New 2" min. Pipe Rail Match Existing "IN KIND"

**SOUTH ELEVATION**



**WEST ELEVATION**



## RESOLUTION NO.

A RESOLUTION OF THE HISTORIC PRESERVATION COMMISSION OF THE CITY OF ONTARIO, APPROVING FILE NO. PHP20-003, A CERTIFICATE OF APPROPRIATENESS FOR THE ADAPTIVE REUSE OF THE WILLIAM B. FALLIS HOUSE (LOCAL HISTORIC LANDMARK NO. 1), A 3,658 SQUARE FOOT SINGLE-FAMILY RESIDENTIAL BUILDING, TO A MULTI-MODAL MOBILITY HUB ON 0.41 ACRES OF LAND LOCATED AT 122 SOUTH VINE AVENUE, WITHIN THE MU-1 (DOWNTOWN MIXED USE) ZONING DISTRICT AND MAKING FINDINGS IN SUPPORT THEREOF – APN: 1049-021-19

WHEREAS, City of Ontario (“Applicant”) has filed an application for the approval of a Certificate of Appropriateness, File No. PHP20-003, as described in the title of this Resolution (hereinafter referred to as “Project”); and

WHEREAS, the City’s character and history are reflected in its cultural, historical, and architectural heritage, with an emphasis on the “Model Colony” as declared by an act of the Congress of the United States and presented at the St. Louis World’s Fair in 1904; and

WHEREAS, the City’s historical foundations should be preserved as living parts of community life and development in order to foster an understanding of the City’s past, so that future generations may have a genuine opportunity to appreciate, enjoy, and understand Ontario’s rich heritage; and

WHEREAS, the Community Development and the Aesthetic, Cultural, Open Space, and Recreational Resources Elements of the Policy Plan component of The Ontario Plan sets forth Goals and Policies to conserve Ontario’s historic buildings and districts; and

WHEREAS, the William B. Fallis House is worthy of preservation and was designated as Local Landmark No. 1 by the City Council on May 18, 1993, and determined to be a Tier II Historic Resource by the Historic Preservation Commission on July 20, 2007; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as “CEQA”); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Historic Preservation Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the proposed project is located within the Airport Influence Area of Ontario International Airport (“ONT”) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (“ALUCP”) for ONT; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 14, 2020, the Historic Preservation Subcommittee of the City of Ontario conducted a hearing and issued Decision No. HPSC20-003, recommending the Historic Preservation Commission grant the Application; and

WHEREAS, on May 26, 2020, the Historic Preservation Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, BE IT RESOLVED, that the Historic Preservation Commission of the City of Ontario as follows:

**SECTION 1: Environmental Determination and Findings.** As the decision-making authority for the Project, the Historic Preservation Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Historic Preservation Commission, the Historic Preservation Commission finds as follows:

(1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from environmental review pursuant to Section 15331 (Class 31, Historical Resource Restoration/Rehabilitation) of the CEQA Guidelines, which consists of maintenance, repairs, stabilization, rehabilitation, restoration, preservation, conservation, or reconstruction of historical resources in a manner consistent with the Secretary of the Interior’s Standards for the Treatment of

Historic Properties with Guidelines for Preserving, Rehabilitating, Restoring, and Reconstructing Historic Buildings (1995), Weeks and Grimmer. The Guidelines were utilized in the development of the project design and, as a result, do not pose any adverse impacts to the historic resource; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Historic Preservation Commission.

**SECTION 2: Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Historic Preservation Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the HISTORIC PRESERVATION COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

**SECTION 3: Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Historic Preservation Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 and 2, above, the Historic Preservation Commission hereby concludes as follows:

(1) ***The proposed project will not detrimentally change, destroy or adversely affect any significant architectural feature of the resource.*** The Project will result in demolition of features of the building and site that were constructed much later than the original construction and have not acquired any historical significance. Furthermore, the project preserves all the significant architectural features of the historic

resource and those features will be preserved and restored to original state including the front porch, siding, windows, doors, roofing; and removal of the exterior staircase and second story doorway.

(2) ***The proposed project will not detrimentally change, destroy or adversely affect the historic character or value of the resource.*** The majority of the visual changes will occur on the west and south sides of the site, away from the front and street view, thereby not adversely affecting the historic character, value, or context. Furthermore, the new construction is designed so that it is historically appropriate.

(3) ***The proposed project will be compatible with the exterior character-defining features of the historic resource.*** The proposed alterations to the south elevation and grounds will have minimal impact to the exterior character-defining features of the historical resource because the design and materials will be differentiated, compatible, and appropriate to the existing.

**SECTION 4: Historic Preservation Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 3, above, the Historic Preservation Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A" and incorporated herein by this reference.

**SECTION 5: Indemnification.** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

**SECTION 6: Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

**SECTION 7: Certification to Adoption.** The Secretary shall certify to the adoption of the Resolution.

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Historic Preservation Commission Resolution  
File No. PHP20-003  
May 26, 2020  
Page 5

The Secretary Pro Tempore for the Historic Preservation Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Historic Preservation Commission of the City of Ontario at a regular meeting thereof held on the 26th day of May 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

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Jim Willoughby  
Historic Preservation Commission  
Chairman

ATTEST:

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Cathy Wahlstrom  
Planning Director and  
Secretary of Historic Preservation  
Commission

Historic Preservation Commission Resolution  
File No. PHP20-003  
May 26, 2020  
Page 6

STATE OF CALIFORNIA            )  
COUNTY OF SAN BERNARDINO    )  
CITY OF ONTARIO                )

I, Gwen Berendsen, Secretary Pro Tempore of the Historic Preservation Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. PC20-XXX, was duly passed and adopted by the Historic Preservation Commission of the City of Ontario at their regular meeting held on May 26, 2020 by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

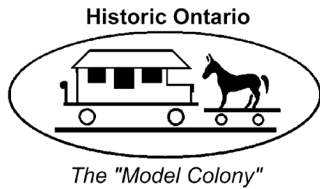
ABSTAIN:

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Gwen Berendsen  
Secretary Pro Tempore

**ATTACHMENT A:**  
**File No. PHP20-003**  
**Departmental Conditions of Approval**

*(Departmental conditions of approval follow this page)*



# CERTIFICATE OF APPROPRIATENESS

## CONDITIONS OF APPROVAL

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**Date:** May 26, 2020  
**File No.:** PHP20-003  
**Location:** 122 South Vine Avenue  
(APN: 1049-021-19)  
**Prepared By:** Diane Ayala, Senior Planner

**Description:** A Certificate of Appropriateness for the adaptive reuse of the William B. Fallis House (Local Historic Landmark No. 1), a 3,658 square-foot single family residential building, to a multi-modal mobility hub on 0.41 acres of land located at the northwest corner of South Vine Avenue and Brooks Street, within the MU-1 (Downtown Mixed Use) zoning district.

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### CONDITIONS OF APPROVAL

The above-described Project shall comply with the following Conditions of Approval.

1. Time Limits.

1.1 The Certificate of Appropriateness shall become void twenty-four (24) months from the date of approval unless a building permit has been issued and work authorized by this approval has commenced prior to the expiration date and is diligently pursued to completion.

2. Fences and Gates.

2.1 All new CMU walls shall be decorative (split face, slump stone, precision with stucco or plaster and painted to match building) and shall have a decorative cap.

2.2 Existing chain link fence along the north property line, approximately 20 feet from front property line, shall be removed and replaced with a solid decorative wall.

2.3 A solid decorative CMU wall with cap shall be constructed to screen new AC condenser units.

2.4 Trash enclosure wall shall be a decorative CMU wall with cap.



**Conditions of Approval**

**File No. PHP20-003**

**May 26, 2020**

**Page 2**

3. Architectural Treatment.

- 3.1 All exterior features shall be restored, repaired, or replaced with like in kind pursuant to the Secretary of the Interior Standards, including, but not limited to, wood shingle treatment, roofing tile and shingle for replacement, and removing of any additional attached nonhistorical materials and repairing or replacing with like in kind those sections that are deteriorated beyond repair.
- 3.2 The National Park Service Preservation Briefs numbered 3, 4, 9, 10, 16, 19, 18, 24, 28, 47, and any other Brief deemed necessary shall be the primary reference guide for rehabilitation and restoration of the property.
- 3.3 Existing clay roof shall be removed and reinstalled per plan. If replacement tiles are needed, they shall be like for like and evenly distributed throughout the roof area.
- 3.4 A cut sheet/brochure of the proposed F.R. wood roof shingles shall be submitted to Planning for review and approval prior to issuance of building permit or installation.
- 3.5 Solid wood shall be used on construction of the trellis, posts, siding, shingle roof, and fascia boards. Only sections of wood that is deteriorated and damaged beyond repair shall be removed and replaced through a "dutchman" repair for a seamless condition.
- 3.6 All original and new doors and windows shall be framed with wood, wood with aluminum clad, or wood with fiberglass clad framed. Aluminum and vinyl framed windows are prohibited. Submit a door and window cut/spec sheet or brochure for new windows or doors to Planning Department for review prior to issuance of building permits.
- 3.7 The paint to be used for Fallis House should be of historical quality and follow the appropriate color scheme. Submit a paint sample/brochure, if deviating from existing colors, to Planning for review and approval prior to commencement of work.
- 3.8 A cut sheet/brochure of the proposed sunshade canopy ("Sunbrella") with proposed color to be placed on the south façade of structure shall be submitted to Planning for review and approval prior to issuance of building permit or installation.
- 3.9 Exterior light fixtures shall be compatible with the architectural style. Submit a cut sheet to Planning for review and approval prior to issuance of building permit.

**Conditions of Approval**

**File No. PHP20-003**

**May 26, 2020**

**Page 3**

4. Signs.

- 4.1 Sign plans shall be submitted and approved for the site prior to the installation of any signs. Signs shall conform to Division 8.01 (Sign Regulations) of the Ontario Development Code.

5. Landscape.

- 5.1 Replace the *Romneya coulteri* (easily overwatered and dies out in the summer); consider Aloe, Salvia, Santolina, Sedum, Yarrow.
- 5.2 Plant material shall be densely planted; provide large swatches of color and texture with the various plant material as shown in the rendering presented during the meeting on December 12<sup>th</sup>, 2019.
- 5.3 DG, cobble and/or boulders shall be limited to 5% of landscape area.
- 5.4 Landscape construction plans shall meet the requirements of the Landscape Development Guidelines. See <http://www.ontarioca.gov/landscape-planning/standards>

6. Other.

- 6.1 After issuance building permits but prior to commencement of any work, including demolition, an on-site pre-construction meeting with architect, project manager, and construction teams is required.
- 6.2 The applicant shall obtain a building permit prior to any demolition or construction.
- 6.3 Any deviation from the approved plans shall require approval of the Planning Department and, if necessary, the Historic Preservation Commission.
- 6.4 Conditions of Approval shall be reproduced onto all plans submitted for permits.
- 6.5 Prior to occupancy and building permit final, the Planning Department shall inspect the premises to ensure the Conditions of Approval have been met and that the project has been constructed per the approved plans.



# PLANNING COMMISSION STAFF REPORT

May 26, 2020

**FILE NOS.:** PDEV18-022 and PCUP18-021

**SUBJECT:** A Development Plan (File No. PDEV18-022) to construct a 6,870 square-foot industrial building in conjunction with a Conditional Use Permit (File No. PCUP18-021) to establish and operate an outdoor automobile storage yard (tow yard) on 3.1 acres of land located at 580 East Belmont Street, within the IL (Light Industrial) zoning district (APNs: 1049-491-01, 1049-491-02 and 1049-491-03); **submitted by Four Sisters Enterprises, LLC.**

**PROPERTY OWNER:** Four Sisters Enterprises, LLC

**RECOMMENDED ACTION:** That the Planning Commission consider and approve File Nos. PDEV18-022 and PCUP18-021, pursuant to the facts and reasons contained in the staff report and attached resolutions, and subject to the conditions of approval contained in the attached departmental reports.

**PROJECT SETTING:** The Project site is comprised of 3.1 gross acres of land located at the southwest corner of Belmont Street and Monterey Avenue, at 580 East Belmont Street, within the IL (Light Industrial) zoning district. The site is currently undeveloped and is bordered by Belmont Street to the north, Sultana Avenue to the west, Phillips Street to the south, and Southern Pacific Railroad and Monterey Avenue to the east, and is depicted in Figure 1: Project Location Map.



**Figure 1: Project Location Map**

Case Planner:	Denny D. Chen
Planning Director Approval:	
Submittal Date:	6/21/2018

Hearing Body	Date	Decision	Action
DAB	5/18/2020	Approval	Recommend
PC	5/26/2020		Final
CC			

Industrial land uses are to the north and residential land uses surround the site on the east, south and west. The Policy Plan (General Plan) land use designations surrounding the site are Industrial to the north, Low Density Residential/Industrial Overlay to the east, Low Density Residential to the south, and Low Medium Density Residential to the west, as depicted in Figure 2 below.

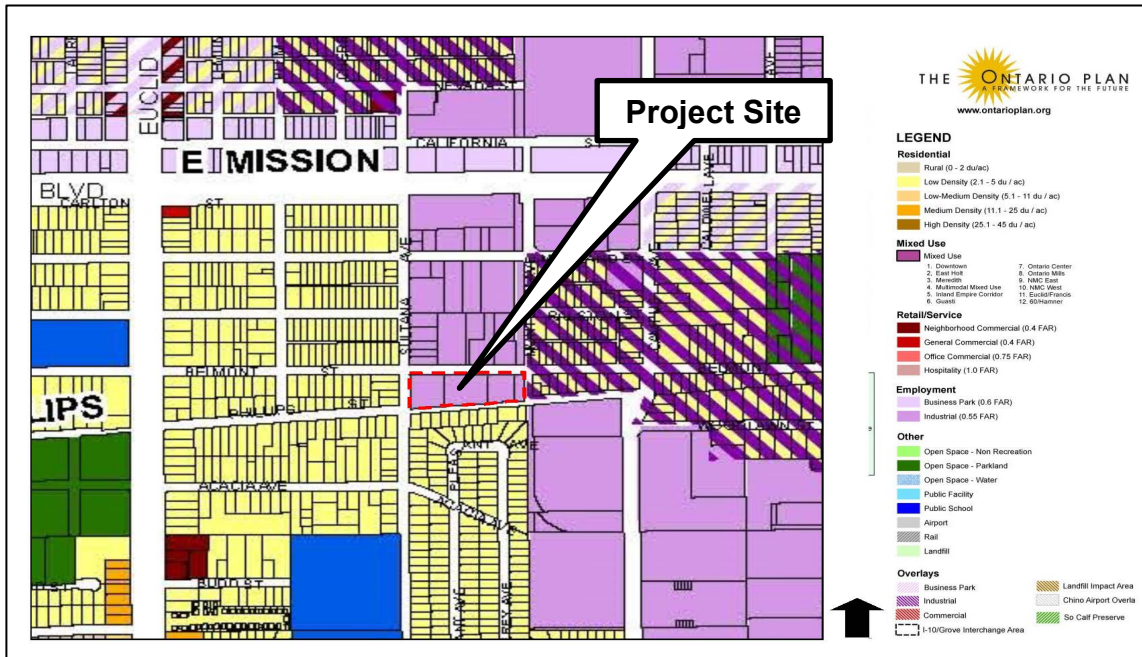


Figure 2: TOP Policy Plan Land Use Map

**PROJECT ANALYSIS:**

[1] Background — On June 21, 2018, the Applicant submitted a Development Plan application requesting approval to construct a 6,870 square foot industrial building having a floor area ratio (FAR) of 0.05, in conjunction with a Conditional Use Permit (File No. PCUP18-021) to establish and operate a towing service and motor vehicle impound storage yard on the Project site.

On May 18, 2020, the Development Advisory Board reviewed the subject applications and recommended that the Planning Commission approve the proposed Project subject to the departmental conditions of approval included with this report.

[2] Site Design/Building Layout — The proposed building is located at the southeast corner of the Project site, fronting on to Monterey Avenue (to the east) and Phillips Street (to the south). The new building is setback 48 feet along Belmont Street (north) property

line, 10 feet from the (east) property line, 10 feet from Phillips Street (south) property line, and approximately 540 feet from Sultana Avenue (west) property line. The building's office and main entrance is situated at the northeast corner of the building, fronting onto Belmont Street. The employee and visitor parking areas are located to the north and northwest of the building and are secured by an 8-foot tall decorative tube steel fence.

The motor vehicle impound yard is proposed on the westerly two-thirds of the Project site, covering 79,569 square feet designed to park 324 impounded vehicles. The yard area will be screened from public view by a decorative 8-foot tall wall consisting of split-face masonry block with decorative pilasters and cap, located along the north, west, and south sides of the Project site.

An 800 square foot (50 feet long by 16 feet wide) outdoor truck wash area and an above ground 1,000-gallon fuel tank are also proposed along the south side of the Project site, near the proposed building (see Exhibit B—Site Plan, attached).

To ensure that the proposed truck wash will not generate noise levels beyond the City's allowed noise levels within the adjacent residential zones to the west, south and east of the Project site, a Noise Study was completed by the applicant. Per the Ontario Municipal Code Noise Ordinance, the maximum exterior noise level allowed within residential zoning districts is 65 decibels (dBA), between the hours of 7 a.m. to 10 p.m. According to the Noise Study, the exterior noise level generated by the truck wash area will not exceed 59 dBA; therefore, the proposed outdoor truck washing activities will comply with the City's maximum allowed exterior noise level for residential zoning districts and are not anticipated to adversely affect the adjacent residents.

[3] Site Access/Circulation — Primary vehicle access is provided by two driveways located along Belmont Street, immediately west of Monterey Avenue. A secondary vehicle access point is provided from Sultana Avenue, mid-block between Belmont and Phillips Streets (see Exhibit B—Site Plan, attached).

[4] Parking — The Project is required to provide a minimum of 16 off-street parking spaces and a total of 20 off-street parking spaces have been provided, exceeding the minimum number of required parking spaces. Parking was calculated as shown in Table 1 – Required Off-Street Parking, below.

<b>Table 1 – Required Off-Street Parking</b>			
<b>Proposed Use</b>	<b>Area</b>	<b>Parking Ratio</b>	<b>Required Parking</b>
Office:	1,056	4 spaces per 1,000 SF of Building GFA exceeding 10% of building GFA (approximately 475 SF).	2
Outdoor Vehicle Storage:	79,569	0.1 space per 1,000 GFA.	8
Warehouse:	5,814	One space per 1,000 SF for first 20,000 GFA and 0.5 per 1,000 SF for building GFA greater than 20,000 SF; plus 1 tractor-trailer parking space per 4 dock-high loading doors; plus required parking for “general business offices” when those uses exceed 10% of building GFA.	6
<b>Total Building Area:</b>	<b>6,870</b>		
<b>Total Parking Required:</b>			<b>16</b>
<b>Total Parking Provided:</b>			<b>20</b>

[5] Architecture – The proposed building elevations illustrate the type of high-quality architecture promoted by the Ontario Development Code’s development standards and design guidelines. The building will be composed of pre-painted/smooth faced standing seam metal wall panels that incorporate a contemporary architectural style (see Figure 2: Building Perspective, below, and Exhibits D and E—Elevations, attached). The building design incorporates articulation in the building footprint, incorporating a combination of recessed and popped-out wall areas, and articulation in the building parapet/roof line, which serves to accentuate the building’s roofline. To accent the building design, the following materials, finishes, and fixtures have been incorporated into the design of the building:

- Vertical changes in building height that correspond to horizontal changes in the building footprint;
- Smooth-faced, pre-painted seamed metal panels;
- Decorative metal awnings;
- Double-glazed windows with a clear anodized mullion system;
- Decorative split-face block at the base of the building; and
- Decorative cornices on all four sides of the building.



**Figure 3 – Building Perspective**

[6] Landscaping — The Project proposes 15 percent landscape coverage, which meets the minimum landscape coverage required by the Development Code for corner lots located within the IL (Light Industrial) zoning district. The landscape pallet for the Project incorporates a mixture of 24-inch, 36-inch, and 48-inch box accent and shade trees. Some of the proposed trees include Raywood Ash, Western Redbud, Crape Myrtle, Purple Leaf Plum, Coast Live Oak, along with a variety of low water use grasses, shrubs, and groundcovers.

Additionally, the Project has provided the required 10-foot landscaped setback areas along all Project street frontages, including Belmont Street, Sultana Avenue, Phillips Street, and Monterey Avenue/Southern Pacific Railroad (see Exhibit G—Landscape Plan, attached).

[7] Utilities (Drainage, Sewer) — Public utilities (water and sewer) are available to serve the Project. Additionally, the Applicant has submitted a Preliminary Water Quality Management Plan (PWQMP), which establishes the Project’s compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures

that capture runoff and pollutant transport by minimizing impervious surfaces and maximizing low impact development (LID) best management practices (BMPs), such as retention and infiltration, biotreatment, and evapotranspiration. The PWQMP proposes a vegetated swale designed to accept runoff from building roofs, parking lots and Project roadways, which lead to an underground stormwater infiltration system. The proposed underground infiltration system is located along the southeast portion of the site and any overflow drainage will be conveyed to the surrounding streets.

[8] Signage — The project is proposing two monument signs along the perimeter of the Project site. All project signage shall be reviewed and approved by the Planning Department and shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

[9] Conditional Use Permit — Pursuant to the Ontario Development Code, Conditional Use Permit approval is required to establish an outdoor automobile storage yard (tow yard) within the IL (Light Industrial) zoning district. A Conditional Use permit is required in order adequately review the potential impacts the use might have on the surrounding properties and eliminate or mitigate those impacts. Currently, Pepe's Towing operates another towing facility in City, located at 810 East Main Street. The towing facility was approved with Conditional Use Permit File No. PCUP17-016 on February 21, 2018, by the Zoning Administrator.

The applicant is now requesting to establish and operate an additional vehicle towing facility and motor vehicle impound storage yard on the Project site. The proposed towing facility and vehicle impound storage yard, consists of a 79,569 square foot outdoor vehicle storage area that will accommodate up to 324 motor vehicles, a 6,870 square foot, two-story industrial building, and an 800 square foot outdoor truck wash area. The industrial building is comprised of an office area, that includes a dispatch office, warehouse/storage area, and a vehicle evidence room. The evidence room will serve as a staging and storage area for non-operational vehicles that were involved in serious accidents that need to be secured for the Police Department inspections (see Exhibit F—Floor Plans, attached).

Vehicles not involved in serious accidents will be kept within the outdoor storage yard area, which is screened by a decorative 8-foot tall masonry split face block wall, with decorative pilasters and cap, designed to match the new building. The towing service operation will have a staff of 3 full-time employees. The proposed business hours of operation are Monday thru Friday, 8:00 a.m. to 5:00 p.m., for fifty-weeks out of each year. Pepe's Towing serves as a contracted Rotational Towing Service with the City and, therefore, will be required to operate 24-hours, 7-days a week for a two-week period out of each year.

[10] Community Outreach — The Project site is surrounded by residential properties to the east, south, and west. To ensure community input from the surrounding residents, a Notice was mailed to all property owners within 300-feet of the Project site, in both



English and Spanish. The intent of the notification was to inform the surrounding property owners of the proposed Project and to address any questions or concerns they may have regarding the Project. To date, staff has received one phone call and one email requesting clarification of the proposed use.

Additionally, on May 11, 2020, a letter was received from a resident on Phillips Street, regarding the Project, which mentioned the following concerns:

[a] *Increased air pollution emissions generated by heavy duty (diesel) trucks in the area.*

Response: The business will operate with a total of 3 tow trucks: two gasoline powered Class A (flatbed) trucks; and one gasoline powered Class C (heavy duty) truck. No diesel trucks or semi-trucks will be used at the Project site.

[b] *Traffic accidents and speeding at the corner of Sultana Avenue and Phillips Street.*

Response: The Engineering Department, Traffic Division, has reviewed the Project for access and circulation and does not anticipate traffic or noise to increase beyond the traffic and noise that currently exists on Sultana Avenue & Phillips Street and the surrounding areas. The Project will also be required to construct street frontage improvements (Sidewalk and Parkways) along Sultana Avenue, Phillips Street and Belmont Street. In addition, the Project will only be accessed from Belmont Street. No access will be taken from Sultana Avenue (Emergency access only) and Phillips Street.

[c] *The 24-hour operation of the business, and the loading and unloading of vehicles on a daily basis.*

Response: The tow yard's standard hours of operation are Monday thru Friday, from 8:00 a.m. to 5:00 p.m., and closed on Saturday and Sunday. Since the Applicant has a contract with the City of Ontario's Police Department, it will be required to operate for 24-hrs/7-days a week for two weeks out of the year. During the remaining fifty-weeks of the year, the business will operate by the aforementioned standard business hours. The Project site will be screened by an 8-foot-high block wall along the south, east and west frontages.

**COMPLIANCE WITH THE ONTARIO PLAN:** The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)

[2] Vision.

**Distinctive Development:**

- Commercial and Residential Development
  - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

[3] Governance.

**Decision Making:**

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
  - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

[4] Policy Plan (General Plan)

**Land Use Element:**

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
  - LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).
- Goal LU2: Compatibility between a wide range of uses.
  - LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

**Community Economics Element:**

- Goal CE1: A complete community that provides for all incomes and stages of life.
- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.
  - CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.
  - CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.
  - CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.
  - CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

**Safety Element:**

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.
  - S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

**Community Design Element:**

- Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.
  - CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.
  - CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

▪ Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

➤ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➤ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➤ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-12 Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

- Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

- CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

- CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.

- CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.

- CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.

- CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

- Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

- CD5-1 Maintenance of Buildings and Property. We require all public and privately-owned buildings and property (including trails and easements) to be properly and consistently maintained.

- CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

**HOUSING ELEMENT COMPLIANCE:** The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

**AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE:** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the

Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

**ENVIRONMENTAL REVIEW:** The Project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines and meets each of the following conditions: [1] the Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations; [2] the proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses; [3] the Project site has no value as habitat for endangered, rare, or threatened species; [4] approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and [5] the Project site can be adequately served by all required utilities and public services.

**CONDITIONS OF APPROVAL:** See attached department reports.

**TECHNICAL APPENDIX:**

**Surrounding Zoning and Land Use:**

	<i>Existing Land Use</i>	<i>General Plan Designation</i>	<i>Zoning Designation</i>	<i>Specific Plan Land Use</i>
<i>Site</i>	Vacant Land	Industrial	IL (Light Industrial)	N/A
<i>North</i>	Metal Rebar Storage Yard	Industrial	IL (Light Industrial)	N/A
<i>South</i>	Single-Family Residential Homes	Low Density Residential	LDR5 (Low Density Residential – 2.1 to 5.0 DUs/Acre)	N/A
<i>East</i>	Railroad & Single-Family Residential Homes	Low Density Residential/Industrial Overlay	LDR5 (Low Density Residential – 2.1 to 5.0 DUs/Acre)	N/A
<i>West</i>	Single-Family Residential Homes	Low Medium Density Residential	MDR11 (Medium Density Residential – 5.1 to 11 DUs/Acre)	N/A

**General Site & Building Statistics**

<i>Item</i>	<i>Proposed</i>	<i>Min./Max. Standard</i>	<i>Meets Y/N</i>
<i>Project Area:</i>	3.14 Ac (136,778 SF)	10,000 SF (Min.)	Y
<i>Building Area</i>	6,870 SF	75,228 SF (Max.)	Y
<i>Floor Area Ratio (FAR)</i>	0.05	0.55 (Max.)	Y
<i>Building Height:</i>	34 FT	55 FT (Max.)	Y

**Off-Street Parking:**

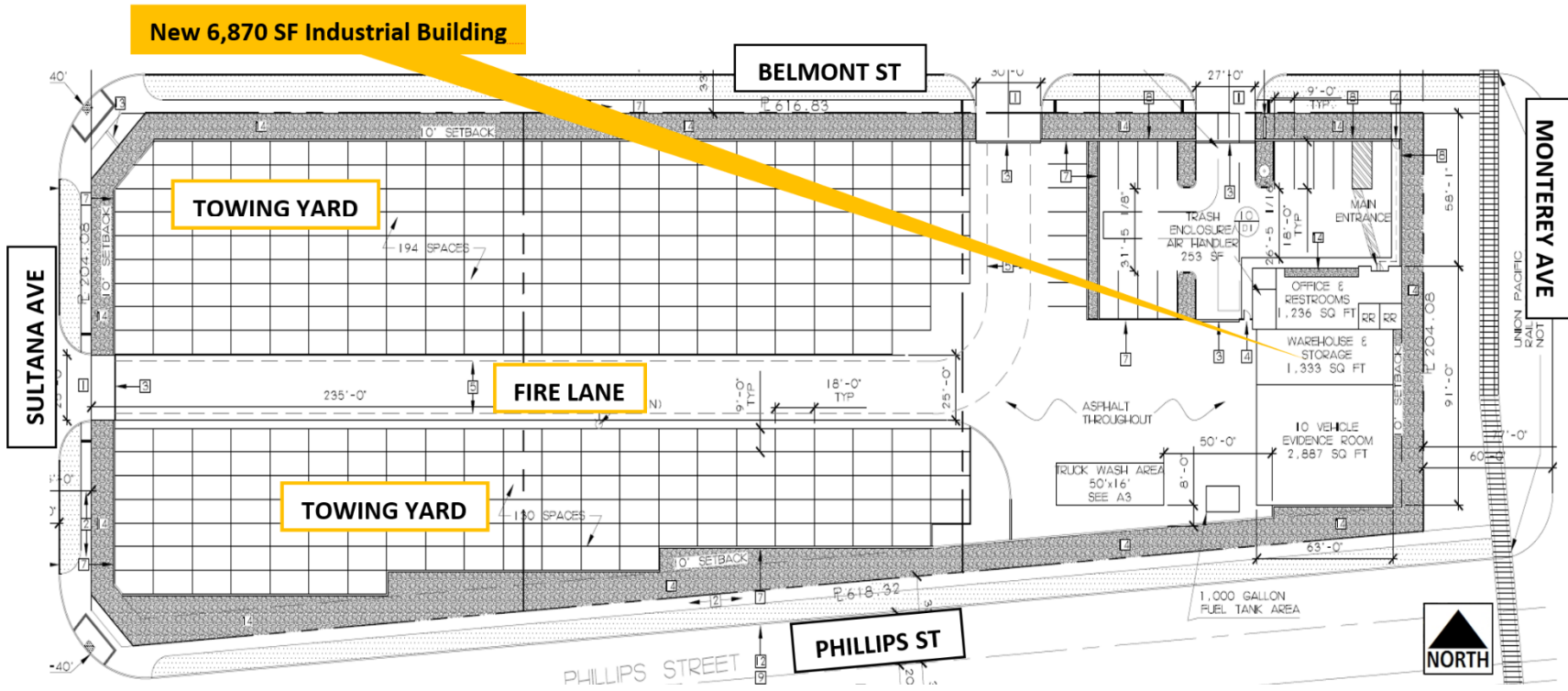
<i>Type of Use</i>	<i>Building Area</i>	<i>Parking Ratio</i>	<i>Spaces Required</i>	<i>Spaces Provided</i>
<i>Warehouse Building</i>	5,814 SF	1 per 1,000 for first 20,000 GFA and 0.5 per 1,000 for building GFA greater than 20,000; plus, required parking for “general business offices” when those uses exceed 10% of building GFA (Gross Floor Area).	6	7
<i>Office</i>	1,056 SF	4 spaces per 1,000 of building GFA	2	13
<i>Outdoor Vehicle Storage Area</i>	79,569 SF	0.1 space per 1,000 SF of GFA	8	324
<i>TOTAL Building Area</i>	6,870 SF		16	20

**Exhibit A—PROJECT LOCATION**





**Exhibit B—SITE PLAN**



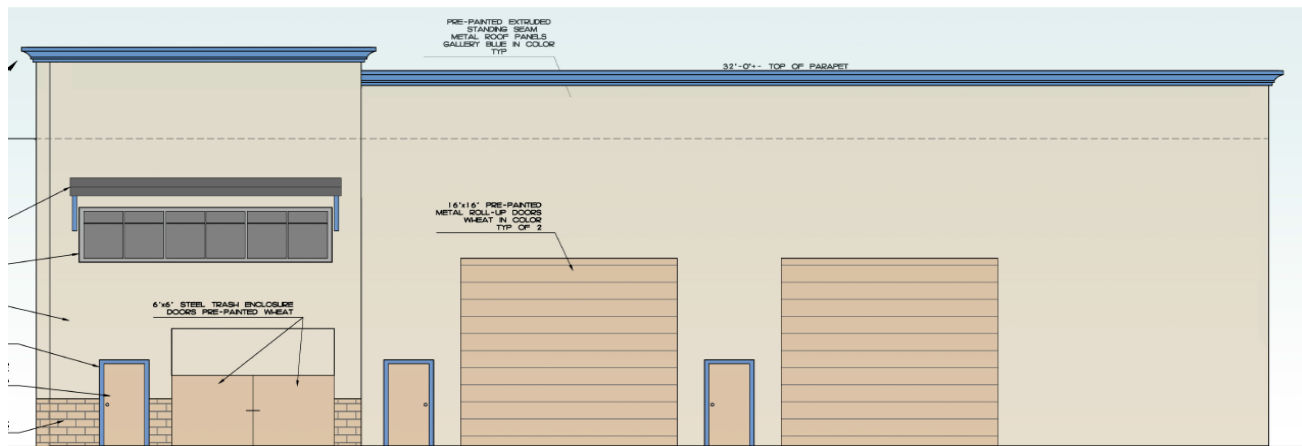
***Exhibit C—BUILDING PERSPECTIVE***



**Exhibit D—ELEVATIONS**

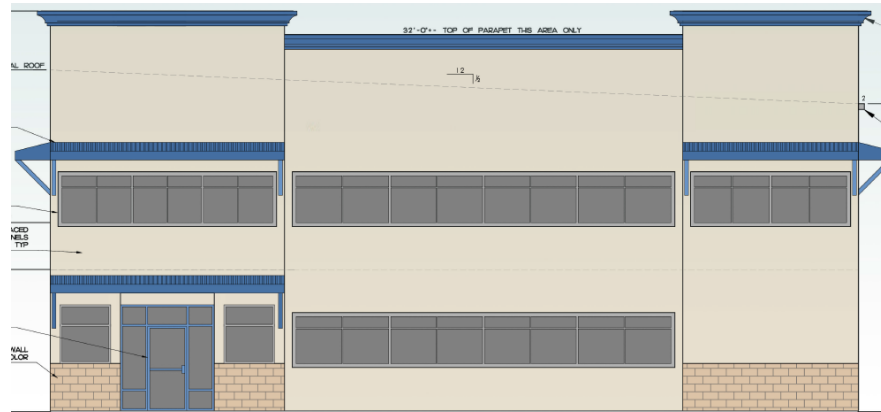


**East (Monterey Avenue) Elevation**

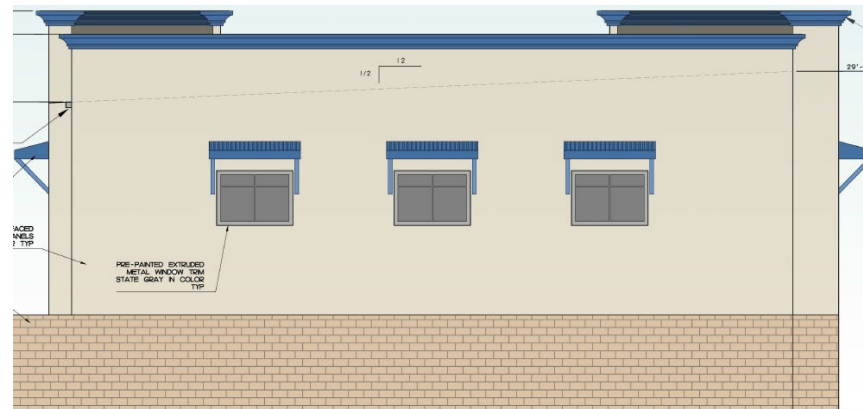


**West Elevation**

**Exhibit E—ELEVATIONS**



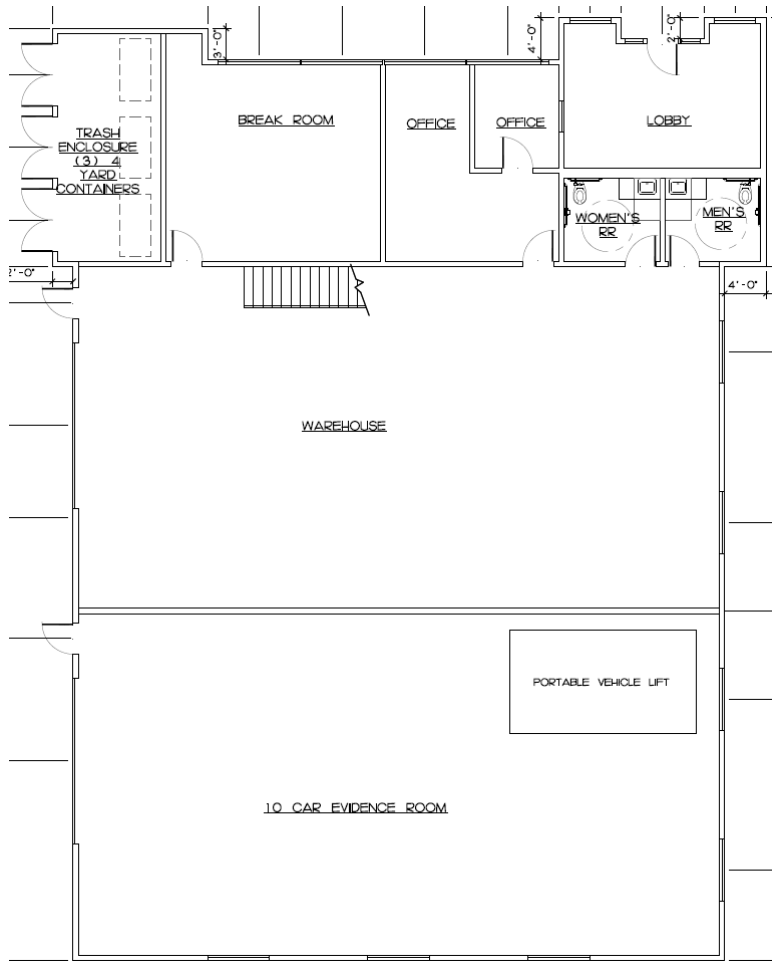
**East (Monterey Avenue) Elevation**



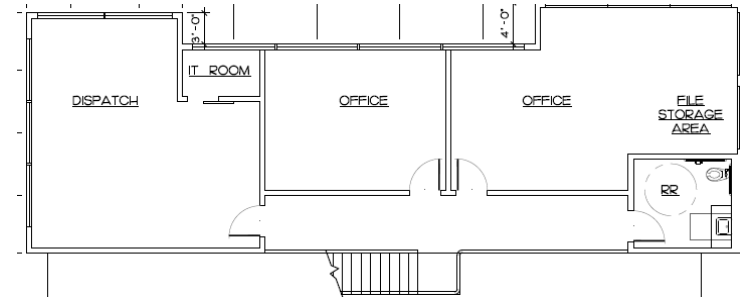
**West Elevation**

**Main Entrance**

**Exhibit F—FLOOR PLANS**

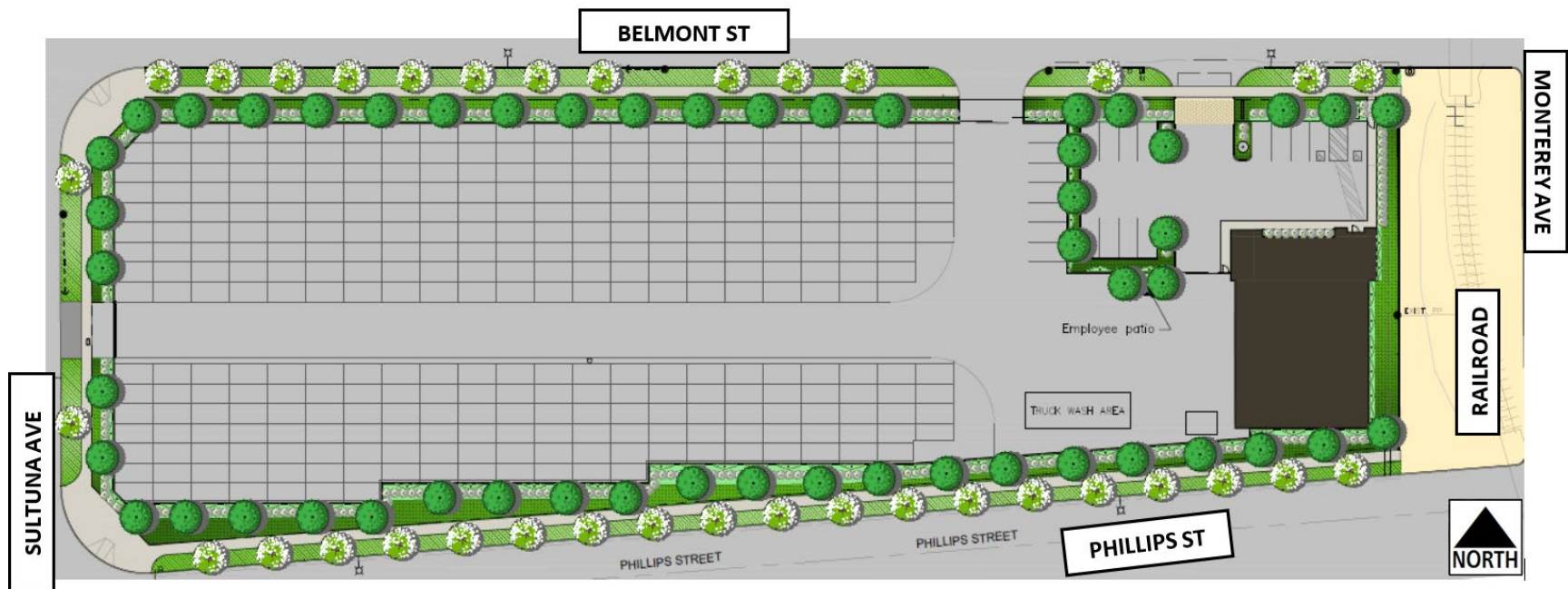


**First Floor**



**Second Floor**

**Exhibit G—LANDSCAPE PLAN**



***Exhibit H—SITE PHOTOS***



**Northwest Corner View of Project Site (Belmont Street and Sultana Avenue)**



**Southwest Corner View of Project Site (Sultana Ave & Phillips St)**

**Exhibit I—SITE PHOTOS**



**Southeast Corner View of Project Site (Phillips St & Railroad Tracks)**



**Northeast Corner of Project Site (Railroad Easement and Belmont Street)**



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PCUP18-021, A CONDITIONAL USE PERMIT TO ESTABLISH AND OPERATE AN OUTDOOR AUTOMOBILE STORAGE YARD (TOW YARD) ON 3.1 ACRES OF LAND LOCATED AT 580 EAST BELMONT STREET, WITHIN THE IL (LIGHT INDUSTRIAL) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF (APNS: 1049-491-01, 1049-491-02 & 1049-491-03).

WHEREAS, Four Sisters Enterprises, LLC ("Applicant") has filed an Application for the approval of a Conditional Use Permit, File No. PCUP18-021, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 3.1 acres of land located at the southwest corner of Belmont Street and Monterey Avenue, within the IL (Light Industrial) zoning district; and

WHEREAS, the property to the north of the project site is within the IL (Light Industrial) zoning district and is developed with industrial buildings and a metal rebar storage yard. The properties to the east are within the LDR-5 (Low Density Residential – 2.1 to 5.0 DUs/Acre) & IL (Light Industrial) zoning districts and are developed with the Southern Pacific Railroad and Single-Family Homes. The property to the south is located within the LDR-5 (Low Density Residential – 2.1 to 5.0 DUs/Acre) and is developed with single-family homes. The property to the west is within the MDR-11 (Medium Density Residential – 5.1 to 11.0 DUs/Acre) zoning district and is developed with single-family homes; and

WHEREAS, on June 21, 2018, the applicant submitted a Conditional Use Permit (File No. PCUP18-021), in conjunction with Development Plan (File No. PDEV18-022), to establish and operate an outdoor automobile storage yard (tow yard) and to construct a 6,870 square foot industrial building on the Project site; and

WHEREAS, the proposed outdoor automobile storage yard (tow yard) consists of a 79,569 square foot outdoor vehicle storage area that will accommodate up to 324 motor vehicles, a 6,870 square foot two-story industrial building, and an 800 square foot outdoor truck wash area; and

WHEREAS, pursuant to the Ontario Development Code, Conditional Use Permit approval is required to establish an outdoor automobile storage yard (tow yard) within the IL (Light Industrial) zoning district. A Conditional Use permit is required in order adequately review the potential impacts the use might have on the surrounding properties and eliminate or mitigate those impacts; and

WHEREAS, the Applicant operates another towing facility in the City of Ontario, located at 810 East Main Street (File No. PCUP17-016), which was approved by the Zoning Administrator on February 21, 2018. The Applicant is now requesting approval to establish and operate an additional vehicle towing facility and motor vehicle impound storage yard in the City; and

WHEREAS, vehicles not involved in serious accidents will be kept within the outdoor storage yard area, which is screened by a decorative 8-foot tall masonry split face block wall, with decorative pilasters and cap, designed to match the new building. The towing service operation will have a staff of 3 full-time employees. The proposed business hours of operation are Monday thru Friday, 8:00 a.m. to 5:00 p.m., for fifty-weeks out of each year. Pepe's Towing serves as a contracted Rotational Towing Service with the City and, therefore, will be required to operate 24-hours, seven-days a week for a two-week period out of each year; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 18, 2020, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision DAB20-023, recommending the Planning Commission approve the Application; and

WHEREAS, on May 26, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

**SECTION 1: Environmental Determination and Findings.** As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines and meets each of the following conditions: (a) the Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations; (b) the proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses; (c) the Project site has no value as habitat for endangered, rare, or threatened species; (d) approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and (e) the Project site can be adequately served by all required utilities and public services; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

**SECTION 2: *Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

**SECTION 3: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport (“ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the Planning Commission, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

**SECTION 4: *Concluding Facts and Reasons.*** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The scale and intensity of the proposed land use would be consistent with the scale and intensity of land uses intended for the particular zoning or land use district.*** The proposed location of the Conditional Use Permit is in accord with the objectives and purposes of the City of Ontario Development Code and the IL (Light Industrial) zoning district, and the scale and intensity of land uses intended for the zoning

district in which the use is proposed to be located. Furthermore, the proposed outdoor automobile storage yard (tow yard) will be established and operated consistent with the objectives and purposes, and development standards and guidelines, of the IL (Light Industrial) zoning district; and

(2) ***The proposed use at the proposed location, and the manner in which it will be operated and maintained, is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed industrial building and outdoor automobile storage yard (tow yard) will be located within the industrial land use district of the Policy Plan Land Use Map, and the IL (Light Industrial) zoning district. The development standards, and the conditions of approval under which the proposed land use will be established, operated, and maintained, are consistent with the goals, policies, plans, and exhibits of the Vision, City Council Priorities, and Policy Plan (General Plan) components of The Ontario Plan. The proposed facility will provide infrastructure compatibility and will be aesthetically pleasing and in context with the community character (LU2-6). Furthermore, the proposed facility is consistent with City Council goals, such as: invest in the growth and evolution of the City's Economy, and focus resources in Ontario's commercial and residential neighborhoods; and

(3) ***The proposed use at the proposed location, and the manner in which it will be operated and maintained, is consistent with the objectives and requirements of the Development Code and any applicable specific plan or planned unit development.*** The proposed outdoor automobile storage yard (tow yard) is located within the Industrial land use district, and the IL (Light Industrial) zoning district, and has been reviewed and conditioned to ensure the establishment, operation and maintenance of the proposed land use is consistent with all applicable objectives, purposes, standards, and guidelines of the Development Code and IL (Light Industrial) zoning district. The proposed outdoor automobile storage yard (tow yard) and industrial building will be located on the southeast corner of the Project site and adjacent to the Southern Pacific Railroad. The towing yard area will be screened by 10-foot landscaping and an 8-foot tall, decorative split-face block wall, which will screen the towing yard parking area from public view; and

(4) ***The establishment, maintenance, and operation of the proposed use at the proposed location would not be detrimental or injurious to property and improvements within the vicinity, nor would it be detrimental to the health, safety, or general welfare of persons residing or working in the surrounding neighborhood.*** The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: (a) the purposes of the Development Code are maintained; (b) the Project will not endanger the public health, safety or general welfare; (c) the Project will not result in any significant

environmental impacts; and (d) the Project will be in harmony with the surrounding area in which it is proposed to be located.

**SECTION 5: *Planning Commission Action.*** Based on the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the Application, subject to each and every condition set forth in the Department reports included as “Attachment A” and incorporated herein by this reference.

**SECTION 6: *Indemnification.*** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

**SECTION 7: *Custodian of Records.*** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East “B” Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario. The records are available for inspection by any interested person, upon request.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of May 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

---

Jim Willoughby  
Planning Commission Chairman

ATTEST:

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Cathy Wahlstrom  
Planning Director and  
Secretary of Planning Commission

STATE OF CALIFORNIA            )  
COUNTY OF SAN BERNARDINO )  
CITY OF ONTARIO                )

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution [Insert PC No.] was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 26, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

---

Gwen Berendsen  
Secretary Pro Tempore



**ATTACHMENT A:**

**File No. PCUP18-021  
Departmental Conditions of Approval**

*(Departmental conditions of approval to follow this page)*



City of Ontario  
Planning Department  
303 East B Street  
Ontario, California 91764  
Phone: 909.395.2036  
Fax: 909.395.2420

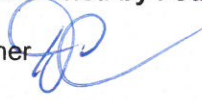
## ***Planning Department Land Development Division Conditions of Approval***

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**Meeting Date:** May 26, 2020

**File Nos.:** PDEV18-022 & PCUP18-021

**Project Description:** A Development Plan (File No PDEV18-022) to construct a 6,870 square-foot industrial building and a Conditional Use Permit (File No. PCUP18-021) to establish and operate a towing service on 3.1 acres of land located at 580 East Belmont Street within the IL (Light Industrial) zoning district. (APNs: 1049-491-01, 1049-491-02 and 1049-491-03); **submitted by Four Sisters Enterprises, LLC.**

**Prepared By:** Denny D. Chen, Associate Planner   
Phone: 909.395.2424 (direct)  
Email: dchen@ontarioca.gov

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The Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

**1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

**2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

**2.1 Time Limits.**

(a) Development Plan (File No. PDEV18-022) approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

(b) Conditional Use Permit (File No. PCUP18-021) approval shall become null and void 1 year following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

**1.1 General Requirements.** The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

#### 1.2 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(c) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

(d) Any dead or missing landscaping shall be replaced and a sprinkler system shall be installed that is set to a timer.

(e) Outside plaza areas, in front of the office pods, shall feature decorative color paving.

1.3 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

#### 1.4 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) All drive approaches shall be provided with an enhanced color pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(e) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(f) Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

**1.5 Outdoor Loading and Storage Areas.**

(a) Loading facilities shall be designed and constructed pursuant to Development Code Division 6.03 (Off-Street Parking and Loading).

(b) Areas designated for off-street parking, loading, and vehicular circulation and maneuvering, shall not be used for the outdoor storage of materials or equipment.

(c) Outdoor loading and storage areas, and loading doors, shall be screened from public view pursuant to the requirements of Development Code Paragraph 6.02.025.A.2 (Screening of Outdoor Loading and Storage Areas, and Loading Doors) Et Seq.

(d) Outdoor loading and storage areas shall be provided with gates that are view-obstructing by one of the following methods:

(i) Construct gates with a perforated metal sheet affixed to the inside of the gate surface (50 percent screen); or

(ii) Construct gates with minimum one-inch square tube steel pickets spaced at maximum 2-inches apart.

(e) The minimum gate height for screen wall openings shall be established based upon the corresponding wall height, as follows:

<b>Screen Wall Height</b>	<b>Minimum Gate Height</b>
14 feet:	10 feet
12 feet:	9 feet
10 feet:	8 feet
8 feet:	8 feet
6 feet:	6 feet

**1.6 Site Lighting.**

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

(c) Wall packs will not be allowed within the public view areas. All fixtures shall be decorative. Cut sheets shall be submitted to Planning during plan check review.

(d) Decorative light fixtures shall be used on the building to match the proposed architectural style. Applicant shall work with staff, during the plan check process, to final light locations.

**1.7 Mechanical and Rooftop Equipment.**

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

**1.8** Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

**1.9** Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

**1.10** Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

**1.11** Environmental Review.

(a) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines, meeting the following conditions:

(i) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations;

(ii) The proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses;

(iii) The project site has no value as habitat for endangered, rare, or threatened species;

(iv) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and

(v) The Project site can be adequately served by all required utilities and public services.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

**1.12** Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

**1.13** Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County "Clerk of the Board of Supervisors," along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

#### 1.14 Additional Requirements.

(a) The proposed metal canopy over the office entrance and all canopies located on the first floor (North Elevation) shall project from the face of the building a minimum of 5-feet. All other metal canopies proposed on the second floor shall project a minimum of 4-feet. The applicant shall work with staff, during the plan check process, to finalize the dimensions/projections of the metal canopies.

(b) An 8-foot tall decorative masonry block wall with decorative cap, shall be constructed along the north, west, and south sides of the property to screen the towing yard area from public view. The minimum height of the proposed tubular steel/metal fence to be constructed along the east and north sides of the property shall be 8 feet, from grade level.

(c) All walls shall be painted with anti-graffiti paint to prevent tagging.

(d) The maximum height of fences and walls, within the project's front setback area, shall be 6 feet and shall be non-view obstructing type of fencing, such as wrought iron or tubular steel. Installation of chain link fences around the perimeter of the project site shall be prohibited. All steel/metal fencing shall be powder coated to prevent rust.

(e) Building roof projections shall have returns of a minimum length of 6-feet, to avoid a false front/unfinished appearance. Applicant shall work with staff during plan check process to finalize the minimum length of the roof returns along the building's North, East, South & West elevations, and wherever roof returns are required to be provided. If rear of towers will be visible, towers shall be enclosed on all sides.

(f) Applicant shall work with Planning Department staff during the plan check process, to ensure that all building elevations & architectural treatments are consistent with the approved building elevations and to also ensure that the project will feature a modern architecture style that exemplifies the high-quality architecture required by the Ontario Development Code Design Guidelines.

(g) A minimum of 16 on-site parking spaces shall be provided.

(h) Decorative paving shall be provided at the employee break area, on the two main driveway entries along Belmont Street, and on the driveway along Sultana Avenue.

(i) Applicant shall work with Planning staff, during the plan check process, to add a metal cover on the south side of the truck wash canopy, in order to screen the canopy's steel columns and the trucks utilizing the truck wash area. The metal cover and/or screen material shall match the proposed industrial building's color, texture & architecture.

(j) Additional decorative split-face block shall be added, along the building's north elevation, below the window area. The decorative block shall match the material and color on the two towers.

**(k)** All new & proposed signage shall be reviewed and approved by the Planning Department and shall comply with the requirements of the Ontario Development Code – Sign Regulations (Division 8.1).

**(l)** A final site inspection shall be required from the Planning Department prior to Building Department's final occupancy.

**(m)** Pepe's Towing shall obtain and maintain an active City Business License.

**(n)** All applicable Conditions of Approval from other City departments shall be met and addressed by the applicant.

**(o)** The Conditional Use Permit is subject to all applicable provisions of the City's Rotational Towing Services Ordinance No. 3008.



**ENGINEERING DEPARTMENT  
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario  
Municipal Utilities Company and Information Technology & Management Services Department Conditions incorporated)

<input checked="" type="checkbox"/> <b>DEVELOPMENT PLAN</b> <input type="checkbox"/> <b>OTHER</b>	<input type="checkbox"/> <b>PARCEL MAP</b>	<input type="checkbox"/> <b>TRACT MAP</b>  <input type="checkbox"/> <b>FOR CONDOMINIUM PURPOSES</b>
<b>PROJECT FILE NO. PDEV18-022</b>  <b>RELATED FILE NO(S). PCUP18-021</b>		
<input checked="" type="checkbox"/> <b>ORIGINAL</b> <input type="checkbox"/> <b>REVISED: ___/___/___</b>		

**CITY PROJECT ENGINEER & PHONE NO:**      **Jesus Plasencia, (909) 395-2128**

**CITY PROJECT PLANNER & PHONE NO:**      **Denny Chen, (909) 395-2424**

**DAB MEETING DATE:**      **May 18, 2020**

**PROJECT NAME / DESCRIPTION:**      **A Development Plan to construct a 6,870 square foot industrial building in conjunction with a CUP to establish and operate a towing service on 3.1 acres of land.**

**LOCATION:**      **580 East Belmont Street**

**APPLICANT:**      **Four Sisters Enterprises, LLC**

**REVIEWED BY:**           5/5/20  
    **Bryan Lirley, P.E.**      **Date**  
    **Principal Engineer**

**APPROVED BY:**           5/7/20  
    **Raymond Lee, P.E.**      **Date**  
    **Assistant City Engineer**





**THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.**

**1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL:** **Check When Complete**

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:   
 \_\_\_\_\_ feet on \_\_\_\_\_  
 Property line corner 'cut-back' required at the intersection of \_\_\_\_\_  
 and \_\_\_\_\_.
- 1.02 Dedicate to the City of Ontario, the following easement(s): \_\_\_\_\_   
 \_\_\_\_\_
- 1.03 Restrict vehicular access to the site as follows: \_\_\_\_\_
- 1.04 Vacate the following street(s) and/or easement(s): \_\_\_\_\_
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.
- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.
- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).
- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Management Services Department at (909) 395-2124 regarding this requirement.   
 (1) \_\_\_\_\_  
 (2) \_\_\_\_\_
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.



- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: [www.ci.ontario.ca.us](http://www.ci.ontario.ca.us)) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Management Services at (909) 395-2353 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments: 
  - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
  - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
  - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: \_\_\_\_\_

**2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:**

**A. GENERAL  
 ( Permits includes Grading, Building, Demolition and Encroachment )**

- 2.01 Record Parcel Map/Tract Map No. \_\_\_\_\_ pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a duplicate photo mylar of the recorded map to the City Engineer's office.
- 2.03 Note that the subject parcel is a recognized parcel in the City of Ontario per \_\_\_\_\_
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of \_\_\_\_\_
- 2.05 Apply for a:  Conforming Grant Deed;  Lot Line Adjustment 
  - Make a Dedication of Easement (see Condition 2.11)
- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan ( WQMP), as applicable to the project.



- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).
- 2.08 **Submit a soils/geology report.**
- 2.09 **Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:**
- State of California Department of Transportation (Caltrans)
  - San Bernardino County Road Department (SBCRD)
  - San Bernardino County Flood Control District (SBCFCD)
  - Federal Emergency Management Agency (FEMA)
  - Cucamonga Valley Water District (CVWD) for sewer/water service
  - United States Army Corps of Engineers (USACE)
  - California Department of Fish & Game
  - Inland Empire Utilities Agency (IEUA)
  - Other: Union Pacific Railroad**
- 2.10 **Dedicate to the City of Ontario the right-of-way described below:**
1. 33 feet north of centerline on Phillips Street along the project frontage.
  2. Property line corner 'cut-back' required at the intersection of Sultana Avenue and Belmont Street.
  3. Property line corner 'cut-back' required at the intersection of Sultana Avenue and Phillips Street.
- 2.11 **Dedicate to the City of Ontario the following easement(s) only if necessary:**
1. Sidewalk easement behind the proposed drive approaches located along Sultana Avenue and Belmont Street.
- 2.12 **New Model Colony (NMC) Developments:**
- 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
  - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.
  - 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall).
- 2.13 **Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.**



- 2.14 The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.
- 2.15 Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately \$69,139, shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.
- 2.16 Other conditions:
  - 1. The applicant/developer shall process a Lot Line Adjustment and a Conforming Grant Deed in order to consolidate the site into one (1) parcel and render the subject property as a recognized parcel in the City of Ontario. The Lot Line Adjustment and Conforming Grant Deed shall be recorded prior to issuance of a building permit.



**B. PUBLIC IMPROVEMENTS**  
 (See attached Exhibit 'A' for plan check submittal requirements.)

2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Phillips Street	Sultana Avenue	Belmont Street	Monterey Avenue
<b>Curb and Gutter</b>	<input checked="" type="checkbox"/> New; 20 ft. from C/L north <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New; 18 ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New; 18 ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
<b>AC Pavement</b>	<input checked="" type="checkbox"/> Replacement <sup>(1)</sup> <input checked="" type="checkbox"/> Widen 18 feet from C/L north along frontage, including pavm't transitions	<input checked="" type="checkbox"/> Replacement <sup>(1)</sup> <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input checked="" type="checkbox"/> Replacement <sup>(1)</sup> <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
<b>PCC Pavement (Truck Route Only)</b>	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Drive Approach</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Sidewalk</b>	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>ADA Access Ramp</b>	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Parkway</b>	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
<b>Raised Landscaped Median</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Fire Hydrant</b>	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation



<b>Sewer</b> (see Sec. 2.C)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
<b>Water</b> (see Sec. 2.D)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Traffic Signing and Striping</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Street Light</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
<b>Fiber Optics</b> (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
<b>Overhead Utilities</b>	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input checked="" type="checkbox"/> Relocate <sup>(2)</sup>	<input checked="" type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
<b>Removal of Improvements</b>	Remove existing metal poles			
<b>Other Improvements</b>				

**Specific notes for improvements listed in item no. 2.17, above:**

1. Refer to Condition of Approval No. 2.19.
2. Relocate existing power poles as needed to accommodate the construction of curb & gutter along the project frontage.

2.18 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s):



- 2.19 **Reconstruction of the full pavement structural section on Phillips Street, Sultana Avenue, and Belmont Street, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. A minimum 2" asphalt concrete grind and overly may be allowed in-lieu of the full pavement reconstruction if recommended by the soils engineer. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.**
- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide  water service   
 sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 **Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892).**
- 2.22 Other conditions: \_\_\_\_\_

**C. SEWER**

- 2.23 **An 8-inch sewer main is available for connection by this project in Belmont Street. (Ref: Sewer plan bar code: S12680)**
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 **Other conditions:** 
  1. **The Occupant of the building shall apply for a Wastewater Discharge Permit for their Establishment, and shall comply with all the requirements of their Wastewater Discharge Permit. Requirements of Wastewater Discharge Permit may include, but not limited to including: possibly installing a monitoring manhole, clarifier, or other sewer pretreatment equipment.**

**D. WATER**

- 2.27 A \_\_\_\_\_ inch water main is available for connection by this project in \_\_\_\_\_. (Ref: Water plan bar code: \_\_\_\_\_)
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.
- 2.29 **Other conditions:** 
  1. **Replace the existing 4" water line in Belmont, between Sultana and Monterey, with a new 12". All existing appurtenances shall be reconnected to the new 12" main via the installation of new service laterals (no coupling) from main to appurtenance. Upgrade any/all hydrants with breakaway check valve per City standards.**
  2. **Install one fire hydrant on the north side of Phillips Street (south side of property), connecting off the 1074 PZ water line, fronting the property.**

**E. RECYCLED WATER**

- 2.30 A \_\_\_\_\_ inch recycled water main is available for connection by this project in \_\_\_\_\_. (Ref: Recycled Water plan bar code: \_\_\_\_\_)
- 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.



2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.

2.33 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.

2.34 Other conditions: \_\_\_\_\_

**F. TRAFFIC / TRANSPORTATION**

2.35 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:

1. On-site and off-site circulation
2. Traffic level of service (LOS) at 'build-out' and future years
3. Impact at specific intersections as selected by the City Engineer

2.36 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.

2.37 Other conditions:

1. **The applicant/developer shall be responsible to design and construct the following streets to their ultimate half-width along the project frontage:**

- Sultana Avenue (66'R/W, 36' C/C)
- Belmont Street (66' R/W, 36' C/C)
- Phillips Street (66' R/W, 40' C/C)

Improvements shall include, but not be limited to, concrete curb and gutter, sidewalk, LED street lights, landscaped parkways, signing & striping, and necessary pavement transitions as deemed necessary by the City Engineer.

2. **The Applicant/Developer shall be responsible to design and construct in-fill public street lights along the property frontages of Sultana Avenue, Belmont Street and Phillips Street, in accordance with the Traffic and Transportation Design Guidelines Section 1.4 Street Light Plans, City of Ontario Standard Drawing No. 5101, and to the satisfaction of the City Engineer.**

3. **Driveways shall be designed and constructed in accordance with City of Ontario Standard Drawing No. 1204 for Commercial Driveway Approach.**

4. **Property frontages along Sultana Avenue, Belmont Street, Phillips Street and Monterey Avenue shall be signed "No Parking Anytime."**

5. **Proposed gates on Belmont Street shall remain open at all times during business hours.**

6. **The applicant/developer's engineer-of-record shall meet with City Engineering staff prior to starting signing/striping and/or street lighting design.**

7. **The applicant/developer shall obtain all rights-of-way necessary to construct all required improvements.**

8. **All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City of Ontario Standard Drawing No. 1309.**





**G. DRAINAGE / HYDROLOGY**

- 2.38 A \_\_\_\_\_ inch storm drain main is available to accept flows from this project in \_\_\_\_\_. (Ref: Storm Drain plan bar code:\_\_\_\_\_)
- 2.39 **Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.**
- 2.40 **An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100-year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.**
- 2.41 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.
- 2.42 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.43 **Other conditions:**
  1. **The applicant/developer shall pay an in-lieu fee for future storm drain in Sultana Avenue along project frontage valued at \$75,454.**

**H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)**

- 2.44 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels. If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted. Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.45 **Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.**
- 2.46 Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.



- 2.47 Other conditions:
1. **Activities resulting in land disturbance of one acre or more is required to obtain coverage under the Construction General Permit (CGP). The owner is the legally responsible person (LRP) of the site and shall have a Stormwater Pollution Prevention Plan (SWPPP) developed and submitted through the SMARTS website at <https://smarts.waterboards.ca.gov/smarts/faces/SwSmartsLogin.xhtml>**

**J. SPECIAL DISTRICTS**

- 2.48 File an application, together with an initial payment deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community facilities District Act of 1982. The application and fee shall be submitted a minimum four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact the Financial Services Department at (909) 395-2353 to initiate the CFD application process.
- 2.49 Other conditions: \_\_\_\_\_

**K. FIBER OPTIC**

- 2.50 **Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole if within close proximity. If there is no OntarioNet handhole, conduit infrastructure shall terminate at either end of project frontage in a handhole.**
- 2.51 **Refer to the City's Fiber Optic Master Plan and other related documents on the City website for design and layout guidelines. Contact the Information Technology Department at (909) 395-2000, for more information regarding the requirement to install fiber optic improvements.**

**L. Solid Waste**

- 2.52 Onsite solid waste shall be designed in accordance with the City's Solid Waste Manual location at:
- <http://www.ontarioca.gov/municipal-utilities-company/solid-waste>
- 2.53 Other conditions:
1. **Prior to approval of the any building permits, the Conceptual Solid Waste Handling Plan needs to be updated and converted into a Final Solid Waste Handling Plan Sheet and shall be submitted accompanying the Precise Grading Plan Submittal to the City/OMUC for review and approval. See "Solid Waste Handling Plan (SWHP) Requirements" document for details.**
  2. **The applicant shall submit an Integrated Waste Management Report for review and approval with the Precise Grading Plan. This report shall address the management of all integrated waste (Refuse, Recycling, Organics, etc.). The IWMR shall demonstrate compliance with the "Integrated Waste Management Report Requirements" document.**



**3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:**

- 3.01 **Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.**
- 3.02 Complete all requirements for recycled water usage. 
  - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
  - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
  - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 **The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.**
- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 **Confirm payment of all Development Impact Fees (DIF) to the Building Department.**
- 3.06 **Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studie and reports (i.e. hydrology, traffic, WQMP, etc.).**

**4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:**

- 4.01 **Complete all Conditions of Approval listed under Sections 1-3 above.**
- 4.02 **Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.**
- 4.03 **The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.**



**EXHIBIT 'A'**

**ENGINEERING DEPARTMENT  
First Plan Check Submittal Checklist**

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**Project Number: PDEV18-022**

**The following items are required to be included with the first plan check submittal:**

1.  **A copy of this check list**
2.  **Payment of fee for Plan Checking**
3.  **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4.  **One (1) copy of project Conditions of Approval**
5.  Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).
6.  **Three (3) sets of Public Street improvement plan with street cross-sections**
7.  Three (3) sets of Private Street improvement plan with street cross-sections
8.  **Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
9.  Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10.  Four (4) sets of Public Sewer improvement plan
11.  Five (5) sets of Public Storm Drain improvement plan
12.  **Three (3) sets of Public Street Light improvement plan**
13.  **Three (3) sets of Signing and Striping improvement plan**
14.  **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15.  **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
16.  Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
17.  **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
18.  **One (1) copy of Hydrology/Drainage study**
19.  **One (1) copy of Soils/Geology report**
20.  Payment for Final Map/Parcel Map processing fee
21.  Three (3) copies of Final Map/Parcel Map




- 22.  One (1) copy of approved Tentative Map
- 23.  **One (1) copy of Preliminary Title Report (current within 30 days)**
- 24.  One (1) copy of Traverse Closure Calculations
- 25.  One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.
- 26.  Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 27.  **Other: Update the sewer improvement plan for Belmont Street to show the proposed service connection to the project site.**

**CITY OF ONTARIO  
LANDSCAPE PLANNING  
DIVISION**

303 East "B" Street, Ontario, CA 91764

**CONDITIONS OF APPROVAL**

Sign Off

  
Jamie Richardson, Sr. Landscape Planner

2/13/20  
Date

Reviewer's Name:  
**Jamie Richardson, Sr. Landscape Planner**

Phone:  
**(909) 395-2615**

D.A.B. File No.:  
PDEV18-022 Rev 4

Case Planner:  
Denny Chen

Project Name and Location:

**Pepe's Towing**  
554 E Belmont St.

Applicant/Representative:  
Howard Parcell Company  
4854 Main St  
Yorba Linda, CA 92886

**A Preliminary Landscape Plan (dated 2/13/20) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.**

**A Preliminary Landscape Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.**

**A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE**

**PREVIOUS COMMENTS – 11/13/19**

Civil/ Site Plans

1. The tree inventory identifies 48" of trunk diameter to be mitigated; Schinus mole No. 14.
  - a. New 15 gallon trees min 1" diameter trunk, in addition to trees required; total of 48 trees.
  - b. New 24" box trees min 1.5" diameter trunk, in addition to trees required; total of 32 trees
  - c. Upsizing trees on the plan one size larger such as 15 gallon to 24" box, or 24" to 36" box size.
  - d. Monetary value of the trees removed as identified in the "Guide for Plant Appraisal", approved certified arborist plant appraiser, or may be equal to the value of the installation cost of planting, fertilizing, staking and irrigating 15 gallon trees, (100\$ each) to the City of Ontario Historic Preservation Fund for city tree planting or city approved combination of the above items; total of \$4,800.
2. Show transformers and backflow devices located in planter areas and set back 4' from paving; dimension on plan. Locate on level grade. Coordinate with landscape plans.
3. ~~2/13/20 Spoke with Mr. George Estrada over the counter; locate the wall at the Belmont entry of the tow yard so the landscape planter is in the public parking lot. Provide an employee break area on the inside of the tow yard adjacent to the 4 public parking spaces include a table and/or bench and 2 broad canopy shade trees such as Platanus racemosa 24" box. Provide irrigation to trees and mulch.~~ 3/12/20 updated plans received. Condition has been addressed.

Landscape Plans

4. Locate light standards, fire hydrants, water and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans



# CITY OF ONTARIO MEMORANDUM

## Development Plan Review

Project: PDEV18-022 (PCUP18-021)

Date: February 13<sup>th</sup>, 2020

Location: 554 East Belmont Street

By: Nathan Kuan

*The following shall become conditions of approval for the project.*

### Conditions:

1. The applicant/developer shall be responsible to design and construct the following streets to their ultimate half-width along the project frontage:
  - Sultana Avenue (66' R/W, 36' C/C)
  - Belmont Street (66' R/W, 36' C/C)
  - Phillips Street (66' R/W, 40' C/C)Improvements shall include, but not be limited to: concrete curb and gutter, sidewalk, LED street lights, landscaped parkways, signing & striping, and necessary pavement transitions as deemed necessary by the City Engineer.
2. The Applicant/Developer shall be responsible to design and construct in-fill public street lights and service pedestals along the property frontages of Sultana Avenue, Belmont Street and Phillips Street, in accordance with the Traffic and Transportation Design Guidelines Section 1.4 Street Light Plans, City of Ontario Standard Drawing No. 5101, and to the satisfaction of the City Engineer.
3. Driveways shall be designed and constructed in accordance with City of Ontario Standard Drawing No. 1204 for Commercial Driveway Approach.
4. Property frontages along Sultana Avenue, Belmont Street, Phillips Street and Monterey Avenue shall be signed "No Parking Anytime."
5. Proposed gates on Belmont Street shall remain open at all times during business hours.
6. The applicant/developer's engineer-of-record shall meet with City Engineering staff prior to starting signing/striping and/or street lighting design.
7. The applicant/developer shall obtain all rights-of-way necessary to construct all required improvements.
8. All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City of Ontario Standard Drawing No. 1309.

nk;

# CITY OF ONTARIO

## MEMORANDUM

**TO:** PLANNING DEPARTMENT, Denny Chen  
**FROM:** BUILDING DEPARTMENT, Kevin Shear  
**DATE:** June 11, 2019  
**SUBJECT:** PDEV18-022

- 
- The plan **does** adequately address the departmental concerns at this time.
- No comments
- Report below.

---

### Conditions of Approval

1. See previous report for Conditions.

KS:lr



# CITY OF ONTARIO

## MEMORANDUM

**TO:** PLANNING DEPARTMENT, Denny Chen  
**FROM:** BUILDING DEPARTMENT, Kevin Shear  
**DATE:** June 27, 2018  
**SUBJECT:** PDEV18-022

- 
- The plan **does** adequately address the departmental concerns at this time.
- No comments
- Report below.

---

### Conditions of Approval

1. The site address for this project will be 580 E Belmont St.
2. The lot lines are to be removed.

KS:lm



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Denny Chen, Associate Planner  
Planning Department

**FROM:** Paul Ehrman, Deputy Fire Chief/Fire Marshal  
Fire Department

**DATE:** July 6, 2018

**SUBJECT:** PDEV18-022 - A Development Plan to construct a 6,660-square foot industrial building on 3.01 acres of land located at the southwest corner of Belmont Street and Monterey Avenue, at 554 East Belmont Street, within the IL (Light Industrial) zoning district (APNs: 1049-491-01, 1049-491-02 and 1049-491-03). Related File: PCUP18-021

- 
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
- 

### **SITE AND BUILDING FEATURES:**

- A. 2016 CBC Type of Construction: V
- B. Type of Roof Materials: Ordinary
- C. Ground Floor Area(s): 5,460 Sq. Ft.
- D. Number of Stories: 2
- E. Total Square Footage: 6,660 Sq. Ft.
- F. 2016 CBC Occupancy Classification(s): B, S

## CONDITIONS OF APPROVAL:

### 1.0 GENERAL

- 1.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at [www.ontarioca.gov](http://www.ontarioca.gov), click on "Fire Department" and then on "Standards and Forms."
- 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

### 2.0 FIRE DEPARTMENT ACCESS

- 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25') inside and forty-five feet (45') outside turning radius per Standard #B-005.
- 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150') in length shall have an approved turn-around per Standard #B-002.
- 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-six (26) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services..

### 3.0 WATER SUPPLY

- 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 2000 gallons per minute (g.p.m.) for 2 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

### 4.0 FIRE PROTECTION SYSTEMS

- 4.1 On-site private fire hydrants are required per Standard #D-005, and identified in accordance with Standard #D-002. Installation and locations(s) are subject to the approval of the Fire Department. An application with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- 4.4 Wood frame buildings that are to be sprinkled shall have these systems in service (but not necessarily finalized) before the building is enclosed.

### 5.0 BUILDING CONSTRUCTION FEATURES

- 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.

- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.
- ☒ 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.

## 6.0 OTHER SPECIAL USES

- ☒ 6.1 The storage, use, dispensing, or handling of any hazardous materials shall be approved by the Fire Department, and adequate fire protection features shall be required. If hazardous materials are proposed, a Fire Department Hazardous Materials Information Packet, including Disclosure Form and Information Worksheet, shall be completed and submitted with Material Safety Data Sheets to the Fire Department along with building construction plans.
- ☒ 6.3 Underground fuel tanks, their associated piping and dispensers shall be reviewed, approved, and permitted by Ontario Building Department, Ontario Fire Department, and San Bernardino County Fire Department Hazardous Materials Division. In fueling facilities, an exterior emergency pump shut-off switch shall be provided.



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Denny Chen, Planning Department

**FROM:** Douglas Sorel, Police Department

**DATE:** July 26, 2018

**SUBJECT:** PDEV18-022 & PCUP18-021: A DEVELOPMENT PLAN AND  
CONDITIONAL USE PERMIT TO ESTABLISH A TOW YARD AT 554  
EAST BELMONT STREET

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The “Standard Conditions of Approval” contained in Resolution No. 2017-027 and Ontario Development Code Section 5.03.310 apply. The applicant shall read and be thoroughly familiar with these conditions, including but not limited to, the requirements listed below.

- Required lighting for all walkways, driveways, doorways, parking areas, and other areas used by the public shall be provided and operate on photosensor. Photometrics shall be provided to the Police Department. Photometrics shall include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the building as stated in the Standard Conditions. The numbers shall be at a minimum 3 feet tall and 1 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.
- The indoor storage of motor vehicles shall comply with all requirements of the fire and building codes.
- All vehicles stored outdoors shall be screened from public view by a minimum 8-foot high decorative masonry block wall.
- All vehicles stored shall comply with the requirements for the base zoning district.

In addition to the Standard Conditions, the Ontario Police Department places the following conditions on the project:

- While the Applicant serves as a contracted Rotational Towing Service with the City of Ontario they shall comply with all terms of Title 4-19 of the Ontario Municipal Code and their current Professional Services Agreement with the City.

- The applicant will be responsible for keeping the grounds of the business clean from debris and litter.
- Graffiti abatement by the business owner/licensee, or management shall be immediate and on-going on the premises, but in no event shall graffiti be allowed unabated on the premises for more than 72 hours. Abatement shall take the form of removal, or shall be covered/painted over with a color reasonably matching the color of the existing building, structure, or other surface being abated. Additionally, the business owner/licensee, or management shall notify the City within 24 hours at (909) 395-2626 (graffiti hotline) of any graffiti elsewhere on the property not under the business owner/licensee's or management control so that it may be abated by the property owner and/or the City's graffiti team.

The Applicant is invited to contact Douglas Sorel at (909) 408-1873 with any question or concerns regarding these conditions.

# AIRPORT LAND USE COMPATIBILITY PLANNING CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV18-022 & PCUP18-021

Address: 554 East Belmont Street

APN: 1049-491-01, 02 & 03

Existing Land Use: Vacant

Proposed Land Use: Development Plan and CUP to establish a tow yard and 6,600 SF 2-story industrial/warehouse building

Site Acreage: 3.01 ac Proposed Structure Height: 33 FT

ONT-IAC Project Review: N/A

Airport Influence Area: ONT

Reviewed By:

Lorena Mejia

Contact Info:

909-395-2276

Project Planner:

Rudy Zeledon

Date: 7/30/18

CD No.: 2018-036

PALU No.: n/a

## The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="radio"/> High Terrain Zone	<input type="radio"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input checked="" type="checkbox"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input checked="" type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input type="radio"/> Real Estate Transaction Disclosure
<input type="radio"/> Zone 3	<input type="radio"/> 60 - 65 dB CNEL	<input checked="" type="checkbox"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: 110 FT	
<input type="radio"/> Zone 5			

## The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1   
  Zone 2   
  Zone 3   
  Zone 4   
  Zone 5   
  Zone 6

Allowable Height: \_\_\_\_\_

## CONSISTENCY DETERMINATION

This proposed Project is:  Exempt from the ALUCP   
 Consistent   
 Consistent with Conditions   
 Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

See Attached Conditions

Airport Planner Signature: \_\_\_\_\_



# AIRPORT LAND USE COMPATIBILITY PLANNING

## CONSISTENCY DETERMINATION REPORT

CD No.: 2018-036

PALU No.: \_\_\_\_\_

### PROJECT CONDITIONS

1. This project is located within Airspace Avigation Easement Area and is required to file and record an Avigation Easement with the Ontario International Airport Authority prior to obtaining a Certificate of Occupancy.

## RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV18-022, A DEVELOPMENT PLAN TO CONSTRUCT A 6,870 SQUARE FOOT INDUSTRIAL BUILDING ON 3.1 ACRES OF LAND LOCATED AT 580 EAST BELMONT STREET, WITHIN THE IL (LIGHT INDUSTRIAL) ZONING DISTRICT AND MAKING FINDINGS IN SUPPORT THEREOF (APNS: 1049-491-01, 1049-491-02 & 1049-491-03).

WHEREAS, Four Sisters Enterprises, LLC ("Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV18-022, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 3.1 acres of land located on the southwest corner of Belmont Street and Monterey Avenue, at 580 East Belmont Street, within the IL (Light Industrial) zoning district; and

WHEREAS, on June 21, 2018, the applicant submitted a Development Plan (File No. PDEV18-022) to construct a 6,870 square foot industrial building on the Project site, in conjunction with a Conditional Use Permit (File No. PCUP18-021) to establish and operate an outdoor automobile storage yard (tow yard); and

WHEREAS, the property to the north of the project site is within the IL (Light Industrial) zoning district and is developed with industrial buildings and a metal rebar storage yard. The properties to the east are within the LDR-5 (Low Density Residential – 2.1 to 5.0 DUs/Acre) and IL (Light Industrial) zoning districts and are developed with the Southern Pacific Railroad rail line located in the Monterey Avenue right-of-way and single-family homes across Monterey Avenue. The property to the south is located within the LDR-5 (Low Density Residential – 2.1 to 5.0 DUs/Acre) and is developed with single-family homes. The property to the west is within the MDR-11 (Medium Density Residential – 5.1 to 11.0 DUs/Acre) zoning district and is developed with single-family homes; and

WHEREAS, the proposed building will be located at the southeast corner of the Project site, fronting on to Monterey Avenue (to the east) and Phillips Street (to the south). The new building is setback 48 feet from the Belmont Street (north) property line, 10 feet from the (east) property line, 10 feet from the Phillips Street (south) property line, and approximately 540 feet from the Sultana Avenue (west) property line; and

WHEREAS, the building's office and main entrance is situated at the northeast corner of the building, fronting onto Belmont Street. The employee and visitor parking areas are located to the north and northwest of the building and is secured by an 8-foot tall decorative tube steel fence; and

WHEREAS, the motor vehicle impound yard is proposed on the westerly two-thirds of the Project site, covering 79,569 square feet and is designed to park 324 impounded vehicles. The yard area will be screened from public view by a decorative 8-foot tall wall consisting of split-face masonry block with decorative pilasters and cap, located along the north, west, and south sides of the Project site; and

WHEREAS, the Project is required to provide a minimum of 16 off-street parking spaces and a total of 20 off-street parking spaces have been provided, exceeding the minimum number of required parking spaces; and

WHEREAS, primary vehicle access is provided by two driveways located along Belmont Street, immediately west of Monterey Avenue. A secondary vehicle access point is provided from Sultana Avenue, mid-block between Belmont and Phillips Streets; and

WHEREAS, the proposed building elevations illustrate the type of high-quality architecture promoted by the Ontario Development Code's development standards and design guidelines. The building will be composed of pre-painted/smooth faced sheet metal wall panels that incorporate a contemporary architectural style. The building design incorporates articulation in the building footprint, incorporating a combination of recessed and popped-out wall areas, and articulation in the building parapet/roof line, which serves to accentuate the building's roofline; and

WHEREAS, the Project proposes 15 percent landscape coverage, which meets the minimum Development Code requirement for corner lots located within the IL (Light Industrial) zoning district. The landscape pallet for the Project incorporates a mixture of 24-inch, 36-inch, and 48-inch box accent and shade trees; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration

of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (“ALUCP”), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 18, 2020, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision DAB20-024, recommending the Planning Commission approve the Application; and

WHEREAS, on March 26, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

**SECTION 1: Environmental Determination and Findings.** As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines and meets each of the following

conditions: (a) the Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations; (b) the proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses; (c) the Project site has no value as habitat for endangered, rare, or threatened species; (d) approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and (e) the Project site can be adequately served by all required utilities and public services; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

**SECTION 2: *Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

**SECTION 3: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport (“ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the Planning Commission, therefore, finds and determines

that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

**SECTION 4: Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the Industrial land use district of the Policy Plan Land Use Map, and the IL (Light Industrial) zoning district. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. The proposed towing yard facility will provide infrastructure compatibility and will be aesthetically pleasing and in context with the community character (LU2-6). Furthermore, the proposed development is consistent with City Council goals, such as: invest in the growth and evolution of the City's Economy, maintain the current high level of public safety, and focus resources in Ontario's commercial and residential neighborhoods; and

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the IL (Light Industrial) zoning district, as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions. In addition, the proposed 6,870 square foot industrial building will be construct on the southeast corner of the Project site to minimize any potential impacts on the residential homes. In addition, the proposed vehicle impound yard will be screened from public views by a combination of landscaping and an 8-foot tall, decorative split face block wall; and

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Planning Commission has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: (a) the purposes of the IL (Light Industrial) zoning district are maintained; (b) the Project will not endanger the public health, safety or general welfare; (c) the Project will not result in any significant environmental impacts; (d) the Project will be in harmony

with the area in which it is located; and (e) the Project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan; and

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.*** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the IL (Light Industrial) zoning district that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines. As a result of this review, the Planning Commission has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the IL (Light Industrial) zoning district.

**SECTION 5: Planning Commission Action.** Based on the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the Application, subject to each and every condition set forth in the Department reports included as “Attachment A” and incorporated herein by this reference.

**SECTION 6: Indemnification.** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

**SECTION 7: Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East “B” Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario. The records are available for inspection by any interested person, upon request.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of May 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

---

Jim Willoughby  
Planning Commission Chairman

ATTEST:

---

Cathy Wahlstrom  
Planning Director and  
Secretary of Planning Commission



STATE OF CALIFORNIA            )  
COUNTY OF SAN BERNARDINO )  
CITY OF ONTARIO                )

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution [Insert PC No.] was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 26, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

---

Gwen Berendsen  
Secretary Pro Tempore

**ATTACHMENT A:**

**File No. PDEV18-022  
Departmental Conditions of Approval**

*(Departmental conditions of approval to follow this page)*



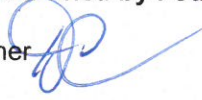
City of Ontario  
Planning Department  
303 East B Street  
Ontario, California 91764  
Phone: 909.395.2036  
Fax: 909.395.2420

## ***Planning Department Land Development Division Conditions of Approval***

**Meeting Date:** May 26, 2020

**File Nos.:** PDEV18-022 & PCUP18-021

**Project Description:** A Development Plan (File No PDEV18-022) to construct a 6,870 square-foot industrial building and a Conditional Use Permit (File No. PCUP18-021) to establish and operate a towing service on 3.1 acres of land located at 580 East Belmont Street within the IL (Light Industrial) zoning district. (APNs: 1049-491-01, 1049-491-02 and 1049-491-03); **submitted by Four Sisters Enterprises, LLC.**

**Prepared By:** Denny D. Chen, Associate Planner   
Phone: 909.395.2424 (direct)  
Email: dchen@ontarioca.gov

The Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

**1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

**2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

**2.1 Time Limits.**

(a) Development Plan (File No. PDEV18-022) approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

(b) Conditional Use Permit (File No. PCUP18-021) approval shall become null and void 1 year following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

**1.1 General Requirements.** The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

#### 1.2 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(c) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

(d) Any dead or missing landscaping shall be replaced and a sprinkler system shall be installed that is set to a timer.

(e) Outside plaza areas, in front of the office pods, shall feature decorative color paving.

1.3 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

#### 1.4 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) All drive approaches shall be provided with an enhanced color pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(e) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(f) Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

**1.5 Outdoor Loading and Storage Areas.**

(a) Loading facilities shall be designed and constructed pursuant to Development Code Division 6.03 (Off-Street Parking and Loading).

(b) Areas designated for off-street parking, loading, and vehicular circulation and maneuvering, shall not be used for the outdoor storage of materials or equipment.

(c) Outdoor loading and storage areas, and loading doors, shall be screened from public view pursuant to the requirements of Development Code Paragraph 6.02.025.A.2 (Screening of Outdoor Loading and Storage Areas, and Loading Doors) Et Seq.

(d) Outdoor loading and storage areas shall be provided with gates that are view-obstructing by one of the following methods:

(i) Construct gates with a perforated metal sheet affixed to the inside of the gate surface (50 percent screen); or

(ii) Construct gates with minimum one-inch square tube steel pickets spaced at maximum 2-inches apart.

(e) The minimum gate height for screen wall openings shall be established based upon the corresponding wall height, as follows:

<b>Screen Wall Height</b>	<b>Minimum Gate Height</b>
14 feet:	10 feet
12 feet:	9 feet
10 feet:	8 feet
8 feet:	8 feet
6 feet:	6 feet

**1.6 Site Lighting.**

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

(c) Wall packs will not be allowed within the public view areas. All fixtures shall be decorative. Cut sheets shall be submitted to Planning during plan check review.

(d) Decorative light fixtures shall be used on the building to match the proposed architectural style. Applicant shall work with staff, during the plan check process, to final light locations.

**1.7 Mechanical and Rooftop Equipment.**

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

**1.8** Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

**1.9** Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

**1.10** Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

**1.11** Environmental Review.

(a) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines, meeting the following conditions:

(i) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations;

(ii) The proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses;

(iii) The project site has no value as habitat for endangered, rare, or threatened species;

(iv) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and

(v) The Project site can be adequately served by all required utilities and public services.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

**1.12** Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

**1.13** Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County "Clerk of the Board of Supervisors," along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

#### 1.14 Additional Requirements.

(a) The proposed metal canopy over the office entrance and all canopies located on the first floor (North Elevation) shall project from the face of the building a minimum of 5-feet. All other metal canopies proposed on the second floor shall project a minimum of 4-feet. The applicant shall work with staff, during the plan check process, to finalize the dimensions/projections of the metal canopies.

(b) An 8-foot tall decorative masonry block wall with decorative cap, shall be constructed along the north, west, and south sides of the property to screen the towing yard area from public view. The minimum height of the proposed tubular steel/metal fence to be constructed along the east and north sides of the property shall be 8 feet, from grade level.

(c) All walls shall be painted with anti-graffiti paint to prevent tagging.

(d) The maximum height of fences and walls, within the project's front setback area, shall be 6 feet and shall be non-view obstructing type of fencing, such as wrought iron or tubular steel. Installation of chain link fences around the perimeter of the project site shall be prohibited. All steel/metal fencing shall be powder coated to prevent rust.

(e) Building roof projections shall have returns of a minimum length of 6-feet, to avoid a false front/unfinished appearance. Applicant shall work with staff during plan check process to finalize the minimum length of the roof returns along the building's North, East, South & West elevations, and wherever roof returns are required to be provided. If rear of towers will be visible, towers shall be enclosed on all sides.

(f) Applicant shall work with Planning Department staff during the plan check process, to ensure that all building elevations & architectural treatments are consistent with the approved building elevations and to also ensure that the project will feature a modern architecture style that exemplifies the high-quality architecture required by the Ontario Development Code Design Guidelines.

(g) A minimum of 16 on-site parking spaces shall be provided.

(h) Decorative paving shall be provided at the employee break area, on the two main driveway entries along Belmont Street, and on the driveway along Sultana Avenue.

(i) Applicant shall work with Planning staff, during the plan check process, to add a metal cover on the south side of the truck wash canopy, in order to screen the canopy's steel columns and the trucks utilizing the truck wash area. The metal cover and/or screen material shall match the proposed industrial building's color, texture & architecture.

(j) Additional decorative split-face block shall be added, along the building's north elevation, below the window area. The decorative block shall match the material and color on the two towers.

(k) All new & proposed signage shall be reviewed and approved by the Planning Department and shall comply with the requirements of the Ontario Development Code – Sign Regulations (Division 8.1).

(l) A final site inspection shall be required from the Planning Department prior to Building Department's final occupancy.

(m) Pepe's Towing shall obtain and maintain an active City Business License.

(n) All applicable Conditions of Approval from other City departments shall be met and addressed by the applicant.

(o) The Conditional Use Permit is subject to all applicable provisions of the City's Rotational Towing Services Ordinance No. 3008.





## ENGINEERING DEPARTMENT CONDITIONS OF APPROVAL

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario  
Municipal Utilities Company and Information Technology & Management Services Department Conditions incorporated)

<input checked="" type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input type="checkbox"/> TRACT MAP  <input type="checkbox"/> FOR CONDOMINIUM PURPOSES
<b>PROJECT FILE NO. PDEV18-022</b>  <b>RELATED FILE NO(S). PCUP18-021</b>	
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: ___/___/___	

**CITY PROJECT ENGINEER & PHONE NO:**      **Jesus Plasencia, (909) 395-2128**

**CITY PROJECT PLANNER & PHONE NO:**      **Denny Chen, (909) 395-2424**

**DAB MEETING DATE:**      **May 18, 2020**

**PROJECT NAME / DESCRIPTION:**      **A Development Plan to construct a 6,870 square foot industrial building in conjunction with a CUP to establish and operate a towing service on 3.1 acres of land.**

**LOCATION:**      **580 East Belmont Street**

**APPLICANT:**      **Four Sisters Enterprises, LLC**

**REVIEWED BY:**        
    Bryan Lirley, P.E.      5/5/20  
    Principal Engineer      Date

**APPROVED BY:**        
    Raymond Lee, P.E.      5/7/20  
    Assistant City Engineer      Date



**THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.**

**1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL:** **Check When Complete**

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:   
\_\_\_\_\_ feet on \_\_\_\_\_  
Property line corner 'cut-back' required at the intersection of \_\_\_\_\_  
and \_\_\_\_\_.
- 1.02 Dedicate to the City of Ontario, the following easement(s): \_\_\_\_\_   
\_\_\_\_\_
- 1.03 Restrict vehicular access to the site as follows: \_\_\_\_\_
- 1.04 Vacate the following street(s) and/or easement(s): \_\_\_\_\_
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.
- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.
- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).
- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Management Services Department at (909) 395-2124 regarding this requirement.   
(1) \_\_\_\_\_  
(2) \_\_\_\_\_
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.



- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: [www.ci.ontario.ca.us](http://www.ci.ontario.ca.us)) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Management Services at (909) 395-2353 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments: 
  - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
  - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
  - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: \_\_\_\_\_

**2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:**

**A. GENERAL  
 ( Permits includes Grading, Building, Demolition and Encroachment )**

- 2.01 Record Parcel Map/Tract Map No. \_\_\_\_\_ pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a duplicate photo mylar of the recorded map to the City Engineer's office.
- 2.03 Note that the subject parcel is a recognized parcel in the City of Ontario per \_\_\_\_\_
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of \_\_\_\_\_
- 2.05 Apply for a:  Conforming Grant Deed;  Lot Line Adjustment 
  - Make a Dedication of Easement (see Condition 2.11)
- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan ( WQMP), as applicable to the project.



- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).
- 2.08 **Submit a soils/geology report.**
- 2.09 **Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:**
- State of California Department of Transportation (Caltrans)
  - San Bernardino County Road Department (SBCRD)
  - San Bernardino County Flood Control District (SBCFCD)
  - Federal Emergency Management Agency (FEMA)
  - Cucamonga Valley Water District (CVWD) for sewer/water service
  - United States Army Corps of Engineers (USACE)
  - California Department of Fish & Game
  - Inland Empire Utilities Agency (IEUA)
  - Other: Union Pacific Railroad**
- 2.10 **Dedicate to the City of Ontario the right-of-way described below:**
1. 33 feet north of centerline on Phillips Street along the project frontage.
  2. Property line corner 'cut-back' required at the intersection of Sultana Avenue and Belmont Street.
  3. Property line corner 'cut-back' required at the intersection of Sultana Avenue and Phillips Street.
- 2.11 **Dedicate to the City of Ontario the following easement(s) only if necessary:**
1. Sidewalk easement behind the proposed drive approaches located along Sultana Avenue and Belmont Street.
- 2.12 **New Model Colony (NMC) Developments:**
- 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
  - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.
  - 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall).
- 2.13 **Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.**



- 2.14 The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.
- 2.15 Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately \$69,139, shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.
- 2.16 Other conditions:
  - 1. The applicant/developer shall process a Lot Line Adjustment and a Conforming Grant Deed in order to consolidate the site into one (1) parcel and render the subject property as a recognized parcel in the City of Ontario. The Lot Line Adjustment and Conforming Grant Deed shall be recorded prior to issuance of a building permit.



**B. PUBLIC IMPROVEMENTS**  
 (See attached Exhibit 'A' for plan check submittal requirements.)

2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Phillips Street	Sultana Avenue	Belmont Street	Monterey Avenue
<b>Curb and Gutter</b>	<input checked="" type="checkbox"/> New; 20 ft. from C/L north <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New; 18 ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New; 18 ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
<b>AC Pavement</b>	<input checked="" type="checkbox"/> Replacement <sup>(1)</sup> <input checked="" type="checkbox"/> Widen 18 feet from C/L north along frontage, including pavm't transitions	<input checked="" type="checkbox"/> Replacement <sup>(1)</sup> <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input checked="" type="checkbox"/> Replacement <sup>(1)</sup> <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
<b>PCC Pavement (Truck Route Only)</b>	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Drive Approach</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Sidewalk</b>	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>ADA Access Ramp</b>	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Parkway</b>	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
<b>Raised Landscaped Median</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Fire Hydrant</b>	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation



<b>Sewer</b> (see Sec. 2.C)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
<b>Water</b> (see Sec. 2.D)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Traffic Signing and Striping</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Street Light</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2.G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
<b>Fiber Optics</b> (see Sec. 2.K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
<b>Overhead Utilities</b>	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input checked="" type="checkbox"/> Relocate <sup>(2)</sup>	<input checked="" type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
<b>Removal of Improvements</b>	Remove existing metal poles			
<b>Other Improvements</b>				

**Specific notes for improvements listed in item no. 2.17, above:**

1. Refer to Condition of Approval No. 2.19.
2. Relocate existing power poles as needed to accommodate the construction of curb & gutter along the project frontage.

2.18 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s):





- 2.19 **Reconstruction of the full pavement structural section on Phillips Street, Sultana Avenue, and Belmont Street, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. A minimum 2" asphalt concrete grind and overly may be allowed in-lieu of the full pavement reconstruction if recommended by the soils engineer. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.**
- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide  water service   
 sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 **Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892).**
- 2.22 Other conditions: \_\_\_\_\_

**C. SEWER**

- 2.23 **An 8-inch sewer main is available for connection by this project in Belmont Street. (Ref: Sewer plan bar code: S12680)**
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 **Other conditions:** 
  1. **The Occupant of the building shall apply for a Wastewater Discharge Permit for their Establishment, and shall comply with all the requirements of their Wastewater Discharge Permit. Requirements of Wastewater Discharge Permit may include, but not limited to including: possibly installing a monitoring manhole, clarifier, or other sewer pretreatment equipment.**

**D. WATER**

- 2.27 A \_\_\_\_\_ inch water main is available for connection by this project in \_\_\_\_\_. (Ref: Water plan bar code: \_\_\_\_\_)
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.
- 2.29 **Other conditions:** 
  1. **Replace the existing 4" water line in Belmont, between Sultana and Monterey, with a new 12". All existing appurtenances shall be reconnected to the new 12" main via the installation of new service laterals (no coupling) from main to appurtenance. Upgrade any/all hydrants with breakaway check valve per City standards.**
  2. **Install one fire hydrant on the north side of Phillips Street (south side of property), connecting off the 1074 PZ water line, fronting the property.**

**E. RECYCLED WATER**

- 2.30 A \_\_\_\_\_ inch recycled water main is available for connection by this project in \_\_\_\_\_. (Ref: Recycled Water plan bar code: \_\_\_\_\_)
- 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.





2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.

2.33 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.

2.34 Other conditions: \_\_\_\_\_

#### F. TRAFFIC / TRANSPORTATION

2.35 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:

1. On-site and off-site circulation
2. Traffic level of service (LOS) at 'build-out' and future years
3. Impact at specific intersections as selected by the City Engineer

2.36 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.

2.37 Other conditions:

1. The applicant/developer shall be responsible to design and construct the following streets to their ultimate half-width along the project frontage:

- Sultana Avenue (66'R/W, 36' C/C)
- Belmont Street (66' R/W, 36' C/C)
- Phillips Street (66' R/W, 40' C/C)

Improvements shall include, but not be limited to, concrete curb and gutter, sidewalk, LED street lights, landscaped parkways, signing & striping, and necessary pavement transitions as deemed necessary by the City Engineer.

2. The Applicant/Developer shall be responsible to design and construct in-fill public street lights along the property frontages of Sultana Avenue, Belmont Street and Phillips Street, in accordance with the Traffic and Transportation Design Guidelines Section 1.4 Street Light Plans, City of Ontario Standard Drawing No. 5101, and to the satisfaction of the City Engineer.

3. Driveways shall be designed and constructed in accordance with City of Ontario Standard Drawing No. 1204 for Commercial Driveway Approach.

4. Property frontages along Sultana Avenue, Belmont Street, Phillips Street and Monterey Avenue shall be signed "No Parking Anytime."

5. Proposed gates on Belmont Street shall remain open at all times during business hours.

6. The applicant/developer's engineer-of-record shall meet with City Engineering staff prior to starting signing/striping and/or street lighting design.

7. The applicant/developer shall obtain all rights-of-way necessary to construct all required improvements.

8. All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City of Ontario Standard Drawing No. 1309.



### G. DRAINAGE / HYDROLOGY

- 2.38 A \_\_\_\_\_ inch storm drain main is available to accept flows from this project in \_\_\_\_\_.  
(Ref: Storm Drain plan bar code: \_\_\_\_\_)
- 2.39 **Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.**
- 2.40 **An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100-year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.**
- 2.41 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.
- 2.42 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.43 **Other conditions:**
  - 1. **The applicant/developer shall pay an in-lieu fee for future storm drain in Sultana Avenue along project frontage valued at \$75,454.**

### H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.44 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.  
If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.  
Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.45 **Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.**
- 2.46 Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.



- 2.47 Other conditions: 
  - 1. Activities resulting in land disturbance of one acre or more is required to obtain coverage under the Construction General Permit (CGP). The owner is the legally responsible person (LRP) of the site and shall have a Stormwater Pollution Prevention Plan (SWPPP) developed and submitted through the SMARTS website at <https://smarts.waterboards.ca.gov/smarts/faces/SwSmartsLogin.xhtml>

**J. SPECIAL DISTRICTS**

- 2.48 File an application, together with an initial payment deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community facilities District Act of 1982. The application and fee shall be submitted a minimum four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact the Financial Services Department at (909) 395-2353 to initiate the CFD application process.
- 2.49 Other conditions: \_\_\_\_\_

**K. FIBER OPTIC**

- 2.50 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole if within close proximity. If there is no OntarioNet handhole, conduit infrastructure shall terminate at either end of project frontage in a handhole.
- 2.51 Refer to the City's Fiber Optic Master Plan and other related documents on the City website for design and layout guidelines. Contact the Information Technology Department at (909) 395-2000, for more information regarding the requirement to install fiber optic improvements.

**L. Solid Waste**

- 2.52 Onsite solid waste shall be designed in accordance with the City's Solid Waste Manual location at:   
<http://www.ontarioca.gov/municipal-utilities-company/solid-waste>
- 2.53 Other conditions: 
  - 1. Prior to approval of the any building permits, the Conceptual Solid Waste Handling Plan needs to be updated and converted into a Final Solid Waste Handling Plan Sheet and shall be submitted accompanying the Precise Grading Plan Submittal to the City/OMUC for review and approval. See "Solid Waste Handling Plan (SWHP) Requirements" document for details.
  - 2. The applicant shall submit an Integrated Waste Management Report for review and approval with the Precise Grading Plan. This report shall address the management of all integrated waste (Refuse, Recycling, Organics, etc.). The IWMR shall demonstrate compliance with the "Integrated Waste Management Report Requirements" document.



**3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:**

- 3.01 **Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.**
- 3.02 Complete all requirements for recycled water usage. 
  - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
  - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
  - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 **The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.**
- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 **Confirm payment of all Development Impact Fees (DIF) to the Building Department.**
- 3.06 **Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studie and reports (i.e. hydrology, traffic, WQMP, etc.).**

**4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:**

- 4.01 **Complete all Conditions of Approval listed under Sections 1-3 above.**
- 4.02 **Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.**
- 4.03 **The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.**



**EXHIBIT 'A'**

**ENGINEERING DEPARTMENT  
First Plan Check Submittal Checklist**

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**Project Number: PDEV18-022**

**The following items are required to be included with the first plan check submittal:**

1.  **A copy of this check list**
2.  **Payment of fee for Plan Checking**
3.  **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4.  **One (1) copy of project Conditions of Approval**
5.  Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).
6.  **Three (3) sets of Public Street improvement plan with street cross-sections**
7.  Three (3) sets of Private Street improvement plan with street cross-sections
8.  **Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
9.  Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10.  Four (4) sets of Public Sewer improvement plan
11.  Five (5) sets of Public Storm Drain improvement plan
12.  **Three (3) sets of Public Street Light improvement plan**
13.  **Three (3) sets of Signing and Striping improvement plan**
14.  **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15.  **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
16.  Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
17.  **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
18.  **One (1) copy of Hydrology/Drainage study**
19.  **One (1) copy of Soils/Geology report**
20.  Payment for Final Map/Parcel Map processing fee
21.  Three (3) copies of Final Map/Parcel Map




- 22.  One (1) copy of approved Tentative Map
- 23.  **One (1) copy of Preliminary Title Report (current within 30 days)**
- 24.  One (1) copy of Traverse Closure Calculations
- 25.  One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.
- 26.  Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 27.  **Other: Update the sewer improvement plan for Belmont Street to show the proposed service connection to the project site.**

**CITY OF ONTARIO  
LANDSCAPE PLANNING  
DIVISION**

303 East "B" Street, Ontario, CA 91764

**CONDITIONS OF APPROVAL**

Sign Off

  
Jamie Richardson, Sr. Landscape Planner

2/13/20  
Date

Reviewer's Name:  
**Jamie Richardson, Sr. Landscape Planner**

Phone:  
**(909) 395-2615**

D.A.B. File No.:  
PDEV18-022 Rev 4

Case Planner:  
Denny Chen

Project Name and Location:

**Pepe's Towing**  
554 E Belmont St.

Applicant/Representative:  
Howard Parcell Company  
4854 Main St  
Yorba Linda, CA 92886

**A Preliminary Landscape Plan (dated 2/13/20) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.**

**A Preliminary Landscape Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.**

**A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE**

**PREVIOUS COMMENTS – 11/13/19**

Civil/ Site Plans

1. The tree inventory identifies 48" of trunk diameter to be mitigated; Schinus mole No. 14.
  - a. New 15 gallon trees min 1" diameter trunk, in addition to trees required; total of 48 trees.
  - b. New 24" box trees min 1.5" diameter trunk, in addition to trees required; total of 32 trees
  - c. Upsizing trees on the plan one size larger such as 15 gallon to 24" box, or 24" to 36" box size.
  - d. Monetary value of the trees removed as identified in the "Guide for Plant Appraisal", approved certified arborist plant appraiser, or may be equal to the value of the installation cost of planting, fertilizing, staking and irrigating 15 gallon trees, (100\$ each) to the City of Ontario Historic Preservation Fund for city tree planting or city approved combination of the above items; total of \$4,800.
2. Show transformers and backflow devices located in planter areas and set back 4' from paving; dimension on plan. Locate on level grade. Coordinate with landscape plans.
3. ~~2/13/20 Spoke with Mr. George Estrada over the counter; locate the wall at the Belmont entry of the tow yard so the landscape planter is in the public parking lot. Provide an employee break area on the inside of the tow yard adjacent to the 4 public parking spaces include a table and/or bench and 2 broad canopy shade trees such as Platanus racemosa 24" box. Provide irrigation to trees and mulch.~~ 3/12/20 updated plans received. Condition has been addressed.

Landscape Plans

4. Locate light standards, fire hydrants, water and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans



# CITY OF ONTARIO MEMORANDUM

## Development Plan Review

Project: PDEV18-022 (PCUP18-021)

Date: February 13<sup>th</sup>, 2020

Location: 554 East Belmont Street

By: Nathan Kuan

*The following shall become conditions of approval for the project.*

### Conditions:

1. The applicant/developer shall be responsible to design and construct the following streets to their ultimate half-width along the project frontage:
  - Sultana Avenue (66' R/W, 36' C/C)
  - Belmont Street (66' R/W, 36' C/C)
  - Phillips Street (66' R/W, 40' C/C)Improvements shall include, but not be limited to: concrete curb and gutter, sidewalk, LED street lights, landscaped parkways, signing & striping, and necessary pavement transitions as deemed necessary by the City Engineer.
2. The Applicant/Developer shall be responsible to design and construct in-fill public street lights and service pedestals along the property frontages of Sultana Avenue, Belmont Street and Phillips Street, in accordance with the Traffic and Transportation Design Guidelines Section 1.4 Street Light Plans, City of Ontario Standard Drawing No. 5101, and to the satisfaction of the City Engineer.
3. Driveways shall be designed and constructed in accordance with City of Ontario Standard Drawing No. 1204 for Commercial Driveway Approach.
4. Property frontages along Sultana Avenue, Belmont Street, Phillips Street and Monterey Avenue shall be signed "No Parking Anytime."
5. Proposed gates on Belmont Street shall remain open at all times during business hours.
6. The applicant/developer's engineer-of-record shall meet with City Engineering staff prior to starting signing/striping and/or street lighting design.
7. The applicant/developer shall obtain all rights-of-way necessary to construct all required improvements.
8. All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City of Ontario Standard Drawing No. 1309.

nk;



# CITY OF ONTARIO

## MEMORANDUM

**TO:** PLANNING DEPARTMENT, Denny Chen  
**FROM:** BUILDING DEPARTMENT, Kevin Shear  
**DATE:** June 11, 2019  
**SUBJECT:** PDEV18-022

- 
- The plan **does** adequately address the departmental concerns at this time.
- No comments
- Report below.

---

### Conditions of Approval

1. See previous report for Conditions.

KS:lr

# CITY OF ONTARIO

## MEMORANDUM

**TO:** PLANNING DEPARTMENT, Denny Chen  
**FROM:** BUILDING DEPARTMENT, Kevin Shear  
**DATE:** June 27, 2018  
**SUBJECT:** PDEV18-022

- 
- The plan **does** adequately address the departmental concerns at this time.
- No comments
- Report below.

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### Conditions of Approval

1. The site address for this project will be 580 E Belmont St.
2. The lot lines are to be removed.

KS:lm



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Denny Chen, Associate Planner  
Planning Department

**FROM:** Paul Ehrman, Deputy Fire Chief/Fire Marshal  
Fire Department

**DATE:** July 6, 2018

**SUBJECT:** PDEV18-022 - A Development Plan to construct a 6,660-square foot industrial building on 3.01 acres of land located at the southwest corner of Belmont Street and Monterey Avenue, at 554 East Belmont Street, within the IL (Light Industrial) zoning district (APNs: 1049-491-01, 1049-491-02 and 1049-491-03). Related File: PCUP18-021

- 
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
- 

### **SITE AND BUILDING FEATURES:**

- A. 2016 CBC Type of Construction: V
- B. Type of Roof Materials: Ordinary
- C. Ground Floor Area(s): 5,460 Sq. Ft.
- D. Number of Stories: 2
- E. Total Square Footage: 6,660 Sq. Ft.
- F. 2016 CBC Occupancy Classification(s): B, S

## CONDITIONS OF APPROVAL:

### 1.0 GENERAL

- 1.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at [www.ontarioca.gov](http://www.ontarioca.gov), click on "Fire Department" and then on "Standards and Forms."
- 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

### 2.0 FIRE DEPARTMENT ACCESS

- 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25') inside and forty-five feet (45') outside turning radius per Standard #B-005.
- 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150') in length shall have an approved turn-around per Standard #B-002.
- 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-six (26) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services..

### 3.0 WATER SUPPLY

- 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 2000 gallons per minute (g.p.m.) for 2 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

### 4.0 FIRE PROTECTION SYSTEMS

- 4.1 On-site private fire hydrants are required per Standard #D-005, and identified in accordance with Standard #D-002. Installation and locations(s) are subject to the approval of the Fire Department. An application with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- 4.4 Wood frame buildings that are to be sprinkled shall have these systems in service (but not necessarily finalized) before the building is enclosed.

### 5.0 BUILDING CONSTRUCTION FEATURES

- 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.

- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.
- ☒ 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.

## 6.0 OTHER SPECIAL USES

- ☒ 6.1 The storage, use, dispensing, or handling of any hazardous materials shall be approved by the Fire Department, and adequate fire protection features shall be required. If hazardous materials are proposed, a Fire Department Hazardous Materials Information Packet, including Disclosure Form and Information Worksheet, shall be completed and submitted with Material Safety Data Sheets to the Fire Department along with building construction plans.
- ☒ 6.3 Underground fuel tanks, their associated piping and dispensers shall be reviewed, approved, and permitted by Ontario Building Department, Ontario Fire Department, and San Bernardino County Fire Department Hazardous Materials Division. In fueling facilities, an exterior emergency pump shut-off switch shall be provided.



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Denny Chen, Planning Department

**FROM:** Douglas Sorel, Police Department

**DATE:** July 26, 2018

**SUBJECT:** PDEV18-022 & PCUP18-021: A DEVELOPMENT PLAN AND  
CONDITIONAL USE PERMIT TO ESTABLISH A TOW YARD AT 554  
EAST BELMONT STREET

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The "Standard Conditions of Approval" contained in Resolution No. 2017-027 and Ontario Development Code Section 5.03.310 apply. The applicant shall read and be thoroughly familiar with these conditions, including but not limited to, the requirements listed below.

- Required lighting for all walkways, driveways, doorways, parking areas, and other areas used by the public shall be provided and operate on photosensor. Photometrics shall be provided to the Police Department. Photometrics shall include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the building as stated in the Standard Conditions. The numbers shall be at a minimum 3 feet tall and 1 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.
- The indoor storage of motor vehicles shall comply with all requirements of the fire and building codes.
- All vehicles stored outdoors shall be screened from public view by a minimum 8-foot high decorative masonry block wall.
- All vehicles stored shall comply with the requirements for the base zoning district.

In addition to the Standard Conditions, the Ontario Police Department places the following conditions on the project:

- While the Applicant serves as a contracted Rotational Towing Service with the City of Ontario they shall comply with all terms of Title 4-19 of the Ontario Municipal Code and their current Professional Services Agreement with the City.

- The applicant will be responsible for keeping the grounds of the business clean from debris and litter.
- Graffiti abatement by the business owner/licensee, or management shall be immediate and on-going on the premises, but in no event shall graffiti be allowed unabated on the premises for more than 72 hours. Abatement shall take the form of removal, or shall be covered/painted over with a color reasonably matching the color of the existing building, structure, or other surface being abated. Additionally, the business owner/licensee, or management shall notify the City within 24 hours at (909) 395-2626 (graffiti hotline) of any graffiti elsewhere on the property not under the business owner/licensee's or management control so that it may be abated by the property owner and/or the City's graffiti team.

The Applicant is invited to contact Douglas Sorel at (909) 408-1873 with any question or concerns regarding these conditions.



# AIRPORT LAND USE COMPATIBILITY PLANNING CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV18-022 & PCUP18-021

Address: 554 East Belmont Street

APN: 1049-491-01, 02 & 03

Existing Land Use: Vacant

Proposed Land Use: Development Plan and CUP to establish a tow yard and 6,600 SF 2-story industrial/warehouse building

Site Acreage: 3.01 ac Proposed Structure Height: 33 FT

ONT-IAC Project Review: N/A

Airport Influence Area: ONT

Reviewed By:

Lorena Mejia

Contact Info:

909-395-2276

Project Planner:

Rudy Zeledon

Date: 7/30/18

CD No.: 2018-036

PALU No.: n/a

## The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="radio"/> High Terrain Zone	<input type="radio"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input checked="" type="checkbox"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input checked="" type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input type="radio"/> Real Estate Transaction Disclosure
<input type="radio"/> Zone 3	<input type="radio"/> 60 - 65 dB CNEL	<input checked="" type="checkbox"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: 110 FT	
<input type="radio"/> Zone 5			

## The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1   
  Zone 2   
  Zone 3   
  Zone 4   
  Zone 5   
  Zone 6

Allowable Height: \_\_\_\_\_

## CONSISTENCY DETERMINATION

This proposed Project is:  Exempt from the ALUCP   
 Consistent   
 Consistent with Conditions   
 Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

See Attached Conditions

Airport Planner Signature: \_\_\_\_\_

# AIRPORT LAND USE COMPATIBILITY PLANNING

## CONSISTENCY DETERMINATION REPORT

CD No.: 2018-036

PALU No.: \_\_\_\_\_

### PROJECT CONDITIONS

1. This project is located within Airspace Avigation Easement Area and is required to file and record an Avigation Easement with the Ontario International Airport Authority prior to obtaining a Certificate of Occupancy.



# PLANNING COMMISSION STAFF REPORT

May 26, 2020

**FILE NOS.:** PMTT19-011 / TT20291 and PDEV19-043

**SUBJECT:** A Tentative Tract Map (File No. PMTT19-011 / TT 20291) to subdivide 0.73-acre of land into a single lot for condominium purposes in conjunction with a Development Plan (File No. PDEV19-043) to construct a multi-family project with 18 dwelling units located at 1005 and 1023 East Sixth Street, within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district. (APNs: 1047-172-17 and 1047-172-22); **submitted by TAVK Holding LLC / T&S Allied Investment LLC.**

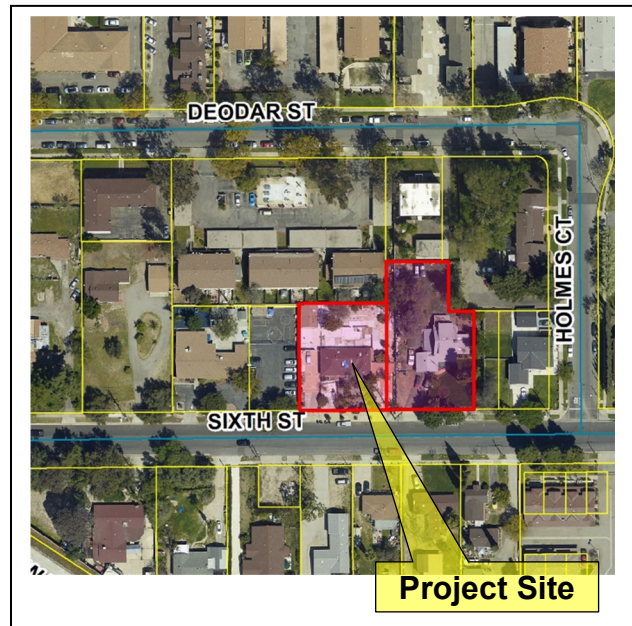
**PROPERTY OWNER:** TAVK Holding LLC / T&S Allied Investment LLC

**RECOMMENDED ACTION:** That the Planning Commission consider and approve File Nos. PMTT19-011 and PDEV19-043, pursuant to the facts and reasons contained in the staff report and attached resolutions, and subject to the conditions of approval contained in the attached departmental reports.

**PROJECT SETTING:** The Project site is comprised of 0.73-acre of land located at 1005 and 1023 East Sixth Street, within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district, and is depicted in Figure 1: Project Location, below. The property surrounding the Project site is characterized primarily by a combination of multi-family and single-family residential land uses to the north, south and east, and Bright Star Montessori Preschool to the west. The existing surrounding land uses, zoning and general plan land use designations are listed in the “Surrounding Zoning & Land Uses” table located in the Technical Appendix of this report.

**PROJECT ANALYSIS:**

[1] Background — On July 19, 2019, TAVK Holding LLC / T&S Allied Investment LLC (“Applicant”), submitted a Tentative Tract Map (File No. PMTT19-011/TT 20291) to subdivide 0.73 acres of land into a single lot for condominium purposes, in



**Figure 1: Project Location**

Case Planner:	Jeanie Irene Aguilo
Planning Director Approval:	
Submittal Date:	07/19/2019

Hearing Body	Date	Decision	Action
DAB	05/18/2020	Approval	Recommend
PC	05/26/2020		Final
CC			

conjunction with a Development Plan (File No. PDEV19-043) to construct a multi-family project with 18 dwelling units, as depicted in Exhibit A—Aerial Map, attached.

On May 18, 2020, the Development Advisory Board reviewed the subject applications and recommended that the Planning Commission approve the proposed Project subject to the departmental conditions of approval included with this report.

[2] Site Design/Building Layout — The Project site consists of two multiple-family apartment buildings, each designed with a first-floor concrete podium slab and bearing walls that house a parking garage, refuse collection facilities, and tenant storage facilities (see Exhibit B – Site Plan, attached). Each building has a two-story wood-framed structure designed on top of each podium, for an overall height of 3 stories (33 to 36 feet, on average). Building 1, located on the western half of the site, consists of 8 units and Building 2, located on the eastern half of the site, consists of 10 units. The two buildings are separated by a common open space area that is approximately 18 feet wide, with pedestrian pathways that connect the two buildings. A 10-foot landscape setback has been provided along the perimeter of the Project boundary, which incorporates pedestrian pathways and connects to common open space areas throughout the Project site.

Five different floor plans are proposed, which range from 1334 to 1558 square feet in size. The Project provides 12 units with two-bedroom floor plans and 6 units with three-bedroom floor plans. The dwelling unit breakdown is as follows:

<b><i>Dwelling Unit Summary – Building 1 (1005 East Sixth Street)</i></b>				
<b><i>Plan Type No.</i></b>	<b><i>No. of Bedrooms/ Bathrooms</i></b>	<b><i>Total SF</i></b>	<b><i>No. of Units</i></b>	<b><i>Percent of Units</i></b>
A-1	3 Bedroom/2.5 Bathroom	1,546 SF	5	62.5%
A-2	3 Bedroom/2.5 Bathroom	1,558 SF	1	12.5%
B-2	2 Bedroom/2.5 Bathroom	1,493 SF	2	25%
<b>TOTAL</b>		12,274 SF	8	100%

<b><i>Dwelling Unit Summary – Building 2 (1023 East Sixth Street)</i></b>				
<b><i>Plan Type No.</i></b>	<b><i>No. of Bedrooms/ Bathrooms</i></b>	<b><i>Total SF</i></b>	<b><i>No. of Units</i></b>	<b><i>Percent of Units</i></b>
A-1	3 Bedroom/2.5 Bathroom	1,546 SF	4	40%
A-2	3 Bedroom/2.5 Bathroom	1,558 SF	2	20%
B-1	2 Bedroom/2.5 Bathroom	1,481 SF	2	20%
C	2 Bedroom/2.5 Bathroom	1,334 SF	2	20%
<b>TOTAL</b>		14,930 SF	10	100%

The open space requirements of the HDR-45 zoning district contain a recommendation that a project provide a minimum of 60 square feet (20-percent) of private open space and 250 square feet (80-percent) of common open space, per dwelling unit. The Development Codes allows for deviations in private and common open space so long as the total amount of open space provided equals 310 square feet per unit.

The Project includes common open space and recreation amenities in support of the 18 dwelling units, such as a fitness trail, tot lot with play structure, picnic and BBQ area, and sitting areas. The large common open areas with recreational amenities have been strategically located for the convenience of the residents, and to enhance the Project. In addition, large courtyards with meandering walkways between the units have been provided throughout the Project.

<i>Open Space Summary</i>		
<i>Open Space</i>	<i>Total Area Required</i>	<i>Total Area Provided</i>
Common Open Space – Active	5,580 SF	4,388 SF
Common Open Space – Passive		2,674 SF
Common Open Space – Passive		
Private Open Space	1,080 SF	2,421 SF
<b>TOTAL</b>	<b>6,660 SF</b>	<b>9,483 SF</b>

[3] Tentative Tract Map — The Tentative Tract Map will facilitate the construction of the above-mentioned Development Plan and the future sale of individual units within the Project. The single-lot condominium plan will be recorded with the Department of Real Estate and will delineate the airspace for each unit, parking, common areas, and access (see Exhibit F—Tentative Tract Map No. 20291, attached).

The Tentative Tract Map is 0.73-acre in size. The Development Code requires development projects within the HDR-45 zoning district to have a minimum lot size of 2.5-acres. The Development Code also states that if a parcel is substandard and is a legally recognized parcel, the parcel is permitted to be developed, provided that the minimum density requirements of the zoning district are met. The Project is providing 18 units and meets the minimum density requirement of the HDR-45 zoning district (25.1 dwelling units per acre). The proposed map will include Covenants, Conditions and Restrictions (CC&Rs), which will establish rules and regulations for the property owner’s association. In addition, the CC&Rs will be recorded with the final map to ensure access and common maintenance of landscaped areas, common open space areas, parking facilities, and utility and drainage easements.

[4] Site Access/Circulation — Project access is provided by a driveway approach for each building, with access taken from Sixth Street. Vehicular access to gated garages is provided by 25-foot wide auto court lanes, which incorporate decorative paving. The

Project will also provide adequate turn around areas to facilitate fire access and trash service. Sixth Street is fully improved with a curb, gutter, parkway, and a sidewalk.

[5] Parking — The Project has provided off-street parking pursuant to the “Multiple-Family Residential” parking standards specified in the Development Code. A total of 45 parking spaces have been provided on-site, while 2 guest spaces are provided on-street, along the Project’s street frontage. The off-street parking calculations for each building are as follows:

<i>Type of Use</i>	<i>No. of Units</i>	<i>Parking Ratio</i>	<i>Spaces Required</i>	<i>Spaces Provided</i>
2-bedroom units	6	2.0 spaces per dwelling, including one space in a garage or carport	12	
3-bedroom units	12	2.5 spaces per dwelling, including one space in a garage or carport	30	
Visitor	18	1 Space Per 4 (< 50 Units)	5	
<b>TOTAL</b>			<b>47</b>	<b>47</b>

[6] Architecture — The architectural style proposed consists of a modern interpretation of a Mid-Century Modern design, with a simple box form and accent elements at entries, flat roofs with parapet detailing, and cantilever awnings. The mixture of building materials proposed, includes medium and course textured stucco finishes with a neutral beige color palette and metal reveals, aluminum cantilevered awnings, and metal guardrails. Additionally, mechanical equipment will be roof-mounted and obscured from public view by parapet walls (see Exhibits C—Elevations and Exhibits E—Model Photos, attached).

Staff believes that the proposed Project illustrates the type of high-quality residential architecture promoted by the City’s Development Code. This is exemplified through the use of:

- Articulation in building footprints, incorporating horizontal changes in the in the exterior building walls (combinations of recessed and popped-out wall areas).
- Articulation in the building parapet and roof lines, which serves to accentuate the building’s entries and openings, and breaks up large expanses of building wall.
- Variations in building massing.
- A mix of exterior materials, finishes and fixtures.

- Incorporation of horizontal changes in the exterior wall plane, and changes in exterior color (use of color blocking) and materials.

[7] Landscaping — The Project provides substantial landscaping along the Sixth Street frontage and throughout the Project site for an overall landscape coverage of 20.7 percent. The proposed on-site and off-site landscape improvements will assist towards creating a walkable, safe area for pedestrians to access the Project site. The landscape plan incorporates a combination of 48-inch, 36-inch, and 24-inch box trees along Sixth Street and throughout the Project site, which includes a mix of Coast Live Oak, Crape Myrtle and Eastern Redbud trees. In addition, a mix of 15-gallon and 24-inch accent and shade trees will be provided throughout the Project site that includes Brisbane Box and Long Leafed Podocarpus trees. A variety of shrubs and groundcovers are also being provided, which are low water usage or drought tolerant (see Exhibit D —Landscape Plan, attached).

[8] Utilities (drainage, sewer) — All necessary public utilities (water and sewer) were previously installed in Sixth Street. Furthermore, the Applicant has submitted a Preliminary Water Quality Management Plan (PWQMP), which establishes the Project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development (LID) best management practices (BMPs), such as retention and infiltration, biotreatment, and evapotranspiration. The PWQMP proposes the use of vegetated swales, which lead to underground stormwater infiltration systems installed for the Project. Any overflow drainage will be conveyed to the public street by way of parkway culverts.

**COMPLIANCE WITH THE ONTARIO PLAN:** The proposed Project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed Project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)
- Encourage, Provide or Support Enhanced Recreational, Educational, Cultural and Healthy City Programs, Policies and Activities

[2] Vision.

**Distinctive Development:**

- Commercial and Residential Development
  - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

[3] Governance.

**Decision Making:**

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
  - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

[4] Policy Plan (General Plan)

**Land Use Element:**

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
  - LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.
  - LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).
- Goal LU2: Compatibility between a wide range of uses.
  - LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.



### **Housing Element:**

- Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

- H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.

Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.

- H5-2 Family Housing. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation and other amenities.

### **Community Economics Element:**

- Goal CE1: A complete community that provides for all incomes and stages of life.

- CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

- CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

- CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

- CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

➤ CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

**Safety Element:**

▪ Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.

➤ S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

**Community Design Element:**

▪ Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➤ CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

➤ CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

➤ CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

▪ Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

➤ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➤ CD2-2 Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb.

➤ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➤ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

➤ CD2-12 Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

▪ Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

➤ CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

➤ CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.

➤ CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.

➤ CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.

➤ CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

▪ Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

➤ CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.

➤ CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

**HOUSING ELEMENT COMPLIANCE:** The Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

**AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE:** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and

requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed Project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

**ENVIRONMENTAL REVIEW:** The Project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines and meets each of the following conditions: [1] the Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations; [2] the proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses; [3] the Project site has no value as habitat for endangered, rare, or threatened species; [4] approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and [5] the Project site can be adequately served by all required utilities and public services.

**CONDITIONS OF APPROVAL:** See attached department reports.

**TECHNICAL APPENDIX:**

**Surrounding Zoning and Land Use:**

	<i>Existing Land Use</i>	<i>General Plan Designation</i>	<i>Zoning Designation</i>	<i>Specific Plan Land Use</i>
<i>Site</i>	Single Family Residential	HDR (High Density Residential)	HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre)	N/A
<i>North</i>	Multi-Family Residential	HDR (High Density Residential)	HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre)	N/A
<i>South</i>	Single Family Residential	HDR (High Density Residential)	HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre)	N/A
<i>East</i>	Single Family Residential	HDR (High Density Residential)	HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre)	N/A
<i>West</i>	Bright Star Montessori Preschool	HDR (High Density Residential)	HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre)	N/A

**General Site & Building Statistics**

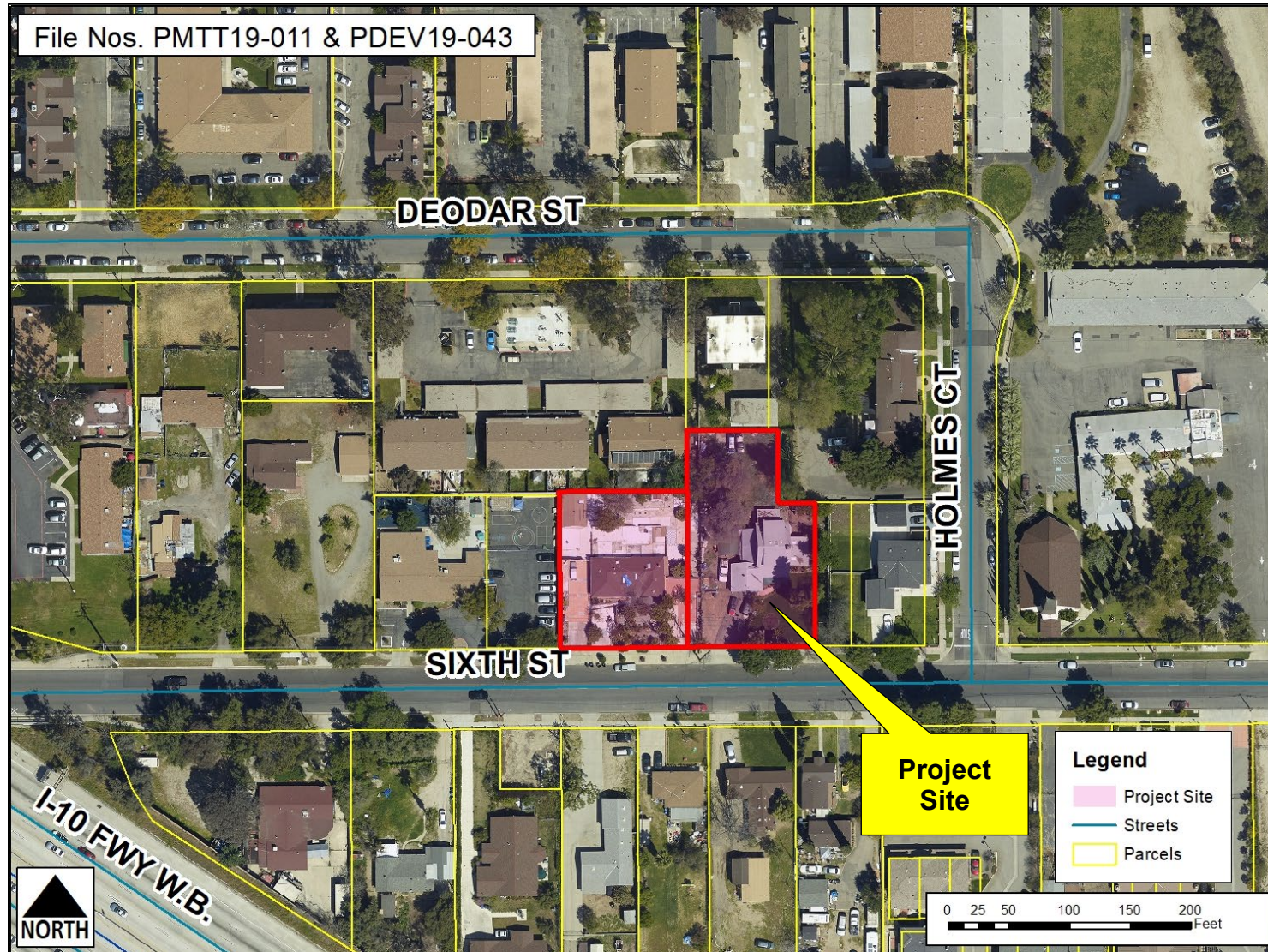
<i>Item</i>	<i>Required Min./Max.</i>	<i>Provided (Ranges)</i>	<i>Meets Y/N</i>
<i>Project area (in acres):</i>	2.5 AC	0.73 AC	Y*
<i>Maximum project density (dwelling units/ac):</i>	25.1 to 45.0 DU/Acre	25.1 DU/Acre	Y
<i>Maximum coverage (in %):</i>	100%	100%	Y
<i>Minimum lot size (in SF):</i>	2.5 AC	0.73 AC	Y*
<i>Minimum lot depth (in FT):</i>	330 FT	161 FT	Y*
<i>Minimum lot width (in FT):</i>	330 FT	221 FT	Y*
<i>Front yard setback (in FT):</i>	10 FT	10 FT	Y
<i>Side yard setback (in FT):</i>	10 FT	10 FT	Y
<i>Rear yard setback (in FT):</i>	10 FT	10FT	Y
<i>Drive aisle setback (in FT):</i>	5 FT	10 FT	Y
<i>Parking setback (in FT):</i>	5 FT	10 FT	Y
<i>Maximum height (in FT):</i>	75 FT	33 to 36 FT, on average	Y
<i>Parking – resident:</i>	42	42	Y
<i>Parking – guest:</i>	5	5	Y
<i>Open space – private:</i>	1,080 SF	2,421 SF	Y
<i>Open space – common:</i>	5,580 SF	7,062 SF	Y

*\*An existing lot of record that is substandard as to minimum "project" area and/or dimension(s), shall be permitted all of the development rights of the zone in which it is located, except that the maximum density shall be limited to the minimum number of dwelling units allowed within the density range.*

<b>Dwelling Unit Summary – Building 1 (1005 East Sixth Street)</b>				
<b>Plan Type No.</b>	<b>No. of Bedrooms/ Bathrooms</b>	<b>Total SF</b>	<b>No. of Units</b>	<b>Percent of Units</b>
A-1	3 Bedroom/2.5 Bathroom	1,546 SF	5	62.5%
A-2	3 Bedroom/2.5 Bathroom	1,558 SF	1	12.5%
B-2	2 Bedroom/2.5 Bathroom	1,493 SF	2	25%
<b>TOTAL</b>		12,274 SF	8	100%

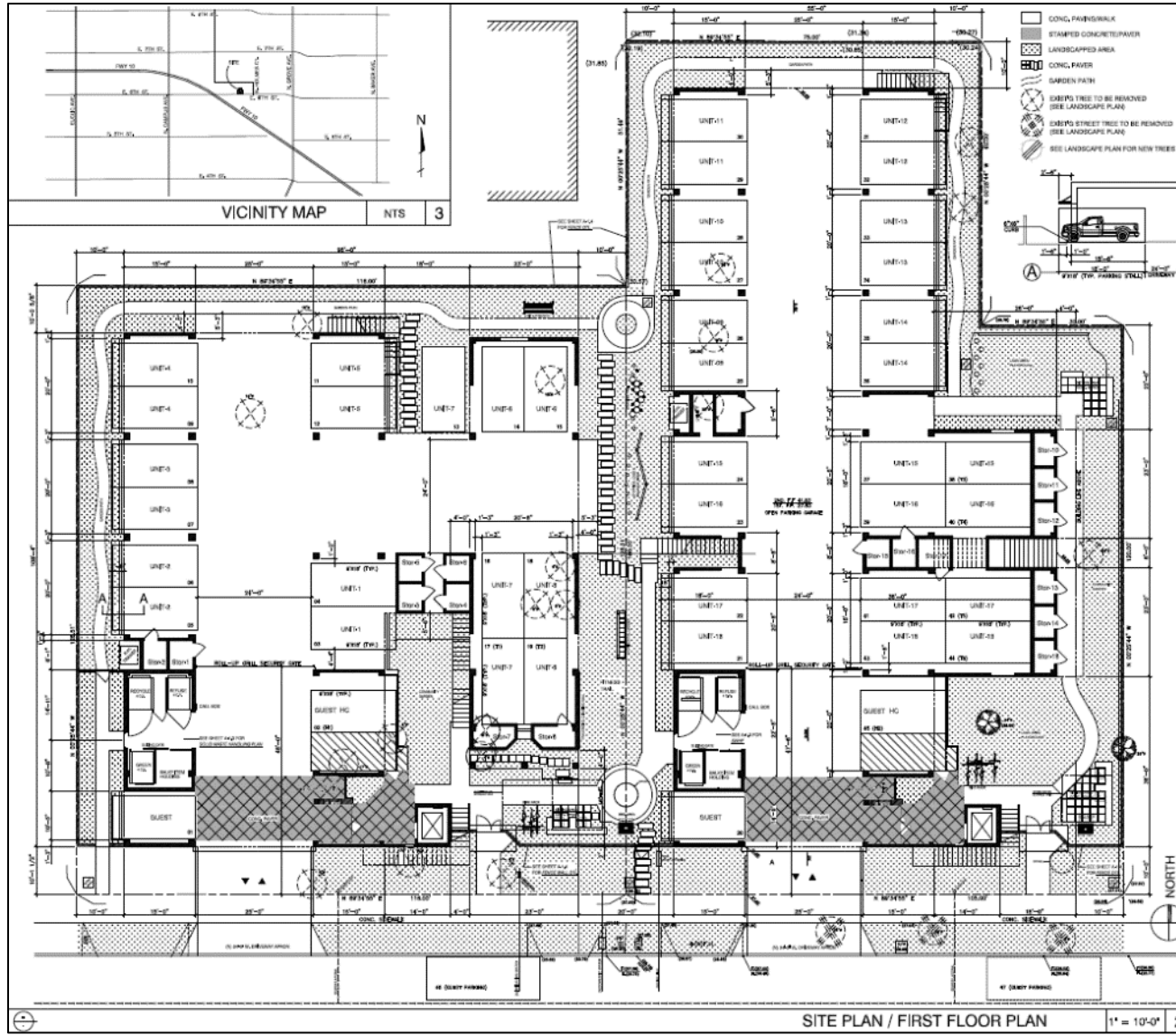
<b>Dwelling Unit Summary – Building 2 (1023 East Sixth Street)</b>				
<b>Plan Type No.</b>	<b>No. of Bedrooms/ Bathrooms</b>	<b>Total SF</b>	<b>No. of Units</b>	<b>Percent of Units</b>
A-1	3 Bedroom/2.5 Bathroom	1,546 SF	4	40%
A-2	3 Bedroom/2.5 Bathroom	1,558 SF	2	20%
B-1	2 Bedroom/2.5 Bathroom	1,481 SF	2	20%
C	2 Bedroom/2.5 Bathroom	1,334 SF	2	20%
<b>TOTAL</b>		14,930 SF	10	100%

**Exhibit A—PROJECT LOCATION MAP**

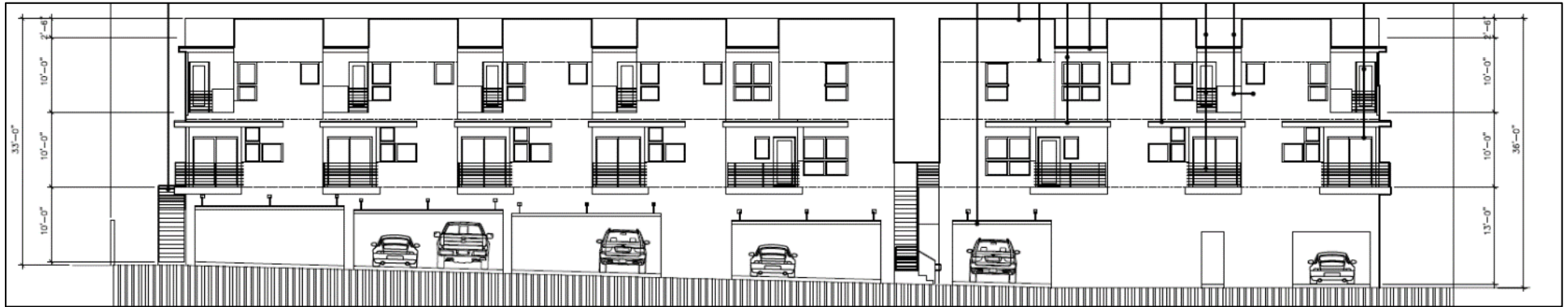




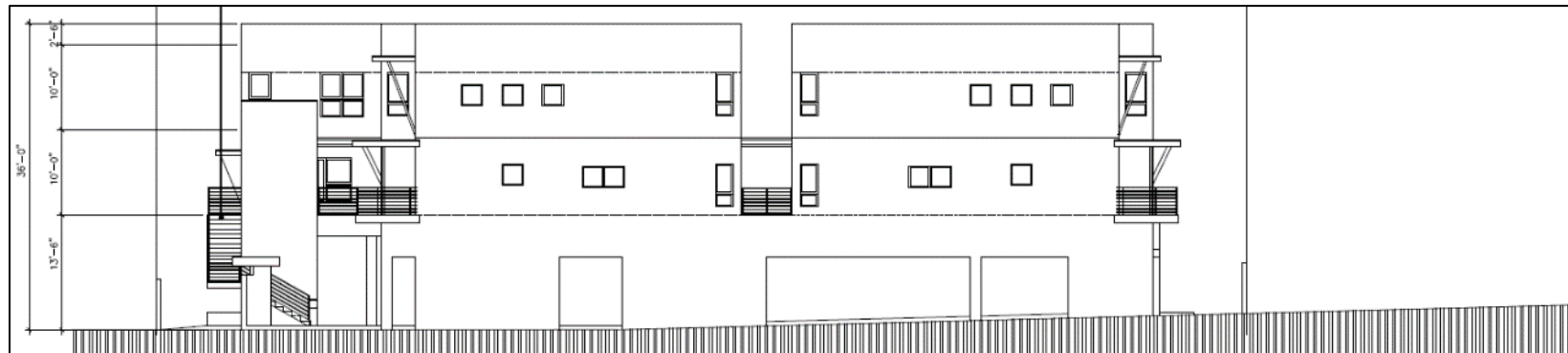
**Exhibit B—SITE PLAN**



**Exhibit C—ELEVATIONS – Building 1 (1005 East Sixth Street)**

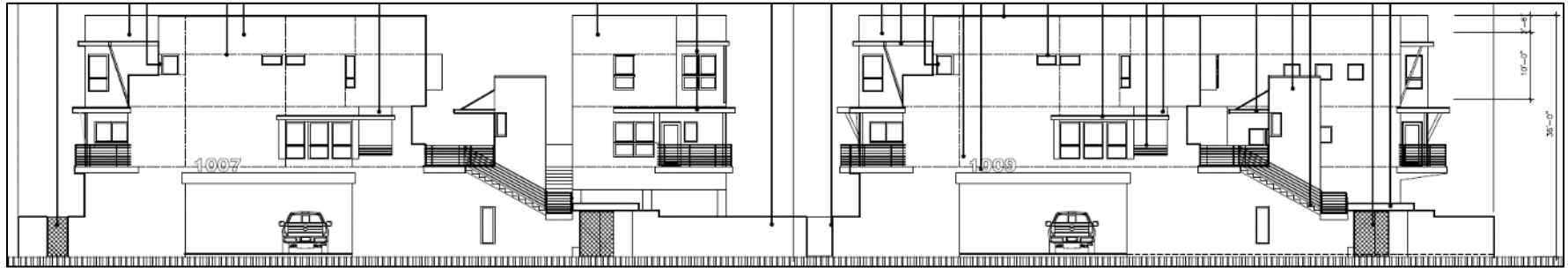


**West Elevation**

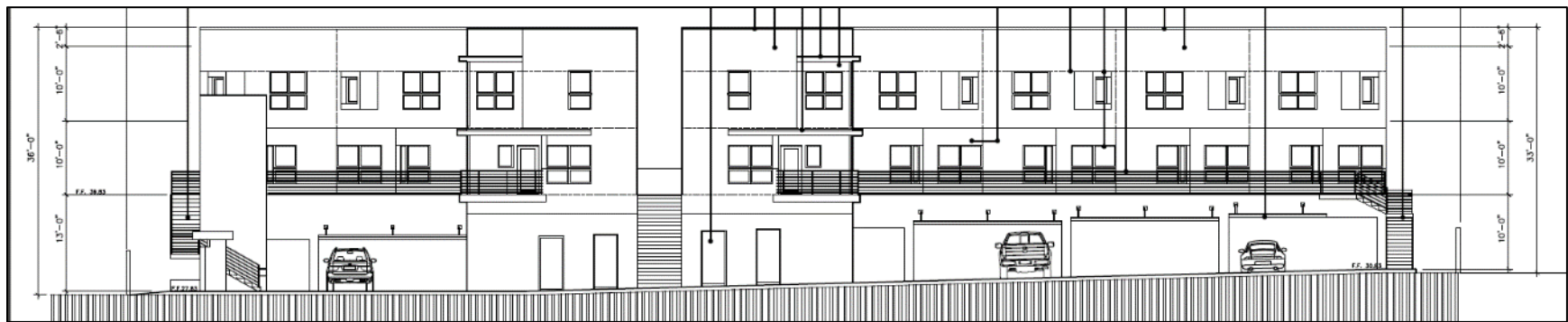


**East Elevation**

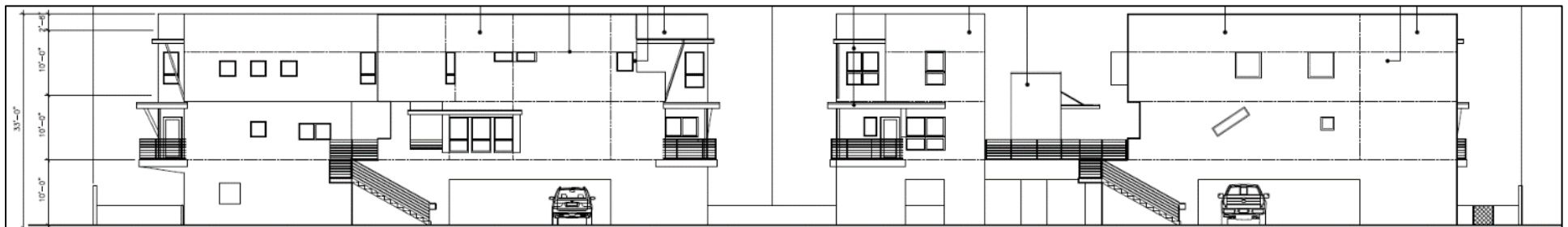
**Exhibit C—ELEVATIONS – Building 2 (1023 East Sixth Street)**



**South Elevation (Facing Sixth Street)**



**East Elevation**

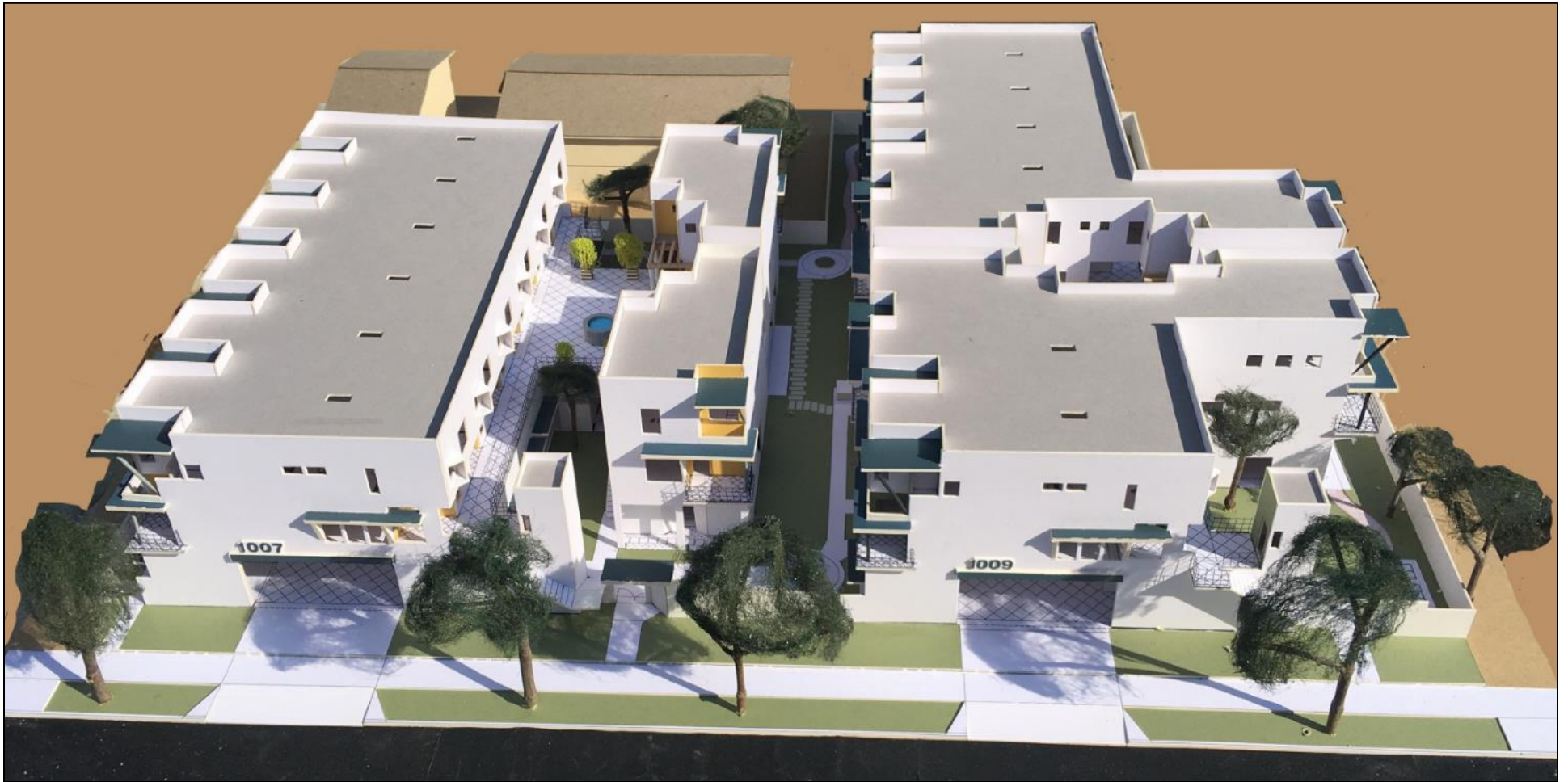


**North Elevation**

**Exhibit D—LANDSCAPE PLAN**



***Exhibit E—MODEL PHOTOS***



***Exhibit E—MODEL PHOTOS***



Birdseye View Northern Elevation facing South

***Exhibit E—MODEL PHOTOS***



South Elevation (Facing Sixth Street – From Southwest Corner)



South Elevation (Facing Sixth Street – From Southeast Corner)

***Exhibit E—MODEL PHOTOS***



East Elevation



North Elevation

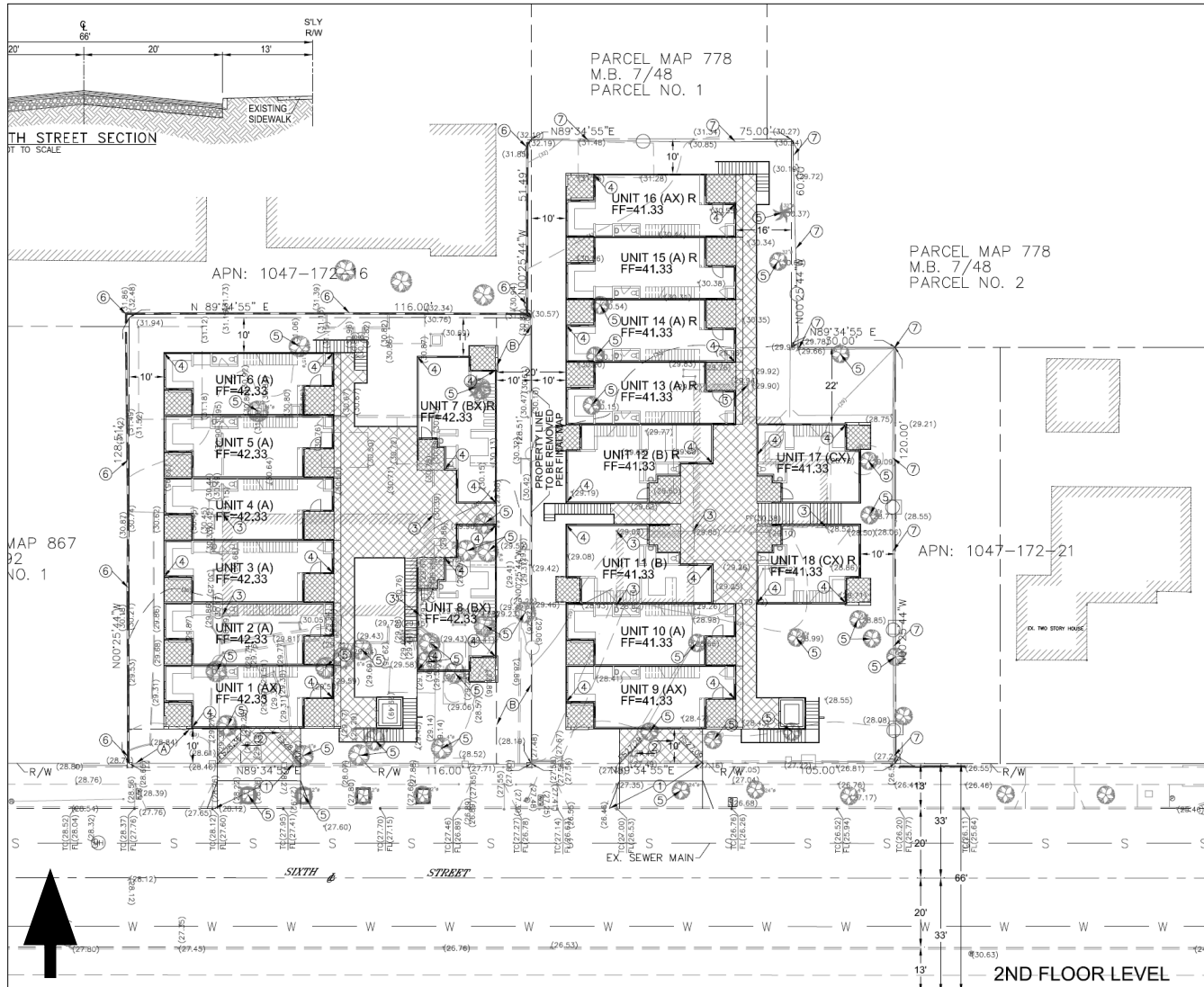


***Exhibit E—MODEL PHOTOS***



West Elevation

**Exhibit F—Tentative Tract Map No. 20291**



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PMTT19-011 / TT 20291, A TENTATIVE TRACT MAP TO SUBDIVIDE 0.73-ACRE OF LAND INTO A SINGLE LOT FOR CONDOMINIUM PURPOSES, LOCATED AT 1005 AND 1023 EAST SIXTH STREET, WITHIN THE HDR-45 (HIGH DENSITY RESIDENTIAL - 25.1 TO 45.0 DU/ACRE) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 1047-172-17 AND 1047-172-22.

WHEREAS, TAVK HOLDING LLC / T&S ALLIED INVESTMENT LLC, ("Applicant") has filed an Application for the approval of a Development Plan, File No. PMTT19-011 / TT 20291, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 0.73-acre of land, located at 1005 and 1023 East Sixth Street, within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district, and is presently improved with a single-family residence; and

WHEREAS, the property to the north of the Project site is within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district and is developed with multiple-family dwellings. The property to the east is within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district and is developed with single-family dwellings. The property to the south is within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district and is developed with single-family dwellings. The property to the west is within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district and is developed with Bright Star Montessori Preschool; and

WHEREAS, on July 19, 2019, the Applicant submitted a Tentative Tract Map application requesting to subdivide the Project site into a single lot for condominium purposes; and

WHEREAS, a Development Plan application (File No. PDEV19-043) was submitted in conjunction with the Tentative Tract Map, requesting approval to construct 18 multiple-family dwellings on the Project site; and

WHEREAS, on May 18, 2020, the Development Advisory Board reviewed the subject application and recommended that the Planning Commission approve the proposed Project subject to the departmental conditions of approval; and

WHEREAS, the Tentative Tract Map will facilitate the construction of the above-mentioned Development Plan and the future sale of individual units within the Project; and

WHEREAS, the single-lot condominium plan will be recorded with the Department of Real Estate and will delineate the airspace for each unit, parking, common areas, and access. Furthermore, the proposed map will include Covenants, Conditions and Restrictions (CC&Rs) which will establish rules and regulations for the property owners association. The CC&Rs will be recorded with the final map to ensure access and common maintenance of landscaped areas, common open space areas, parking facilities, and utility and drainage easements; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act, commencing with Public Resources Code Section 21000 (hereinafter referred to as "CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 18, 2020, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that

date, voting to issue Decision No. DAB20-026, recommending that the Planning Commission approve the Application; and

WHEREAS, on May 26, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

**SECTION 1: *Environmental Determination and Findings.*** As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines and meets each of the following conditions: [1] the Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations; [2] the proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses; [3] the Project site has no value as habitat for endangered, rare, or threatened species; [4] approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and [5] the Project site can be adequately served by all required utilities and public services; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

**SECTION 2: *Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based

on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

**SECTION 3: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

**SECTION 4: *Concluding Facts and Reasons.*** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed Tentative Tract Map is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable area and specific plans, and planned unit developments.*** The proposed Tentative Tract Map is located within the High Density Residential land use district of the Policy Plan Land Use Map, and the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district. The proposed subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the Project will contribute to providing “a spectrum of housing types and price ranges that match the jobs in the City, and that make it possible for people to live and work in Ontario and maintain a quality of life” (Goal LU1). Furthermore, the Project will

promote the City's policy to "incorporate a variety of land uses and building types that contribute to a complete community where residents at all stages of life, employers, workers, and visitors, have a wide spectrum of choices of where they can live, work, shop, and recreate within Ontario" (Policy LU1-6 *Complete Community*).

(2) ***The design or improvement of the proposed Tentative Tract Map is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable specific plans and planned unit developments.*** The proposed Tentative Tract Map is located within the High-Density Residential land use district of the Policy Plan Land Use Map, and the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district. The proposed design or improvement of the subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the Project will contribute to providing "[a] high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct" (Goal CD2). Furthermore, the Project will promote the City's policy to "create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
  - Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the "outdoor living room"), as appropriate; and
  - Landscaped parkways, with sidewalks separated from the curb." (Policy CD2-2 *Neighborhood Design*).

(3) ***The site is physically suitable for the type of development proposed.*** The Project site is located within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district, and is physically suitable for the type of residential development proposed in terms of zoning, land use and development activity proposed, and existing and proposed site conditions.

(4) ***The site is physically suitable for the density/intensity of development proposed.*** The Project site is proposed for residential development at a density of 25.1 DUs/acre. The Project site is located within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district and is physically suitable for this proposed density/intensity of development.

(5) ***The design of the subdivision or the proposed improvements thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.*** The Project site is not located in an area that has been identified as containing species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations or by the California Department of Fish and Wildlife or the U.S. Fish and Wildlife Service, nor does the site contain any riparian habitat or other sensitive natural community, and no wetland habitat is present on site; therefore, the design of the subdivision, or improvements proposed thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.

(6) ***The design of the subdivision, or the type of improvements thereon, are not likely to cause serious public health problems.*** The design of the proposed subdivision, and the improvements existing or proposed on the Project site, are not likely to cause serious public health problems, as the Project is not anticipated to involve the transport, use, or disposal of hazardous materials during either construction or Project implementation, include the use of hazardous materials or volatile fuels, nor are there any known stationary commercial or industrial land uses within close proximity to the subject site that use/store hazardous materials to the extent that they would pose a significant hazard to visitors or occupants to the Project site.

(7) ***The design of the subdivision, or the type of improvements thereon, will not conflict with easements acquired by the public at large for access through, or use of property within, the proposed subdivision.*** The proposed subdivision has provided for all necessary public easements and dedications for access through, or use of property within, the proposed subdivision. Furthermore, all such public easements and dedications have been designed pursuant to: (a) the requirements of the Policy Plan component of The Ontario Plan and applicable area plans; (b) applicable specific plans or planned unit developments; (c) applicable provisions of the City of Ontario Development Code; (d) applicable master plans and design guidelines of the City; and (e) applicable Standard Drawings of the City.

**SECTION 5: Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

**SECTION 6: Indemnification.** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the



applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

**SECTION 7: Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

**SECTION 8: Certification to Adoption.** The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of May 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

---

Jim Willoughby  
Planning Commission Chairman

ATTEST:

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Cathy Wahlstrom  
Planning Director and  
Secretary to the Planning Commission

STATE OF CALIFORNIA                    )  
COUNTY OF SAN BERNARDINO        )  
CITY OF ONTARIO                        )

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. \_\_\_\_\_ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 26, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

---

Gwen Berendsen  
Secretary Pro Tempore

**ATTACHMENT A:**

**File No. PMTT19-011 / TT 20291  
Departmental Conditions of Approval**

*(Departmental conditions of approval to follow this page)*



City of Ontario  
Planning Department  
303 East B Street  
Ontario, California 91764  
Phone: 909.395.2036  
Fax: 909.395.2420

## ***Planning Department Land Development Division Conditions of Approval***

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**Meeting Date:** May 26, 2020  
**File No:** PMTT19-011 / TT 20291  
**Related Files:** PDEV19-043

**Project Description:** A Tentative Tract Map (File No. PMTT19-011 / TT 20291) to subdivide 0.73-acre of land into a single lot for condominium purposes, located at 1005 and 1023 East Sixth Street, within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district. (APNs: 1047-172-17 and 1047-172-22); **submitted by TAVK Holding LLC / T&S Allied Investment LLC.**

**Prepared By:** Jeanie Irene Aguilo, Associat  
Phone: 909.395.2418 (direct)  
Email: jaguilo@ontarioca.gov

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The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

**1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

**2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

**2.1** Time Limits.

**(a)** Tentative Parcel/Tract Map approval shall become null and void 2 years following the effective date of application approval, unless the final parcel/tract map has been recorded, or a time extension has been approved by the Planning Commission pursuant to Development Code Section 2.02.025 (Time Limits and Extensions). This Permit does not supersede any individual time limits specified herein for performance of specific conditions or improvements.

**(b)** Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

**2.2** Subdivision Map.

**(a)** The Final Tract/Parcel Map shall be in conformance with the approved Tentative Tract/Parcel Map on file with the City. Variations from the approved Tentative Tract/Parcel Map may be reviewed and approved by the Planning Department. A substantial variation from the approved Tentative

Tract/Parcel Map may require review and approval by the Planning Commission, as determined by the Planning Director.

(b) Tentative Tract/Parcel Map approval shall be subject to all conditions, requirements and recommendations from all other departments/agencies provided on the attached reports/memorandums.

(c) The subject Tentative Tract/Parcel Map for condominium purposes shall require the recordation of a condominium plan concurrent with the recordation of the Final Tract/Parcel Map and CC&Rs.

(d) Pursuant to California Government Section 66474.9, the subdivider agrees that it will defend, indemnify, and hold harmless the City of Ontario or its agents, officers and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer of this subdivision, which action is brought within the time period provided for in Government Code Section 66499.37. The City of Ontario shall promptly notify the subdivider of any such claim, action or proceeding and the City of Ontario shall cooperate fully in the defense.

**2.3** General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

**2.4** Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements.

(a) CC&Rs shall be prepared for the Project and shall be recorded prior to the issuance of a building permit.

(b) The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owners association and the CC&Rs shall be reviewed and approved by the City.

(c) CC&Rs shall ensure reciprocal parking and access between parcels.

(d) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:

(i) Landscaping and irrigation systems within common areas

(ii) Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02

(iii) Shared parking facilities and access drives

(iv) Utility and drainage easements

(e) CC&Rs shall include authorization for the City's local law enforcement officers to enforce City and State traffic and penal codes within the project area.

(f) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.

(g) A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

## 2.5 Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

## 2.6 Environmental Review.

(a) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to **Section 15332 (Class 32, In-Fill Development Projects)** of the CEQA Guidelines, meeting the following conditions:

(i) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations;

(ii) The proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses;

(iii) The project site has no value as habitat for endangered, rare, or threatened species;

(iv) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and

(v) The Project site can be adequately served by all required utilities and public services.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.7 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of

Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

**2.8**     Additional Fees.

**(a)**     Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

**(b)**     After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.



**ENGINEERING DEPARTMENT  
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Information Technology & Management Services Department Conditions incorporated)

<input checked="" type="checkbox"/> <b>DEVELOPMENT PLAN</b> <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input checked="" type="checkbox"/> <b>TRACT MAP</b> <input type="checkbox"/> FOR CONDOMINIUM PURPOSES
<b>PROJECT FILE NO. <u>TM-20291</u></b>  <b>RELATED FILE NO(S). <u>PMTT19-011 &amp; PDEV19-043</u></b>	
<input checked="" type="checkbox"/> <b>ORIGINAL</b> <input type="checkbox"/> REVISED: <u>  </u> / <u>  </u> / <u>  </u>	

CITY PROJECT ENGINEER & PHONE NO:      Antonio Alejos *X.A.* (909) 395-2384

CITY PROJECT PLANNER & PHONE NO:      Jeanie Aguilo (909) 395-2418

DAB MEETING DATE:      May 18<sup>th</sup>, 2020

PROJECT NAME / DESCRIPTION:      TM-20291, a Tentative Tract Map to subdivide 0.76 acres of land into one common lot for condominium purposes.

LOCATION:      1005 & 1023 East Sixth Street

APPLICANT:      Tzyh Der Sun

REVIEWED BY:      *[Signature]*      4/22/20  
                                  Bryan Lirley, P.E.      Date  
                                  Principal Engineer

APPROVED BY:      *[Signature]*      5/1/20  
                                  Raymond Lee, P.E.      Date  
                                  Assistant City Engineer





**THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.**

**1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL: Check When Complete**

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:   
 \_\_\_\_\_ feet on \_\_\_\_\_  
 Property line corner 'cut-back' required at the intersection of \_\_\_\_\_  
 and \_\_\_\_\_.
- 1.02 Dedicate to the City of Ontario, the following easement(s): \_\_\_\_\_   
 \_\_\_\_\_
- 1.03 Restrict vehicular access to the site as follows: \_\_\_\_\_
- 1.04 Vacate the following street(s) and/or easement(s): \_\_\_\_\_
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.
- 1.06 **Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.**
- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).
- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Management Services Department at (909) 395-2124 regarding this requirement.   
 (1) \_\_\_\_\_  
 (2) \_\_\_\_\_
- 1.09 **Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.**



- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: [www.ontarioca.gov](http://www.ontarioca.gov)) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Management Services at (909) 395-2353 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments: 
  - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
  - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
  - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: \_\_\_\_\_

**2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:**

**A. GENERAL  
 ( Permits includes Grading, Building, Demolition and Encroachment )**

- 2.01 Record Tract Map No. 20291 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a duplicate photo mylar of the recorded map to the City Engineer's office.
- 2.03 Note that the subject parcel is a recognized parcel in the City of Ontario per \_\_\_\_\_
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of \_\_\_\_\_
- 2.05 Apply for a:  Certificate of Compliance with a Record of Survey;  Lot Line Adjustment 
  - Make a Dedication of Easement.
- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.



2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).

2.08 **Submit a soils/geology report.**

2.09 Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:

- State of California Department of Transportation (Caltrans)
- San Bernardino County Road Department (SBCRD)
- San Bernardino County Flood Control District (SBCFCD)
- Federal Emergency Management Agency (FEMA)
- Cucamonga Valley Water District (CVWD) for sewer/water service
- United States Army Corps of Engineers (USACE)
- California Department of Fish & Game
- Inland Empire Utilities Agency (IEUA)
- Other: \_\_\_\_\_

2.10 Dedicate to the City of Ontario the right-of-way described below:   
 \_\_\_\_\_ feet on \_\_\_\_\_

Property line corner 'cut-back' required at the intersection of \_\_\_\_\_ and \_\_\_\_\_.

2.11 Dedicate to the City of Ontario the following easement(s): \_\_\_\_\_   
 \_\_\_\_\_

2.12 New Model Colony (NMC) Developments:

1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.

2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.

3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall).

2.13 **Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.**



- 2.14 The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.
- 2.15 Pay all Development Impact Fees (DIF) to the Building Department. Final fee shall be determined based on the approved site plan.
- 2.16 Other conditions: \_\_\_\_\_



**B. PUBLIC IMPROVEMENTS**  
 (See attached Exhibit 'A' for plan check submittal requirements.)

- 2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Sixth St	Street 2	Street 3	Street 4
<b>Curb and Gutter</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> <b>Remove and replace</b> <input type="checkbox"/> Replace damaged	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged
<b>AC Pavement</b> (see Sec. 2.18)	<input checked="" type="checkbox"/> <b>Replacement</b> <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
<b>PCC Pavement</b> (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Drive Approach</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> <b>New</b> <input checked="" type="checkbox"/> <b>In-fill existing driveway approach no longer to be used</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Sidewalk</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> <b>Replace damaged</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>ADA Access Ramp</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Parkway</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> <b>Trees</b> <input checked="" type="checkbox"/> <b>Landscaping (w/irrigation)</b>	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
<b>Raised Landscaped Median</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Fire Hydrant</b> (see Sec. 2.D)	<input checked="" type="checkbox"/> <b>New / Upgrade</b> <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation



<b>Sewer</b> (see Sec. 2.C)	<input checked="" type="checkbox"/> <b>New lateral w/ clean-outs &amp; monitoring man-hole</b>	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
<b>Water</b> (see Sec. 2.D)	<input checked="" type="checkbox"/> <b>New service for domestic use only w/ meter &amp; backflow device</b> <input checked="" type="checkbox"/> <b>New service for irrigation use only w/ meter &amp; backflow device</b> <input checked="" type="checkbox"/> <b>New service for fire use only w/ DCDA</b>	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Street Light</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> <b>New</b>	<input type="checkbox"/> New / Upgrade	<input type="checkbox"/> New / Upgrade	<input type="checkbox"/> New / Upgrade
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
<b>Fiber Optics</b> (see Sec. 2K)	<input checked="" type="checkbox"/> <b>Conduit / Appurtenances</b>	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
<b>Removal of Improvements</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> <b>Abandon existing utilities no longer to be used</b> <input checked="" type="checkbox"/> <b>Remove all hardscape within parkway landscape areas</b>	_____	_____	_____



Other Improvements				
--------------------	--	--	--	--

**Specific notes for improvements listed in item no. 2.17, above:**

1. If there is a utility conflict during the construction of the proposed improvements that requires the relocation of existing utility devices, utility lines/poles, etc. Ultimate location of these utilities shall be in accordance with City Standard Drawing Numbers 1216 and 1217.
2. Existing improvements no longer to be used shall be abandoned or removed per the latest City Design Guidelines/Standard Drawings. All services/laterals shall be abandoned at the point of connection with the main.
3. The applicant/developer shall submit delta revisions to approved Potable Water, Recycled Water, and Sewer Improvement plans which reflect the installation of the proposed service laterals, and/or abandonment of unused existing service laterals. Delta revisions shall be submitted to the City for review and approval.

- 2.18 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s): 
  1. Sixth Street - Minimum limits of construction shall be along the entire property frontage, from street centerline to curb/gutter.
- 2.19 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.
- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide  water service  sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately \_\_\_\_\_, for undergrounding of utilities in accordance with Section 7-7.303.e of the City's Municipal Code.
- 2.22 Other conditions: \_\_\_\_\_

**C. SEWER**

- 2.23 An 8-inch sewer main is available for connection by this project in Sixth Street. (Ref: Sewer plan bar code: S12580)
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 Other conditions: 
  1. The applicant/developer shall install two (2) sewer laterals to the existing 8-inch sewer main in Sixth Street and equip each lateral with a monitoring manhole and clean-outs behind the property line per the latest City Design Guidelines/Standard Drawings.

**D. WATER**

- 2.27 An 8-inch water main is available for connection by this project in Sixth Street. (Ref: Water Drawing Number: Unknown)
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.



- 2.29 Other conditions:**
1. The applicant/developer shall install at minimum one (1) fire hydrant along Sixth Street property frontage per City Standard Drawing Number 4101.
  2. The applicant/developer shall install two (2) domestic water services to the existing 8-inch domestic water main in Sixth Street and equip a backflow device to the service behind the property line per the latest City Design Guidelines/Standard Drawings.
  3. The applicant/developer shall install at minimum one (1) separate water service (for irrigation purposes only) to the existing 8-inch domestic water main in Sixth Street and equip a backflow device to the service behind the property line per the latest City Design Guidelines/Standard Drawings.
  4. The applicant/developer shall install at minimum one (1) fire service to the existing 8-inch domestic water main in Sixth Street and equip the service with a DCDA behind the property line per the latest City Design Guidelines/Standard Drawings.

**E. RECYCLED WATER**

- 2.30 A recycled water main is not available for connection by this project. (Ref: Recycled Water Drawing Number: None)**
- 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
- 2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.
- 2.33 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.
- Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.
- 2.34 Other conditions: \_\_\_\_\_

**F. TRAFFIC / TRANSPORTATION**

- 2.35 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
1. On-site and off-site circulation
  2. Traffic level of service (LOS) at 'build-out' and future years
  3. Impact at specific intersections as selected by the City Engineer
- 2.36 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.
- 2.37 Other conditions:**
1. The applicant/developer shall replace the existing type 'A' curb along the entire property frontage with new curb and gutter per City Standard Drawing Number 1201.
  2. The applicant/developer shall construct all new driveway approaches per City Standard Drawing Number 1203.
  3. The applicant/developer shall in-fill the existing driveway approach no longer to be used with full height curb/gutter, parkway landscaping and sidewalk per the latest City Design Guidelines/Standard Drawings.
  4. The applicant/developer shall replace any sidewalk panels currently damaged or damaged during construction per City Standard Drawing Number 1209.





5. The applicant/developer shall install parkway trees/landscaping along Sixth Street property frontage per the Landscape Planning Division Requirements.
6. All landscaping, block walls and other obstructions in the parkway shall be compatible with the stopping sight distance requirements per City Standard Drawing Number 1309.
7. The applicant/developer shall be responsible to design and construct in-fill public street lights along Sixth Street property frontage. Street lighting shall be LED-type and in accordance with City's Traffic and Transportation Design Guidelines.
8. The applicant/developer shall remove all hardscape (concrete, rocks, etc.) from the within the parkway landscape area along the entire property frontage per Landscape Planning Division Requirements.

#### G. DRAINAGE / HYDROLOGY

- 2.38 A storm drain main is not available to accept flows from this project. (Ref: Storm Drain Drawing Number: None)
- 2.39 Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.
- 2.40 An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100-year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.
- 2.41 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.
- 2.42 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.43 Other conditions: \_\_\_\_\_

#### H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.44 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.  
 If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.  
 Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.45 **Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.**
- 2.46 Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector



screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.

2.47 Other conditions: \_\_\_\_\_

**J. SPECIAL DISTRICTS**

2.48 File an application, together with an initial payment deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community facilities District Act of 1982. The application and fee shall be submitted a minimum four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact the Management Services Department at (909) 395-2353 to initiate the CFD application process.

2.49 Other conditions: \_\_\_\_\_

**K. FIBER OPTIC**

2.50 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand-hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Generally located along Sixth Street property frontage, see Fiber Optic Exhibit herein for reference.

2.51 Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Information Technology Department at (909) 395-2000, regarding this requirement.

**L. Solid Waste**

2.52 Onsite solid waste shall be designed in accordance with the City's Solid Waste Manual location at:

<http://www.ontarioca.gov/municipal-utilities-company/solid-waste>

2.53 Other conditions:

1. The applicant/developer shall submit a Solid Waste Handling Plan (SWHP) and incorporate the following requirements:
  - a. For designated storage and collection areas not accessible by the standard overhead loading solid waste vehicles (such as areas within parking structures), scouting services utilizing scouting vehicles may be utilized. To utilize scouting services, the following items must be addressed:
    - i. Show scouting vehicle path of travel to assure path of scouting vehicle travel meets minimum access requirement.
    - ii. Show turning radius. inside: # feet. outside: # feet and vertical clearance (floor to overhead obstructions): # feet (including through surface grade breaks).
    - iii. Show typical scaled cross sections of the vertical path of travel through entrances, surface grade breaks, and anywhere the total vertical clearance may change.
  - b. Each staging area must be accessible to the overhead loading solid waste vehicles meeting those vehicle access standards.
  - c. The total of all the staging areas must be sufficiently sized to temporarily locate all the bins on-site for collections and cannot compete/conflict with parking or traffic.

For questions on the SWHP submittal requirements, please contact:

Peter Tran, Associate Engineer  
[ptran@ontarioca.gov](mailto:ptran@ontarioca.gov)  
 Phone: (909) 395-2677



**3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:**

- 3.01 **Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.**
- 3.02 Complete all requirements for recycled water usage. 
  - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
  - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
  - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 **The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.**
- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 **Confirm payment of all Development Impact Fees (DIF) to the Building Department.**
- 3.06 **Submit electronic copies (PDF & Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).**

**4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:**

- 4.01 **Complete all Conditions of Approval listed under Sections 1-3 above.**
- 4.02 **Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.**
- 4.03 **The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.**



**EXHIBIT 'A'**

**ENGINEERING DEPARTMENT  
First Plan Check Submittal Checklist**

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**Project Number: PDEV19-043 and Tract Map No. 20291**

**The following items are required to be included with the first plan check submittal:**

1.  **A copy of this check list**
2.  **Payment of fee for Plan Checking**
3.  **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4.  **One (1) copy of project Conditions of Approval**
5.  **Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).**
6.  Three (3) sets of Public Street improvement plan with street cross-sections
7.  Three (3) sets of Private Street improvement plan with street cross-sections
8.  Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)
9.  Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10.  Four (4) sets of Public Sewer improvement plan
11.  Five (5) sets of Public Storm Drain improvement plan
12.  **Three (3) sets of Public Street Light improvement plan**
13.  Three (3) sets of Signing and Striping improvement plan
14.  **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15.  **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
16.  Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
17.  **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
18.  **One (1) copy of Hydrology/Drainage study**
19.  **One (1) copy of Soils/Geology report**
20.  **Payment for Final Map/Parcel Map processing fee**
21.  **Three (3) copies of Final Map/Parcel Map**



- 22.  One (1) copy of approved Tentative Map
- 23.  One (1) copy of Preliminary Title Report (current within 30 days)
- 24.  One (1) copy of Traverse Closure Calculations
- 25.  One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.
- 26.  Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 27.  Other: \_\_\_\_\_

# FIBER OPTIC EXHIBIT LAND FILE NO. TM-20291 1005 & 1023 E SIXTH ST

**OWNER/APPLICANT:**  
CALLAND ENGINEERING, INC.  
10297 ACADIA STREET  
BREDA, CA 92314  
PHONE: (626) 705-2110  
FAX: (626) 705-2110

**ARCHITECT:**  
Z. SUN & ASSOCIATES, INC.  
200 S. GARDEN STREET  
BREDA, CA 92310  
PHONE: (626) 705-2210  
FAX: (626) 705-2210

**BENCHMARK:**  
BENCH MARK STATION 500  
20' BRASS GROUND STATION 740' OF SO. CAL.  
4" NORTH OF INTERSECTION OF  
NORTH AND NORTHWEST CORNER  
ELEVATION: 118.83'

**ADDITIONAL TO ALL ELEVATIONS TO OBTAIN  
CORRECT ELEVATION:**

**CIVIL ENGINEER:**  
CALLAND ENGINEERING, INC.  
576 E. LAMBERT ROAD  
BREDA, CA 92314, CA, USA  
PHONE: (714) 671-1050  
FAX: (714) 671-1050

**SOILS ENGINEER:**  
SOILS ENGINEERING, INC.  
576 E. LAMBERT ROAD  
BREDA, CA 92314, CA, USA  
PHONE: (714) 671-1050  
FAX: (714) 671-1050

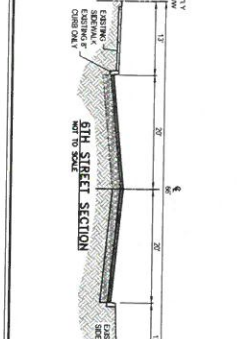
**DATE OF BEARING:**  
ENGINEERING OF 6TH STREET IN 89'x34'x5' E PER  
PARCEL MAP NO. 70, 2011-1-44

**LEGAL DESCRIPTION:**  
A PARTITION OF LOT 172, OAKHARD COLONY LOTS, IN THE CITY OF OAKHARD, COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, AS PER PLAT RECORDED IN BOOK 11, PAGE 5425, IN THE COUNTY RECORDS OFFICE OF SAID COUNTY.  
APN: 1047-172-17 AND -22

ITEM NO.	DESCRIPTION	QUANTITY	UNIT
1	CONCRETE	1.233	CF
2	STEEL	1.443	LF
3	BRICK	1.443	SF
4	PAVING	1.443	SF
5	LANDSCAPING	1.443	SF
6	WATER	1.443	GF
7	SEWER	1.443	GF
8	CONCRETE	1.443	CF
9	STEEL	1.443	LF
10	BRICK	1.443	SF
11	PAVING	1.443	SF
12	LANDSCAPING	1.443	SF
13	WATER	1.443	GF
14	SEWER	1.443	GF
15	CONCRETE	1.443	CF
16	STEEL	1.443	LF
17	BRICK	1.443	SF
18	PAVING	1.443	SF
19	LANDSCAPING	1.443	SF
20	WATER	1.443	GF
21	SEWER	1.443	GF
22	CONCRETE	1.443	CF
23	STEEL	1.443	LF
24	BRICK	1.443	SF
25	PAVING	1.443	SF
26	LANDSCAPING	1.443	SF
27	WATER	1.443	GF
28	SEWER	1.443	GF
29	CONCRETE	1.443	CF
30	STEEL	1.443	LF
31	BRICK	1.443	SF
32	PAVING	1.443	SF
33	LANDSCAPING	1.443	SF
34	WATER	1.443	GF
35	SEWER	1.443	GF
36	CONCRETE	1.443	CF
37	STEEL	1.443	LF
38	BRICK	1.443	SF
39	PAVING	1.443	SF
40	LANDSCAPING	1.443	SF
41	WATER	1.443	GF
42	SEWER	1.443	GF
43	CONCRETE	1.443	CF
44	STEEL	1.443	LF
45	BRICK	1.443	SF
46	PAVING	1.443	SF
47	LANDSCAPING	1.443	SF
48	WATER	1.443	GF
49	SEWER	1.443	GF
50	CONCRETE	1.443	CF

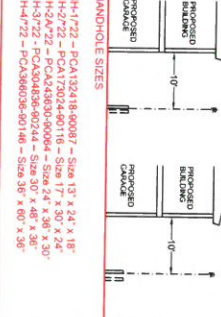
**NOTES:**  
1. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.  
2. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE CALIFORNIA BUILDING CODE AND ALL APPLICABLE ORDINANCES.  
3. ALL UTILITIES SHALL BE DEPTH MARKED AND PROTECTED IN PLACE.  
4. ALL EXISTING UTILITIES SHALL BE DEPTH MARKED AND PROTECTED IN PLACE.  
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- 50. PROPOSED DRIVEWAY



## CONCEPTUAL GRADING PLAN

TM-20291  
1005 & 1023 EAST 6TH STREET  
COURTESY DESIGN PLAN EXHIBIT  
05/07/2020 - AM/VACA

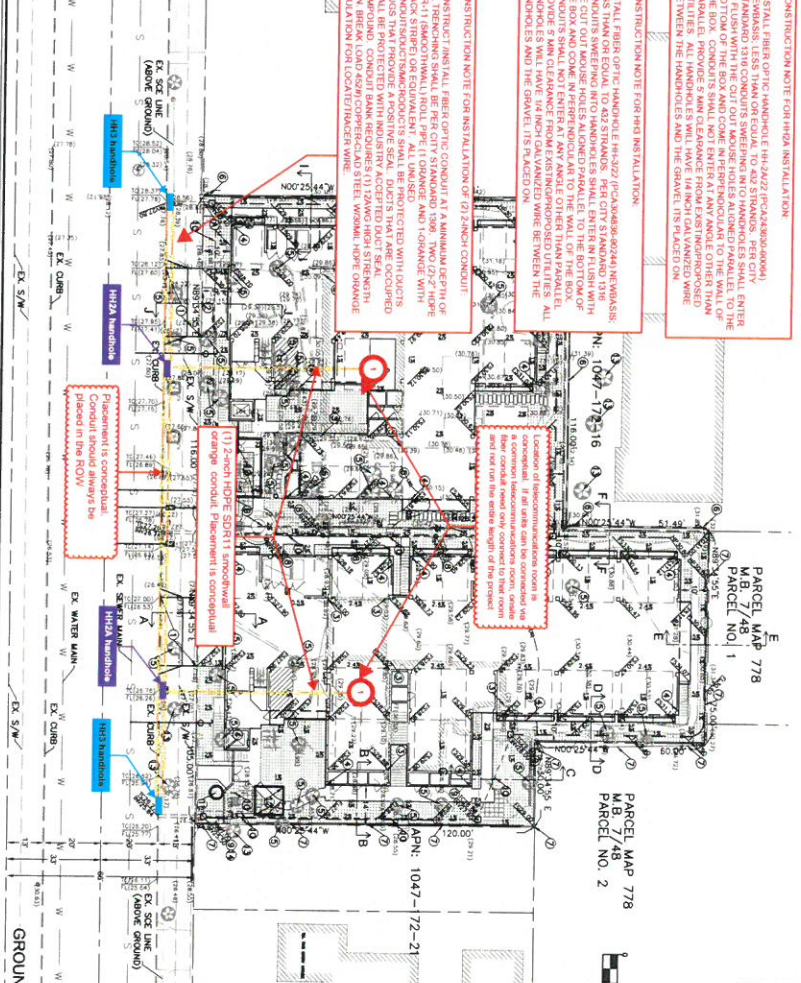


**CONSTRUCTION NOTE FOR HIGH INSTALLATION:**  
INSTALL FIBER OPTIC HANDHOLE (H-1722) PER THE FOLLOWING DIMENSIONS:  
NEARWAYS: LESS THAN OR EQUAL TO 482 STANDBY (SEE SECTION 1722)  
STANDARD: 1722 EQUAL TO OR GREATER THAN 482 STANDBY (SEE SECTION 1722)  
THE BOTTOM OF THE BOX AND COVER SHALL BE HIGHER PARALLEL TO THE UTILITY. ALL HANDHOLES AND THE GRAVEL ITS PLACED ON BETWEEN THE HANDHOLES AND THE GRAVEL ITS PLACED ON.

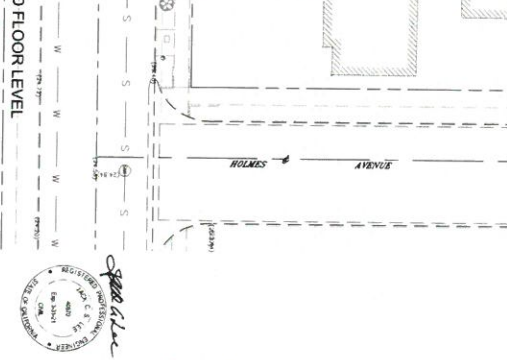
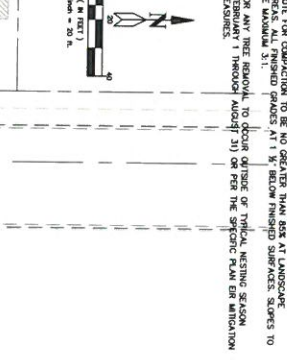
**CONSTRUCTION NOTE FOR HIGH INSTALLATION:**  
INSTALL FIBER OPTIC HANDHOLE (H-3022) PER THE FOLLOWING DIMENSIONS:  
NEARWAYS: LESS THAN OR EQUAL TO 482 STANDBY (SEE SECTION 1722)  
STANDARD: 3022 EQUAL TO OR GREATER THAN 482 STANDBY (SEE SECTION 1722)  
THE BOTTOM OF THE BOX AND COVER SHALL BE HIGHER PARALLEL TO THE UTILITY. ALL HANDHOLES AND THE GRAVEL ITS PLACED ON BETWEEN THE HANDHOLES AND THE GRAVEL ITS PLACED ON.

**CONSTRUCTION NOTE FOR HIGH INSTALLATION:**  
INSTALL FIBER OPTIC HANDHOLE (H-4222) PER THE FOLLOWING DIMENSIONS:  
NEARWAYS: LESS THAN OR EQUAL TO 482 STANDBY (SEE SECTION 1722)  
STANDARD: 4222 EQUAL TO OR GREATER THAN 482 STANDBY (SEE SECTION 1722)  
THE BOTTOM OF THE BOX AND COVER SHALL BE HIGHER PARALLEL TO THE UTILITY. ALL HANDHOLES AND THE GRAVEL ITS PLACED ON BETWEEN THE HANDHOLES AND THE GRAVEL ITS PLACED ON.

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INSTALL FIBER OPTIC HANDHOLE (H-4222) PER THE FOLLOWING DIMENSIONS:  
NEARWAYS: LESS THAN OR EQUAL TO 482 STANDBY (SEE SECTION 1722)  
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**ADDITIONAL NOTES:**  
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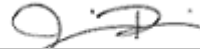


<b>C-1</b>	<p><b>PROJECT LOCATION:</b> 1005 &amp; 1023 E. 6TH STREET, ONTARIO, CA 91764</p>	<p><b>DATE:</b> 03-04-2020 <b>SCALE:</b> 1" = 20' <b>SHEET:</b> 1 OF 2 SHEET</p>	<p><b>REGISTERED PROFESSIONAL ENGINEER</b> CALLAND ENGINEERING, INC. 576 E. LAMBERT ROAD, BREDA, CA 92821 TEL: (714) 671-1050 FAX: (714) 671-1050</p>
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**CITY OF ONTARIO**  
**LANDSCAPE PLANNING DIVISION**  
 303 East "B" Street, Ontario, CA 91764

**CONDITIONS OF APPROVAL**

**Sign Off**



Jamie Richardson, Sr. Landscape Planner

3/31/2020

Date

Reviewer's Name:

**Jamie Richardson, Sr. Landscape Planner**

Phone:

**(909) 395-2615**

D.A.B. File No.:

PMTT19-011

Related Files:

Case Planner:

Jeanie Irene Aguilo

Project Name and Location:

18 Unit Multi Family – Condo Tentative Tract Map  
 TM20291

Applicant/Representative:

TZYH DER SUN (Cliff Sun)  
 9431 Longden Ave.  
 Temple City, CA 91780



**A Tentative Tract Map (dated) has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.**



**A Tentative Tract Map (dated 3/17/2020) has not been approved. Corrections noted below are required prior to DAB approval.**

**CONDITIONS REQUIRED**

1. Show and identify any on-site storm water infiltration areas or storm water infiltration devices proposed in parkways or other landscape areas.
2. Show on demo plans and landscape construction plans trees to be preserved, removed or mitigation measures for trees removed. Tree inventory identifies 129 inches of trunk diameter of heritage trees that will be removed due to construction. Provide mitigation measures as defined below:
  - a. New 15 gallon trees min 1" diameter trunk, in addition to trees required (129-15 gallon)
  - b. New 24" box trees min 1.5" diameter trunk, in addition to trees required. (86-24" box)
  - c. Upsizing trees on the plan one size larger such as 15 gallon to 24" box, or 24" to 36" box size. (to equal trunk replacement)
  - d. Monetary value of the trees removed as identified in the "Guide for Plant Appraisal", approved certified arborist plant appraiser, or may be equal to the value of the installation cost of planting, fertilizing, staking and irrigating 15 gallon trees, (100\$ each) to the City of Ontario Historic Preservation Fund for city tree planting or city approved combination of the above items. (\$12,900)
3. The arborist report identifies various healthy fruit trees that can be relocated to provide a viable resource to the community (see arborist report tree #4, 9, 10, 14, 15, 22, 23, 24, 33, 37, 38, 39, 40, 42 & 44). Contact Arthur Levine (917) 734-8108 at Huerta Del Valle – Ontario Community Garden to coordinate the possible relocation of fruit trees identified to be healthy and vigorous with a rating of B- and above.
  1. Coordinate with the City of Ontario Parks and Maintenance Department – Adrian Escamilla, Parks and Maintenance Manager (909) 395-2635 to relocate existing Heritage Tree #20 Phoenix canariensis.
2. After a project's entitlement approval, the applicant shall pay all applicable fees at a rate established by resolution of the City Council. Fees are:

Plan Check—5 or more acres.....	\$1,301.00
Inspection—Construction (per phase up to 3 inspections).....	\$278.00
Total.....	\$2,604.00
Inspection—Field - additional.....	\$83.00

# AIRPORT LAND USE COMPATIBILITY PLANNING

## CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV19-043 & PMTT19-011

Address: 1005 & 1023 East 6th Street

APN: 1047-172-17 & 22

Existing Land Use: Single Family Residential

Proposed Land Use: Development Plan 18 unit multi-family townhome complex & Tentative Tract Map for Condominium purposes

Site Acreage: 0.73 Proposed Structure Height: 36 ft

ONT-IAC Project Review: N/A

Airport Influence Area: ONT

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Jeanie Aguilo

Date: 11/6/19

CD No.: 2019-053

PALU No.: n/a

### The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input checked="" type="checkbox"/> High Terrain Zone	<input type="radio"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input type="radio"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input checked="" type="checkbox"/> Real Estate Transaction Disclosure
<input type="radio"/> Zone 3	<input type="checkbox"/> 60 - 65 dB CNEL	<input type="radio"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: <u>70 FT</u>	
<input type="radio"/> Zone 5			

### The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1   
  Zone 2   
  Zone 3   
  Zone 4   
  Zone 5   
  Zone 6

Allowable Height: \_\_\_\_\_

## CONSISTENCY DETERMINATION

This proposed Project is:  Exempt from the ALUCP     Consistent     Consistent with Conditions     Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Real Estate Transaction Disclosure Required

Airport Planner Signature: \_\_\_\_\_



# CITY OF ONTARIO

## MEMORANDUM

**TO:** PLANNING DEPARTMENT, Jeanie Irene Aguilo  
**FROM:** BUILDING DEPARTMENT, Kevin Shear  
**DATE:** July 24, 2019  
**SUBJECT:** PMTT19-011

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1. The plan **does** adequately address the departmental concerns at this time.  
No comments.

KS:lr

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV19-043, A DEVELOPMENT PLAN TO CONSTRUCT 18 MULTIPLE-FAMILY DWELLING UNITS ON 0.73-ACRE OF LAND LOCATED AT 1005 AND 1023 EAST SIXTH STREET, WITHIN THE HDR-45 (HIGH DENSITY RESIDENTIAL - 25.1 TO 45.0 DU/ACRE) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 1047-172-17 AND 1047-172-22.

WHEREAS, TAVK HOLDING LLC / T&S ALLIED INVESTMENT LLC, ("Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV19-043, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 0.73-acre of land, located at 1005 and 1023 East Sixth Street, within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district, and is presently improved with a single-family residence; and

WHEREAS, the property to the north of the Project site is within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district, and is developed with multi-family residential. The property to the east is within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district and is developed with single-family residential. The property to the south is within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district and is developed with single-family residential. The property to the west is within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district and is developed with Bright Star Montessori Preschool; and

WHEREAS, on July 19, 2019, the Applicant submitted a Development Plan application requesting to construct 18 multiple-family dwelling units on the Project site. A Tentative Tract Map (File No. PMTT19-011/TT 20291) was submitted in conjunction with the Development Plan, requesting approval to subdivide the Project site into a single lot for condominium purposes to facilitate the future sale of individual units; and

WHEREAS, on May 18, 2020, the Development Advisory Board reviewed the subject application and recommended that the Planning Commission approve the proposed Project subject to the departmental conditions of approval; and

WHEREAS, the Project site consists of two multiple-family apartment buildings, each designed with a first-floor concrete podium slab and bearing walls that house a parking garage, refuse collection facilities and tenant storage facilities; and

WHEREAS, each building has a two-story wood-framed structure designed on top of each podium, for an overall height of 3 stories (33 to 36 feet, on average). Building 1 is located on the western half of the site and consists of 8 units. Building 2 is located on the eastern half of the site and consists of 10 units; and

WHEREAS, five different floor plans are proposed, which range from 605 to 800 square feet in size. The Project provides 12 units with two-bedroom floor plans and 6 units with three-bedroom floor plans; and

WHEREAS, the open space requirements of the HDR-45 zoning district contains a recommendation that a project provide a minimum of 60 square feet (20 percent) of private open space and 250 square feet (80 percent) of common open space per dwelling unit. The Development Codes allows for deviations in private and common open space so long as the total amount of open space provided equals a minimum of 310 square feet per dwelling unit; and

WHEREAS, the Project includes common open space and recreation amenities in support of the 18 dwelling units, such as a fitness trail, tot lot with play structure, picnic and BBQ area, and sitting areas. The large common open areas with recreational amenities have been strategically located for the convenience of the residents and to enhance the Project. In addition, large courtyards with meandering walkways between the units have been provided throughout the Project; and

WHEREAS, Project access is provided by a driveway approach for each building, with access taken from Sixth Street; and

WHEREAS, the Project has provided off-street parking pursuant to the "Multiple-Family Residential" parking standards specified in the Development Code. A total of 45 parking spaces have been provided on-site, while 2 guest spaces are provided on-street, along the Project's street frontage; and

WHEREAS, the architectural style proposed consists of a modern interpretation of a Mid-Century Modern design, with a simple box form and accent elements at entries, flat roofs with parapet detailing, and cantilever awnings. The mixture of building materials proposed includes medium and course textured stucco finishes with a neutral beige color palette and metal reveals, aluminum cantilevered awnings, and metal guardrails. Additionally, mechanical equipment will be roof-mounted and obscured from public view by parapet walls; and

WHEREAS, the Project provides substantial landscaping along the Sixth Street frontage and throughout the Project site, for an overall landscape coverage of 20.7 percent. The landscape plan incorporates a combination of 48-inch, 36-inch, and 24-inch box trees along Sixth Street and throughout the Project site, which includes a mix of Coast

Live Oak, Crape Myrtle and Eastern Redbud trees. In addition, a mix of 15-gallon and 24-inch accent and shade trees will be provided throughout the Project site that includes Brisbane Box and Long Leafed Podocarpus trees. A variety of shrubs and groundcovers are also being provided, which are low water usage or drought tolerant; and

WHEREAS, all necessary public utilities (water and sewer) were previously installed in Sixth Street. Furthermore, the Applicant has submitted a Preliminary Water Quality Management Plan (PWQMP), which establishes the Project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development (LID) best management practices (BMPs), such as retention and infiltration, biotreatment, and evapotranspiration. The PWQMP proposes the use of vegetated swales, which lead to underground stormwater infiltration systems installed for the Project. Any overflow drainage will be conveyed to the public street by way of parkway culverts; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act, commencing with Public Resources Code Section 21000 (hereinafter referred to as "CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 18, 2020, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB20-027, recommending that the Planning Commission approve the Application; and

WHEREAS, on May 26, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

**SECTION 1: Environmental Determination and Findings.** As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines and meets each of the following conditions: [1] the Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations; [2] the proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses; [3] the Project site has no value as habitat for endangered, rare, or threatened species; [4] approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and [5] the Project site can be adequately served by all required utilities and public services; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

**SECTION 2: *Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

**SECTION 3: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

**SECTION 4: *Concluding Facts and Reasons.*** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the HDR (High Density Residential) land use district of the Policy Plan Land Use Map, and the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning

district. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan; and

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district, including standards relative to the particular land use proposed (multiple-family dwellings), as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions; and

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Development Code are maintained; [ii] the Project will not endanger the public health, safety or general welfare; [iii] the Project will not result in any significant environmental impacts; [iv] the Project will be in harmony with the area in which it is located; and [v] the Project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan; and

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.*** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Development Code that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (multiple-family dwellings). As a result of this review, the Development Advisory Board has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the Development Code.

**SECTION 5: Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set

forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

**SECTION 6: *Indemnification.*** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

**SECTION 7: *Custodian of Records.*** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

**SECTION 8: *Certification to Adoption.*** The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of May 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

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Jim Willoughby  
Planning Commission Chairman

ATTEST:

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Cathy Wahlstrom  
Planning Director and  
Secretary to the Planning Commission



STATE OF CALIFORNIA                    )  
COUNTY OF SAN BERNARDINO        )  
CITY OF ONTARIO                        )

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. \_\_\_\_\_ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 26, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

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Gwen Berendsen  
Secretary Pro Tempore

**ATTACHMENT A:**

**File No. PDEV19-043  
Departmental Conditions of Approval**

*(Departmental conditions of approval to follow this page)*



City of Ontario  
Planning Department  
303 East B Street  
Ontario, California 91764  
Phone: 909.395.2036  
Fax: 909.395.2420

## ***Planning Department Land Development Division Conditions of Approval***

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**Meeting Date:** May 26, 2020  
**File No:** PDEV19-043  
**Related Files:** PMTT19-011 / TT 20291

**Project Description:** A Development Plan (File No. PDEV19-043) to construct 18 multiple-family dwellings on 0.73-acre of land, located at 1005 and 1023 East Sixth Street, within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district. (APNs: 1047-172-17 and 1047-172-22); **submitted by TAVK Holding LLC / T&S Allied Investment LLC.**

**Prepared By:** Jeanie Irene Aguilo, Associate Planner  
Phone: 909.395.2418 (direct)  
Email: jaguilo@ontarioca.gov

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The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

**1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

**1.1 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

**(a) Time Limits.** Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

**1.2 General Requirements.** The Project shall comply with the following general requirements:

**(a)** All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

**(b)** The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

**(c)** The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

**1.3**     Landscaping.

(a)     The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b)     Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c)     Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d)     Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

**1.4**     Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

**1.5**     Parking, Circulation and Access.

(a)     The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b)     All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c)     Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d)     The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(e)     Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(f)     Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

**1.6**     Site Lighting.

(a)     All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

**1.7** Mechanical and Rooftop Equipment.

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

**1.8** Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

**1.9** Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.0 (Sign Regulations).

**1.10** Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

**1.11** Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements.

(a) CC&Rs shall be prepared for the Project and shall be recorded prior to the issuance of a building permit.

(b) The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owners association and the CC&Rs shall be reviewed and approved by the City.

(c) CC&Rs shall ensure reciprocal parking and access between parcels.

(d) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:

(i) Landscaping and irrigation systems within common areas

(ii) Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02

(iii) Shared parking facilities and access drives

(iv) Utility and drainage easements

(e) CC&Rs shall include authorization for the City's local law enforcement officers to enforce City and State traffic and penal codes within the project area.

(f) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.

(g) A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not

occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

**1.12** Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

**1.13** Environmental Review.

(a) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to **Section 15332 (Class 32, In-Fill Development Projects)** of the CEQA Guidelines, meeting the following conditions:

(i) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations.

(ii) The proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses

(iii) The project site has no value as habitat for endangered, rare, or threatened species.

(iv) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality.

(v) The Project site can be adequately served by all required utilities and public services.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

**1.14** Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

**1.15** Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the

requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

**1.16** Additional Requirements.

(a) Provide decorative bollards at the ends of each driveway along the garden/sidewalk path for each building. Final design shall be subject to Planning Director review and approval.

(b) The approval of File No. PDEV19-043 shall be final and conclusive upon the approval of File No. PMTT19-011 / TM 20291 by the Planning Commission.



**ENGINEERING DEPARTMENT  
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Information Technology & Management Services Department Conditions incorporated)

<input checked="" type="checkbox"/> <b>DEVELOPMENT PLAN</b> <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input checked="" type="checkbox"/> <b>TRACT MAP</b> <input type="checkbox"/> FOR CONDOMINIUM PURPOSES
<b>PROJECT FILE NO. <u>TM-20291</u></b>  <b>RELATED FILE NO(S). <u>PMTT19-011 &amp; PDEV19-043</u></b>	
<input checked="" type="checkbox"/> <b>ORIGINAL</b> <input type="checkbox"/> REVISED: <u>  </u> / <u>  </u> / <u>  </u>	

CITY PROJECT ENGINEER & PHONE NO:      Antonio Alejos *X.A.* (909) 395-2384

CITY PROJECT PLANNER & PHONE NO:      Jeanie Aguilo (909) 395-2418

DAB MEETING DATE:      May 18<sup>th</sup>, 2020

PROJECT NAME / DESCRIPTION:      TM-20291, a Tentative Tract Map to subdivide 0.76 acres of land into one common lot for condominium purposes.

LOCATION:      1005 & 1023 East Sixth Street

APPLICANT:      Tzyh Der Sun

REVIEWED BY:      *[Signature]*      4/22/20  
                                  Bryan Lirley, P.E.      Date

APPROVED BY:      *[Signature]*      5/1/20  
                                  Raymond Lee, P.E.      Date  
                                  Assistant City Engineer





**THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.**

**1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL: Check When Complete**

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:   
 \_\_\_\_\_ feet on \_\_\_\_\_  
 Property line corner 'cut-back' required at the intersection of \_\_\_\_\_  
 and \_\_\_\_\_.
- 1.02 Dedicate to the City of Ontario, the following easement(s): \_\_\_\_\_   
 \_\_\_\_\_
- 1.03 Restrict vehicular access to the site as follows: \_\_\_\_\_
- 1.04 Vacate the following street(s) and/or easement(s): \_\_\_\_\_
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.
- 1.06 **Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.**
- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).
- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Management Services Department at (909) 395-2124 regarding this requirement.   
 (1) \_\_\_\_\_  
 (2) \_\_\_\_\_
- 1.09 **Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.**



- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: [www.ontarioca.gov](http://www.ontarioca.gov)) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Management Services at (909) 395-2353 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments: 
  - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
  - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
  - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: \_\_\_\_\_

**2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:**

**A. GENERAL  
 ( Permits includes Grading, Building, Demolition and Encroachment )**

- 2.01 Record Tract Map No. 20291 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a duplicate photo mylar of the recorded map to the City Engineer's office.
- 2.03 Note that the subject parcel is a recognized parcel in the City of Ontario per \_\_\_\_\_
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of \_\_\_\_\_
- 2.05 Apply for a:  Certificate of Compliance with a Record of Survey;  Lot Line Adjustment 
  - Make a Dedication of Easement.
- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.



2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).

2.08 **Submit a soils/geology report.**

2.09 Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:

- State of California Department of Transportation (Caltrans)
- San Bernardino County Road Department (SBCRD)
- San Bernardino County Flood Control District (SBCFCD)
- Federal Emergency Management Agency (FEMA)
- Cucamonga Valley Water District (CVWD) for sewer/water service
- United States Army Corps of Engineers (USACE)
- California Department of Fish & Game
- Inland Empire Utilities Agency (IEUA)
- Other: \_\_\_\_\_

2.10 Dedicate to the City of Ontario the right-of-way described below:   
\_\_\_\_\_ feet on \_\_\_\_\_

Property line corner 'cut-back' required at the intersection of \_\_\_\_\_  
and \_\_\_\_\_.

2.11 Dedicate to the City of Ontario the following easement(s): \_\_\_\_\_   
\_\_\_\_\_

2.12 New Model Colony (NMC) Developments:

1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.

2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.

3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall).

2.13 **Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.**



- 2.14 The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.
- 2.15 Pay all Development Impact Fees (DIF) to the Building Department. Final fee shall be determined based on the approved site plan.
- 2.16 Other conditions: \_\_\_\_\_



**B. PUBLIC IMPROVEMENTS**  
 (See attached Exhibit 'A' for plan check submittal requirements.)

- 2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Sixth St	Street 2	Street 3	Street 4
<b>Curb and Gutter</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> <b>Remove and replace</b> <input type="checkbox"/> Replace damaged	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged
<b>AC Pavement</b> (see Sec. 2.18)	<input checked="" type="checkbox"/> <b>Replacement</b> <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
<b>PCC Pavement</b> (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Drive Approach</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> <b>New</b> <input checked="" type="checkbox"/> <b>In-fill existing driveway approach no longer to be used</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Sidewalk</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> <b>Replace damaged</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>ADA Access Ramp</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Parkway</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> <b>Trees</b> <input checked="" type="checkbox"/> <b>Landscaping (w/irrigation)</b>	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
<b>Raised Landscaped Median</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Fire Hydrant</b> (see Sec. 2.D)	<input checked="" type="checkbox"/> <b>New / Upgrade</b> <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation



<b>Sewer</b> (see Sec. 2.C)	<input checked="" type="checkbox"/> <b>New lateral w/ clean-outs &amp; monitoring man-hole</b>	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
<b>Water</b> (see Sec. 2.D)	<input checked="" type="checkbox"/> <b>New service for domestic use only w/ meter &amp; backflow device</b> <input checked="" type="checkbox"/> <b>New service for irrigation use only w/ meter &amp; backflow device</b> <input checked="" type="checkbox"/> <b>New service for fire use only w/ DCDA</b>	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Street Light</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> <b>New</b>	<input type="checkbox"/> New / Upgrade	<input type="checkbox"/> New / Upgrade	<input type="checkbox"/> New / Upgrade
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
<b>Fiber Optics</b> (see Sec. 2K)	<input checked="" type="checkbox"/> <b>Conduit / Appurtenances</b>	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
<b>Removal of Improvements</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> <b>Abandon existing utilities no longer to be used</b> <input checked="" type="checkbox"/> <b>Remove all hardscape within parkway landscape areas</b>	_____	_____	_____



Other Improvements				
--------------------	--	--	--	--

**Specific notes for improvements listed in item no. 2.17, above:**

1. If there is a utility conflict during the construction of the proposed improvements that requires the relocation of existing utility devices, utility lines/poles, etc. Ultimate location of these utilities shall be in accordance with City Standard Drawing Numbers 1216 and 1217.
2. Existing improvements no longer to be used shall be abandoned or removed per the latest City Design Guidelines/Standard Drawings. All services/laterals shall be abandoned at the point of connection with the main.
3. The applicant/developer shall submit delta revisions to approved Potable Water, Recycled Water, and Sewer Improvement plans which reflect the installation of the proposed service laterals, and/or abandonment of unused existing service laterals. Delta revisions shall be submitted to the City for review and approval.

- 2.18 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s): 
  1. Sixth Street - Minimum limits of construction shall be along the entire property frontage, from street centerline to curb/gutter.
- 2.19 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.
- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide  water service  sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately \_\_\_\_\_, for undergrounding of utilities in accordance with Section 7-7.303.e of the City's Municipal Code.
- 2.22 Other conditions: \_\_\_\_\_

**C. SEWER**

- 2.23 An 8-inch sewer main is available for connection by this project in Sixth Street. (Ref: Sewer plan bar code: S12580)
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 Other conditions: 
  1. The applicant/developer shall install two (2) sewer laterals to the existing 8-inch sewer main in Sixth Street and equip each lateral with a monitoring manhole and clean-outs behind the property line per the latest City Design Guidelines/Standard Drawings.

**D. WATER**

- 2.27 An 8-inch water main is available for connection by this project in Sixth Street. (Ref: Water Drawing Number: Unknown)
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.



- 2.29 Other conditions:**
1. **The applicant/developer shall install at minimum one (1) fire hydrant along Sixth Street property frontage per City Standard Drawing Number 4101.**
  2. **The applicant/developer shall install two (2) domestic water services to the existing 8-inch domestic water main in Sixth Street and equip a backflow device to the service behind the property line per the latest City Design Guidelines/Standard Drawings.**
  3. **The applicant/developer shall install at minimum one (1) separate water service (for irrigation purposes only) to the existing 8-inch domestic water main in Sixth Street and equip a backflow device to the service behind the property line per the latest City Design Guidelines/Standard Drawings.**
  4. **The applicant/developer shall install at minimum one (1) fire service to the existing 8-inch domestic water main in Sixth Street and equip the service with a DCDA behind the property line per the latest City Design Guidelines/Standard Drawings.**

**E. RECYCLED WATER**

- 2.30 A recycled water main is not available for connection by this project. (Ref: Recycled Water Drawing Number: None)**
- 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
- 2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.
- 2.33 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.
- Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.
- 2.34 Other conditions: \_\_\_\_\_

**F. TRAFFIC / TRANSPORTATION**

- 2.35 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
1. On-site and off-site circulation
  2. Traffic level of service (LOS) at 'build-out' and future years
  3. Impact at specific intersections as selected by the City Engineer
- 2.36 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.
- 2.37 Other conditions:**
1. **The applicant/developer shall replace the existing type 'A' curb along the entire property frontage with new curb and gutter per City Standard Drawing Number 1201.**
  2. **The applicant/developer shall construct all new driveway approaches per City Standard Drawing Number 1203.**
  3. **The applicant/developer shall in-fill the existing driveway approach no longer to be used with full height curb/gutter, parkway landscaping and sidewalk per the latest City Design Guidelines/Standard Drawings.**
  4. **The applicant/developer shall replace any sidewalk panels currently damaged or damaged during construction per City Standard Drawing Number 1209.**





5. The applicant/developer shall install parkway trees/landscaping along Sixth Street property frontage per the Landscape Planning Division Requirements.
6. All landscaping, block walls and other obstructions in the parkway shall be compatible with the stopping sight distance requirements per City Standard Drawing Number 1309.
7. The applicant/developer shall be responsible to design and construct in-fill public street lights along Sixth Street property frontage. Street lighting shall be LED-type and in accordance with City's Traffic and Transportation Design Guidelines.
8. The applicant/developer shall remove all hardscape (concrete, rocks, etc.) from the within the parkway landscape area along the entire property frontage per Landscape Planning Division Requirements.

#### G. DRAINAGE / HYDROLOGY

- |                                     |      |  |                          |
|-------------------------------------|------|--|--------------------------|
| <input checked="" type="checkbox"/> | 2.38 | A storm drain main is not available to accept flows from this project.<br>(Ref: Storm Drain Drawing Number: <u>None</u> )  | <input type="checkbox"/> |
| <input checked="" type="checkbox"/> | 2.39 | Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study. | <input type="checkbox"/> |
| <input type="checkbox"/>            | 2.40 | An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100-year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.  | <input type="checkbox"/> |
| <input type="checkbox"/>            | 2.41 | Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.   | <input type="checkbox"/> |
| <input type="checkbox"/>            | 2.42 | Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.   | <input type="checkbox"/> |
| <input type="checkbox"/>            | 2.43 | Other conditions: _____  | <input type="checkbox"/> |

#### H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- |                                     |      |  |                          |
|-------------------------------------|------|--|--------------------------|
| <input type="checkbox"/>            | 2.44 | 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.<br><br>If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.<br><br>Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130. | <input type="checkbox"/> |
| <input checked="" type="checkbox"/> | 2.45 | <b>Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <a href="http://www.sbcounty.gov/dpw/land/npdes.asp">http://www.sbcounty.gov/dpw/land/npdes.asp</a>.</b>  | <input type="checkbox"/> |
| <input type="checkbox"/>            | 2.46 | Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector   | <input type="checkbox"/> |



screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.

2.47 Other conditions: \_\_\_\_\_

**J. SPECIAL DISTRICTS**

2.48 File an application, together with an initial payment deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community facilities District Act of 1982. The application and fee shall be submitted a minimum four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact the Management Services Department at (909) 395-2353 to initiate the CFD application process.

2.49 Other conditions: \_\_\_\_\_

**K. FIBER OPTIC**

2.50 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand-hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Generally located along Sixth Street property frontage, see Fiber Optic Exhibit herein for reference.

2.51 Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Information Technology Department at (909) 395-2000, regarding this requirement.

**L. Solid Waste**

2.52 Onsite solid waste shall be designed in accordance with the City's Solid Waste Manual location at:

<http://www.ontarioca.gov/municipal-utilities-company/solid-waste>

2.53 Other conditions:

1. The applicant/developer shall submit a Solid Waste Handling Plan (SWHP) and incorporate the following requirements:
  - a. For designated storage and collection areas not accessible by the standard overhead loading solid waste vehicles (such as areas within parking structures), scouting services utilizing scouting vehicles may be utilized. To utilize scouting services, the following items must be addressed:
    - i. Show scouting vehicle path of travel to assure path of scouting vehicle travel meets minimum access requirement.
    - ii. Show turning radius. inside: # feet. outside: # feet and vertical clearance (floor to overhead obstructions): # feet (including through surface grade breaks).
    - iii. Show typical scaled cross sections of the vertical path of travel through entrances, surface grade breaks, and anywhere the total vertical clearance may change.
  - b. Each staging area must be accessible to the overhead loading solid waste vehicles meeting those vehicle access standards.
  - c. The total of all the staging areas must be sufficiently sized to temporarily locate all the bins on-site for collections and cannot compete/conflict with parking or traffic.

For questions on the SWHP submittal requirements, please contact:

Peter Tran, Associate Engineer  
[ptran@ontarioca.gov](mailto:ptran@ontarioca.gov)  
 Phone: (909) 395-2677



**3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:**

- 3.01 **Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.**
- 3.02 Complete all requirements for recycled water usage. 
  - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
  - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
  - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 **The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.**
- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 **Confirm payment of all Development Impact Fees (DIF) to the Building Department.**
- 3.06 **Submit electronic copies (PDF & Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).**

**4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:**

- 4.01 **Complete all Conditions of Approval listed under Sections 1-3 above.**
- 4.02 **Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.**
- 4.03 **The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.**



## **EXHIBIT 'A'**

### **ENGINEERING DEPARTMENT First Plan Check Submittal Checklist**

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**Project Number: PDEV19-043 and Tract Map No. 20291**

**The following items are required to be included with the first plan check submittal:**

1.  **A copy of this check list**
2.  **Payment of fee for Plan Checking**
3.  **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4.  **One (1) copy of project Conditions of Approval**
5.  **Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).**
6.  Three (3) sets of Public Street improvement plan with street cross-sections
7.  Three (3) sets of Private Street improvement plan with street cross-sections
8.  Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)
9.  Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10.  Four (4) sets of Public Sewer improvement plan
11.  Five (5) sets of Public Storm Drain improvement plan
12.  **Three (3) sets of Public Street Light improvement plan**
13.  Three (3) sets of Signing and Striping improvement plan
14.  **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15.  **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
16.  Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
17.  **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
18.  **One (1) copy of Hydrology/Drainage study**
19.  **One (1) copy of Soils/Geology report**
20.  **Payment for Final Map/Parcel Map processing fee**
21.  **Three (3) copies of Final Map/Parcel Map**



- 22.  One (1) copy of approved Tentative Map
- 23.  One (1) copy of Preliminary Title Report (current within 30 days)
- 24.  One (1) copy of Traverse Closure Calculations
- 25.  One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.
- 26.  Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 27.  Other: \_\_\_\_\_

**OWNER/APPLICANT:**  
 10257 ACADIA STREET  
 BREDA, CA 92814  
 PHONE: (626) 705-2110

**ARCHITECT:**  
 Z. SUN & ASSOCIATES, INC.  
 2000 W. 17TH STREET  
 BREDA, CA 92810  
 PHONE: (626) 705-2210

**BENCHMARK:**  
 20. BRASS MARK STAMPED "M&O OF SO. CAL."  
 4" NORTH OF INTERSECTION OF 6TH STREET  
 AND WEST OF INTERSECTION OF 1023  
 AVENUE AND SIXTH STREET.  
 ELEVATION: 118.83'

**LEGAL DESCRIPTION:**  
 A PORTION OF LOT 172, OAKHARD COLONY LOTS, IN THE CITY OF OAKHARD, COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, AS PER PLAT RECORDED IN BOOK 11, PAGE 574-575, IN THE COUNTY RECORDS OFFICE OF SAID COUNTY.  
 APN: 1047-172-17 AND -22

**ADD 100' TO ALL DIMENSIONS TO OBTAIN CORRECT ELEVATION.**

**UTILITIES DEPARTMENT:**  
 1. GAS LINE  
 2. WATER MAIN  
 3. SANITARY SEWER  
 4. POWER POLE  
 5. TELEPHONE  
 6. CABLE

**ON DENSITY RESIDENTIAL:**  
 1. CONCRETE  
 2. GROUNDWORK  
 3. FOUNDATION  
 4. FLOORING  
 5. ROOFING  
 6. EXTERIOR FINISH  
 7. INTERIOR FINISH  
 8. MECHANICAL  
 9. ELECTRICAL  
 10. PLUMBING  
 11. PAINTING  
 12. LANDSCAPE

**REVISIONS:**  
 1. 10/24/20  
 2. 11/24/20  
 3. 12/24/20

**NOTES:**  
 1. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.  
 2. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.  
 3. ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE.

**REVISIONS:**  
 1. 10/24/20  
 2. 11/24/20  
 3. 12/24/20

**REVISIONS:**  
 1. 10/24/20  
 2. 11/24/20  
 3. 12/24/20

**REVISIONS:**  
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**REVISIONS:**  
 1. 10/24/20  
 2. 11/24/20  
 3. 12/24/20

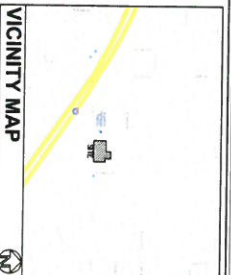
**REVISIONS:**  
 1. 10/24/20  
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 3. 12/24/20

**REVISIONS:**  
 1. 10/24/20  
 2. 11/24/20  
 3. 12/24/20

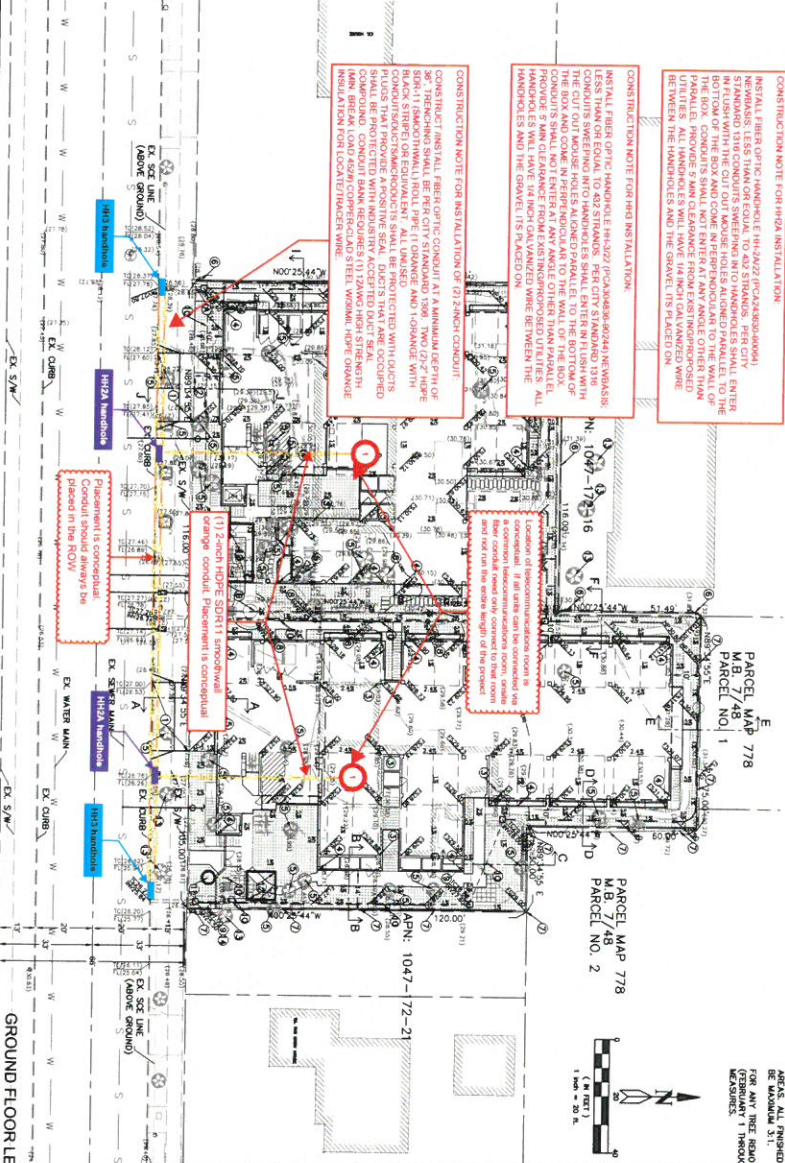
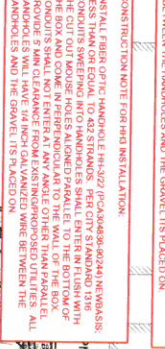
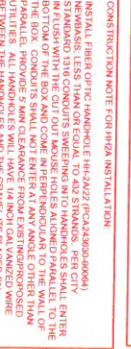
**CONCEPTUAL GRADING PLAN**

TM-20291

1005 & 1023 EAST 6TH STREET  
 COURTESY DESIGN PLAN EXHIBIT  
 05/07/2020 - AMVACA



**ADDITIONAL NOTES:**  
 1. THE DESIGNER HAS CONDUCTED VISUAL ANALYSIS OF THE PROPOSED PROJECT AND HAS DETERMINED THAT THE PROJECT IS VISUALLY SCREENED BY THE EXISTING BUILDINGS AND TREES. VISUAL ANALYSIS WAS CONDUCTED FROM THE NEAREST NEIGHBORHOOD AND FROM THE PUBLIC RIGHT-OF-WAY. VISUAL ANALYSIS WAS CONDUCTED FROM THE NEAREST NEIGHBORHOOD AND FROM THE PUBLIC RIGHT-OF-WAY. VISUAL ANALYSIS WAS CONDUCTED FROM THE NEAREST NEIGHBORHOOD AND FROM THE PUBLIC RIGHT-OF-WAY.



**REGISTERED PROFESSIONAL ENGINEER**  
 STATE OF CALIFORNIA  
 CIVIL ENGINEERING  
 No. 1047-172-21  
 DATE: 03-04-2020  
 200 N. LA BREA AVE., SUITE 1001  
 LOS ANGELES, CA 90012  
 TEL: 213-688-0001  
 FAX: 213-688-0002

**C-1**  
 SHEET 1 OF 2 SHEETS

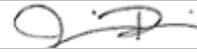
**PROJECT LOCATION:**  
 1005 & 1023 E. 6TH STREET,  
 ONTARIO, CA 91764

**CALLAND ENGINEERING, INC.**  
 dba QUARTECH CONSULTANTS  
 576 E. LAMBERT ROAD, BREDA, CA 92821  
 TEL: (714) 671-1050 FAX: (714) 671-1090

**CITY OF ONTARIO**  
**LANDSCAPE PLANNING DIVISION**  
 303 East "B" Street, Ontario, CA 91764

**DAB CONDITIONS OF APPROVAL**

Sign Off



Jamie Richardson, Sr. Landscape Planner

5/7/20

Date

Reviewer's Name: <b>Jamie Richardson, Sr. Landscape Planner</b>	Phone: <b>(909) 395-2615</b>
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D.A.B. File No.: PDEV19-043	Case Planner: Jeanie Aguilo
--------------------------------	--------------------------------

Project Name and Location:  
 18 Unit Multi Family – Condo Tentative Tract Map  
 TM20291

Applicant/Representative:  
 TZYH DER SUN (Cliff Sun)  
 9431 Longden Ave.  
 Temple City, CA 91780

<input checked="" type="checkbox"/>	<b>A Preliminary Landscape Plan (dated 5/6/2020) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.</b>
<input type="checkbox"/>	<b>A Preliminary Landscape Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.</b>

**A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE.**  
 Landscape construction plans with plan check number may be emailed to: [landscapeplancheck@ontarioca.gov](mailto:landscapeplancheck@ontarioca.gov)  
**DIGITAL SUBMITTALS MUST BE 10MB OR LESS.**

**Civil/ Site Plans**

1. Show on demo plans and landscape construction plans trees to be preserved, removed or mitigation measures for trees removed. Tree inventory identifies 129 inches of trunk diameter of heritage trees that will be removed due to construction. Provide mitigation measures as defined below:
  - a. New 15 gallon trees min 1" diameter trunk, in addition to trees required (129-15 gallon)
  - b. New 24" box trees min 1.5" diameter trunk, in addition to trees required. (86-24" box)
  - c. Upsizing trees on the plan one size larger such as 15 gallon to 24" box, or 24" to 36" box size. (to equal trunk replacement)
  - d. Monetary value of the trees removed as identified in the "Guide for Plant Appraisal", approved certified arborist plant appraiser, or may be equal to the value of the installation cost of planting, fertilizing, staking and irrigating 15 gallon trees, (100\$ each) to the City of Ontario Historic Preservation Fund for city tree planting or city approved combination of the above items. (\$12,900)
2. The arborist report identifies various healthy fruit trees that can be relocated to provide a viable resource to the community (see arborist report tree #4, 9, 10, 14, 15, 22, 23, 24, 33, 37, 38, 39, 40, 42 & 44). Contact Arthur Levine (917) 734-8108 at Huerta Del Valle – Ontario Community Garden to coordinate the possible relocation of fruit trees identified to be healthy and vigorous with a rating of B- and above.
3. Coordinate with the City of Ontario Parks and Maintenance Department – Adrian Escamilla, Parks and Maintenance Manager (909) 395-2635 to relocate existing Heritage Tree #20 Phoenix canariensis.
4. Show transformers set back 5' from paving all sides. Coordinate with landscape plans.
5. Show backflow devices set back 4' from paving all sides. Locate on level grade

**Landscape Plans**

- 6. L-1 Existing Tree Matrix: Change “Relocate by City” to “Relocate by Huerte Del Valle.”
- 7. Show backflow devices with 36” high strappy leaf shrub screening and trash enclosures and transformers, a 4’-5’ high evergreen hedge screening. Do not encircle utility, show as masses and duplicate masses in other locations on regular intervals.
- 8. L-2 Landscape Hydrozones; remove “synthetic turf”.
- 9. Show 8’ diameter of mulch at all new tree locations, 12’ min. at existing trees. Detail irrigation dripline outside of mulched root zone.
- 10. Landscape construction plans shall meet the requirements of the Landscape Development Guidelines. See <http://www.ontarioca.gov/landscape-planning/standards>
- 11. After a project’s entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council. Fees are:
  - Plan Check—less than 5 acres.....\$1,301.00
  - Inspection—Construction (up to 3 inspections per phase).....\$278.00
  - Total.....\$1,579.00
  - Inspection—Field – any additional.....\$83.00

Landscape construction plans with building permit number for plan check may be emailed to: [landscapeplancheck@ontarioca.gov](mailto:landscapeplancheck@ontarioca.gov)



# AIRPORT LAND USE COMPATIBILITY PLANNING

## CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV19-043 & PMTT19-011

Address: 1005 & 1023 East 6th Street

APN: 1047-172-17 & 22

Existing Land Use: Single Family Residential

Proposed Land Use: Development Plan 18 unit multi-family townhome complex & Tentative Tract Map for Condominium purposes

Site Acreage: 0.73 Proposed Structure Height: 36 ft

ONT-IAC Project Review: N/A

Airport Influence Area: ONT

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Jeanie Aguilo

Date: 11/6/19

CD No.: 2019-053

PALU No.: n/a

### The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input checked="" type="checkbox"/> High Terrain Zone	<input type="radio"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input type="radio"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input checked="" type="checkbox"/> Real Estate Transaction Disclosure
<input type="radio"/> Zone 3	<input type="checkbox"/> 60 - 65 dB CNEL	<input type="radio"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: <u>70 FT</u>	
<input type="radio"/> Zone 5			

### The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1   
  Zone 2   
  Zone 3   
  Zone 4   
  Zone 5   
  Zone 6

Allowable Height: \_\_\_\_\_

## CONSISTENCY DETERMINATION

This proposed Project is:  Exempt from the ALUCP     Consistent     Consistent with Conditions     Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Real Estate Transaction Disclosure Required

Airport Planner Signature: \_\_\_\_\_



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Jeanie Irene Aguilo, Associate Planner  
Planning Department

**FROM:** Paul Ehrman, Deputy Fire Chief/Fire Marshal  
Fire Department

**DATE:** August 7, 2019

**SUBJECT:** PDEV19-043 – A Development Plan to construct 18 multiple-family dwellings on 0.76 acres of land located at 1005 and 1023 East Sixth Street, within the HDR-45 (High Density Residential - 25.1 to 45.0 DU/Acre) zoning district. (APNs: 1047-172-17 and 1047-172-22).

- 
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.

---

### **SITE AND BUILDING FEATURES:**

- A. 2016 CBC Type of Construction: Type V
- B. Type of Roof Materials: Ordinary
- C. Ground Floor Area(s): 19,380 Sq. Ft
- D. Number of Stories: 3
- E. Total Square Footage: 46,552 Sq. Ft.
- F. 2016 CBC Occupancy Classification(s): S2, R2

## **CONDITIONS OF APPROVAL:**

### **1.0 GENERAL**

- ☒ 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at [www.ontarioca.gov](http://www.ontarioca.gov), click on “Fire Department” and then on “Standards and Forms.”
- ☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

### **2.0 FIRE DEPARTMENT ACCESS**

- ☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- ☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- ☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- ☒ 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- ☒ 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- ☒ 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- ☒ 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services.

### 3.0 WATER SUPPLY

- ☒ 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 2675 gallons per minute (g.p.m.) for 4 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- ☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- ☒ 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

### 4.0 FIRE PROTECTION SYSTEMS

- ☒ 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- ☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard ~~Choose an item.~~ All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.4 Wood frame buildings that are to be sprinkled shall have these systems in service (but not necessarily finalized) before the building is enclosed.
- ☒ 4.5 Fire Department Connections (FDC) shall be located on the address side of the building within one hundred fifty feet (150') of a public fire hydrant on the same side of the street. Provide identification for all fire sprinkler control valves and fire department connections per Standard #D-007. Raised curbs adjacent to Fire Department connection(s) shall be painted red, five feet either side, per City standards.
- ☒ 4.6 A fire alarm system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 72. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.7 Portable fire extinguishers are required to be installed prior to occupancy per Standard #C-001. Please contact the Fire Prevention Bureau to determine the exact number, type and placement required.

- ☒ 4.8 A fixed fire extinguishing system is required for the protection of hood, duct, plenum and cooking surfaces. This system must comply with National Fire Protection Association (NFPA) Standards 17A and 96. An application with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

## **5.0 BUILDING CONSTRUCTION FEATURES**

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.
- ☒ 5.4 Multiple unit building complexes shall have building directories provided at the main entrances. The directories shall be designed to the requirements of the Fire Department, see Section 9-1 6.06 of the Ontario Municipal Code and Standard #H-003. .
- ☒ 5.5 All residential chimneys shall be equipped with an approved spark arrester meeting the requirements of the California Building Code.
- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.

## **7.0 PROJECT SPECIFIC CONDITIONS**

- ☒ 7.1 Due to the proposed depth of construction off of 6<sup>th</sup> Street, a public fire hydrant shall be required within 100 ft. from the main entrance to the project



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Jeanie Irene Aguilo, Associate Planner

**FROM:** Officer Emily Hernandez, Police Department

**DATE:** August 7, 2019

**SUBJECT:** PDEV19-043: A DEVELOPMENT PLAN TO A CONSTRUCT 18-UNIT MULTI-FAMILY TOWNHOME COMPLEX LOCATED AT 1005 AND 1023 EAST SIXTH STREET.

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The “Standard Conditions of Approval” contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including but not limited to, the requirements listed below.

- Required lighting for all walkways, driveways, doorways, parking areas, and other areas used by the public shall be provided and operate on photosensor at the prescribed foot-candle levels. Photometrics shall be provided to the Police Department. Photometrics shall include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- The Applicant shall comply with all construction site security requirements as stated in the Standard Conditions. This includes the provisions for perimeter lighting, site lighting, fencing and/or uniformed security.
- First floor common stairwells shall be constructed so as to either allow for visibility through the stairwell risers or to prohibit public access to the areas behind stairwells.
- Rooftop addresses shall be installed on the buildings as stated in the Standard Conditions. The numbers shall be at a minimum 3 feet tall and 1 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street.

The Applicant is invited to call Emily Hernandez at (909) 408-1755 with any questions regarding these conditions.

# CITY OF ONTARIO

## MEMORANDUM

**TO:** PLANNING DEPARTMENT, Jeanie Irene Aguilo  
**FROM:** BUILDING DEPARTMENT, Kevin Shear  
**DATE:** July 24, 2019  
**SUBJECT:** PDEV19-043

- 
- The plan **does** adequately address the departmental concerns at this time.
- No comments
- Report below.

---

### Conditions of Approval

1. The Site addresses for this project will be  
1007 E 6<sup>th</sup> St Units 1-8  
1009 E 6<sup>th</sup> St Units 9-18
2. Standard conditions of approval apply.

KS:lr



# PLANNING COMMISSION STAFF REPORT

May 26, 2020

**FILE NO.:** PSPA19-007

**SUBJECT:** An Amendment to the Parkside Specific Plan (File No. PSPA19-007) to: [1] Reconfigure residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 21; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). The project site is bounded by Ontario Ranch Road to the north, Eucalyptus Avenue to south, Archibald Avenue to the east and Carpenter Avenue to the west. (APN's): 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, and 0218-221-10); **submitted by SC Ontario Development Company, LLC. City Council action is required.**

**PROPERTY OWNER:** SC Ontario Development Company, LLC

**RECOMMENDED ACTION:** That the Planning Commission consider and recommend City Council adoption of an Addendum to the Parkside Specific Plan Environmental Impact Report and approval of File No. PSPA19-007, pursuant to the facts and reasons contained in the staff report and attached resolutions.

**PROJECT SETTING:** The project site, the Parkside Specific Plan area, is comprised of approximately 250.89 gross acres. San Bernardino County Flood Control District owns the right-of-way for the Cucamonga Creek Channel that comprises approximately 13.00 gross acres, and effectively divides the eastern half of the project site from the western



**Figure 1: Project Location**

Case Planner:	Rudy Zeledon
Planning Director Approval:	
Submittal Date:	September 4, 2019

Hearing Body	Date	Decision	Action
DAB	N/A	N/A	N/A
PC	05/26/2020		Recommend
CC			Final



half, crossing the site from north to south approximately 1,650 feet west of Archibald Ave. The project site is bounded by Ontario Ranch Road to the north, Eucalyptus Avenue to south, Archibald Avenue to the east and Carpenter Avenue to the west and is depicted in Figure 1: Project Location, above. The Parkside Specific Plan area is generally undeveloped and general agricultural operations are scattered throughout the area of the project site. The approximate 250.89-gross acre project site has historically been used for general crop-related agricultural purposes (alfalfa, corn, milo, sorghum, etc.) and flood control.

## **PROJECT ANALYSIS:**

[1] Background — The Parkside Specific Plan and the Environmental Impact Report (EIR) were approved by the City Council in September 2006. The Parkside Specific Plan established the land use designations, development standards, design guidelines and development capacity of 1,947 residential units and 115,000 square feet of commercial uses for the Specific Plan area. The Specific Plan is comprised of twenty-six (26) land use districts, incorporating nineteen (19) distinctive neighborhoods and offering a variety of residential products. On July 25, 2006, the Planning Commission approved Tentative Tract Map 18048 (“A” Map) to facilitate the backbone infrastructure improvements (major streets, sewer, water and storm drain facilities) and the creation of a parcel lot for the future City Fire Station 9, recreational facility, parks and residential neighborhoods within the eastern portion of the Specific Plan area.

On September 4, 2019, the Applicant submitted an Amendment to the Parkside Specific Plan (File No. PSPA19-007) to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 21; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7).

In addition to the Amendment to the Parkside Specific Plan, the Applicant has submitted Tentative Tract Map (File No. PMTT19-020/TT20316) for condominium purposes to subdivide 56.99 acres of land into four (4) numbered residential lots and twelve (12) lettered lot for common areas, located at the northwest corner of Archibald Avenue and Eucalyptus Avenue, within Planning areas 1 thru 4 (Conventionally-Loaded Lane Homes/Detached Green Court Cluster Homes), 17 and 18 (Townhome), 21 (Great Park), 22 and 23 (Private Parks) of the Parkside Specific Plan. The 4 residential lots will provide for (Lots 1-4) for detached single-family conventional lane loaded and detached cluster condominium homes. Subsequently, the Tentative Tract Map will facilitate a Development Plan (File No. PDEV19-064), submitted by the New Home Company, to construct 540 conventional land loaded single family homes.

[2] Amendment to the Parkside Specific Plan — The Amendment to the Parkside Specific Plan (SPA) proposes the following:

*a) Reconfigure residential Planning Areas 1 through 4, and 17 through 19 and the Great Park Planning Area 22 (east of the Cucamonga Creek Channel).*

The land use plan for the Parkside Specific Plan offers a strong neighborhood character and identity for residents and visitors through a traditional approach to street design, architecture, and landscape design. To activate the streetscene and promote friendly interaction between neighbors, the Amendment to the Parkside Specific Plan proposes to reconfigure and expand Planning Areas 1 through 4 as shown in Exhibit B: Proposed Land Use Plan. The reconfiguration and expansion of the Planning Areas 1 through 4 will allow for a more effective traditional residential single-family grid street design that enhances the pedestrian mobility, streetscene, and connectivity to the future Great Park.

Attached Exhibit A illustrates the existing approved Parkside Specific Plan Land Use Plan. The reconfiguration and expansion of the Planning Areas 1 through 4, will also shift the Great Park (Planning Area 22) north eliminating Planning Area 19 and reconfiguring Planning Areas 17 and 18. Shifting the Great Park, north will create opportunities for an integrated mixed-use concept (village) with the Great Park, surrounding residential areas (Planning Areas 17 and 18) and the future commercial center (Planning Area 21), located at the southwest corner of Ontario Ranch Road and Archibald Avenue. This integration will also be accomplished through pedestrian walkways, bike network and trail linkages from the Great Park to the residential neighborhoods and future commercial center.

The reconfiguring of residential Planning Areas 1 through 4, 17 through 19 and Planning Area 22 (Great Park), will not increase or decrease the total amount of residential units (1,947) or commercial square feet (115,000 SF) approved for the Specific Plan. Subsequently, the shifting of the Great Park north, does increase or decrease the park acreage (58.99 gross acres) approved by the Specific Plan and is consistent with the assumed TOP Policy Plan Land Use Plan park acreage for the Specific Plan area.

In addition to the reconfiguration of the Planning Areas, as discussed above, the Specific Plan Land Use Plan has been simplified, recolored and the Planning Areas have been renumbered (See **Exhibit B: Proposed Land Use Plan**) and the product type descriptions on the Land Use Plan have been eliminated and codified in the Specific Plan Land Use Summary Table (See **Exhibit C: Table 1 – Parkside Land Use Summary Table**). The Land Use Plan also includes the Chino Basin Desalter Authority's well site (Planning Area 25) within the Great Park.

*b) Revise internal circulation to improve access into the neighborhood commercial Planning Area 21.*

To provide for a more efficient movement of vehicular traffic throughout the Specific Plan, as well as a pleasant environment for pedestrian and bicycle circulation, the internal circulation network within the Specific Plan has been revised (See **Exhibit D: Proposed Circulation Master Plan**). The revised internal circulation network eliminates the loop

and u-shaped street network to a simplified street grid pattern design, which helps enforce and ensure traditional neighborhood design. To improve access to the neighborhood commercial center, a new street is proposed along the southwestern boundary of the commercial center. The street will provide access to the future commercial center from the residential Planning Areas 17 and 18 and to Archibald Avenue from Grand Park Drive.

*c) Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles.*

The Parkside Specific Plan provides for a mix predominately multi-family products with some single-family detached green cluster products (6 and 8 pack). The Amendment proposes to introduce a new Conventionally Loaded Lane single family home product, and a new Detached Green Court Cluster (Up to 8 -10 Plex) single family home product. The characteristics of the two products are as follows:

- (1) **Conventionally-Loaded Lanes Homes:** The Conventionally-Loaded Lane small lot single family homes are characterized by a private lane (Alley) that provides garage and primary entry access into each unit, with the exception of street facing units that have primary entry access from a street (**See Exhibit E: Conventionally-Loaded Lanes Homes**). The products feature private yards and patio areas. Parking is provided in garages and driveways.
- (2) **Detached Green Court Cluster (Up to 8-10 Plex):** The Specific Plan currently allows for a Detached Green Court (Primary entry from street or paseo) and Motor Court (Primary entry from street or motor court alley) Cluster (6 and 8 Plex) single family homes products. The new Detached Green Court Cluster product will allow for a cluster of up to 8 to 10 single family homes, with smaller and more efficient home footprints. The single-family homes are clustered around a green paseo, with primary entry access from a paseo or street (**See Exhibit F: Detached Green Court Cluster (Up to 8-10 Plex)**). Parking for residents are provided in garages or uncovered parking spaces. The products feature private side yards and patio areas.

The introduction of the new home product types provides for additional single-family detached homes within the Specific Plan, that are more attainable than traditional larger lot homes. While adding to the variety of housing products allowed within the Specific Plan, these new single-family detached products provide housing opportunities for every stage of life and price points to encourage the development of housing supportive of our efforts to attract business in growing sectors of the community (TOP Policy CE1-6).

In addition to the introduction of new home products types, the Amendment proposes to introduce a new Millennium series architectural styles. The Parkside Specific Plan allows for a variety of traditional American styles found throughout California and Ontario. These styles include American Traditional, Craftsman, and Mediterranean.

The Millennium Collection represents subtle differences in the architectural vocabulary using creative interpretation of traditional styles utilizing embellished statements and details to express an iconic style in a new vocabulary (**See Exhibit G: Millennium Collection Architectural Styles**). Elevations can be abstracted architectural expressions of a recognizable style. These elevations are encouraged to push the artistic envelope of design to incorporate new, modern or alternative forms, details and materials in the modern context of architecture. Styles within this series include the following:

- Adaptive American Traditional
- Adaptive Farmhouse
- Adaptive Mediterranean
- Contemporary

d) *Update and revise Landscape Standards (Section 7.7).*

The Amendment to the Parkside Specific Plan proposes to redesign the community entry monumentation and perimeter wall theme. Monumentation occurs throughout the Parkside Specific Plan community and is designed to establish a hierarchy for entering each area of the community. Along the perimeter edges there are several entry points into the community. At key entries, a landscape and monumentation program will be utilized to help identify the community as well as convey a “welcoming” feeling for both vehicular and pedestrian traffic. Five entry and monument treatments are used to set this hierarchy: Community Gateway Monuments, Primary Community Gateway Entries, Major Community Gateway Entries, Secondary Community Entries, and Secondary Community Entry Monuments. (**See Exhibit H: Community Gateway Monument Plan**).

The current Specific Plan community monumentation design calls out for a more traditional design, with decorative brick, stone or split face block pilasters (with sign inlay) and face walls. The new proposed monumentation and wall design features a more iconic and modern design. The proposed modern monumentation design incorporates (**See Exhibit I: Proposed Community Gateway Monument Concept**) the following:

- 11-foot-high stone veneer column, with a pre-cast cap and steel eyebrow trellis
- 6-foot-high monumentation wall will feature an exterior “board form” tile finish, project sign logo location, community sign with raised letters on top of a horizontal mantel.

The perimeter community split face block wall design, that transitions from the community monumentation walls, has been revised to feature a “canyon bluff” tan exterior wall color. The “canyon bluff” tan color will replace the darker “sienna brown” exterior wall color required by Specific Plan. The new “canyon bluff” tan exterior wall color will complement and provide a unified design with the new proposed monumentation design.

All changes and additions to the Specific Plan (exhibits, tables, development standards and design guidelines) are contained within the revised Specific Plan document

accompanying this report. All additions to the Specific Plan have been highlighted in blue and all deletion have been highlighted in red with a strikethrough.

**COMPLIANCE WITH THE ONTARIO PLAN:** The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)
- Ensure the Development of a Well Planned, Balanced, and Self-Sustaining Community in Ontario Ranch

[2] Vision.

**Distinctive Development:**

- Commercial and Residential Development
  - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

[3] Governance.

**Decision Making:**

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
  - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

[4] Policy Plan (General Plan)

**Land Use Element:**

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.

➤ LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.

➤ LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).

- Goal LU2: Compatibility between a wide range of uses.

➤ LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

### **Housing Element:**

- Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

➤ H2-4 New Model Colony. We support a premier lifestyle community in the New Model Colony distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.

➤ H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.

Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.

➤ H5-2 Family Housing. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation and other amenities.

### **Community Economics Element:**

- Goal CE1: A complete community that provides for all incomes and stages of life.

➤ CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every

stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

- CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

- CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

### **Community Design Element:**

- Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

- CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

- CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

- CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

- Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

- CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➤ CD2-2 Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb.

➤ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➤ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.



- Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

- CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

- Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

- CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.

- CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

**HOUSING ELEMENT COMPLIANCE:** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project is consistent with the 1,947 number of dwelling Specified in the Available Land Inventory. Per the Available Land Inventory, the Parkside Specific Plan is required to provide 1,947 dwelling units with an overall density range of between 9 and 21 DU/AC.

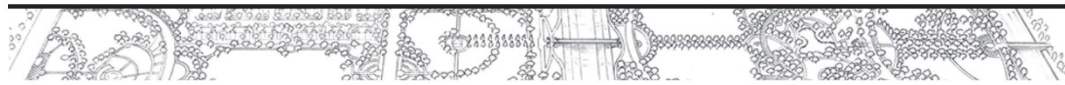
**AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE:** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the

ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

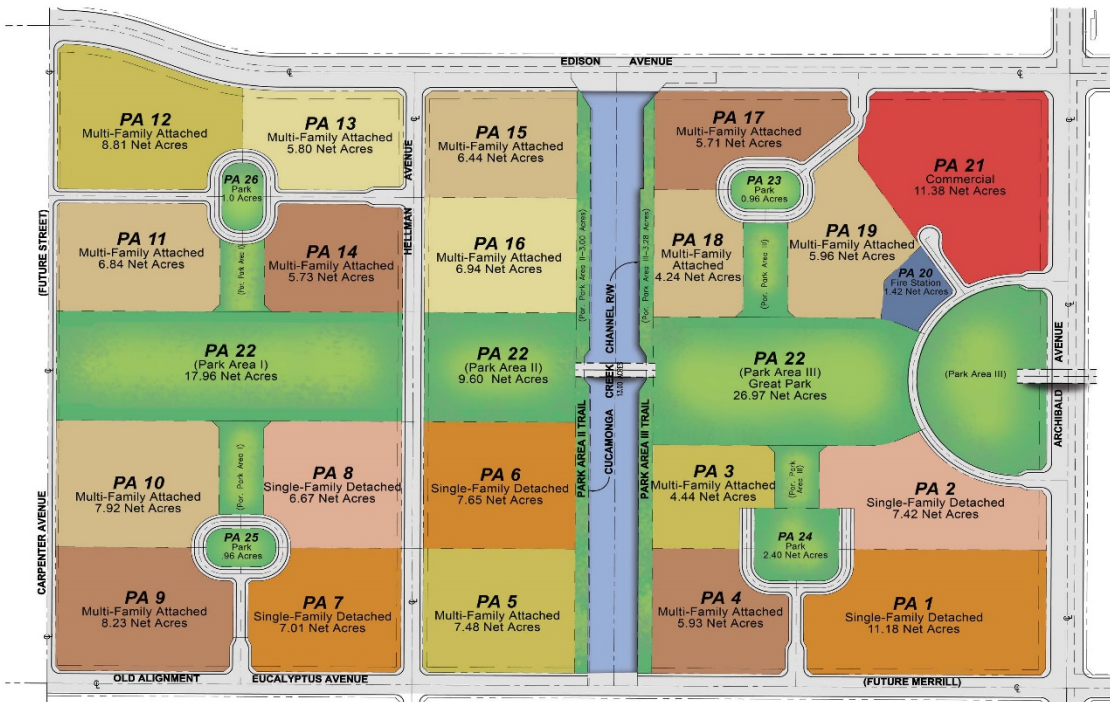
**ENVIRONMENTAL REVIEW:** The environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.

**CONDITIONS OF APPROVAL:** See attached department reports.

**Exhibit A – Existing Approved Parkside Specific Plan Land Use Plan**



Parkside



**Notes:**

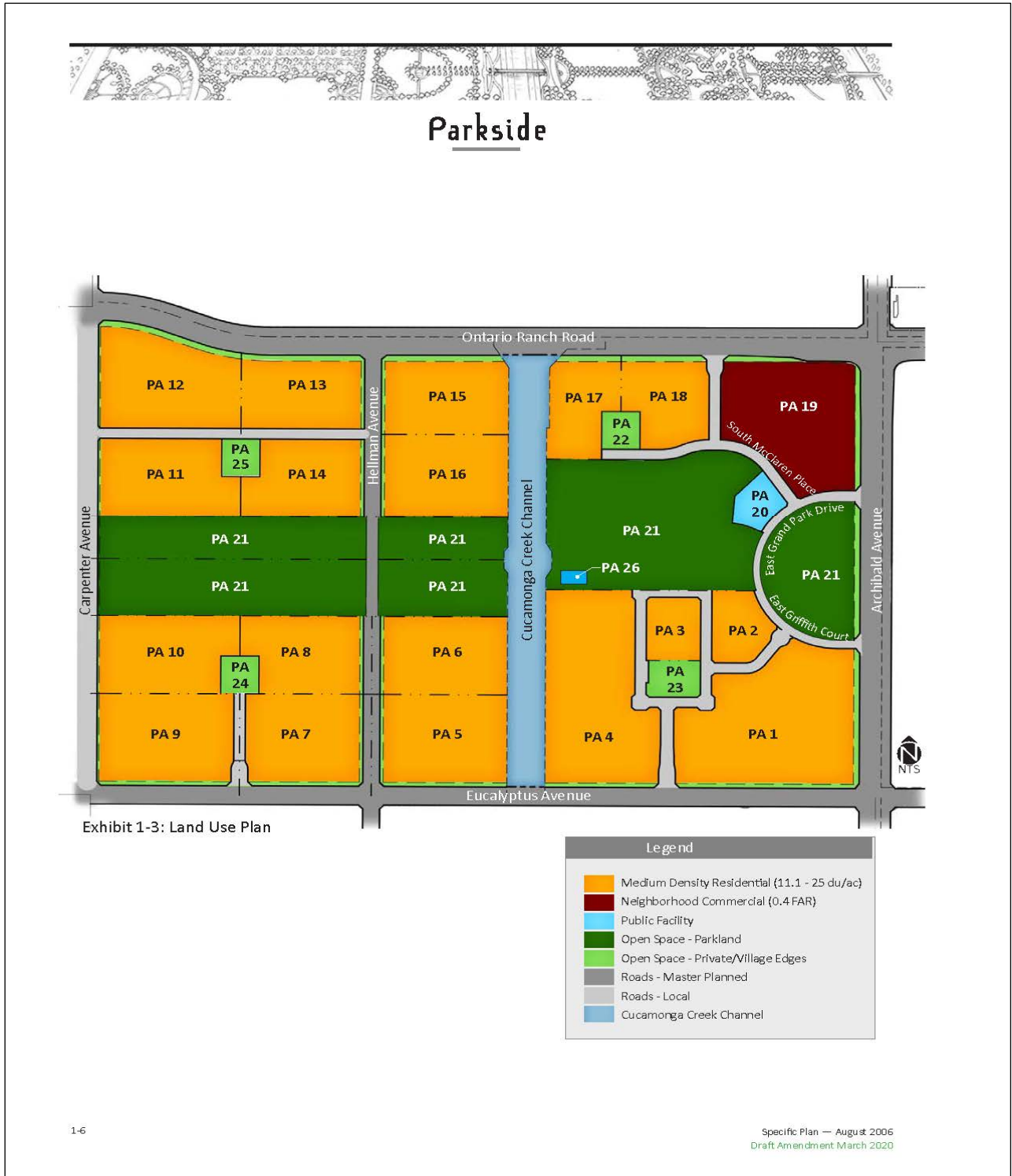
Net acres is Planning Area (PA) acreage, minus street R/W

© 2006 WILLIAM HEZMALI HALCH ARCHITECTS, INC



**Exhibit 10—Land Use Plan**

**Exhibit B – Proposed Parkside Specific Plan Land Use Plan**



**Exhibit C – Table 1 – Parkside Land Use Summary**



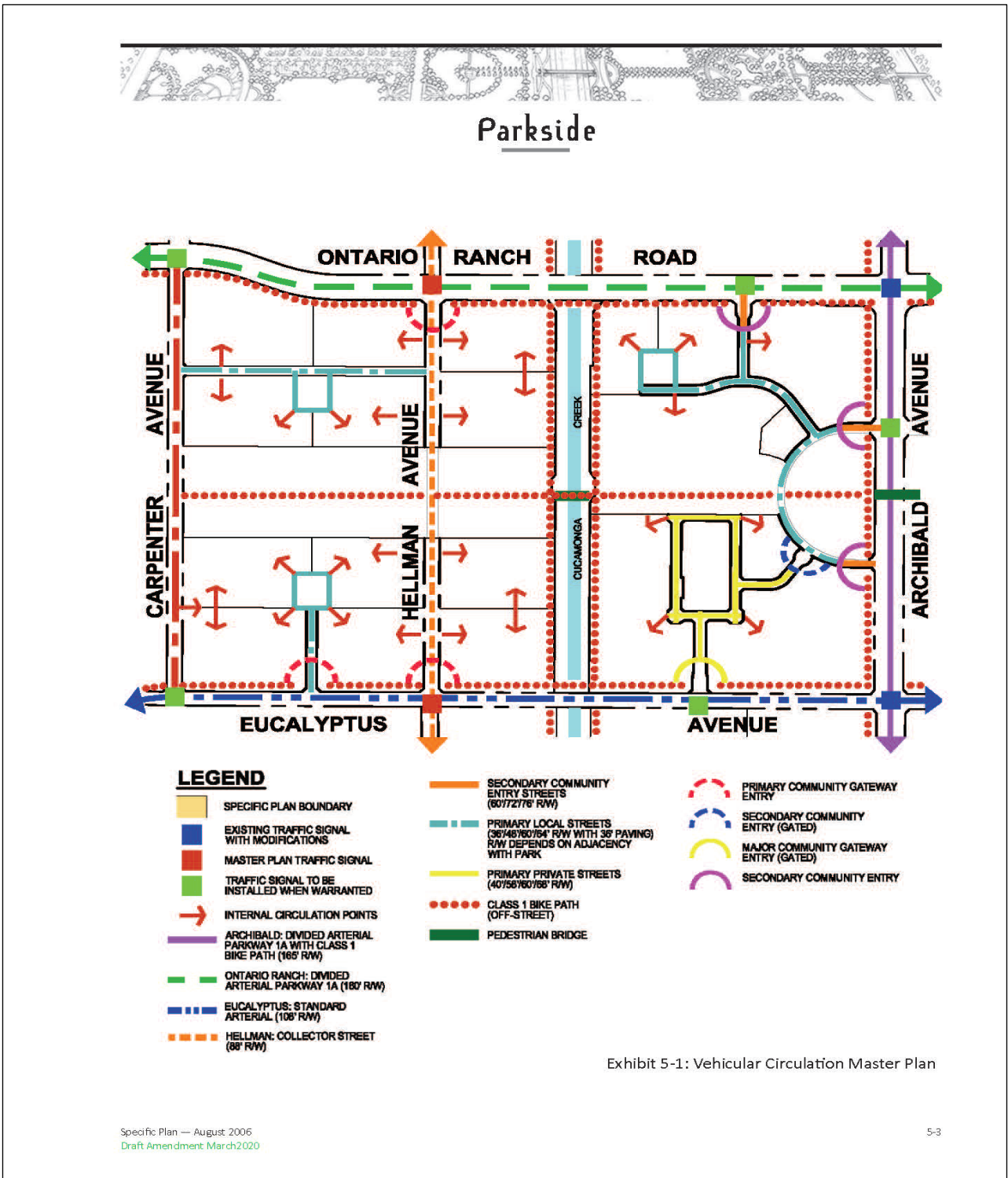
**Parkside**

Table 1-1: Parkside Land Use Summary

Land Use	Lot Size / Use	Gross Acreage	Net Acreage	*Density Range / S.F.	Lots / D.U.
<b>Residential</b>					
PA-1 (Green Court/Lane Loaded Products 1, 2 & 4)	SFD	19.97	15.57	10-18	236
PA-2 (Lane Loaded Product 3)	SFD	3.57	2.66	10-18	41
PA-3 (Lane Loaded Product 3)	SFD	3.06	2.38	10-18	42
PA-4 (Green Court/Lane Loaded Products 1, 2, 3 & 4)	SFD	15.50	13.62	10-18	221
PA-5 (HD Townhomes)	MF	9.57	8.14	15-22	139
PA-6 (Attached Auto Court)	MF	7.34	6.89	8-14	75
PA-7 (Auto Court 6-8 Plex)	SFD	9.30	7.49	8-14	68
PA-8 (Green Court 8-10 Plex)	SFD	6.64	6.18	10-16	68
PA-9 (Triplex)	MF	10.49	8.70	10-16	120
PA-10 (Row Townhomes)	MF	7.56	7.22	17-25	143
PA-11 (HD Townhomes)	MF	8.11	7.22	15-22	123
PA-12 (Triplex)	MF	11.80	9.30	12-18	127
PA-13 (Attached Auto Court)	MF	8.53	6.34	8-14	64
PA-14 (Duplex)	MF	7.12	6.18	12-18	99
PA-15 (Duplex)	MF	8.62	6.97	12-18	111
PA-16 (Triplex)	MF	7.83	7.35	10-16	90
PA-17 (Row Townhomes)	MF	4.87	4.54	17-25	82
PA-18 (HD Townhomes)	MF	6.74	5.04	15-22	98
<b>Residential Subtotal</b>		<b>156.62</b>	<b>131.79</b>	<b>14.77</b>	<b>1,947</b>
<b>Non-Residential</b>					
PA-19 (Commercial)		15.66	11.15	115,000 SF	-
PA-20 (Fire Station)		1.72	1.39	-	-
PA-21 (Great Park)		58.86	54.40	-	-
PA-22 (Private Rec)		1.00	1.00	-	-
PA-23 (Private Rec)		1.90	1.34	-	-
PA-24 (Private Rec)		1.00	1.00	-	-
PA-25 (Private Rec)		1.00	1.00	-	-
PA-26 (CD Well Site)		0.13	0.13	-	-
<b>Non-Residential Subtotal</b>		<b>81.27</b>	<b>71.41</b>	<b>115,000 SF</b>	<b>-</b>
Channel ROW		13.00	13.00	-	-
<b>Total Project</b>		<b>250.89</b>	<b>216.20</b>		<b>1,947</b>

NOTES:  
 1) Gross acres taken to center line of streets.  
 2) Adjusted gross acres taken to street ROW dedicated to the City  
 3) Net acres taken to street right of way  
 4) A total of 34.69 acres of master planned roadway is included in the project  
 \* Density range is shown & based on net acres. The General Plan is based on adjusted gross acres  
 5) Proposed product types in each Planning Area are subject to change based on final development submittal

**Exhibit D – Proposed Parkside Specific Plan Circulation Plan**



### Exhibit E – Conventionally-Loaded Lanes Homes

**Parkside**

**Conventionally Loaded Lane Homes (PA 1-4)**



**Characteristics**

- Single-family small lot detached dwellings face street or lanes (private alleys);
- Automobile access via streets or lanes (private alleys);
- Primary entries and walks face streets or lanes (private alleys);
- Private yards/patios;
- Reciprocal use easements may extend private spaces on sides of buildings;
- Parking for residents provided in garages and driveways; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

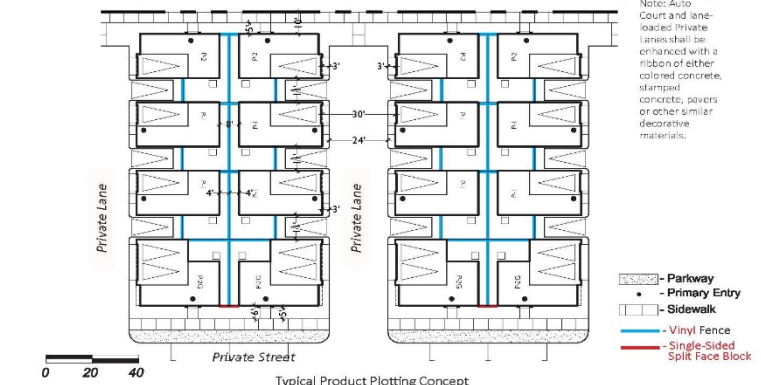
**Notes**

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
- Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.

7-52 Specific Plan — August 2006  
Draft Amendment April 2020

**Parkside**

**Conventionally Loaded Lane Homes - Product 2 (PA 1 & 4)**



**Typical Product Plotting Concept 2-Story Diagram**

Note: Auto Court and lane-loaded Private Lanes shall be enhanced with a ribbon of either colored concrete, stamped concrete, pavers or other similar decorative materials.

**Table 7-1: Conventionally Loaded Lane Homes Development Standards**

Building Separation		Garage Setbacks	
Front to side living space	8'	Garage face to garage face	30'
Side to side living space	8'	Garage face to lane (drive apron)	3'
Side to side living space (with parking space in between)	11'	Drive aprons shall be 5' or less in length or 18" in length or greater.	
Rear living to rear living	8'		
Front Setbacks (Private Street Facing) - To Back of Sidewalk		Maximum Encroachments into Building Setbacks	
Living space	5'	Second/third floor cantilever over garage	3'
Porch, balcony	3'	Fireplace, media niche (8' max. length), bay windows, furr outs, etc.	2'
Garden wall	3'	Potshelves, brackets, eaves, awnings, eyebrows, canopies, cantilever decks, etc.	Additional 36" over porch or furring setback
Front setbacks (Private Lane facing)	3'		
Side Setbacks - To Parcel Edge/Site Boundary		Private Open Space Per Unit	
Living space to property line	10'	Minimum dimension	4'
Non-living space (garage-single story)	6'	Total per unit	70 s.f.
Living space (adjacent to Cucamonga Creek Channel)	4'		
Rear Setbacks - To Parcel Edge/Site Boundary		Parking	
Living space (2-story)	10'	2 spaces per homes, at least 1 is required to be covered	
Living space (3-story)	10'		
		Building Height	
		Architectural projections may exceed the maximum building height of 42' by an additional 3'.	

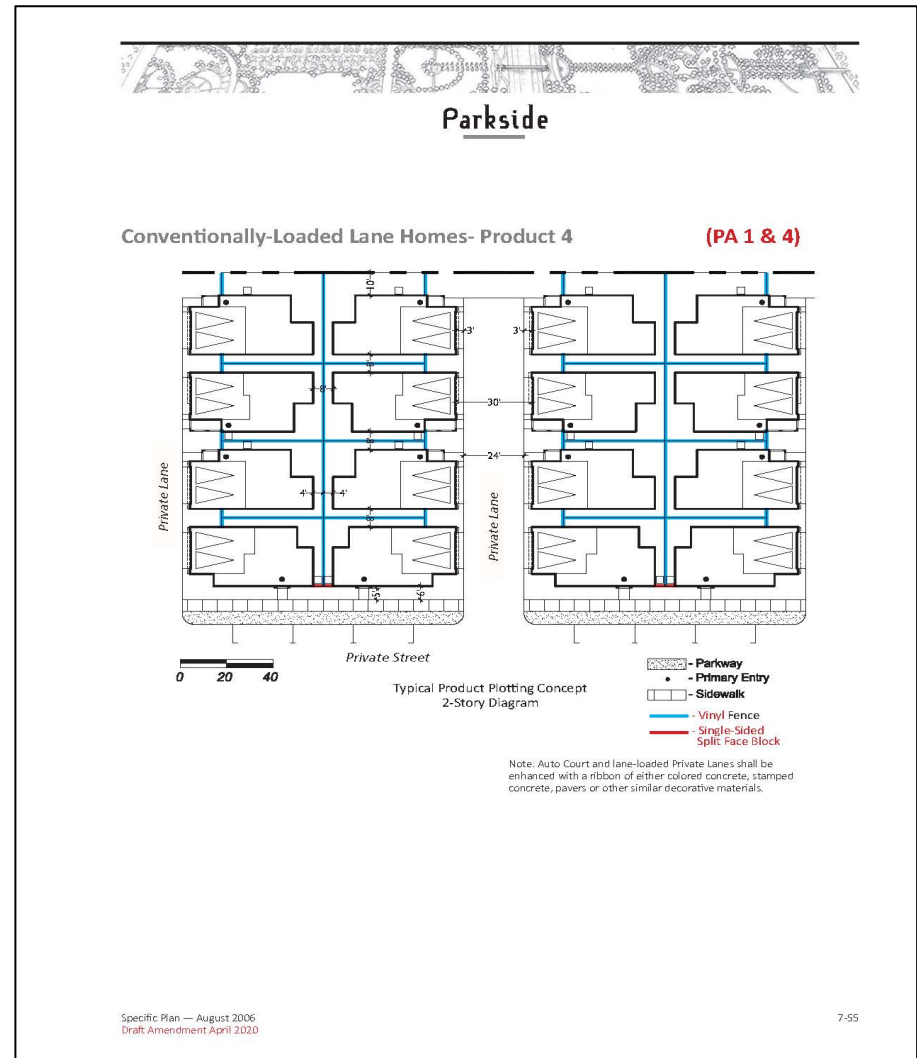
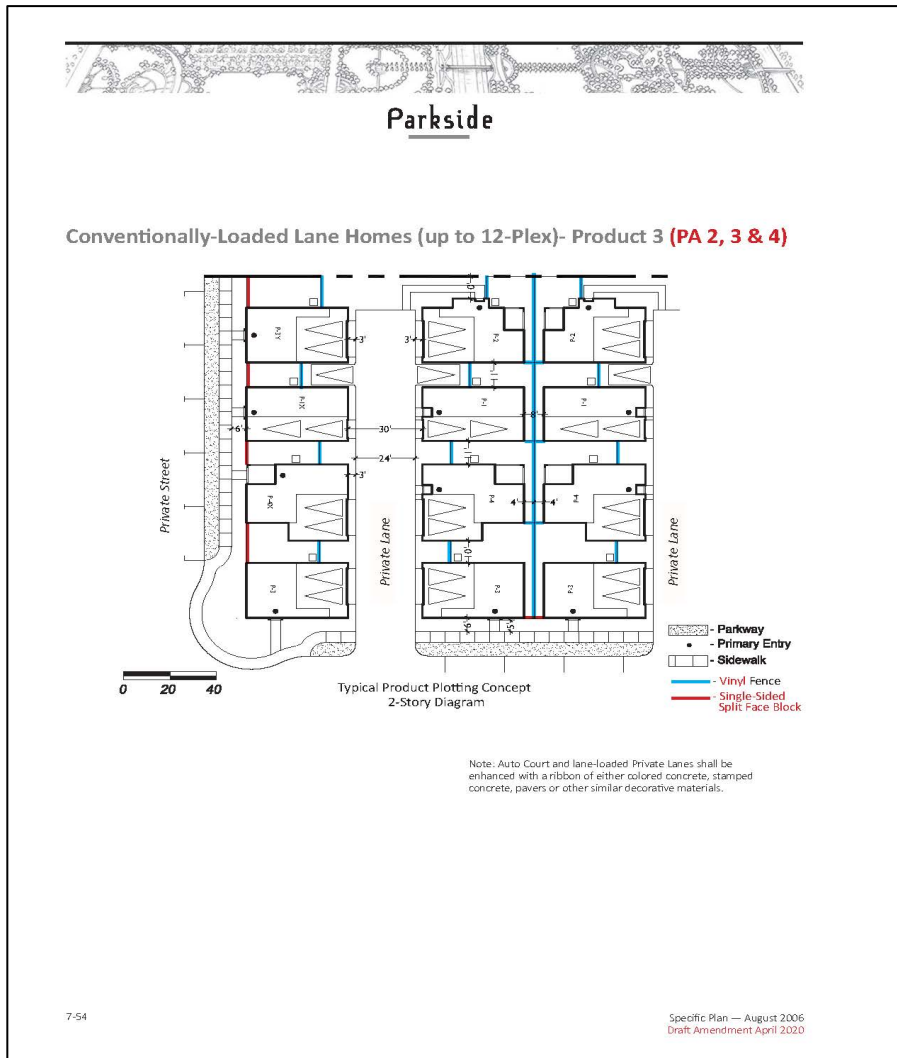
Legend:  
  Parkway  
  Primary Entry  
  Sidewalk  
  Vinyl Fence  
  Single-Sided Split Face Block

Scale: 0 20 40

Specific Plan — August 2006  
Draft Amendment April 2020

7-53

**Exhibit E – Conventionally-Loaded Lanes Homes Continued**





**Exhibit E – Conventionally-Loaded Lanes Homes Continued**

**Parkside**

**Conventionally Loaded Lane Homes (PA 1, 7 & 8)\***



**Characteristics**

- Conventionally loaded lane homes are dwellings face street or lanes (private alleys);
- Automobile access via streets or lanes (private alleys);
- Primary entries and walks face streets or lanes (private alleys);
- Private yards/patios;
- Reciprocal use easements may extend private spaces on sides of buildings;
- Parking for residents provided in garages and driveways; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

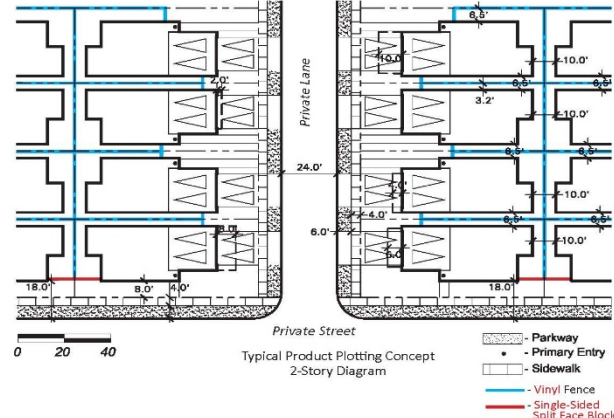
**Notes**

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
- Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.

7-60 Specific Plan — August 2006  
Draft Amendment April 2020

**Parkside**

**Conventionally Loaded Lane Home (PA 1, 7 & 8)\***  
\* Proposed product types in each Planning Area are subject to change based on final development submittal.



Note: Auto Court and lane-loaded Private Lanes shall be enhanced with a ribbon of either colored concrete, stamped concrete, pavers or other similar decorative materials.

**Table 7-3: Conventionally Loaded Lane Home Development Standards**

Building Separation		Maximum Encroachments into Building Setbacks	
Front to side living space	N/A	Second floor cantilever over garage	10'
Side to side living space	6.5'		
Rear living to rear living	10'		
Front Setbacks (Private Street Facing) - To Back of Sidewalk		Private Open Space Per Unit	
Living space	8'	Minimum dimension	10'
Porch, balcony	5'	Total per unit	100 s.f.
Garden wall	3'		
Garage Setbacks		Parking	
Garage face to lane (drive apron)	18'	Per Ontario Development Code	
Drive aprons shall be 5' or less in length or 18' in length or greater.		Building Height	
		Architectural projections may exceed the maximum building height of 35' by an additional 5'.	

Typical Product Plotting Concept 2-Story Diagram

Legend:  
 - Parking  
 - Primary Entry  
 - Sidewalk  
 - Vinyl Fence  
 - Single-Sided Split Face Block

Specific Plan — August 2006  
Draft Amendment April 2020 7-61

**Exhibit E – Conventionally-Loaded Lanes Homes Continued**

**Parkside**

**Conventionally Loaded Lane Home (PA 1, 7 & 8)\***  
 \* Proposed product types in each Planning Area are subject to change based on final development submittal.

**Characteristics**

- Conventionally Loaded Lane Homes are detached dwellings face street or lanes (private alleys);
- Automobile access via streets or lanes (private alleys);
- Primary entries and walks face streets or lanes (private alleys);
- Private yards/patios;
- Reciprocal use easements may extend private spaces on sides of buildings;
- Parking for residents provided in garages and driveways; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

**Notes**

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
- Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.

7-62 Specific Plan — August 2006  
Draft Amendment April 2020

**Parkside**

**Conventionally Loaded Lane Home (PA 1, 7 & 8)\***  
 \* Proposed product types in each Planning Area are subject to change based on final development submittal.

**Table 7-4: Conventionally Loaded Lane Home Development Standards**


Building Separation		Maximum Encroachments into Building Setbacks	
Front to side living space	6.5'	Second floor cantilever over garage	2'
Side to side living space	6.5'		
Rear living to rear living	10'		
Front Setbacks (Private Street Facing) - To Back of Sidewalk		Private Open Space Per Unit	
Living space	8'	Minimum dimension	10'
Porch, balcony	5'	Total per unit	100 s.f.
Garden wall	3'		
Garage Setbacks		Parking	
Garage face to lane (drive apron)	8'	Per Ontario Development Code	
Drive aprons shall be 5' or less in length or 18' in length or greater.		Building Height	
		Architectural projections may exceed the maximum building height of 35' by an additional 5'	

7-63 Specific Plan — August 2006  
Draft Amendment April 2020

### Exhibit F – Detached Green Court Cluster

**Parkside**

**Detached Green Court Cluster (8 to 10 Plex) (PA 1, 7 & 8)\***



**Characteristics**

- Single-family detached dwellings clustered around a Auto Court;
- Automobile access via common Auto Court driveway;
- Primary entries and walks face Auto Court or street;
- Private side and rear yards;
- Parking for residents provided in garages;
- Guest parking provided on local streets, designated parking areas, or as approved by the City; and
- Reciprocal use easements extend private space.

**Notes**

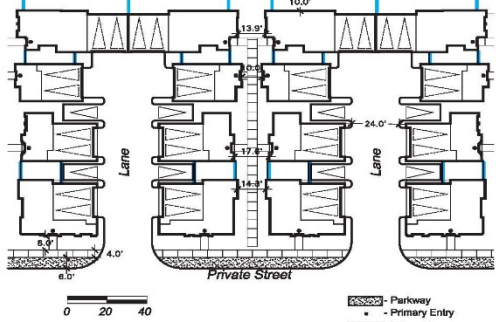
- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
- Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.

7-66 Specific Plan — August 2006  
Draft Amendment April 2020

**Parkside**

**Detached Green Court Cluster (8 to 10-Plex) (PA 1, 7 & 8)\***

\* Proposed product types in each Planning Area are subject to change based on final development submittal.



Typical Product Plotting Concept  
2-3 Story Diagram


Building Separation		Maximum Encroachments into Building Setbacks	
Front to front living space	10'	Fireplace, media niche (8' max length), bay windows (cantilevered), pots/shelves, brackets, etc.	2'
Side to side living space	8' or 2' airspace	Second floor cantilever over garage	2'
<b>Front (Private Street-Facing) Setbacks - To Back of Sidewalk</b>		<b>Private Open Space Per Unit</b>	
Living space	8'	Minimum dimension	7'
Porch, balcony	5'	Total per unit	70 s.f.
Garden wall	3'	<b>Parking</b>	
<b>Side Setbacks - To Lot Edge</b>		Per Ontario Development Code	
Living space to property line	4'	<b>Building Height</b>	
<b>Rear Setbacks - To Lot Edge</b>		Architectural projections may exceed the maximum building height of 42' by an additional 3'. * 3' min. from PL	
Living space	10'	<b>Note:</b> Green Court pedestrian entries may have decorative arbor (or other similar decorative design feature to enhance pedestrian scale and street scene.	
<b>Garage Setbacks</b>			
Garage face to garage face	30'		
Garage face to driveway (apron)	3'-5'		
Drive aprons shall be 5' or less in length or 18" in length or greater.			
Note: Lane width will increase to 24' when the length exceeds 150'			

7-67 Specific Plan — August 2006  
Draft Amendment April 2020

Exhibit F – Detached Green Court Cluster Continued

**Parkside**

**Detached Auto Court Cluster (6 to 8-Plex) (PA 1, 7 & 8)\***



**Characteristics**

- Single-family detached dwellings clustered around Auto Court;
- Automobile access via common Auto Court driveway;
- Primary entries and walks face Auto Court or street
- Private side and rear yards;
- Parking for residents provided in garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.
- Reciprocal use easements extend private space

**Notes**

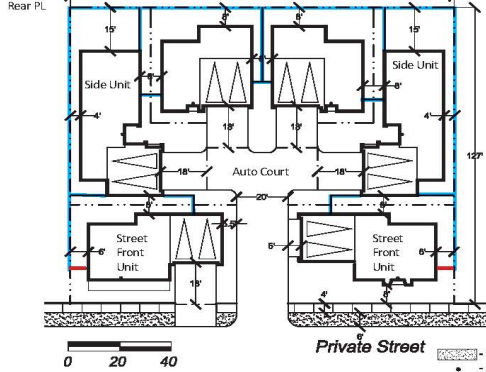
- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
- Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.

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**Parkside**

**Detached Auto Court Cluster (6 to 8-Plex) (PA 1, 7, & 8)\***

\* Proposed product types in each Planning Area are subject to change based on final development submittal.



Typical Product Plotting Concept  
2-Story Diagram

Note: Auto Court and lane-loaded Private Lanes shall be enhanced with a ribbon of either colored concrete, stamped concrete, pavers or other similar decorative materials.

**Table 7-7: Detached Auto Court Cluster (6-Plex) Development Standards**

Building Separation		Maximum Encroachments into Building Setbacks	
Front to side living space	8' (4'+4')	Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc.	2**
Side to side living space	8' (4'+4')	Second floor cantilever over garage	2'
Front Setbacks (Private Street Facing) - To Back of Sidewalk		Private Open Space Per Unit	
Living space	8'	Minimum dimension	8'
Porch, balcony	5'	Total per unit	100' s.f.
Garden wall	3'		
Side Setbacks - To Lot Edge		Parking	
Living space	4'	Per Ontario Development Code	
Garage Setbacks		Building Height	
Garage facing street	18'	Architectural projections may exceed the maximum building height of 35' by an additional 5'.	
Garage face to garage face	30'	*3' min. from PL	
Garage face to lane (drive apron)	5'		
Drive aprons shall be 5' or less in length or 18' in length or greater.			
Note: Lane width will increase to 24' when the length exceeds 150'			

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**Exhibit G – Millennium Collection Architectural Styles**

**Parkside**

## Adaptive American Traditional

American Traditional is a style evolved in the early 20th century originating from the New England states, spreading westward, and becoming established throughout the rest of the country.

The adaptive version of this style retains the massing and form that is reminiscent of early American heritage but with simpler lines and contemporary details. The massing breaks down the façade from 2 to 1-story forms providing a natural balance to the front elevation. Roofs are simple in form most often with accent gables. This contemporized American style has a fresh appeal while retaining its charming origins.

7-24

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 Draft Amendment March 2020

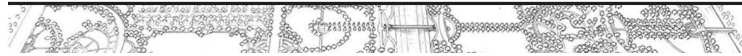
**Parkside**

Element	Menu of Common Features
<b>Form</b>	<ul style="list-style-type: none"> <li>Asymmetrical massing with a vertical and a horizontal break</li> </ul>
<b>Roof</b>	<ul style="list-style-type: none"> <li>4:12 roof pitch</li> <li>12" to 16" overhangs</li> <li>Concrete roof tile- flat or shake appearance</li> <li>Asphalt shingles</li> <li>Front to back gable or hip roof with intersecting hip or gable roofs</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>Stucco</li> <li>Horizontal siding- may be combined with stucco</li> <li>Wrapped material on sides and terminate at logical end (ret turn block wall/fence) or inside corner</li> </ul>
<b>Windows</b>	<ul style="list-style-type: none"> <li>Vertical multi-paned window at front elevations</li> <li>Multi-paned window on side and rear elevations at visible edges</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>Simplified cornice trim at gable ends</li> <li>Header window wood accent trim</li> <li>Simple 2 x 4 window and door trim- wood on siding, foam on stucco, 2 x 4 window and door trim may be used on specific product types, architectural styles subject to Planning Department approval.</li> <li>Surface mounted fixtures on front elevations must complement architectural style</li> <li>Porches and/or balconies with horizontal railing</li> <li>Front and garage door may have modern styling to complement style</li> <li>Square wood columns with trim</li> <li>Shutters, brick accents permitted</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>Field: Whites, off-white, dark or light colors</li> <li>Trim: White or contrasting with field color</li> <li>Accents: Light or dark colors in contrast or harmony with field colors</li> </ul>

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**Exhibit G – Millennium Collection Architectural Styles Continued**

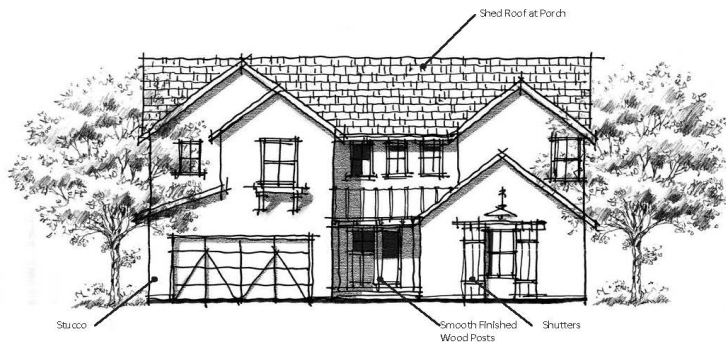


**Parkside**

**Adaptive Farmhouse**

Due to the simple and honest nature of this architectural form, the Farmhouse style can easily be adapted to a more abstract solution and is appropriate within many of the higher density settings. Since this style's roof shapes, variety of textures, colors and materials can easily be extrapolated to more contemporary applications, one can find many examples of this transformation throughout the West.

The Adaptive Farmhouse style blends the sleek clean lines of contemporary design with warm farmhouse charm to create a uniquely fresh take on this country living inspired style.



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**Parkside**



Board and Batten Siding



Proper Terminate Points for Siding and Color Accents

Element	Menu of Common Features
<b>Form</b>	<ul style="list-style-type: none"> <li>Symmetrical or asymmetrical massing</li> </ul>
<b>Roofs</b>	<ul style="list-style-type: none"> <li>Roof pitches 4:12 to 6:12, possibly lower pitch at main roof</li> <li>Shed roofs at porches, may use metal seam</li> <li>0" - 12" overhang at eaves</li> <li>Asphalt shingle roof or tile shake pattern</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>Stucco</li> <li>Board and batten or horizontal siding</li> <li>Siding should return on sides and terminate at logical end (return block wall/fence) or inside corner</li> </ul>
<b>Windows</b>	<ul style="list-style-type: none"> <li>Vertically or horizontally proportioned</li> <li>Smooth finished posts (6" x 6" min.) with smooth finish cap and base trim</li> <li>Smooth finished beams</li> <li>Door and window surrounds shall consist of one of the following materials:               <ul style="list-style-type: none"> <li>Well-proportioned, stucco-wrapped, high density foam trim or</li> <li>Smooth textured wood</li> </ul> </li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>Raised panel-style entry door and garage door</li> <li>Wood brackets below gables and/or rafter tails</li> <li>Awnings, may use metal seam</li> <li>Porches and/or balconies with horizontal railing</li> <li>Shutters</li> <li>Cementitious or stucco trim (with finish matching body of house) may be permitted at front doors/porch entries</li> <li>Front and garage doors may have modern styling to complement style</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>Field: Whites or light tinted colors</li> <li>Trim: Whites or light shades complementary to field color</li> <li>Accents: Light or dark shades in contrast with field color</li> </ul>

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**Exhibit G – Millennium Collection Architectural Styles Continued**

**Parkside**

## Adaptive Mediterranean

The Adaptive Mediterranean style draws inspiration from Southern European countries along the Mediterranean Sea. Italian, Spanish, Portuguese and Greek architecture lend their influence to this style.

Simplified details borrowed from these Mediterranean styles include streamlined forms which move toward a modern and unpretentious aesthetic – including low-pitched, hipped or parapet roofs combined with a lighter color palette.

From: Entry Door Without a Porch,  
Recessed From Front Facade

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**Parkside**

Light or dark colors in contrast and in harmony with field colors

Limited ornamentation and no shutters

Element	Menu of Common Features
<b>Form</b>	<ul style="list-style-type: none"> <li>Simple geometric forms</li> </ul>
<b>Roofs</b>	<ul style="list-style-type: none"> <li>Hip or gable roofs</li> <li>Roof pitches 4:12 to 6:12 where applicable, possibly lower pitch at main roof; may have a shed roof accent</li> <li>0'-12" overhang</li> <li>S-shape, or flat tile, or asphalt shingle</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>Stucco</li> <li>Optional decorative ceramic tile</li> </ul>
<b>Windows</b>	<ul style="list-style-type: none"> <li>Vertically or horizontally proportioned windows or ganged windows</li> <li>Round top or arched primary window or architectural feature</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>Front entry doors without a porch, recessed from front facade</li> <li>Limited ornamentation and no shutters</li> <li>Decorative metal elements or ceramic tile</li> <li>Cementitious or stucco trim (with finish matching body of house) may be permitted at front doors/porch entries</li> <li>Style appropriate light fixtures</li> <li>Horizontal railings at porches or balconies</li> <li>Built-up eave</li> <li>Color Blocking</li> <li>Front door and garage door may have modern styling to complement style</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>Field: Toned whites and light to medium light value warm colors</li> <li>Trim: Medium dark value browns reminiscent of stained wood</li> <li>Accents: Light or dark colors in contrast or harmony with field colors</li> </ul>

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**Exhibit G – Millennium Collection Architectural Styles Continued**

**Parkside**

## Contemporary

Clean lines and ordered appearance. An architectural expression that is forward facing and suggests a separation from the old world.

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**Parkside**

Parapet Roof      Horizontal Grouped Windows

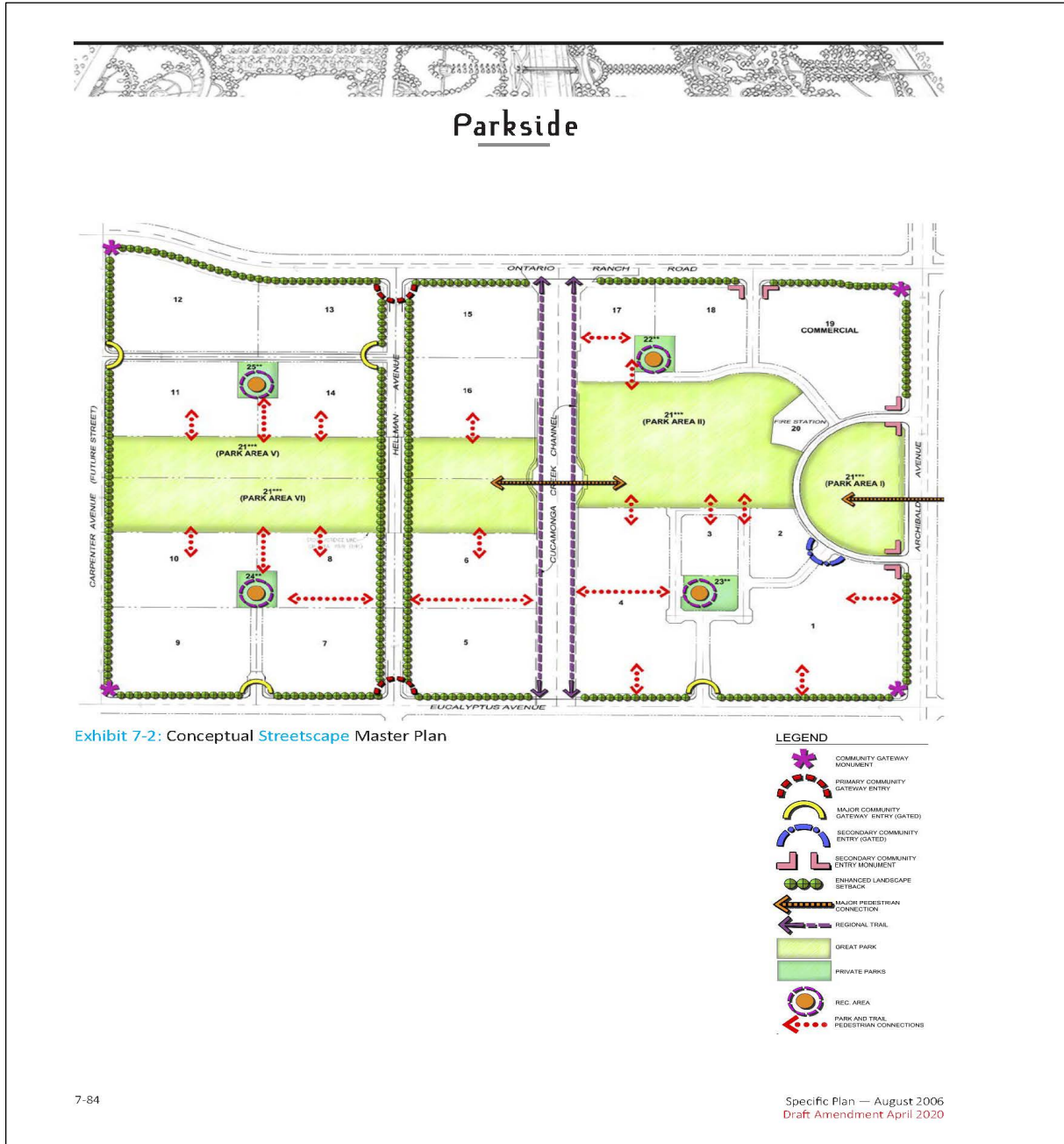
Element	Menu of Common Features
<b>Form</b>	<ul style="list-style-type: none"> <li>Simple rectangular form massing</li> <li>Signature form, detail or feature</li> </ul>
<b>Roofs</b>	<ul style="list-style-type: none"> <li>Parapet, hip or shed roof</li> <li>Simple unadorned parapet walls</li> <li>Roof pitch range: flat or 4:12 to 6:12</li> <li>0'- 12" overhang at eaves</li> <li>Shed element at entry with metal seam roof</li> <li>Asphalt shingles</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>Stucco finish</li> <li>Cementitious siding</li> </ul>
<b>Windows</b>	<ul style="list-style-type: none"> <li>Vertically or horizontally proportioned or grouped windows</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>Entry accents such as awnings with metal seam roof or metal eyebrows</li> <li>Cementitious or stucco trim (with finish matching body of house) may be permitted at front doors/porch entries</li> <li>Style appropriate light fixtures</li> <li>Horizontal railings at porches or balconies</li> <li>Front door and garage door may have modern styling to complement style and may include clear or frosted glass window details</li> <li>Infill panels</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>Field: Whites or light shades</li> <li>Accents: Light or dark shades in contract with field color</li> </ul>

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Draft Amendment March 2020



### Exhibit H – Community Gateway Monument Plan

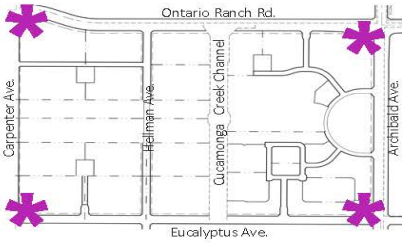


## Exhibit I – Community Gateway Monument Plan Concept



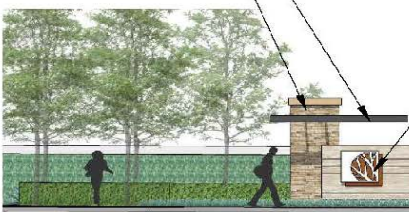
### Parkside

**Community Gateway Monuments**  
 The community's identity will be defined at the four corners of the Specific Plan area. These four icons or "landmarks" will announce the Parkside community as somewhere truly special. Refer to Exhibit 7-12: Community Gateway Monument Concept for illustration.



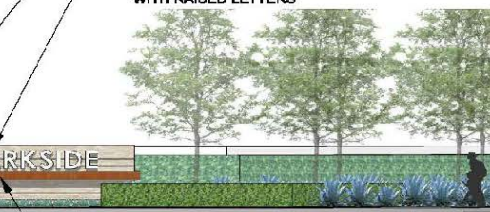
**STEEL EYEBROW TRELLIS**

**ICON COLUMN**  
 4' SQ. x 11' HT. STONE VENEER W/ PRE-CAST CAP



**PROJECT LOGO**

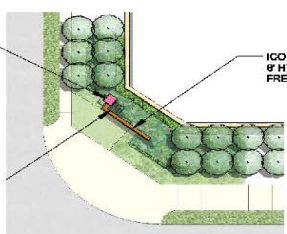
**ICON SIGN WALL**  
 6' HT. "BOARD FORM" TILES WITH RAISED LETTERS




**HORIZONTAL MANTEL**


**ICON COLUMN**  
 STONE VENEER W/ PRE-CAST CAP

**STEEL EYEBROW TRELLIS**



**ICON SIGN WALL**  
 6' HT. "BOARD FORM" FREE STANDING WALL





**Exhibit 7-12: Community Gateway Monument Concept**

Specific Plan — August 2006  
 Draft Amendment April 2020

7-95

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING CITY COUNCIL APPROVAL OF AN ADDENDUM TO THE PARKSIDE SPECIFIC PLAN ENVIRONMENTAL IMPACT REPORT (SCH#2004011008), PURSUANT TO THE REQUIREMENTS OF THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, AS AMENDED, FOR FILE NO. PSPA19-007.

WHEREAS, SC Ontario Development Company, LLC, (hereinafter referred to as "Applicant") has filed an Application for the approval of a an Amendment to the Parkside Specific Plan, File No. PSPA19-007, which consists of: [1] Reconfigure residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 21; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). The project site is bounded by Ontario Ranch Road to the north, Eucalyptus Avenue to south, Archibald Avenue to the east and Carpenter Avenue to the west, in the City of Ontario, California (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Parkside Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011008) was certified on September 5, 2006 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario has prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the EIR Addendum concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, pursuant to State CEQA Guidelines Section 15164(a), a lead agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary to a project, but the preparation of a subsequent or supplemental EIR is not required; and

WHEREAS, the City determined that none of the conditions requiring preparation of a subsequent or supplemental EIR would occur from the Project, and that preparation of an Addendum to the Certified EIR was appropriate; and

WHEREAS, the City of Ontario is the lead agency on the Project, and the Planning Commission is the recommending authority for the requested approval to construct and otherwise undertake the Project; and

WHEREAS, the Planning Commission has reviewed and considered the EIR Addendum for the Project, has concluded that none of the conditions requiring preparation of a subsequent of supplemental EIR have occurred, and intends to take actions on the Project in compliance with CEQA and state and local guidelines implementing CEQA; and

WHEREAS, the EIR Addendum for the Project are on file in the Planning Department, located at 303 East B Street, Ontario, CA 91764, are available for inspection by any interested person at that location and are, by this reference, incorporated into this Resolution as if fully set forth herein; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

**SECTION 1: Environmental Determination and Findings.** As the recommending authority for the Project, The Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of this project were reviewed in conjunction with an Addendum to Parkside Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011008), certified by the Ontario City Council on September 5, 2006, in conjunction with File No. PSP03-002; and

(2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and

(4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and

(5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and

(6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.

**SECTION 2: Additional Environmental Review Not Required.** Based on the Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Environmental Impact Report is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

**SECTION 3: *Planning Commission Action.*** Based upon the findings and conclusions set forth in Sections 1 and 2, above, the Planning Commission hereby recommends the City Council finds that based upon the entire record of proceedings before it, and all information received, that there is no substantial evidence that the Project will constitute substantial changes to the Certified EIR, and does hereby approve the EIR Addendum, attached hereto as "Attachment A," and incorporated herein by this reference.

**SECTION 4: *Indemnification.*** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

**SECTION 5: *Custodian of Records.*** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

**SECTION 6: *Certification to Adoption.*** The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26<sup>th</sup> day of May 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

---

Jim Willoughby  
Planning Commission Chairman

ATTEST:

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Cathy Wahlstrom  
Planning Director and  
Secretary to the Planning Commission

STATE OF CALIFORNIA                    )  
COUNTY OF SAN BERNARDINO        )  
CITY OF ONTARIO                        )

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. \_\_\_\_\_ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 26, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

---

Gwen Berendsen  
Secretary Pro Tempore



**ATTACHMENT A:**

**Addendum to Parkside Specific Plan  
Environmental Impact Report**

*(Addendum to follow this page)*



City of Ontario  
Planning Department  
303 East B Street  
Ontario, California 91764  
Phone: 909.395.2036  
Fax: 909.395.2420

## **California Environmental Quality Act Addendum to The Ontario Plan Environmental Impact Report**

**Project Title/File No.:** PSPA19-007

**Lead Agency:** City of Ontario, 303 East "B" Street, Ontario, California 91764, (909) 395-2036

**Contact Person:** Rudy Zeledon, Assistant Planning Director - 909-395-2422

**Project Sponsor:** City of Ontario, 303 East B Street, Ontario, CA 91764

**Project Location:** The project site is located in southwestern San Bernardino County, within the City of Ontario. The City of Ontario is located approximately 40 miles from downtown Los Angeles, 20 miles from downtown San Bernardino, and 30 miles from Orange County. As illustrated on Figures 1 through 3, below, the project site is located on Assessor Parcel Numbers (APN's): 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, and 0218-221-10) which is comprised of 250 gross acres bounded by Ontario Ranch Road to the north, Eucalyptus Avenue to south, Archibald Avenue to the east and Carpenter Avenue to the west.

**Figure 1: REGIONAL LOCATION MAP**

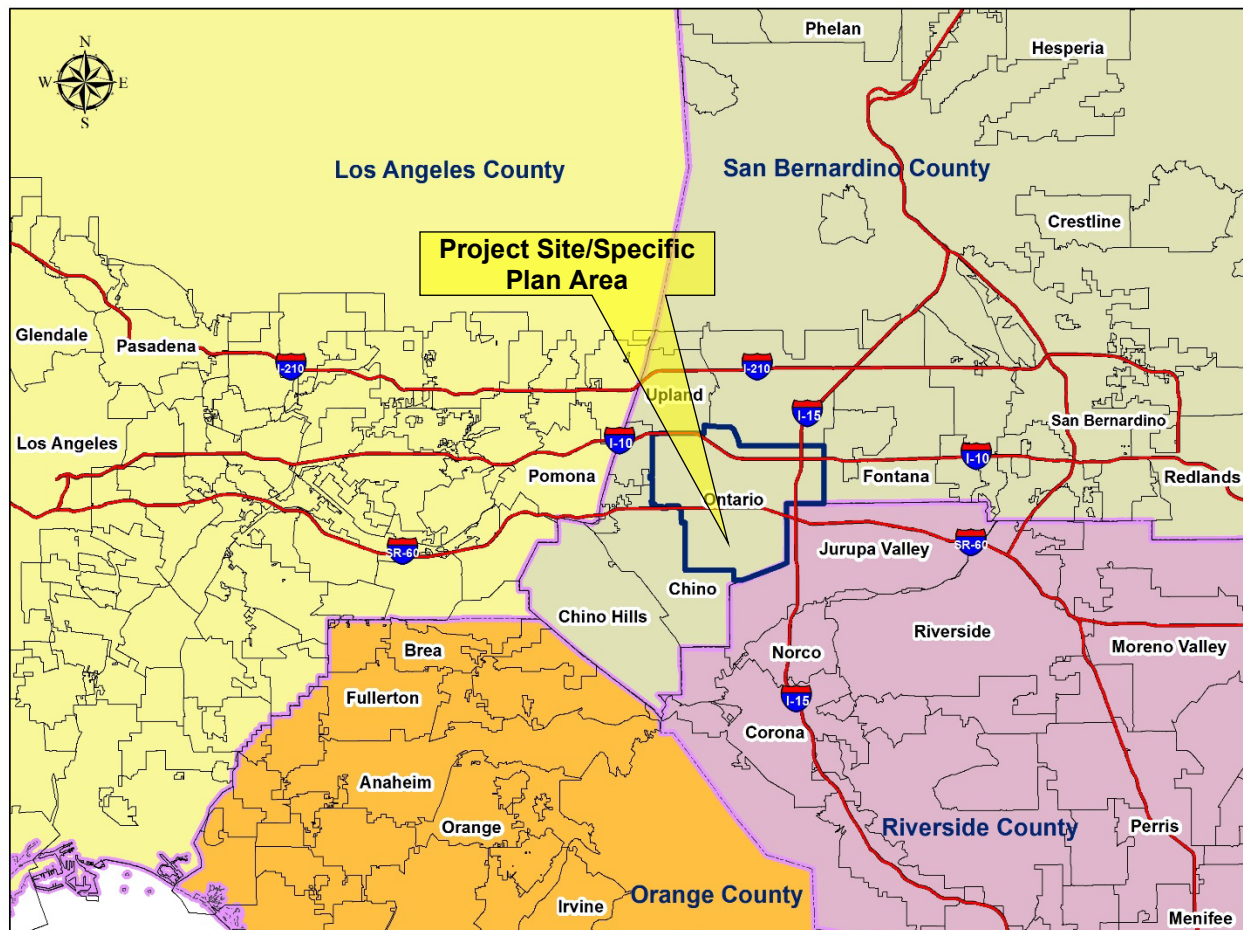


Figure 2: VICINITY MAP

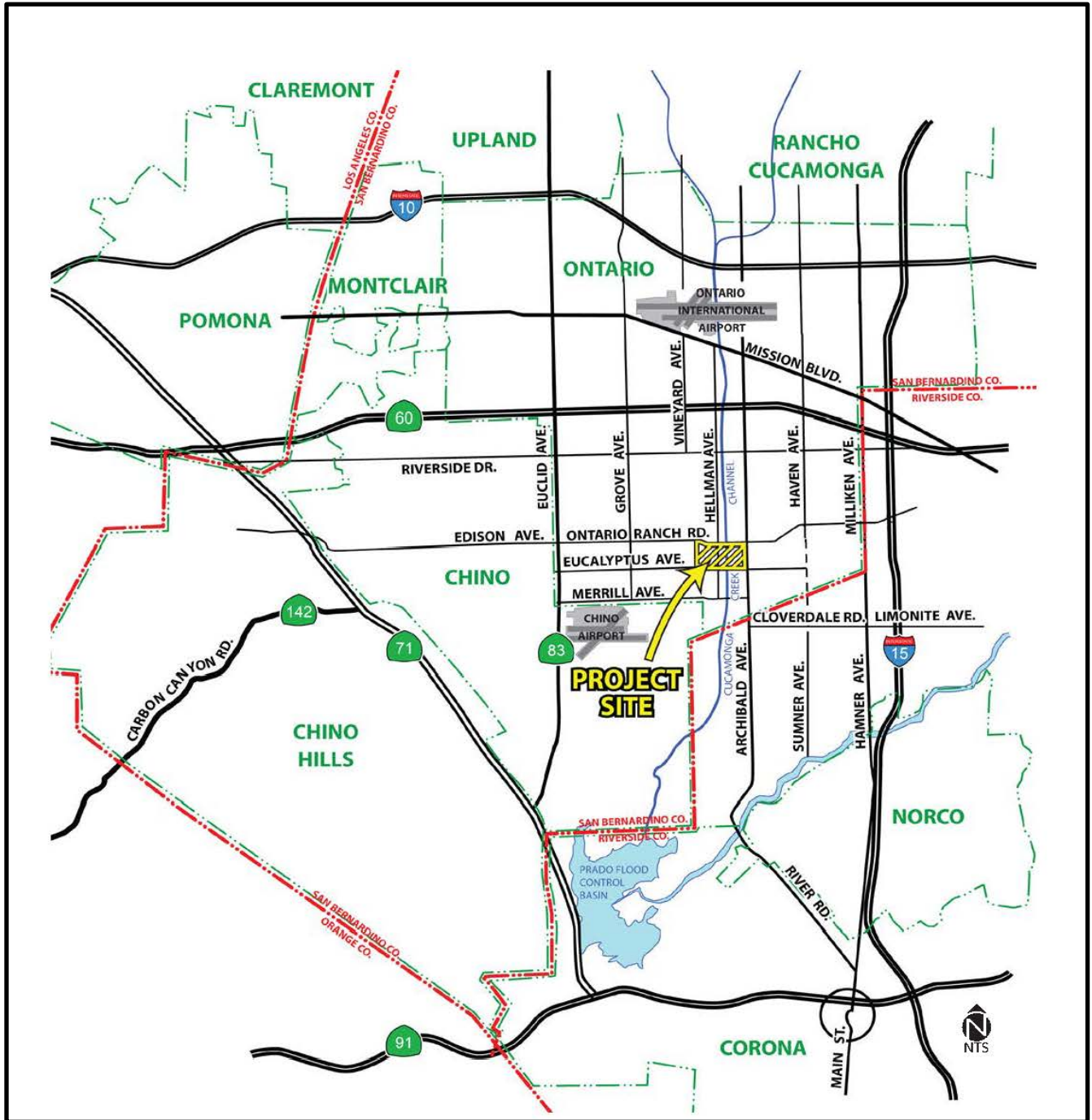


Figure 3: AERIAL PHOTOGRAPH



**General Plan Designation:** Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space Parkland.

**Zoning:** Specific Plan – Parkside Specific Plan

**Description of Project:** An Amendment to the Parkside Specific Plan (File No. PSPA19-007) to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7).

**Project Setting:** The project site, the Parkside Specific Plan area, is comprised of approximately 250.89 gross acres. SC Ontario owns approximately 237.89 gross acres of the project site. San Bernardino County Flood Control District owns the right-of-way for the Cucamonga Creek Channel that comprises approximately 13.00 gross acres, and effectively divides the eastern half of the project site from the western half, crossing the site from north to south approximately 1,650 feet west of Archibald Ave. The project site is bounded by Ontario Ranch Road to the north, Eucalyptus Avenue to south, Archibald Avenue to the east and Carpenter Avenue to the west. The Parkside Specific Plan area is generally undeveloped and general agricultural operations are scattered throughout the area, including an irrigation pond located within the northwestern portion of the project site. The approximate 250.89-gross acre project site has historically been used for general crop-related agricultural purposes (alfalfa, corn, milo, sorghum, etc.) and flood control.

**Background:** On January 27, 2010, the Ontario City Council adopted The Ontario Plan (“TOP”). TOP serves as the framework for the City’s business plan and provides a foundation for the City to operate as a municipal corporation that consists of six (6) distinct components: 1) Vision; 2) Governance Manual; 3) Policy Plan; 4) Council Priorities; 5) Implementation; and 6) Tracking and Feedback. The Policy Plan component of TOP meets the function. On January 27, 2010, the Ontario City Council adopted The Ontario Plan (TOP). TOP serves as the framework for the City’s business plan and provides a foundation for the City to operate as a municipal corporation that consists of six (6) distinct components: 1) Vision; 2) Governance Manual; 3) Policy Plan; 4) Council Priorities; 5) Implementation; and 6) Tracking and Feedback. The Policy Plan component of TOP meets the functional and legal mandate of a General Plan and contains nine elements: Land Use, Housing, Parks and Recreation, Environmental Resources, Community Economics, Safety, Mobility, Community Design and Social Resources.

An Environmental Impact Report (EIR) was prepared for TOP (SCH # 2008101140) and certified by the City Council on January 27, 2010 that included Mitigation Findings and a Statement of Overriding Considerations pursuant to CEQA. TOP EIR analyzed the direct and physical changes in the environment that would be caused by TOP; focusing on changes to land use associated with the buildout of the proposed land use plan, in the Policy Plan and impacts resultant of population and employment growth in the City. The significant unavoidable adverse impacts that were identified in the EIR included agriculture resources, air quality, cultural resources, greenhouse gas emissions, noise and transportation/traffic.

Prior to adoption of The Ontario Plan (TOP), an Environmental Impact Report (EIR) was prepared for the Parkside Specific Plan and certified (SCH# 2004011008) by the City Council on September 5, 2006, with a Statement of Overriding Considerations. The EIR analysis identified the potential environmental impacts associated with the Specific Plan. The significant unavoidable adverse impacts that were identified in the EIR included air quality, agriculture, traffic, water quality, solid waste and noise. In conjunction with the EIR, the City Council also approved the Parkside Specific Plan (File No. PSP03-002). The Specific Plan established the land use designations, development standards, and design guidelines for the existing Parkside Specific Plan project area boundary that presently encompasses approximately 250 acres of land for residential and commercial uses. The project was approved for a total of 1,947 single-family and multi-family residential units and 115,000 square feet of commercial. The proposed Amendment to the Parkside Specific Plan will not increase the total number of residential units (1,947) or commercial square (115,000 SF) feet and allow for additional single-family detached product types within the proposed Planning Areas 1 through 4.

**Analysis:** According to the California Environmental Quality Act Guidelines Section 15164, an Addendum to a previously Certified Parkside Specific Plan EIR may be used if some changes or additions are necessary, but none of the conditions described in Section 15162 requiring the preparation of a subsequent Negative Declaration or EIR have occurred. The CEQA Guidelines require that a brief explanation be provided to support the findings that no subsequent EIR or Negative Declaration are needed for further discretionary approval. These findings are described below:

1) *Required Finding: Substantial changes are not proposed for the project that will require major revisions of the previous EIR due to the involvement of new, significant environmental effects or a substantial increase in the severity of previously identified effects.*

Substantial changes are not proposed by the project and its implementation will not require revisions to the Certified Parkside EIR. The Parkside Specific Plan Certified EIR analyzed the direct and physical changes in the environment that would be caused by the development of 250.89-acre site; focusing on changes to land use associated with the buildout of the proposed land use plan of the Specific Plan. The Amendment to the Parkside Specific Plan (File No. PSPA19-007) proposes to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). The project was approved for a total of 1,947 single-family and multi-family residential units and 115,000 square feet of commercial. The proposed Amendment to the Parkside Specific Plan will not increase the total number of residential units (1,947) or commercial square feet and allow for additional single-family detached units within the proposed Planning Areas 1 through 4; therefore, the proposed amendment will result in the same or less development than Parkside Specific EIR analyzed at buildout.

In addition, all previously adopted mitigation measures of the Certified Parkside Specific Plan EIR are conditions of project approval or mitigation measures and are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

2) *Required Finding: Substantial changes have not occurred with respect to the circumstances under which the project is undertaken, that would require major revisions of the previous Environmental Impact Report due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects.*

Substantial changes have not occurred with respect to the circumstances under which the project was undertaken, that would not require major revisions to the Certified Parkside Specific Plan EIR in that the proposed changes would be in keeping with the surrounding area. The Certified Parkside Specific Plan EIR evaluated the site as residential, open space parkland, and commercial with a maximum number of 1,947 units 115,000 square feet of commercial. The proposed Amendment to the Parkside Specific Plan will not increase the total number of residential units (1,947) or commercial uses (115,000 SF) and allow for additional single-family detached units within the proposed Planning Areas 1 through 4. The proposed Specific Plan Amendment is implementing the Certified EIR's residential, commercial and parkland land use designations for the site and is consistent with The Ontario Plan (TOP) land use designation of Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space Parkland; therefore, no proposed changes or revisions to the Certified Parkside Specific Plan EIR are required. In addition, all previously adopted mitigation measures of the Certified Parkside Specific Plan EIR are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

3) *Required Finding: No new information has been provided that would indicate that the proposed project would result in one or more significant effects not discussed in the previous EIR.*

No new information has been provided that would indicate the proposed project would result in any new significant effects not previously discussed in the Certified Parkside Specific Plan EIR. As stated above in Section 2, no substantial changes have occurred with respect to the circumstances under which the Project was undertaken. Parkside Specific Plan evaluated the site as Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space Parkland (see attached Exhibit "A"- Parkside Specific Plan Figure 1-3, Proposed Land Use Plan). In addition, the proposed Specific Plan Amendment is implementing the Certified EIR's residential, commercial and open space parkland land use designations for the site and is consistent with The Ontario Plan (TOP) land use designation of Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space-Parkland (see attached Exhibit "B"- TOP Policy Plan (Exhibit LU-01) Land Use Plan Map); therefore, no proposed changes or revisions to the Certified Parkside Specific Plan EIR are required. In addition, all previously adopted mitigation measures are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

**CEQA Requirements for an Addendum:** If changes to a project or its circumstances occur or new information becomes available after adoption of a negative declaration, the lead agency may: (1) prepare a subsequent EIR if the criteria of State CEQA Guidelines Section 15162(a) are met, (2) prepare a subsequent negative declaration, (3) prepare an addendum, or (4) prepare no further documentation. (State CEQA Guidelines Section 15162(b)). When only minor technical changes or additions to the negative declaration are necessary and none of the conditions described in section 15162 calling for the preparation of a subsequent EIR or negative declaration have occurred, CEQA allows the lead agency to prepare and adopt an addendum. (State CEQA Guidelines Section 15164(b).)

Under Section 15162, a subsequent EIR or negative declaration is required only when:

1) Substantial changes are proposed in the project which will require major revisions of the previous negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;

2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the negative declaration due to the involvement of any new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or

3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the negative declaration was adopted, shows any of the following:

a) The project will have one or more significant effects not discussed in the previous negative declaration;

b) Significant effects previously examined will be substantially more severe than shown in the previous EIR;

c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or

d) Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

Thus, if the Project does not result in any of the circumstances listed in Section 15162 (i.e., no new or substantially greater significant impacts), the City may properly adopt an addendum to the Certified EIR.

**Conclusion:** The Parkside Specific Plan EIR, certified by City Council on September 5, 2006, was prepared

as a Program EIR in accordance with CEQA, the State CEQA Guidelines, and the City’s Rules for the Implementation of CEQA and in accordance with Section 15121(a) of the State CEQA Guidelines (California Code of Regulations, Title 14, Division 6, Chapter 3). The Parkside Specific Plan EIR, considered the direct physical changes and reasonably foreseeable indirect physical changes in the environment that would be caused by Specific Plan. Consequently, Parkside Specific Plan EIR focused on impacts from changes to land use associated with buildout of the Specific Plans Land Use Plan, with impacts resulting from population and employment growth in the City. The proposed Specific Plan Amendment coordinates with the existing uses of the properties and uses within the surrounding areas. As described on page 4 above, the proposed Amendment to the Parkside Specific Plan will not increase the total number of residential units (1,947) or commercial square (115,000 SF) feet of the approved Specific Plan and allow for additional single-family detached units within the proposed Planning Areas 1 through 4. Subsequent activities within TOP Program EIR have been evaluated to determine whether an additional CEQA document needs to be prepared.

Accordingly, and based on the findings and information contained in the Certified Parkside Specific Plan EIR, the analysis above, the attached Initial Study, and CEQA statute and State CEQA Guidelines, including Sections 15164 and 15162, the Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary, nor is there a need for any additional mitigation measures; therefore, pursuant to State CEQA Guidelines Section 15164, the Ontario City Council hereby adopts this Addendum to the Certified EIR.

**Surrounding Land Uses:**

	<b>Existing Land Use</b>	<b>General Plan Designation</b>	<b>Zoning Designation</b>	<b>Specific Plan Land Use</b>
Site:	Vacant	Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space-Parkland	Parkside Specific Plan	Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space-Parkland
North:	Agriculture\Diary Uses	Low Density Residential (2.1 – 5 du/ac)	The Avenue Specific Plan	Low Density Residential
South:	Industrial Development/ Vacant Land	Business Park (0.6 FAR) Low Density Residential (2. Commercial (0.40 FAR) 1 – 5 du/ac)	West Ontario Commerce Center Specific Plan Subarea 29 Specific Plan	Business Park Low Density Residential
East:	Vacant/Agriculture\Diary Uses	Medium Density Residential (11.1 – 25 du/ac) and Open Space-Parkland	Grand Park Specific Plan	High Density Residential Great Park
West:	Vacant/ Agriculture\Diary Uses	Mixed-Use (NMC West)	Agriculture Overlay/Specific Plan	N/A

**Other public agencies whose approval is required (e.g., permits, financing approval or participation agreement):** None

**Tribal Consultation:** Have California Native American tribes traditionally and culturally affiliated with the project area requested consultation pursuant to Public Resources Code section 21080.3.1?  Yes  No

If “yes,” has consultation begun?  Yes  No  Completed



### ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- |  |   |   |
|--|---|---|
| <input type="checkbox"/> Aesthetics                | <input type="checkbox"/> Agriculture/Forestry Resources | <input type="checkbox"/> Air Quality                        |
| <input type="checkbox"/> Biological Resources      | <input type="checkbox"/> Cultural Resources             | <input type="checkbox"/> Geology / Soils                    |
| <input type="checkbox"/> Greenhouse Gas Emissions  | <input type="checkbox"/> Hazards & Hazardous Materials  | <input type="checkbox"/> Hydrology / Water Quality          |
| <input type="checkbox"/> Land Use / Planning       | <input type="checkbox"/> Mineral Resources              | <input type="checkbox"/> Noise                              |
| <input type="checkbox"/> Population / Housing      | <input type="checkbox"/> Public Services                | <input type="checkbox"/> Recreation                         |
| <input type="checkbox"/> Transportation            | <input type="checkbox"/> Utilities / Service Systems    | <input type="checkbox"/> Mandatory Findings of Significance |
| <input type="checkbox"/> Tribal Cultural Resources | <input type="checkbox"/> Wildfire                       | <input type="checkbox"/> Energy                             |

### DETERMINATION (To be completed by the Lead Agency)

On the basis of this initial evaluation:

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

\_\_\_\_\_  
Signature

\_\_\_\_\_  
Date

Rudy Zeledon, Assistant Planning Director  
Printed Name and Title

City of Ontario  
For

### EVALUATION OF ENVIRONMENTAL IMPACTS

1) A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g. the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general

standards (e.g. the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).

2) All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.

3) Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect is significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.

4) "Negative Declaration: Less Than Significant with Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less than Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from the "Earlier Analyses" Section may be cross-referenced).

5) Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or Negative Declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:

a) Earlier Analyses Used. Identify and state where they are available for review.

b) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.

c) Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.

6) Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g. general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.

7) Supporting Information Sources. A source list should be attached, and other sources used, or individuals contacted should be cited in the discussion.

8) This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project's environmental effects in whatever format is selected.

9) The explanation of each issue should identify:

a) The significance criteria or threshold, if any, used to evaluate each question; and

b) The mitigation measure identified, if any, to reduce the impact to less than significance.

<i>Issues</i>	<i>Potentially Significant Impact</i>	<i>Less Than Significant with Mitigation</i>	<i>Less Than Significant Impact</i>	<i>Impacts Previously Analyzed in the Parkside SP EIR &amp; TOP EIR</i>
<b>1. AESTHETICS.</b> Would the project:				
<b>a.</b> Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<b>Issues</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant with Mitigation</b>	<b>Less Than Significant Impact</b>	<b>Impacts Previously Analyzed in the Parkside SP EIR &amp; TOP EIR</b>
<p><b>b.</b> Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p><b>c.</b> In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p><b>d.</b> Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p><b>2. AGRICULTURE AND FOREST RESOURCES.</b> In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest protocols adopted by the California Air Resources Board. Would the project:</p>				
<p><b>a.</b> Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p><b>b.</b> Conflict with existing zoning for agricultural use, or a Williamson Act contract?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p><b>c.</b> Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p><b>d.</b> Result in the loss of forest land or conversion of forest land to non-forest use?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p><b>e.</b> Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<b>Issues</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant with Mitigation</b>	<b>Less Than Significant Impact</b>	<b>Impacts Previously Analyzed in the Parkside SP EIR &amp; TOP EIR</b>
<b>3. AIR QUALITY.</b> Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:				
<b>a.</b> Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>b.</b> Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>c.</b> Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>d.</b> Result in other emissions (such as those leading to odors adversely affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>4. BIOLOGICAL RESOURCES.</b> Would the project:				
<b>a.</b> Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>b.</b> Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>c.</b> Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>d.</b> Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>e.</b> Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>f.</b> Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>5. CULTURAL RESOURCES.</b> Would the project:				

<b>Issues</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant with Mitigation</b>	<b>Less Than Significant Impact</b>	<b>Impacts Previously Analyzed in the Parkside SP EIR &amp; TOP EIR</b>
<b>a.</b> Cause a substantial adverse change in the significance of a historical resource pursuant to Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>b.</b> Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>c.</b> Disturb any human remains, including those interred outside of dedicated cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>6. ENERGY.</b> Would the project:				
<b>a.</b> Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>b.</b> Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>7. GEOLOGY AND SOILS.</b> Would the project:				
<b>a.</b> Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury or death involving:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>i.</b> Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>ii.</b> Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>iii.</b> Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>iv.</b> Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>b.</b> Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>c.</b> Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>d.</b> Be located on expansive soil, as defined in Table 18 1 B of the Uniform Building Code, creating substantial direct or indirect risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>e.</b> Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<b>Issues</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant with Mitigation</b>	<b>Less Than Significant Impact</b>	<b>Impacts Previously Analyzed in the Parkside SP EIR &amp; TOP EIR</b>
f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>8. GREENHOUSE GAS EMISSIONS.</b> Would the project:				
a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emission of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>9. HAZARDS AND HAZARDOUS MATERIALS.</b> Would the project:				
a. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>10. HYDROLOGY AND WATER QUALITY.</b> Would the project:				
a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<b>Issues</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant with Mitigation</b>	<b>Less Than Significant Impact</b>	<b>Impacts Previously Analyzed in the Parkside SP EIR &amp; TOP EIR</b>
<b>b.</b> Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>c.</b> Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>i.</b> result in substantial erosion or siltation on- or off-site;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>ii.</b> substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or offsite;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>iii.</b> create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>iv.</b> impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>d.</b> In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>e.</b> Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>11. LAND USE AND PLANNING.</b> Would the project:				
<b>a.</b> Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>b.</b> Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>12. MINERAL RESOURCES.</b> Would the project:				
<b>a.</b> Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>b.</b> Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>13. NOISE.</b> Would the project result in:				
<b>a.</b> Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>b.</b> Generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in the Parkside SP EIR & TOP EIR
<p>c. For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>14. POPULATION AND HOUSING.</b> Would the project:				
<p>a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>15. PUBLIC SERVICES.</b> Would the project:				
<p>a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:</p>				
<p>i. Fire protection?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>ii. Police protection?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>iii. Schools?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>iv. Parks?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>v. Other public facilities?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>16. RECREATION.</b> Would the project:				
<p>a. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which have an adverse physical effect on the environment?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>17. TRANSPORTATION.</b> Would the project:				
<p>a. Conflict with a program plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>b. Would the project conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>



<b>Issues</b>	<b>Potentially Significant Impact</b>	<b>Less Than Significant with Mitigation</b>	<b>Less Than Significant Impact</b>	<b>Impacts Previously Analyzed in the Parkside SP EIR &amp; TOP EIR</b>
c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>18. TRIBAL CULTURAL RESOURCES.</b> Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is				
a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>19. UTILITIES AND SERVICE SYSTEMS.</b> Would the project:				
a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in the Parkside SP EIR & TOP EIR
<b>20. WILDFIRES.</b> If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:				
<b>a.</b> Substantially impair an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>b.</b> Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>c.</b> Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>d.</b> Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>21. MANDATORY FINDINGS OF SIGNIFICANCE.</b>				
<b>a.</b> Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>b.</b> Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>c.</b> Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p><u>Note:</u> Authority cited: Public Resources Code sections 21083, 21083.05, 21083.09.</p> <p><u>Reference:</u> Gov. Code section 65088.4; Public Resources Code sections 21073, 21074, 21080(c), 21080.1, 21080.3, 21080.3.1, 21080.3.2, 21082.3, 21083, 21083.3, 21083.5, 21084.2, 21084.3, 21093, 21094, 21095 and 21151; <i>Sundstrom v. County of Mendocino</i> (1988) 202 Cal.App.3d 296; <i>Leonoff v. Monterey Board of Supervisors</i> (1990) 222 Cal.App.3d 1337; <i>Eureka Citizens for Responsible Govt. v. City of Eureka</i> (2007) 147 Cal.App.4th 357; <i>Protect the Historic Amador Waterways v. Amador Water Agency</i> (2004) 116 Cal.App.4th 1099, 1109; <i>San Franciscans Upholding the Downtown Plan v. City and County of San Francisco</i> (2002) 102 Cal.App.4th 656.</p>				

**EXPLANATION OF ISSUES**

**1. AESTHETICS.** Would the project:

**a. Have a substantial adverse effect on a scenic vista?**

Discussion of Effects: The Policy Plan (General Plan) does not identify scenic vistas within the City. However, TOP Policy Plan (Policy CD1-5) requires all major north-south streets be designed and redeveloped to feature views of the San Gabriel Mountains. The project site is located at the northwest corner of Archibald Avenue and Eucalyptus Avenue. Archibald Avenue is identified as Principal Arterial and Eucalyptus Avenue as a Collector Street in the Functional Roadway Classification Plan (Figure M-2) of the Mobility Element within the Policy Plan. The proposed Specific Plan Amendment will not result in adverse environmental impacts with regard to views of the San Gabriel Mountains. Therefore, no adverse impacts are anticipated in relation to the project.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR or TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings and historic buildings within a state scenic highway?**

Discussion of Effects: The City of Ontario is served by three freeways: I-10, I-15, and SR-60. I-10 and SR-60 traverse the northern and central portion of the City, respectively, in an east–west direction. I-15 traverses the northeastern portion of the City in a north–south direction. These segments of I-10, I-15, and SR-60 have not been officially designated as scenic highways by the California Department of Transportation. In addition, there are no historic buildings, or any scenic resources identified on or in the vicinity of the project site. Therefore, it will not result in adverse environmental impacts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?**

Discussion of Effects: The Project would not degrade the existing visual character or quality of the site or its surroundings. The project site is in an area that is characterized by agriculture uses and residential development and is surrounded by urban land uses.

Any development proposals that would subsequently occur from the proposed Project will be required to be in accordance with the policies of the Community Design Element of the Policy Plan (General Plan) and development regulations and design guidelines of the Parkside Specific Plan. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**c. Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?**

Discussion of Effects: The proposed land use change itself will not cause lighting to be installed in the Project. New lighting will be introduced to the site with the development of the project. Pursuant to the requirements of the City's Development Code, on-site lighting will be shielded, diffused or indirect, to avoid glare to pedestrians or motorists. In addition, lighting fixtures will be selected and located to confine the area of illumination to within the project site and minimize light spillage.

Site lighting plans will be subject to review by the Planning Department and Police Department prior to issuance of building permits (pursuant to the City's Building Security Ordinance). Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**2. AGRICULTURE AND FOREST RESOURCES.** In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. Would the project:

**a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?**

Discussion of Effects: As discussed in the Certified Parkside Specific Plan EIR (2006), a considerable portion of the project site has been used for agricultural/dairy farming. The project will convert this land, which is considered to be Prime Farmland pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to nonagricultural use. The conversion of farmland to urban uses was determined to be a potentially significant impact that is unavoidable. The changes to the Project do not change this conclusion and there is no additional mitigation presently available that could potentially reduce this impact. The impact will remain as a significant unavoidable impact.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?**

Discussion of Effects: The 250-acre Parkside Specific Plan is currently used for irrigated crop production to be converted to urban use. The Parkside Specific Plan EIR determined that the conversion of agricultural uses to urban uses was a potentially significant impact that is unavoidable. Furthermore, there are no Williamson Act contract in effect within the 250-acre Parkside Specific Plan area. Therefore, no impacts to agricultural uses are anticipated, nor will there be any conflict with existing or Williamson Act contracts. The proposed Amendment to the Parkside Specific Plan do not change this conclusion and there is no additional mitigation presently available that could potentially reduce this impact. The impact will remain as a significant unavoidable impact.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?**

Discussion of Effects: The project proposes an Amendment to the Parkside Specific Plan to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). This would not result in the rezoning of forest land, timberland, or timberland zoned Timberland Production because such land use designations do not exist within the City of Ontario. Therefore, no impacts to forest or timberland are anticipated.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**d. Result in the loss of forest land or conversion of forest land to non-forest use?**

**Discussion of Effects:** There is currently no land in the City of Ontario that qualifies as forest land as defined in Public Resources Code section 12220(g). Neither TOP nor the City's Zoning Code provide designations for forest land. Consequently, the proposed project would not result in the loss or conversion of forest land.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**e. Involve other changes in the existing environment, which, due to their location or nature, could individually or cumulatively result in loss of Farmland to non-agricultural use or conversion of forest land to non-forest use?**

**Discussion of Effects:** The 250-acre Parkside Specific Plan area is currently used for irrigated crop production to be converted to urban use. The project will convert this land, which is considered to be Prime Farmland pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to nonagricultural use. The conversion of farmland to urban uses was determined to be a potentially significant impact that is unavoidable. The changes to the Project do not change this conclusion and there is no additional mitigation presently available that could potentially reduce this impact. The impact will remain as a significant unavoidable impact.

Additionally, there is currently no land in the City of Ontario that qualifies as forest land as defined in Public Resources Code Section 12220(g). Neither TOP nor the City's Zoning Code provide designations for forest land. Consequently, to the extent that the proposed project would result in changes to the existing environment, those changes would not impact forest land.

**Mitigation Required:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**3. AIR QUALITY.** Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

**a. Conflict with or obstruct implementation of the applicable air quality plan?**

**Discussion of Effects** The project will not conflict with or obstruct implementation of any air quality plan. As noted in The Ontario Plan FEIR (Section 5.3), pollutant levels in the Ontario area already exceed Federal and State standards. To reduce pollutant levels, the City of Ontario is actively participating in efforts to enhance air quality by implementing Control Measures in the Air Quality Management Plan for local jurisdictions within the South Coast Air Basin.

The proposed Specific Plan Amendment is implementing the Certified Parkside Specific Plan EIR's residential, commercial and parkland land use designations for the site and which is consistent with The Ontario Plan (TOP) land use designation of Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space-Parkland. The project will not conflict with or obstruct implementation of any air quality plan.

The proposed project is consistent with The Ontario Plan, for which the EIR was prepared and impacts evaluated. Furthermore, the project is consistent with the City's participation in the Air Quality Management Plan and will not conflict with or obstruct implementation of the plan. Mitigation (Mitigation Measure 5.3-2) has been adopted by the City that requires fugitive dust control measures pursuant to SCAQMD's Rule 403, use of Tier 3 construction equipment, proper service and maintenance of construction equipment, limiting nonessential idling of construction equipment, and use of Super-Compliant VOC paints for coating and architectural surfaces. Any future development proposals on the project site will be required,

as a condition of approval, to comply with Mitigation Measure 5.3-2. No new impacts beyond those identified in the Certified Parkside Specific Plan EIR or TOP EIR that would result from Project implementation.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?**

Discussion of Effects: The project will not result in a cumulatively considerable net increase of any criteria pollutant for which the region is in non-attainment under an applicable federal or state ambient air quality. The proposed Amendment to the Parkside Specific Plan will not increase the total number of residential units (1,947) or commercial uses (115,000 SF) and will allow for additional single-family detached units within the proposed Planning Areas 1 through 4. The proposed Specific Plan Amendment is implementing the Certified EIR's residential, commercial and parkland land use designations for the site, which is consistent with The Ontario Plan (TOP) land use designation of Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space Parkland. Mitigation (Mitigation Measure 5.3-1) has already been adopted by the City that would facilitate continued City cooperation with the SCAQMD and SCAG to achieve regional air quality improvement goals, promote energy conservation design and development techniques, encourage alternative modes of transportation, and implement transportation demand strategies. The project will comply with the air quality standards of the Certified Parkside Specific Plan EIR, TOP EIR and the SCAQMD resulting in impacts that are less than significant [please refer to Sections 3(a) and 3(b)].

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**c. Expose sensitive receptors to substantial pollutant concentrations?**

Discussion of Effects: The subject site was previously analyzed by TOP EIR as Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space Parkland is surrounded on the north, east, west and south by future residential and commercial uses. As discussed in Section 5.3 of TOP EIR, the proposed Project is within a non-attainment region of the South Coast Air Basin (SCAB). The proposed Specific Plan Amendment is implementing the Certified Parkside Specific Plan EIR's residential, commercial and parkland land use designations for the site and is consistent with The Ontario Plan (TOP) land use designation of Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space Parkland. Adequate mitigation (TOP EIR Mitigation Measure 5.3-5) has already been adopted by the City that would require new developments to be consistent with recommended buffer distances of the California Air Resources Board's Air Quality and Land Use Handbook: A Community Health Perspective (April 2005). Under this mitigation, new development that is inconsistent with the recommended buffer distances shall only be approved if all feasible mitigation measures, such as high efficiency Minimum Efficiency Reporting Value filters, have been incorporated into the project design to protect future sensitive receptors from harmful concentrations of air pollutants as a result of proximity to existing air pollution sources. No new impacts beyond those identified in the Certified Parkside Specific Plan EIR or TOP EIR would result from Project implementation.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**d. Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?**

Discussion of Effects: The subject site was previously analyzed by TOP EIR as Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space Parkland is surrounded on the north, east, west and south by future residential and commercial uses. The uses proposed on the subject site, as well as those permitted within the residential and commercial district

Parkside Specific Plan, do not create objectionable odors. Further, the project shall comply with the policies of the Ontario Municipal Code and the Policy Plan (General Plan). Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**4. BIOLOGICAL RESOURCES.** Would the project:

**a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?**

Discussion of Effects: The Ontario Plan FEIR (Section 5.4) and the Parkside Specific Plan EIR (2007) identified the potential habitat for the federally listed Raptor and Burrowing Owl. According to the Certified Parkside Specific Plan EIR, the project site is located within an area that has been identified as containing species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations or by the California Department of Fish and Game or the U.S. Fish and Wildlife Service. These species include: Burrowing Owl, and various species of nesting birds protected under the Migratory Bird Treaty Act. However, the proposed Amendment to the Parkside Specific Plan would not have an impact on these species because no development project is proposed at this time that would have the ability to disturb the site. Any future development within the Specific Plan area would be required to conduct pre-construction surveys for the Burrowing Owls, and focused surveys for migratory bird nests. If the surveys identify any of these species on the Project site, the proper measures must be undertaken by the future project applicant to ensure that these impacts are reduced to less than significant. Therefore, because the proposed Project is an Amendment to the Parkside Specific Plan and no development of the site is proposed at this time, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?**

Discussion of Effects: The subject site was previously analyzed by the Certified Parkside Specific Plan EIR as residential, open space parkland and commercial uses. The site does not contain any riparian habitat or other sensitive natural community identified by the Department of Fish & Game or Fish & Wildlife Service. Therefore, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**c. Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?**

Discussion of Effects: No wetland habitat is present on site. Therefore, project implementation would have no impact on these resources.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?**

Discussion of Effects: The site is a vacant and used for crop production and that is bounded on all four sides by fragmented residential development. As a result, there are no wildlife corridors connecting this site to other areas. Therefore, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?**

Discussion of Effects: The City of Ontario does not have any specific policies or ordinances protecting biological resources. Further, the Parkside Specific Plan area does not contain any mature trees necessitating the need for preservation. As a result, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**f. Conflict with the provisions of an adopted Habitat Conservation Plan (HCP), Natural Community Conservation Plan (NCCP), or other approved local, regional, or state habitat conservation plan?**

Discussion of Effects: The site is not part of an adopted HCP, NCCP or other approved habitat conservation plan. As a result, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**5. CULTURAL RESOURCES.** Would the project:

**a. Cause a substantial adverse change in the significance of a historical resource pursuant to Section 15064.5?**

Discussion of Effects: The subject site was previously analyzed by Parkside Specific Plan EIR for residential, open space parkland, and commercial uses. The subject site is vacant and does not contain any buildings or structures constructed more than 50 years ago and cannot be considered for eligibility for listing in the California Register of Historic Resources. A Cultural Resources Records Search was conducted on July 16, 2019 at the California Historic Resource Inventory System at the South Central Coastal Information Center (CHRIS-SCCIC). The records search indicated that there are no cultural resources (prehistoric, historic, or built environments) recorded within the Project boundaries. There was one (1) historic resource (CA-SBR-008857H) located within a one-half mile radius of the Project Site. The historic resource is a section of the Southern California Edison Company's Lugo-Mira No. 1 500kv Transmission Line. The transmission line was determined eligible for listing in the National Register of Historic Place (NRHP) under Criteria A and C, and therefore, is eligible for listing in the California Register of Historic Resources. This historic resource will not be impacted (directly or indirectly) by the proposed Project. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?**



**Discussion of Effects:** The Certified Parkside Specific Plan EIR (Section 4 Culture Resources, page III-4-6 &7) indicates no archeological sites or resources have been recorded in the City with the Archeological Information Center at San Bernardino County Museum. The NAHC Sacred Lands File search also failed to indicate archaeological resources or artifacts associated with Tribal Cultural Resources (TCRs) within the Project site. The Project site has been highly disturbed by modern human activities to with agricultural production since the 1950's. Therefore, the proposed Project will not impact cultural (prehistoric, historic, or historic built environments) resources and no mitigation measures are recommended. While no adverse impacts to archeological resources are anticipated at this site due to its urbanized nature, standard conditions have been imposed on the project that in the event of unanticipated archeological discoveries, construction activities will not continue or will moved to other parts of the project site and a qualified archaeologist shall be contacted to determine significance of these resources. If the find is discovered to be historical or unique archaeological resources, as defined in Section 15064.5 of the CEQA Guidelines, avoidance or other appropriate measures shall be implemented.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**c. Disturb any human remains, including those interred outside of formal cemeteries?**

**Discussion of Effects:** The proposed project is in an area that has been previously disturbed by human activity. No known religious or sacred sites exist within the Project area. Thus, human remains are not expected to be encountered during any construction activities. However, in the unlikely event that human remains are discovered, existing regulations, including the California Public Resources Code Section 5097.98, would afford protection for human remains discovered during development activities. Furthermore, standard conditions have been imposed on the project that in the event of unanticipated discoveries of human remains are identified during excavation, construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and/or Native American consultation has been completed, if deemed applicable.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**6. ENERGY** Would the project:

**a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?**

**Discussion of Effects:** Energy was not analyzed in the Certified Parkside Specific Plan EIR but has been included as part of the 2019 revisions to the State CEQA Guidelines. Implementation of the Project would increase the demand for electricity and natural gas at the Project site and gasoline consumption in the region during construction and operation. A detailed discussion is provided below.

**Electricity**

**Construction.** Temporary electric power would be required for lighting and electronic equipment (e.g., computers) located in trailers used by the construction crew. However, the electricity used for such activities would be temporary and would have a negligible contribution to the project's overall energy consumption.

**Operational.** The project proposes an Amendment to the Parkside Specific Plan proposes to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). The Project does not include construction of an actual development. However, during hypothetical operation of the Project, a residential use or commercial, use would require electricity for multiple purposes, such as: building heating and cooling, lighting, appliances, and electronics. Any future development on the site would be required to comply with the

CALGreen Building Code requirements in effect at the time of development, which are more efficient than the 2016 standards. Moreover, the Project includes a sample Greenhouse Gas Reduction Measure Screening Table for Residential and Commercial Development. The Screening Table includes measures energy efficient development, indoor space efficiency measures, building efficiency measures, renewable energy measures, and water conservation measures. Measures that would reduce electricity consumption include, but are not limited to: greatly enhanced window insulation, an enhanced cool-roof, an improved efficiency heating, ventilation, and air conditioning (“HVAC”) system, blower doors HERS verified Envelope leakage or equivalent, enhanced duct insulation, Energy Star commercial appliances, water efficient landscaping and irrigation systems, and water-efficient toilets and faucets. Although electricity consumption would increase at the site under implementation of the Project, the building envelope, HVAC, lighting, and other systems, would be designed to maximize energy performance. The project would also be subject to statewide mandatory energy requirements as outlined in the CALGreen Code. In addition, the project would implement additional measures, as detailed in the GHG reduction measures screening table, that would further reduce electricity consumption. Electricity that would be consumed by the Project would also be subject to the cap-and-trade regulation. For these reasons, the electricity that would be consumed by the Project is not considered to be inefficient or wasteful, and impacts would be less than significant.

### **Natural Gas**

*Construction.* Natural gas consumption is not anticipated during construction of the Project. Fuels used for construction would generally consists of diesel and gasoline, which are discussed in the next subsection. Any amounts of natural gas that may be consumed during project construction would be nominal and would have a negligible contribution to the project's overall energy consumption.

*Operational.* The project proposes an Amendment to the Parkside Specific Plan proposes to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). The Project does not include construction of an actual development. However, during hypothetical operation of the Project, a residential and commercial use would require natural gas consumption for various purposes, such as building heating and cooling. Any future structure developed on the site would be built to the 2016 Title 24 CALGreen efficiency requirements or the code in effect at the time of development. In addition, measures will be applied based on the information contained in the County's GHG DRP checklist. These measures include, but are not limited to: enhanced wall, attic, and window insulation; high efficiency water heater, and optimized building orientation. Although natural gas consumption would increase at the site under implementation of the Project, the building envelope, HVAC, lighting, and other systems, would be designed to maximize energy performance. The project would be subject to statewide mandatory energy requirements as outlined in the CALGreen Code. In addition, the project would implement additional measures, as detailed in the GHG reduction measures screening table, that would further reduce natural gas consumption. Natural gas that would be consumed by the Project would also be subject to the cap-and-trade regulation. For these reasons, the natural gas that would be consumed by the project is not considered to be inefficient or wasteful, and impacts would be less than significant.

### **Diesel and Gasoline Fuel**

*Construction.* Diesel and gasoline fuels, also referred to as petroleum in this subsection, would be consumed throughout construction of the Project. Fuel consumed by construction equipment would be the primary energy resource consumed over the course of construction, and vehicle miles traveled (“VMT”) associated with the transportation of construction materials (e.g., deliveries to the site) and worker trips to and from the site would also result in petroleum consumption. Whereas on-site, heavy-duty construction equipment and delivery trucks would predominantly use diesel fuel, construction workers would generally rely on gasoline-powered vehicles. Any future development would be required to comply with CARB's Airborne Toxic Control Measures, which restricts heavy-duty diesel vehicle idling to five minutes. Since petroleum use during construction would be temporary and required to conduct development activities, it would not be wasteful or inefficient, and impacts would be less than significant.

*Operational.* Fuel consumption associated with development pursuant to the Project's operational phase would primarily be attributable to workers commuting to and from the Project and the operation of

large, diesel-powered trucks (e.g., semi-trucks) needed to transport goods. Over the lifetime of the Project, the fuel efficiency of the vehicles being used by the employees is expected to increase. As such, the amount of petroleum consumed as a result of vehicular trips to and from the Project site during operation is anticipated to decrease over time. There are numerous regulations in place that require and encourage fuel efficiency. For example, CARB has adopted an approach to passenger vehicles by combining the control of smog-causing pollutants and GHG emissions into a single, coordinated package of standards. The approach also includes efforts to support an accelerate the number of plug-in hybrids and ZEVs in California. In addition, per the requirements identified in SB 375, CARB adopted a regional goal for the SCAG region of reducing per-capita GHG emissions from 2005 levels by 8 percent by 2020 and 19 percent by 2035 for light-duty passenger vehicles. Accordingly, operation of the Project is expected to decrease the amount of petroleum it consumes in the future due to advances in fuel economy. Although the Project would increase petroleum use in the region during construction and operation, the use would be a small fraction of the statewide use and, due to efficiency increases, would diminish over time. As such, petroleum consumption associated with the Project would not be considered inefficient or wasteful and would result in a less-than-significant impact.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?**

Discussion of Effects: The Project would not conflict with or obstruct a state or local plan adopted for the purposes of increasing the amount of renewable energy or energy efficiency. The California Title 24 Building Code contains energy efficiency standards for residential and commercial buildings. These standards address electricity and natural gas efficiency in lighting, water, heating, and air conditioning, as well as the effects of the building envelope (e.g., windows, doors, walls and roofs, etc.) on energy consumption. As described above, the Project would be required to comply with the 2019 Title 24 CALGreen standards and would implement additional measures as identified in the County's GHG DRP checklist. Since the Project would comply with applicable State standards and adhere to the County's energy reductions measures identified in the GHG Emissions Reduction Plan, the Project would not conflict with nor obstruct a state or local plan for renewable energy or energy efficiency. This impact would be less than significant.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**7. GEOLOGY & SOILS.** Would the project:

**a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury or death involving:**

**i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.**

Discussion of Effects: There are no active faults known on the site and the project site is located outside the Fault Rapture Hazard Zone (formerly Alquist-Priolo Zone). The Certified TOP EIR (Section 5.7/Figure 5.7-2) identifies eight active or potentially active fault zones near the City. Given that the closest fault zone is located more than ten miles from the project site, fault rupture within the project area is not likely. All development will comply with the Uniform Building Code seismic design standards to reduce geologic hazard susceptibility. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**ii. Strong seismic ground shaking?**

Discussion of Effects: There are no active faults known on the site and the project site is located outside the Fault Rapture Hazard Zone (formerly Alquist-Priolo Zone). The Certified TOP EIR (Section 5.7/Figure 5.7-2) identifies eight active or potentially active fault zones near the City. The closest fault zone is located more than ten miles from the project site. The proximity of the site to the active faults will result in ground shaking during moderate to severe seismic events. All construction will comply with the California Building Code, the Ontario Municipal Code, The Ontario Plan and all other ordinances adopted by the City related to construction and safety. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**iii. Seismic-related ground failure, including liquefaction?**

Discussion of Effects: As identified in the Certified TOP EIR (Section 5.7), groundwater saturation of sediments is required for earthquake induced liquefaction. In general, groundwater depths shallower than 10 feet to the surface can cause the highest liquefaction susceptibility. Depth to ground water at the project site during the winter months is estimated to be between 250 to 450 feet below ground surface. Therefore, the liquefaction potential within the project area is minimal. Implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**iv. Landslides?**

Discussion of Effects: The project would not expose people or structures to potential adverse effects, including the risk of loss, injury, or death involving landslides because the relatively flat topography of the project site (less than 2 percent slope across the City) makes the chance of landslides remote. The allowed residential and commercial uses within the Parkside Specific Plan will not create greater landslide potential impacts than were identified in the Certified Parkside Specific Plan EIR or TOP EIR. Implementation of TOP EIR strategies, Uniform Building Code and Ontario Municipal Code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR or TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Result in substantial soil erosion or the loss of topsoil?**

Discussion of Effects: Amendment to the Parkside Specific Plan proposes to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles, will not create greater erosion impacts than were identified in the Certified Parkside Specific Plan EIR and TOP EIR. Impacts will be less than significant with mitigation.

The project will not result in significant soil erosion or loss of topsoil because of the previously disturbed nature of the Project site and the limited size and scope of the Project. Grading increases the potential for erosion by removing protective vegetation, changing natural drainage patterns, and constructing slopes. However, compliance with the California Building Code and review of grading plans by the City Engineer will ensure no significant impacts will occur. In addition, the City requires an erosion/dust control plan for projects located within this area. Implementation of a NPDES program, the Environmental Resource Element of the Policy Plan (General Plan) strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?**

**Discussion of Effects:** Amendment to the Parkside Specific Plan proposes to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles, will not create greater landslide potential impacts than were identified in the Certified Parkside Specific Plan EIR and TOP EIR. In addition, the associated projects would not result in the location of development on a geologic unit or soil that is unstable, or that would become unstable because as previously discussed, the potential for liquefaction and landslides associated with the project is less than significant. TOP EIR (Section 5.7) indicates that subsidence is generally associated with large decreases or withdrawals of water from the aquifer. The project would not withdraw water from the existing aquifer. Further, implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**d. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?**

**Discussion of Effects:** The majority of Ontario, including the project site, is located on alluvial and eolian soil deposits. These types of soils are not considered to be expansive. Therefore, no adverse impacts are anticipated.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**e. Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?**

**Discussion of Effects:** The area is served by the local sewer system and the use of alternative systems is not necessary. There will be no impact to the sewage system.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?**

**Discussion of Effects:** The City of Ontario is underlain by deposits of Quaternary and Upper-Pleistocene sediments deposited during the Pliocene and early Pleistocene time, Quaternary Older Alluvial sediments may contain significant, nonrenewable, paleontological resources and are, therefore, considered to have high sensitivity at depths of 10 feet or more below ground surface. In addition, the Certified TOP EIR (Section 5.5) indicates that one paleontological resource has been discovered in the City. Moreover, Results of the paleontological resources records search through the Natural History Museum of Los Angeles County (NHMLAC) indicate that there are no known vertebrate fossil localities or unique geological features that have been previously identified within the Project area or within a one-mile radius. The results of the literature review and the search at the NHMLAC indicate that the Project site has surficial sediments

composed of younger Quaternary Alluvium, derived as alluvial fan deposits from the San Gabriel Mountains to the north or as dune sands. These deposits typically do not contain significant vertebrate fossils, at least in the uppermost layers, but they may be underlain by older sedimentary materials at estimated depths greater than 9 feet (McLeod 2019). Therefore, it is concluded that the proposed Project will not impact paleontological resources or unique geological features and as such no mitigation measures are recommended. While no adverse impacts are anticipated, standard conditions have been imposed on the Project that in the event of unanticipated paleontological resources are identified during excavation, construction activities will not continue or will be moved to other parts of the Project site and a qualified paleontologist shall be contacted to determine significance of these resources. If the find is determined to be significant, avoidance or other appropriate measures shall be implemented.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**8. GREENHOUSE GAS EMISSIONS.** Would the project:

**a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?**

**Discussion of Effects:** The subject site was previously analyzed by the Certified TOP EIR as residential, open space parkland and commercial uses. Additionally, the impact of buildout of The Ontario Plan on the environment due to the emission of greenhouse gases ("GHGs") was analyzed in the Certified Parkside Specific Plan EIR. According to the EIR, this impact would be significant and unavoidable (Recirculated Portions of the Ontario Plan Draft Environmental Impact Report, p. 2-118.) This EIR was certified by the City on January 27, 2010, at which time a statement of overriding considerations was also adopted for The Ontario Plan's significant and unavoidable impacts, including that concerning the emission of greenhouse gases. Amending the Specific Plan Planning Areas will not create significantly greater impacts than were identified in the Certified TOP EIR or Parkside Specific Plan EIR. All development projects proposed within the Specific Plan will be required to complete a sample GHG Reduction Measures Screening Threshold Table, which provides guidance in measuring the reduction of greenhouse gas ("GHG") emissions attributable to certain design and construction measures incorporated into development projects. The analysis, methodology, and significance determination (thresholds) are based upon the City's Climate Action Plan ("CAP"), which includes GHG emission inventories (2008 and 2020 forecasts), a year 2020 emission reduction target, the goals and policies to reach the target, together with the Addendum prepared for the CAP. The Screening Table assigns points for each option incorporated into a project as mitigation or a project design feature (collectively referred to as "feature"). The point values correspond to the minimum emissions reduction expected from each feature. The menu of features allows maximum flexibility and options for how development projects can implement the GHG reduction measures. The point levels are based upon improvements compared to 2008 emission levels of efficiency. Projects that garner at least 100 points will be consistent with the reduction quantities anticipated in the City's CAP. As such, those projects that garner a total of 100 points or greater would not require quantification of project specific GHG emissions. Consistent with CEQA Guidelines, such projects would be determined to have a less than significant individual and cumulative impact for GHG emissions. As shown in the Project GHG Reduction Measures Screening Table, the Project garners a total of 103 points, and is therefore consistent with the reduction quantities anticipated in the City's CAP. Therefore, quantification of Project-specific GHG emissions is not required.

Additionally, pursuant to Public Resources Code Section 21083.3, this impact need not be analyzed further, because (1) the proposed project would result in an impact that was previously analyzed in the Certified TOP EIR, which was certified by the City; (2) the proposed project would not result in any greenhouse gas impacts that were not addressed in the Certified TOP EIR; (3) the proposed project is consistent with The Ontario Plan. The proposed impacts of the project were already analyzed in the Certified TOP EIR and the project will be built to current energy efficient standards. Potential impacts of project implementation will be less than significant with mitigation already required under the Certified TOP EIR and current energy efficiency standards. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**Mitigation Required:** No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to Parkside Specific Plan EIR and TOP EIR analyses are necessary. The mitigation measures adopted as part of TOP EIR adequately address any potential significant impacts and there is no need for any additional mitigation measures. The City has reviewed the emission reduction measures and concepts in The Ontario Plan EIR's MM 6-2 and 6-3, and has determined that the following actions apply and shall be undertaken by the applicant in connection with the project: energy efficient design, efficient irrigation systems, electric vehicle charging stations, and compliance with Title 24 of the California Code of Regulations.

**b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?**

**Discussion of Effects:** The subject site was previously analyzed by the Certified TOP EIR for residential, parkland and commercial uses. The proposed Project is consistent with The Ontario Plan Goal ER 4 of improving air quality by, among other things, implementation of Policy ER4-3, regarding the reduction of greenhouse gas emissions in accordance with regional, State, and federal regulations. In addition, the proposed Project is consistent with the policies outlined in Section 5.6.4 of the Environmental Impact Report for The Ontario Plan, which aims to reduce the City's contribution of greenhouse gas emissions at build-out by fifteen (15 percent), because the project is upholding the applicable City's adopted mitigation measures as represented in 6-1 through 6-6 and energy efficient design, efficient irrigation systems, electric vehicle charging stations, and compliance with Title 24 of the California Code of Regulations. The Project is consistent with the City's Climate Action Plan. Therefore, the proposed Project does not conflict with an applicable plan, policy or regulation adopted for the purpose of reducing emissions of greenhouse gases.

**Mitigation Required:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**9. HAZARDS & HAZARDOUS MATERIALS.** Would the project:

**a. Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials?**

**Discussion of Effects:** The subject site was previously analyzed by the Parkside Specific Plan EIR for residential, open space parkland and commercial uses. The project is not anticipated to involve the transport, use or disposal of hazardous materials during either construction or project implementation. Therefore, no adverse impacts are anticipated. However, in the unlikely event of an accident, implementation of the strategies included in The Ontario Plan will decrease the potential for health and safety risks from hazardous materials to a less than significant impact.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?**

**Discussion of Effects:** The subject site was previously analyzed by the Certified Parkside Specific Plan EIR and TOP EIR for residential, open space parkland and commercial uses. The proposed project does not include the use of hazardous materials or volatile fuels. In addition, there are no known stationary commercial or industrial land uses within close proximity to the subject site, which use/store hazardous materials to the extent that they would pose a significant hazard to visitors/occupants to the subject site, in the event of an upset condition resulting in the release of a hazardous material.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances or waste within one-quarter mile of an existing or proposed school?**

Discussion of Effects: The proposed project does not include the use, emissions or handling of hazardous or acutely hazardous materials, substances, or waste. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?**

Discussion of Effects: The proposed project site is not listed on the hazardous materials site compiled pursuant to Government Code Section 65962.5. Therefore, the project would not create a hazard to the public or the environment and no impact is anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**e. For a project located within the safety zone of the airport land use compatibility plan for ONT or Chino Airports, would the project result in a safety hazard for people residing or working in the project area?**

Discussion of Effects: The proposed project was reviewed and found to be located within the Airport Influence Area of Ontario International Airport ("ONT") and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan ("ALUCP") for ONT. The subject site is required to file and record an Avigation Easement with the Ontario International Airport Authority prior to obtaining a Certificate of Occupancy. The site is located within the airport influence area but outside the airport safety zones. The proposed Specific Plan Amendment will continue to allow residential uses within Planning Areas 1-4, and 17-19 is compatible with the ALUCP for ONT. Therefore, any impacts would be reduced to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?**

Discussion of Effects: The City's Safety Element, as contained within The Ontario Plan, includes policies and procedures to be administered in the event of a disaster. The Ontario Plan seeks interdepartmental and inter-jurisdictional coordination and collaboration to be prepared for, respond to and recover from every day and disaster emergencies. In addition, the project will comply with the requirements of the Ontario Fire Department and all City requirements for fire and other emergency access. Because the project is required to comply with all applicable City codes, any impacts would be reduced to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**g. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?**

Discussion of Effects: The project site is not located in or near wildlands. Therefore, no impacts are anticipated.



Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**10. HYDROLOGY & WATER QUALITY.** Would the project:

**a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?**

Discussion of Effects: The grading and drainage of the Parkside Specific Plan area shall be designed to detain, filter, and treat surface runoff, in a manner and combination which is practical, to comply with the most recent requirements of the San Bernardino County NPDES Stormwater Program's Water Quality Management (WQMP) for significant new development projects. The objective of the WQMP for this project is to minimize the detrimental effects of urbanization on the beneficial uses of receiving waters, including effects caused by increased pollutants and changes in hydrology. These effects may be minimized through the implementation of site designs that reduce runoff and pollutant transport by minimizing impervious surfaces and maximizing on-site infiltration, Source Control Best Management Practices (BMPs) and/or either on-site structural Treatment Control BMP's, or participation in regional or watershed-based Treatment Control BMPs. Prior to the issuance of a grading or construction permit, a Storm Water Pollution Prevention Plan (SWPPP) will also be prepared. The SWPPP will be prepared to comply with the California State Water Resources Control Board's (State Water Board) current, "General Permit to Discharge Storm Water Associated with Construction Activity" and the current Areawide Urban Storm Water Runoff (Regional NPDES Permit). The SWPPP will identify and detail all appropriate Best Management Practices (BMPs) to be implemented or installed during construction of the project. In addition to the preparation of a SWPPP for construction related activities, and as part of the approval of any grading plans within the Specific Plan Area, the applicant will be required to submit a Water Quality Management Plan (WQMP) on the regional model form provided by the City. The WQMP shall identify and detail all Site Design, Source Control, and Treatment Control BMPs to be implemented or installed at this site in order to reduce storm water pollutants and site runoff. All Priority Land Use (PLU) areas within the Specific Plan Area shall comply with the statewide Trash Provisions adopted by the State Water Resources Control Board (SWRCB) and trash requirements in the most current San Bernardino County Area-Wide MS4 Permit. Drainage from such as a Debris Separating Baffle Box (DSBB), Continuous Deflective System (CDS), or an equivalent hydrodynamic separator which has been approved by the SWRCB. The Specific Plan area is located within the sub-watershed area of a regional storm water runoff treatment facility. This regional treatment facility serves as an alternative to complete on-site treatment of all pollutants of concern. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?**

Discussion of Effects: The subject site was previously analyzed by the Certified Parkside Specific Plan EIR for residential, open space parkland and commercial uses. No increases in the current amount of water flow to the project site are anticipated, and the proposed project will not deplete groundwater supplies, nor will it interfere with recharge. The water use associated with the proposed use of the property was included in the Certified Parkside Specific Plan EIR analysis. The development of the site will require the grading of the site and excavation is expected to be less than three feet and would not affect the existing aquifer, estimated to be about 230 to 250 feet below the ground surface. No adverse impacts are anticipated.

Mitigation: No additional mitigation required.

**c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:**

**i. Result in substantial erosion or siltation on- or off-site?**

**Discussion of Effects:** It is not anticipated that the Project would alter the drainage pattern of the site or area, in a manner that would result in erosion, siltation or flooding on-or-off site, nor will the proposed Project increase the erosion of the subject site or surrounding areas. The existing drainage pattern of the site will not be altered, and it will have no significant impact on downstream hydrology. Stormwater generated by the project will be discharged in compliance with the statewide NPDES General Construction Activities Stormwater Permit and San Bernardino County MS4 permit requirements. With the full implementation of a Storm Water Pollution Prevention Plan developed in compliance with the General Construction Activities Permit requirements, the Best Management Practices included in the SWPPP, and a stormwater monitoring program would reduce any impacts to below a level of significance. No streams or streambeds are present on the site. No changes in erosion off-site are anticipated.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**ii. Substantially increase the rate or amount of surface runoff water in a manner which would result in flooding on- or off-site?**

**Discussion of Effects:** The proposed Project is not anticipated to increase the flow velocity or volume of storm water runoff to cause environmental harm from the site and will not create a burden on existing infrastructure. Furthermore, with the implementation of an approved Water Quality Management Plan developed for the site, in compliance with the San Bernardino County MS4 Permit requirements, stormwater runoff volume shall be reduced to below a level of significance.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**iii. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?**

**Discussion of Effects:** It is not anticipated that the project would create or contribute runoff water that would exceed the capacity of existing or planned stormwater drainage systems or create or contribute stormwater runoff pollutants during construction and/or post-construction activity. The stormwater flows will enter an existing storm pipe in Jurupa Street. Pursuant to the requirements of The Ontario Plan, the City's Development Code, and the San Bernardino County MS4 Permit's "Water Quality Management Plan" ("WQMP"), individual developments must provide site drainage and WQMP plans according to guidelines established by the City's Engineering Department. If master drainage facilities are not in place at the time of project development, then standard engineering practices for controlling post-development runoff may be required, which could include the construction of on-site storm water detention and/or retention/infiltration facilities. Therefore, no impacts are anticipated.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**iv. Impede or redirect flood flows?**

**Discussion of Effects:** Urbanization in the areas surrounding the project site have resulted in increased responsiveness of the basin to rainfall. The increase in impervious surfaces such as roofs, roads, and parking lots has resulted in a decrease in groundwater infiltration and larger storm surges. The Project site currently slopes southeast, and the existing drainage pattern is characterized by sheet flows that follow the slope to the northwest. The project site is not impacted by offsite flows. The project site is not located in a FEMA Firm Panel designated Flood Zone Risk, and according to the United States Fish and Wildlife Service National Wetlands Inventory ("NWI") no wetlands exist on the property. The Project could lead to the conversion of permeable surfaces to impermeable surfaces such as parking areas and building foundation areas. Any future development on the Project site would discharge onsite flows into an existing storm drain facility. As such, the proposed project would not impede or redirect flood flows. With adherence to existing federal, state, and local regulation no changes to the existing flood flows would occur.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**d. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?**

Discussion of Effects: Impacts associated with flooding are primarily related to the construction or placement of structures in areas prone to flooding including within an unprotected 100-year flood zone, and in areas susceptible to high tides, tsunamis, seiches, mudflows or sea level rise. Specifically, structures placed in flood prone areas, if flooded, would be damaged, and could subject people to injury or death. The National Flood Insurance Act of 1968 requires the identification of floodplain areas and establishment of flood-risk zones within those areas. FEMA administers the programs and coordinates with communities to establish effective floodplain management standards. According to FEMA, the Project is not located in a known floodplain. Furthermore, this area is not known to flood and is not typically subjected to flooding. The Project site is not located in a floodplain as shown in Figure S-2 of TOP. The Project site is dominated by Agricultural fallow fields and does not contain any vegetation associated with riparian features. No wetlands have been mapped on the project site according to the NWI. According to the FEMA, the Project is not located in an area that is subject to flood hazard, tsunami, or seiche zones. The project site is located over 60 miles east of the Pacific Ocean and is not located in a mapped tsunami zone. Therefore, the project would not have a significant risk of flood hazard, tsunami, seiche zones, release of pollutants due to project inundation.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**e. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?**

Discussion of Effects: The Regional Water Quality Control Board's Basin Plan is designed to preserve and enhance water quality and protect the beneficial uses of all regional waters. Specifically, the Basin Plan (i) designates beneficial uses for surface and ground waters, (ii) sets narrative and numerical objectives that must be attained or maintained to protect the designated beneficial uses and conform to the state's anti-degradation policy, and (iii) describes implementation programs to protect all waters in the region. Development allowed by the Project would be required to adhere to requirements of the water quality control plan, including all existing regulation and permitting requirements. This would include the incorporation of best management practices ("BMPs") to protect water quality during construction and operational periods. Development of the Project would be subject to all existing water quality regulations and programs, as described in the regulatory section above, including all applicable construction permits. Existing General Plan policies related to water quality would also be applicable to the Project. Implementation of these policies, in conjunction with compliance with existing regulatory programs, would ensure that water quality impacts related to the Project would be less than significant.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**11. LAND USE & PLANNING.** Would the project:

**a. Physically divide an established community?**

Discussion of Effects: The project site is in an area that is currently being developed with residential and future commercial land uses. This project will be of similar design and size to surrounding development. No adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified

Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?**

Discussion of Effects: The subject site was previously analyzed by the Certified Parkside Specific Plan EIR residential, parkland and commercial uses. The Amendment to the Parkside Specific Plan (File No. PSPA19-007) proposes to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7) and will not create greater impacts than were identified in the Certified Parkside Specific Plan EIR. The proposed project does not interfere with any policies for environmental protection. As such, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**12. MINERAL RESOURCES.** Would the project:

**a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?**

Discussion of Effects: The Project site is located within a mostly developed area surrounded by residential and future commercial uses. There are no known mineral resources in the area. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?**

Discussion of Effects: There are no known mineral resources in the area. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**13. NOISE.** Would the project result in:

**a. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?**

Discussion of Effects: The subject site was previously analyzed by the Certified Parkside Specific Plan EIR for residential, parkland and commercial uses. In addition, the Project will not expose people to or generate noise levels in excess of standards as established in the Certified TOP EIR (Section 5.12). The subject site is surrounded on the north, east, and south sides by residential new and future residential uses, to the east of the subject site is the Cucamonga Flood Control channel, and the subject site is not within the landing approach of the Ontario International Airport or Chino Airport. No additional analysis will be required at the time of site development review.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Generation of excessive groundborne vibration or groundborne noise levels?**

Discussion of Effects: The subject site was previously analyzed by the Certified Parkside Specific Plan EIR for residential, parkland and commercial uses and the uses associated with this project normally do not induce groundborne vibrations. As such, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**c. For a project located within the vicinity of a private airstrip or the noise impact zones of the airport land use compatibility plan for ONT and Chino Airports, would the project expose people residing or working in the project area to excessive noise levels?**

Discussion of Effects: The proposed Amendment was reviewed and found to be located within the Airport Influence Area of Ontario International Airport ("ONT") and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan ("ALUCP") for ONT. The project site is located outside of the Safety, Noise Impact and Airspace Protection Zones. A portion of the project site is located within the 65-70 dB CNEL Noise Impact Zones; however, the proposed zone change is a compatible land use. In addition, the project site lies outside the boundaries of the Chino Airport Influence Area. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**14. POPULATION & HOUSING.** Would the project:

**a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?**

Discussion of Effects: The subject site was previously analyzed by the Certified Parkside Specific Plan EIR for residential, open space parkland and commercial uses and the Amendment to the Parkside Specific Plan is consistent with General Plan land use designations and would not induce significant population growth. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?**

Discussion of Effects: The project site does not contain existing housing. Changing the permitted zoning on the site will not create existing housing impacts.

Mitigation: No additional mitigation required. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**PUBLIC SERVICES.** Would the project:

**c. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:**

**i. Fire protection?**

Discussion of Effects: The site is in a developed area currently served by the Ontario Fire Department. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. In addition, permits have issued for the construction of the City of Ontario Fire Station 9, which is in Planning Area 20 of the Specific Plan. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**ii. Police protection?**

Discussion of Effects: The site is in a developed area, currently served by the Ontario Police Department. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**iii. Schools?**

Discussion of Effects: Upon development, the Project will be required to pay school fees as prescribed by state law prior to the issuance of building permits. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**iv. Parks?**

Discussion of Effects: The site is in a developed area, currently served by the City of Ontario. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. In addition, TOP Policy Plan (Policy PR1-5) has established a standard of 5 acres of parkland (public and private) per 1,000 residents, with a minimum of 2 acres of developed private park space per 1,000 residents (Policy PR1-6). Private parks are required to be approximately a quarter mile walking/biking distance from each residence. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**v. Other public facilities?**

Discussion of Effects: The site is in a developed area, currently served by the City of Ontario. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**RECREATION.** Would the project:

**d. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?**

**Discussion of Effects:** This project is not proposing any new housing or large employment generator that would cause an increase in the use of neighborhood parks or other recreational facilities. In addition, TOP Policy Plan (Policy PR1-5) has established a standard of 5 acres of parkland (public and private) per 1,000 residents, with a minimum of 2 acres of developed private park space per 1,000 residents (Policy PR1-6). Private parks are required to be approximately a quarter mile walking/biking distance from each residence. No impacts are anticipated.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**e. Does the project include recreational facilities or require the construction or expansion of recreational facilities that have an adverse physical effect on the environment?**

**Discussion of Effects:** This project is not proposing any new housing or large employment generator that would require the construction of neighborhood parks or other recreational facilities. No impacts are anticipated.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**15. TRANSPORTATION.** Would the project:

**a. Conflict with a program plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?**

**Discussion of Effects:** The proposed Amendment to the Parkside Specific Plan EIR will not create greater impacts than were identified in the certified the Parkside Specific Plan EIR. The proposed Amendment will not increase the residential units (Total 1,1947) and commercial square footage (115,000 SF) that was analyzed in the Parkside Specific Plan EIR. In addition, the number of vehicle trips per day will not increase above the established thresholds within Table III-11-E; *Project Trip Generation* of the Parkside Specific Plan EIR. Therefore, the traffic impacts will be consistent with and less than the traffic impacts projected and analyzed under the Certified Parkside Specific Plan EIR. The project will not create a substantial increase in the number of vehicle trips, traffic volume or congestion at intersections. Less than significant impacts are anticipated.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?**

**Discussion of Effects:** CEQA Guidelines Section 15064.3 subdivision (b) has been included in the 2018 CEQA Guidelines as part of the implementation of SB 743 which requires local jurisdictions to use Vehicle Miles Travelled (VMT) instead of Level of Service (LOS) methodologies for the purpose of determining the significance of traffic impacts under CEQA. Also, as part of the implementation of SB 743 local jurisdiction are given until July 1, 2020 to develop and implement thresholds of significance criteria and methodologies for evaluating VMT under the new SB 743 requirements. The City of Ontario has not yet established a VMT analysis threshold or analysis methodology. Therefore, impacts with respect to CEQA Guidelines Section 15064.3(b) are less than significant.

**Mitigation:** No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?**

Discussion of Effects: The Project is in an area that is being developed. Street improvements are along the Archibald Avenue and Eucalyptus complete, and no alterations are proposed for adjacent intersections or arterials. Each proposed development within the Specific Plan will be required to construct certain improvements to the streets and intersection per the circulation component of the Specific Plan. The Project will, therefore, not create a substantial increase in hazards due to a design feature. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**d. Result in inadequate emergency access?**

Discussion of Effects: Development of the Project will be designed to provide access for all emergency vehicles and will therefore not create an inadequate emergency access. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**e. Result in inadequate parking capacity?**

Discussion of Effects: The Amendment to the Parkside Specific Plan will not affect the parking capacity in the Specific Plan. Future development of the site will be required to meet parking standards established by the Ontario Development Code and will therefore not create an inadequate parking capacity. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**16. TRIBAL CULTURAL RESOURCES.** Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

**a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?**

Discussion of Effects: The Certified Parkside Specific Plan EIR (Section 4 Culture Resources, page III-4-6 &7) indicates no archeological sites or resources have been recorded in the City with the Archeological Information Center at San Bernardino County Museum. The NAHC Sacred Lands File search also failed to indicate archaeological resources or artifacts associated with Tribal Cultural Resources (TCRs) within the Project site. The Project site has been highly disturbed by modern human activities to with agricultural production since the 1950's. However, as part of TOP EIR Mitigation Measure 5-4, prior to the issuance of grading permits for a Specific Plan or project that requires a General Plan amendment which the CEQA document defines cultural resource mitigation for potential tribal resources, the project applicant shall contact the designated tribe(s) to notify them of the grading, excavation, and monitoring program. The applicant shall coordinate with the City of Ontario and the tribal representative(s) to develop mitigation measures that address the designation, responsibilities, and participation of tribal monitors during grading, excavation, and ground-disturbing activities; scheduling; terms of compensation; and treatment and final disposition of any cultural resources, sacred sites, and human remains discovered on the site. The City of Ontario shall be the final arbiter of the conditions for projects within the City's jurisdiction. Therefore, it is concluded that the proposed Project will not impact Tribal Cultural Resources or Native America artifacts relating to TCRs and as such, no mitigation measures are recommended.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified



Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.**

Discussion of Effects: The subject site is not listed in the California Register of Historic Resources. It is anticipated that during the application process the Lead Agency will notify the tribes of the proposed Specific Plan Amendment and will commence AB 52 Consultations as specified in the regulations. In addition, the results of the records research compiled from the CHRIS-SCCIC, the Scared Lands File Search (commissioned through the NAHC) failed to indicate known Tribal Cultural Resources (“TCR”) within the Project boundaries or within a one-mile radius of the Project area as specified in Public Resources Code (“PRC”): 210741, 5020.1(k), or 5024. Moreover, there was no indication of known TCRs within the Project site or within a one-mile radius of the Project Area. No impacts are anticipated through Project implementation.

Mitigation: No new mitigation measures are required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

#### **17. UTILITIES AND SERVICE SYSTEMS.** Would the project:

**a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?**

Discussion of Effects: The proposed Project is served by City of Ontario water system and has a 12-inch water line available for connection in Archibald Avenue adequate water supply for the Project. The proposed Project is served by the City of Ontario sewer system, which has a 36-inch trunk sewer line available for Archibald Avenue, and which has waste treated by the Inland Empire Utilities Agency at the RP-5 treatment plant. RP-5 is not at capacity and future development of this Project site will not cause RP-5 to exceed capacity. The Project will therefore not require the construction of new water or wastewater treatment facilities, or the expansion of existing facilities. No impacts are anticipated.

As discussed in the energy section above, the Project will have less than significant impacts with regard to electric power and natural gas. In addition, the Project will not have an impact on telecommunications facilities.

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Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years? In making this determination, the City shall consider whether the project is subject to the water supply assessment requirements of Water Code Section 10910, et seq. (SB 610), and the requirements of Government Code Section 664737 (SB 221).**

Discussion of Effects: The Project site is served by the City of Ontario water system. There is currently sufficient water supply available to the City of Ontario to serve this Project as per the findings of TOP EIR. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**c. Result in a determination by the wastewater treatment provider that serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?**

Discussion of Effects: The Project site is served by the City of Ontario sewer system, which has waste treated by the Inland Empire Utilities Agency at the RP-5 treatment plant. RP-5 is not at capacity and future development of this project site will not cause RP-5 to exceed capacity. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**d. Generate solid waste in excess of state or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?**

Discussion of Effects: City of Ontario serves the Project site. Currently, the City of Ontario contracts with a waste disposal company that transports trash to a landfill with sufficient capacity to handle the City's solid waste disposal needs. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to analyses are necessary.

**e. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?**

Discussion of Effects: This Project complies with federal, state, and local statutes and regulations regarding solid waste. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**18. WILDFIRE.** If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:

**a. Substantially impair an adopted emergency response plan or emergency evacuation plan?**

Discussion of Effects: Wildfire impacts were not analyzed in the Certified Parkside Specific Plan EIR. A discussion of potential wildfire impacts is provided herein.

The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?**

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?**

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?**

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**19. MANDATORY FINDINGS OF SIGNIFICANCE.**

**a. Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?**

Discussion of Effects: The proposed Project does not have the potential to reduce wildlife habitat and threaten a wildlife species; therefore, no environmental impacts resulting from the Project are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**b. Does the project have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals?**

Discussion of Effects: The Project does not have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**c. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)**

Discussion of Effects: The Project does not have impacts that are cumulatively considerable.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

**d. Does the project have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly?**

Discussion of Effects: The Project does not have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

### **EARLIER ANALYSES**

*(Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or Negative Declaration. Section 15063(c)(3)(D)):*

- 1) Earlier Analyses Used. Identify earlier analyses used and state where they are available for review.
  - a) The Parkside Specific Plan EIR
  - b) The Ontario Plan Final EIR
  - c) The Parkside Specific Plan
  - d) The Ontario Plan (TOP)
  - e) City of Ontario Official Zoning Map
  - f) City of Ontario Development Code
  - g) Ontario International Airport Land Use Compatibility Plan
  - h) Ontario International Airport Land Use Compatibility Plan Negative Declaration (SCH 2011011081)

All documents listed above are on file with the City of Ontario Planning Department, 303 East "B" Street, Ontario, California 91764, (909) 395-2036.

- 2) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of, and adequately analyzed in, an earlier document pursuant to applicable legal standards.

### **MITIGATION MEASURES**

*(For effects that are "Less than Significant with Mitigation Incorporated," describe the mitigation measures, which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.)*

The Mitigation Measures contained in the Certified Parkside Specific Plan EIR adequately mitigate the impacts of the proposed Project. These mitigation measures are contained in the attached Mitigation Monitoring Program.

No additional mitigation beyond that previously imposed is required.

Exhibit A— Certified Parkside Specific Plan EIR Figure 1-3, Proposed Land Use Plan

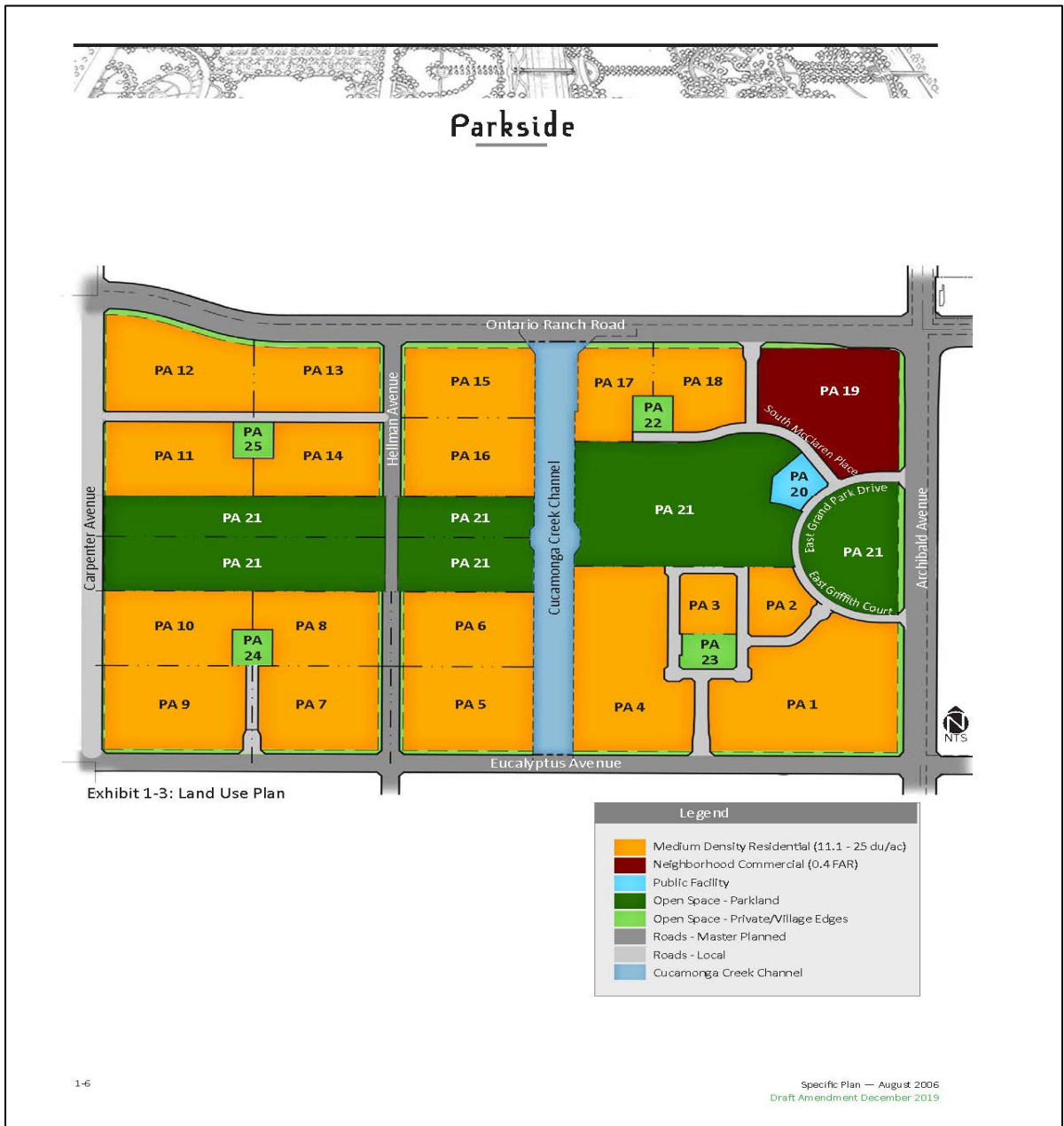


Exhibit B—2010 TOP Policy Plan (Exhibit LU-01) Land Use Plan

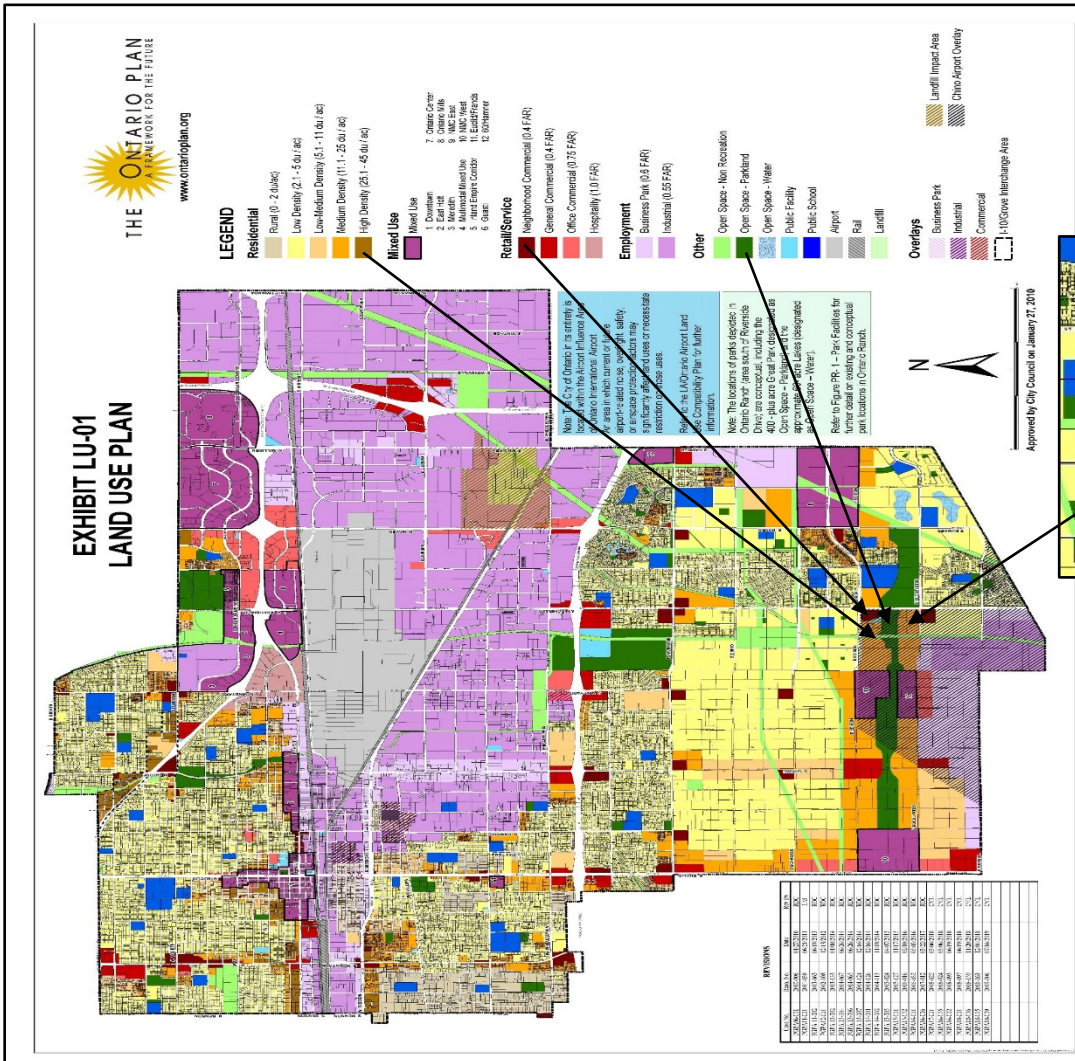


Exhibit C

***Mitigation Monitoring Program for  
Parkside Specific Plan EIR***

***(to follow this page)***

## **5.0 MITIGATION MONITORING PROGRAM**

**CITY OF ONTARIO, CALIFORNIA**

**Parkside Specific Plan  
SCH Number 2004011008**

**July 2006**

**Prepared for:**

City of Ontario  
303 East "B" Street  
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This statement is prepared in  
compliance with the California  
Environmental Quality Act



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## **INTRODUCTION**

### **CEQA Requirements**

The California Environmental Quality Act (CEQA) requires that when a public agency completes an environmental document that includes measures to mitigate or avoid significant environmental effects, the public agency must adopt a reporting or monitoring program for the changes to the project which it has adopted or made a condition of project approval in order to mitigate or avoid significant effects on the environment. The reporting or monitoring program must be designed to ensure compliance during project implementation (Public Resources Code Section 21081.6, CEQA Guidelines Section 15097).

The City of Ontario will coordinate monitoring of the implementation of all mitigation measures for the Parkside Specific Plan project. Monitoring will include: 1) verification that each mitigation measure has been implemented; 2) recordation of the actions taken to implement each mitigation measure; and 3) retention of records in the project file.

### **Program Objectives**

The objectives of the mitigation monitoring program for the Parkside Specific Plan project are:

- To provide assurance and documentation that mitigation measures are implemented as planned;
- To collect analytical data to assist the City in its determination of the effectiveness of the adopted mitigation measures;
- To make available to the public, upon request, the City 's record of compliance with project mitigation measures.

By including both monitoring and reporting provisions, the City of Ontario has voluntarily exceeded the minimum requirements of Public Resource Code Section 21081.6, which allows selection of monitoring or reporting, but does not require both.

### **Overview of the Project**

The Specific Plan consists of approximately 250 acres located within the 8,200-acre New Model Colony, and is bounded by Edison Avenue to the north, Archibald Avenue to the east, and Eucalyptus Avenue to the south. Cucamonga Creek flows in a southerly direction approximately through the center of the project area. The Specific Plan is an application for 430 single-family residential dwelling units, 1,517 multi-family residential units, 11.5 acres of commercial use, a 50-acre Great Park, and 6 acres of recreational trails.

More detailed information regarding the project is provided in the March 2006 Draft Environmental Impact Report related to this project.

## **Organization of the Mitigation Monitoring Program**

Introduction: Provides an overview of CEQA's monitoring and reporting requirements, program objectives, the project for which the program has been prepared, and the manner in which the mitigation monitoring program has been organized.

Description of Program: Describes the City of Ontario entities responsible for implementation of the mitigation monitoring program, the program scope, procedures for monitoring and reporting, public availability of documents, the process for making changes to the program, types of mitigation measures and the manner in which monitoring will be coordinated to ensure implementation of mitigation measures.

Mitigation Monitoring and Reporting Summary: Outlines the impacts and mitigation measures, responsible entities, and the timing for monitoring and reporting for each mitigation measure included in the program.

Report Preparation: Lists the individuals involved in development of this mitigation monitoring program.

## DESCRIPTION OF PROGRAM

### Mitigation Monitoring Procedures

This mitigation monitoring program delineates responsibilities for monitoring the project, but also allows responsible parties flexibility and discretion in determining the best manner of monitoring implementation. Monitoring procedures will vary according to the type of mitigation measure. The timing for monitoring and reporting is described in the monitoring and reporting summary table included as part of this program. Adequate monitoring consists of demonstrating that monitoring procedures took place and that mitigation measures were implemented.

In order to enhance the effectiveness of the monitoring program, the city will utilize existing systems where appropriate. For instance, with any major construction project, the city generally has at least one inspector assigned to monitor project construction. These inspectors are familiar with a broad range of regulatory issues and will provide first line oversight for much of the monitoring program.

### Reporting Procedures

A plan check review and construction inspection process will be utilized as the first line for much of the monitoring program, and will also serve to provide the background documentation for the reporting program.

Reporting consists of establishing a record that a mitigation measure is being implemented, and generally involves the following steps:

- Reporting forms are distributed to the appropriate responsible entity or its representative (as indicated in the summary form) or existing reporting processes are used for verification of compliance.
- Responsible entities or their representatives verify compliance by signing the monitoring and reporting form and/or documenting compliance using their own internal procedures when monitoring is triggered.
- Responsible entities or their representatives provide the city with verification that monitoring has been conducted and ensure, as applicable, that mitigation measures have been implemented.
- Construction inspectors prepare construction activities reports during the construction phase and provide project reports, as appropriate, to the city.

The city will also be responsible for assisting responsible entities and/or their representatives with reporting responsibilities to ensure that they understand their charge and complete their reporting procedures accurately and on schedule.

## **Public Availability**

All monitoring reporting forms, summaries, data sheets, and correction instructions related to the mitigation monitoring program for the Parkside Specific Plan project will be available for public review upon request at the City of Ontario Planning Department.

## **Program Changes**

Minor changes to the mitigation monitoring program, if required, will be made in accordance with CEQA and would be permitted after further review and approval by the city. Such changes could include reassignment of monitoring and reporting responsibilities and/or program redesign to make any appropriate improvements. No change will be permitted unless the mitigation monitoring and reporting program continues to satisfy the requirements of Public Resources Code Section 21081.6.

## **Implementation of Mitigation Measures Being Monitored**

In general, implementation of the mitigation monitoring program will require the following actions:

- Responsible entities or their representatives with reporting responsibilities will review the EIR, which provides general background information on the reasons for including specified mitigation measures.
- Problems or exceptions to compliance will be addressed by the City, as appropriate.
- Periodic meetings may be held during project implementation to report on compliance with mitigation measures.

**Mitigation Monitoring Program Matrix**

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
Agricultural Resources	The proposed project would conflict with existing agricultural uses.	<b>MM Ag 1:</b> In order to minimize conflicts between urban and agricultural land uses, each Specific Plan developed for properties within the NMC must comply with the Agricultural Overlay District requirements for urban development in proximity to existing agricultural operations. The proposed project shall establish a minimum 100-foot separation between active agricultural operations and new, non-agricultural development, or an equivalent easement that is approved by the City of Ontario.	Prior to construction.	Planning Department	Less than Significant	
Agricultural Resources	The proposed project would conflict with existing agricultural uses.	<b>MM Ag 2:</b> In order to minimize conflicts between urban and agricultural land uses, all residential units in the Parkside Specific Plan shall be provided with a deed disclosure, or similar notice, approved by the City Attorney regarding the proximity and nature, including odors, of neighboring agricultural uses.	Prior to opening of model homes	City Attorney	Less than Significant	
Agricultural Resources	The proposed project would result in the cancellation of Williamson Act contracts, loss of prime Farmland, loss of existing agricultural use, and provide	No feasible mitigation measures were found. See Section III-1 for complete analysis.	NA	NA	Significant	

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
						Signature	Date Comments
	infrastructure which might cause other ag. lands to convert.						
<b>Air Quality</b>	Emissions from project construction equipment.	<b>MM Air 1:</b> During construction, mobile construction equipment will be properly maintained at an offsite location, which includes proper tuning and timing of engines. Equipment maintenance records and equipment design specification data sheets shall be kept on-site during construction.	During construction.	Contractor	Significant		
<b>Air Quality</b>	Emissions from project construction equipment.	<b>MM Air 2:</b> During construction of the proposed improvements, all contractors will be advised not to idle construction equipment on site for more than ten minutes.	During construction.	Contractor	Significant		
<b>Air Quality</b>	Emissions from project construction equipment.	<b>MM Air 3:</b> Configure construction parking to minimize traffic interference.	During construction.	Contractor	Significant		
<b>Air Quality</b>	Emissions from project operation.	<b>MM Air 4:</b> Local transit agencies shall be contacted to determine bus routing in the project area that can accommodate bus stops at the project access points and the project shall provide bus passenger benches and shelters at these project access points.	Prior to approval of street improvement plans.	Specific Plan Developer and Engineering Department	Significant		
<b>Biological Resources</b>	Adversely affect any endangered or threatened species, or any species identified as a candidate, sensitive or	<b>MM Bio 1:</b> There may be a probability of owl colonization within the project site considering the presence of foraging habitat and previous records of presence. To ensure that no direct loss of individuals occurs, mitigation shall be completed prior to initiation of on-site grading activities for each development phase. A pre-construction survey for	Prior to grading permit	Planning Department	Less than significant		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
	<p>special status.</p> <p>According to the Habitat Evaluation conducted for the project site, there may be a probability of owl colonization prior to site construction due to their presence in the vicinity of the site.</p>	<p>resident burrowing owls will be conducted by a qualified biologist. The survey will be conducted 30 days prior to construction activities. If ground-disturbing activities are delayed or suspended for more than 30 days after the preconstruction survey, the site should be resurveyed for owls.</p> <p>If owls are determined to be present within the construction footprint, they will be captured and relocated. If non-breeding owls must be moved away from the disturbance area, passive relocation techniques will be used. The pre-construction survey and any relocation activity will be conducted in accordance with the CDFG Report on Burrowing Owl Mitigation, 1995. According to CDFG guidelines, mitigation actions will be conducted from September 1 to January 31, which is prior to the nesting season. However, burrowing owl nesting activity is variable, and as such the time frame will be adjusted accordingly. Should eggs or fledglings be discovered in any owl burrow, the burrow cannot be disturbed (pursuant to CDFG guidelines) until the young have hatched and fledged (matured to a stage that they can leave the nest on their own).</p> <p>Occupied burrows will not be disturbed during the nesting season (February 1 through August 31) unless a qualified biologist approved by the Department of Fish and Game verifies through non-</p>				



Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
		<p>invasive methods that either: a) the adult birds have not begun egg-laying and incubation; or b) the juveniles from the occupied burrows are foraging independently and are capable of independent survival. If a biologist is unable to verify one of the above conditions, then no disturbance shall occur within 300 feet of the burrowing owls nest during the breeding season to avoid abandonment of the young.</p> <p>Passive relocation can be used to exclude owls from their burrows (outside the breeding season or once the young are able to leave the nest and fly) by installing one-way doors in burrow entrances. These one-way doors allow the owl to exit the burrow, but not enter it. These doors should be left in place 48 hours to ensure owls have left the burrow. Artificial burrows should be provided nearby. The project area should be monitored daily for one week to confirm owl use of burrows before excavating burrows in the impact area. Burrows should be excavated using hand tools and refilled to prevent reoccupation. Sections of flexible pipe should be inserted into the tunnels during excavation to maintain an escape route for any animals inside the burrow.</p>				
<b>Biological Resources</b>	The proposed project will affect open foraging habitat.	<b>MM Bio 2:</b> The project proponent shall be required to pay City of Ontario open space mitigation fees. Fees collected will be used “to acquire and restore mitigation lands to offset impacts to species now living in the New Model Colony and impacts to	Prior to grading permit	Planning Department	Less than Significant	

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
		existing open space,” according to the City of Ontario Development Impacts Fee Calculation Report and the Settlement and general Release Agreement. Development is currently required to pay \$4,320 per acre. Therefore, the proposed project will pay approximately \$2,298,240 for open space acquisition based upon the current fee.				
<b>Biological Resources</b>	The proposed project will affect open foraging habitat.	<b>MM Bio 3:</b> While project impacts to individual raptor species were considered to be not significant, the following mitigation measure will also be incorporated in order to eliminate or reduce any potential impacts to raptors and/or migratory birds. Construction and/or removal of windrow trees will occur outside of the nesting season (February 1 through August 31). If tree removal activities must occur during the breeding season, the mitigation measure in MM Bio 4 shall be implemented.	Prior to grading permit	Planning Department	Less than Significant	
<b>Biological Resources</b>	Adversely affect any endangered or threatened species and any species identified as candidate, sensitive or special status through the loss of habitat.	<b>MM Bio 4:</b> If project construction activities involving heavy equipment and/or windrow tree removal are to occur during the nesting/breeding season (between February 1 <sup>st</sup> and August 31 <sup>st</sup> ) of potentially occurring sensitive bird species, a pre-construction field survey shall be conducted by a qualified biologist to determine if active nests of species protected by MBTA or CDFG are present in the construction zone or within a buffer of 500 feet. Pre-construction nesting/breeding surveys shall be conducted in all CDFG jurisdictional areas and within windrow trees. If no active nests are found during	Prior to issuance of grading permits	Planning Department	Less than significant	

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
						Signature	Date Comments
		<p>the survey, construction activities may proceed.</p> <p>If active nests are located during the pre-construction surveys, no grading, heavy equipment or tree removal activities shall take place within at least 500 feet of an active listed species or raptor nest, 300 feet of other sensitive bird nests (non-listed), and 100 feet of most common songbird nests.</p>					
<b>Cultural Resources</b>	The proposed project could affect unknown buried cultural resources.	<b>MM Cultural 1:</b> Should any cultural and/or archaeological resources be accidentally discovered during construction, construction activities shall be moved to other parts of the project site and a qualified archaeologist shall be contacted to determine the significance of these resources. If the find is determined to be an historical or unique archaeological resource, as defined in Section 15064.5 of the CEQA Guidelines, avoidance or other appropriate measures shall be implemented.	During construction	Planning Department	Less than significant		
<b>Cultural Resources</b>	The proposed project could affect unknown buried cultural resources.	<b>MM Cultural 2:</b> If human remains are uncovered at any time, all activities in the area of the find shall be halted by the developer or its contractor and the County Coroner shall be notified immediately pursuant to CA Health & Safety Code Section 7050.5 and CA PRC Section 5097.98. If the Coroner determines that the remains are of Native American origin, the Coroner shall proceed as directed in Section 15064.5(e) of the CEQA Guidelines.	During construction	Planning Department	Less than significant		
<b>Cultural</b>	The proposed	<b>MM Cultural 3:</b> Prior to the issuance	Prior to grading	Planning	Less than		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
Resources	project has the potential to affect unknown buried paleontological resources	<p>of grading permits, a qualified paleontologist shall be retained to prepare a Paleontological Resources Survey of the project site, for approval by the City, to determine the site specific potential of finding paleontological resources within the project site. If the approved Paleontological Resources Survey determines that it is unlikely that paleontological resources will be uncovered by earth-moving activities, grading and construction activities may proceed, subject to compliance with all other mitigation measures. However, if the approved Paleontological Resources Survey determines that it is likely that paleontological resources will be uncovered during earth-moving activities, a qualified paleontologist shall be retained to develop a Paleontological Resources Monitoring and Treatment Plan (PRMTP) for approval by the City. Following City approval of the PRMTP, grading and construction activities may proceed in compliance with the provisions of the approved PRMTP. The PRMTP shall include the following measures:</p> <ol style="list-style-type: none"> <li>a. Identification of those locations within the project site where paleontological resources are likely to be uncovered during grading.</li> <li>b. A monitoring program specifying the procedures for the monitoring of grading activities by a qualified paleontologist or qualified designee.</li> </ol>	permits	Department	significant	

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
		<p>c. If fossil remains large enough to be seen are uncovered by earth-moving activities, a qualified paleontologist or qualified designee shall temporarily divert earth-moving activities around the fossil site until the remains have been evaluated for significance and, if appropriate, have been recovered; and the paleontologist or qualified designee allows earth-moving activities to proceed through the site. If potentially significant resources are encountered, a letter of notification shall be provided in a timely manner to the City, in addition to the report (described below) that is filed at completion of grading.</p> <p>d. If a qualified paleontologist or qualified designee is not present when fossil remains are uncovered by earth-moving activities, these activities shall be stopped and a qualified paleontologist or qualified designee shall be called to the site immediately to evaluate the significance of the fossil remains.</p> <p>e. At a qualified paleontologist or qualified designee's discretion and to reduce any construction delay, a construction worker shall assist in removing fossiliferous rock samples to an adjacent location for temporary stockpiling pending eventual transport to a laboratory facility for processing.</p>				

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
		<p>f. A qualified paleontologist or qualified designee shall collect all significant identifiable fossil remains. All fossil sites shall be plotted on a topographic map of the project site.</p> <p>g. If the qualified paleontologist or qualified designee determines that insufficient fossil remains have been found after fifty percent of earthmoving activities have been completed, monitoring can be reduced or discontinued.</p> <p>h. Any significant fossil remains recovered in the field as a result of monitoring or by processing rock samples shall be prepared, identified, catalogued, curated, and accessioned into the fossil collections of the San Bernardino County Museum, or another museum repository complying with the Society of Vertebrate Paleontology standard guidelines. Accompanying specimen and site data, notes, maps, and photographs also shall be archived at the repository.</p> <p>i. Within 6 months following completion of the above tasks, a qualified paleontologist or qualified designee shall prepare a final report summarizing the results of the mitigation program and presenting an inventory and describing the scientific significance of any fossil remains accessioned into the museum repository. The report shall be</p>				

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
		submitted to the City Planning Department and the museum repository. The report shall comply with the Society of Vertebrate Paleontology standard guidelines for assessing and mitigating impacts on paleontological resources.				
Geology/Soils	The project has the potential increase erosion of topsoil by wind.	<b>MM Geo 1:</b> To reduce impacts associated with erosion due to high winds, prior to construction, all tentative tracts and other construction activities will apply for and adhere to the permit given by the City of Ontario and enforced by the Building Official found in Title 6, Chapter 12, sections 6-12.01 – 6-12.07. The permit lasts for one (1) year, therefore, all construction lasting for a period of more than one calendar year from the date of issue will reapply for the permit and pay appropriate annual fees. At a minimum, the permit prohibits the disturbance of the surface or subsurface of more than one (1) acre of land without meeting permit requirements which can include such things as the application of soil stabilizers and limitations on grading activities during wind events.	Prior to grading permits	Building Department	Less than significant	

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
						Signature	Date Comments
<b>Geology/Soils</b>	The project has the potential to include/affect soils which are unsuitable for construction.	<b>MM Geo 2:</b> To properly assess and address the suitability of on-site soils to be used as fill, a geotechnical evaluation shall be performed by a qualified professional prior to the approval of the Tentative Tract map or site plan for a given phase of development. This evaluation will include an analysis of the organic matter content of soils on the site. If the organic matter content of the soils is greater than 2 percent when mixed with subsurface soils and/or imported fill, then manure will be removed from the site prior to grading operations.	Prior to tentative map approval report shall be submitted. Removal of unsuitable soils prior to grading.	Planning and Building Departments	Less than significant		
<b>Geology/Soils</b>	The project has the potential to have soils that are/could become unstable due to high organic content.	<b>MM Geo 3:</b> Site materials should be continuously tested and excavated to a minimum of 4 feet where soils generally become denser. Actual removal depths will be determined during grading when subsurface conditions are exposed. Input of crop residues and application of organic fertilizers at this site could have resulted in high soil organic matter contents. The mitigation proposed in Section III-6, Hazards/Hazardous Materials, will also mitigate for the management of organic matter in the soil.	Prior to grading permits	Building Department	Less than significant		
<b>Hazards/Hazardous Materials</b>	The proposed project could be located on a site that has been impacted by hazardous materials.	<b>MM Haz 1:</b> During development of the Specific Plan, if soils are found to be contaminated with petroleum products or other hazardous materials, they will be excavated and properly disposed of. After removal of contaminated soils, confirmation samples will be collected from the excavation to confirm adequate removal of petroleum-impacted soils.	During grading	Planning Department	Less than significant		



Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
<b>Hazards/ Hazardous Materials</b>	The proposed project could be located on a site that has been impacted by hazardous materials.	<b>MM Haz 2:</b> All septic tanks encountered on the project site will be properly removed and disposed of, per City and State procedures, prior to site development. All water wells on the project site which are proposed to be abandoned will be properly destroyed prior to site development in accordance with City requirements. These activities will be subject to the City of Ontario Building Safety requirements.	Prior to grading permits	Building Department	Less than significant	
<b>Hazards/ Hazardous Materials</b>	The proposed project could be located on a site that has been impacted by hazardous materials.	<b>MM Haz 3:</b> If, while performing any excavation as part of project construction, material that is believed to be hazardous waste is discovered, as defined in Section 25117 of the California Health & Safety Code, the developer shall contact the City of Ontario Fire Department and the County of San Bernardino Fire Department Hazardous Materials Division. Excavation shall be stopped until the material has been tested and the presence of hazardous waste has been confirmed. If no hazardous waste is present, excavation may continue. If hazardous waste is determined to be present, the California Department of Toxic Substances Control shall be contacted and the material shall be removed and disposed of pursuant to applicable provisions of California law.	Prior to grading permits	Planning Department	Less than significant	
<b>Hazards/ Hazardous Materials</b>	The proposed project will create a significant hazard to the public or the	<b>MM Haz 4:</b> Prior to demolition of all onsite buildings and remaining foundations that were built before 1978 shall be evaluated for the presence of asbestos, mercury and lead-based paint and those materials shall be removed	Prior to grading permits	Planning Department	Less than significant	

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
	environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment.	according to the applicable regulations and guidelines established by the South Coast Management District, Department of Toxic Substances Control, and the United States Environmental Protection Agency. As per HM-2 in the GPA for the NMC Final EIR, page 5.10-6, the developer shall submit documentation to the City Building Department that asbestos, mercury and lead-based paint are not present on their site, or that the above removal process has occurred.				
<b>Hazards/ Hazardous Materials</b>	The proposed project would create a significant hazard to the public or the environment through ground cracking or the presence or release of methane gas.	<b>MM Haz 5:</b> To properly assess and address the suitability of on-site soils to be used as fill, a geotechnical evaluation shall be performed by a qualified professional prior to the approval of the Tentative Tract map or site plan for a given phase of development. Fill material imported from other areas shall be tested prior to placement on-site to assess that is suitable to be used as fill, including testing for unsafe levels of hazardous materials. This evaluation, on both on- and off-site soils, will include an analysis of the organic matter content of the soils. If the organic matter content of the soils is greater than 2 percent when mixed with subsurface soils and/or imported fill, then manure will be removed from the site prior to grading operations.	Prior to grading permits	Planning Department	Less than significant	

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
<b>Hazards/ Hazardous Materials</b>	The proposed project would create a significant hazard to the public or the environment through ground cracking or the presence or release of methane gas.	<b>MM Haz 6:</b> To reduce the risk of ground cracking, manure shall be removed from the site, such that the organic matter content of on-site soils shall not exceed 2 percent (a 2 percent total organic content is allowed, of which no more than 1 percent can be manure) in the building foundation areas when mixed with underlying clean soils and imported fill.	Prior to building permits	Planning Department	Less than significant	
<b>Hazards/ Hazardous Materials</b>	The proposed project would expose people or property to risk associated with proximity to an airport.	<b>MM Haz 7:</b> To mitigate for any potential impacts related to proximity to the Chino Airport, all development within the Specific Plan will comply with the building height constraints identified in the GPA for the NMC (1998).	Prior to building permits	Planning Department	Less than significant	
<b>Hazards/ Hazardous Materials</b>		<b>MM Haz 8:</b> To disclose to the buyer or lessee of subdivided lands within the Parkside Specific Plan project of the proximity of this site to the Chino Airport as required by AB 2776, the City shall disclose, and ensure that the developer makes such disclosures, as required by law to all future buyers.	Prior to building permits	Planning Department	Less than significant	
<b>Hydrology/ Water Quality</b>	During project construction, the project could create or contribute runoff water that would violate any water quality	<b>MM Hydro 1:</b> In order to ensure that construction activities associated with the Parkside Specific Plan will not cause a violation of any water quality standard or waste discharge requirements and to assure no substantial degradation of water quality occurs, and to implement the intent of mitigation measures included in the Final EIR for the GPA	Prior to and during construction	Engineering Department	Less than significant	

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
	standards or waste discharge requirements, including the terms of the City's municipal separate stormwater sewer system permit.	for the NMC, developments within the project area shall comply with all applicable provisions of the state's General Permit for Construction Activities (Order No. 99-08-DWQ, or most recent version) during all phases of construction. A copy of evidence of the receipt of a Waste Discharge Identification Number from the State Regional Water Quality Control Board shall be filed with the City Engineer along with a copy of the Storm Water Pollution Prevention Plan (SWPPP) maps and BMPs. The City Engineer shall review and approve the provisions of the SWPPP prior to implementation of any SWPPP provision or starting any construction activity.				
<b>Hydrology/ Water Quality</b>	During project construction, the project could create or contribute runoff water that would violate any water quality standards or waste discharge requirements, including the terms of the City's municipal separate stormwater sewer system	<b>MM Hydro 2:</b> In order to ensure that development within the Specific Plan will not cause or contribute to violations of any water quality standard or waste discharge requirements, and to assure no substantial degradation of water quality occurs, the project will complete a Water Quality Management Plan (WQMP) pursuant to the MS4 permit (Order No. 2002-0012) adopted by the City of Ontario. The project shall incorporate Site Design BMPs and Source Control BMPs, and potentially Treatment Control BMPs. The following tables (Table III-7-F and G) provide guidelines and BMPs that shall be incorporated as appropriate into project design (on construction drawings) and/or project specifications and implemented in the field to reduce the expected pollutants	Prior to and during construction	Engineering Department	Less than significant	

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
						Signature	Date Comments
	permit.	from various types of development. Table III-7-G correlates each BMP to the pollutants of concern which it removes/reduces and/or meets the design objectives for the BMP.					
<b>Hydrology/ Water Quality</b>	During project construction, the project could create or contribute runoff water that would violate any water quality standards or waste discharge requirements, including the terms of the City's municipal separate stormwater sewer system permit.	<b>MM Hydro 3:</b> To assure that development within the Specific Plan will not cause a violation of any water quality standard or waste discharge requirements, including San Bernardino County's MS4 permit issued by the SARWQCB, and to assure that no substantial degradation to water quality occurs after construction, any loading docks present within the academic or retail areas designated in the Specific Plan will be designed with devices to trap oil and grease, such that these pollutants are not discharged from the site in storm water or non-storm water discharges.	Prior to, during and after construction	Engineering Department	Less than significant		
<b>Hydrology/ Water Quality</b>	Significantly alter the flow velocity or volume of stormwater runoff in a manner that results in environmental harm.	<b>MM Hydro 4:</b> In order to reduce the risk of flooding and to implement mitigation measures included in the GPA for the NMC Final EIR, prior to issuance of grading permits, the City of Ontario shall coordinate with the San Bernardino County Flood Control District to ensure that the project meets County flood control requirements.	Prior to grading permits	Engineering Department	Less than significant		
<b>Hydrology/</b>	Substantially	<b>MM Hydro 5:</b> In order to conserve	Post construction	Planning	Less than		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
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<b>Water Quality</b>	deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted).	water and to mitigate for any potential unforeseen adverse impacts to a reduction in ground water recharge, the following measure has been recommended by the Chino Basin Water Conservation District. Landscaping within individual development projects and the 52-acre Great Park will retain and percolate both applied irrigation water and storm water in vegetated areas of parking lots and other areas, where appropriate; “depressed” planted areas bordered by shrubbery screens will be implemented rather than “mounded” grass and shrubbery planted screens. Neighborhood Edges and parks will be irrigated via reclaimed water.		Department	significant		
<b>Hydrology/ Water Quality</b>	After the project is completed, create or contribute runoff water that would violate any	<b>MM Hydro 6:</b> In order to reduce pollutants in post construction run-off and to implement mitigation measures included in the Final Environmental Impact Report for the NMC, the individual project owners and operators (e.g., homeowner associations, retail center owners, school district, parks	Post construction	Engineering Department	Less than significant		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
						Signature	Date Comments
	water quality standards or waste discharge requirements, including the terms of the City’s municipal separate stormwater sewer system permit.	department, etc.) shall ensure that all pest control, herbicide, insecticide and other similar substances used as part of maintenance of project features are handled, stored, applied and disposed of by those conducting facility maintenance in a manner consistent with all applicable federal, state and local regulations. The city Engineer shall monitor and enforce this provision.					
<b>Hydrology/ Water Quality</b>	After the project is completed, create or contribute runoff water that would violate any water quality standards or waste discharge requirements, including the terms of the City’s municipal separate stormwater sewer system permit.	<b>MM Hydro 7:</b> To mitigate possible temporary run-off from undeveloped properties located north (up-gradient) of the project site, drainage from properties north of the project site shall be conveyed to appropriate drainage facilities, as approved by the City Engineer.	Post construction	Engineering Department	Less than significant		
<b>Noise</b>	The project will result in a substantial temporary or	<b>MM Noi 1:</b> The construction activities of the proposed project shall comply with the City of Ontario noise ordinance that prohibits construction activities on	During construction	Planning Department	Less than significant		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification			
						Signature	Date	Comments	
	periodic increase in ambient noise levels in the project vicinity above levels existing without the project.	Sundays, federal holidays, and other days between the hours of 7:00 p.m. and 7:00 a.m.							
Noise	The project will result in a substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project.	<b>MM Noi 2:</b> Construction staging areas shall not be located within 150 feet of existing sensitive receptors and construction equipment shall be fitted with properly operating and maintained mufflers.	During construction	Planning Department	Less than significant				
<p><i>To reduce or eliminate impacts related to exterior and interior noise levels within the project exceeding City of Ontario standards, the following mitigation measures shall be implemented. However, the wall heights recommended in MM Noi 3 through 67 only apply to lots which have backyards directly adjacent to the roadways. For lots with front yards adjacent to the roadways, the windows and/or doors would need to have upgraded sound rated glazing products in order to comply with the City of Ontario's interior noise standards.</i></p>									
Noise	The project will expose people to, or generate, noise levels in excess of standards established in the local general plan or noise	<b>MM Noi 3:</b> A sound wall at least 7 feet high (relative to pad elevation) shall be constructed along the project site boundary for all perimeter	Prior to occupancy	Planning Department	Less than significant				



Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments	
	ordinance or applicable standards.	lots adjacent to Archibald Avenue. If any residential structures are two-stories high, then windows facing Archibald Avenue would need to have upgraded sound rated glazing products and the rooms would need to have supplemental ventilation. A final acoustical report shall be submitted to address wall heights based on final grading plans. The report shall be reviewed and approved by the Planning Department prior to issuance of building permits.					

Impact Category	Impact	Mitigation Measure		Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
							Signature	Date Comments
Noise	The project will expose people to, or generate, noise levels in excess of standards established in the local general plan or noise ordinance or applicable standards.	MM Noi 4: A sound wall at least 6 feet high (relative to pad elevation) shall be constructed along the project site boundary for all perimeter lots adjacent to Edison Avenue. If any residential structures are two-stories high, then windows facing Edison Avenue would need to have upgraded sound rated glazing products and the rooms would need to have supplemental ventilation. A final acoustical report shall be submitted to address wall heights based on final grading plans.	Prior to occupancy	Planning Department	Less than significant			

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments	
		The report shall be reviewed and approved by the Planning Department prior to issuance of building permits.					
Noise	The project will expose people to, or generate, noise levels in excess of standards established in the local general plan or noise ordinance or applicable standards.	<b>MM Noi 5:</b> A sound wall at least 7 feet high (relative to pad elevation) shall be constructed along the project site boundary for all perimeter lots adjacent to Eucalyptus Avenue. If any residential structures are two-stories high, then windows facing Eucalyptus Avenue would need to have upgraded sound rated glazing products and the rooms	Prior to occupancy	Planning Department	Less than significant		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments	
		would need to have supplemental ventilation. A final acoustical report shall be submitted to address wall heights based on final grading plans. The report shall be reviewed and approved by the Planning Department prior to issuance of building permits.					
Noise	The project will expose people to, or generate, noise levels in excess of standards established in the local general plan or noise ordinance or applicable standards.	<b>MM Noi 6:</b> Architectural plans shall be submitted to the City of Ontario for an acoustical plan check prior to the issuance of building permits to assure the proper window and/or doors are upgraded for sound reduction and	Prior to occupancy	Planning Department	Less than significant		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
						Signature	Date Comments
		proper ventilation systems are incorporated in order to meet the interior noise level requirement.					
<b>Public Services</b>	The project could result in impacts to fire services.	<b>MMServ 1:</b> To reduce fire hazards, wood-shingled and shake-shingled roofs are prohibited.	Prior to occupancy	Fire Department	Less than significant		
<b>Public Services</b>	The project could result in impacts to fire services.	<b>MMServ 2:</b> To reduce fire hazards, fire hydrant locations and water main sizes shall meet standards established by the City Fire Department and reviewed and implemented by the Engineering Department.	Prior to occupancy	Fire Department	Less than significant		
<b>Public Services</b>	The project could result in impacts to fire services.	<b>MMServ 3:</b> To reduce fire hazards when water is	Prior to occupancy	Fire Department	Less than significant		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
						Signature	Date Comments
		provided to the site, adequate fire flow pressure shall be provided for residential areas and non-residential projects in accordance with currently adopted standards (2001 California Fire Code Appendix III-A).					
<b>Public Services</b>	The project could result in impacts to fire services.	<b>MMServ 4:</b> To reduce fire hazards, adequate water supply shall be provided by the Fire Department prior to the framing stages of construction.	Prior to construction	Engineering and Fire Departments	Less than significant		
<b>Public Services</b>	The project could result in impacts to fire services.	<b>MMServ 5:</b> To reduce fire hazards, houses located on cul-de-sacs longer than 300 feet shall	Prior to occupancy	Planning Department	Less than significant		

Impact Category	Impact	Mitigation Measure		Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
							Signature	Date Comments
		be constructed with residential fire sprinklers.						
<b>Public Services</b>	The project could result in impacts to fire services.	<b>MMServ 6:</b> To reduce fire hazards, access roadways designed in accordance with Fire Department standards to within 150' of all structures, shall be provided prior to the framing stages of construction. This access is to be maintained in an unobstructed manner throughout construction.	Prior to occupancy	Planning Department	Less than significant			

Impact Category	Impact	Mitigation Measure		Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
							Signature	Date Comments
Public Services	The project could result in impacts to fire services.	<b>MM Serv 7:</b> A fire station located within the Specific Plan must be operational prior to the issuance of any certificates of occupancy in the Specific Plan.	Prior to occupancy	Planning Department		Less than significant		
Public Services	The project could impact public services.	<b>MMServ 8:</b> The developer shall pay library, police, and fire service development impact fees.	Prior to permits	Planning Department		Less than significant		
Public Services	The project could impact school services.	<b>MMServ 9:</b> The developer shall pay school fees or otherwise meet project obligations to schools, as required by Mountain View Unified and Chaffey Joint Union High School Districts.	Prior to permits	Planning Department		Less than significant		
Public Services	The project could impact parks.	<b>MMServ 10:</b> The portions of the Great	Prior to occupancy, as described.	Planning Department		Less than significant		



Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments		
		<p>Park (PA 22) located east of Cucamonga Creek shall be constructed no later than the issuance of the Certificate of Occupancy for the last housing unit in PAs 1 - 4 and PAs 17 - 19. The portion of the Great Park located west of Cucamonga Creek in PA 22 east of Hellman Avenue shall be constructed no later than the issuance of the Certificate of Occupancy for the last housing unit in PA 6 and PA 16. The remainder of PA 22 located west of Hellman Avenue shall be constructed no later than the issuance of</p>						

Impact Category	Impact	Mitigation Measure		Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments		
		the Certificate of Occupancy for the last housing unit in PAs 7 – 10 and 11 – 14.							

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<b>Transportation/Traffic</b>	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<p><b>*MM Trans 1:</b> Modify the intersection of Archibald Avenue/Edison Avenue to include the following geometrics:</p> <p>Northbound: Two left-turn lanes. Four through lanes. One right-turn lane.</p> <p>Southbound: Two left-turn lanes. Four through lanes. One right-turn lane.</p> <p>Eastbound: Two left-turn lanes. Three through lanes. Two right-turn lanes.</p> <p>Westbound: Two left-turn lanes. Three through lanes. One right-turn lane.</p>	Prior to occupancy	Engineering Department	Less than significant
<b>Transportation/Traffic</b>	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<p><b>*MM Trans 2:</b> Modify the intersection of future Carpenter Street/Edison Avenue to include the following geometrics:</p> <p>Northbound: One shared left-turn, through and right-turn lane.</p> <p>Southbound: N/A</p> <p>Eastbound: Two through lanes. One shared through and right-turn lane.</p> <p>Westbound: One left-turn lane. Three through lanes.</p> <p>Intersection: Control: Install Signal.</p>	Prior to occupancy of adjacent planning areas	Engineering Department	Less than significant

\*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<b>Transportation/Traffic</b>	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<p><b>*MM Trans 3:</b> Modify the intersection of future Carpenter Street/Merrill / Avenue to include the following geometrics:                      Northbound: N/A                      Southbound: One shared left-turn, through and right-turn lane.                      Eastbound: One shared left-turn and through lane. One through lane.                      Westbound: One through lane. One shared through and right-turn lane.                      Intersection: Control: TWSC.</p>	Prior to occupancy of adjacent planning areas	Engineering Department	Less than significant
<b>Transportation/Traffic</b>	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<p><b>MM Trans 4:</b> Construction of full width of internal roadways and part width of the following roadways shall comply with City of Ontario Standards:</p> <ul style="list-style-type: none"> <li>Construct partial width improvements on the westerly side of Archibald Avenue at its ultimate cross-section as a divided arterial parkway 1A with bikeway (165’ right-of-way) adjacent to project boundary line.</li> <li>Construct partial width improvements on the southerly side of Edison Avenue at its ultimate cross-section as a divided arterial parkway 1A (160’ right-of-way) adjacent to project boundary line.</li> <li>Construct partial width improvements on the northerly side of Merrill Avenue at its ultimate cross-section as a standard arterial (108’ right-of-way) adjacent to project boundary line.</li> </ul>	Prior to occupancy of adjacent planning areas	Engineering Department	Less than significant

\*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<b>Transportation/Traffic</b>	The project will substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment).	<b>MM Trans 4a:</b> Intersection, median opening, and traffic signal spacing shall be in accordance with the City of Ontario New Model Colony Access Guidelines.	To be shown on tract maps. Prior to map approval.	Engineering Department	Less than significant
<b>Transportation/Traffic</b>	The project will substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment).	<b>MM Trans 5:</b> Sight distance at the project entrance roadways should be reviewed with respect to standard City of Ontario sight distance standards at the time of preparation of final grading, landscape and street improvement plans.	During plan check, prior to grading permit	Engineering Department	Less than significant
<b>Transportation/Traffic</b>	The project will substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment).	<b>MM Trans 6:</b> Signing/stripping should be implemented in conjunction with detailed construction plans for the project site.	During plan check, prior to approval of final road construction plans	Engineering Department	Less than significant
<b>Transportation/Traffic</b>	The project will exceed, either individually or cumulatively, the level of service standard established	<b>MM Trans 7:</b> The City should work with Omnitrans to develop additional routes and service for both local and regional service to the project area.	Prior to building permits	Engineering Department	Less than significant

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Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
	by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.				
<b>Transportation/Traffic</b>	The project will conflict with adopted policies, plans or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks).	<b>MM Trans 8:</b> The City should establish a Transportation System Management (TSM) Program with the goal of reducing vehicle trips to and from land uses within the City, and particularly focusing on the reduction of drive-alone vehicle use in work commuting. The program should set the overall policy and goals for trip reduction measures within the City, and require new developments to implement programs and measures to ensure compliance with those goals, such as preferential parking for carpools and vanpools, flex-time work hours, compressed work week, and distribution of information about ridesharing and transit services.	Ongoing	Planning Department	Less than significant
<b>Transportation/Traffic</b>	The project will conflict with adopted policies, plans or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks).	<b>MM Trans 9:</b> The project will participate in the cost of off-site improvements through fair-share payment of the Development Impact fee as established by the City of Ontario. These fees should be collected and utilized as needed by the City to construct the improvements necessary to maintain the required level of service.	Ongoing	Planning Department	Less than significant
<i>The following Mitigation Measures (MM Trans 10 through MM Trans 31) have been identified to reduce the cumulative traffic impacts to a less than significant level and are required to attain the required LOS of intersections in the project area. The project will either install these improvements or pay their fair share mitigation fee, as determined by the City Engineer.</i>					

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Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<b>Transportation/ Traffic</b>	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 10:</b> Modify the intersection of Euclid Avenue/ Riverside Drive to include the following geometrics: Northbound: Two left-turn lanes. Four through lanes. One shared right-turn/ through lane. Southbound: One left-turn lane. Four through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Three through lanes. One right-turn lane. Westbound: One left-turn lane. Three through lanes. One shared right-turn/through lane.	Prior to occupancy	Engineering Department	Less than significant
<b>Transportation/ Traffic</b>	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 11:</b> Modify the intersection of Euclid Avenue/ Chino Avenue to include the following geometrics: Northbound: Two left-turn lanes. Four through lanes. One share right-turn/through lane. Southbound: One left-turn lane. Four through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Three through lanes. One right-turn lane. Westbound: Two left-turn lanes. One through lane. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

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Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<b>Transportation/ Traffic</b>	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 12:</b> Modify the intersection of Euclid Avenue/ Schaefer Avenue to include the following geometrics: Northbound: Two left-turn lanes. Four through lanes. One right-turn lane. Southbound: One left-turn lane. Four through lanes. One shared right-turn/ through lane. Eastbound: One left-turn lane. Two through lanes. One right-turn lane. Westbound: One left-turn lane. Two through lanes. One shared right-turn/ through lane.	As determined by the City Engineer	Engineering Department	Less than significant
<b>Transportation/ Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 13:</b> Modify the intersection of Euclid Avenue/ Edison Avenue to include the following geometrics: Northbound: Two left-turn lanes. Four through lanes. One right-turn lane. Southbound: Two left-turn lanes. Four through lanes. One right-turn lane. Eastbound: One left-turn lane. Three through lanes. Two right-turn lanes. Westbound: Two left-turn lanes. Three through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

\*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.



Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<b>Transportation/Traffic</b>	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 14:</b> Modify the intersection of Euclid Avenue/ Merrill Avenue to include the following geometrics: Northbound: One left-turn lane. Four through lanes. Two right-turn lanes. Southbound: Two left-turn lanes. Four through lanes. Eastbound: N/A Westbound: Two left-turn lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant
<b>Transportation/Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 15:</b> Modify the intersection of Grove Avenue/ Riverside Drive to include the following geometrics: Northbound: One left-turn lane. Three through lanes. One shared right-turn/ through lane. Southbound: One left-turn lane. Three through lanes. One right-turn lane. Eastbound: One left-turn lane. Two through lanes. One shared right-turn/ through lane. Westbound: One left-turn lane. Two through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

\*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<b>Transportation/ Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 16:</b> Add traffic signal and modify the intersection of Grove Avenue/ Chino Avenue to include the following geometrics: Northbound: One left-turn lane. Three through lanes. One right-turn lane. Southbound: One left-turn lane. Three through lanes. One right-turn lane. Eastbound: One left-turn lane. Two through lanes. One right-turn lane. Westbound: One left-turn lane. Two through lanes. One shared right-turn/ through lane.	As determined by the City Engineer	Engineering Department	Less than significant
<b>Transportation/ Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 17:</b> Add traffic signal and modify the intersection of Grove Avenue/ Edison Avenue to include the following geometrics: Northbound: Two left-turn lanes. Two through lanes. One right-turn lane. Southbound: Two left-turn lanes. Three through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Two through lanes. One right-turn lane. Westbound: Two left-turn lanes. Two through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

\*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<b>Transportation/Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets	<b>*MM Trans 18:</b> Add traffic signal and modify the intersection of Grove Avenue/ Merrill Avenue to include the following geometrics: Northbound: N/A Southbound: One shared left-turn and right-turn lane. One right-turn lane. Eastbound: One left-turn lane. Two through lanes. Westbound: Two through lanes. One shared right-turn/through lane.	As determined by the City Engineer	Engineering Department	Less than significant
<b>Transportation/Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 19:</b> Modify the intersection of Vineyard Avenue/ Riverside Drive to include the following geometrics: Northbound: Two left-turn lanes. Three through lanes. One right-turn lane. Southbound: Two left-turn lanes. Three through lanes. One right-turn lane. Eastbound: One left-turn lane. Two through lanes. One right-turn lane. Westbound: One left-turn lane. Two through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

\*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<b>Transportation/ Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 20:</b> Modify the intersection of Archibald Avenue/ SR-60 WB Ramps to include the following geometrics: Northbound: One left-turn lane. Three through lanes. Southbound: Three through lanes. One right-turn lane. Eastbound: N/A Westbound: One left-turn lane. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant
<b>Transportation/ Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 21:</b> Modify the intersection of Archibald Avenue/ SR-60 EB Ramps to include the following geometrics: Northbound: Three through lanes. One right-turn lane. Southbound: One left-turn lane. Three through lanes. Eastbound: One left-turn lane. One right-turn lane. Westbound: N/A	As determined by the City Engineer	Engineering Department	Less than significant

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Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<b>Transportation/Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 22:</b> Modify the intersection of Archibald Avenue/ Riverside Drive to include the following geometrics: Northbound: One left-turn lane. Three through lanes. One shared right-turn/ through lane. Southbound: One left-turn lane. Three through lanes. One right-turn lane. Eastbound: One left-turn lane. Three through lanes. One shared right-turn/ through lane. Westbound: One left-turn lane. Three through lanes. One shared right-turn/ through lane.	As determined by the City Engineer	Engineering Department	Less than significant
<b>Transportation/Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 23:</b> Modify the intersection of Archibald Avenue/ Chino Avenue to include the following geometrics: Northbound: One left-turn lane. Three through lanes. One right-turn lane. Southbound: One left-turn lane. Three through lanes. One right-turn lane. Eastbound: One left-turn lane. Three through lanes. One right-turn lane. Westbound: Two left-turn lanes. Two through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

\*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<b>Transportation/ Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 24:</b> Add traffic signal and modify the intersection of Archibald Avenue/ Schaefer Avenue to include the following geometrics: Northbound: Two left-turn lanes. Three through lanes. One shared right-turn/ through lane. Southbound: One left-turn lane. Three through lanes. One right-turn lane. Eastbound: Two left-turn lanes. One through lane. Two right-turn lanes. Westbound: One left-turn lane. One through lane. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant
<b>Transportation/ Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 25:</b> Modify the intersection of Archibald Avenue/ Edison Avenue to include the following geometrics: Northbound: Two left-turn lanes. Four through lanes. One right-turn lane. Southbound: Two left-turn lanes. Four through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Three through lanes. Two shared right-turn/ through lanes. Westbound: Two left-turn lanes. Three through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

\*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<b>Transportation/Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 26:</b> Add traffic signal and modify the intersection of Archibald Avenue/ Merrill Avenue to include the following geometrics: Northbound: Two left-turn lanes. Four through lanes. One right-turn lane. Southbound: Two left-turn lanes. Four through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Three through lanes. One right-turn lane. Westbound: Two left-turn lanes. Three through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant
<b>Transportation/Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 27:</b> Modify the intersection of Archibald Avenue/ Cloverdale Road to include the following geometrics: Northbound: Four through lanes. One right-turn lane. Southbound: Two left-turn lanes. Four through lanes. Eastbound: N/A Westbound: Two left-turn lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

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Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<b>Transportation/ Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 28:</b> Modify the intersection of Haven Avenue/ Riverside Drive to include the following geometrics: Northbound: One left-turn lane. Two through lanes. Two right-turn lanes. Southbound: One left-turn lane. Two through lanes. One right-turn lane. Eastbound: One left-turn lane. Three through lanes. One right-turn lane. Westbound: One left-turn lane. Two through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant
<b>Transportation/ Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 29:</b> Add traffic signal and modify the intersection of Haven Avenue/ Edison Avenue to include the following geometrics: Northbound: One left-turn lane. Two through lanes. One shared right-turn/ through lane. Southbound: One left-turn lane. Two through lanes. One right-turn lane. Eastbound: Two left-turn lanes. One through lane. One shared right-turn/ through lane. Westbound: One left-turn lane. One through lane. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

\*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.



Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<b>Transportation/ Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 30:</b> Add traffic signal and modify the intersection of Hamner Avenue/ Eucalyptus Avenue to include the following geometrics: Northbound: Two left-turn lanes. Three through lanes. Southbound: Three through lanes. Two right-turn lanes. Eastbound: Two left-turn lanes. One right-turn lane. Westbound: N/A	As determined by the City Engineer	Engineering Department	Less than significant
<b>Transportation/ Traffic</b>	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<b>*MM Trans 31:</b> Modify the intersection of Hamner Avenue/ Bellegrave Avenue to include the following geometrics: Northbound: One left-turn lane. Two through lanes. One right-turn lane. Southbound: Two left-turn lanes. Three through lanes. One right-turn lane. Eastbound: One left-turn lane. Two through lanes. One right-turn lane. Westbound: Two left-turn lanes. Three through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

\*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<p><b>Transportation/Traffic</b></p>	<p>Due to the unknown construction timing of area-wide improvements, the project will temporarily exceed the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.</p>	<p><b>See MM Trans 7 through 31</b></p>	<p>Unknown, as determined by the City Engineer</p>	<p>Engineering Department</p>	<p>Temporary significant cumulative impacts</p>

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Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Utilities	Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.	<b>MM Util 1:</b> All water and sewer pipelines within and adjacent to the project boundaries shall be constructed and/or funded for construction on a fair-share basis based on the NMC Infrastructure Master Plans and/or the interim sewer plan herein, and to the satisfaction of the City.	Prior to occupancy	Engineering Department	Less than Significant
Utilities	Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.	<b>MM Util 2:</b> The Archibald trunk sewer line off-site connection to the IEUA Kimbal Avenue interceptor shall be complete and operational prior to issuance of first certificate of occupancy for development located east of the Cucamonga Creek Channel. The applicant shall participate on a fair share basis in the development of the necessary sewer facilities.	Prior to issuance of building permits for the development located east of Cucamonga Creek	Engineering Department and IEUA	Less than Significant
Utilities	Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.	<b>MM Util 3:</b> The planning areas located west of Cucamonga Creek Channel shall have sewer lines in place to connect, via master planned lines with the western area trunk sewer system in Euclid Avenue, or via the Carpenter Street interim connection to the eastern area trunk sewer system. The interim condition that may exist for the Specific Plan where the wastewater generated may be tied into the Eastern Trunk System (ETS) will be via a 36-inch line located in Vineyard Avenue, north of Merrill Avenue, a 15-inch line in Merrill Avenue from Vineyard Avenue to Carpenter Avenue, and a 15-inch line in Carpenter Avenue from Merrill Avenue to the ETS. This interim connection shall be constructed per the approved sewer master plan. Thus, should the approved master plan require larger size pipelines, the developer will be required to construct them. Installation of one of these connections shall be in place and operable prior to issuance of building permits	Prior to issuance of building permits for the development located west of Cucamonga Creek	Engineering Department and IEUA	Less than Significant

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
		for the development located west of Cucamonga Creek to the satisfaction of the City and IEUA.			
<b>Utilities</b>	Result in adverse impacts to natural gas or other dry utility systems.	<b>MM Util 4:</b> Off-site water lines, tanks, interconnectors and other facilities required in the Water Master Plan to provide water to the site shall be in place and operational prior to issuance of the first certificate of occupancy. The applicant shall participate on a fair share basis in the development of these off-site facilities.	Prior to first certificate of occupancy	Engineering Department	Less than Significant
<b>Utilities</b>	Result in adverse impacts to natural gas or other dry utility systems.	<b>MM Util 5:</b> Prior to obtaining grading permit(s), the project proponent shall coordinate with the applicable natural gas, electrical, and telephone utility providers for the project site to ensure that all existing underground and overhead lines are not damaged during project construction.	Prior to grading permits	Engineering Department	Less than significant
<b>Utilities</b>	Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.	<b>MM Util 6:</b> To reduce the quantity of energy used and to conserve water resources, the project developer and City of Ontario should work to include sustainable systems for use of water and energy within the project design. One source of assistance in this regard is Southern California Gas Company Commercial/Industrial Support Center at 1-800-GAS-2000, which should be contacted at the time of development of the commercial center located within the project.	Ongoing	Engineering Department	Less than significant
<b>Utilities</b>	Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.	<b>MM Util 7:</b> The project applicant shall plan and construct a dual pipe system to supply reclaimed water when available in the future (GP Policy 5.1.4). An Engineer's Report approved by the City and the Department of Health Services is required prior to the use of recycled water.	Prior to occupancy	Engineering Department	Less than significant
<b>Utilities</b>	Disruption of adequate temporary water supply.	<b>MM Util 8:</b> All existing agricultural wells on the project site will be destroyed and abandoned per the California Department of Health Services guidelines. A	Prior to demolition permit.	Engineering Department	Less than significant

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
		well use/destruction plan and schedule for all existing agricultural wells on the project site shall be prepared and submitted for approval, prior to the issuance of grading permits. This plan shall also include a temporary water supply plan, as applicable, in order to avoid potential significant temporary impacts resulting from the disruption of current water supply through the abandonment of on-site wells, the developer of any parcel located within the Specific Plan which contains a well that services one or more adjacent parcels that are not proposed to be developed in the current phase, shall provide the City Engineer with a temporary water supply plan for approval. Construction of any temporary pipes or facilities needed to provide water to the existing uses which are to temporarily remain shall be installed per City requirements at the developer's expense.			
<b>Utilities</b>	Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.	<b>MM Util 9:</b> Prior to approval of the Specific Plan and EIR, a hydraulic analysis of the area served by the interim sewer main to be located in Carpenter Avenue shall be submitted to the City Engineer.	Prior to approval of the Specific Plan and EIR	Engineering Department	Less than significant
<b>Utilities</b>	Be served by a landfill with insufficient permitted capacity to accommodate the project's solid waste disposal needs.	No feasible mitigation measures exist that would eliminate or substantially lessen the cumulative impacts to solid waste facilities.	Not applicable	Not applicable	Significant cumulative impacts

## RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING CITY COUNCIL APPROVAL OF FILE NO. PSPA19-007, AN AMENDMENT TO THE PARKSIDE SPECIFIC PLAN TO: [1] RECONFIGURE THE RESIDENTIAL PLANNING AREAS 1 THROUGH 4, AND 17 THROUGH 19; [2] RECONFIGURE THE GREAT PARK PLANNING AREA 22 (EAST OF THE CUCAMONGA CREEK CHANNEL); [3] REVISE INTERNAL CIRCULATION TO IMPROVE ACCESS INTO THE NEIGHBORHOOD COMMERCIAL PLANNING AREA 19; [4] UPDATE AND REVISE RESIDENTIAL DESIGN GUIDELINES (SECTIONS 7.1 THROUGH 7.6) TO INTRODUCE NEW HOUSING TYPES AND ARCHITECTURAL STYLES; AND [5] UPDATE AND REVISE LANDSCAPE STANDARDS (SECTION 7.7). THE PROJECT SITE IS BOUNDED BY ONTARIO RANCH ROAD TO THE NORTH, EUCALYPTUS AVENUE TO THE SOUTH, ARCHIBALD AVENUE TO THE EAST AND CARPENTER AVENUE TO THE WEST, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, AND 0218-221-10.

WHEREAS, SC Ontario Development Company, LLC (hereinafter referred to as "Applicant") has filed an Application for the approval of a an Amendment to the Parkside Specific Plan, File No. PSPA19-007, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 250.89 gross acres of land bounded by Ontario Ranch Road to the north, Eucalyptus Avenue to the south, Archibald Avenue to the east and Carpenter Avenue to the west, within the Parkside Specific Plan, and is presently improved with general crop-related agricultural purposes (alfalfa, corn, milo, sorghum, etc.) and flood control; and

WHEREAS, the properties to the north of the Project site are within the Low-Density Residential zoning district of the Avenue Specific Plan and are vacant with some agriculture/dairy uses. The properties to the east are within the High Density Residential/Great Park zoning district of the Grand Park Specific Plan and are vacant with some agriculture/dairy uses. The properties to the south are within the Business Park zoning district of the West Ontario Commerce Specific Plan and developed with industrial uses and within the Low-Density Residential zoning district of the Subarea 29 Specific Plan and vacant. The properties to the west are within the Agriculture Overlay/ Specific Plan zoning district, and are vacant with some agriculture/dairy uses; and

WHEREAS, on September 4, 2019; SC Ontario Development Company LLC, submitted an Amendment to the Parkside Specific Plan to: 1] Reconfigure residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 21; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7); and

WHEREAS, the Parkside Specific Plan Land Use Plan Exhibit 1-3, the Land Use Summary Table 1.1, Vehicular Circulation Master Plan Exhibit 5.1, Residential Design Guidelines Sections 7.1 – 7.6, and Landscape Standards Section 7.7 have been updated to reflect the Amendment to Parkside Specific Plan. In addition, the Amendment includes minor updates to the development standards, exhibits and text changes. All changes and additions to the Specific Plan (exhibits, tables, development standards and design guidelines) are contained within the revised Parkside Specific Plan document accompanying this Resolution (Attachment “A”). All additions to the Specific Plan have been highlighted in red; and

WHEREAS, revisions were made to the Specific Plan for compliance with the Development Code and TOP Compliance; and

WHEREAS, the Parkside Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011008) was certified on September 5, 2006 (hereinafter referred to as “Certified EIR”), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as “EIR Addendum”) in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as “CEQA”); and

WHEREAS, the environmental impacts of this project were thoroughly analyzed in the EIR Addendum, which concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an EIR Addendum has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make recommendation to City Council on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, as the first action on the Project, on May 26, 2020, the Planning Commission issued a Resolution recommending City Council adopt the EIR Addendum, finding that the proposed Project introduces no new significant environmental impacts and applying all previously adopted mitigation measures to the Project, which were incorporated by reference; and

WHEREAS, on May 26, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:



**SECTION 1: *Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the recommending authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project is consistent with the 1,947 number of dwelling Specified in the Available Land Inventory. Per the Available Land Inventory, the Parkside Specific Plan is required to provide 1,947 dwelling units with an overall density range of between 9 and 21 DU/AC.

**SECTION 2: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

**SECTION 3: *Concluding Facts and Reasons.*** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 and 2, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed Specific Plan, or amendment thereto, is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Amendment to the Parkside Specific is consistent with Vision, Policy Plan (General Plan), and City Council Priorities components of TOP. Section 9.0, General Plan Consistency, of the Parkside Specific Plan describes the manner in which Parkside Specific Plan complies with the Policy Plan goals and policies.

(2) ***The proposed Specific Plan, or amendment thereto, would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City.*** The TOP EIR and Parkside Specific Plan EIR include safeguards and imposed certain mitigation measures to ensure that development within the Parkside Specific Plan boundary area would not be detrimental to public interest, health, safety, or general welfare of City.

(3) ***In the case of an application affecting specific property(ies), the proposed Specific Plan, or amendment thereto, will not adversely affect the harmonious relationship with adjacent properties and land uses.*** The project site is in an area that is characterized by agriculture uses and residential development and is surrounded by urban land uses. Any development proposals that would subsequently occur from the proposed Project will be required to be in accordance with the policies of the Community Design Element of the Policy Plan (General Plan) and development regulations and design guidelines of the Parkside Specific Plan and therefore, will not adversely affect the harmonious relationship with adjacent properties and land uses.

(4) ***In the case of an application affecting specific property(ies), the subject site is physically suitable, including, but not limited to, parcel size, shape, access, and availability of utilities, for the request and anticipated development.*** The Amendment the Parkside Specific Plan will maintain the appropriate balance of land uses within the City consistent with TOP Policy Plan. In addition, development within the Parkside Specific Plan will be required to construct the necessary infrastructure and public services that will support future residential and commercial/office uses.

**SECTION 4: Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 3, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment "B," and incorporated herein by this reference.

**SECTION 5: Indemnification.** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

**SECTION 6: Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

**SECTION 7: Certification to Adoption.** The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26<sup>th</sup> day of May, 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

---

Jim Willoughby  
Planning Commission Chairman

ATTEST:

---

Cathy Wahlstrom  
Planning Director and  
Secretary to the Planning Commission

STATE OF CALIFORNIA                    )  
COUNTY OF SAN BERNARDINO        )  
CITY OF ONTARIO                        )

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. \_\_\_\_\_ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 26, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

---

Gwen Berendsen  
Secretary Pro Tempore

**ATTACHMENT A:**  
**File No. PSPA19-007**  
**Parkside Specific Plan**

*(Document to follow this page)*

**ATTACHMENT B:**

**File No. PSPA19-007  
Departmental Conditions of Approval**

*(Departmental conditions of approval to follow this page)*



# Parkside

Specific Plan

---

Adopted August 2006  
May 2020 Amendment Draft

2001368





# Parkside

## SPECIFIC PLAN

Portions of Subareas 22 and 23  
Parkside at New Model Colony  
PSP03-002

Approved by City of Ontario City Council on 08/15/2006  
Amended XX 2020

### **Community Developer**

**SC ONTARIO DEVELOPMENT COMPANY, LLC**

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### **Community Planner, Architect**

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### **Landscape Architect**

**SITESCAPES**

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### **Civil Engineer**

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## Chapter 1: Specific Plan Overview



New York City, New York: Park Row and City Hall Park







# Parkside

## 1.0 Specific Plan Overview

Parkside is ~~proposed as a new~~ comprised of approximately 250.899 gross acre ~~planned community located in the south/central portion of the New Model Colony (NMC) acres.~~ The Specific Plan includes the western portion of Subarea 23, between Cucamonga Creek and Archibald Ave., and the eastern portion of Subarea 22, between Cucamonga Creek and future Carpenter Ave. The entire Specific Plan area amounts to 237.899 acres in ~~26~~ 25 planning areas (~~exclusive~~ of Cucamonga Creek). Net project acreage amounts to 203.2 acres. Refer to Table 1-1: ~~Parkside~~ Land Use Summary.

The regional context and local setting of the Specific Plan area are illustrated in ~~the Exhibit 1-1: Regional Location Map Exhibit 1~~ and ~~Exhibit 1-2: Vicinity Map Exhibit 2.~~

The ~~project~~ Specific Plan is consistent with and implements the ~~gross residential density~~ Medium Density Residential allocation (11.1 to 25 dwelling units permitted per gross acre) and broad “community design” policy requirements for the ~~New Model Colony Ontario Ranch~~, including a general neo-traditional community design. These include:

- Integration with and connectivity to the Great Park;
- Creation of a “sense of place;”
- Diversity in housing product and architecture;
- Pedestrian connectivity;
- Smaller “neighborhoods;” and
- Elimination of the “subdivision” feel.

~~The New Model Colony General Plan Amendment adopted by the City of Ontario designates the subject~~

~~portions of Planning Subareas 22 and 23 for development of low and high density residential and park land uses. Residential land uses are delineated with various areas assigned for development with a total of 1,947 residential units permitted. As part of the project, a General Plan Amendment will be adopted concurrently with the Parkside Specific Plan, modifying only the General Plan Land Use Policy Map to 1) reassign the locations of the high density residential uses, eliminate low density designations, and create new medium density designations; 2) add a Neighborhood Center commercial designation at the intersection of Archibald Ave. and Edison Ave., and 3) add a community facility designation (fire station), west of the Neighborhood Center.~~

On January 26th, 2010, the City adopted The Ontario Plan (TOP) which serves as the City’s new business plan and includes a long-term Vision and a principle based Policy Plan (General Plan). The City’s Policy Plan, which acts as the City’s General Plan, designates (Policy Plan Exhibit LU-1- Land Use Plan) the project site for development of Medium Density Residential (11.1 to 25 dwelling units per acre), Neighborhood Commercial, Public Facility, and Open Space-Parkland.

Consistent with the adopted General Plan, Parkside is allowed a maximum of 1,947 residential units, Neighborhood Commercial area, a fire station and a Chino Desalter Authority (CDA) production well site; see Table 1-1: Parkside Land Use Summary.



# Parkside

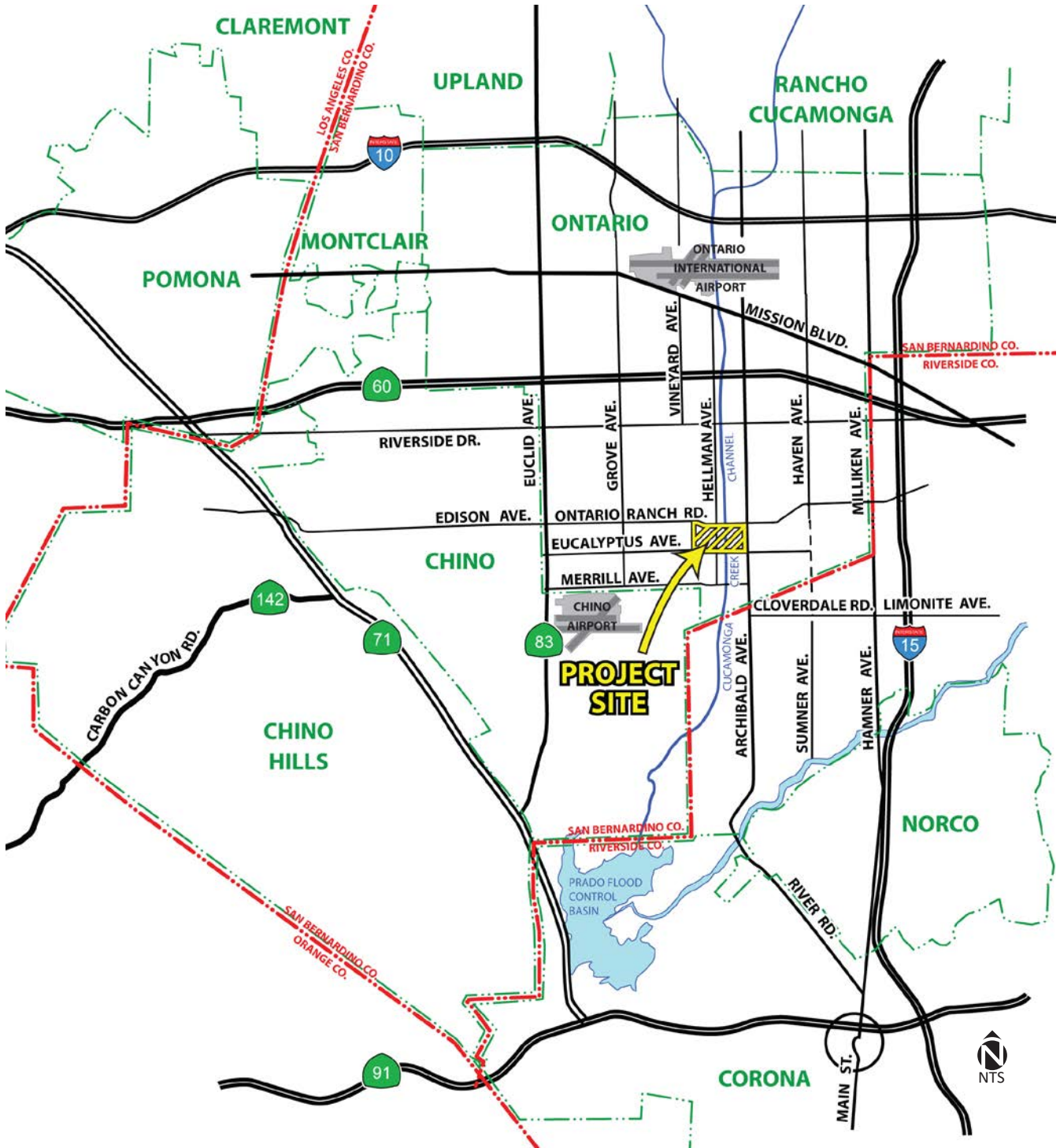
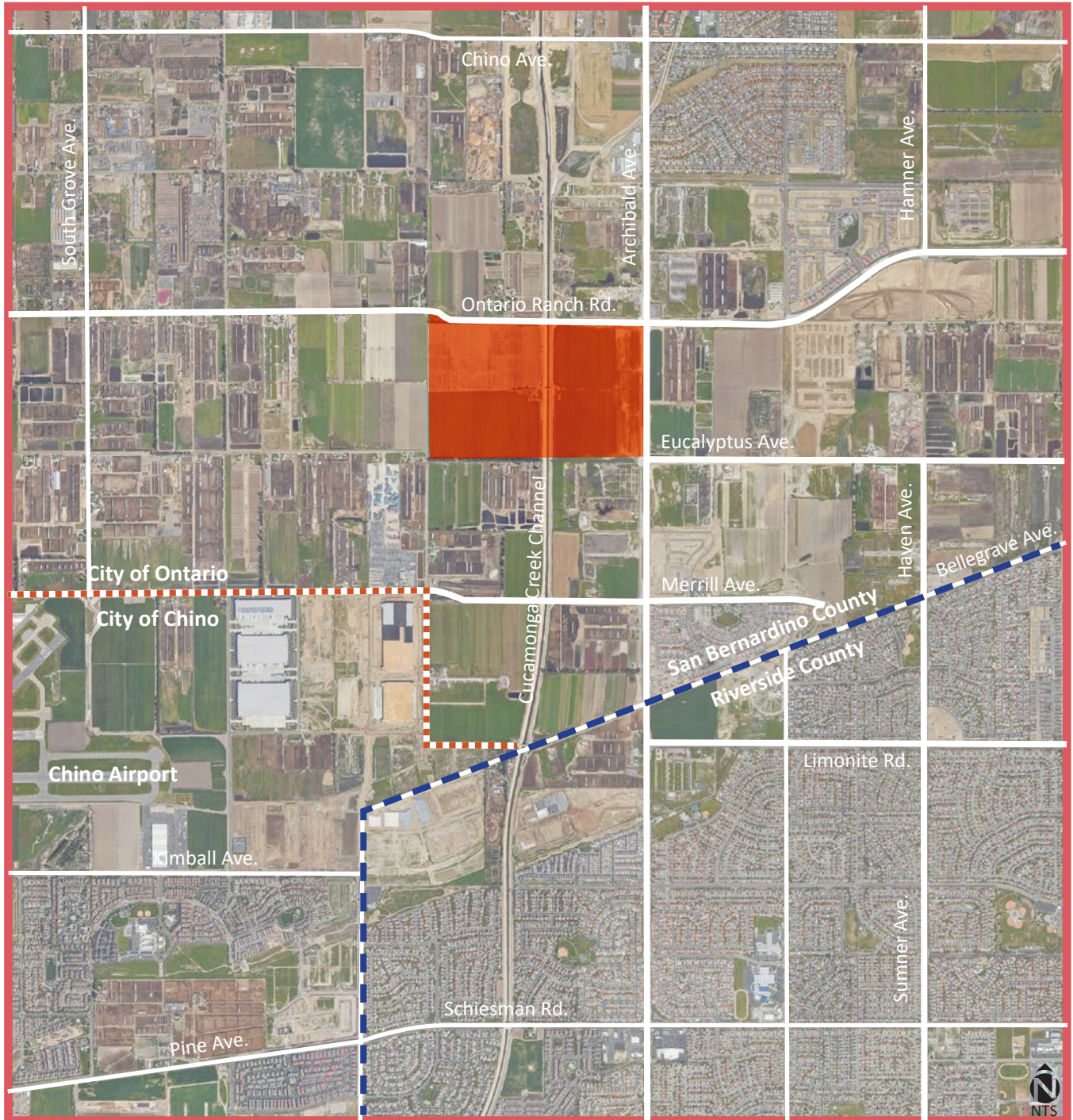


Exhibit 1-1: Regional Location Map



# Parkside



**Legend**

- Specific Plan Site
- County Boundaries
- City Boundaries

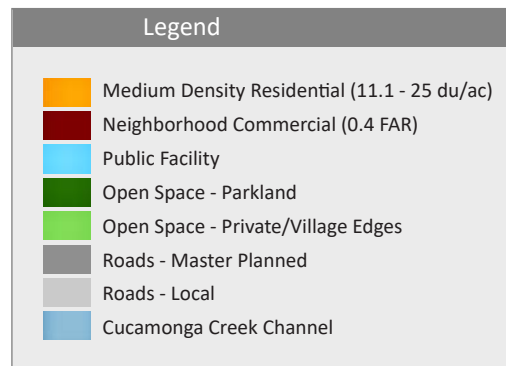
Exhibit 1-2: Vicinity Map



# Parkside



Exhibit 1-3: Land Use Plan





# Parkside

A variety of residential neighborhoods are planned within Parkside, with varying product design and architectural character. Residential products envisioned for Parkside include, but are not limited to, [conventional/compact single family detached homes](#), green/motorcourt clusters, triplexes, townhomes, condominiums, and apartments. A maximum total of 1,947 dwelling units are proposed for Parkside.

It should be noted that use of the term “Parkside” throughout this document applies to the entire Specific Plan area, held under a single ownership, by SC Ontario Development [Corporation Company, LLC](#) (“SC Ontario”) except for the Cucamonga Creek Channel.

The Parkside Specific Plan area occupies a strategic location [in the overall New Model Colony General Plan Land Use Plan](#), providing a crucial link between major active recreation uses planned east of Archibald Ave., and the future Town Center planned to the west of Parkside.

**Table 1-1: Parkside Land Use Summary**

Land Use	Lot Size / Use	Gross Acreage	Net Acreage	*Density Range / S.F.	Lots / D.U.
<b>Residential</b>					
PA-1 (Green Court/Lane Loaded Products 1, 2 & 4)	SFD	19.97	15.57	10-18	236
PA-2 (Lane Loaded Product 3)	SFD	3.57	2.66	10-18	41
PA-3 (Lane Loaded Product 3)	SFD	3.06	2.38	10-18	42
PA-4 (Green Court/Lane Loaded Products 1, 2, 3 & 4)	SFD	15.50	13.62	10-18	221
PA-5 (HD Townhomes)	MF	9.57	8.14	15-22	139
PA-6 (Attached Auto Court)	MF	7.34	6.89	8-14	75
PA-7 (Auto Court 6-8 Plex)	SFD	9.30	7.49	8-14	68
PA-8 (Green Court 8-10 Plex)	SFD	6.64	6.18	10-16	68
PA-9 (Triplex)	MF	10.49	8.70	10-16	120
PA-10 (Row Townhomes)	MF	7.56	7.22	17-25	143
PA-11 (HD Townhomes)	MF	8.11	7.22	15-22	123
PA-12 (Triplex)	MF	11.80	9.30	12-18	127
PA-13 (Attached Auto Court)	MF	8.53	6.34	8-14	64
PA-14 (Duplex)	MF	7.12	6.18	12-18	99
PA-15 (Duplex)	MF	8.62	6.97	12-18	111
PA-16 (Triplex)	MF	7.83	7.35	10-16	90
PA-17 (Row Townhomes)	MF	4.87	4.54	17-25	82
PA-18 (HD Townhomes)	MF	6.74	5.04	15-22	98
<b>Residential Subtotal</b>		<b>156.62</b>	<b>131.79</b>	<b>14.77</b>	<b>1,947</b>
<b>Non-Residential</b>					
PA-19 (Commercial)		15.66	11.15	115,000 SF	-
PA-20 (Fire Station)		1.72	1.39	-	-
PA-21 (Great Park)		58.86	54.40	-	-
		1.00	1.00	-	-



# Parkside

PA-23 (Private Rec)	1.90	1.34	-	-
PA-24 (Private Rec)	1.00	1.00	-	-
PA-25 (Private Rec)	1.00	1.00	-	-
PA-26 (CD Well Site)	0.13	0.13	-	-
<b>Non-Residential Subtotal</b>	<b>81.27</b>	<b>71.41</b>	<b>115,000 SF</b>	<b>-</b>
Channel ROW	13.00	13.00	-	-
<b>Total Project</b>	<b>250.89</b>	<b>216.20</b>		<b>1,947</b>

NOTES:

- 1) Gross acres taken to center line of streets.
- 2) Adjusted gross acres taken to street ROW dedicated to the City
- 3) Net acres taken to street right of way
- 4) A total of 34.69 acres of master planned roadway is included in the project
- \* Density range is shown is based on net acres. The General Plan is based on adjusted gross acres
- 5) Proposed product types in each Planning Area are subject to change based on final development submittal.

Homes will frame the Great Park community recreation area, defining public space, activities, and visually establishing a community “sense of place.”

Non-residential uses proposed in the Specific Plan include a ~~15.49-gross-acre~~ commercial ~~parcel~~ lot on the southwest corner of Ontario Ranch Rd. and Archibald Ave. A public fire station ~~is included~~ directly west of the commercial site ~~and that~~ fronts on the Great Park, ~~and a CDA well site.~~

The Great Park ~~consisting of 58.47 gross acres,~~ extends east to west through the center of the Specific Plan area ~~with segments extending north and south into the residential planning areas,~~ and provides a unique community sense of place. All of the Parkside community is within a 1/4 mile walk of the Great Park, establishing Parkside as a walkable community.

A system of pedestrian trails ~~and sidewalks links~~ all residential neighborhoods with the planned park and internal private recreation amenities. Additionally, connections to the ~~paseos along~~ Cucamonga Creek Channel ~~trail~~ are planned.

The internal street system includes a modified grid and landscaped parkways in all local residential streets. Refer to Exhibit 1-3: Land Use Plan.

## 1.0.1 Purpose for Amendment

This Specific Plan Amendment (2019) modifies the land use plan to improve the interface between the residential neighborhoods and the Great Park, provide better internal circulation and access to the private recreation centers, incorporate a CDA production well site, introduce new home types to support the goals of the Specific Plan, add additional architectural styles and reallocate density between planning areas. The range of home types provides for a varied and dynamic neighborhood composition serving a wide range of family and income types. New home types with development standards have been introduced to increase the home type variation in the neighborhoods and increase opportunities for homes with architecture-front designs. The new home types include high density detached homes, cluster configurations, condominiums and apartments.



# Parkside

The modifications to the Specific Plan have been incorporated into the body of the text, standards, and exhibits of this document. Revisions to the Design Guidelines, ownership name change and minor text revisions have also been completed. The amendment shall not be referenced for the remainder of this document as the standards and guidelines of the Specific Plan are designed to create a cohesive community.

## 1.0.2 Governing Documents

Development of Parkside will be regulated by the following documents:

- The [City of Ontario Sphere of Influence Plan \(TOP\) Policy Plan \(General Plan Amendment \(January 1998\); as amended\)](#) which establishes policies governing land use, circulation, housing, conservation and open space, noise, safety, and public facilities within the [Parkside](#) Specific Plan area;
- The Parkside Specific Plan, [to include](#) a Land Use Plan, Infrastructure Plan, Design Guidelines, and Development Regulations;
- The [City of Ontario Development Code](#), as applicable to the project [site](#), where the [Parkside](#) Specific Plan is silent on development standards and regulations;
- The [Airport Land Use Compatibility Plans of Chino Airport and Ontario Subdivision Ordinance, regulating the subdivision of land within the International Airport](#);
- [The approved Parkside project area Environmental Impact Report](#); and

- Covenants, Conditions, and Restrictions (CC&R's) to be established by the [developer of Parkside, and other](#) developers of Parkside as a means of ensuring and enforcing quality design and development of the master planned community.

## 1.0.3 Specific Plan Components

The Parkside Specific Plan is organized into the following sections in addition to Section 1, Specific Plan Overview.

### Section 2: Introduction

The Introduction provides:

- The project setting;
- A general description of the project proposal;
- The goals and policies of the Specific Plan;
- The entitlements to accompany the Specific Plan;
- The relationship of the Specific Plan to [The Ontario New Model Colony Plan \(TOP\) Policy Plan \(General Plan Amendment, as amended\)](#) and the Ontario Development Code.

### Section 3: Existing Conditions

This section describes the setting for the project, outlining the existing physical conditions on and around the site [at the time of the original approval](#).

### Section 4: Land Use

The Land Use section describes the various residential planning areas and residential types, commercial planning area, public use planning area, and the parks and [multi-purpose](#) trails within the planned community.



# Parkside

## **Section 5: Infrastructure and Public Improvements**

This section provides information on proposed circulation improvements, the planned backbone water, sewer, and storm drain systems, the grading concept for the project [site](#), and a discussion of public utilities and services to serve the Specific Plan area.

## **Section 6: Development Regulations**

The Development Regulations identify permitted uses and standards regulating the development of various residential product types, commercial use, fire station, [well site](#) and parks. The relationship of the Parkside Specific Plan development regulations to the Ontario Development [Ordinance Code](#) is also provided. The policies and procedures for the City's review and approval of future development proposals within the Specific Plan are included in this section. This section provides the methods and procedures for interpreting and amending the Parkside Specific Plan, as necessary [in the future](#).

## **Section 7: Design Guidelines**

The Specific Plan Design Guidelines are intended to direct the site planning, landscaping, and architectural quality of the development. Streetscapes, entries, edge treatments, walls and fencing, lighting, signage, and architectural design are addressed in the Design Guidelines.

## **Section 8: Commercial Design Guidelines**

This section will direct site planning, architectural design, and landscaping for the commercial [parcel lot](#).

## **Section 9: General Plan Consistency**

This section includes the [City of Ontario](#) General Plan consistency matrix describing the relationship of the Specific Plan to each policy of [the New Model Colony The Ontario Plan \(TOP\) Policy Plan](#) (General Plan [Amendment](#)).



## Chapter 2: Introduction



New York City, New York: Central Park





# Parkside

## 2.0 Introduction

The Specific Plan is comprised of 250.899 gross acres included in Planning Subareas 22 and 23 of the **New Model Colony Ontario Ranch** within the City of Ontario. The Specific Plan portions of Planning Subareas 22 and 23 are comprised of 133.76 gross acres and 104.13 gross acres respectively, exclusive of 13 acres included in the Cucamonga Creek Channel. The project site illustrated in Exhibit 1-2: Vicinity Map is bounded by **Edison Ave. Ontario Ranch Rd.** on the north, Archibald Ave. on the east, **Merrill Eucalyptus Ave.** on the south, and Carpenter Street on the west. The Parkside Specific Plan is a comprehensive plan proposed by SC Ontario Development **Corp Company, LLC** for the development of residential neighborhoods designed to reflect the small town character of **older** established communities. The Specific Plan also includes a commercial site, parks (public and private), **and** a fire station, **and a well site.**

## 2.1 Purpose and Objectives

### 2.1.1 Purpose

~~The Specific Plan comprehensively describes residential, recreational, commercial, and public (fire station) land uses planned for the site. The resolution adopting the Specific Plan establishes the zoning for the site and defines the development regulations, requirements, and design guidelines governing development of the project, as well as the procedures and requirements to approve development within the Specific Plan, to ensure that the City of Ontario's Sphere of Influence General Plan Amendment, as amended, is implemented.~~

The Parkside Specific Plan serves to implement the City's Policy Plan (General Plan) for the project site (refer to Exhibit 2-1: Policy Plan Land Use Plan and provides

zoning regulations for development of the project site by establishing permitted land use, development standards, infrastructure requirements, and implementation requirements for development. A comprehensive set of design guidelines and development regulations are included to guide and regulate site planning, landscape, and architectural character within the community ensuring that excellence in community design is achieved during project development. The Parkside Specific Plan establishes the procedures and requirements to approve new development within the project site.

The Specific Plan is designed to address the following guiding planning principles:

- Diversity and choice of single-family and multi-family housing types and opportunities to address a variety of lifestyles, home sizes, and economic segments of the marketplace;
- Connectivity among land uses within the Specific Plan area and to surrounding public facilities and the existing Ontario community, including the Great Park; [Refer to Exhibit 2-2: Policy Plan Functional Roadway Classification Plan,](#)
- Use of traditional development patterns found in older established neighborhoods in Southern California;
- Recreational amenities within walking distance of all residential neighborhoods;
- Bicycle and pedestrian accessibility and mobility to encourage alternative modes of travel, within the project site, and along the Cucamonga Channel Regional Trail;
- Diversity in architectural design; and



# Parkside

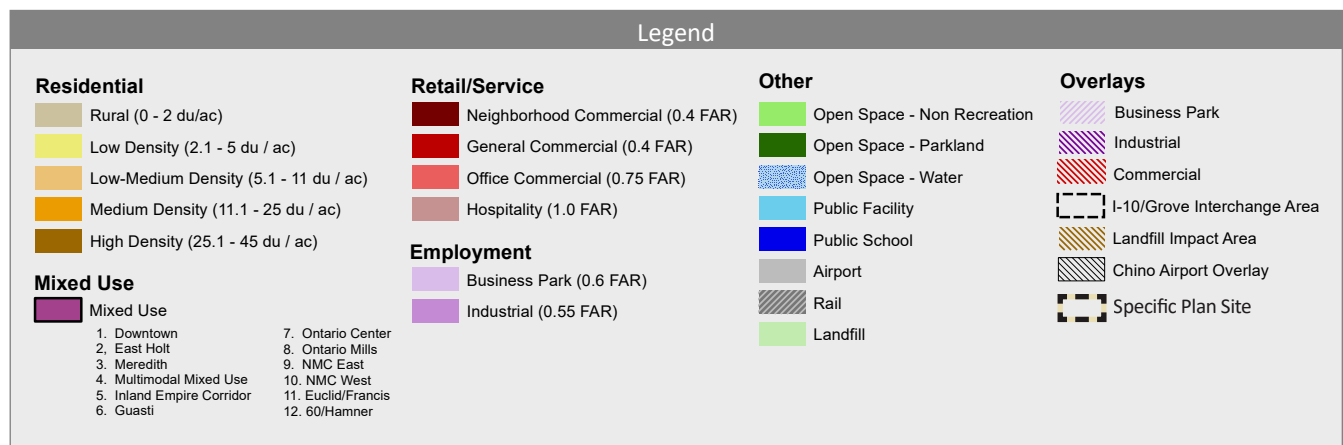
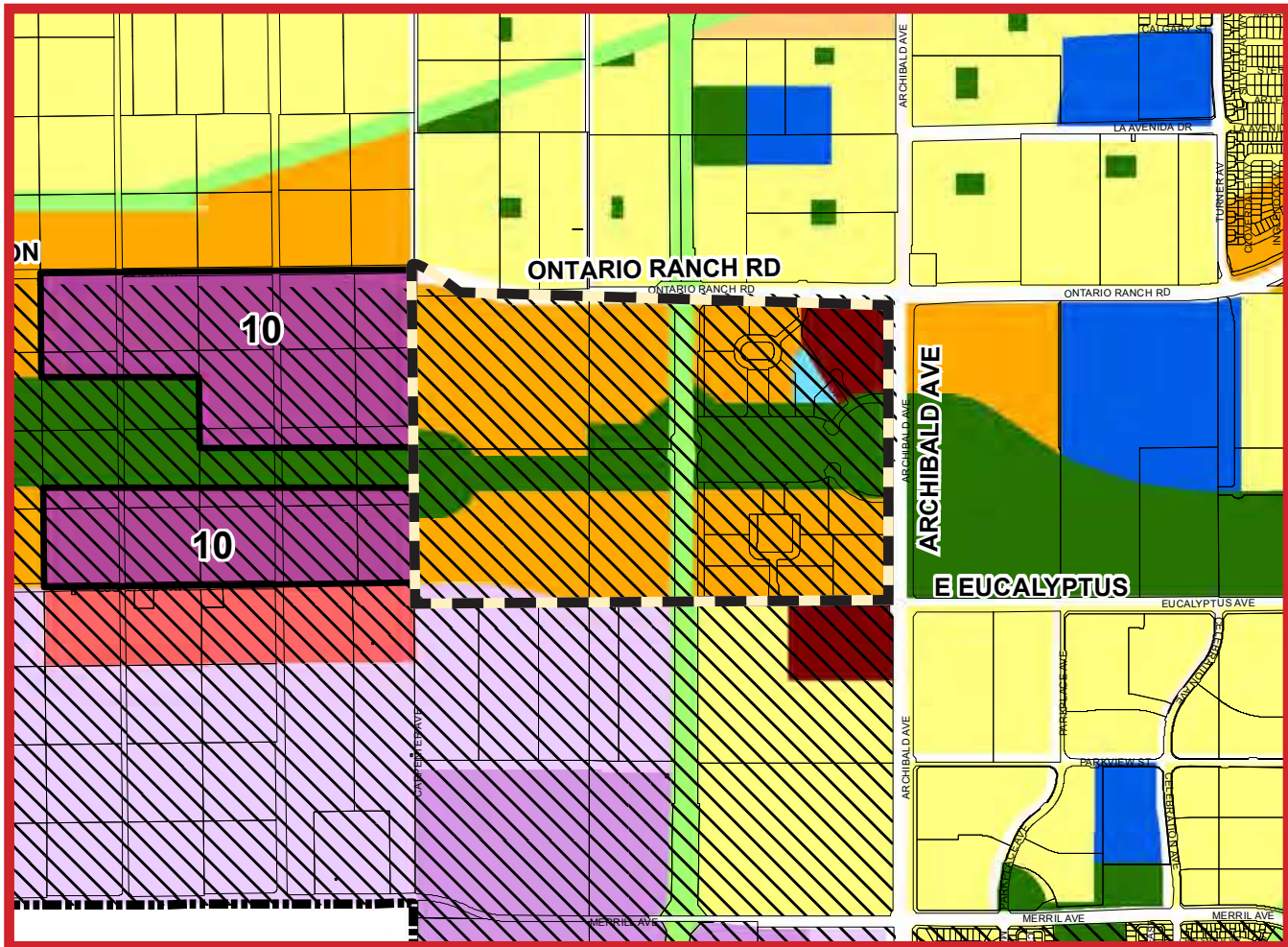


Exhibit 2-1: Policy Plan Land Use Plan



# Parkside

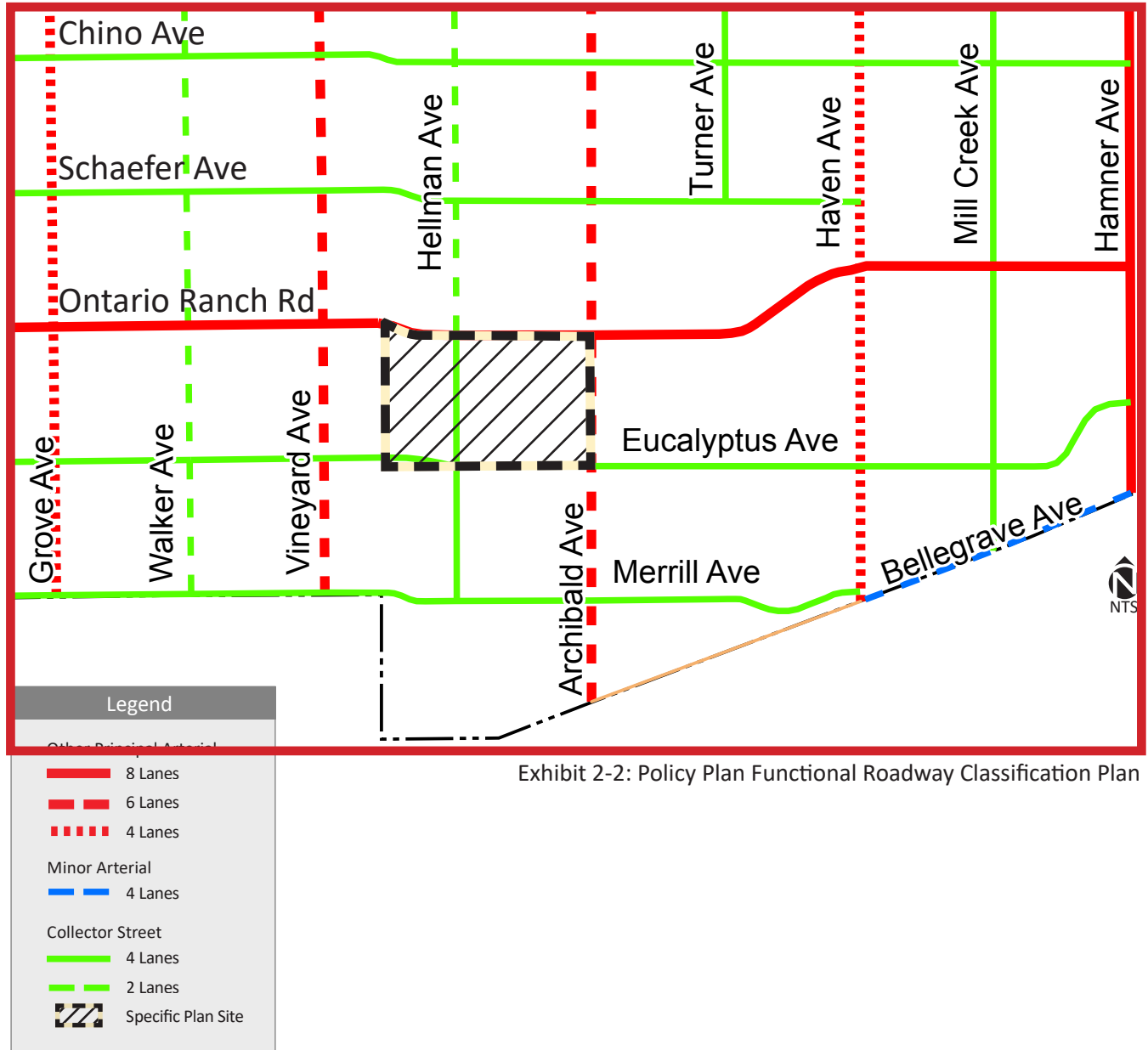


Exhibit 2-2: Policy Plan Functional Roadway Classification Plan



# Parkside

- Sustainable development practices addressing energy efficiency.

## 2.1.2 Objectives

The following objectives are established for the Specific Plan.

### Residential Areas

- Residential neighborhoods designed at a “human scale” and oriented to pedestrian activity;
- Connectivity provided among residential neighborhoods and recreational areas through a network of pedestrian sidewalks ~~and on-street bicycle trails~~;
- Connectivity provided between residential neighborhoods and the adjacent commercial land use, as well as to the central Great Park, by ~~means of pedestrian and bicycle~~ multi-purpose trail linkages, paseos and sidewalks;
- A variety of housing types incorporated into the land use plan addressing lifestyle considerations of singles, families, and empty nesters;
- Residential neighborhoods designed around a Great Park green core, framing green spaces and promoting passive recreational activity and casual social interaction among neighbors; and
- Residential neighborhoods with diverse architectural styles ~~and with both traditional and modern~~ design elements ~~reflecting the characteristics of older established Ontario neighborhoods~~.

### Commercial Area

- Neighborhood commercial uses to meet the needs of the residential community within the project site as well as the larger surrounding market area;
- Provision for ~~paseos, trails, and sidewalks~~ connecting the residential community with the commercial center and adjacent uses;
- The development of plaza areas and other amenities within the commercial center providing space for social interaction; and
- Orientation of commercial buildings to the street and Great Park wherever possible, to create an urban edge and sense of arrival.

### Streets and Pedestrian/Bicycle Mobility

- Streets and lanes (private alleys) designed in a modified grid pattern, reminiscent of traditional neighborhood streets;
- Streets with landscaped parkways and pedestrian walkways separated from the street, to create a pleasant pedestrian environment, promoting friendly interaction among neighbors;
- Several traffic calming techniques within the proposed street design, including enhanced parkway landscaping, median landscaping, tapered street intersections, and alley entrances to influence a driver’s peripheral vision and encourage motorists to drive more slowly; and
- Inside turning radii at corners reduced to slow traffic at corners.



# Parkside

## Recreation/Multi-purpose Trails

- New recreational opportunities for residents through the development of the Great Park to include areas for passive recreation;
- Private recreation amenities;
- ~~A portion~~ Extension of the City's ~~Master Plan of planned multipurpose~~ trails ~~provided through development of pedestrian trails~~ the Specific Plan including the multi-purpose trail within the Cucamonga Creek Trail Corridor connecting ~~Edison Ave.~~ Ontario Ranch Rd. with ~~Merrill Eucalyptus Ave.~~, and the surrounding area; and
- A system of ~~on-street bikeways~~ multipurpose trails integrated into the ~~project~~ Specific Plan area providing pedestrian and bicycle access from the residential community to surrounding areas.

## Sustainable Development

- Native plant materials and non-invasive ornamental landscape materials will be incorporated into the landscaping plan.
- Residential development will be connected to the commercial land use area located within the site through pedestrian and bicycle trails promoting walking and bicycling as an alternative means of ~~transportation~~ automobile travel to and from these areas;
- Residential development will be equipped with the latest technology for internet access allowing residents to shop and work on-line, thereby reducing vehicle trips to employment centers and shopping;

- ~~Homes will be sited on east/west street alignments wherever feasible, to allow for a southerly orientation of residential development to promote energy efficiency;~~
- Canopy trees will be utilized in public areas such as parkways, medians, and the Great Park ~~to achieve natural ventilation and cooling~~ to provide shade to counteract the heat island effect;
- Roadway widths will be reduced as much as possible to minimize heat generating asphalt surfaces; and
- Recycled water will be utilized for the Great Park, neighborhood edges and other common landscape areas.

## 2.2 Specific Plan Proposal

### 2.2.1 Project Summary

The Specific Plan Land Use Plan is described below and in the Table 1-1: Parkside Land Use Summary.

#### A. Residential Uses

The Specific Plan will provide for development of a variety of residential housing types, many oriented toward park and open space amenities and designed to promote walkability and interaction among residents. Residential development within ~~156.83 gross acres of~~ the Specific Plan area will contain up to 1,947 dwelling units (per Table 1-1: Parkside Land Use Summary), providing a ~~variety~~ varied mix of single-family detached, single-family attached, and multi-family attached housing types as described below.



# Parkside

## 1. Residential Detached

Examples of single-family detached residential products that may be used in the Specific Plan include:

### 2- and 3-Story Detached Homes

Single-family detached homes may consist of residential units with vehicular access provided from lanes (private alleys) or motor courts, via interior streets, with direct access garages. These residential areas will be developed at an approximate density of 8-14 dwelling units per acre.

### Green Court and Motor Court (~~PA1, PA2, PA6, PA7, PA8~~):

Green Court/Cluster or Motor Court/Cluster single-family detached residential development may consist of residential units with vehicular access provided from lanes (private alleys) or motor courts, via interior streets, with direct access garages. These residential areas will be developed at an approximate density of 10-16 dwelling units per acre.

Larger Green Court/Cluster single-family detached residential development consists of lane-served residential units designed with an orientation to the street or green court spaces. Residential areas will be developed at an approximate density of 8-14 dwelling units per acre.

## 2. Residential Attached

Examples of multi-family attached residential products that may be used in the Specific Plan include:

### Triplex (~~PA13, PA16~~):

~~Residential development in PA13, PA16 may consist of triplex-Triplex residential homes will be~~ designed with

vehicular access provided from lanes. This residential area will be developed at an approximate density of 10-16 dwelling units per acre, with some buildings fronting the Great Park. Triplex buildings ~~would~~ could include townhomes, flat or carriage units in 3 unit per ~~building structure~~ configurations.

~~Townhomes (PA3, PA4, PA5, PA9, PA10, PA11, PA12, PA14, PA15, PA17, PA18, PA19):~~

### Townhomes:

Multi-family attached units with orientation to the Great Park and local streets with direct access garages, in varying orientations. Residential neighborhoods will be developed at an approximate density of 12-25 dwelling units per acre, in townhome and condominium designs.

### Multi-family Attached Homes

High density multi-family attached homes accessed from lanes (private alleys). Densities average approximately 18-25 dwelling units per acre, and include such residential uses as apartments, condominiums, and townhomes. ~~Multi-family buildings in these four (4) planning areas will front on the "Great Park."~~

## 2.3 Authority and Requirements

### 2.3.1 Authority

State ~~of California~~ Government Code, Title 7, Division 1, Chapter 3, Article 8, Section 65450-57 grants authority to cities to adopt Specific Plans for purposes of implementing the goals and policies of their General Plans. The Government Code specifies that specific plans may be adopted either by resolution or by ordinance and that the Specific Plan is required to be consistent with the General Plan. The City ~~of Ontario will adopt the Subareas~~





# Parkside

22 and 23 Specific Plan adopts specific plans by resolution ordinance, thereby establishing the zoning regulations for development of the project site. The requirements of the Specific Plan shall take precedence over the Ontario Development Code. In instances where the Specific Plan is silent, the City of Ontario Development Code shall prevail.

## 2.3.2 Requirements of the Specific Plan

California Government Code Section 65451 sets forth the minimum requirements and review procedures for Specific Plans as follows:

A Specific Plan shall include a text and a diagram or diagrams, which specify all of the following in detail:

- The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan;
- The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the project area covered by the plan and needed to support the land uses described in the plan;
- Standards and criteria by which improvements will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable;
- A program of implementation measures including regulations, programs, public works projects and the financing measures necessary to carry out the paragraphs 1, 2, and 3 above; and

- The Specific Plan shall include a statement of the relationship of the Specific Plan to the General Plan.

The Parkside Specific Plan meets the requirements of the State of California Government Code.

## 2.3.3 Development Agreement Required

Unless done in a coordinated manner and with adequate fiscal planning, development projects within the New Model Colony Ontario Ranch are likely to present a challenge in their implementation because of the lack of existing public facilities, including, streets, sewerage, transportation, drinking water, school, and utility facilities. California law has established a mechanism for ensuring the adequate provision of such facilities, while at the same time providing assurances to applicants that, upon approval of the project, the applicants can proceed with their projects. Approval of this Specific Plan without a development agreement may result in a waste of resources, escalate the cost of housing to the consumer, and discourage investment in and commitment to comprehensive planning, as envisioned by the City, which seeks to make maximum efficient utilization of resources at the least economic cost to the public.

Therefore, a statutory development agreement, authorized pursuant to California Government Code sections 65864 et seq., shall be required as part of the approval of this Specific Plan. For the above-mentioned reasons, the development agreement for this Specific Plan shall include, among other things, methods for financing acquisition and construction of infrastructure, acquisition and development of adequate levels of park land, and schools, as well as the provision of adequate housing opportunities for various segments of the



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community consistent with the regional housing needs assessment. Such development agreement shall have been fully approved before the issuance of the first building permit for this project.

## 2.4 Development Approval Components

The components of the development approval process for the Specific Plan are as follows:

### General Plan ~~Amendment~~ Policy Plan

The Ontario Plan (TOP) establishes the direction and vision for the City providing a single guidance system that will shape the Ontario community for the future. TOP provides for policies to accommodate change over a 30-year period commencing in 2010, the beginning of the planning period. TOP consists of a six part Component Framework: 1) Vision, 2) Governance Manual, 3) Policy Plan, 4) City Council Priorities, 5) Implementation, and 6) Tracking and Feedback.

### Specific Plan

~~A General Plan Amendment will be adopted for the project prior to the approval of the Specific Plan. The General Plan Amendment, when adopted, will modify the General Plan Land Use Map to:~~

- ~~• Reassign the high density residential uses around the “Great Park” as the central organizational element of the community;~~
- ~~• Eliminate low density designations;~~
- ~~• Create new medium density designations;~~

The Policy Plan (General Plan) requires the approval of a Specific Plan for development of the project site to ensure that sufficient land area is included to achieve unified districts and neighborhoods. The City has zoned the project site as Specific Plan. This zoning designation requires that a Specific Plan be approved to guide development of the project site and to implement the goals and policies of the Policy Plan. The Specific Plan provides the zoning regulations to govern development of the project site. The requirements of the Specific Plan shall take precedence over the Ontario Development Code. In instances where the Specific Plan is silent, the Ontario Development Code shall prevail.

~~Center designation adjacent to the park and Archibald Avenue;~~

- ~~• Add a community facility (fire station) designation directly west of the Neighborhood Center Commercial site; and~~

- ~~• Create a revised 237.9-acre planning area.~~

~~No other changes to the General Plan are proposed. The Specific Plan is consistent with overall residential land use intensity established by the General Plan. Refer to Exhibits 4, 5, and 6, General Plan Land Use Plan, General Plan Amendment, and Specific Plan Subareas.~~

~~Table 2a, General Plan Acreages (Existing) summarizes the total Parkside land use acreages for Subarea 23 in the New Model Colony General Plan, as adopted in 1998:~~

~~Table 2a—General Plan Acreages (Existing)~~

~~Land Use Designation — Approx. Acres (gross)~~



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K-5/K-6 School—10

Neighborhood Center—10

Residential—192

Parks/Village Green—205

Total—417 (Subarea 23)

Table 2b, General Plan Acreages (Proposed) summarizes land use acreages consistent with the proposed General Plan Amendment.

Table 2b—General Plan Acreages (Proposed)

Land Use Designation—Approx. Acres (gross)

Residential Medium Density

(12 du average per gross acre)—63.53

Residential High Density

(18 du average per gross acre)—93.51

Neighborhood Center—15.49

Great Park, Paseos—63.79

Public Use (fire station)—1.57

Total—\*237.89

(Subareas 22 and 23)

\*Acreage does not include Cucamonga Creek Channel

## Specific Plan

The Specific Plan provides the zoning for the entire site. It serves as “blueprint” for future development by establishing the distribution of land use and criteria for development as set forth herein. The Specific Plan also serves as the legal document to implement the City’s General Plan, as amended by this Specific Plan proposal.

## Subdivision Maps

Tentative tract map(s) will be approved reviewed by the City of Ontario for the residential portion of the Specific Plan indicating the approximate location of lot lines, streets, and proposed grading. Following approval by the City of the tentative tract map(s), a final map(s) will be prepared. Parcel Lot maps will be approved by the City of Ontario for development proposed within the commercial portion of the Specific Plan.

## 2.5 CEQA Compliance

A project site Environmental Impact Report (EIR) prepared by the City for the Specific Plan, in accordance with the California Environmental Quality Act (CEQA), addressed impacts associated with the Specific Plan and subdivision map. The EIR recommended mitigation measures to reduce impacts of the project site to a less than significant level. The EIR was prepared as the basis for the environmental review for all subsequent discretionary and ministerial actions.



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## 2.6 Relationship to General Plan and Zoning

The ~~City of Ontario New Model Colony General City's Policy Plan Amendment~~, which acts as the City's General Plan, designates the ~~site as a portion of Planning Subareas 22 and 23 project site for development of the following land uses~~ as follows:

- Medium Density Residential (11.1-25 dwelling units per acre)
- Neighborhood Commercial
- Public Facilities
- Open Space-Parkland

The Policy Plan (General Plan) establishes a ~~maximum~~ development capacity of 1,947 ~~residential~~ dwelling units and ~~approximately~~ 115,000 square feet of commercial ~~use for the uses within Planning Subareas 22 and 23. Residential development capacity is single-family detached and multi-family attached.~~

~~The City of Ontario has pre-zoned the project site as "SP" (Specific Plan Ag Preserve). The zoning designation of "SP" requires use for the area to be developed with a Specific Plan that carries out objectives of the New Model Colony General Plan, with land uses consistent with the approved General Plan project site.~~

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits the adoption and administration of specific plans as an implementation tool for elements contained in the local general plan. Specific plans must demonstrate consistency in regulations, guidelines, and programs with the goals and policies set forth in the general plan. The Parkside Specific Plan has been prepared in conformance with the goals and policies of the City Policy Plan (General Plan). The policy analysis is located in the Appendix of this document, describes the manner in which the Parkside Specific Plan complies with the Policy Plan policies applicable to the project.

## 2.7 Development Plan Review

Following the approval of the Specific Plan, all development proposals for individual residential neighborhoods or product areas within the Specific Plan will be subject to the Development Plan Review process pursuant to ~~Article 8: Development Plan Review Chapter 2.0 Administration and Procedures~~ of the ~~City of~~ Ontario Development Code.

## 2.8 Airport Land Use Compatibility Planning Consistency

All development proposals or Specific Plan Amendments are required to be consistent with the Airport Land Use Compatibility Plans of Chino Airport and Ontario International Airport.

## Chapter 3: Existing Conditions/Utilities



Denver, Colorado: City Park, Robert Burns Monument





# Parkside

## 3.0 Existing Conditions/**Utilities**

### 3.1 Property Ownership

The Specific Plan area is comprised of approximately 250.9 gross acres. SC Ontario owns approximately 237.9 gross acres of the project site. San Bernardino County Flood Control District owns the right-of-way for the Cucamonga Creek Channel that comprises approximately 13.0 gross acres, and effectively divides the eastern half of the project site from the western half, crossing the site from north to south approximately 1,650 feet west of Archibald Ave. Exhibit 3-1: Existing Property Ownership depicts the property ownership within the Parkside Specific Plan area.

### 3.2 Airport Land Use Compatibility Plan Consistency

The Project Site is located within the Airport Influence Areas of Chino Airport and Ontario International Airport (ONT).

### 3.3 Existing Site Conditions and Improvements

The Parkside Specific Plan area is generally undeveloped and general agricultural operations are scattered throughout the area, including an irrigation pond located within the northwestern portion of the project site. The approximate 250.89-gross acre project site has historically been used for general crop-related agricultural purposes (alfalfa, corn, milo, sorghum, etc.) and flood control.

### 3.4 Surrounding Land Uses

Uses surrounding the Parkside Specific Plan area include rural residential, dairy farming, agricultural, nursery, Southern California Edison (SCE) Substation, and roadways. The surrounding land uses are as follows:

North: Dairy farming, poultry farming, nursery, crops, rural residential, and SCE Substation

South: Dairy farming and rural residential

East: Dairy farming, crops, and rural residential

West: Dairy farming, crops, and rural residential

Refer to Exhibit 3-2: Aerial Photo for further detail relative to existing and surrounding land uses.

### 3.5 Topography

The existing ground within the project site generally slopes to the southwest at approximately 0.8% to 2.0%. Exhibit 5-16: Conceptual Grading Plan illustrates the topographic features of the Parkside Specific Plan area.

### 3.6 Hydrology

The Parkside Specific Plan area is presently in agricultural use; therefore, only a limited portion of the project site is now covered with impervious surfaces. Normal rainfall for the area is, therefore, able to percolate through on-site soils and does not result in high volumes of surface runoff, typically associated with urban areas. The entire project site drains generally to the south and southeast towards the Cucamonga Creek. The project site is located approximately one-half mile south of the Cucamonga Basin. The Cucamonga Basin provides regional storm water runoff and drainage capacity to portions of south Ontario. The Cucamonga Creek Channel, which bisects the Parkside Specific Plan, is an improved concrete trapezoidal channel.



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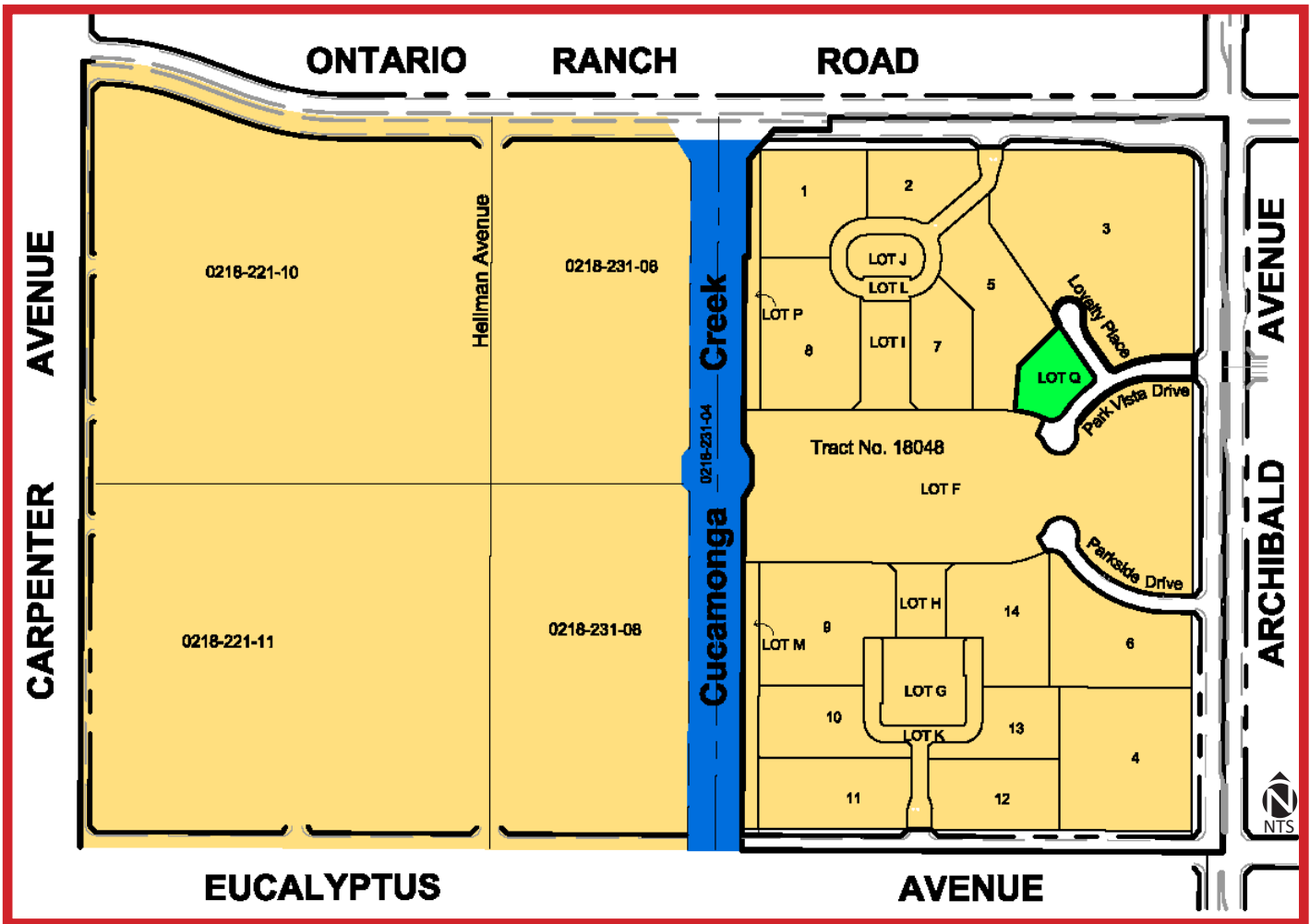


Exhibit 3-1: Existing Property Ownership

### Tract. No 18048: SC Ontario Development Company, LLC

Lot 1: 0218-231-09	Lot 9: 0218-231-17	Lot H: 0218-231-30
Lot 2: 0218-231-10	Lot 10: 0218-231-18	Lot I: 0218-231-31
Lot 3: 0218-231-11	Lot 11: 0218-231-19	Lot J: 0218-231-32
Lot 4: 0218-231-12	Lot 12: 0218-231-20	Lot K: 0218-231-33
Lot 5: 0218-231-13	Lot 13: 0218-231-21	Lot L: 0218-231-34
Lot 6: 0218-231-14	Lot 14: 0218-231-22	Lot M: 0218-231-35
Lot 7: 0218-231-15	Lot F: 0218-231-28	Lot P: 0218-231-38
Lot 8: 0218-231-16	Lot G: 0218-231-29	Lot Q: 0218-231-39

Legend	
<span style="display: inline-block; width: 15px; height: 15px; background-color: yellow; border: 1px solid black;"></span>	SC Ontario Development Company, LLC
<span style="display: inline-block; width: 15px; height: 15px; background-color: green; border: 1px solid black;"></span>	City of Ontario
<span style="display: inline-block; width: 15px; height: 15px; background-color: blue; border: 1px solid black;"></span>	San Bernardino County Flood Control





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Groundwater within and surrounding the project site, as a whole, contains certain high concentrations of salt, attributable to historic agricultural activities such as dairy farming. The high organic content of on-site soils has contributed incrementally to the degradation of surface and groundwater quality. Removal of the organic materials, which constitute by-products of those dairy operations, and compliance with National Pollution Discharge Elimination System (NPDES) and other storm water permit requirements will ~~beneficially impact regional water quality~~ be required as part of project site development.

~~The project level EIR, to be prepared for the Parkside Specific Plan, will provide additional drainage information:~~

## 3.7 Existing Circulation/Access

### 3.7.1 Regional Circulation

The Pomona Freeway, State Route (SR-60), is located 2.25 miles north of the project site. Currently, access to SR-60 is to the north, via Archibald Ave. In this region, SR-60 has full diamond-type interchanges with Euclid Ave., Grove Ave., Vineyard Ave., Archibald Ave., Haven Ave., and Milliken Ave. SR-60 carries approximately 160,000 vehicles per day in this area.

The Ontario Freeway, Interstate 15 (I-15), is located 2.4 miles east of the project site ~~The nearest existing~~ and carries approximately 90,000 vehicles per day in the vicinity of the Ontario Ranch area. The closest arterial ~~interchange to the Parkside Specific Plan area~~

is located at Limonite Ave. in Riverside County, which is 3.8 miles to the east, via Archibald Ave. to ~~Cloverdale Road (Cloverdale Road turns into~~ Limonite Ave. east of Hamner Ave). ~~The Cantu-Galleano or at Ontario Ranch Road is master-planned and will provide the Parkside Specific Plan access to I-15. The Cantu Rd./Cantu/Galleano Ranch Rd., located~~ 2.5 miles to the east via Edison Ave., from the project site.

State Highway SR-83 (Euclid Ave.) extends along the western boundary of the ~~New Model Colony~~ Ontario Ranch and is 2.5 miles west of the project site Specific Plan area via Ontario Ranch Road. Another major State Highway in the area is SR-71 (Chino Valley Freeway). This highway freeway is a major commuter route in southwestern San Bernardino County, connecting the SR-91 freeway with SR-60 and I-10 freeways. SR-71 is located 6.5 miles west of the Parkside Specific Plan area via Ontario Ranch Rd./Edison Ave.

### 3.7.2 Local Circulation

Access to the project site is provided from streets adjacent to the Parkside Specific Plan area. ~~Edison Ave.-Ontario Ranch Rd.~~ abuts the project site on the north and is currently improved with two travel lanes extending from west of the Archibald Ave. intersection, easterly to and beyond the western property boundary. Archibald Ave. abuts the project site on the east and



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Existing Groundwater Well Location

Exhibit 3-2: Aerial Photo



# Parkside

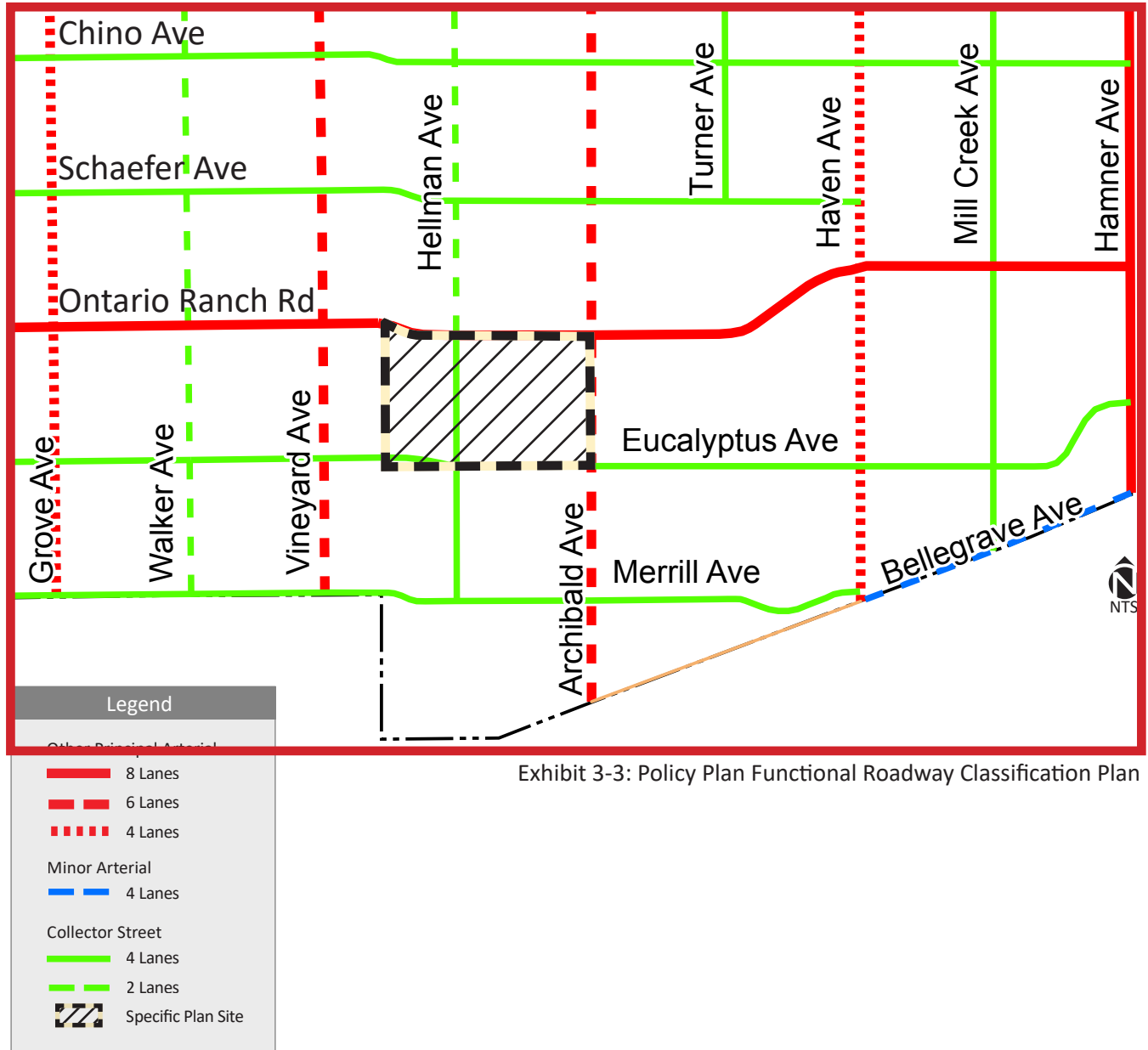


Exhibit 3-3: Policy Plan Functional Roadway Classification Plan



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is currently improved with four travel lanes, two travel lanes southbound and two travel lanes northbound and a landscape median. Eucalyptus Ave. (~~future Merrill Ave.~~) abuts the project site on the southwestern half of the Parkside Specific Plan area and is currently unimproved.

Exhibit 1-2, Regional Location Map illustrates the existing regional and local circulation surrounding the Parkside Specific Plan area. Exhibit 3-3: Policy Plan Functional Roadway Classification Plan illustrates the arterial streets in the area.

## 3.8. Existing Infrastructure/Utilities

### 3.8.1 Water

~~Currently there are no~~ New water mains are located in ~~either Edison Ave.~~ or both Ontario Ranch Rd. and Archibald Ave. adjacent to the Parkside Specific Plan area.

### 3.8.2 Groundwater Wells

In compliance with the Chino Basin Water Master's Well Procedure for Developers, a well use/destruction plan and schedule for all existing private/agricultural wells shall be submitted to the City of Ontario for approval prior to the issuance of permits for any construction activity. If a private well is actively used for water supply, the Developer shall submit a plan to abandon such well and connect users to the City's water system (residential to the domestic water system and agricultural to the recycled water system) when available. Wells shall be destroyed/abandoned per the California Water Resource Guidelines and require permitting from County Health Department. The location of existing wells are shown

~~in Figure 2.#, "Existing Wells". On-site residential and agricultural uses within the project site are served by three private wells, as illustrated on Exhibit 3-2: Aerial Photo. Upon development of the well site areas, these wells, or any other wells found on the property, will be abandoned per California Department of Water Resources Health Guidelines and the City Guidelines. In addition, a well use/destruction plan, as approved by the City, and schedule for all existing wells will be required. If a private well is actively used for water supply, the Developer shall submit a plan to abandon such well and connect users to the City's water system (residential to the domestic water system and agricultural to the recycled water system) when available. Wells shall be destroyed/abandoned per the California Water Resource Guidelines and require permitting from County Health Department. The location of existing wells are shown in Figure 3-2: Aerial Photo. If a water connection is not available an alternate source may still be proposed by the Developer and approved by the City. The City's Water Master Plan provides significant new water facilities to serve the project site including a new production well to be constructed on-site by the Chino Basin Desalter Authority. If the Developer proposes temporary use of an existing agricultural well for purposes other than agriculture, such as grading, dust control, etc., the developer shall make a formal request to the City for such use prior to issuance of permits for any construction activity. Upon approval, the Developer shall enter into an agreement with the City and pay any applicable fees as set forth by the agreement.~~



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### 3.8.3 Sewer

~~The City of Ontario does not have sewer facilities within the vicinity of the project site. Wastewater Sewer mains are located in both Ontario Ranch Rd. and Archibald Ave. Currently, wastewater~~ disposal within the Specific Plan area is currently provided through septic tanks and subsurface disposal fields. Prior to grading operations, existing septic tanks and subsurface disposal fields will need to be abandoned in accordance with Department of Health Services requirements. ~~The City of Ontario's Sewer Master Plan provides significant new facilities to serve the project site.~~

### 3.8.4 Stormwater/Drainage Facilities

The Cucamonga Creek Channel bisects the Parkside Specific Plan area. The Cucamonga Creek Channel is an improved concrete lined trapezoidal channel. During periods of heavy rainfall, when ground surfaces are saturated, surface runoff is collected in the existing drainage ditches located within the Specific Plan area. The project site is located approximately one-half mile south of the Cucamonga Basin. The Cucamonga Basin acts as a regional flood control facility for portions of south Ontario. The City's Master Plan of Drainage includes storm drain lines in ~~Merrill Eucalyptus~~ Ave., with connections to the Cucamonga Creek Channel, to drain on-site flows from the Specific Plan area.

### 3.8.5 Solid Waste Disposal

~~The City of Ontario Public Works Agency currently provides~~ There is no solid waste ~~collection and~~ disposal in the ~~New Model Colony~~ vicinity of the project site.

### 3.8.6 Natural Gas

The Southern California Gas Company (SCG) provides natural gas service within the project site. Facilities within the area include an existing 8 inch line, reducing to 6 inches, along Archibald Ave., a 6-inch line and a 36-inch transmission line along Ontario Ranch Rd. (the 36-inch transmission line is located along the south side of ~~Edison Ave-Ontario Ranch Rd.~~).

### 3.8.7 Electrical Facilities

Southern California Edison Company (SCE) provides electricity to the project site. SCE has overhead facilities in the area servicing the existing farms and dairies. Existing facilities surrounding the Specific Plan area, outside of the Specific Plan boundary, include 66 KV and 12 KV lines extending along the north side of Ontario Ranch Rd. and 66 KV and 12 KV lines extending along the east side of Archibald Ave. Existing facilities within the project site include a 12 KV line and a 120 V line located west of the Cucamonga Creek Channel. The development of the Specific Plan area will increase electrical power requirements and SCE will develop the required distribution system. All existing lines, less than 34.2 KV, within the Specific Plan area shall be removed or undergrounded.

### 3.8.8 Communication Systems

~~Verizon Spectrum~~ provides telephone service within the project ~~site area~~. Currently, telephone service is provided to the residences, dairies, and farms in the area.



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## 3.9 Geology and Soils

~~The City of Ontario General Plan EIR identifies the project site as underlain by Pleistocene age (older than 12,000 years) and Holocene age (less than 12,000 years old) alluvial deposits. The youngest surficial deposit is eolian sands (Qhs), comprising wind-blown sands having fine-to-medium-sized grains. These loose sands form sheets and low-dune deposits that have been stabilized by vegetation. These deposits are exposed in the eastern portion of the New Model Colony area and extend westward to an area defined generally by a diagonal line extending from Harrison (Turner) Ave. on the south to Vineyard Ave. on the north. It is expected that most of these materials will be uncemented and subject to consolidation when saturated under structural loads. Erosion potential is considered high. Foundation and backfill suitability should be satisfactory with proper over-excavation, mixing with a finer-grained binder material, and compaction.~~

The City General Plan EIR identified the project site as containing Delhi series soils, as mapped by the United States Department of Agriculture, Soil Conservation Service in 1971 and 1980. Delhi series soils have been used for agriculture, primarily for grapes and citrus, since the 1800's.

~~As part of the EIR prepared for the Parkside Specific Plan, additional geologic and soils information for the project site will be provided.~~

## 3.10 Seismicity

The ~~City of Ontario Sphere of Influence~~ General Plan identified numerous earthquake faults within a 50-mile radius of the project site. Major mapped faults include, but are not limited to, the Chino, Whittier and North Elsinore, and Cucamonga Faults. For the “maximum probable earthquake” (MPE), defined as the 100-year event normally considered in the design of non-critical structures, the values range from about 0.13 to 0.20 g (i.e., the unit force of gravity). In the design of certain critical or important facilities such as hospitals and dams, the “maximum credible earthquake” (MCE) ~~event is considered. For the three (3) faults, the MCE~~ should yield an estimated peak horizontal acceleration in the range of 0.33 to 0.52 g.

A zone of concentrated, relatively low-magnitude seismicity extends to the southwest from the San Jacinto fault zone (Rialto-Colton branch) along ~~what is referred to as~~ an “inferred fault near Fontana.” Where the “inferred fault” (Fontana trend) stops, this zone of micro-seismicity continues in a southwesterly to westerly direction terminating in the ~~Sphere of Influence area~~ **Ontario Ranch**. It is expected that the MPE for this fault structure could produce horizontal accelerations in the range of 0.3 to 0.5 g. More distant faults are capable of larger earthquakes with a higher probability of occurrence. The San Andreas fault is expected to generate a MCE event every 150 to 200 years, yielding a peak horizontal ground acceleration of approximately 0.21 to 0.26 g.

~~In accordance with the~~ **The** Uniform Building Code (UBC), ~~the Specific Plan area is located within Seismic Zone No.~~



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4. UBC establishes procedures designed to ensure that all subsequent development occurs in a safe manner relative to those known hazards.

As part of the EIR prepared for the Parkside Specific Plan, additional seismicity analysis will be prepared:

## 3.11 Vegetation

The project site has historically been extensively used for general crop-related agricultural purposes (alfalfa, corn, milo, sorghum, etc.) and is currently utilized for operations, including both dairy use and the cultivation of row crops. Rural Those areas not in active agricultural production are occupied by rural residential housing, farm buildings, and other ancillary facilities occupy those areas not in active agricultural production. The natural vegetation and soils conditions that once occurred throughout the project site have been significantly altered through human uses.

## 3.12 Biological Resources

The project EIR prepared for the City's NMC General Plan Amendment states that the Ontario NMC General Plan area has been greatly altered from natural conditions, under the influence of intensive agriculture and dairy industry:

Agriculture comprises the vast majority of the total land use within Ontario Ranch area (GPA EIR for the NMC, 1998). Consequently, the project area has been exposed to widespread and extreme levels of human-related disturbances through agricultural uses. The 250-acre site contains existing structures associated with crop production (e.g., sheds), evidence of former residential structures, cultivated areas, ruderal areas, and an irrigation pond. The large pond was historically

filled with industrial process wastewater generated by Sunkist Growers, Inc. as well as extracted ground water. The irrigation pond no longer receives water from Sunkist Growers, Inc. or wells and has dried up. Cucamonga Creek Channel, a concrete lined flood control facility, traverses the center of the project site, but offers little habitat function or value. Windrows, located on the north, south, and east project boundaries, represent the tallest vegetation on the site and offer perching and nesting opportunities for birds. Agricultural land uses surround the project site; however, urban development exists to the northeast within the City of Ontario, and to the southeast in Riverside County.

## 3.12 — STUDIES AND ASSESSMENTS PERFORMED

The following are the conclusions and recommendations of the varying studies/assessments performed on the project site:

- Blasland, Bouck & Lee, Inc was retained by Lewis Homes Enterprises to conduct a Phase I Environmental Site Assessment for the project site (see Appendix B1, under separate cover, "Phase I Environmental Site Assessment Sunkist Property, Chino, California"). The report states; "Based on the findings of this investigation, the subject properties appear environmentally suitable for their intended uses as residential developments."

- LOR Geotechnical Group, Inc was retained by Lewis Investment Company, LLC to conduct a Phase I Environmental Site Assessment Update for the project site (see Appendix B2, under separate cover, "Phase I Environmental Site Assessment Update Sunkist Property 9343 Edison Ave. Ontario California"). The report states; "Based on the findings of the site investigation and



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with consideration given of recognized environmental conditions herein, (1) the subject property exhibits no evidence of recognized environmental conditions that would prohibit its intended use as residential/mixed use development, and (2) no further tests or investigations are recommended except as expressly stated in this report for such intended use.”

• Glenn Lukos Associates was retained by Lewis Operating Corp. to perform a Biological Constraints Analysis for the project site (see Appendix B3, under separate cover, “Results of Biological Constraints Analysis for 230-acre property located Southwest of the Archibald Ave. and Edison Ave. intersection, San Bernardino County, California”). The report states:

“No sensitive habitats were identified on the site.”

“Although Delhi sands have been identified for a majority of the property, habitat for the federally-endangered Delhi sands giant flower-loving fly is not present on site due to severe degradation of habitat associated with raising cattle and lack of the preferred DSFL host plants on site. Therefore, the site does not represent suitable habitat for this species and no further action regarding this species is required.”

• Larry Munsey International was retained by Lewis Investment Company LLC to perform a Focused Survey for Delhi Sands Flower-Loving Fly for the years 2002, 2003 and 2005 (see Appendix B4, under separate cover, “Report of the Year 2002 Focused Survey For Delhi Sands Flower-Loving Fly at Sunkist project site San Bernardino County, California” and Appendix B5, under separate cover, “Report of the Year 2003 Focused Survey For Delhi Sands Flower-Loving Fly at Sunkist project site

San Bernardino County, California”).

The 2002 survey states:

“No DSF or DSF sign (i.e., discarded pupal cases) were observed on the Survey Site during the survey.”

The 2003 survey states:

“No DSF or DSF sign (i.e., discarded pupal cases) were observed on the Survey Site during the current survey, nor was the DSF detected on the site during the prior year’s survey (LMI 2002).”

“Based upon the following factors it may be concluded that the Survey Site is not occupied by the DSF nor is any suitable habitat for the species present:

- ◇ highly disturbed condition of entire site;
  - ◇ disturbed condition of Delhi Sands soils;
  - ◇ absence of California buckwheat, California croton, and telegraph weed;
  - ◇ low diversity of plants;
  - ◇ high proportion of non-native invasives in the site’s plant composition; and
  - ◇ type and condition of the habitat surrounding the site.”
- ◇ Petra Environmental Division was retained by Lewis Homes to perform a Preliminary Methane Investigation and Soil Testing on seven sites, one being the project site (i.e. Sunkist), (see Appendix C6, under separate cover, “Report of Preliminary Methane Investigation





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and Soil Testing”). The report states: “Methane was not identified in the probes located in the field portion of the Idsinga site nor was methane identified in any of the probes located at the Van Vliet, Durrington, Rohrs, Stueve, Vernola, and Sunkist sites.”



# Parkside

## Chapter 4: Land Use



San Francisco, California: Golden Gate Park





# Parkside

## 4.0 Land Use

### 4.1 Introduction

The ~~250.89 gross acre Specific Plan for Parkside~~ is comprised of approximately 250.9 gross acres. The Specific Plan includes the western portion of Subarea 23, between Cucamonga Creek and Archibald Ave., and the eastern portion of Subarea 22, between Cucamonga Creek and future Carpenter Ave. The entire Specific Plan area amounts to 237.9 acres in 25 planning areas (exclusive of Cucamonga Creek). Net project acreage amounts to 203.2 acres.

The Specific Plan is consistent with and implements the Medium Density Residential allocation (11.1 to 25 dwelling units permitted per gross acre) and broad “community design” policy requirements for the Ontario Ranch, including a general neo-traditional community design. These include:

- Integration with and connectivity to the Great Park;
- Creation of a “sense of place;”
- Diversity in housing product and architecture;
- Pedestrian connectivity;
- Smaller “neighborhoods;” and
- Elimination of the “subdivision” feel.

Parkside is envisioned to provide a neighborhood lifestyle in a wide range of planning areas, featuring a variety of residential housing types, all designed around a major community amenity, the Great Park. All residential neighborhoods are designed within easy walking distance to internal recreational amenities, commercial retail uses and the Great Park. Connectivity within the Specific Plan is provided through a system of ~~pedestrian and bicycle sidewalks~~, multi-purpose trails and on-street bike lanes linking residential neighborhoods to one

another, parks, off-site schools, and to the commercial retail area. A component of the trail system ~~will be provided through the improvement of a portion of~~ is the Cucamonga Creek Trail within the Specific Plan area. These improvements represent a part of the City’s Master Planned multi-purpose trail system planned for the ~~New Model Colony~~ Ontario Ranch.

Residential development is designed to address a variety of income levels and lifestyles, such as singles, families, executives, and “empty nesters.” Housing types will include such types as clustered “green court” or “motor court” single-family detached homes, and a wide variety of multi-family attached products including, but not limited to, triplexes, row townhomes, condominiums, and apartments.

A commercial land use is provided for in the Specific Plan, in Planning Area ~~21~~ 24. The Specific Plan allows for the development of approximately 115,000 square feet of neighborhood commercial, retail and service uses conveniently located adjacent to Archibald Ave. and ~~Edison Ave~~ Ontario Ranch Rd.

The Land Use diagram illustrated in Exhibit ~~10~~ 4-1: Land Use Plan illustrates the overall land use pattern within ~~portions of Subareas 22 and 23~~ the Specific Plan area. Table 4-1: Parkside Land Use Summary documents statistical totals for the Specific Plan.

### 4.2 Residential Use

Residential land uses within the Specific Plan comprise approximately ~~157.04~~ 156.62 gross acres. The Specific Plan will permit the development of up to 1,947 single-



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family detached and attached multi-family residential dwelling units. Residential land use areas are contained within distinctive neighborhoods, linked by a network of sidewalks, paseos, and on-street bicycle paths connecting all the neighborhoods to the central Great Park.

## 4.2.1 Variety of Housing Types

Parkside provides a unique mix of housing types to address the needs of a variety of lifestyle choices and economic segments. A variety of detached and attached residential products, with a variety of architectural styles and features will be offered. (See Chapter 7.2 for further details).

## 4.2.2 Traditional Neighborhood Design

The community plan for Parkside offers a strong neighborhood character and identity for residents and visitors through a traditional approach to street design, architecture, and landscape design elements to reflect a similar character to that of older traditional Southern California neighborhoods, including Ontario with a [modern interpretation of traditional styles](#). The design features described below “activate” the streetscene and promote friendly interaction between neighbors.



Exhibit 4-1: Land Use Plan



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Table 4-1: Parkside Land Use Summary

Land Use	Lot Size / Use	Gross Acreage	Net Acreage	*Density Range / S.F.	Lots / D.U.
<b>Residential</b>					
PA-1 (Green Court/Lane Loaded Products 1, 2 & 4)	SFD	19.97	15.57	10-18	236
PA-2 (Lane Loaded Product 3)	SFD	3.57	2.66	10-18	41
PA-3 (Lane Loaded Product 3)	SFD	3.06	2.38	10-18	42
PA-4 (Green Court/Lane Loaded Products 1, 2, 3 & 4)	SFD	15.50	13.62	10-18	221
PA-5 (HD Townhomes)	MF	9.57	8.14	15-22	139
PA-6 (Attached Auto Court)	MF	7.34	6.89	8-14	75
PA-7 (Auto Court 6-8 Plex)	SFD	9.30	7.49	8-14	68
PA-8 (Green Court 8-10 Plex)	SFD	6.64	6.18	10-16	68
PA-9 (Triplex)	MF	10.49	8.70	10-16	120
PA-10 (Row Townhomes)	MF	7.56	7.22	17-25	143
PA-11 (HD Townhomes)	MF	8.11	7.22	15-22	123
PA-12 (Triplex)	MF	11.80	9.30	12-18	127
PA-13 (Attached Auto Court)	MF	8.53	6.34	8-14	64
PA-14 (Duplex)	MF	7.12	6.18	12-18	99
PA-15 (Duplex)	MF	8.62	6.97	12-18	111
PA-16 (Triplex)	MF	7.83	7.35	10-16	90
PA-17 (Row Townhomes)	MF	4.87	4.54	17-25	82
PA-18 (HD Townhomes)	MF	6.74	5.04	15-22	98
<b>Residential Subtotal</b>		<b>156.62</b>	<b>131.79</b>	<b>14.77</b>	<b>1,947</b>
<b>Non-Residential</b>					
PA-19 (Commercial)		15.66	11.15	115,000 SF	-
PA-20 (Fire Station)		1.72	1.39	-	-
PA-21 (Great Park)		58.86	54.40	-	-
PA-22 (Private Rec)		1.00	1.00	-	-
PA-23 (Private Rec)		1.90	1.34	-	-
PA-24 (Private Rec)		1.00	1.00	-	-
PA-25 (Private Rec)		1.00	1.00	-	-
PA-26 (CD Well Site)		0.13	0.13	-	-
<b>Non-Residential Subtotal</b>		<b>81.27</b>	<b>71.41</b>	<b>115,000 SF</b>	<b>-</b>
Channel ROW		13.00	13.00	-	-
<b>Total Project</b>		<b>250.89</b>	<b>216.20</b>		<b>1,947</b>

NOTES:

- 1) Gross acres taken to center line of streets.
- 2) Adjusted gross acres taken to street ROW dedicated to the City
- 3) Net acres taken to street right of way
- 4) A total of 34.69 acres of master planned roadway is included in the project
- \* Density range is shown is based on net acres. The General Plan is based on adjusted gross acres
- 5) Proposed product types in each Planning Area are subject to change based on final development submittal.



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- A traditional grid and geometric street design in residential neighborhoods, with sidewalks separated by landscaped parkways, which provide visual interest, slower traffic, lower traffic volumes, and enhance a pedestrian orientation for neighborhoods. Sidewalks separated by a landscaped parkway promote pedestrian mobility, beautify the streetscene and lend a pedestrian scale;
- Streets adjacent to the Great Park may include on-street parking for park and recreation uses to the north and south, and a “linear” landscape treatment on both sides of the street. Parkway adjacent to both sides of the street are planned to be heavily landscaped;
- The architecture for residences within Parkside will be designed to focus on human scale details which will enhance the pedestrian friendly character of the community. These features may include the use of front porches, railings, enhanced entries, a mix of materials and textures, and authentic detailing on elements such as windows and doors, columns, balconies, and lighting; and
- Innovative garage designs will be utilized where appropriate in order to de-emphasize the visual impact of garage doors on the streetscene. ~~Design techniques may include varied garage setback requirements, split-garages, turn-in garages, garages located on rear lanes (private alleys), or other similar techniques that de-emphasize the view of garage doors from the street.~~

## 4.3 Parks, Open Space and Recreational Facilities

Parkside has been designed as a planned community “in the park.” The Great Park is the key organizational element for the entire community. ~~A conceptual plan for the “Great Park” is illustrated in Exhibit 33, Great Park Concept Plan.~~ Further, refined recreation planning is currently underway by the City on this segment of the Great Park and adjacent portions of the entire facility. Other open space and recreation opportunities for Parkside are included in the private recreation facilities which are integral to the neighborhoods in the project.

### 4.3.1 Parks

An integral part of the development of the residential program within the development boundary of the 250.9-acre Specific Plan is approximately 63.9 gross acres of park and recreational areas, including the Great Park, which provides primarily passive recreational opportunities for residents of the project and the surrounding community. The City’s ~~Quimby Act~~ park dedication requirements ~~will~~ may be met through a combination of improved park land and payment of fees. Refer to Chapter 7.7, Landscape, included in the overall Residential Design Guidelines for the project, for park plans for Parkside, including the Great Park Plan and blow-ups of the pocket parks and private community recreation acres.

~~The Quimby Act, provides for the dedication of 3 acres of parkland per 1,000 residents. However, pursuant to the General Plan Amendment Goal 12.0, Objective 12.1, Policy 12.1.3, each specific plan is required to~~





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~~define a park acquisition and improvement financing plan to ensure the achievement of a park standard of 5 acres of parkland per 1,000 residents. Pursuant to the foregoing policy, the total parkland requirement for the project is 34.21 gross acres. The Parkside Specific Plan; however, will provide 63.99 gross acres of parkland and recreational areas. Such areas shall either be dedicated to the City or transferred to a homeowners association:~~

The Policy Plan (Policy PR1-5) has established a standard of 5 acres of parkland (public and private) per 1,000 residents, with a minimum of 2 acres of developed private park space per 1,000 residents (Policy PR1-6). Private parks are required to be approximately a quarter mile walking/biking distance from each residence. This private park requirement may be met within any residential development, or by satisfying the in-lieu park development impact fee as approved by the City. Fees will be paid to fulfill the balance of the City's park requirement (the remaining 3 acres per 1,000 residents).

## 4.3.2 Multi-Purpose Trails

~~Bicycle~~ Multi-purpose trails are an integral element in creating accessibility and mobility within the Specific Plan area. An off-street ~~Class I bicycle~~ multi-purpose trail system will be provided within the right-of-way on the west side of Archibald Ave. adjacent to the ~~development~~ Specific Plan area between ~~Edison and Merrill Ave~~ Ontario Ranch Rd. and Eucalyptus Ave. Adequate right-of-way will be provided for on-street bicycle routes lanes with the improvement of Merrill Eucalyptus Ave. This bicycle trail system will link residential neighborhoods to the planned parks and the commercial site located within the development area, as well as providing connectivity to future development areas within the project Specific Plan area

## 4.4 Cucamonga Creek Channel Trail

A link within of the City's Master Plan of Trails includes a portion of the multi-purpose trail which will be developed in the Specific Plan area, extending a multi-purpose trail from Edison Ave Ontario Ranch Rd. southerly to future Merrill Eucalyptus Ave. in the Cucamonga Creek Channel. Access to this trail will be provided at key points within the Great Park to provide for pedestrian and bicycle accessibility. ~~The bridge over the Cucamonga Creek Channel shall be strong enough to support a police vehicle for needed access.~~ A conceptual plan for the Cucamonga Creek Trail is illustrated in Exhibit 12, Pedestrian and Bicycle Circulation Plan, and Exhibits 37-37a, Channel Regional Trail Easement Plan, in Chapter 7, Design Guidelines.

## 4.5 Commercial

The Specific Plan includes approximately 15.7 gross acres designated for development of neighborhood commercial uses.

### 4.5.1 Neighborhood Center

The Specific Plan provides for the development of approximately 115,000 square feet of Neighborhood Center Commercial land uses adjacent to Archibald Ave. and Edison Ave Ontario Ranch Rd. Commercial development at this location is conveniently located to serve the residential community of Parkside as well as the surrounding community. Pedestrian and bicycle connectivity between residential land use areas within Parkside and the future commercial center will be provided through an extensive network of trails. Commercial uses which could be developed within this land use district include, but are not limited to,



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local servicing commercial uses, such as grocery, drug store, small restaurants, small retail shops, and personal services.

## 4.6 Public Use (Fire Station and Well Sites)

A site consisting of 1.57 1.72 gross acres proposed directly west of the Neighborhood Center and fronting on the Great Park devoted, is allocated to future use as a City Fire Station as illustrated. Additionally, a CDA well site consisting of approximately 0.13 acres is proposed in the Great Park. Refer to Exhibit 4-1: Land Use Plan. The final site design and definition of uses will be provided by the City.

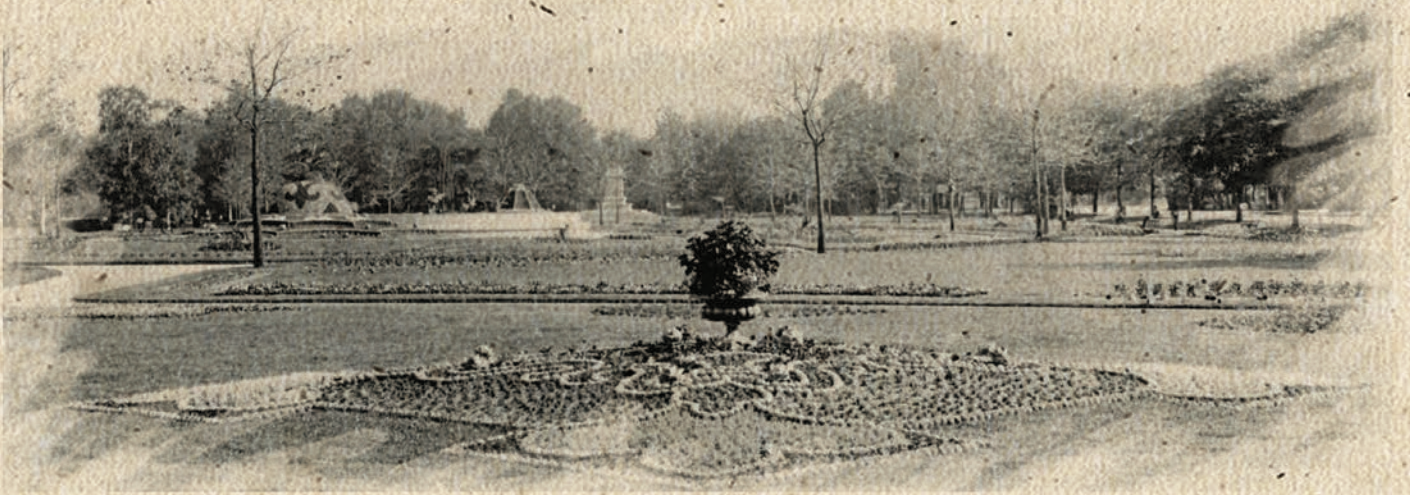
## 4.7 Dairy Farming

If the Specific Plan Area The Ontario Ranch area contains existing dairies and feed lots. A minimum 100-foot separation shall be required between a new residential, commercial, or industrial development, or structures used for public assembly and an existing animal feed trough, corral/pen, or an existing dairy/feed lot, including manure stockpiles and related wastewater detention basins. The 100-foot separation requirement may be satisfied by an off-site easement acceptable to the Planning Director with adjacent properties, submitted with the initial final map and recorded prior to or concurrent with the final map.

## 4.8 Airport Land Use Compatibility Planning (ALUCP)

The Specific Plan area is located within the Airport Influence Area of Chino Airport and the Ontario International Airport (ONT). The area is considered to be an “Existing Land Use” as defined by the Riverside County ALUCP for Chino Airport and ONT ALUCP. There is an existing Development Agreement in place for the property and is therefore not subject to the policies and criteria of the ALUCP for either airport. However it is recommended that the standard real estate transaction disclosures be applied any development.

## Chapter 5: Infrastructure & Services



Chicago, Illinois: Lincoln Park





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## 5.0 Infrastructure and Services

The infrastructure, utilities, and public services to be provided to support development of the Parkside Specific Plan are discussed in this section.

### 5.1 Circulation

The circulation plan for the Parkside Specific Plan area will provide efficient movement of vehicular traffic throughout the project site community as well as a pleasant environment for pedestrian movement and bicycle traffic thereby circulation, reducing the reliance on the automobile as a means for travel. The plan reinforces the concept of traditional neighborhood design with a grid street pattern. Exhibit 5-1: Vehicular Circulation Master Plan illustrates the roadway system planned for the Specific Plan area.

The project site will be served by a system of new and varied parkway, arterial and collector roadway classifications to be improved per the requirements of the City's adopted roadway classifications as illustrated in the New Model Colony General Plan Amendment (Figure 4-15 "Roadway Classifications—Parkway 1 and 1A Options," Figure 4-17, "Roadway Classifications") City Council Approved 2010 The Ontario Plan (TOP), General Plan, the 2012 Master Plan of Streets and Highways, and through proposed primary local streets to be constructed as part of the project site development. Improvements will be constructed for Archibald Ave., Edison Ave, Merrill Ontario Ranch Rd., Eucalyptus Ave., and Carpenter Ave., all of which border the project site. Proposed Hellman Ave. will be fully developed to serve the project site. Within the Parkside Specific Plan area, local collector streets and local interior streets will provide for the efficient movement of vehicles and pedestrians roadways

through the project. The developer will be designed to meet current and approved design standards. The traffic study will verify the need for additional rights-of-way at critical intersections required to accommodate additional left and right turn lanes. On street submit a parking and landscaping adjacent analysis to streets may be restricted within 100-150 feet of primary, secondary, and local street intersections the the City to ensure the proposal is in order to accommodate adequate sight distances conformance with the City's Parking Code for residential developments.

#### 5.1.1 Streets

The developer(s) of the Parkside Specific Plan area will be responsible for the off-site improvements along the entire project frontage or as otherwise indicated. Phasing of the improvements will be implemented as required by the City Engineer and pursuant to the mitigation measures identified in the EIR and/or the Conditions of Approval established on the approved tentative maps for the project. Additionally, bus turnouts within the project site Specific Plan area may be required to be constructed to the reasonable satisfaction of the City and Omnitrans.

The following is a description of the streets to be improved by the Parkside Specific Plan and associated improvements:

The minimum design speeds to be used for centerline curve radii, superelevation, corner sight distance, vertical and horizontal alignment, and sight distance, etc. are listed below:



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Ontario Ranch Rd. 50 m.p.h.

Archibald Ave. 50 m.p.h.

Eucalyptus Ave. 45 m.p.h.

Hellman Ave. 40 m.p.h.

The traffic study prepared for the Parkside Specific Plan will determine the need for additional rights-of-way at critical intersections in order to accommodate additional left and right turn lanes.

On-street parking is prohibited on all master planned roadways.

## Ontario Ranch Rd.

Archibald Ave. is designated as a Divided Arterial Parkway 1A street, with bikeway.

~~Archibald Ave. will carry regionally oriented traffic along the eastern boundary of the Specific Plan site and provide north/south access to and from the project site. Access to this street via driveways from residential units and on-street parking will be prohibited. Improvements to Archibald Ave. are illustrated on Exhibit 11a, Arterial Street Sections.~~

~~Edison Ave. is designated as a Divided Arterial Parkway 1A street. Edison Ave. Ontario Ranch Rd. is designated as an eight-lane Principal Arterial. Ontario Ranch Rd. provides east/west access to and from the project site along the northern boundary of the Specific Plan area. Access to this street via driveways from residential units and on-street parking will be prohibited. Exhibit 5-2: Principal Arterial Street Sections—Ontario Ranch Rd. illustrates the ultimate improvements to Edison Ave Ontario Ranch Rd.~~

## Archibald Ave.

Per the City's 2012 Master Plan of Streets and Highways, Archibald Ave. is designated as a Principal Arterial. The street section for Archibald Ave. is per the approved Archibald Ave. Street Improvement Plans. Per this plan set, Archibald Ave. has a 165-foot right-of-way between Eucalyptus Ave. to Ontario Ranch Road. Archibald Ave. will provide north/south access along the eastern portion of the Specific Plan. Per The Ontario Plan (TOP) Figure M-3: Multipurpose Trails and Bikeway Corridor Plan, Archibald Ave. is to include an off-street multi-purpose trail. Access to this street via driveways from residential units and on-street parking will be prohibited. Improvements to Archibald Ave. are illustrated on Exhibit 5-2: Collector Street Sections—Archibald Ave.

## Eucalyptus Ave.

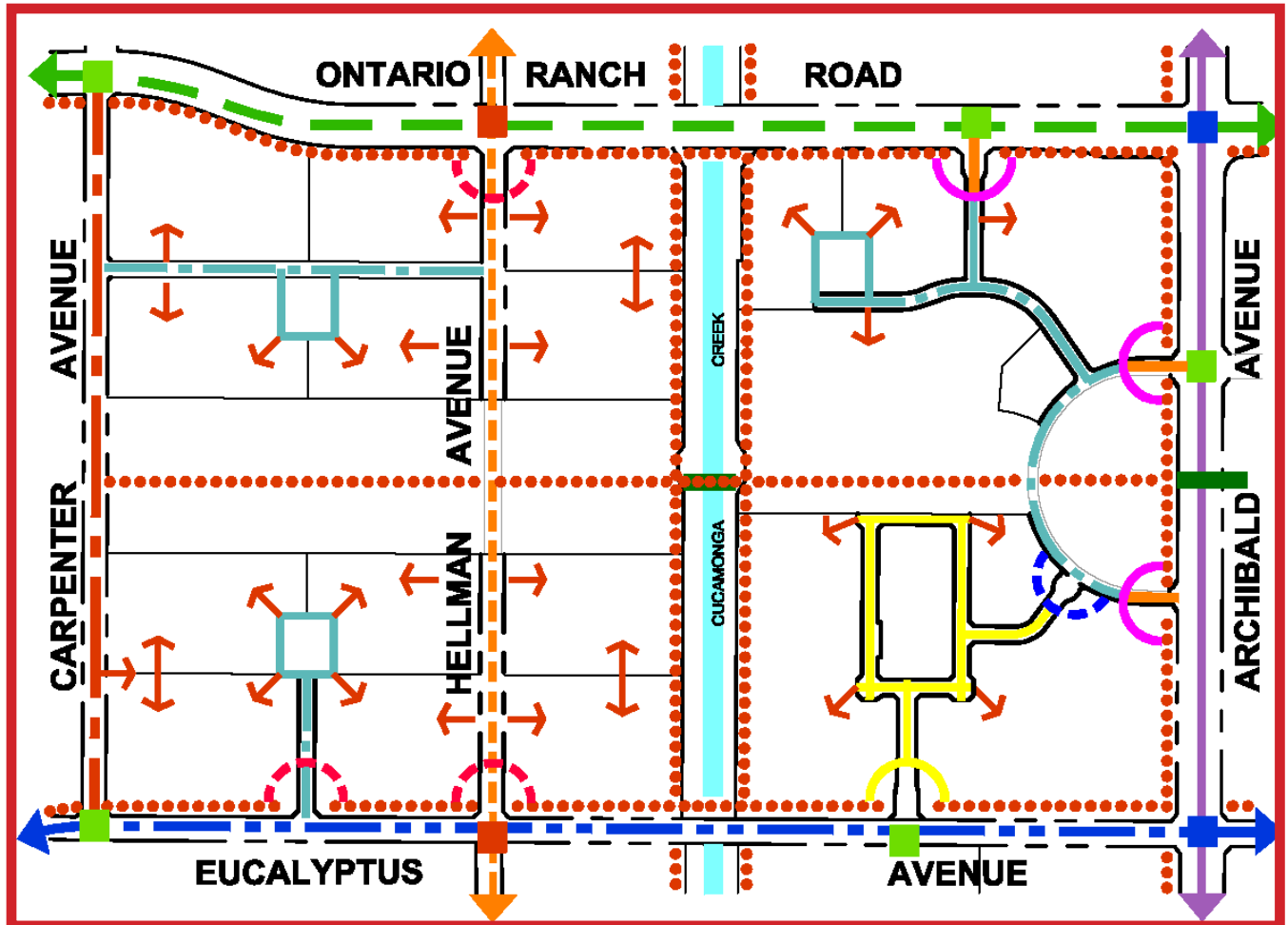
Eucalyptus Ave. is designated as a Collector. Eucalyptus Ave. will provide east/west traffic to and from the project site along the southern boundary of the Specific Plan area. Exhibit ~~11a, Arterial~~ 5-3: Collector Street Sections— ~~Merrill~~— Eucalyptus Ave. illustrates the ultimate improvements to ~~Merrill~~ Eucalyptus Ave. Access to ~~Merrill~~ Eucalyptus Ave. via driveways from residential units and on-street parking will be prohibited.

## Hellman Ave.

~~The~~ As a Collector, ~~Street~~ Hellman Ave., is designed to carry less traffic than the ~~standard arterial and the Parkway 1A streets~~ Principal Arterials. Hellman Ave., located within the western half of the project site, will be improved as depicted on Exhibit 5-2: Collector Street Sections—Hellman Ave. Access to this street via driveways from residential units and on-street parking will be prohibited.



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## LEGEND

- |   |  |   |  |   |                                       |
|---|--|---|--|---|---------------------------------------|
|  | SPECIFIC PLAN BOUNDARY   |  | SECONDARY COMMUNITY ENTRY STREETS (60'/72'/76' R/W)  |  | PRIMARY COMMUNITY GATEWAY ENTRY       |
|  | EXISTING TRAFFIC SIGNAL WITH MODIFICATIONS                               |  | PRIMARY LOCAL STREETS (36'/48'/60'/64' R/W WITH 36' PAVING) R/W DEPENDS ON ADJACENCY WITH PARK |  | SECONDARY COMMUNITY ENTRY (GATED)     |
|  | MASTER PLAN TRAFFIC SIGNAL   |  | PRIMARY PRIVATE STREETS (40'/58'/60'/68' R/W)  |  | MAJOR COMMUNITY GATEWAY ENTRY (GATED) |
|  | TRAFFIC SIGNAL TO BE INSTALLED WHEN WARRANTED                            |  | CLASS 1 BIKE PATH (OFF-STREET)   |  | SECONDARY COMMUNITY ENTRY             |
|  | INTERNAL CIRCULATION POINTS  |  | PEDESTRIAN BRIDGE  |   |                                       |
|  | ARCHIBALD: DIVIDED ARTERIAL PARKWAY 1A WITH CLASS 1 BIKE PATH (165' R/W) |   |  |   |                                       |
|  | ONTARIO RANCH: DIVIDED ARTERIAL PARKWAY 1A (180' R/W)                    |   |  |   |                                       |
|  | EUCALYPTUS: STANDARD ARTERIAL (108' R/W)                                 |   |  |   |                                       |
|  | HELLMAN: COLLECTOR STREET (88' R/W)                                      |   |  |   |                                       |

Exhibit 5-1: Vehicular Circulation Master Plan



# Parkside

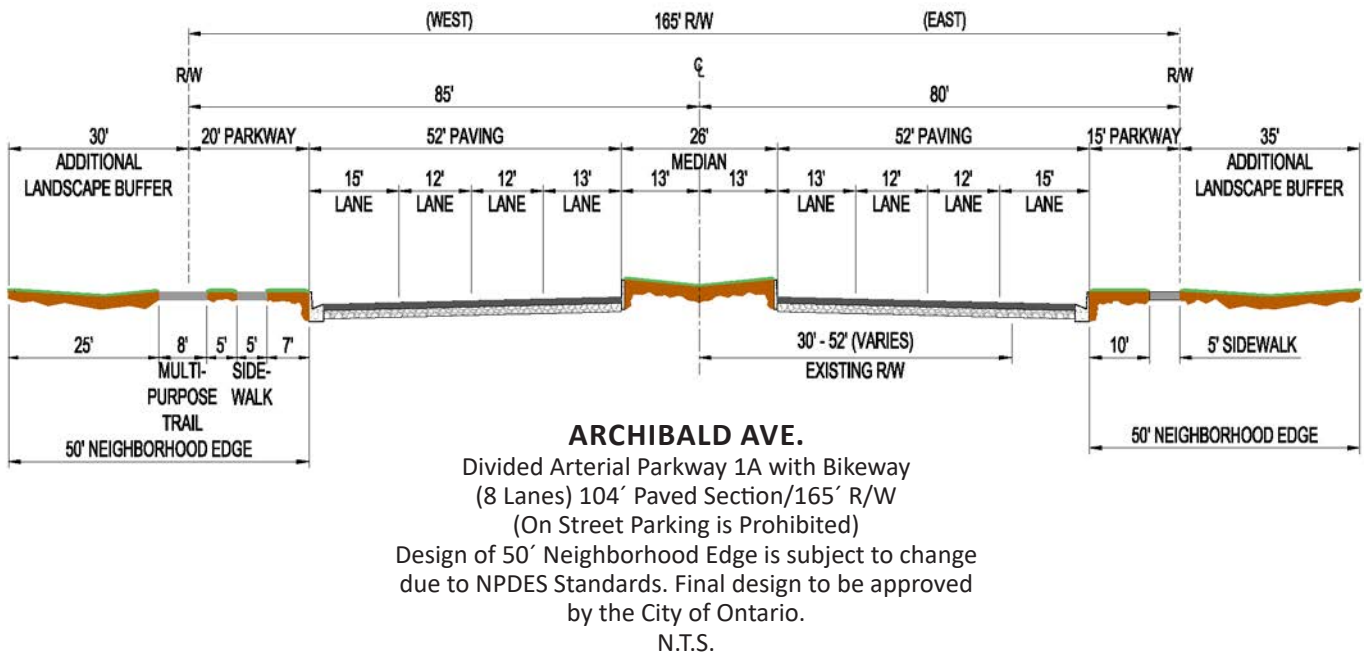
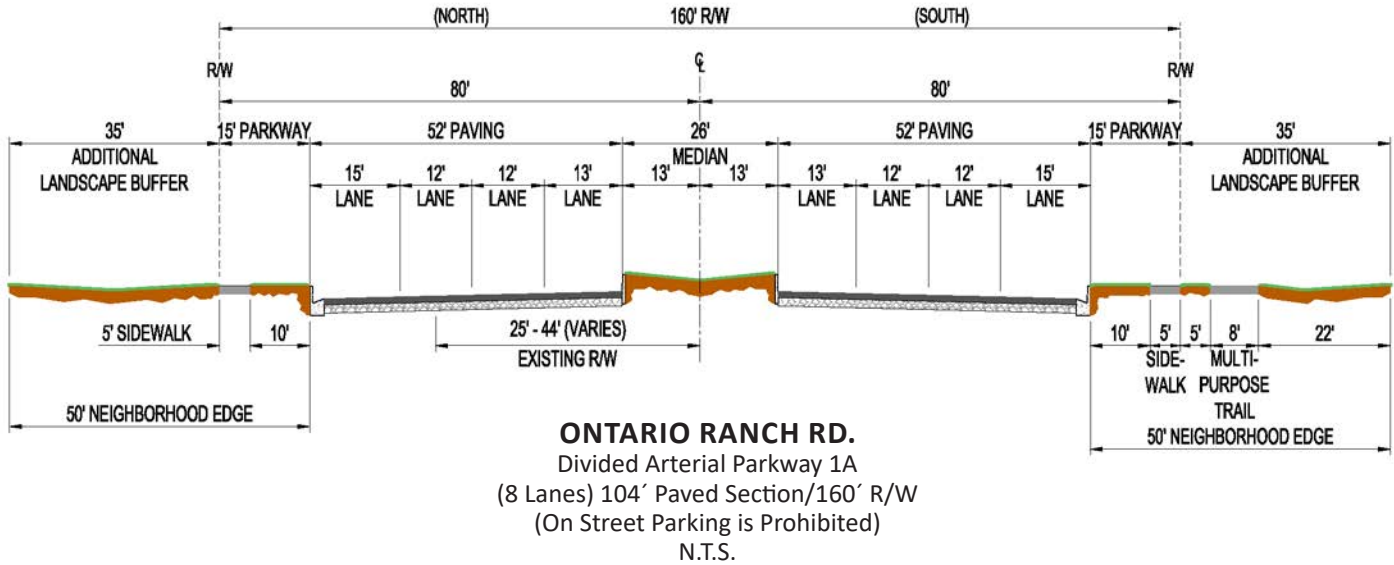
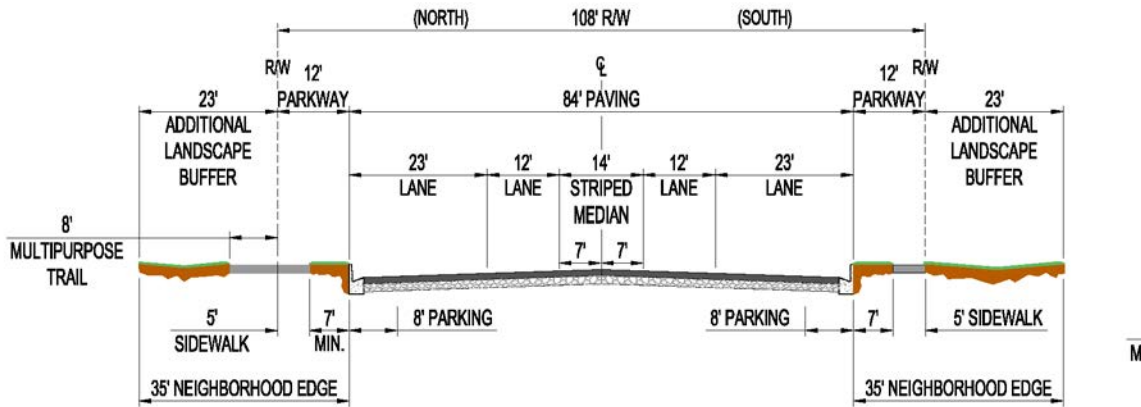


Exhibit 5-2: Principal Arterial Street Sections





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## EUCALYPTUS AVE.

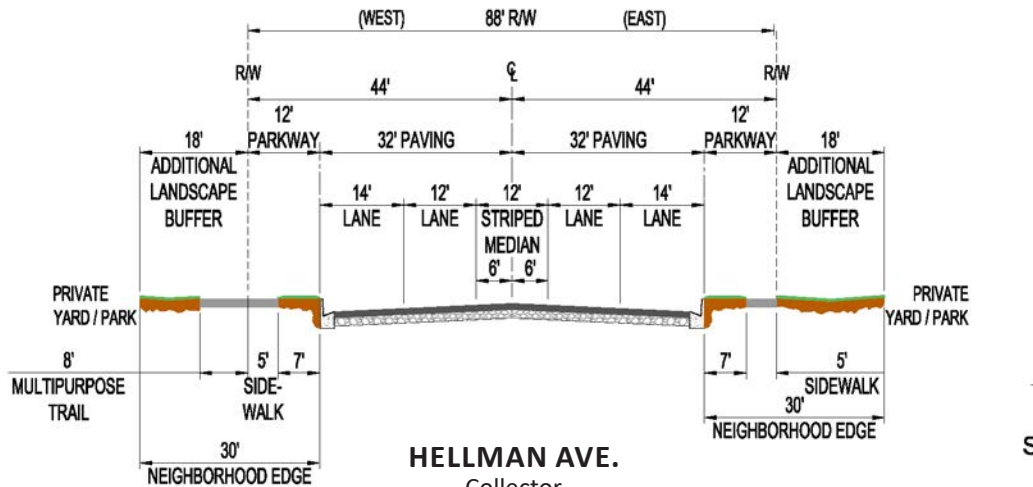
Collector

(4 Lanes) 84' Paved Section/108' R/W

(On Street Parking is Allowed)

(Sidewalk and Bike Path Configuration is Subject to City Approval)

N.T.S.



## HELLMAN AVE.

Collector

(2 Lanes) 64' Paved Section/88' R/W

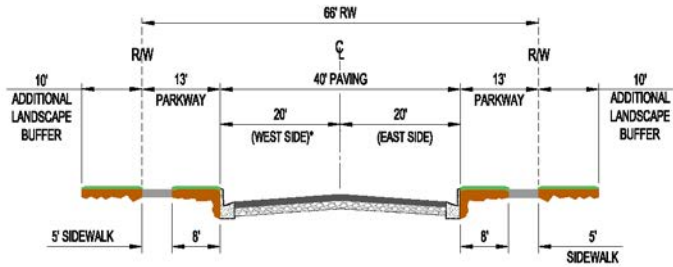
(On Street Parking is Prohibited)

N.T.S.

Exhibit 5-3: Collector Street Sections



# Parkside



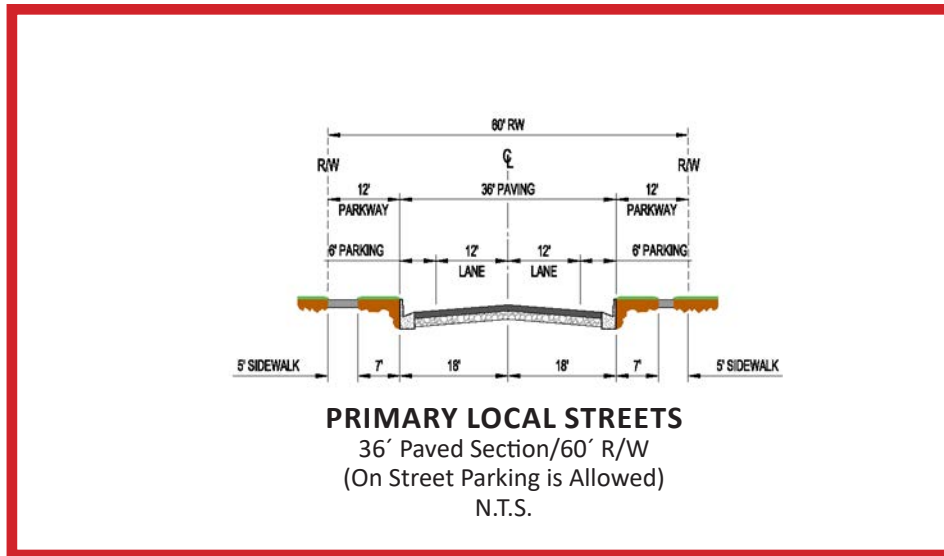
## CARPENTER AVE.

40' Paved Section/66' R/W

(On Street Parking is Allowed)

\*Finished Parkway Improvements West of Curb and Gutter by Others

N.T.S.



## PRIMARY LOCAL STREETS

36' Paved Section/60' R/W

(On Street Parking is Allowed)

N.T.S.

Exhibit 5-4: Local Street Sections



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## Carpenter Ave.

Carpenter Ave., a Primary Local Street, bounds the project site to the west and will provide north/south access to and from the Parkside Specific Plan area. Exhibit 11c, Collector, Primary Residential and Park Entry 5-4: Local Street Sections—Carpenter Ave. illustrates the ultimate improvements to Carpenter Ave. Easterly improvements will be constructed on Carpenter Ave. by this project.

## Primary Residential and Park Entry Local Streets

Each Primary Residential and Park Entry Street will include 52 feet of paving with a 12-foot wide raised median. A 12-foot landscaped parkway (both sides) will include a 5-foot sidewalk. Exhibit 11c, Collector, Primary Residential and Park Entry Street Sections illustrates the proposed Primary Residential and Park Entry Streets.

## Local Streets

The Primary Local Streets are designated as collector streets for local traffic. The 60-foot right-of-way including is composed of a total paved width of 36 feet and a 12-foot landscaped parkway, which also includes a 5-foot sidewalk (buffered from the street by 7 feet of landscaping) is proposed, as illustrated on). Exhibit 11d 5-4: Local Street Sections illustrates these improvements.

## Secondary Community Entry Streets

Secondary Community Entry Streets have 36, 48 or 52 feet of paving. An optional landscaped median is acceptable subject to City approval. 12-foot landscaped parkways are provided on both sides and may include a 5-foot sidewalk on one or both sides. Exhibit 5-5: Entry Street Sections illustrates the proposed Park Entry Streets.

The Local Streets within the project site will be constructed with a 60-foot right-of-way. The Local Street Section is designed to achieve a small neighborhood scale street, internal to the project, which will carry neighborhood related traffic only. The Local Street section will have a paving width of 36 feet, and 12-foot landscaped parkways, containing 5-foot sidewalks (buffered from the street by seven (7) feet of landscaping) and one (1) travel lane in each direction, as illustrated on Exhibit 11d, Local Street Sections.

## One Way Streets at the Recreation Area

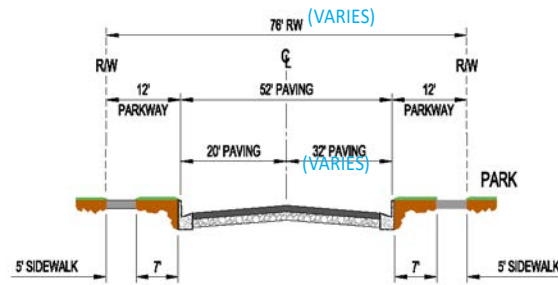
Two (2) of the proposed private recreation areas leading into the “Great Park” will include a one-way street encircling those areas. This street will provide access to the private recreation areas, to residential neighborhoods, and provides a point of pedestrian connection to the “Great Park.” The approaches and departures to and from the loops shall also be private, and designed with flares and splitter islands as per the City’s Roundabout Guidelines to properly direct motorists from the two-way streets into the corresponding one-way portion and vice versa. A plan view and typical section is illustrated on Exhibit 11e, 1-Way Street to Recreation Areas for PA17, 18, 19 and for PA 1, 8, 9, 10.

## Primary Private Streets

Primary Private streets within the Specific Plan will include right of way widths of 40 feet, 58 feet, 60 feet and 68 feet. The 40-foot right of way has a 24-foot paved section, a 5-foot parkway on one side and a 6-foot parkway with 5-foot sidewalk on the other side. Both the 58-foot and 60-foot right of way have 36 feet of pavement. The 58-foot right of way has 6-foot parkways



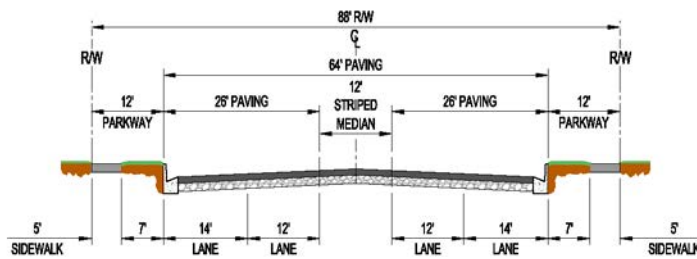
# Parkside



## SECONDARY COMMUNITY ENTRY STREETS

52' Paved Section/76' R/W  
 (On Street Parking is Allowed)  
 N.T.S.

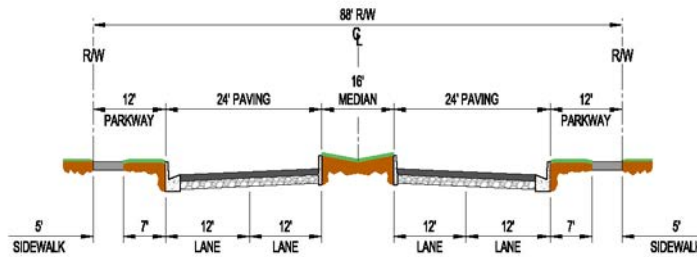
Note: An optional landscaped median is acceptable subject to City approval.



## PRIMARY RESIDENTIAL ENTRY STREETS

52' Paved Section/88' R/W  
 (On Street Parking is not Allowed)  
 N.T.S.

Note: An optional landscaped median is acceptable subject to City approval.



## PRIMARY RESIDENTIAL ENTRY STREETS "GATED"

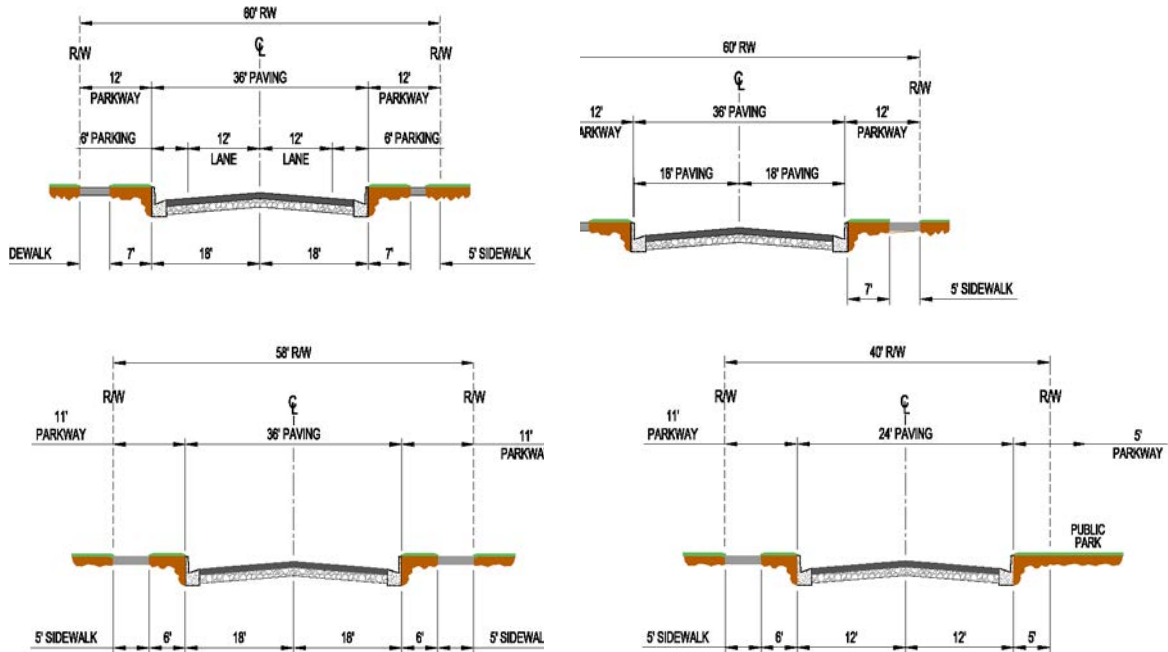
52' Paved Section/88' R/W  
 (On Street Parking is not Allowed)  
 N.T.S.

Note: An optional landscaped median is acceptable subject to City approval.

Exhibit 5-5: Entry Street Sections



# Parkside

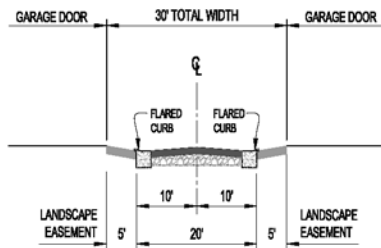


## PRIMARY PRIVATE STREETS

24', 34' & 36' Paved Section/40', 58', 60' & 68' R/W  
 (On Street Parking is Allowed)  
 N.T.S.

Note: All private street design is subject to City Approval.

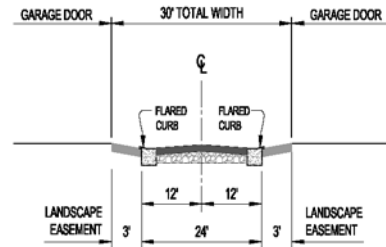
Exhibit 5-6: Primary Private Street Sections



## LANES (PRIVATE ALLEYS)

20' Paved Section/30' Width to Garage  
 (On Street Parking is Prohibited)  
 N.T.S.

Note: Where emergency access alleys are to be used within the residential development, a 24-foot paved section with 3-foot minimum landscaping will be utilized.



## LANES (PRIVATE ALLEYS)

24' Paved Section/30' Width to Garage  
 (On Street Parking is Prohibited)  
 N.T.S.

Exhibit 5-7: Private Lane/Alley Sections



# Parkside

and 5-foot sidewalks on each side. The 60-foot right of way has 7-foot parkways and 5-foot sidewalks. Lastly, the 68-foot right of way has a 34-foot paved section, a 10-foot median and 7-foot parkways with 5-foot sidewalks on both sides. On-street parking is allowed on the 58-foot and 60-foot right of ways. Exhibit 5-6 illustrates the Primary Private Street Sections.

In conditions where Private Streets have a gated entry, all entry drives will be designed to City Engineering and Ontario Fire Department standards. Final design of the gated entries will be reviewed during the Development Plan process.

## Private Lanes/Alleys

Lanes within the Specific Plan will primarily have 20 feet of paved area with 5 feet of landscaping on each side. Planning Areas 1-4 will have lanes with 24 feet of paved area and 3 feet of landscaping on each side. Exhibit 5-7: Private Lane/Alley Section illustrates the proposed lane sections. Lanes with “dead end” conditions will have a maximum length of 150 feet (subject to Fire Department approval). Parking is prohibited along lanes. Tapers will be incorporated at the point where private lanes intersect public streets. Hammer heads may be incorporated into the design of the lanes if the City determines they are required.

## 5.1.2 Non-NMC Ontario Ranch Planned Street Condition Along Park/Paseo Uses

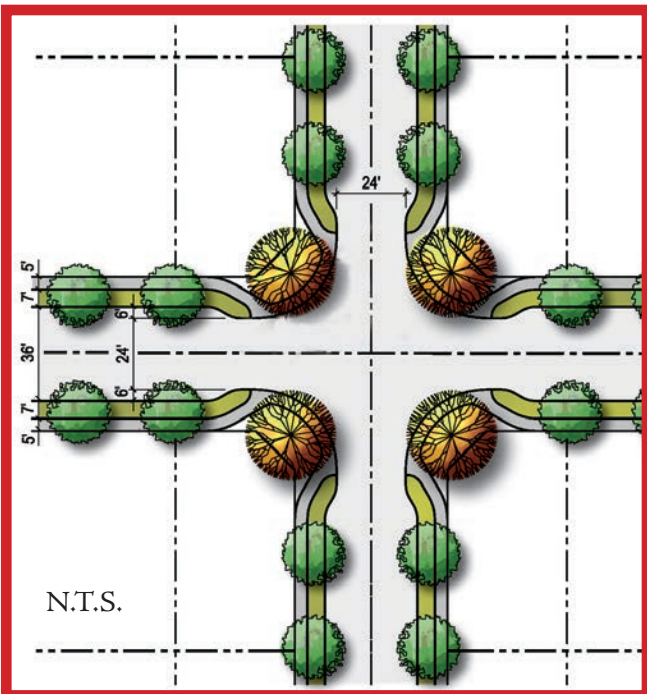
Where any non-New Model Colony Ontario Ranch General Plan Master Planned Street is adjacent to a park, paseo area, or paseo link, the street parkway will be included as park/paseo area/acreage.

## 5.1.3 Traffic Calming

Parkside provides for traffic calming within residential neighborhoods to slow traffic and reduce traffic noise on streets, contributing to livable neighborhoods in which to walk, bike, and drive.

Traffic calming within the Parkside Specific Plan area is designed to address the following:

- Reduction in traffic speeds;
- Reduction in traffic related noise;
- Reduction in cut-through traffic within residential neighborhoods;
- A secure pedestrian friendly circulation system,

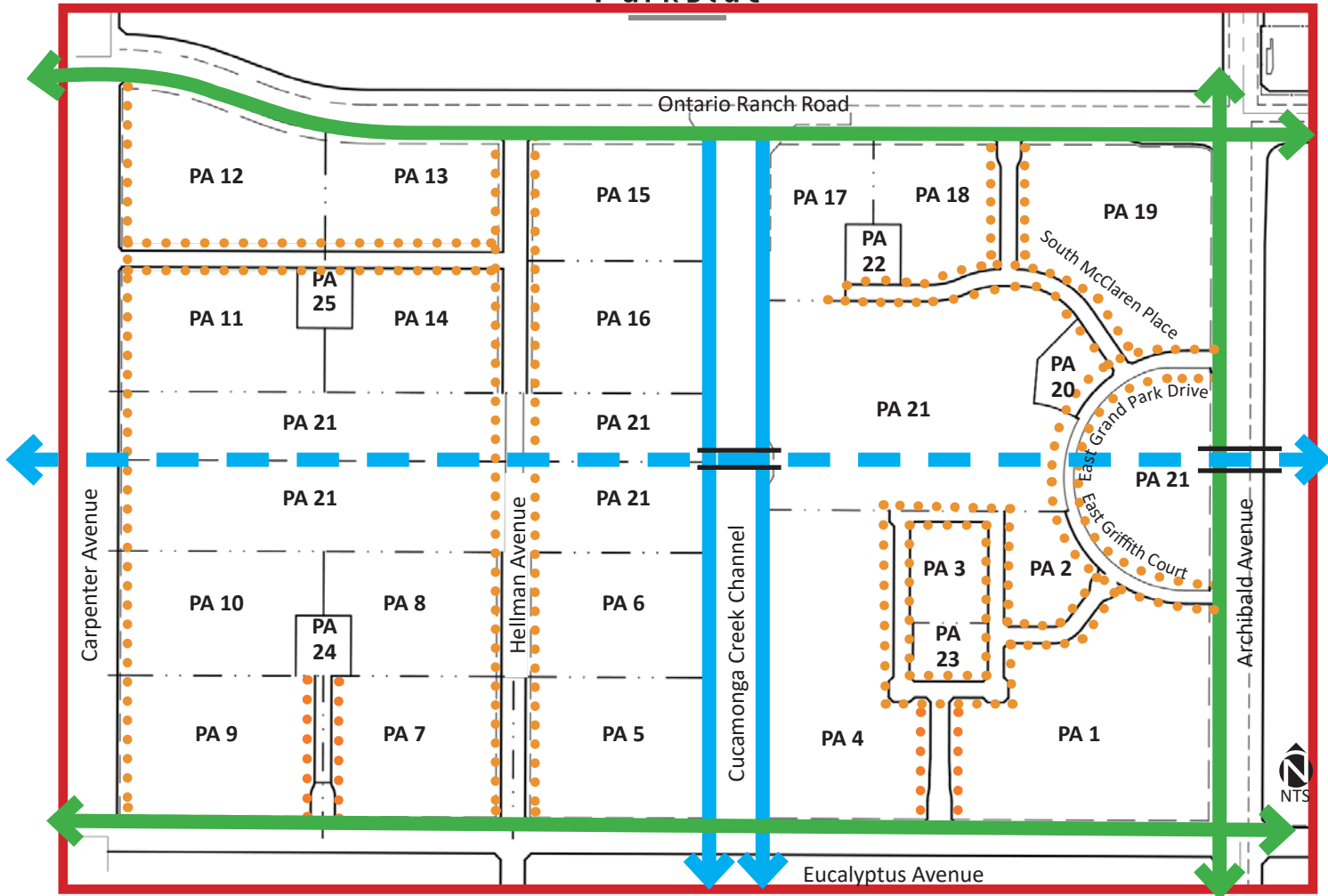






Note: Refer to the City Standard Drawing for Local Street Chokers.

Exhibit 5-8: Neighborhood Street Tapers



# Parkside



Legend	
	Multi-purpose Trail (8' wide)
	Cucamonga Creek Multi-purpose Trail (8' wide)
	Grand Park Multi-purpose Trail
	Pedestrian Sidewalks

**Notes:**

Future Pedestrian Bridges across Archibald Avenue and Cucamonga Creek Channel are proposed

Exhibit 5-9: Pedestrian and Bicycle Circulation Master Plan



# Parkside

which encourages walking;

- Allow for non-restricted access for emergency service vehicles such as police, fire, and ambulances; and
- Reduction in radii of streets and elimination of knuckles.

Factors affecting traffic speeds are those that influence the driver's perception of the roadway, such as:

- Type of adjacent development and the distance of development from the roadway;
- Frequency of access points onto the roadway;
- Roadway alignment and curvature;
- Type and massing of landscaping adjacent to the roadway;
- Frequency of traffic control devices along the roadway;
- **Width** of travel lanes; and
- **Roundabout locations.**

The following traffic calming techniques **may** be implemented in the design of the roadways within the Parkside Specific Plan area.

## Local Neighborhood Street Design

**Local Neighborhood** streets within Parkside are designed in a grid with ~~landscaped parkways~~ landscaping on either side **within parkways** to add interest in the street, encouraging drivers to slow their travel speed and observe their surroundings. **Incorporating Traffic speeds on local residential streets may be reduced by incorporating** tapered intersections for local streets, as illustrated in Exhibit 5-8: Neighborhood Street Tapers, ~~will reduce traffic speeds on local residential streets.~~ **Taper may not be appropriate at all intersections.**

## Lane ( Private Alley) Design

Lanes (private alleys) within Parkside ~~will consist of 20 feet of paved travel area with five (5) feet of landscaping on each side, as illustrated in Exhibit 11h, Lane (Private Alley) Tapers.~~ **Incorporating may incorporate** tapers at the entrance to these lanes ~~can reduce speeds to slow traffic in these areas and to provide a visual element to discourage drivers from using lanes as a through street.~~ **Speeds** for traffic entering the lanes ~~as well as~~ and cut-through traffic ~~can be reduced by incorporating tapers at the entrance to these lanes as illustrated on Exhibit 5-8: Neighborhood Street Tapers.~~

## Landscaping

Landscaping adjacent to streets within the **Specific Plan area** will combine the use of shade trees, shrubs, and groundcover adjacent to sidewalks to create a more intimate streetscape, encouraging drivers to reduce driving speeds. The landscape concept for Parkside is designed to contribute a sense that the street system is a pedestrian "protected area" to promote slower traffic speeds. ~~Refer to the Landscape Design Guidelines, Chapter 7 Exhibit 5-8: Neighborhood Street Tapers illustrates these design features.~~

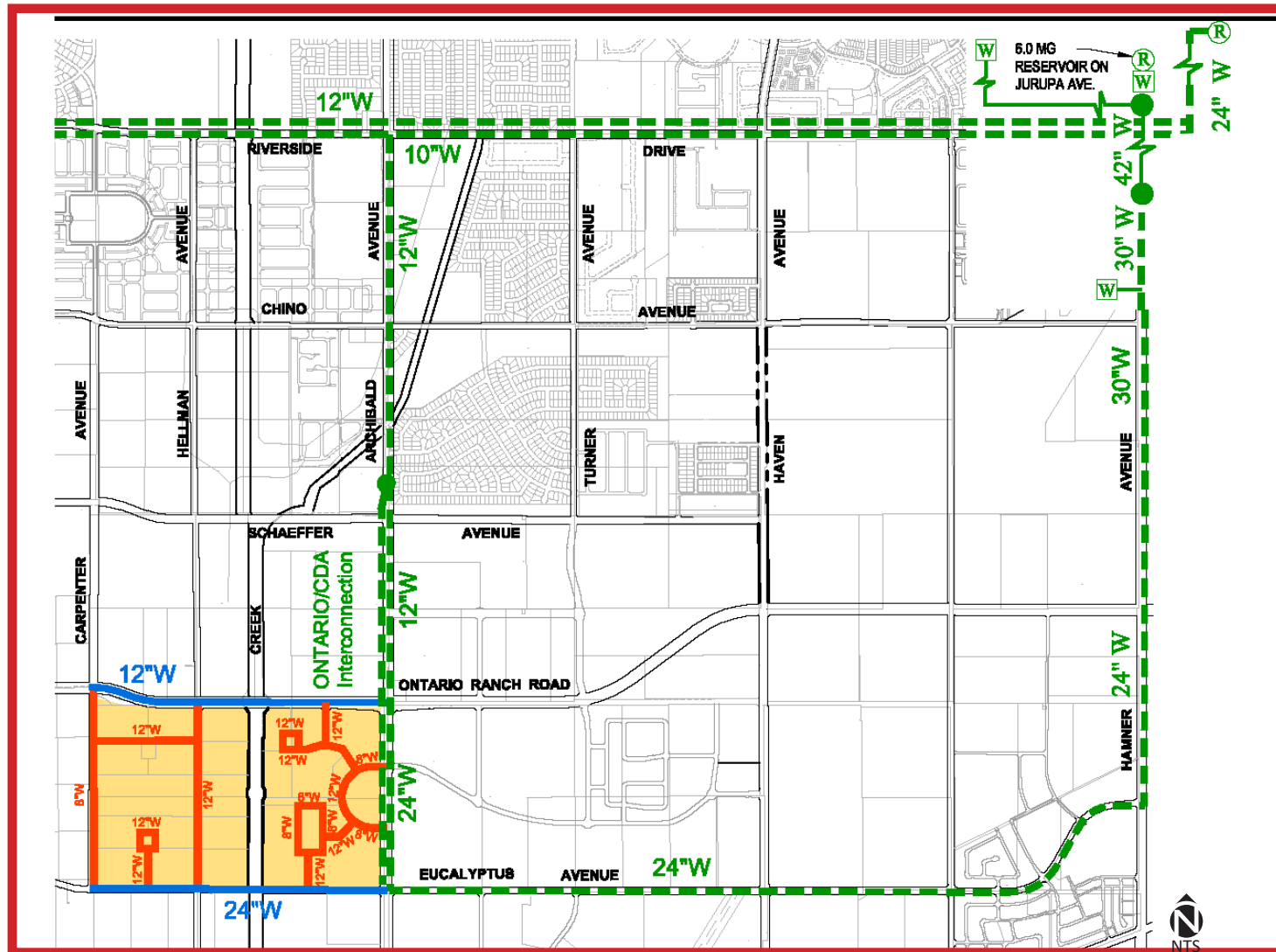
## 5.1.4 Pedestrian Circulation

Off-street pedestrian circulation will be available throughout the Parkside Specific Plan area ~~by means of with~~ an interconnected paved sidewalk system within the roadway right-of-way, separated from vehicular travel lanes by a landscaped parkway. Pedestrian access points to the Cucamonga Creek Channel will be provided at 2 points in the Great Park, east and west of the channel, and potentially in other locations within the project.





# Parkside



Legend	
	Specific Plan Boundary
	NMC Master Plan Water
	NMC Water Supply Phase 1
	Proposed On-Site Water
	Pressure Reducing Station
	Reservoir
	Well

Exhibit 5-10: Conceptual Domestic Water Master Plan



# Parkside

## 5.1.5 Bicycle Plan

~~Bicycle Multi-purpose~~ trails are an integral element in creating accessibility and mobility within the Specific Plan. ~~An eight-foot wide~~ multi-purpose pedestrian and bicycle trail will be provided within ~~the~~ Cucamonga Channel ~~(Area 3 Trail and Area 4 Trail)~~. ~~Class 1 Paths will be provided and~~ within the west parkway of Archibald Ave., within the ~~south~~ parkway of Ontario Ranch Rd. within the north parkway of Eucalyptus Avenue, ~~and within the Great Park. (east to west) and along the west side of the Cucamonga Creek Channel.. These bike paths will provide a link to~~ These ~~multipurpose trails are identified~~ in the City's ~~master-planned bike paths system. Exhibit 11, Master Plan of Circulation, illustrates the locations of proposed bike paths~~ Policy Plan Multipurpose Trails and Bikeway Corridor Plan. Refer to Exhibit ~~12 5-8: Pedestrian and Bicycle Circulation~~ Master Plan, for locations of these paths and trails.

## 5.2 Water Master Plans

~~Water and recycled water may be designated as “public utilities” if located within the public or private streets. All public utilities within private streets shall be designed per City standards and contained within acceptable easements. The CC&R’s shall contain language that requires all proposed work within said easements to be plan checked and inspected by the City, including applicable fees. Generally, utilities will not be accepted as public within private alleys, parking areas, or driveways. Utilities within commercial and industrial parking lots and loading areas will be designated as private. The extent to which said utilities will be accepted as public utilities shall be determined, at the full discretion of the City, during final design plan review.~~

The ultimate sizing and alignment of all waterline improvements shall follow the most currently approved Master Plan. A Conceptual Technical Study and Final Technical Study shall be prepared and approved by the City prior to approval of Construction Drawings and Permits.

### 5.2.1 Domestic and Recycled Water

#### Master Plan

Domestic water for the project site will be provided by the City. The ~~New Model Colony~~ Ontario Ranch Water Master Plan Phase 1 (Phases 1a–1d) identifies new water facilities to include one reservoir, 3 groundwater wells and potable and recycled transmission water lines. Construction of the on-site and off-site Master Plan water service improvements shall be the responsibility of the developer(s) and is required prior to issuance of building permits (excluding models and private recreation facilities) for Parkside. One of the waterlines included in the Phase I improvements is a 24-inch water main in Archibald Ave., which borders Parkside on the east. The City is in the process of updating the Water Master Plan. Any changes resulting from the update will be incorporated into the Parkside Domestic Water and Recycled Water Master Plan. ~~A hydraulic modeling analysis report is required to demonstrate that the proposed water system will meet peak demands including maximum day plus fire demand and peak hour demand.~~

Master planned domestic and recycled water main lines serving and surrounding the Specific Plan, as identified in the adopted Water Master Plan, shall be constructed prior to issuance of first occupancy ~~On-site public system sizing is subject to the recommendations and approval of the Hydraulic Analysis. of each phase.~~ The sizing and alignment of all in-tract and looping



# Parkside

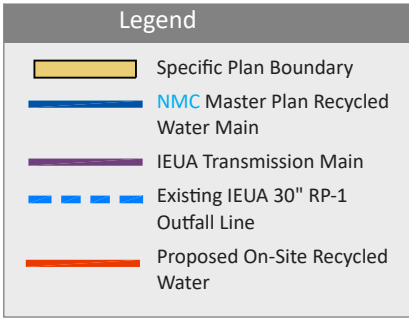
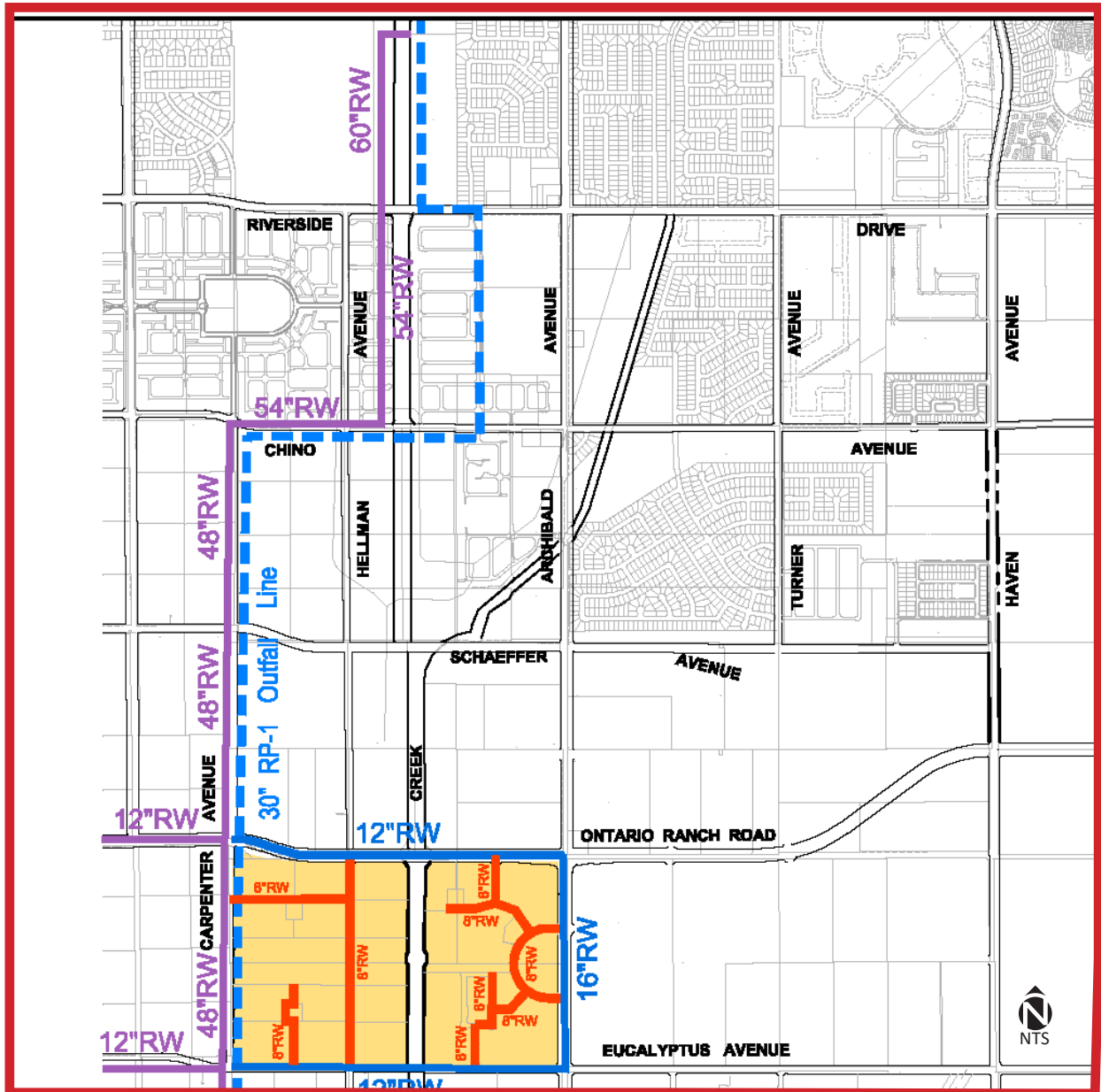


Exhibit 5-11: Conceptual Recycled Water Plan



# Parkside

waterline improvements shall follow the results of the approved water hydraulic analysis. The public system location shall be approved during final design review.

## 5.2.2 Domestic Water

The Parkside Specific Plan area lies within the 925 Pressure Zone. New domestic water mains to be constructed as part of the development of Parkside will include a 24-inch Master Plan water main in Edison Eucalyptus Ave., from Archibald Ave. to Carpenter Ave. Also included is a 12-inch Master Plan water main in Ontario Ranch Rd. from Archibald Ave to Carpenter Ave. ~~Merrill Ave., and in Merrill Ave., from Carpenter Ave. to Archibald Ave. An estimated 8-inch line in Carpenter Ave., from Edison Ave. to Merrill Ave., will be constructed as part~~ The sizing and alignment of all in-tract and looping waterline improvements shall follow the project to complete the loop system. Within the project site, a network results of eight (8) inch water lines will be installed the approved hydraulic analysis.

The Master Plan for domestic water for Parkside is illustrated in Exhibit 13 5-10: Conceptual Domestic Water Master Plan.

## 5.2.3 Master Planned Recycled

### Water System

~~New recycled water lines in conformance with the City's Recycled Water Master Plan (not yet adopted) will be constructed as part of the development of Parkside~~ The ultimate sizing and alignment of all recycled waterline improvements shall follow the most currently approved Master Plan. In order to assure conformance with Master Plan Design Criteria during the Implementation process: a Conceptual Technical Study and a Final Technical Study shall be prepared and approved by the City prior to approval of Construction Drawings and Permits. The source of recycled water is the Inland Empire Utility Agency (IEUA) 30-inch RP-1 Outfall Line in Carpenter Ave. There is an existing 16-inch recycled water line in Archibald Ave., from Ontario Ranch Rd. to Eucalyptus Ave. There is a proposed IEUA 48-inch recycled transmission main in Carpenter Ave. New recycled master planned water mains to be constructed as part of the Parkside development will include a 12-inch line in Edison Ontario Ranch Rd., from Carpenter Ave. to Archibald Ave., a 12-inch line in Merrill Eucalyptus Ave., from Carpenter Ave. to Archibald Ave., ~~a 16-inch line in Archibald Ave., from Edison Ave. to Merrill Ave.,~~ and an 8-inch line in Hellman Ave., ~~from Edison Ave. to Merrill Ave.~~ On-site from Ontario Ranch Rd. to Eucalyptus Ave. The sizing and alignment of all in-tract and looping recycled waterline improvements shall follow the results of the approved recycled water lines will be installed as required by the City Engineer and/or by the Recycled Water Master Plan, ~~yet to be adopted by the City of Ontario, to provide irrigation for public landscape areas, such as parkways, buffer areas and parks~~ hydraulic analysis. The Parkside Specific Plan shall comply with ~~City Ordinance 2689~~ Ontario Municipal Code Chapter 8C (Recycled



# Parkside

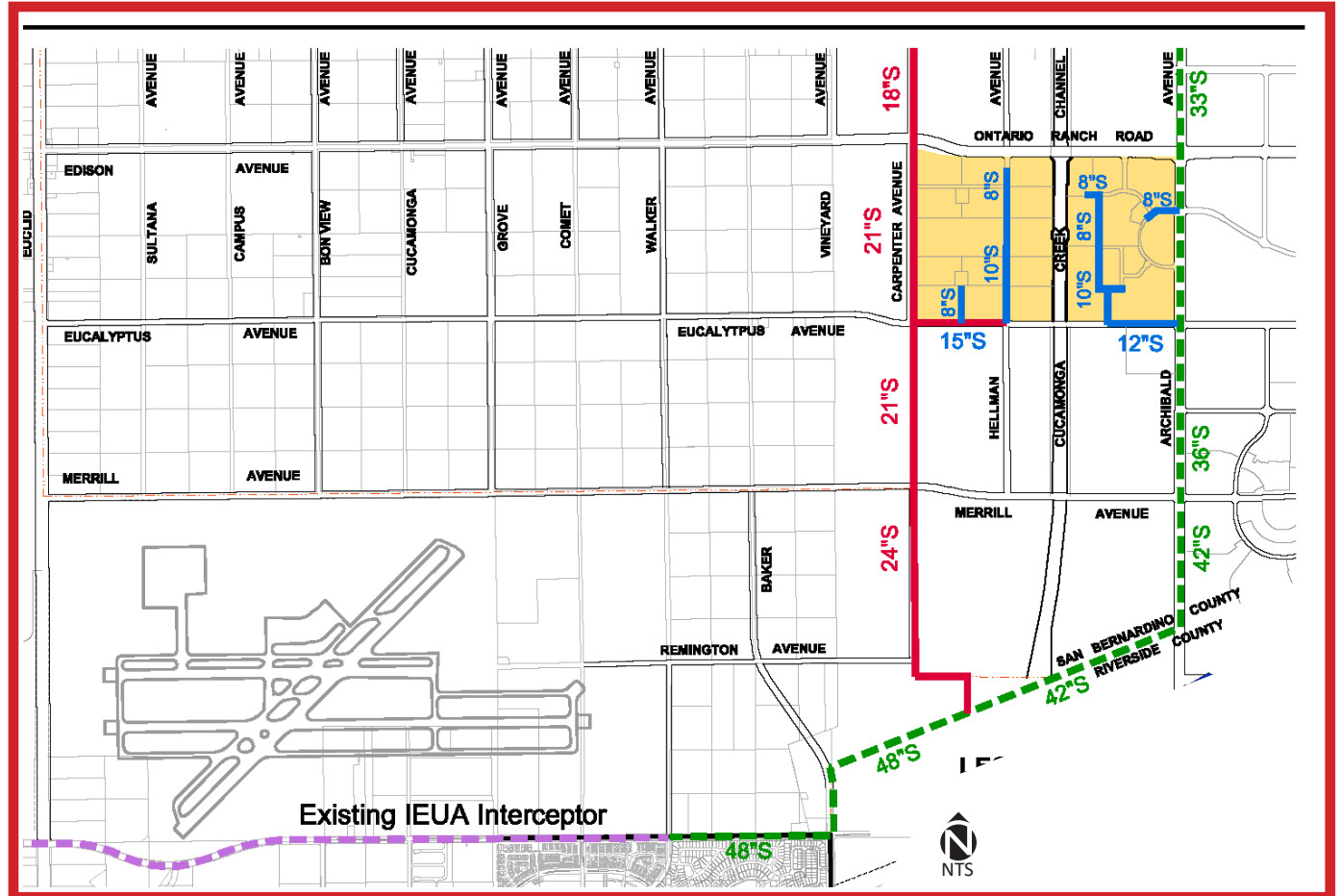







Exhibit 5-12: Conceptual Sewer Master Plan

Legend	
	Specific Plan Boundary
	NMC Master Planned Sewer Main
	Eastern Trunk Sewer
	Proposed On Site Sewer
	Existing IEUA Interceptor



# Parkside

Water Use) and provide recycled water for all approved uses, including but not limited to irrigation of parks, street landscaping, recreational trails, HOA-maintained on-site common areas and commercial landscaping. An Engineer's Report approved by the City and the Department of Health Services is required prior to the use of recycled water. ~~The on-site recycled water system shall be subject to the approval of the City of Ontario.~~

There may be an interim period where recycled water supply does not have adequate pressure depending on the timing of development and construction of a regional booster station. In this case, the recycled water system will connect to the potable system initially, until such point that recycled water service is available.

The Master Plan for recycled water for Parkside is illustrated on Exhibit 5-11: Conceptual Recycled Water Plan. ~~On-site recycled water usage is illustrated on Exhibit 14a, Conceptual Recycled Water Usage.~~

## 5.3 Sewer Master Plan

~~Sewer utilities may be designated as "public utilities" if located within public or private streets. Reference Section 5.2, Water Master Plans.~~

The ultimate sizing and alignment of all sewer improvements shall follow the most currently approved Master Plan. In order to assure conformance with Master Plan Design Criteria during the Implementation process: a Conceptual Technical Study and a Final Technical Study shall be prepared and approved by the City prior to approval of Construction Drawings and Permits. Sewer service for the ~~project site~~ Specific Plan area will be provided by the City. The City's Sewer Master Plan identifies ultimate sewer facilities from the project site

to the Eastern Trunk Sewer. Master Planned facilities within the project area include the Eastern trunk Sewer Main in Archibald Ave. ~~will that serves~~ that portion of the project site lying easterly of the Cucamonga Creek Channel. The portion of Specific Plan area lying westerly of the Cucamonga Creek Channel will be served by the Eastern Trunk Sewer via the Master Planned sewer main connecting to the Eastern Trunk Sewer near the intersection of Carpenter Avenue and the San Bernardino County Line. ~~These Master Panned facilities include a 36-inch main in Vineyard Ave., from realigned Merrill Ave. (old Eucalyptus Ave.) to existing Merrill Ave.; a 12-inch main in Merrill Ave. to Carpenter Ave.; and an 18-inch main in Carpenter Ave, from Merrill Ave. to the Eastern Trunk Sewer. Completion of these Master Plan improvements is required to provide the ultimate sewer service to the Parkside Specific Plan area. Within the project site a network of 8-inch, 10-inch, and 12-inch sewer lines will be installed. Master planned sewer main lines serving and surrounding~~ This line connects to the existing IEUA Intercept line at Walker and Kimball Ave in the city of Chino.

The sizing and alignment of all in-tract sewer improvements shall follow the results of the approved Sewer Sub-Area Master Plan (SSAMP) analysis. The master planned sewer main line serving the western portion of the Specific Plan, as identified in adopted updated Sewer Master Plan, shall be constructed prior to issuance of first occupancy of the phases west of the Cucamonga Creek Channel. On-site public system sizing is subject to the recommendations and approval of the ~~Hydraulic Analysis. Location of public system shall be approved during final design review. The City is currently~~



# Parkside

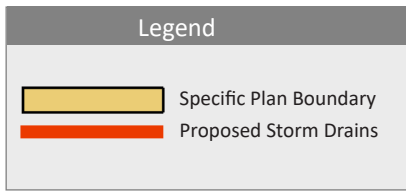
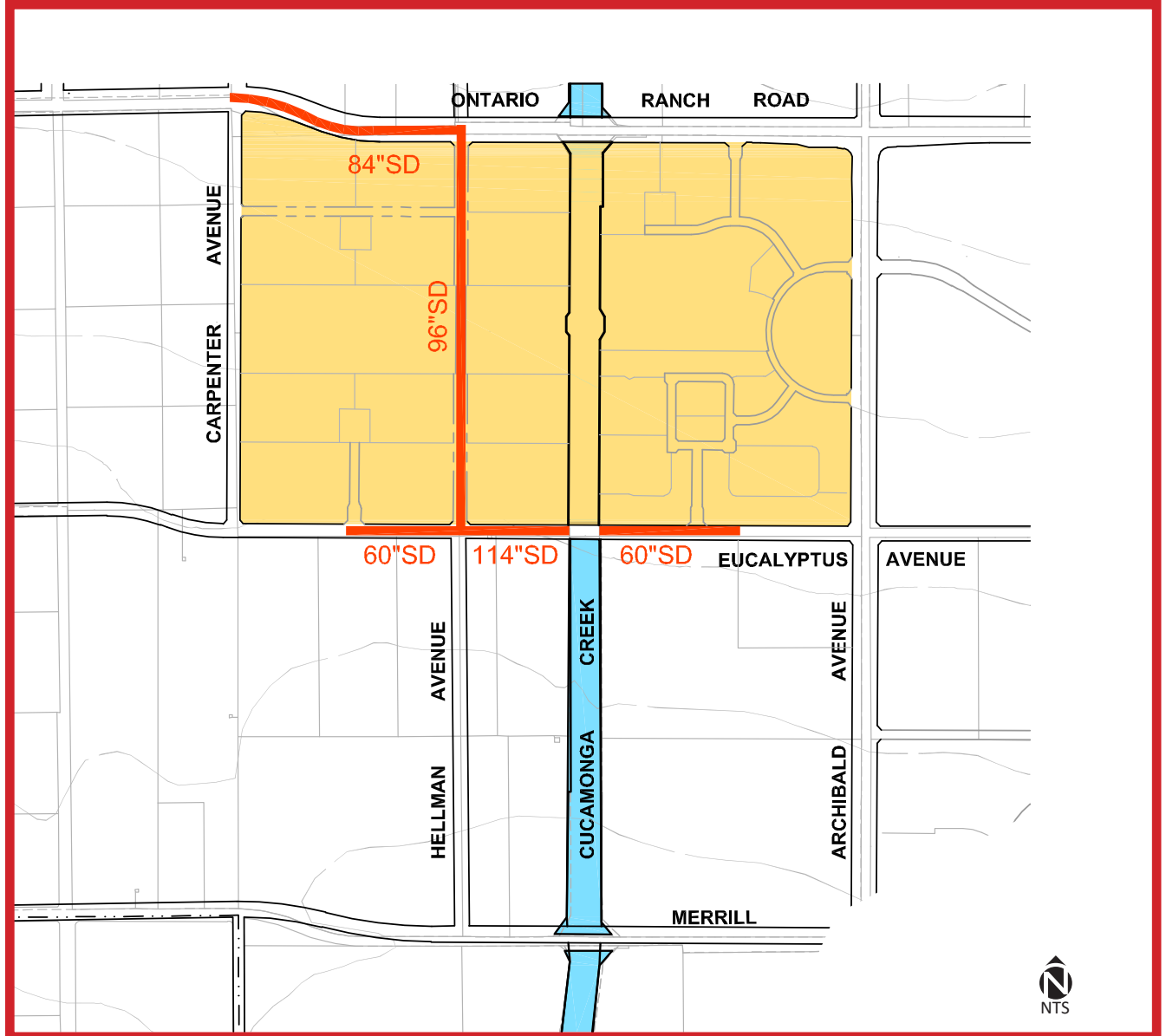


Exhibit 5-13: Conceptual Storm Drain Master Plan



# Parkside

~~updating the Sewer Master Plan. Any changes resulting from the review will be incorporated into the Parkside Sewer Master Plan~~ required sewer analysis.

The Sewer Master Plan for Parkside is illustrated on Exhibit ~~15~~ 5-12: Conceptual Sewer Master Plan.

## 5.4 Drainage

All pipe, improvements, sizing, and alignment shall follow the most current approved version of the Storm Drain Master Plan (currently 2012), in case there are discrepancies between pipe improvements, sizing, and/or alignments as show on the utility exhibits versus the Master Plan. The City's Storm Drain Master Plan identifies new storm drain facilities to serve the project site extending ~~northerly from~~ towards the Cucamonga Creek Channel. These Master Plan improvements include a ~~84-inch storm drain in Ontario Ranch Road from Carpenter to Hellman Ave.~~, a 96-inch storm drain in Hellman Ave., a 60-inch storm drain in Eucalyptus Ave., east of the Cucamonga Creek Channel and a ~~42-inch storm drain in Merrill Ave.~~ west of Hellman Ave., a 114-inch storm drain in Eucalyptus Ave., between Hellman Ave. and the Cucamonga Creek Channel. Completion of these Master Plan improvements will provide permanent storm drain service to Parkside.

On-site storm drains will be constructed to convey the on-site flows to the proposed Master Planned storm drain lines in ~~Merrill~~ Eucalyptus Ave. Size and location of proposed storm drain may change based on the final design.

The Drainage Master Plan for Parkside is illustrated in Exhibit 5-12: Conceptual Storm Drain Master Plan.

### 5.4.1 NPDES Compliance

The grading and drainage of the Parkside Specific Plan area shall be designed to detain, filter, and treat surface runoff, in a manner and combination which is practical, to comply with the most recent requirements of the San Bernardino County NPDES Stormwater Program's Water Quality Management (WQMP) for significant new development projects. The objective of the WQMP for this project is to minimize the detrimental effects of urbanization on the beneficial uses of receiving waters, including effects caused by increased pollutants and changes in hydrology. These effects may be minimized through the implementation of site designs that reduce runoff and pollutant transport by minimizing impervious surfaces and maximizing on-site infiltration, Source Control Best Management Practices (BMPs) and/or either on-site structural Treatment Control BMP's, or participation in regional or watershed-based Treatment Control BMPs.

Prior to the issuance of a grading or construction permit, a Storm Water Pollution Prevention Plan (SWPPP) will also be prepared. The SWPPP will be prepared to comply with the California State Water Resources Control Board's (State Water Board) current, "General Permit to Discharge Storm Water Associated With Construction Activity" and the current Areawide Urban Storm Water Runoff (Regional NPDES Permit). The SWPPP will identify and detail all appropriate Best Management Practices (BMPs) to be implemented or installed during construction of the project.

In addition to the preparation of a SWPPP for construction-related activities, and as part of the approval of any





# Parkside

grading plans within the Specific Plan Area, the applicant will be required to submit a Water Quality Management Plan (WQMP) on the regional model form provided by the City. The WQMP shall identify and detail all Site Design, Source Control, and Treatment Control BMPs to be implemented or installed at this site in order to reduce storm water pollutants and site runoff. All Priority Land Use (PLU) areas within the Specific Plan Area shall comply with the statewide Trash Provisions adopted by the State Water Resources Control Board (SWRCB) and trash requirements in the most current San Bernardino County Area-Wide MS4 Permit. Drainage from such as a Debris Separating Baffle Box (DSBB), Continuous Deflective System (CDS), or an equivalent hydrodynamic separator which as been approved by the SWRCB.

~~A proposed~~The Specific Plan area is located within the sub-watershed area of a regional storm water runoff treatment facility ~~for the sub-watershed area that this project lies within is being considered for construction~~. This regional treatment facility ~~would be part of an overall solution for storm water treatment. If an approved regional storm water treatment facility is constructed, it may~~ serves as an alternative to complete on-site treatment of all pollutants of concern. ~~If the regional storm water treatment facility is not completed and operational prior to construction of this project, all necessary on-site treatment control BMPs and/or temporary water quality devices will be installed pursuant to the requirements of the current regional NPDES Permit and the approved Water Quality Management Plan for this project.~~

## 5.5 Public Services

### 5.5.1 Schools

The project site is located within the Mountain View School District, which serves the needs for kindergarten through eighth grade, and the Chaffey Joint Union High School District, serving the needs for grades 9 through 12. Several elementary, middle, and high schools are master planned throughout the ~~New Model Colony Ontario Ranch~~ and will ultimately serve the Parkside Specific Plan area educational demands. Development of the Parkside Specific Plan area will generate an estimated student population as follows (based on student generation numbers supplied by the City of ~~Ontario~~, "School Generation for New Model Colony Subareas," July 2, 2004):

Grades K-5 Generation Factor .38/D.U.0.38 x 1,947 = 740

Grades 6-8 Generation Factor .22/D.U.0.22 x 1,947 = 429

Grades 9-12 Generation Factor .20/D.U.0.20 x 1,947 = 390

### 5.5.2 Fire Department Services

The City of ~~Ontario~~ provides fire protection for the project site. The nearest fire station (2931 East Philadelphia Street) is approximately 2.5 miles northeasterly of the project site. Response time varies due to availability of units, according to Fire Department statements. A new fire station has been identified within the Parkside Specific Plan area, directly west of the commercial site, in Planning Area 20, which will service the Specific Plan area as well as the surrounding community. Refer to Exhibit ~~10~~ 4-1, Land Use Plan.



# Parkside

## 5.5.3 Police Department Services

The City of Ontario will provide police protection for the Specific Plan area. The police station (2500 Archibald Ave.) was relocated to be more centrally located to the New Model Colony Ontario Ranch and is approximately 2.2 miles north of the project site. Response time varies based on location of police units and the type of calls, according to Police Department statements.

## 5.5.4 Libraries

The City of Ontario provides library service to the City. There are two existing library locations within the City of Ontario. The main library is located within the vicinity of the Civic Center (215 East “D” Street). The Colony Branch Library is located northeasterly of the project site, approximately 2.75 miles away, and can be reached via Archibald Ave. and Riverside Drive, within Colony High School (3859 East Riverside Drive). Both libraries will serve the project site. There will be a need for additional facilities over time. Currently, the City of Ontario has not planned for a library within the Parkside Specific Plan area.

## 5.5.5 Public Transportation

Omnitrans currently provides fixed-route bus and “Access” services within the vicinity of the project site. Omnitrans’ Fixed Bus Route 70 currently operates along Riverside Drive, between Vineyard and Milliken Ave., and is approximately 1.5 miles north of the project site via Archibald Ave. The project site is also served by the “Access” demand-response service for qualified persons with disabilities. The City has indicated that there is currently a study underway to determine the additional public transportation needs of the Ontario Ranch.

## 5.6 Public Utilities

Public utilities including telecommunications telephone, gas, and electrical service and solid waste collection will be available to the project site Specific Plan area and will be extended into the planned community as part of the development of the Parkside Specific Plan. All existing overhead utilities shall be placed underground, where feasible underground, including all on-site utilities and those within the public right-of-way along streets fronting the project site. Within the Specific Plan area, all new utilities will be placed underground.

### 5.6.1 Telecommunications

~~The City of Ontario will be providing OntarioNet, fiber-to-the-home. OntarioNet is a fiber-optic telecommunications system capable of providing advanced Internet/data services to all homes and businesses within the New Model Colony. OntarioNet will provide community related services including: traffic management; on-line civic services; meter reading; educational services; and a variety of other community services. Based on the demographics of the New Model Colony and the traffic issues related to traffic flows going into Orange and Los Angeles Counties, the demand for telecommuting in the New Model Colony is anticipated to be significant. OntarioNet and the high-speed data services it provides will allow residents of the Parkside Specific Plan area to effectively telecommute to their jobs and, in general, provide a significant economic benefit to the City of Ontario. Verizon currently provides telephone service within the project site and can provide telecommunication service if they choose to do so.~~

Proposed Fiber Optics (conduits and fiber) will be placed underground within a duct and structure system to be



# Parkside

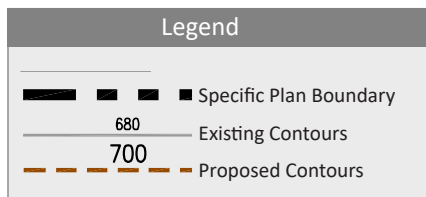
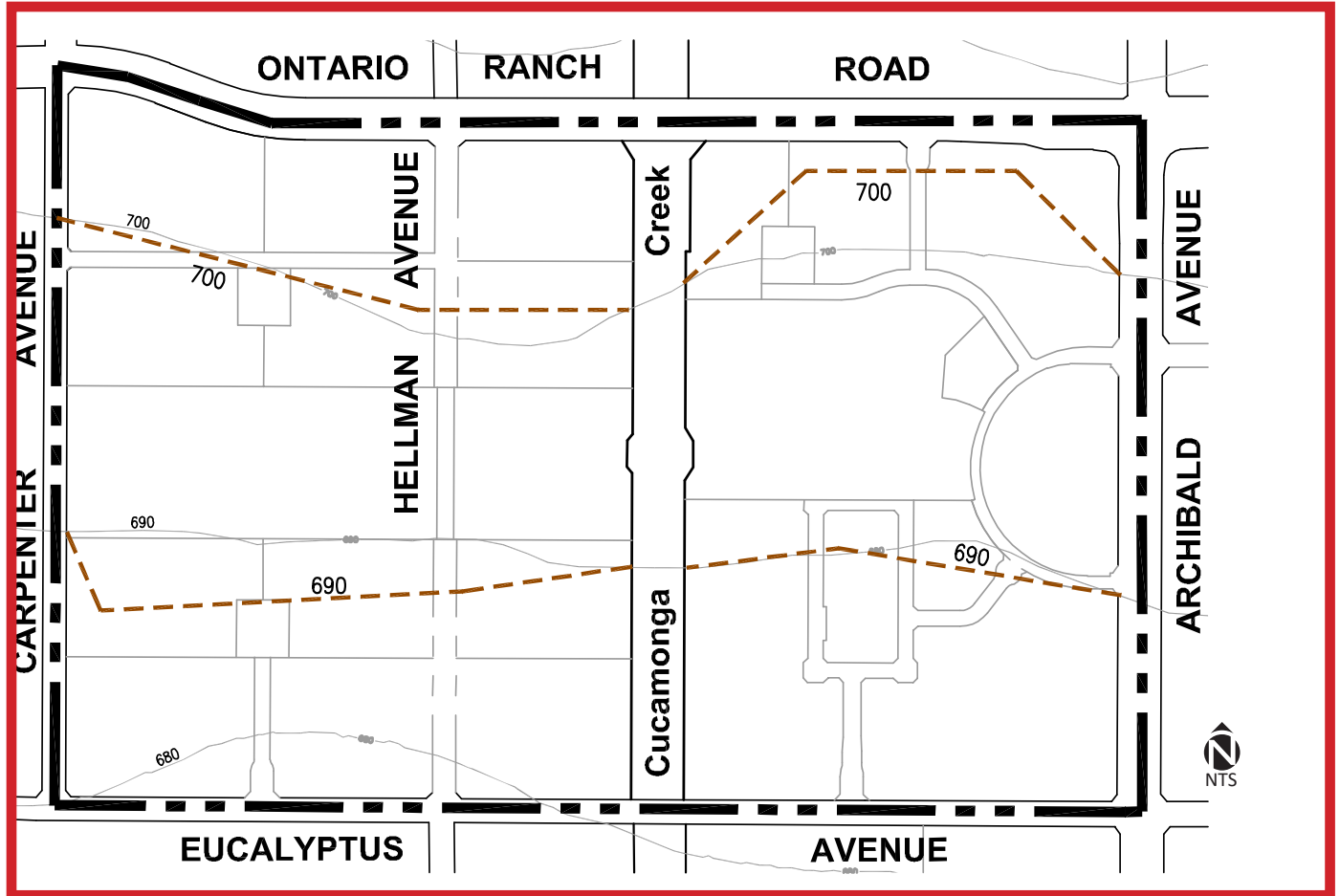


Exhibit 5-14: Conceptual Grading Plan



# Parkside

installed by the Developer. The fiber optic conduits along the backbone streets shall be installed in a joint trench by each Developer as the last lane improvements are completed. In-tract fiber optic conduits shall be installed by the Developer in joint trenches with City Street Light conduits where possible. Maintenance of the installed system will be the responsibility of the City/ Special District. Development of the Project requires the installation by the Developer of all fiber optic conduits and pullboxes as well as payment of Development Impact Fees to cover the installation of the fiber optic cable.

## 5.6.2 Natural Gas

The Southern California Gas Company (SCG) provides natural gas service within the project site. Facilities within the area include an existing 8-inch line, reducing to 6 inches, along Archibald Ave., a 6-inch line and a 36-inch transmission line (located on the south side of ~~Edison Ave~~ Ontario Ranch Rd.) along ~~Edison Ave~~ Ontario Ranch Rd.

The Gas Company will provide natural gas to the project site. SCG indicates that gas service could be provided to the Parkside Specific Plan area, based upon conditions of gas supply, regulatory agencies and in accordance with Southern California Gas Company’s policies and extension rules on file with the California Public Utilities Commission at the time contractual arrangements are made.

## 5.6.3 Electricity

Southern California Edison Company (SCE) currently provides electrical service in the area. All new lines installed within the Parkside Specific Plan area, less than 66 KV, shall be installed underground. Additionally, existing lines within the Specific Plan area, less than 34.5

KV, shall also be placed underground by the developer ~~and according to the City of Ontario Adopted Ordinance per OMC Chapter 5 (Underground Public Utility Districts).~~

## 5.7 Solid Integrated Waste

~~The City of Ontario provides solid waste collection services for the City and will service the Parkside Specific Plan area.~~

~~a. Commercial — Developer shall comply with Municipal Sec. 6-3.314 Commercial Sec. 6-3.314 Commercial Section 6.01.010F.4. (Refuse Storage) Standards, and Sec. 6-3.601 Business Recycling Plan.~~

~~b. Apartment — Areas). For apartments using all commercial bin service developer development, refuse and recyclable materials shall comply with Municipal be stored as provided in Ontario Development Code Sec. 6-3.314 Commercial Section 6.01.015E.17. (Refuse Storage Standards and Sec. 6-3.601 Business Recycling Plan. Areas (Trash Enclosures)).~~

~~c. Residential — For curbside automated container service developer shall comply with Municipal Code Sec. 6-3.308.9(a) and (d), Residential Receptacles, Placement.~~

~~d. Recycling Requirements — Developer shall comply with Municipal Code Article 6. Recycling Requirements for Specified Business Activity, Sec. 6-3.601 Business Recycling Plan, and Sec. 6-3.602 Construction and Demolition Recycling Plan.~~

~~e. Site Improvement Plans shall follow the City of Ontario refuse collections standards~~



# Parkside

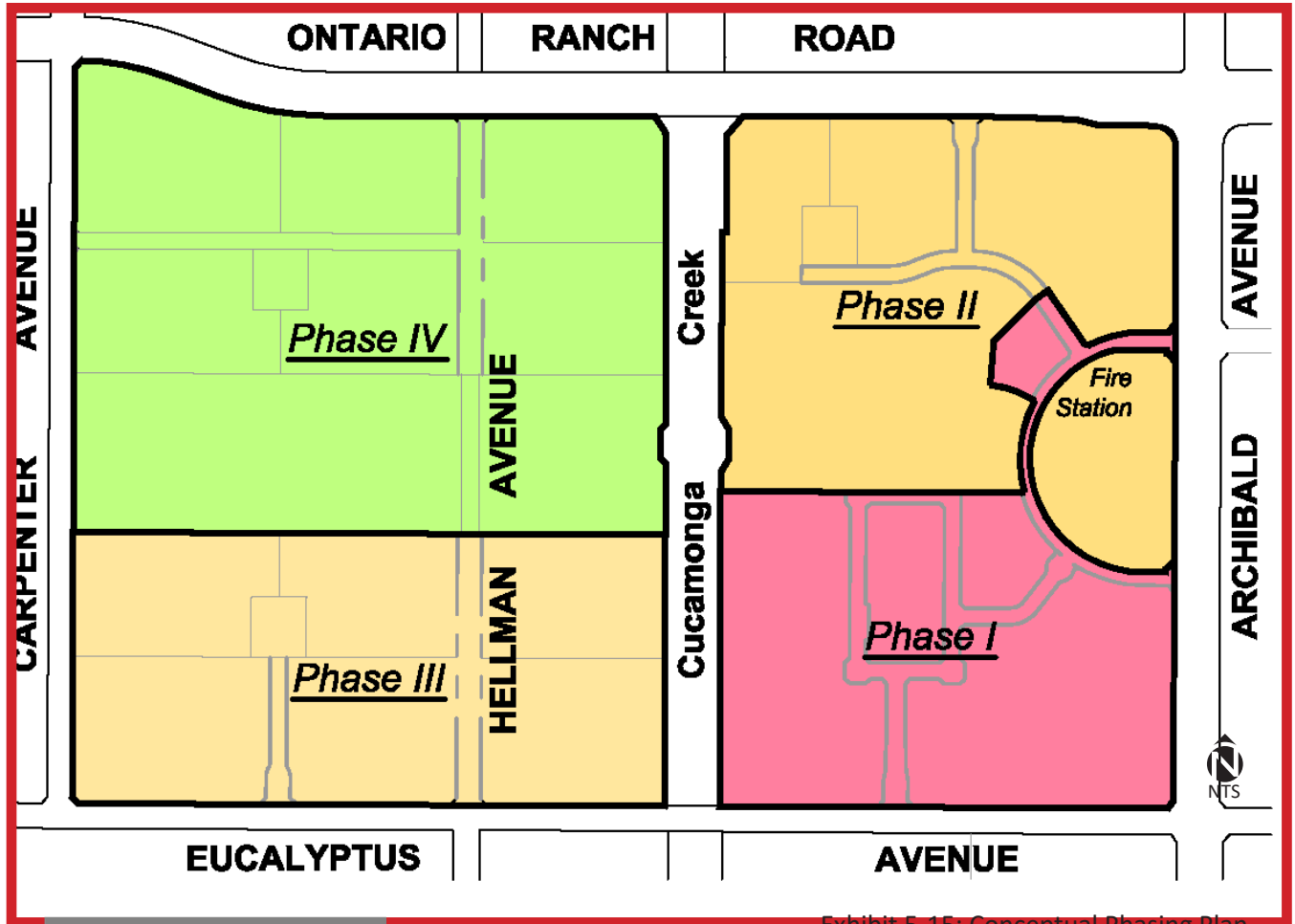


Exhibit 5-15: Conceptual Phasing Plan

Legend	
<span style="color: pink;">■</span>	Phase I
<span style="color: orange;">■</span>	Phase II
<span style="color: yellow;">■</span>	Phase III
<span style="color: green;">■</span>	Phase IV



# Parkside

~~Development Code Section 6.01.010F.4. (Refuse Storage Areas). For all commercial development, refuse and recyclable materials shall be stored as provided in Ontario Development Code Section 6.01.015E.17. (Refuse Storage Areas (Trash Enclosures)).~~

Project sites shall be designed to meet all the Integrated Waste Department's requirements, including the requirements for Sizing of Storage, Location of Collection Area, Accessibility for Collection Vehicles, and Collection of Sorted/Diverted Waste Types.

## 5.8 Grading Concept

The existing ground of the Specific Plan area generally slopes to the southwest at approximately 0.8% to 2.0%. The grading concept for Parkside is to work with the existing topography to maintain natural grade and elevations wherever possible. The grading operation for the Specific Plan area will generally consist of the removal of organic material remaining from farming operations, clearing and grubbing, demolition of existing structures, and moving of surface soils to construct building pads and streets. Additionally, where slope conditions are present, the property line shall be located at the top of a slope. In areas adjacent to a ridgeline or in moderate slope areas, dwelling units and buildings should be sited to:

- Use the natural ridgeline as a backdrop for structure buildings;
- Use landscape plant materials as a backdrop; and
- Use structure-buildings to maximize concealment of cut slope.

If retaining walls are required, the following criteria

shall be used:

- Exposed retaining walls facing roadways shall be no greater than 3 feet in height, except as necessary for acoustical purposes to satisfy the intent of the noise ordinance; and
- Where retaining walls face roadways, they shall be built of decorative materials consistent with the wall theme of the neighborhood.

The conceptual grading plan for the residential portion of Parkside, as illustrated in Exhibit 5-14: Conceptual Grading Plan will attempt to balance of cut/fills for the site.

Grading plans for each tract in the Specific Plan area will be reviewed and approved by the by the City of Ontario Building, Planning, and Engineering Departments prior to the issuance of grading permits. All grading plans and activities will conform to the City grading ordinance and dust and erosion control requirements.

## 5.9 Phasing

Development of the Specific Plan area will be phased to meet the following objectives:

- Orderly build-out of the Specific Plan area, based upon market and economic conditions;
- Provision for adequate infrastructure and public facilities as determined and deemed necessary by the City Engineer, and concurrent with development of each phase; and
- Protection of public health and welfare.

Development phasing of the project site will be determined by the landowner and/or developer based



# Parkside

upon real estate market conditions. Phasing will occur as appropriate levels of infrastructure, community facilities, and open space dedications are provided. Phasing sequencing is subject to change over time to respond to these various factors and individual phases may overlap or develop concurrently. As the Specific Plan is Phased, the following minimum criteria shall be met for each Subdivision and Development of each Phase:

- For Domestic Water, all the Master Plan, Regional and Local Infrastructure identified in Section 5 and the Conceptual Domestic Water Plan for the Water Pressure Zone that the Phase is within shall be completed as part of the Phase's infrastructure requirements or as described in the Project's Development Agreement. Also, any domestic water infrastructure necessary to provide primary looping, secondary looping, and meet all hydraulic criteria and fire flow shall be completed as part of the Phase's infrastructure requirements or as described in the Project's Development Agreement.
- For Recycled Water, all the Master Plan, Regional and Local Infrastructure identified in Section 5 and the Conceptual Recycled Water Plan for the Recycled Water Pressure Zone that the Phase is within shall be completed as part of the Phase's infrastructure requirements or as described in the Project's Development Agreement. Also, any recycled water infrastructure necessary to provide primary looping, secondary looping, and meet all hydraulic criteria shall be completed as part of the Phase's infrastructure requirements or as described in the Project's Development Agreement.
- For Sewer, all the Master Plan, Regional and Local Infrastructure identified in Section 5 and the Conceptual Sewer Plan for the Master Plan Sewer Tributary Area that the Phase is within and upstream of shall be completed as part of the Phase's infrastructure requirements or as described in the Project's Development Agreement. Also, any sewer infrastructure necessary to meet all hydraulic criteria shall be completed as part of the Phase's infrastructure requirements or as described in the Project's Development Agreement.

Exhibit ~~18~~ 5-14: Conceptual Phasing Plan illustrates Phases ~~A and C~~ I-IV for the project. Phases ~~A and C~~ I-IV are dependent on area-wide infrastructure and master plan improvements, probably beginning in ~~2006-2007~~. ~~Phase B is also dependent on master plan infrastructure, particularly sewer service.-2020-2021.~~

Backbone infrastructure to serve all areas of Parkside shall be installed by the developer(s) in accordance with the City's adopted Master Plan for the areas or any approved amendments to it. Infill service mains will be installed/constructed in phases as development proceeds and conditioned by the City [Engineering Department](#) to support individual phases of development, which may require installation of off-site infrastructure improvements beyond a given phase boundary.

Mass grading and installation of infrastructure to serve Parkside is anticipated to be completed in [multiple](#) phases. ~~Phase A is that portion of Parkside lying westerly of the Cucamonga Creek Channel, Phase B is the residential~~



# Parkside

~~portion lying easterly of the Cucamonga Creek Channel and Phase C is the commercial area located on the southwest corner of Archibald Ave. and Edison Ave.~~

The determination of which phase will be constructed first, either **Phase A I** or **Phase B-II** will be decided by the master developer and will be based on the availability of the sewer trunk main which will serve the phase. ~~Phase C~~ **The retail center development** will be constructed when there are a sufficient number of homes constructed to support the **neighborhood shopping center development**. It is anticipated that the fire station will be constructed **along with before** the first phase or at a subsequent phase determined by complying with the City requirement of a 5-minute emergency response time to the Parkside community. The mass grading for each phase will include that portion of the Great Park included within the phase boundaries. Refer to Exhibit ~~18~~ **5-15: Conceptual Phasing Plan.**

Following completion of the mass grading and infrastructure installation for the phase, the merchant builders will construct models for the product types being sold and the recreation area and pocket parks planned within that phase. The timing for installation of infrastructure and utilities and the provision of public services for the residential development within Parkside will be determined as part of the City's approval of tentative maps or development plans. Facilities will be constructed and services available in a timely manner as development progresses.

~~A "Model Home Permit" will be required for all model homes. The "Model Home Permit" will be reviewed through the "Temporary Use Permit" process.~~

**"Model Homes" shall meet the requirements of Section 5.03.395B (Model Homes) of the Ontario Development Code.**



## Chapter 6: Development Regulations



Denver, Colorado: City Park, Indian Monument





# Parkside

## 6.0 Development Regulations

### 6.1 Introduction

The provisions contained herein will regulate design and development within the Parkside Specific Plan area. The regulations contained herein provide for the development of all land use categories with development regulations established for residential uses, commercial uses, public use (fire station), parks and recreational facilities, and landscaping.

### 6.2 Definition of Terms

The meaning and construction of words, phrases, titles, and terms shall be the same as provided in the [City of Ontario Development Code Article 2, "Definitions,"](#) unless otherwise specifically provided herein.

### 6.3 Applicability

The development regulations contained herein provide specific standards for land use development for the Parkside Specific Plan ~~within the subject portions of Subareas 22 and 23.~~ Regulations address residential, commercial, public, and parks and recreation land uses. General landscaping regulations are also provided. Application of the following regulations is intended to encourage the most appropriate use of the land, ensure the highest quality of development, and protect the public health, safety, and general welfare. The adoption of this Specific Plan by [resolution ordinance](#) supersedes otherwise applicable [City of Ontario zoning Development Code](#) regulations unless stated herein to the contrary. Whenever the provisions and development standards contained herein conflict with those contained in the

Ontario Development Code, the provisions of this Specific Plan shall take precedence. Where the Specific Plan is silent, City codes shall apply. These regulations shall reinforce specific site planning, architectural design, and landscape design guidelines contained in Section 7, Residential Design Guidelines, and Section 8, Commercial Design Guidelines, of this Specific Plan.

### 6.4 Administration

The Specific Plan is adopted by ordinance and will serve as the implementation tool for the [Policy Plan \(General Plan\) component of The Ontario Plan](#) as well as the zoning district for the site. The Specific Plan Development Regulations address general provisions, permitted uses, and [some](#) development standards for the [project Specific Plan area](#).

### 6.5 Methods and Interpretation

Development within the Specific Plan shall be implemented through the approval [by the City](#) of parcel maps, tract maps, and development permits. The [administrative](#) process described herein provides the mechanisms for review and approval of development projects within the project, consistent with the Specific Plan objectives.

### 6.6 General Site Development Criteria

The following general site development criteria shall apply to all land development proposed in the Specific Plan.



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**Gross Acres**—Except as otherwise indicated, gross acres for all development areas are measured to the centerline of streets;

**Grading**—Development within the site shall utilize grading techniques as approved by the City of Ontario. Grading concepts shall respond to the grading design included in this Specific Plan which guide the development of land use toward the goal of providing for a livable community with streets and entries designed for walking and resident interaction;

**Structure Modification**—Additions ~~and/or projections into setback areas permitted by the Specific Plan~~ shall match the architectural style of the primary unit and shall be constructed of the same materials and colors as the primary unit;

**Utilities**— All new and existing public utility distribution lines of ~~less than~~ 66 kV or less shall be subsurface throughout the planned community;

**Technology**—All homes and businesses shall accommodate the most modern technology for ~~computer~~ internet access, phone, fax, and television. Broadband fiber optics cable will be installed on all peripheral streets ~~and will be available to provide service to each home~~ per the approved Broadband Master Plan;

**Density**—The Specific Plan Land Use Plan allocates a total number of units to each residential neighborhood ~~as indicated in the Land Use Summary, Table 1, included in Section 4, Land Use, of this Specific Plan. As a master-planned community developer, the developer will review each guest builder's plans and design and strongly encourage the use of current architecture, design, and product type. Accordingly, variations in the number and type of dwelling units within each residential neighborhood may occur~~ [Table 4-1: Land Use](#)

[Summary, included in Section 4, Land Use](#). Variations in the number and type of dwelling units within each residential neighborhood may occur at the time of final design of the neighborhood, depending upon the residential product identified for development. [Water and Sewer Technical Studies shall be prepared for unit transfers that change the quantity and locations of sewer and water demands. If the net changes impact the overall projected water or sewer demands, or the sewer demands are transferred between different Master Plan Trunk Sewer Tributary Areas and Specific Plan Sewer Tributary Areas, then a Water or Sewer Master Plan Amendment may be required.](#) Increases or decreases in allocation of residential units up to a maximum of 15 percent are permitted among the planning areas within the Specific Plan area, provided the total number of units established for the Specific Plan area is not exceeded;

**Maximum Number of Dwelling Units**—The maximum number of residential dwelling units permitted within the Specific Plan ~~for portions of Subareas 22 and 23~~ shall be 1,947 dwelling units;

**Measurement of Lot Widths**—[The measurement of the width of the lots on curvilinear streets shall be taken from the front and rear setback lines. The front setback lines shall be displayed by dotted lines or by plotting the footprint of the dwelling unit; and](#)

**Encroachments**—Buildings or parts of buildings, in particular roof overhangs, shall not encroach into the 3-foot building setback from the property line.



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## 6.7 Implementation

Development proposals within the [Specific Plan area](#) shall be subject to the implementation procedures established herein. The provisions and development standards contained herein ~~conflict with~~ [supersede](#) those contained in the Ontario Development Code. [However, where the development standards are silent, the provisions of the Specific Plan Ontario Development Code shall take precedence apply;](#)

**Severability**—If any portion of these regulations is declared to be invalid or ineffective in whole or in part, such decision shall not affect the validity of the remaining portions thereof. The legislative body hereby declares that they would have enacted these regulations and each portion thereof irrespective of the fact that any one or more portions be declared invalid or ineffective;

**Ambiguity**—Unless otherwise provided, any ambiguity concerning the content or application of the Specific Plan shall be resolved by the Planning Director ~~or his/her designee, hereinafter referred to as Director~~, in a manner consistent with the goals, policies, purpose and intent established in this Specific Plan; and

**Deviation**—Any major deviation from the design guidelines within the Specific Plan shall require a Specific Plan Amendment. The [Planning](#) Director shall determine whether a proposed change constitutes a major deviation.

## 6.8 Development Permit

All development projects within the Specific Plan shall be subject to the Development Plan Review Process as established in on [Table 2.02-1 Review Matrix](#) of the Ontario Development Code. Adoption of ~~the project the~~ [Specific Plan](#) by the City includes adoption of the design guidelines contained within this Specific Plan. Where the Specific Plan design guidelines are silent, the design guidelines contained within the Ontario Development Code shall apply. The design guidelines are intended to be flexible in nature while establishing basic evaluation criteria for the review by the City of developer projects during design review.

Pursuant to these provisions, the Development Permit process constitutes a design review of project architecture, site plans, and landscape plans. All development project applications shall include a landscape and irrigation plan describing plant materials and their growth habits, plant size and spacing, methods of irrigation and landscaping maintenance, site plans, architectural elevations, floor plans, grading plans and other requirements as specified by the City. Development permits will be approved with conditions of approval.

## 6.9 Subdivision Maps

Approval of the Tentative Tract Map(s) and Parcel Maps will create legal lots for development. All Tentative Tract Maps and [Parcel Lot](#) Maps will be reviewed and approved pursuant to applicable provisions of Division 6.08 (Subdivisions) of the Ontario [Subdivision Ordinance Development Code](#) and consistent with the applicable provisions contained within this Specific Plan.



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## 6.10 Specific Plan Modifications and Amendments

### 6.10.1 Minor Modifications

The following constitute minor modifications to the Specific Plan, not requiring a Specific Plan Amendment, and are subject to review and approval by the Zoning Administrator. The Zoning Administrator shall have the discretion to refer any such request for modification to the Planning Commission or the City Council.

- Change in utility and/or public service provider;
- An increase or decrease of more than 15 percent to the number of units within a planning area an individual neighborhood, provided the total number of units for the entire project site do Specific Plan areas does not exceed that established in the approved Specific Plan;
- Adjustment of a neighborhood phase boundary, provided the total acreage of the affected area does not increase or decrease by more than 20% of the total acreage stated in the approved Specific Plan;
- Minor changes to the design guidelines, which are intended to be conceptual in nature only, and are intended to be flexible in implementation; and
- Other modifications of a similar nature to those listed above, which are deemed minor by the Planning Director, which are in keeping with the purpose and intent of the approved Specific Plan and which are in conformance with the General Plan.

### 6.10.2 Specific Plan Amendments

Amendments to the Specific Plan may be requested by the applicant or by the City at any time pursuant to Section 65433(a) of the Government Code. Amendments shall be processed pursuant to the provisions of the Government Code for Specific Plan Amendments. In the event the proposed amendment requires supplemental environmental analysis pursuant to the California Environmental Quality Act (CEQA), the applicant(s) will be responsible for preparing the necessary CEQA documentation.

~~A Specific Plan Amendment shall be required if adjustments are made to housing types or product lines in any neighborhood that do not meet the policy goals of the New Model Colony, Development Standards in Chapter 6, and Design Guidelines in Chapter 7.~~

### 6.11 Appeals

Appeals from any determination of the Zoning Administrator shall be made to the Planning Commission. The applicant(s) or any other entity shall have the right to appeal the decision of the Planning Commission on any determination. Appeals shall be submitted by filing an application on forms provided by the City within 10 days following the final date of action for which an appeal is made. Appeals shall be processed consistent with the provisions of Division 2.04—Appeals of the Ontario Development Code.



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## 6.12 Project Financing

Construction and maintenance of public improvements and facilities to serve the Specific Plan will be financed through a combination of financing mechanisms including the use of public and private funds. In order for the project to be fiscally self-sufficient the following options can be considered for implementation:

### Maintenance

A ~~community facilities lighting and landscape~~ district ~~may can~~ be implemented in order to defray the costs associated with street maintenance and street light costs.

### Facilities and Services

A variety of other financing options ~~may can~~ be considered by the City of Ontario for financing facilities and services to be provided within the Specific Plan area to include:

- Establishment of a Mello-Roos Community Facilities District, which enables governmental entities to finance public infrastructure on a tax-exempt basis;
- Special assessment districts;
- Benefit assessment financing for improvements such as storm drains and street lighting; and
- Integrated financing districts such as landowner assessments that are contingent upon development.

City Council approval is required for the establishment of all special district financing mechanisms. Establishment of a district would enable the City to levy a special tax and to issue bonds for the financing of improvements and/or services. Among the facilities that potentially could be financed are flood control, drainage, utilities, street improvements and other infrastructure, and

related facilities ~~and/or payment for such infrastructure.~~

The district provisions for those services would cover all phases, including planning, design, acquisition, construction, operations, and maintenance. These assessments would only affect the subject portions of ~~Subareas 22 and 23~~ this Specific Plan, and the debt service incurred by these assessments would be paid by landowners or homeowners within the Specific Plan area.

## 6.13 Maintenance Responsibilities

The public and private improvements constructed within the Specific Plan area will be maintained through a combination of public and private entities as described ~~below and~~ in Table 3: Maintenance Responsibilities.

During the course of maintenance of public utilities (including storm drain) within private ~~and public~~ streets, the City will pave the streets and restore landscaping per City standards. Restoration of any enhancements above and beyond City standards, including but not limited to architectural, hardscaping, and landscaping enhancements shall be the responsibility of the HOA or other entity maintaining those enhancements. This applies to all areas where public utilities are located including but not limited to public and private streets, gated communities, ~~lanes (private~~ alleys, etc.

### 6.13.1 Public Maintenance

Water, recycled water, and sewer utilities may be designated as “public utilities” if located within public or private streets. All public utilities within private streets shall be designed and constructed per City standards and contained within acceptable easements. The CC&RS shall contain language that requires all proposed work by the HOA within said easements to be plan checked and



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inspected by the City, including applicable fees. Generally, utilities will not be accepted as public within alleys, parking areas or driveways. Utilities within commercial and industrial parking lots and loading areas shall be designated as private. The extent to which said utilities would be accepted as public utilities shall be determined, at the full discretion of the City, during final design plan review. Refer to Table 6-1: Maintenance Responsibilities.

### 6.13.2 Homeowner Association

Refer to Table 6-1: Maintenance Responsibilities.

Table 6-1: Maintenance Responsibilities

	City CFD	Private Homeowners Association (HOA)	Private (Homeowner/ Property Owners)	Utility Entity
Master Plan Roadways <ul style="list-style-type: none"> <li>• (Archibald Avenue, Eucalyptus Avenue, Ontario Ranch Road, Hellman Avenue, Carpenter Avenue)</li> <li>• Parkways and Neighborhood Edges (curb to perimeter walls including landscape, sidewalks, trees)</li> <li>• Community Theme Wall and Entry Monuments (outside face for graffiti removal and paint)</li> </ul>	X			
Monument Signs (within Neighborhood edges)		X		
Traffic Signals/Street Lights	X			
Traffic Control Signs (Public)	X			
Public Roadways	X			
NPDES Facilities on Arterial Streets (Off-site)	X			
Entry Streets (curb to perimeter walls including landscape, sidewalks, trees)	X			
Interior Project Streets (curb to curb Primary Entry Street, Secondary Entry Streets, Local Streets)	X			
Regional Park	X			
Community Trail (Cucamonga Creek Channel Trail)	X			
Off-site and In-tract water, sewer, and storm drain improvements (only those facilities in public roads and those in private streets within public utilities easements that meet public improvement design criteria)	X			
Police	X			
Fire	X			





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On-site water, sewer, and storm drain improvements (improvements that are: private, laterals, and lines behind meters and/or DCDAs, improvements serving only one lot/parcel, not within public or private roads, not within public utility easements, or not meeting public improvement design criteria)		X	X	
Private Streets in Gated Multi-Family and SFD Complexes		X		
Parkways of Interior Project Streets (landscaping, sidewalks, trees and includes restoration work following public street repairs)		X		
Interior Tract Graffiti Removal		X		
Private Recreation Areas		X		
	<b>City CFD</b>	<b>Private Homeowners Association (HOA)</b>	<b>Private (Homeowner/ Property Owners)</b>	<b>Utility Entity</b>
Monument Signs within tract entry		X		
Traffic Control Signs (Private)		X		
Lanes (Private Alleys)		X		
Pocket Parks		X		
Paseos		X		
Landscaped Common Areas		X		
Community Theme Wall and Entry Monuments on Master Planned Roadways: Surface (interior) opposite street side (structural integrity and face repairs)		X		
Lane (Private Alley) Landscaping and Lighting		X		
Interim Detention Basin		X		
NPDES Facilities (On-site) in Public Right-of-Way		X		
Front Yard/Corner Street Side Landscaping Areas			X	
Private Interior Yard Walls			X	
Private Exclusive Use Landscaped Areas			X	
Driveways and Parking Areas Serving Commercial			X	
Communication Systems	X			X
Electricity				X
Natural Gas				X

## 6.14 Residential Development Standards



# Parkside

## 6.14.1 Residential Detached

### General

This category includes the development of all single-family detached dwelling units. The purpose of the residential standards for single-family detached housing is to establish the minimum criteria for the development of these product types in cluster configurations within the neighborhoods specified within the Specific Plan.

[Additional residential design guidelines can be found in Chapter 7.0: Design Guidelines.](#)

### Permitted Uses

- Single-family detached dwellings, and their accessory uses;
- Public or private parks, recreational buildings, newsstands, coffee concessions and sales of grocery items, greenbelts, or open space;
- Accessory uses to include the following:
  - Garages ([minimum 2-car](#));
  - [Granny flats \(i.e. Second Dwellings\)](#), in accordance with the [Ontario Development Code](#);
  - Home occupations;
  - Swimming pools, spas, sports courts, and other similar outdoor recreational amenities;
  - Patios and patio covers;
  - Storage, garden structures, cabanas, and greenhouses.
  - Monument signage;
  - Temporary uses such as model home and subdivision sales trailers; temporary construction

parking, offices, and facilities; real estate signs, signage indicating future development, [marketing signage](#), and directional signage;

- Second story additions to existing single story dwelling units; and
- Child care facilities up to 7 children.

### Conditionally Permitted Uses

- Places of worship; and
- Child care facilities/day care facilities (8-14 children).

### Determination of Use

Uses not specifically listed may be deemed permitted subject to a “Determination of Use” as provided for in Section [1.02.010](#) ~~1.1310~~ of the Ontario Development Code.



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## **6.14.2 Residential Attached**

### **General**

The category includes the development of all multi-family attached dwelling units. The purpose of the residential standards for multi-family attached housing is to establish the minimum criteria for the development of these product types in street facing and cluster configurations within the neighborhoods specified within the Specific Plan.

### **Permitted Uses**

- Multi-family attached dwellings, and their accessory uses;
- Public or private parks, recreational buildings, coffee concessions, greenbelts, or open space; and
- Accessory uses to include the following:
  - Garages;
  - Senior apartments, townhomes, and/or condominiums;
  - Home occupations;
  - Swimming pools, spas, sports courts, and other similar outdoor recreational amenities;
  - Patios and patio covers;
  - Storage, garden structures, cabanas, and greenhouses;
  - Monument signage;
  - Temporary uses such as model home and subdivision sales trailers; temporary construction parking, offices, and facilities; real estate signs, signage indicating future development,

marketing signage, and directional signage;

- Second story additions to existing single story dwelling units; and
- Child care facilities up to 7 children.

### **Conditionally Permitted Uses**

- Religious Assembly; and
- Child care facilities/day care facilities (8-14 children).

### **Determination of Use**

Uses not specifically listed may be deemed permitted subject to a “Determination of Use” as provided for in Section 1.02.010 of the Ontario Development Code.



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## **6.15 Neighborhood Center Commercial Land Use Development Standards**

### **General**

This section sets forth the development regulations for development of commercial land uses within the Specific Plan.

### **Permitted Uses**

- Bakery (Retail);
- Barber Shop and Beauty/Nail Salon;
- Book Stores;
- Camera and Photographic Supply Stores;
- Clothing and Accessory Stores;
- Convenience Market;
- Daycare Facilities (Single-Family), 8 or less children;
- Delicatessen;
- Dry Cleaner;
- Electronics and Appliance Stores;
- Florist;
- Jewelry and Watches/Clocks;
- Laundry – Self-Service;
- Locksmith/Key Shop;
- Police Storefront/Sub-Station;
- Satellite dishes/Ham Radio Antennas (private use);
- Shoe Repair;
- Specialty Food Stores;
- Tailor; and
- Travel Agency

### **Conditionally Permitted Uses**

- Alcoholic Beverage Sales;
- [Commercial Day Care Facility \(including Adult Day Care\);](#)
- Banks, Credit Unions and Other Depository Institutions;
- Church;
- Money Transmitting;
- Other Financial Services;
- Police/Fire Station;
- Recreation Center; and
- Senior Citizen Center

Table 6-2: Neighborhood Commercial Development Standards

<b>Minimum Site Area</b>	7 acres, unless developed as part of an integrated commercial center. Minimum lot size shall be large enough to accommodate the proposed use and meet all Development Standards as specified within the Specific Plan.
<b>Maximum Development Floor Area Ratio (FAR)</b>	.35
<b>Minimum Building Setbacks <sup>(1)(2)</sup></b>	
• From Public Street Property Lines	20 ft.
• From Private and Local Street Property Lines	15 ft.
• From Interior Property Lines	0 ft.
• From Interior Property Line Adjacent to Residential District	25 ft.
• From Ontario Ranch Rd. Right-of-Way	35 ft.
• From Archibald Ave. Right-of-Way	19 ft.
<b>Parking &amp; Drive Aisle Setbacks<sup>(1)</sup></b>	
• From Public Arterial Streets (R/W)	25 ft.
• From Private and Local Streets & Drive Aisles to Bldgs. (R/W)	10 ft.
• From Interior Property Line	5 ft.
• From Rear Property Line	5 ft.
• From Interior Property Line Adjacent to Residential District	5 ft.
• From Ontario Ranch Rd. Right-of-Way	35 ft.
• From Archibald Ave. Right-of-Way	19 ft.
<b>Minimum Landscape Coverage</b>	15%
<b>Maximum Building Height</b>	
• Main Building	35 ft.
<b>Architectural Projections and Focal Elements such as Towers, Cupolas, and other<sup>(3)</sup></b>	55 ft.
<b>Porte-Cocheres <sup>(4)</sup></b>	1-story
<b>Other Development Regulations</b>	
• Walls, Fences & Hedges	When a non-residential use adjoins a residential district, a 6-foot high masonry wall shall be required at the interior side or rear property lines.
• Parking	Per Division 6.03 (Off-Street Parking and Loading), Ontario Development Code
• Screening	All loading areas shall be screened from adjacent public streets and residential and open space uses through the use of landscaping, earthen berms, decorative walls, or fencing. All storage, including cartons, containers, materials or trash shall be shielded from view within a building or area enclosed by a solid fence or wall not less than 6' in height.
	All roof-mounted equipment, including but not limited to, mechanical equipment, satellite dishes, tanks, ducts, towers, and all equipment appurtenant thereto, shall be screened on all sides from public view from the street adjoining properties, and neighboring residential units, by a parapet wall, decorative enclosure, or other architectural element. Equipment screening shall appear as an integral part of the structure architecture.
• Lighting	All interior and exterior lighting shall be directed away from residential and open space uses.

(1) All setbacks areas shall be landscaped.

(2) All setbacks are measured to habitable area, not architectural appurtenance or projection. An architectural projection is defined as an element that articulates the building elevation such as eaves, window and door pop-out surrounds, bay windows, pot shelves, chimneys, enhanced window sills, shutter detail, window trim, and balconies, and other similar elements. Such elements may project a max. of 3' into setback areas. Building setbacks shall be determined by the hierarchy of streets (major arterial and local streets) and measured from PL for local streets and RW for major arterials.

(3) Architectural elements are not to be used for signage, subject to Planning Director approval. Within commercial areas, towers and vertical projections are limited in size to 10% of the total building square footage.

(4) Porte-Cocheres shall be open on three sides.



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## Determination of Use

Uses not specifically listed may be deemed permitted subject to a “Determination of Use” as provided for in Section 1.02.010 of the Ontario Development Code.

## Design Guidelines

Prior to the approval of the first site development permit and/or lot map within the Community Commercial land use area, a detailed set of design guidelines for the area shall be submitted by the developer and approved by the City.

## 6.16 Public Use (Fire Station) Development Standards

### General

This section sets forth the development regulations for development of the Fire Station land use in the Specific Plan.

### Permitted and Conditionally Permitted Uses

Permitted uses are as defined in Table 5.02-1: Land Use Matrix under the CIV (Civic) Zoning District of the Ontario Development Code.

~~Permitted uses are as defined in Article 13, Sec. 9-1.1300; Permitted, Conditional and Ancillary Land Uses of the City of Ontario Development Code.~~

### Determination of Use

Uses not specifically listed may be deemed permitted subject to a “Determination of Use” as provided for in Section 1.02.010 of the Ontario Development Code.

## 6.17 Landscape Standards

### 6.17.1 General Provisions

- All landscape plans, streetscape plans, and graphic designs which create neighborhood identity or enhance entry monuments shall conform to the design guidelines and regulations as set forth herein and shall be subject to review and approval by the City;
- The landscape/streetscape improvements for the Specific Plan area shall establish a landscape theme reminiscent of the regional landscape character of the surrounding area;
- Installation of landscaping within ~~PA1 through PA19 residential areas of Parkside~~ the front yards of single-family detached housing products will be provided by the home builder; ~~and: Front yard landscaping shall be installed prior to Certificate of Occupancy;~~
- Installation of landscaping in residential areas of Parkside may be provided by the home builder, based on future landscape plans submitted to the City during site plan review;
- Installation of landscaping within the commercial area of the Specific Plan will be provided by the developer/builder; ~~and~~
- All loading areas shall be screened from adjacent public streets, and residential and open space use through the use of landscaping, earthen berms, and/or decorative walls or fencing. All storage including cartons, containers, materials, or trash shall be shielded from view within a building or area enclosed by a solid fence or wall not less than 6 feet in height.



# Parkside

## 6.17.2 Landscape Standards

- Landscaping within the Specific Plan shall be provided in accordance with the Design Guidelines for Streetscapes and Entries utilizing plant materials specified on the Plant Palette included in Section 7, Residential Design Guidelines, and
- Section 8, Commercial Design Guidelines, established for the Specific Plan.
- Boundary landscaping will be required adjacent to residential, commercial, park, and public (fire station) areas. Landscaping shall generally be placed along the entire property line; and
- Landscaping and irrigation systems within the public rights-of-way of the Specific Plan area shall be installed by the developer.
- ~~The developer will provide site inspection of all construction and installation of open space areas in accordance with City of Ontario requirements;~~
- ~~Parking lot landscaping within commercial areas shall be required in accordance with Section 9-1.3040 of the City's Development Code; and~~
- ~~Vegetation that is non-toxic, free of thorns or spines, and without any hazardous characteristics shall be utilized adjacent to all public open space areas.~~

## 6.17.3 Walls and Fences

- Freestanding perimeter walls and view fencing shall be provided within and at the perimeter of the project as specified in the Exhibit 40 Wall and

Fence Master Plan, Section 7, Residential Design Guidelines. Such walls and fences will be constructed concurrently with the construction of improvements required for development of the neighborhoods of the Specific Plan;

- Walls and Fencing—Perimeter walls shall be constructed in locations and of a design consistent with the Residential Design Guidelines and the Fence and Walls Plan, Exhibit ~~40~~ 7-26, and Fence and Wall Details, Exhibits ~~41 and 42~~ 7-27 through 7-30, provided in Section 7, Residential Design Guidelines, and shall not exceed 6 feet in height from finished grade. Perimeter walls may exceed 6 feet in height if required for sound attenuation. Perimeter walls shall be constructed of either decorative masonry (both sides) or other permanent, durable, low maintenance material subject to approval by the City;
- Residential—Side and rear yard walls and fencing shall not exceed 6 feet in height from the highest adjacent finished grade. Walls and fencing within the residential front yard setback area shall not exceed 3 feet in height. Walls shall be decorative masonry construction (both sides) or other permanent low maintenance materials. ~~Decorative all-weather vinyl fencing is not permitted~~ Front yard fences may be constructed of attractive, durable materials, including, but not permitted limited to, wrought iron with pilasters, textured concrete block, formed concrete with reveals, or other materials approved by the Planning Director. View fencing may be of a decorative wrought iron, glass panels, or other durable material subject to approval by the City;



# Parkside

- All perimeter wall and fence materials throughout the Specific Plan ~~shall be constructed of decorative material and~~ area will be of uniform manufacture with colors specified for the overall design theme;
- ~~Decorative block (both sides) is required for all interior block walls;~~
- All interior walls (both sides) that are visible from the public street shall be constructed of attractive, durable materials, including, but not limited to, wrought iron with pilasters, textured concrete block, formed concrete with reveals, or other materials approved by the Planning Director. Colors shall be consistent with the colors specified for the overall design theme and shall be decoratively capped. Decorative all-weather vinyl fencing may be permitted for compact small lot product, subject to Planning Department review and approval;
- ~~Combo walls are also permitted which include decorative block~~The developer will provide site inspection of all construction ~~and wrought iron.~~ Combo walls shall not exceed the maximum height installation of open space areas in accordance with City requirements;
- Parking lot landscaping within commercial areas shall be required in accordance with Division 6.05 (Landscaping) of the Ontario Development Code;
- Non-toxic and non-invasive vegetation shall be utilized adjacent to all public open space areas; and
- All landscaping features shall comply with City requirements for sight distance.

## 6.18 Signage

A Master Sign Program shall be submitted by the developer(s) of each land use component for the Specific Plan and approved by the City pursuant to Division 8.0 (Sign Regulations) of the Ontario Development Code to address residential project entries, residential neighborhood identification signs, commercial center identification, tenant signage, and “way-finding” signs within the Specific Plan area. All other signs shall be subject to the approval of a sign permit pursuant to the Ontario Development Code. All traffic-related signs shall conform to City Traffic and Transportation Design Guidelines and California Manual of Uniform Traffic Control Devices (MUTCD), latest editions.





# Parkside

## **6.18.1 Master Sign Program Contents**

All sign programs shall address, at a minimum, the following:

- Permitted signs;
- Prohibited signs;
- The hierarchy of signage;
- Definition of types of signs;
- Locations and dimensions for monument signs, neighborhood identification signs, and public facilities signs;
- Locations and dimensions of directional signage;
- Provisions for size, location, and duration of display of temporary signs;
- Permitted sign types, styles, construction materials, colors, and lettering styles;
- Requirements for a sign permit application;
- Procedures for obtaining approval of a sign permit; and
- Procedures for amendments to the sign program.

## **6.19 Private Recreational Facilities**

This category includes the development of all private recreation facilities and is subject to the Development Permit process contained herein.

*As consistent with Sections 5.7 and 7.5.2, all project sites shall be designed to meet all the Integrated Waste Department's requirements, including the requirements for Sizing of Storage, Location of Collection Area, Accessibility for Collection Vehicles, and Collection of Sorted/Diverted Waste Types.*



# Parkside

# Chapter 7: Residential Design Guidelines



New York City, New York: Central Park





# Parkside

## 7.0 Residential Design Guidelines

### 7.1 Introduction

Parkside is a new master-planned community located in Ontario, California. The historical character of the Ontario area is partly traditional and agricultural, but being on the path of the advancing railroad, the later automobile Route 66 and eventually Ontario International Airport, Ontario has also typically been associated with transit and forward-moving progress.

The proposed community design integrates the traditional character of the past with a plan which provides a variety in housing choices that serve the needs and lifestyles of today.

Parkside has been designed as a planned community “in the park.” The Great Park is the key organizational element for the entire community. Connections to both neighborhood and regional trails will be provided from this central park area. With a focus on promoting a pedestrian friendly, healthy lifestyle, the Great Park area is within less than a ¼ mile walk from all neighborhoods in the community.

#### Community Overview

Community design elements include:

- The Great Park;
- Neighborhood Commercial;
- A wide variety of home sizes and plans to fulfill the needs of different life stages, in gated and non-gated neighborhoods;
- Tree-lined neighborhood streets with curb-separated sidewalks and generous plantings in parks and

open spaces;

- Private recreational facilities for each neighborhood;
- A network of sidewalks and multi-purpose trails that link all neighborhoods to parks, open spaces, schools and nearby retail; and
- Nearby connections to local, regional, and global transit.

The community character is inspired by:

- The charm and tradition of Ontario’s historic downtown neighborhoods and an eclectic mix of architectural styles;
- The simple, yet stately landscape along Euclid Ave. and the region’s agricultural heritage; and
- Environments that encourage wellness by providing opportunities for pedestrians and outdoor physical activity.

~~Neighborhood retail goods and services will be conveniently located in Planning Area 21 at the southwest corner of the intersection of Edison Ave. and Archibald Ave.~~

~~The Specific Plan~~ Community entries along Archibald Ave., Edison Ave Ontario Ranch Rd., Eucalyptus Ave., and Carpenter Ave. will be announced with community character monuments and entry statements. Signage, lighting and monuments within the project site will reflect an identifiable community character.

#### 7.1.1 Guiding Principles

The following guiding principles have been created to help develop architecture that reflects quality in design, simplicity in form and plan, and contributes charm and



# Parkside

appeal to the **proposed** single-family and multi-family neighborhoods of Parkside.

- The home, not the garage, is the primary focus of the front elevation. A variety of garage placements is encouraged to emphasize the pedestrian environment as opposed to the automobile;
- A variety of compatible architectural styles are provided to ensure a degree of individuality throughout **neighborhoods**;
- Appropriate massing and roof forms play a major role in defining architecture styles, roof framing related to each plan vs. individual elevations;
- Architectural elements, details, and appropriate color palettes are used to reinforce individual architectural styles;
- Rear elevations **visible to the public viewshed should provide** articulated walls, windows, and varied roof profiles, all **provide defining** a level of **quality in** detail and interest in neighborhood architecture; and
- Selected architectural styles are related to those historically represented in Ontario. These are: Farmhouse, Cottage, **Adobe Ranch**, American Colonial, American Traditional, **American Traditional Revival**, Craftsman, **English Revival**, **Italianate**, Andalusian, Spanish Colonial, Tuscan, **Prairie** and Provence. **However, modern variations of these styles are permitted.** Modern interpretation of traditional styles, such as Adaptive American Traditional, Adaptive Farmhouse, Adaptive Mediterranean and Contemporary are permitted.

## 7.2 Architectural Styles

### 7.2.1 Introduction

The style information on the following pages provides the builder and design consultants with the tools to create functional and attractive architectural designs without using gimmicks or sacrificing the integrity of the style.

#### Authenticity

The design criteria **in these guidelines is offered** encourage a high quality of architecture and reasonable level of authenticity of styles through the use of appropriate elements. Although detail elements may be used to further convey the character of a style, the overall massing and appropriate roof forms should be used to establish a recognizable style. Proper scale and proportion of architectural elements and appropriate choice of details are all factors in achieving authenticity.

### 7.2.2 Architectural Philosophy

Looking back to Ontario's older, established neighborhoods for inspiration, the **design intent for Parkside** is to recapture the charm and essence of these original home styles and express them in the simple, honest manner that they originated. These homes reflected function and tradition, simplicity in massing, plan and roof forms, and authentic, regional styles. **Parkside homes will provide the kind of function, quality, value, and appeal that were built into those original homes.** Additional more progressive styles that also reflect this charm are also allowed in Parkside. Neighborhood parks, sidewalks, parkways, and street trees will add to the traditional feeling in these neighborhoods **linking them with the overall community.**



# Parkside

## Design Character

The design character **of the neighborhoods** will be one of hometown appeal characterized by simple, yet charming homes, curb separated sidewalks, and parkways lined with street trees. Homes and streetscenes will reflect an overall feeling of quality and tradition. The architectural palette of styles has a regional historic background **and context in keeping** but includes more modern styles to reflect with the character of Ontario.

## 7.2.3 Architectural Styles

### Architectural Style/Plan Mix - Planning Areas 5-18

An important goal in this community is to develop small, varied, and interesting streetscenes. Typically, the neighborhoods will consist of 80-100 dwelling units. In order to achieve dynamic streetscenes the following architectural style and plan requirements must be met according to the number of homes in a neighborhood:

~~In all~~ All single-family detached neighborhoods shall provide:

- A minimum of 4 plans and 3 elevations for neighborhoods of more than 100 homes;
- A minimum of 3 plans and 3 elevations for neighborhoods of ~~less than or equal to~~ 100 homes or less; and
- A minimum of 4 different color schemes per number of architectural styles used.

### Planning Areas 1-4 (Integrated Neighborhood)

PA 1-4 is envisioned to provide home types that are more attainable than the traditional larger lot homes. Simple yet sophisticated building forms mixed with

smaller square footage that efficiently and effectively utilize less room will help to accomplish this goal. These features help in providing elegant yet cost effective dwellings for a greater market base. PA 1-4 is proposed to be an Integrated Neighborhood that shall provide a minimum of 3 product types.

PA 1-4 Integrated Neighborhood shall provide:

- A minimum of 3 plans and 3 elevations for the Neighborhood; and
- A minimum of 4 different color schemes per number of architectural styles used.

### Cluster Homes Style/Plan Mix - Planning Areas 5-18

Cluster Homes plotting generally results in the same 1 or 2 plans being repeated along the street front. To support varied neighborhood streetscenes, each Cluster Home neighborhood shall provide:

- A minimum of 3 significantly different styles per plan. Although the styles shall be significantly different, they shall be coordinated to provide a harmonious and cohesive neighborhood preferably from a family of related architectural styles; and
- A minimum of 4 different color schemes per number of plans.

Number of plans and elevations shall not be regulated for interior units; the above standards shall apply to any unit plotted along (facing or siding on to) a street more than one time.



# Parkside

## **Architectural Styles**

Every neighborhood will be comprised of a variety of architectural styles selected from the following palette of styles. [There is a Menu of Common Features listed with each style that should act as a guide for design. Every feature does not have to be included in the architecture, but the features listed are characteristic to each style and may be used as appropriate to create authentic and identifiable elevations.](#) Additional styles may be proposed; however, they must follow the same principals and attention to detail as the specific styles here. These additional styles may be added with the review and approval of the Design [Advisory](#) Board.

## **American Heritage Style Series**

This series represents traditional American styles found throughout the country. The architectural form and design elements of these styles descend from the first homes built in the New England colonies in the 17th century. This traditionally influenced American Heritage series became part of the early California vernacular as the new state experienced an influx of money and population from the East Coast during the gold rush era of the mid-19th-century. Styles within this series include the following:

- American Colonial
- American Traditional
- Farmhouse

## **California Heritage Style Series**

California Heritage styles re-interpreted architectural lifestyle choices with the addition of Western materials and innovation while retaining the decorative detailing of exposed wood work, wrought iron hardware, and moldings. Craftsman style homes may use specialized wood working and siding materials to create beautifully simple homes suited to the desirable California landscape. Representing one of California's true vernacular styles, the adobe ranch style evolved using native materials and considerations of climate and lifestyle. Styles within this series include the following:

- Craftsman

## **Mediterranean Heritage Style Series**

The Mediterranean heritage style series include transplanted styles developed in a climate zone similar to the climate found in California. The Mediterranean styles are not strictly European, but rather American stylization of European landmarks and residences that were popular in the late nineteenth century. Primarily stemming from Italian, French and Spanish influences, these styles are principally based on simpler and informal residential living styles of country settlements or old world villages. Their appeal is in their informal, rustic character expressed in warm colors, textures and materials. Although residential adaptations were less formal, sometimes traditional classical elements are included. Styles within this series include the following:

- Andalusian
- Provence
- Spanish Colonial
- Tuscan





# Parkside

## Revival Style Series

Southern California residential neighborhoods of **Each architectural style is defined by a series of “elements” and “common features,” which illustrate the distinguishing typical characteristics of that style:** the 1920s and 1930s captured this aesthetic particularly well, translating a vital mix of people and ideas into an architecture of variety and comfort. These communities can be said to have no inherent style, but instead are interpreted through a number of disciplines to form residences both singular and appropriate to the area. This method of architectural expression can be loosely referred to as “revival,” yet is perhaps more aptly described by its atmosphere, which is relaxed and casual. As a result, these areas manage to retain the romance of their stylistic roots yet remain relatively free of their basic cultural idioms. It is no coincidence then that we can find in these neighborhoods a true evocation of the Southern California aesthetic; one which represents creativity, diversity, imagination, and openness. Styles within this series include the following:

- Cottage

## Millennium Style Series

The Millennium Collection represents subtle differences in the architectural vocabulary using creative interpretation of traditional styles utilizing exaggerated statements and details to express an iconic style in a new vocabulary. Elevations can be abstracted architectural expressions of a recognizable style. These elevations are encouraged to push the artistic envelope of design to incorporate new, modern or alternative forms, details and materials in the modern context of architecture. Styles within this series include the following:

- Adaptive American Traditional
- Adaptive Farmhouse
- Adaptive Spanish
- Adaptive Mediterranean
- European Heritage
- Contemporary

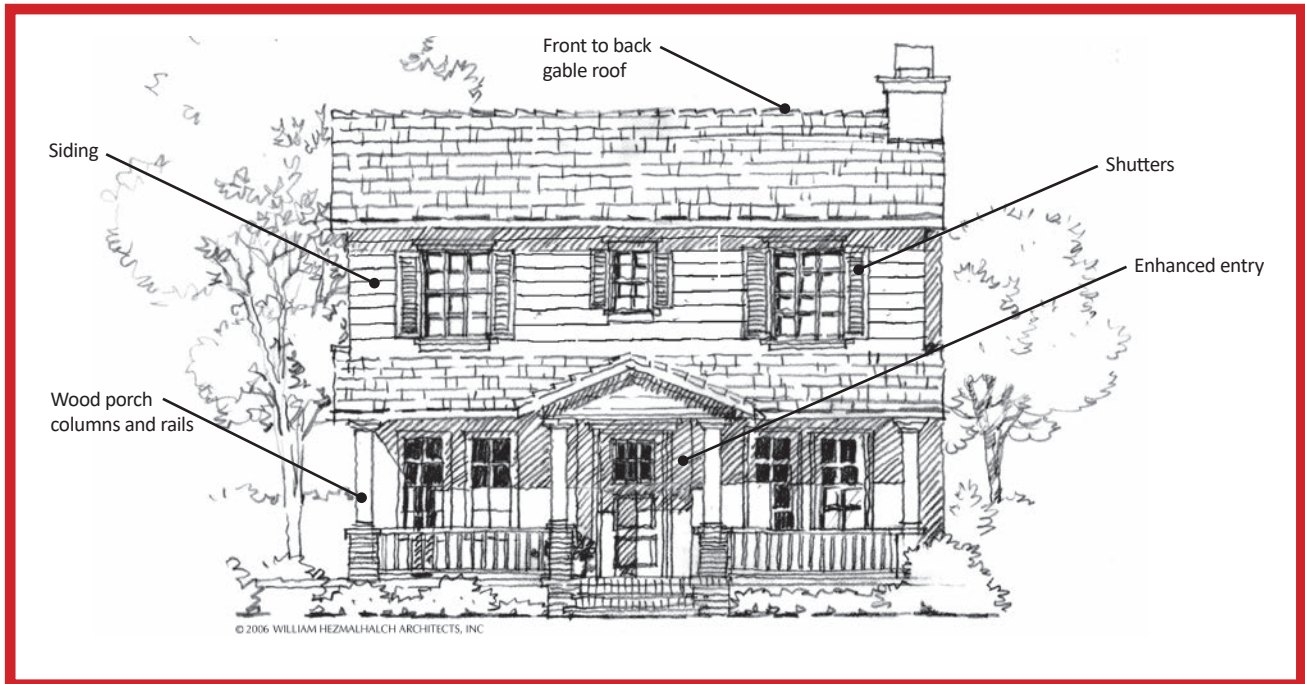


# Parkside

## American Colonial

This classic American style descended directly from the first homes built in the New England colonies in the 17th century. Their beginnings were as small and unpretentious as the one story saltbox, favoring the cultures and traditions of the settlements.

As living functions became more defined and prosperity increased, so did the need for additional space. Second stories with overhangs, dormers and gabled roof forms became favored solutions, later evolving into classic elements of this traditional American style. Wood shutters and an enhanced entry element and/or trim are the finishing details for an otherwise simple and functional form.





# Parkside



Vertical Multi-Pane Windows with Shutters



Accented Entry



Simple massing with a Red Door

Element	Menu of Common Features	
<b>Form</b>	<ul style="list-style-type: none"> <li>• Simple plan form and massing</li> </ul>	<ul style="list-style-type: none"> <li>• Added dormers</li> <li>• Enhanced plan forms</li> </ul>
<b>Roof</b>	<ul style="list-style-type: none"> <li>• 4:12 to 12:12 roof pitch</li> <li>• 12" to 16"-18" overhang</li> <li>• Simple front to back gable roof</li> <li>• Architectural quality asphalt shingles or shingle texture flat concrete tiles</li> </ul>	<ul style="list-style-type: none"> <li>• 12" to 24" overhang</li> <li>• Shed or pitched roof dormers</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>• Horizontal siding may be combined with stucco.</li> <li>• All material changes must occur at an inside corner or logical end (return block wall/fence).</li> </ul>	<ul style="list-style-type: none"> <li>• Wrapped horizontal siding elements on sides and/or rear elevations in high visibility public view areas</li> <li>• Brick as a face material or accent material</li> </ul>
<b>Windows</b>	<ul style="list-style-type: none"> <li>• Vertical multi-paned window inserts at front elevations</li> <li>• Multi-paned window on side and rear elevations in high visibility public view areas</li> <li>• Vinyl wrapped windows</li> </ul>	<ul style="list-style-type: none"> <li>• Bay windows</li> <li>• Dormer window in roof</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>• Simplified cornice trim at gable ends</li> <li>• Wood porch columns and rails</li> <li>• 2 x 6 window and door trim - wood on siding, foam on stucco</li> <li>• Enhanced entry element such as a covered porch</li> <li>• Surface mounted fixtures on front elevations must complement architectural style</li> </ul>	<ul style="list-style-type: none"> <li>• Shutters, enhanced window trims, louvered attic vents</li> <li>• Garage door patterns to complement style</li> <li>• Neoclassic columns or stucco posts at entry</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>• Field: Pastels, whites, light earth tones, off-whites, or dark earth tones</li> <li>• Trim: Light or dark in contrast to field</li> <li>• Accents: Whites, light tones, or medium to dark tones in contrast to field, grayed or dark greens, blues, reds or black</li> </ul>	



## Parkside

# American Traditional

The American Traditional style as manifested in Southern California was often realized as an East Coast derived expression with Cape Cod influences, and displayed the aspects of practicality and functional elegance. This expression evolved from early Colonial beginnings and truly began to proliferate as an American building style from the 1850s to the early 20th century. In Southern California, the style was employed from the foothill neighborhoods of Pasadena to Los Angeles communities such as Westwood and was occasionally used in combination with the Monterey and Ranch styles to create homes which opened to the exterior gracefully yet retained a sense of formality and reserve.

Features of this style include simple gable roof forms, louvered shutters, and articulated entry surrounds. Dormers and second-floor wood decks are also occasionally used.





# Parkside



Element	Menu of Common Features
<b>Form</b>	<ul style="list-style-type: none"> <li>Asymmetrical massing with a vertical and a horizontal break</li> <li>Recessed 2nd floor</li> <li>Vertical and horizontal breaks in massing</li> </ul>
<b>Roof</b>	<ul style="list-style-type: none"> <li>4:12 to 12:12 roof pitch</li> <li>16" to 24" overhangs</li> <li>Concrete roof tile - flat or shake appearance</li> <li>Front to back gable or hip roof with intersecting hip or gable roofs</li> <li>Multiple intersecting hip or gable roofs at first and second floors</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>Stucco with horizontal siding accents allowed</li> <li>All material changes must occur at an inside corner or logical end (return block wall/fence).</li> <li>Wrapped horizontal siding elements on sides and/or rear elevations in high visibility public view areas</li> </ul>
<b>Windows</b>	<ul style="list-style-type: none"> <li>Vertical multi-paned window inserts at front elevations</li> <li>Multi-paned window inserts on side and rear elevations in high visibility public view areas</li> <li>Vinyl wrapped windows</li> <li>Round top accent or bay windows</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>Simplified cornice trim at gable ends</li> <li>Header window wood accent trim</li> <li>2x6 window and door trim - wood on siding, foam on stucco</li> <li>Surface mounted fixtures on front elevations must complement architectural style</li> <li>Full wood porches and/or wood balconies</li> <li>Garage door patterns to complement style</li> <li>Square wood columns with trim</li> <li>Wood window pot shelves</li> <li>Brick veneer or stucco chimney wrap</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>Field: Whites, off-white, dark or light colors</li> <li>Trim: White or contrasting with field color</li> <li>Accents: White, light or dark colors in contrast or harmony with field color</li> </ul>



## Parkside

# Farmhouse

The American Farmhouse represents a practical and picturesque country house. Its beginnings are traced to both Colonial styles from New England and later the Midwest. As the American Frontier moved westward, the American Farmhouse style evolved according to availability of materials and technological advancements, such as balloon framing.

Predominant features of the style are large front and/or wrapping porches with a variety of wood columns and railings. Two-story massing, dormers and symmetrical elevations occur most often on the New England Farmhouse variations. The asymmetrical, casual cottage look, with a more decorated appearance, is typical of the Midwest American Farmhouse. Roof ornamentation is a characteristic detail consisting of cupolas, weather vanes and dovecotes.





# Parkside



Full to Partial Width Front Porch



Shutters with Horizontal Siding Accents



Siding Accents

Element	Menu of Common Features	
<b>Form</b>	<ul style="list-style-type: none"> <li>• Simple plan form massing and simple roof design</li> <li>• Main gable roof with one or two intersecting gable roofs</li> </ul>	<ul style="list-style-type: none"> <li>• 12" to 16" overhangs with open eaves</li> </ul>
<b>Roof</b>	<ul style="list-style-type: none"> <li>• 4:12 to 12:12 roof pitch</li> <li>• Front to back main gable roof</li> <li>• 12" to 16" overhangs with open eaves</li> </ul>	<ul style="list-style-type: none"> <li>• Wrapped horizontal siding elements on sides and/or rear elevations</li> <li>• Tiles with smooth flat or shingled appearance</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>• Horizontal siding may be combined with stucco</li> <li>• All material changes must occur at an inside corner or logical end (return block wall/fence).</li> </ul>	<ul style="list-style-type: none"> <li>• Wrapped horizontal siding elements on sides and/or rear elevations in high visibility public view areas</li> </ul>
<b>Windows</b>	<ul style="list-style-type: none"> <li>• Vertical multi-paned window inserts at front elevation</li> <li>• Multi-paned window inserts on side and rear elevations in high visibility public view areas</li> <li>• Vinyl wrapped windows</li> </ul>	<ul style="list-style-type: none"> <li>• Bay windows</li> <li>• Header trim at front windows</li> <li>• Single hung windows at front</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>• Porches with simple wood columns and wood railings</li> <li>• Shaped wood columns, wood windows and/or door trim</li> <li>• Min. 2x4 window and door trim - wood on siding, foam on stucco</li> <li>• Shutters &amp; layered header trims at front elevation</li> <li>• Cupolas or dormers</li> </ul>	<ul style="list-style-type: none"> <li>• Surface mounted fixtures on front elevations must complement architectural style</li> <li>• Garage door patterns to complement style</li> <li>• Stone/brick/stucco chimney wrap</li> <li>• Pot shelves, louvered attic vents</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>• Field: Whites or light shades of blue, yellow, green, beige or gray</li> <li>• Trim: Whites or light shades complementary to field color</li> <li>• Accents: Medium to dark shades in contrast with field color</li> </ul>	

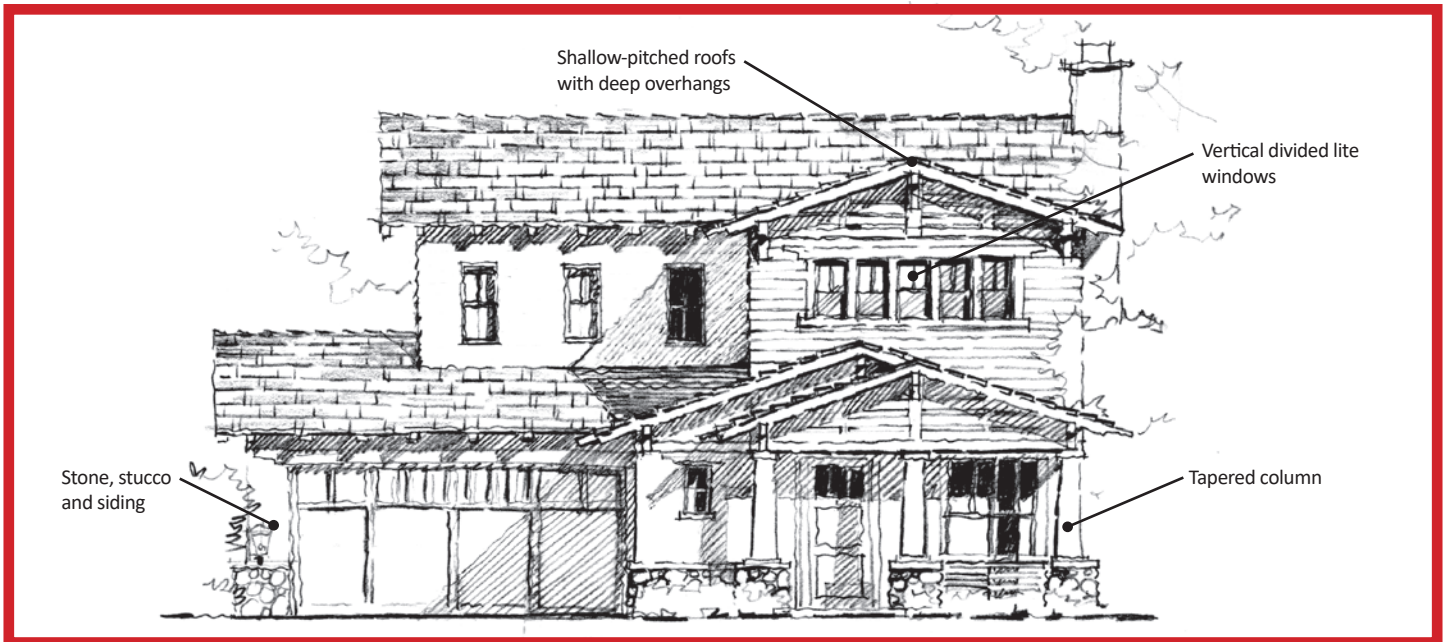


## Parkside

# Craftsman

The Craftsman style was inspired by the English Arts and Crafts Movement of the late 19th century. Of the utmost importance was that all exterior and interior elements received both tasteful and “artful” attention. The movement influenced numerous California architects such as Greene and Greene, and Bernard Maybeck.

The resulting Craftsman style responded with extensive built-in elements and by treating details such as windows or ceilings as if they were furniture. The style was further characterized by the rustic texture of building materials, broad overhangs with exposed rafter tails at the eaves and trellises over the porches. The overall affect was the creation of a natural, warm and livable home. In Southern California, the Craftsman style spun out of bungalows that were the production home of the time.







# Parkside



Front Porch with Battered Columns



Gabled Roof with Deep Overhang



Vertical Divided lite Windows

Element	Menu of Common Features	
<b>Form</b>	<ul style="list-style-type: none"> <li>Simple 2 story boxed massing with vertical and horizontal breaks</li> </ul>	<ul style="list-style-type: none"> <li>Varied plan shapes</li> </ul>
<b>Roof</b>	<ul style="list-style-type: none"> <li>4:12 to 6:12 roof pitch</li> <li>16" to 24" overhangs</li> <li>Flat concrete tile - shingle appearance</li> <li>Basic gabled roof - side to side with cross gables</li> </ul>	<ul style="list-style-type: none"> <li>Varied porch roofs - shed or gabled</li> <li>16" to 24" overhangs</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>Horizontal siding - may be combined with stucco. All material changes must occur on inside corner</li> <li>Stone base accents on walls and/or porch</li> <li>Wrapped horizontal siding elements on sides and/or rear elevation</li> </ul>	<ul style="list-style-type: none"> <li>Shingle siding used as an accent treatment</li> <li>Battered wall accents</li> </ul>
<b>Windows</b>	<ul style="list-style-type: none"> <li>Vertical multi-paned window inserts at front elevation</li> <li>Multi-paned window inserts on side and rear elevations in high visibility public view areas</li> <li>Vinyl wrapped windows</li> <li>Use windows individually or in groups</li> </ul>	<ul style="list-style-type: none"> <li>Single hung windows at front elevation</li> <li>Feature ribbon windows 3 or more</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>Entry porches with heavy square columns or posts on stone or brick piers</li> <li>Shaped wood header trim at windows and doors - wood on siding, foam on stucco</li> <li>Simple knee brace</li> <li>Surface mounted fixtures on front elevations must complement architectural style</li> <li>Garage door patterns to complement style</li> <li>Stone and brick base accents</li> <li>Open eave overhangs with shaped roof rafter tails</li> </ul>	<ul style="list-style-type: none"> <li>Full porches with heavy square columns or posts on stone piers</li> <li>Classically styled columns or battered columns</li> <li>Blended stone and brick chimney</li> <li>Wood trim at doors and windows</li> <li>Decorative ridge beams and purlins</li> <li>Triangulated knee braces</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>Field: Light to dark earth tones</li> <li>Trim: Light to dark earth tones in harmony or contrasting with field</li> <li>Accents: Light to dark earth tones in contrast with field</li> </ul>	



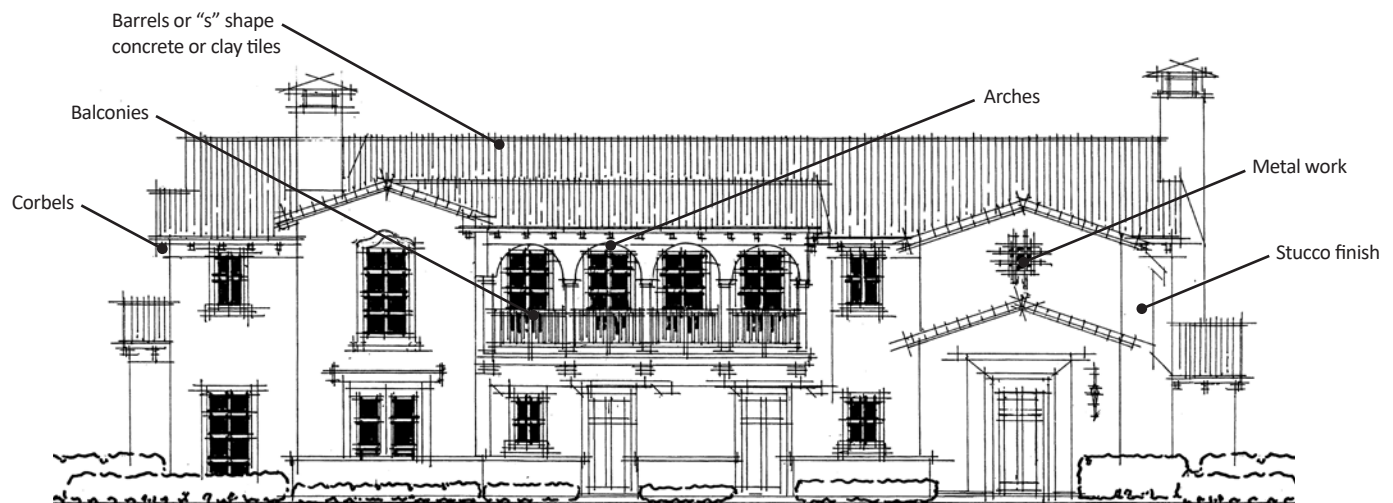
## Parkside

# Andalusian

The Andalusian style is an adaptation of the farmhouses and cottages found in the rural areas throughout Andalusia in **which is located in the South of** southern Spain. Enriched with details and elements derived from Moroccan, Moorish, and classical Roman architecture this style represents a simple, yet elegant blend of cultures.

The informality of rural life originally inspired a simple, rectangular form that evolved organically over time to meet the spatial needs of the owner. Subsequent massing was typically similar in form, but changed in dimension with new functions and arrangement of spaces. The resultant irregularity in massing and appearance adds to the charm and romance of this southern European style. Its informal rustic character is highlighted with sophisticated details, natural, indigenous materials, warm, sun-bleached colors and rich textures.

Architectural distinction is established through the use of details such as softly sculpted or curved wall and chimney articulation, colorful mosaics and tile work, rustic stone accents, balconies, perforated wall patterns and vents, cantilevered windows, classical elements, arches, and/or **wrought iron** metal grillwork accents.





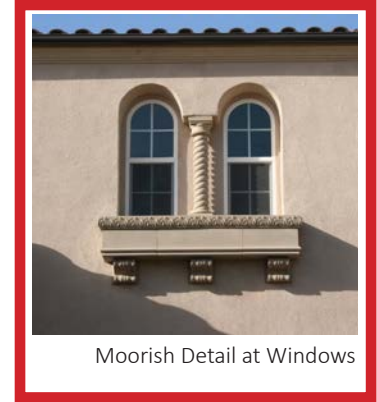
# Parkside



Feature Windows and Simple Massing



Vertical Windows with Metal Detail



Moorish Detail at Windows

Element	Menu of Common Features	
<b>Form</b>	<ul style="list-style-type: none"> <li>• 2 story massing with one story elements at courtyards/ entries</li> </ul>	<ul style="list-style-type: none"> <li>• Courtyard plan form</li> <li>• Recessed upper story elements</li> </ul>
<b>Roof</b>	<ul style="list-style-type: none"> <li>• 4:12 to 6:12 roof pitch</li> <li>• 12" to 16" - 18" overhang</li> <li>• Simple hip or gable roof forms</li> <li>• Barrel or 'S' shape concrete or clay tiles</li> </ul>	<ul style="list-style-type: none"> <li>• Intersecting gabled roofs</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>• Stucco finish</li> </ul>	<ul style="list-style-type: none"> <li>• Stucco - sand finish with rustic brick accents</li> </ul>
<b>Windows</b>	<ul style="list-style-type: none"> <li>• Vertical multi-paned window inserts at front elevations</li> <li>• Multi-paned window inserts on side and rear elevations in high visibility public view areas</li> <li>• Vinyl wrapped windows</li> <li>• Simple 2x window and door trim - wood or stucco over foam</li> </ul>	<ul style="list-style-type: none"> <li>• Feature recessed arched window</li> <li>• Accent beveled glass recessed window</li> <li>• Single or grouped round top windows</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>• Arcades supported by classic columns at courtyards, balconies and porches</li> <li>• Wrought Iron/Metal or wood balconies and accent details</li> <li>• Enhanced articulation at entrances and feature windows</li> <li>• Decorative ceramic tile accents</li> <li>• Wood or wrought iron/metal railings at balconies or porches</li> <li>• Garage door patterns to complement style</li> <li>• Shaped rafter tails at feature arches</li> </ul>	<ul style="list-style-type: none"> <li>• Balustrades at balconies or porches</li> <li>• Sculpted forms at facades, chimneys, feature windows, etc.</li> <li>• Tile vents in gable end</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>• Field: Sunwashed white tones or light tints</li> <li>• Trim: Dark brown, burnt red or ochre</li> <li>• Accents: Dark or medium shades of green, red, beige, blue using sparingly</li> </ul>	



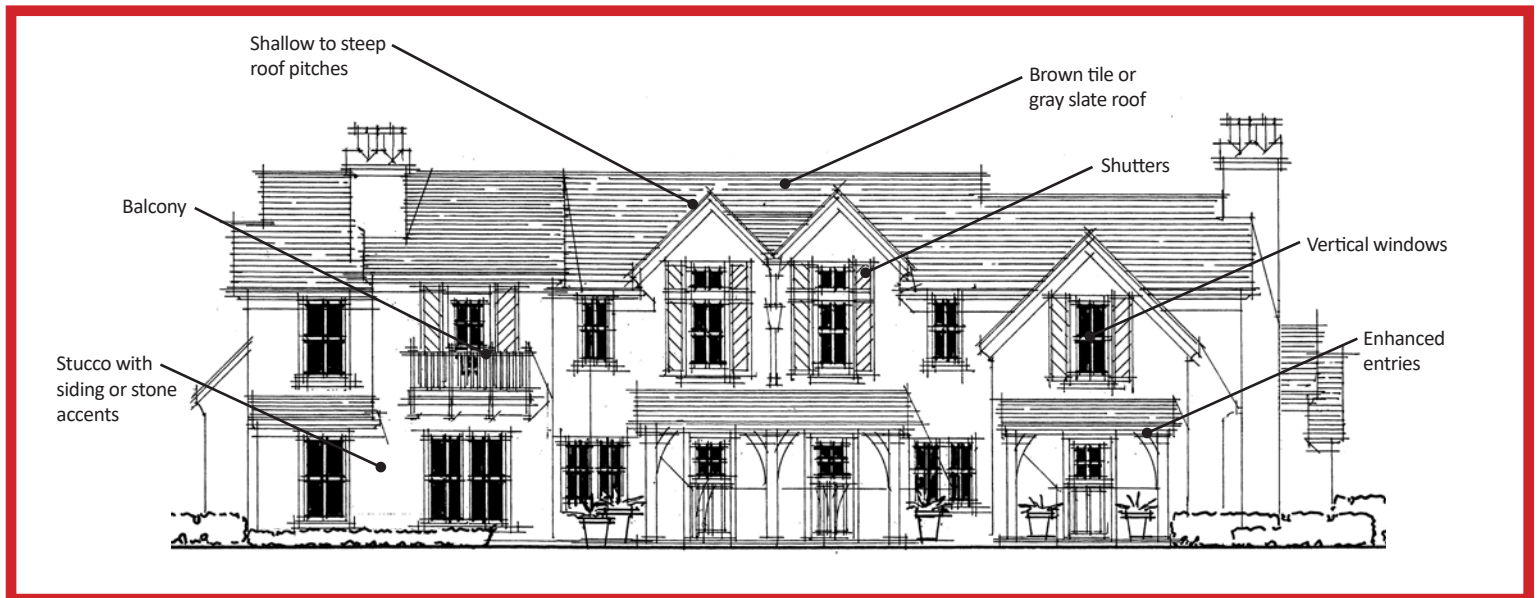
# Parkside

## Provence

The character of the Provence home could be classified as part farmhouse and part château. Its origins are in the simple rectangular farmhouses and cottages that exist in the rural hillsides and villages within the Provence region of France.

Looked upon as an enchanting style, graceful details such as stone lintels and surrounds at doorways and windows, curved or detailed shutters and asymmetrical accent elements capture the aging essence of the Provence style. The use of chiseled stones and enhanced entries are included in a palette of common features that are subtly placed on elevations.

Provence style roof pitches vary from shallow to steep. Rafter tail ends and wood or metal balconies are silhouetted against building facades, with an unassuming brown tile or gray slate roof.





# Parkside



6:12 to 12:12 Roof Pitch



Stone and Wood Accents



Steep Roof Accent

Element	Menu of Common Features	
<b>Form</b>	<ul style="list-style-type: none"> <li>Rectangular plan form massing with some recessed 2nd floor area</li> </ul>	<ul style="list-style-type: none"> <li>Distinctive plan form massing with <b>substantially</b> recessed 2nd floor</li> </ul>
<b>Roof</b>	<ul style="list-style-type: none"> <li>Main roof hip or gable with intersecting gable roofs</li> <li>4:12 to 6:12 roof pitch</li> <li>Shallow overhangs to 12" to 16"</li> <li>Architectural quality asphalt shingles or smooth flat tiles</li> </ul>	<ul style="list-style-type: none"> <li>Main roof hip or gable with intersecting gable roof or steep 2nd story roof breaking over 1st story elements</li> <li>Curved slope at roofline</li> <li>6:12 to 12:12 roof pitch</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>Stucco with siding or stone accents</li> </ul>	
<b>Windows</b>	<ul style="list-style-type: none"> <li>Vertical multi-divisioned window <b>inserts</b> at front elevation</li> <li>Multi-divisioned window <b>inserts</b> on side and rear elevations in high visibility public view areas</li> </ul>	<ul style="list-style-type: none"> <li>Curved or round top accent windows</li> <li>Bay windows</li> <li>Single hung windows at front</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>Entry accents with faux stone</li> <li>Garage door patterns to complement style</li> <li><b>Wrought Iron/Metal</b> or wood balconies and potshelves</li> <li>Shutters</li> <li>Simple <b>2x4</b> window and door trim - wood, stone, siding, or stucco <b>over foam</b></li> </ul>	<ul style="list-style-type: none"> <li><b>Decorative</b> shutters</li> <li>Stone veneer chimney or accented feature elements</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>Field: Pale earth tones</li> <li>Trim: Whites or light shades complementary to field color</li> <li>Accents: Light or dark shades in contrast with field color</li> </ul>	



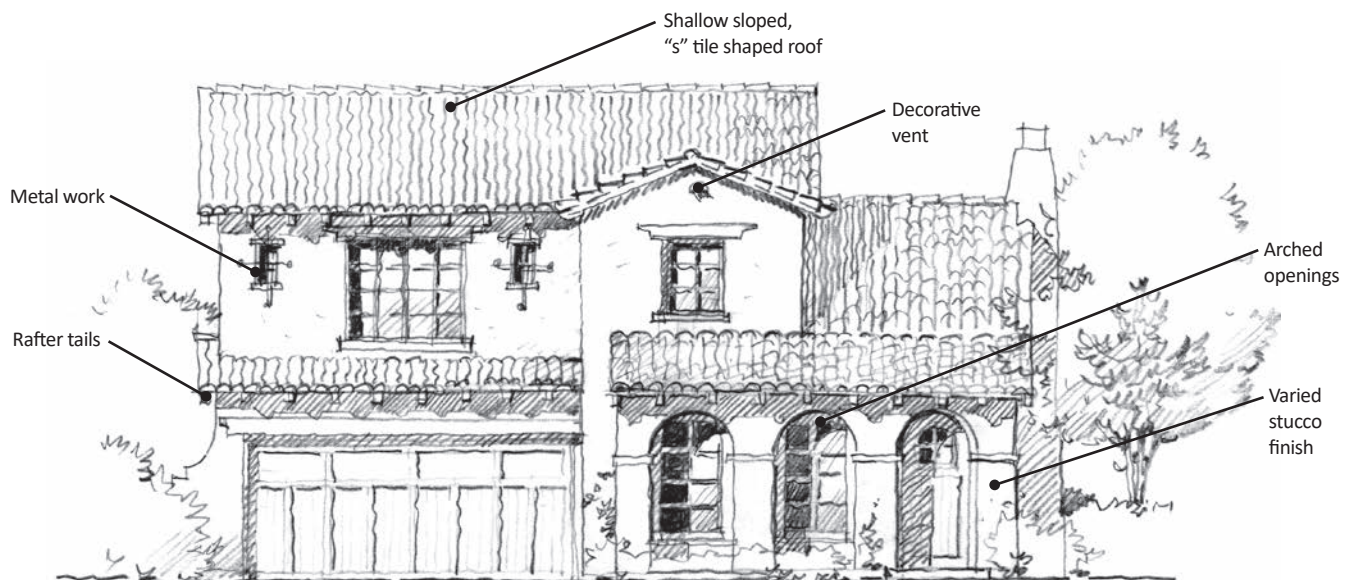
## Parkside

# Spanish Colonial

Spanish Colonial, also known as Spanish Eclectic is an adaptation of Mission Revival enriched with additional Latin American details and elements. The style attained widespread popularity after its use in the Panama-California Exposition of 1915-1916.

The simple courtyards of the Spanish Colonial heritage with hanging pots, a flowering garden and sprawling shade trees are hardly surpassed as foreground design elements. Further architectural distinction was established through the use of tile roofs, stucco walls, heavily textured wooden doors and highlighted ornamental ironwork.

Key features of this style were adapted to the Southern California locale. The plans were informally organized around a courtyard with the front elevation very simply articulated and detailed. The charm of this style lies in the directness, adaptability, and contrast of materials and textures.





# Parkside



Feature Window



Tile Roof, Gable Vent & Metal Accents



Stucco Walls with Arched Openings

Element	Menu of Common Features
<b>Form</b>	<ul style="list-style-type: none"> <li>• 2 story asymmetrical massing with strong one story element</li> <li>• Simply articulated 2 story boxed plan massing with not more than 50% one story element across front elevation</li> </ul>
<b>Roof</b>	<ul style="list-style-type: none"> <li>• 4:12 to 6:12 roof pitch</li> <li>• 12" to 16" overhang</li> <li>• Simple hip or gable roof with one intersecting gable roof</li> <li>• Barrel or 'S' shape concrete tiles</li> <li>• Shed roof over porch</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>• Stucco</li> <li>• Stucco - sand finish</li> </ul>
<b>Windows</b>	<ul style="list-style-type: none"> <li>• Vertical multi-paned window <b>inserts</b> at front elevations</li> <li>• Multi-paned window <b>inserts</b> on side and rear elevations in high visibility public view areas</li> <li>• Vinyl wrapped windows</li> <li>• Simple 2x4 window and door trim - wood or stucco over foam</li> <li>• Feature recessed arched window</li> <li>• Accent beveled glass recessed window</li> <li>• Single or grouped round top windows</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>• Stucco over foam window and door trim</li> <li>• Arched stucco column porches</li> <li>• Surface mounted fixtures on front elevations must complement architectural style</li> <li>• Garage door patterns to complement style</li> <li>• Shaped rafter tails at feature areas</li> <li>• Wrought Iron/Metal balconies and accent details</li> <li>• Entry door design to compliment style</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>• Field: Whites, beige, or warm tints</li> <li>• Trim: Dark shades to contrast field color</li> <li>• Accents: Deep tones of green, blue, red (on shutters, door, balcony trim)</li> </ul>

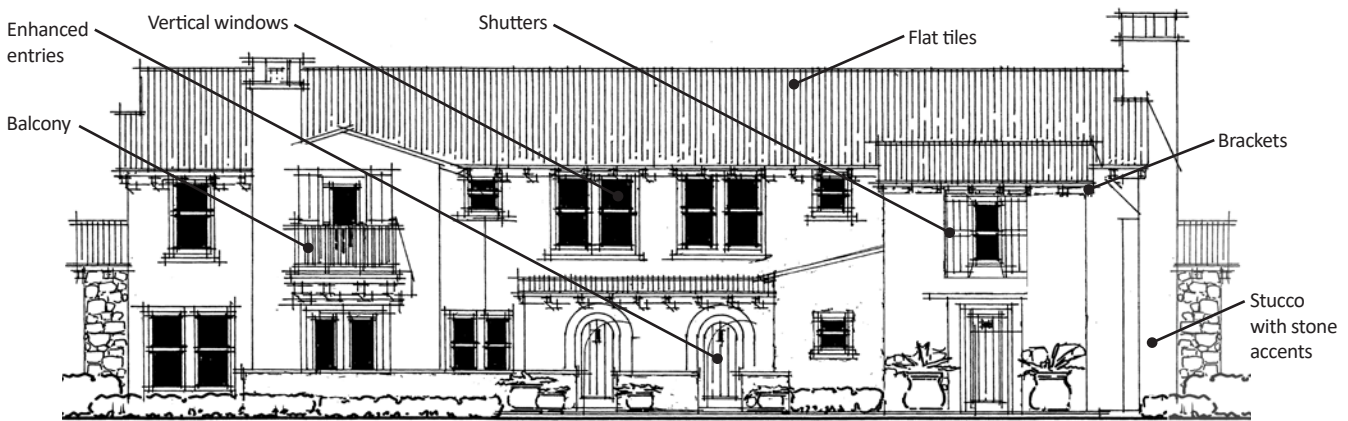


# Parkside

## Tuscan

During the 15th and 16th century large numbers of houses were built along roads and hillsides in rural Tuscany, many of which, although altered, are still in use today. As this region was and is primarily agricultural, these homes reflect the character of the farmhouse estate or 'podere'. Built by their owners with the indigenous materials and colors of the surroundings, these buildings blend naturally with the land.

The building form and massing is an example of simplicity; a plan that began as a simple rectangular form that evolved organically over time. Smaller components similar to the original form were typically added as necessary to meet the spatial needs of the owner. The resulting building, with the flexibility and variety apparent in this style, is what makes it so appealing. The informality of these rural farmhouse and settlement building types, including their traditional squared towers, eventually became the inspiration for Tuscan villas.







# Parkside



Recessed Second Floors Earth Tones



Rectangular Form, Stucco with Stone Accents



Enhanced Entry Articulation Vertical Multi-divided Windows

Element	Menu of Common Features	
<b>Form</b>	<ul style="list-style-type: none"> <li>Rectangular plan form massing with some recessed 2nd floor area</li> <li>Enhanced articulation and detail of entries, windows, doorways and balconies</li> </ul>	<ul style="list-style-type: none"> <li>Distinctive plan form massing with recessed 2nd floor</li> <li>Squared tower elements at entry or upper story</li> </ul>
<b>Roof</b>	<ul style="list-style-type: none"> <li>Main roof hip or gable with intersecting gable roofs</li> <li>4:12 to 6:12 roof pitch</li> <li>16" overhangs at eaves</li> <li>Architectural quality asphalt shingles or smooth flat tiles</li> </ul>	<ul style="list-style-type: none"> <li>Main roof hip or gable with intersecting gable roof or 2nd story roof breaking over 1st story elements</li> <li>Brackets under eaves</li> <li>Deeper overhangs to 24"</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>Stucco with stone veneer accents</li> </ul>	<ul style="list-style-type: none"> <li>Enhanced stone veneer siding</li> </ul>
<b>Windows</b>	<ul style="list-style-type: none"> <li>Vertical multi-divisioned window inserts at front elevation</li> <li>Multi-divisioned window inserts on side and rear elevations in high visibility public view areas</li> </ul>	<ul style="list-style-type: none"> <li>Curved or round top accent windows</li> <li>Single hung windows at front</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>Entry accents with faux stone</li> <li>Garage door patterns to complement style</li> <li>Shutters</li> <li>Balcony or veranda</li> <li>Simple 2x4 window and door trim - wood on stone or siding, stucco on stucco</li> </ul>	<ul style="list-style-type: none"> <li>Decorative shutters</li> <li>Stone veneer chimney and accented feature elements</li> <li>Wrought Iron/Metal or wood balconies and potshelves</li> <li>Rusticated hardware and details</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>Field: Medium, saturated earth tones</li> <li>Trim: Whites tones or light shades complementary to field color</li> <li>Accents: Light or dark shades in contrast with field color</li> </ul>	



## Parkside

# Cottage

Cottage is a picturesque style derived from medieval Norman and Tudor domestic architecture. The resulting English and French inspired “cottage” became extremely popular nationwide after the adoption of stone and brick veneer techniques in the 1920s.

Although the cottage is looked upon as small and not costly, it has been recognized as one of the most popular styles in suburban America. Design of the homes typically reflected in the rural setting in which they evolved. Many established neighborhoods in Riverside, Orange, and Santa Ana contain homes with the charm and character that depict the alluring, yet unpretentious lifestyle of the Cottage home.





# Parkside



Varying Roof Slopes & Forms



Feature Windows & Gable Accents



Stucco with Stone Accents

Element	Menu of Common Features	
<b>Form</b>	<ul style="list-style-type: none"> <li>Rectangular plan form massing with some recessed 2nd floor area</li> </ul>	<ul style="list-style-type: none"> <li>Irregular plan form massing with substantially recessed 2nd floor</li> </ul>
<b>Roof</b>	<ul style="list-style-type: none"> <li>Main roof hip or gable with intersecting gable roofs</li> <li>4:12 to 12:12 roof pitch</li> <li>Curved slope at roofline</li> <li>0" to 12" overhangs - (limit tight overhangs to distinguishing character elements)</li> <li>Architectural quality asphalt shingles or smooth flat tiles</li> </ul>	<ul style="list-style-type: none"> <li>Main roof hip or gable with intersecting gable roof or steep 2nd story roof breaking over 1st story elements</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>Stucco</li> <li>Stucco with wood siding or stone accents</li> </ul>	
<b>Windows</b>	<ul style="list-style-type: none"> <li>Vertical multi-paned window inserts at front elevation</li> <li>Multi-paned window inserts on side and rear elevations in high visibility public view areas</li> <li>Bay windows</li> </ul>	<ul style="list-style-type: none"> <li>Vinyl wrapped windows</li> <li>Curved or round top accent windows</li> <li>Single hung windows at front</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>Entry accents with natural or manufactured stone</li> <li>Shutters</li> <li>Simple wood 2x4 window and door trim - wood on stone or siding, foam on stucco</li> <li>Surface mounted fixtures on front elevations must complement architectural style</li> <li>Garage door patterns to complement style</li> </ul>	<ul style="list-style-type: none"> <li>Decorative shutters</li> <li>Stone veneer chimney</li> <li>Wrought Iron/Metal or wood balconies and wood pot shelves</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>Field: Soft, muted mid-tones of beige, tan, blue, yellow, grey or green</li> <li>Trim: Light or similar mid-shades complementary to field color</li> <li>Accents: Light or dark shades in contrast with field color</li> </ul>	



# Parkside

## Adaptive American Traditional

American Traditional is a style evolved in the early 20th century originating from the New England states, spreading westward, and becoming established throughout the rest of the country.

The adaptive version of this style retains the massing and form that is reminiscent of early American heritage but with simpler lines and contemporary details. The massing breaks down the façade from 2 to 1-story forms providing a natural balance to the front elevation. Roofs are simple in form most often with accent gables. This contemporized American style has a fresh appeal while retaining its charming origins.





# Parkside



Porch



Simplified Cornice Trim at Gable Ends

Element	Menu of Common Features
<b>Form</b>	<ul style="list-style-type: none"> <li>Asymmetrical massing with a vertical and a horizontal break</li> </ul>
<b>Roof</b>	<ul style="list-style-type: none"> <li>4:12 roof pitch</li> <li>12" to 16" overhangs</li> <li>Concrete roof tile- flat or shake appearance</li> <li>Asphalt shingles</li> <li>Front to back gable or hip roof with intersecting hip or gable roofs</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>Stucco</li> <li>Horizontal siding- may be combined with stucco</li> <li>Wrapped material on sides and terminate at logical end (return block wall/fence) or inside corner</li> </ul>
<b>Windows</b>	<ul style="list-style-type: none"> <li>Vertical multi-paned window inserts at front elevations</li> <li>Multi-paned window inserts on side and rear elevations at visible edges</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>Simplified cornice trim at gable ends</li> <li>Header window wood accent trim</li> <li>Simple 2 x 4 window and door trim- wood on siding, foam on stucco, 2 x 4 window and door trim may be used on specific product types, architectural styles subject to Planning Department approval.</li> <li>Surface mounted fixtures on front elevations must complement architectural style</li> <li>Porches and/or balconies with horizontal railing</li> <li>Front and garage door may have modern styling to complement style</li> <li>Square wood columns with trim</li> <li>Shutters, brick accents permitted</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>Field: Whites, off-white, dark or light colors</li> <li>Trim: White or contrasting with field color</li> <li>Accents: Light or dark colors in contrast or harmony with field colors</li> </ul>

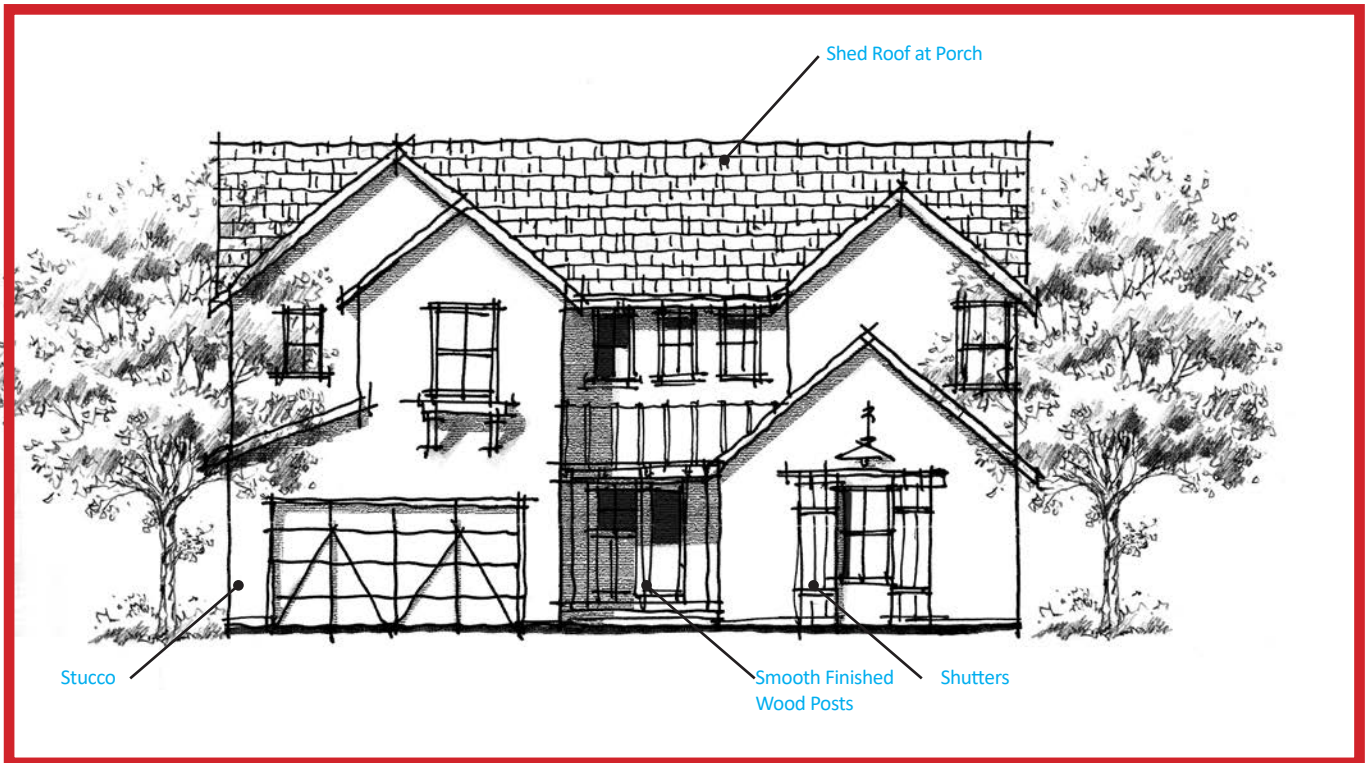


# Parkside

## Adaptive Farmhouse

Due to the simple and honest nature of this architectural form, the Farmhouse style can easily be adapted to a more abstract solution and is appropriate within many of the higher density settings. Since this style's roof shapes, variety of textures, colors and materials can easily be extrapolated to more contemporary applications, one can find many examples of this transformation throughout the West.

The Adaptive Farmhouse style blends the sleek clean lines of contemporary design with warm farmhouse charm to create a uniquely fresh take on this country living inspired style.





# Parkside



Board and Batten Siding



Proper Terminate Points for Siding  
and Color Accents

Element	Menu of Common Features
<b>Form</b>	<ul style="list-style-type: none"> <li>• Symmetrical or asymmetrical massing</li> </ul>
<b>Roofs</b>	<ul style="list-style-type: none"> <li>• Roof pitches 4:12 to 6:12, possibly lower pitch at main roof</li> <li>• Shed roofs at porches, <a href="#">may use metal seam</a></li> <li>• 0" - 12" overhang at eaves</li> <li>• Asphalt shingle roof or tile shake pattern</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>• Stucco</li> <li>• Board and batten <a href="#">or horizontal</a> siding</li> <li>• Siding should return on sides and terminate at logical end (return block wall/fence) or inside corner</li> </ul>
<b>Windows</b>	<ul style="list-style-type: none"> <li>• Vertically or <a href="#">horizontally</a> proportioned</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>• Smooth finished posts (6" x 6" min.) with smooth finish cap and base trim</li> <li>• Smooth finished beams</li> <li>• Door and window surrounds shall consist of one of the following materials:               <ul style="list-style-type: none"> <li>• Well-proportioned, stucco-wrapped, high density foam trim or</li> <li>• Smooth textured wood</li> </ul> </li> <li>• Raised panel-style entry door and garage door</li> <li>• <a href="#">Wood brackets below gables and/or rafter tails</a></li> <li>• <a href="#">Awnings, may use metal seam</a></li> <li>• <a href="#">Porches and/or balconies with horizontal railing</a></li> <li>• <a href="#">Shutters</a></li> <li>• <a href="#">Cementitious or stucco trim (with finish matching body of house) may be permitted at front doors/porch entries</a></li> <li>• <a href="#">Front and garage doors may have modern styling to complement style</a></li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>• Field: Whites or light tinted colors</li> <li>• Trim: Whites or light shades complementary to field color</li> <li>• Accents: Light or dark shades in contrast with field color</li> </ul>



# Parkside

## Adaptive Mediterranean

The Adaptive Mediterranean style draws inspiration from Southern European countries along the Mediterranean Sea. Italian, Spanish, Portuguese and Greek architecture lend their influence to this style.

Simplified details borrowed from these Mediterranean styles include streamlined forms which move toward a modern and unpretentious aesthetic – including low-pitched, hipped or parapet roofs combined with a lighter color palette.







# Parkside



Light or dark colors in contrast and in harmony with field colors



Limited ornamentation and no shutters

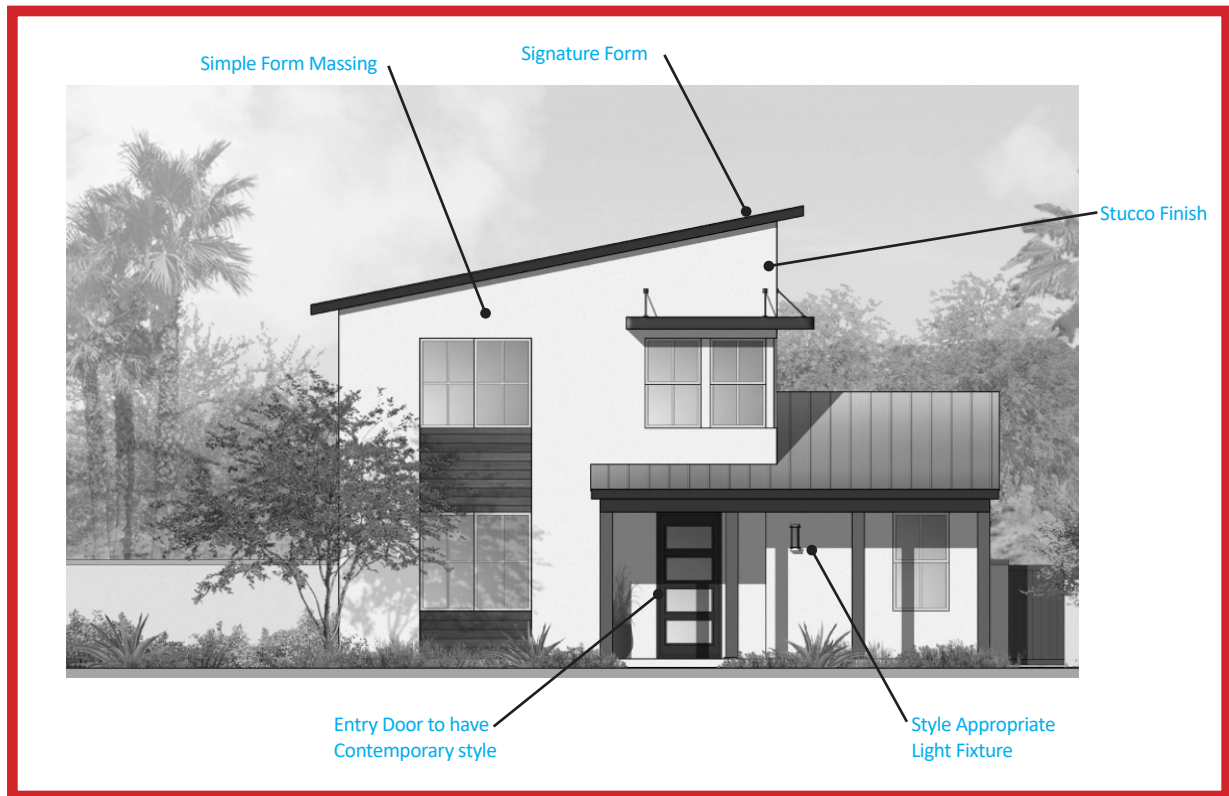
Element	Menu of Common Features
<b>Form</b>	<ul style="list-style-type: none"> <li>Simple geometric forms</li> </ul>
<b>Roofs</b>	<ul style="list-style-type: none"> <li>Hip or gable roofs</li> <li>Roof pitches 4:12 to 6:12 where applicable, possibly lower pitch at main roof; may have a shed roof accent</li> <li>0"-12" overhang</li> <li>S-shape, or flat tile, or asphalt shingle</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>Stucco</li> <li>Optional decorative ceramic tile</li> </ul>
<b>Windows</b>	<ul style="list-style-type: none"> <li>Vertically or horizontally proportioned windows or ganged windows</li> <li>Round top or arched primary window or architectural feature</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>Front entry doors without a porch, recessed from front facade</li> <li>Limited ornamentation and no shutters</li> <li>Decorative metal elements or ceramic tile</li> <li>Cementitious or stucco trim (with finish matching body of house) may be permitted at front doors/porch entries</li> <li>Style appropriate light fixtures</li> <li>Horizontal railings at porches or balconies</li> <li>Built-up eave</li> <li>Color Blocking</li> <li>Front door and garage door may have modern styling to complement style</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>Field: Toned whites and light to medium light value warm colors</li> <li>Trim: Medium dark value browns reminiscent of stained wood</li> <li>Accents: Light or dark colors in contrast or harmony with field colors</li> </ul>



# Parkside

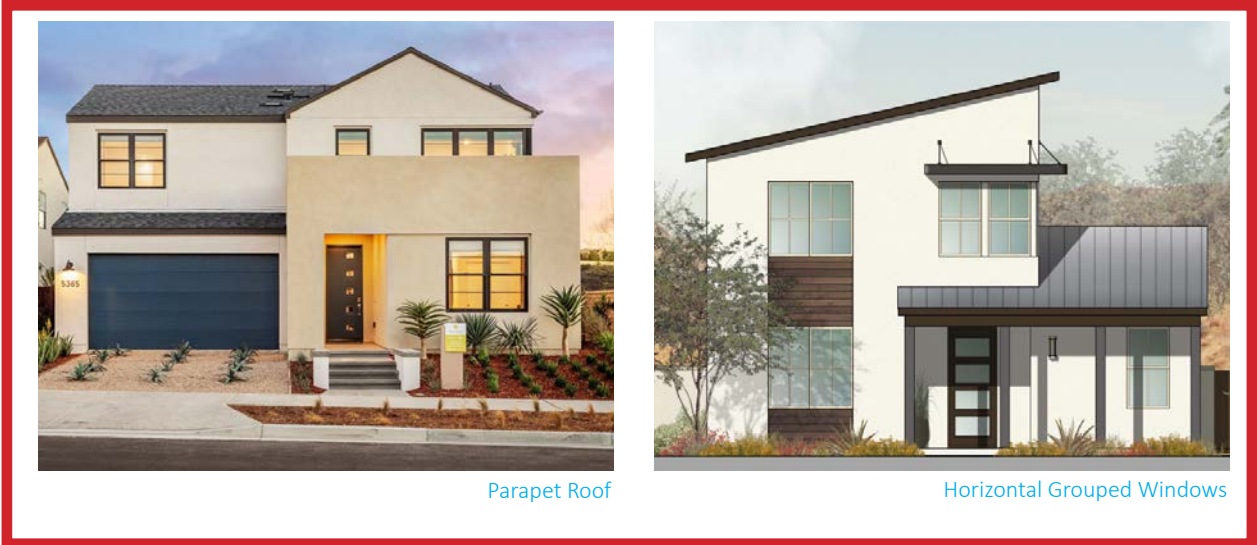
## Contemporary

Clean lines and ordered appearance. An architectural expression that is forward facing and suggests a separation from the old world.





# Parkside



Parapet Roof

Horizontal Grouped Windows

Element	Menu of Common Features
<b>Form</b>	<ul style="list-style-type: none"> <li>• Simple rectangular form massing</li> <li>• Signature form, detail or feature</li> </ul>
<b>Roofs</b>	<ul style="list-style-type: none"> <li>• Parapet, hip or shed roof</li> <li>• Simple unadorned parapet walls</li> <li>• Roof pitch range: flat or 4:12 to 6:12</li> <li>• 0" - 12" overhang at eaves</li> <li>• Shed element at entry with metal seam roof</li> <li>• Asphalt shingles</li> </ul>
<b>Walls</b>	<ul style="list-style-type: none"> <li>• Stucco finish</li> <li>• Cementitious siding</li> </ul>
<b>Windows</b>	<ul style="list-style-type: none"> <li>• Vertically or horizontally proportioned or grouped windows</li> </ul>
<b>Details</b>	<ul style="list-style-type: none"> <li>• Entry accents such as awnings with metal seam roof or metal eyebrows</li> <li>• Cementitious or stucco trim (with finish matching body of house) may be permitted at front doors/porch entries</li> <li>• Style appropriate light fixtures</li> <li>• Horizontal railings at porches or balconies</li> <li>• Front door and garage door may have modern styling to complement style and may include clear or frosted glass window details</li> <li>• Infill panels</li> </ul>
<b>Colors</b>	<ul style="list-style-type: none"> <li>• Field: Whites or light shades</li> <li>• Accents: Light or dark shades in contract with field color</li> </ul>



# Parkside

## 7.3 Massing (Single-Family)

### 7.3.1 Introduction

Creating streetscenes that function well and have visual interest is a primary community objective. The following basic elements and criteria **are intended to develop provide** variations in appearance and a sense of individuality for each home. Neighborhoods that have nearly identical homes and streets without variation in product placement and form will not be approved in design review. This section lists architectural massing and plotting techniques that will help to create a quality environment.

#### Objectives:

- Minimize visual impact of garages;
- Give attention to composition of building mass;
- **Incorporate single-story elements in two-story buildings;**
- Vary setbacks at porches, living and garage areas;
- Minimize 2- and 3-story dominance on **street (including lanes) scene, sidewalks** the streetscene **including lanes/alleys** and open spaces;

- Design with sensitivity to corner lot conditions;
- Use appropriate transition of scale; and
- Use four-sided elevation design.

In PAs that incorporate higher density single-family homes, high quality architecture is expected. Higher density homes call for simpler designs yet creative execution to reflect the Parkside vision. Higher density homes in correlation to the market demand, calls for innovative and more contemporary design. Therefore, the following objectives are encouraged:

- Minimize visual impact of garages;
- Give attention to composition of building mass;
- Where feasible, incorporate single-story elements;
- Design with sensitivity to prominent edge conditions and highly visible architecture;



Single-Story Elements



# Parkside

- Concentrate enhanced design features to visible edges, while creating simple but quality elevations on the interior plans.

## 7.3.2 General Elements

The general elements of building massing include:

- Front articulation;
- Roof form;
- Garage placement;
- Balconies and projections;
- Rear articulation;
- Variable setbacks; and
- Corner lots/critical edge conditions

## Neighborhood Massing and Plotting Concepts

To provide more interesting neighborhood streetscenes, varied massing on all elevations is encouraged.

Where possible when plotting, refrain from strict compliance to minimum garage setbacks to avoid contributing to repetitious streetscenes. Typically, plans are to be reversed and plotted so that garages and entries are adjacent to each other to create an undulating setback. Occasionally, this pattern should be broken to avoid monotony.

## Simple Home Design

In an attempt to design aesthetically pleasing architecture that is economically feasible and functions well, the concept of “The Simple House” is to be implemented. This suggests that starting with simple structural forms and building masses and appropriate roof forms can

lead to the development of an “authentic” style while maintaining acceptable building costs. In order to achieve this the following basic criteria have been established:

- Design “simple,” attractive homes using simple plan forms and style-appropriate massing and roof pitches;
- Use style-appropriate elements and details to enhance good architecture;
- Provide a variety of single-story heights and profiles while stepping back second-story massing when possible;
- Acknowledge both street front facades on corner homes or buildings by ‘wrapping’ usable spaces, architectural elements, and materials around to face the side street frontage;
- Give added attention to side and rear articulation if facades are publicly exposed;
- Use covered and shaded spaces such as porches, landscaped courtyards or structural landscape elements to provide a sense of entry and respond to local climate conditions; and
- Respond to overall community character with appropriate choices of style, color, and materials.

Refer to Sections 7.2, 7.3 and 7.4 for specific guidelines concerning styles, massing, elements, and details of simple residential architectural design.

## Single-Story Elements

Single-story elements [may be incorporated](#) on homes to help establish neighborhood pedestrian scale and add variety to the streetscene.



# Parkside

If product allows, a minimum of 33% of Encourage homes in each neighborhood should to have at least one significant single-story element on an exposed front or side elevation.

Single-story elements include:

- Front or wraparound porch;
- Roofed porte-cochere;
- Single-story living space in conjunction with a second story recess of at least 5 feet; and
- Pop-out gable element (enclosed or open).

This requirement may be satisfied with the addition of a one-story porch or other elements listed above.

Where forward garages are used single-story massing is encouraged.

Two (2) enhanced elements are required on all homes without porches (see individual style matrices, on pages 7-4 to 7-21, for appropriate “enhanced elements”).

## Planning Area 1-4

The goal of PA 1-4 is to provide single-family home-ownership opportunities to a greater span of the buyer population. Although single-story elements are desirable,

smaller home configurations may not lend themselves to this feature. However, where feasible, single-story elements are encouraged.

### 7.3.3 Front Articulation

The appearance of the streetscene and collection of homes within it is typically the first impression of any neighborhood. Most homes are seen primarily from the front and represent the transition from public to private space as well as the character of those who dwell within.

With this in mind, it is important that the front of the home, and any sides exposed to the street receive thoughtful design and articulation to bring character and quality to the individual home and to the neighborhood as a whole. The following guidelines will help to achieve that level of articulation:

- Design neighborhood plan packages with forms and front elevations in mind to work together in controlling the impact of buildings on the streetscene;
- Provide variety in form and massing to reflect interior uses and achieve attractive and interesting elevations;
- Design placement of windows to work with interior uses and provide “eyes on the street;”



Varied Massing on Elevation



Varied Roof Forms



# Parkside

- Consider the entry as a focal point when designing the front elevation; and
- Provide an appropriate level of detail and trim at entries, doors, and windows to enhance good design.

### 7.3.4 Roof Forms

Rows of homes seen from a distance or along arterial roads are perceived by their contrast against the skyline or background. The dominant impact is the shape of the building and roof line. Articulate the building mass and roof lines to express a variety of conditions and minimize the visual impact of repetitious flat planes, similar building silhouettes and similar ridge heights. This can be achieved by designing a discernibly different roof for each home plan. Individual roof plans may be simple but between different plans should exhibit variety by using front to rear, side-to-side, gables, and hipped roofs and/or by the introduction of one-story elements.

#### Planning Areas 1-4

Roof designs that maximize space for photovoltaic panels and are orientation for optimal solar efficiency are encouraged. ~~have less roof variation due to increased roof clearances and solar accessibility needed to meet CA Energy Code requirements. Therefore, homes found in PA 1-4 will exhibit roof variation along streetscenes where possible.~~

### 7.3.5 Garage Placement & Treatment

Create attractive and comfortable streetscenes and pedestrian spaces by designing lane-accessed garages.

#### Lanes/Private Alley Facing Garages

Although lanes and private alleys are thought of as merely

a transitional vehicular space, they also provide garage and sometimes direct residence access, trash collection and other daily utility uses. They may be experienced by homeowners as often as the front entries to these homes.

Therefore, when designing lanes and motorcourts consider both functional and aesthetic issues. It is the intent to provide a pleasant experience for daily users by incorporating both in the elements of lane design.

~~Lanes (private alleys) lined with homes and garages that have similar or identical facades, setbacks, roof lines, color schemes, and landscape treatments are~~



Varied Garage Door Appearance



# Parkside

monotonous and lack individual identity. This condition is not permitted.

Where feasible, in order to avoid the aforementioned condition minimize undesired streetscenes in lane/private alley conditions, the following requirements must be met-are suggestions:

- Avoid a multi-storied, “canyon-like” effect within the lane by providing varied massing, vertical and horizontal offsets of primary architectural elements and roof lines.
- Vary garage door appearance by using door patterns, colors, and windows appropriate to individual architectural styles;
- Where feasible, undulate the line of garage doors where space permits it. All drive apron and building separation setbacks shall be maintained (refer to Section 7.6, Home Types, for specific setback requirements) Where possible, vary ground floor building setbacks to provide undulation in the line of garage doors. All drive apron and building separation setbacks shall be maintained (refer to Section 7.6, Home Types, for specific setback requirements);
- Articulate building elevations with style-appropriate window trim, elements and details;
- Vary garage door appearance by using door patterns,

colors, and windows as appropriate to individual architectural styles; and

- For Conventional Compact Single Family Homes (access from local public/private street) shallow recessed garages shall be limited to 25% of the total umber of units in each builder’s plan package and shall be setback a minimum of 18 feet from property line.
- Design and locate rear privacy walls and pedestrian gate entries to provide ease of access to the rear yard, allow sufficient dimension for landscape requirements, and screen trash can storage from lane; and
- The number of homes with shallow recessed garages shall be limited to 25 percent of the total number of units in each builder’s plan package and shall be setback a minimum of 20 feet from the property line.

The impact of repetitive garages can be reduced by using the following techniques:

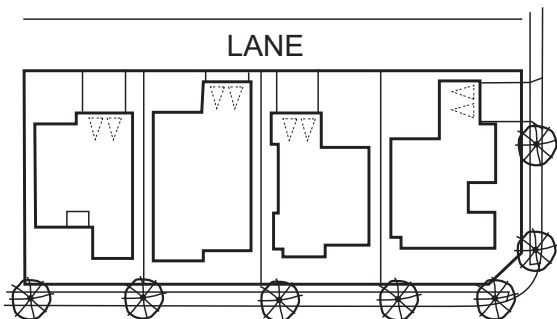
- Provide landscape planters (as feasible) within lanes to soften the appearance of the building elevations.

### Vary garage door appearance

Vary garage door pattern, windows and/or color as appropriate to individual architectural styles. Additional treatments are encouraged to buffer direct or monotonous views of the garage doors.

### Vary garage placement from plan to plan

Provide Where possible, provide varied garage placements in each lane-loaded neighborhood by reversing placement, varying horizontal massing, and offsetting or cantilevering upper-story massing.



Varied Garage Placement— Lane (Private Alley) Facing





# Parkside

This garage placement allows the option of entering from the side street, thereby eliminating the garage and driveway from the front or lane (private alley) side of the house.

### Optional Garage Screening Treatments

Garage screening treatments are encouraged to reduce or buffer the impact of garages and garage doors from the street or lane (private alley). **Install** These optional elements **such as** may include **vine pockets** or an attached trellis beneath a single-story garage roof fascia and/or above garage door header trims.

### 7.3.6 Balconies and Projections

The inclusion of balconies and projections **are encouraged** **may be used** for both aesthetic and practical purposes. They are useful in breaking up large wall planes, offsetting floors, providing shade, creating visual interest, and adding human scale to a building. Balconies also provide the practical advantage of extending living areas to the outdoors and providing elevated spaces and views.

- Balconies may be covered or open;
- They may be partially recessed into the mass of the building or serve as a projecting element ; **balconies and patios must adhere to the building to building setbacks;** and
- **Design** **Balconies should be designed** as integral elements of the building with details, eaves, supports, and railings **in keeping consistent** with the architectural style **and other elements of the building's design.**

Other architectural projections may include elements such as cantilevered massing, secondary roof changes, niches in plan, and bay windows.

### 7.3.7 ~~Articulation of All Elevations~~ Elevation Enhancement

The second-story portion of **all** elevations **of homes that are adjacent to facing public** roads, parks, multi-purpose trails, paseos, or public open space, must include a variety of window treatments, roof projections, **etc.** **Where these conditions of visibility exist, a** **to provide**



Cottage



Farmhouse



# Parkside

an appearance similar in quality to front elevations. A variety of hipped and gabled roof forms must be used in each neighborhood. One-story elements also help to provide this variety.

### 7.3.8 Varied Massing

Provide varied massing and articulated elevations **keeping consistent** with the selected style and product type.

### Floor Plan Form

Keep architectural styles in mind when designing the floor plan. For example, styles such as Cottage or Farmhouse that reflect asymmetrical massing and steeper roof pitches may be applied to a similar floor plan and elevation while both maintain an authentic appearance.

### 7.3.9 Corner Lots/Critical Edge Conditions

Neighborhood quality will be enhanced by adding a home plan designed specifically for the corner, or by enhancing an interior lot plan for use on the corner with additional architectural elements and/or details as found on the front elevation.

~~All corner homes (Conventional, Lane-Loaded, and Cluster) shall wrap materials (where applicable to the style/front elevation) and continue articulation around to the corner side facade.~~

All corner homes must include wrapping materials and continued articulation around to the side facade. All material changes must occur on an inside corner, on a logical end, or coincide with an architectural element that conceals the material change.

### Corner Plans

~~At least two (2) plans per neighborhood~~ Conventional and Lane-Loaded residential product neighborhoods should be designed **for optional use on** with enhanced corner lot treatments. One of these plans ~~shall~~ may include an architectural element that encompasses an interior room or living space that ~~wraps around or~~ occupies the corner side yard.

- A second plan (primarily for conventional product) should include ~~a single-story~~ an element that helps to articulate the side facade such as:



Wrapping Materials



Corner Plan



Corner Plan with Porch



# Parkside

- A wraparound porch;
- Pop-out, side gable;
- Significant wall plane offset; or
- Appropriately detailed second-story pop-out.

These elements will also provide logical stopping points/less conspicuous inside corners for wrapping materials. **Corner plans should also consider use of the following elements:**

- ~~Corner plans should also consider use of a~~ A corner-related front entry door placement; and
- A garage placement that allows entry from the side street or lane.

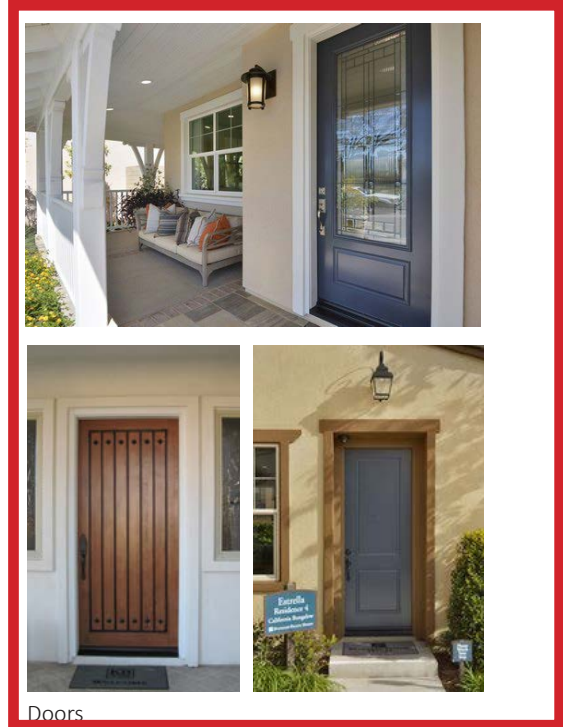
~~Where applicable, Cluster Home products should be designed appropriately to address the front and corner side exposure.~~

- A second plan should include an element that helps articulate the side facade such as:
  - A wraparound porch;
  - Pop-out, side gable;
  - Significant wall plane offset;
  - Appropriately detailed second-story pop-out; or
  - A single-story element.

## Planning Areas 1-4

PA 1-4 will offer progressive housing types geared toward an attainable price-point. Thus, building footprints will be more compact and efficient, leaving less room for considerable architectural pop-outs and features. However, corner enhancements are still encouraged with the following recommendations:

- Front elevation enhancements shall wrap onto the side elevation on all homes where the side elevation faces a loop road, are street side or a corner elevation.



Doors



Windows



# Parkside



Rakes and Eaves

- Where feasible, an additional 5-feet of landscape setback shall be provided along the homes fronting the loop roads and the Great Park.

## Community Recreation and Common Facilities

Private common recreation facilities may include a pool, spa, clubhouse, barbecue areas, or other facilities as appropriate to the community. Common facilities act as key character elements **in these neighborhoods** therefore the following should be considered when designing for such facilities.



- Clubhouse and other common area buildings should exhibit a high level of quality and attention to detail on all visible sides of the building;
- All architectural and community elements, such as street furnishings, benches, and lighting standards should be consistent with the selected overall architectural character for the community; and
- Colors, massing, roof pitch, and materials of surrounding residential buildings should also be considered in conjunction with the design of the common facility.



Lighting and Fixtures

## 7.4 Materials and Details

### 7.4.1 Introduction

Although the character and style of homes should be established primarily through the use of massing, roof forms and key architectural elements, the addition of appropriate secondary character elements, detailing, and accent materials provides an effective means of further reinforcing and enhancing character and style interpretations.

Key elements are those primary architectural features of the home that clearly discern one style from another. Secondary elements and details are identified as those that help to reinforce or enhance an already identifiable style. A list of appropriate elements, colors and materials



# Parkside

for each individual style is found in the requirements matrices in the “Styles” section of this document.

The key or primary elements are:

- Doors and Entries;
- Windows;
- Rakes and Eaves;
- Exterior Lighting Fixtures;
- Roof Materials;
- Colors;
- Accent Materials; and
- Wall Finishes.

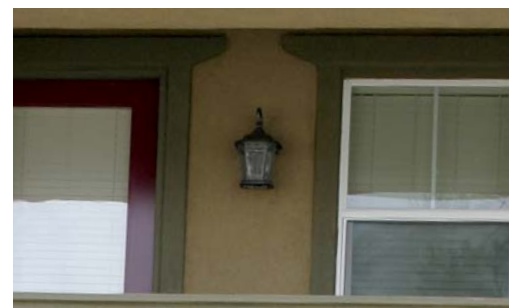
## 7.4.2 Doors and Entries

Building entries represent an important point of interface between public and private spaces typically creating the initial impression for the user and a focal point for the viewer. Front doors and associated entry spaces should ~~thereby~~ be treated with a sense of importance and detailed ~~so as~~ to emphasize their location and purpose. In designing the door and entry space of homes the following guidelines shall be observed:

- Design of door and entry spaces shall be consistent with the architectural style of the home;
- Doors shall be protected by a recess, porch or other covered element; and
- Door and entry spaces shall reflect a level of detail appropriate to their function and architectural style.
- ~~Surface mounted lights shall not be permitted in garage door soffits; and~~



Roof Materials



Accent Materials



# Parkside



Finishes

### 7.4.3 Windows

The following guidelines shall be observed for windows:

- Proportions and alignment shall be appropriate to individual style;
- No highly reflective glazing shall be used;
- Windows may be vinyl-wrapped or wood;
- When using inset windows, provide minimum 2-inch depth;
- Window placement shall be designed to enhance privacy between neighbors;
- Full window trim is encouraged to be a minimum of twelve (12) inches in depth on street facing elevations;

- Shutters shall be sized to match window width and exhibit style-appropriate grates hardware; and
- Where appropriate accent features such as shutters and tile surrounds are encouraged. ;and
- Avoid direct alignment of windows between homes to ensure privacy.

### 7.4.4 Rakes and Eaves

As with other architectural elements, roof edge treatments vary with the character of each style. Rakes and eaves shall reflect that of each individual style and contribute to the overall look and feel of the community.

- Elements and proportions of rakes and eaves shall be appropriate to the individual style;
- Where appropriate to individual style, larger eave overhangs are encouraged to provide opportunities for shading and relief; and
- Rafter tails, when exposed, shall be a minimum of four (4) inches and proportionate to building painted or stained.



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## 7.4.5 Exterior Lighting and Fixtures

In the neighborhoods of the Specific Plan Emphasis shall be placed on lighting for safety, security, and aesthetics. Street poles and wall fixtures shall be used to provide sufficient light for traversal and visibility thereby providing the community with a comfortable level of safety and security at night. Every attempt should be made to reduce ambient overflowing light on adjacent homes and to increase the amount of light in less visible areas.

Opportunities to use lighting for directional and aesthetic reasons should also be considered. Where fixtures are not an important focal point, conceal light sources and concentrate on achieving the maximum effects of good lighting design. Where fixtures are part of architectural detailing they should reflect the selected style.

- Lighting used on walls and walkways shall focus light down and provide appropriate down-casting hardware to minimize glare;
- Ambient light shall be down-cast to reduce the impact on the neighborhood;
- Surface mounted lights shall not be permitted in garage door soffits; and
- Wall mounted lighting fixtures shall be selected according to the individual style of the home or building.

## 7.4.6 Roof Materials

Roof materials, colors, and treatments should correspond to the individual character or style of the home or building and be compatible with the overall look of the neighborhood. Style appropriate materials and colors are found in each appropriate matrix in the “Architectural

Styles” section of this document.

- Where feasible, a variety of roof plans, forms and ridge heights shall be provided using hips, gables, and intersecting elements to avoid a repetitious skyline;
- High quality composition, concrete, or clay tiles may be used in conjunction with the style of home; and
- Skylights are prohibited on sloped roofs facing public streets.

## 7.4.7 Colors

Varied palettes of regional and style-appropriate colors and materials have been selected for the homes in the Parkside Specific Plan area. This inherently rich and diverse palette will provide the variety required for visual interest while providing a sense of unity within the neighborhoods. Style-appropriate color selections will enhance the architectural character and impart individuality to each home.

- Color shall contribute to distinguishing the overall architectural character of the dwelling. Refer to “Architectural Styles” section of this document matrices for individual color palettes;
- Hue variation in adjacent homes shall be provided to create diversity within the neighborhood;
- Diversity of color is encouraged; and
- Roof tile colors will be selected according to architectural styles.



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To further this goal of diversity, the following criteria shall be met:

- Elevation/color schemes shall consider the composition of colors, appropriately combining colors for an aesthetic and varied streetscape. Consider field color, trim color, accent colors, and accent materials colors.
- Individual color schemes must be appropriate to the architectural styles with a harmonious selection of accent materials, roof profiles and colors;
- No adjacent or facing home shall have the same color scheme; and
- No more than 4 of the same floor plan shall be plotted adjacent to one another.
- Architectural style of homes may be identical within individual courts/lanes.

## 7.4.8 Accent Materials

Accent materials ~~should~~ can be used to enhance and reinforce the architectural style and composition of individual homes and provide variety in the streetscene. Selective use of appropriate materials, color, and placement can provide maximum impact while imparting a sense of unique character to each home.

- ~~Culmination of accent materials shall terminate at inside corners or coincide with an edge or architectural element to conceal changes in material. Where views are limited or edges concealed by an architectural element, accent materials may terminate at privacy wall conditions;~~

- All color and material changes shall occur at inside corners or other logical locations to provide natural transitions from one aesthetic to another;
- Natural stone, approved manufactured or cultured stone, painted or natural brick, precast concrete, ceramic tile, slump block, and horizontal or vertical siding or approved manufactured siding, (i.e. cementitious board) are encouraged;
- Accent materials shall be selected to complement the overall color and style of the home or building; and
- ~~Architectural~~ Where incorporated, architectural trim applied to all elevations is encouraged to shall be sized properly and consistent with front elevation and architectural style.

## 7.4.9 Wall Finishes

Style-appropriate wall finishes should visually enhance individual homes. The use and placement of extreme contrasts in color and/or materials should be carefully considered in order to maintain a compatible overall look throughout the neighborhood. Footings shall be exposed no higher than 6 inches above finished grade.

### Permitted Finishes:

- Stucco-light to medium sand finish (~~“Spanish Lace” finish is prohibited~~);
- Exposed masonry walls (brick, slump block, split face, etc.);
- Stone, brick, adobe, brick veneers (accent materials);
- Horizontal plank siding; and
- Board and batten siding.





# Parkside

## **7.5 Attached and Multi-Family Guidelines**

It is the intent for all architecture in Parkside to achieve a high level of quality in building function and visual appearance, assure variety and compatibility in architectural character and to enhance the community's overall value. The goal is to promote these qualities in conjunction with the landscape and planning by using heritage styles combined with modern technology and architectural innovation to provide a pleasant, livable community.

By the very nature of the building types, attached and multi-family home neighborhoods are much like small villages, or communities. Each should be designed for compatibility within itself, using a blend of compatible architectural styles and a tastefully balanced palette of colors and materials.

The following general concepts should be considered when planning for and designing multi-family housing. See the following section for added criteria that is specific to each building type.

- Design and site buildings with a strong physical relationship to public areas of the community.
- Emphasize pedestrian access and connections to public sidewalks, paseos, and open space systems when preparing site plans.



# Parkside

## 7.5.1 Architectural Inspiration - Themes and Styles

One key to ~~this community's~~ success is ~~of this community is an appropriate overall theme and the choice of supporting compatible~~ architectural styles. ~~Each multi-family planning area in Parkside is related to a private recreation area or the "Great Park."~~ Appropriate styles, in a simplified palette, include selected Mediterranean styles of Europe ~~including and Each of these community areas will reflect an architectural and landscape environment that expresses the specific character of that region with the entire community being linked by landscape elements and plantings common to all of these regions:~~

~~An important goal in Parkside is also to develop a strong community character and varied and interesting street scenes. In order to achieve this, an architectural palette has been selected that represents those styles found in each regional area of Mediterranean Europe styles that adapted to the Southern California climate.~~

The selected architectural ~~style regions for Parkside are~~ styles for high density attached units include:

### Mediterranean Heritage Styles

- Andalusian
- Provence
- Spanish Colonial
- Tuscan

~~Each multi-family planning area is designated for a selected primary style that will be reflected in the recreation facilities and parks and most of the architecture for that area. A secondary, different style will also be selected for use in that area by proximity to other styles; using only the style which is in the adjacent or closest planning area.~~

### Millennium Styles

- Adaptive American Traditional
- Adaptive Spanish
- Adaptive Farmhouse
- Adaptive Mediterranean
- European Heritage
- Contemporary

### Plan and Style Requirements for Attached Homes:

- Provide at least 2 building plans per neighborhood (75-175 units), with the ability to reverse plot plans and vary placement or add elements to corner units;
- Provide a minimum of 2 individual unit plans per building. Individual unit plans may be repeated between building plans;
- Provide one or more styles per community as designated on the "Neighborhood Character" exhibit in this document; and
- Provide at least 3 different, yet compatible color schemes for each neighborhood.



# Triplex

# Townhomes

Exhibit 7-1: Plotting and Massing



# Parkside

## Materials and Colors

Individual elevation or building colors should be selected from the community color palettes selected for that style. See individual style matrices for style-appropriate color palettes.

## 7.5.2 Architectural Plotting & Massing Concepts

### Form, Mass & Height

In the case of attached and multi-family buildings there is a tendency to “build out” to the maximum building envelope without articulated treatment on wall planes. This is to be avoided **as the intent is** to prevent the final product from becoming a multi-story stucco box, without vertical or horizontal relief.

- Minimize building “ends” and blank, singular planes oriented toward public views. Provide some architectural elements on all sides of buildings;
- Consider intended styles in conjunction with the development of building plans, massing forms, elements, details, and color;
- Carefully consider the building massing, details, and color, in conjunction with the architectural character for the neighborhood or community;
- Design buildings to define outdoor spaces, with floor plans that have logical and functional relationships between indoor and outdoor spaces; and
- Buildings shall not exceed height standards. (Refer to Section 7.6 for height restrictions).

## Shade and Shadow - Building Relief

- Project a front door image and access to the street front. Articulation of end unit elevations is required to achieve four-sided articulation;
- Place and access garages at rear alleys to separate pedestrian from vehicular areas;
- Provide front porches where style-appropriate and possible for stepped massing and transition to public spaces;
- Vary setbacks on building elements/facades; and
- Vary roof pitches and directions.

## Spaces—Single-Story Elements

**Single story elements are encouraged on all buildings to establish pedestrian scale and add variety to the street scene. This is especially important on larger buildings where inadequate articulation of mass tends to produce monotonous, single planes.**

**On attached buildings, there are multiple opportunities for single-story elements such as interior living spaces, porches and entries. The addition of such elements helps to provide a transitional space between public and private space.**

## Entries

Entries should create an initial impression, locate and frame the doorway, act as an interface between public and private spaces, and further identify individual unit entries.



# Parkside

- Wherever possible, site plans should orient the front door image and principal access toward the public street or entry courtyard;
- Incorporate appropriate roof elements, columns, feature windows and/or architectural forms in the entry statement to emphasize the building character and the location of individual doorways; and
- If front entry location is not immediately obvious due to building configuration, direct and draw the observer to it with added elements such as signs, lighting and landscape.



## **Windows**

Typically the location of windows is determined by the practical considerations of room layout, furniture placement, views, and privacy. Design emphasis here should be of particular concern as windows play an important role in the exterior architectural character of multi-family buildings.



- Within the appropriate style requirements, group and coordinate windows with other design elements to create a sense of composition and order;
- Where appropriate to style and window form, the use of multiple window panes is encouraged; and
- Use appropriate scale and proportion in window design to enhance the elevation style, using shutters, trim, etc. to help convey character.



## **Balconies**

The inclusion of balconies is encouraged for both aesthetic and practical purposes. They are useful in breaking up large wall planes, offsetting floors, creating visual interest, and adding human scale to the building. They provide the practical advantage of creating outdoor living areas and elevated open space.

- Balconies may be covered or open. They may be either recessed into the mass of the building or serve as a projecting element. Balconies and patios must adhere to required setbacks;



Townhome Product Lane Streetscape



# Parkside

- Design balconies as an integral elements of the building with details, eaves, supports, and railings in keeping with the architectural style and other elements of the building’s design; and
- Avoid designing plans with repetitive balconies that occur side by side.

## **Parking**

In the case of attached buildings adjacent to primary streets, the focus of these buildings should be their street front image and pedestrian access. Each project will incorporate interior oriented parking solutions and use the following design techniques to enhance the architecture of the streetscene:

- Distribute parking on-site to provide close proximity as possible to individual units;
- Group unassigned or guest parking in convenient locations;
- Refer to Division 6.03 (Off Street Parking and Loading) of the Ontario Development Code for [parking requirements](#). [Any parking standards that differ from the Ontario Development Code can be found in the development standards \(refer to the tables in Section 7.6: Home Types\)](#).
- [A parking analysis must be submitted to the City as part of the Development Permit process:](#)

## **Community Recreation and Common Facilities**

Each neighborhood planning area is focused around (or is within a short walk from) the Great Park or common recreation facility. These may contain pools, spas, recreation [buildings structures](#), barbecues and/ or open space. Each is planned as appropriate to the demographic profiles identified for the community by the master developer.

- Recreation facilities are key character elements for the community;
- All architectural and community elements, such as street furnishings, benches, lighting standards, and trash receptacles, shall be consistent with the selected overall architectural character for the community; and
- Colors, massing, roof pitch, and materials shall be compatible with residential buildings or project theme.

## **Trash Enclosures**

Enclosures shall be provided to accommodate the numbers and types of trash containers as required by the disposal company. These enclosures shall be positioned in a centrally convenient area for residents. Try to minimize impact on adjacent residences and neighborhood developments by keeping enclosures



# Parkside

from the edges of the community.

- Trash enclosures shall be substantially constructed in a style and decorative wall finish that is consistent with the overall architectural character of the development.
- All trash enclosures shall be equipped with style complementary gates of durable construction, hinged to self-supporting steel posts.
- Lighted pedestrian access is required at each enclosure.
- Trash enclosures shall be placed in a location which does not interfere with on-site circulation.

As consistent with Section 5.7 of this Specific Plan, all project sites shall be designed to meet all the Integrated Waste Department's requirements, including the requirements for Sizing of Storage, Location of Collection Area, Accessibility for Collection Vehicles, and Collection of Sorted/Diverted Waste Types.

## **Garage Placement & Treatment/Lanes**

Provide rear or side-accessed garages for lane-loaded, attached buildings. Tandem garages may be used to accommodate parking requirements. Vary plans where possible, placing garages at different locations relative to the lane. Roll-up garage doors with garage door openers are required on all homes.

## **Lanes (Private Alleys)**

All lanes (private alleys) shall be designed to include landscaping to soften architectural massing. Architectural treatments such as varied massing, color variations, decorative paving, trellises, and window treatments

shall be designed to add interest to building facades.

~~Additionally, drainage swales shall not be constructed within the center of the lanes. The swales shall be constructed along the perimeter of the lanes.~~

## **7.6 Home Types**

### **7.6.1 Introduction**

A primary design objective for the Specific Plan is to provide a variety of home sizes and plans to suit the needs of different life stages and market segments. Parkside allows for individual home-ownership opportunities in a higher density more urban setting than typical neighborhoods. Some of the three-story detached homes may be located on fee simple lots or they may in a one-lot condominium subdivision. All homes will have fire separation distances as required by the California Residential Code that may be as minimal as a few inches.

The following section provides conceptual plotting examples for a variety of residential housing types, arrangements and lot size ranges. The plotting concepts are provided only to illustrate a typical layout of each building housing type. They are not intended to be mandated layouts.

### **7.6.2 Home Type Criteria**

The following are design standards and minimum setbacks by typical residential product and housing type.



# Parkside

## Conventionally Loaded Lane Homes

(PA 1-4)



### Characteristics

- Single-family small lot detached dwellings face street or lanes (private alleys);
- Automobile access via streets or lanes (private alleys);
- Primary entries and walks face streets or lanes (private alleys);
- Private yards/patios;
- Reciprocal use easements may extend private spaces on sides of buildings;
- Parking for residents provided in garages and driveways; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

### Notes

- Actual acreages, lot sizes, unit counts and densities **by typical neighborhood building type** may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
- Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.





# Parkside

## Conventionally Loaded Lane Homes - Product 2

(PA 1 & 4)

Note: Auto Court and lane-loaded Private Lanes shall be enhanced with a ribbon of either colored concrete, stamped concrete, pavers or other similar decorative materials.



Typical Product Plotting Concept  
2-Story Diagram

Table 7-1: Conventionally Loaded Lane Homes Development Standards

### Building Separation

Front to side living space	8'
Side to side living space	8'
Side to side living space (with parking space in between)	11'
Rear living to rear living	8'

### Front Setbacks (Private Street Facing) - To Back of Sidewalk

Living space	5'
Porch, balcony	3'
Garden wall	3'
Front setbacks (Private Lane facing)	3'

### Side Setbacks - To Parcel Edge/Site Boundary

Living space to property line	10'
Non-living space (garage-single story)	6'
Living space (adjacent to Cucumonga Creek Channel)	4'

### Rear Setbacks - To Parcel Edge/Site Boundary

Living space (2-story)	10'
Living space (3-story)	10'

### Garage Setbacks

Garage face to garage face	30'
Garage face to lane (drive apron)	3'
Drive aprons shall be 5' or less in length or 18' in length or greater.	

### Maximum Encroachments into Building Setbacks

Second/third floor cantilever over garage	3'
Fireplace, media niche (8' max. length), bay windows, furr outs, etc.	2'
Potshelves, brackets, eaves, awnings, eyebrows, canopies, cantilever decks, etc.	Additional 36" over porch or furring setback

### Private Open Space Per Unit

Minimum dimension	4'
Total per unit	70 s.f.

### Parking

2 spaces per homes, at least 1 is required to be covered

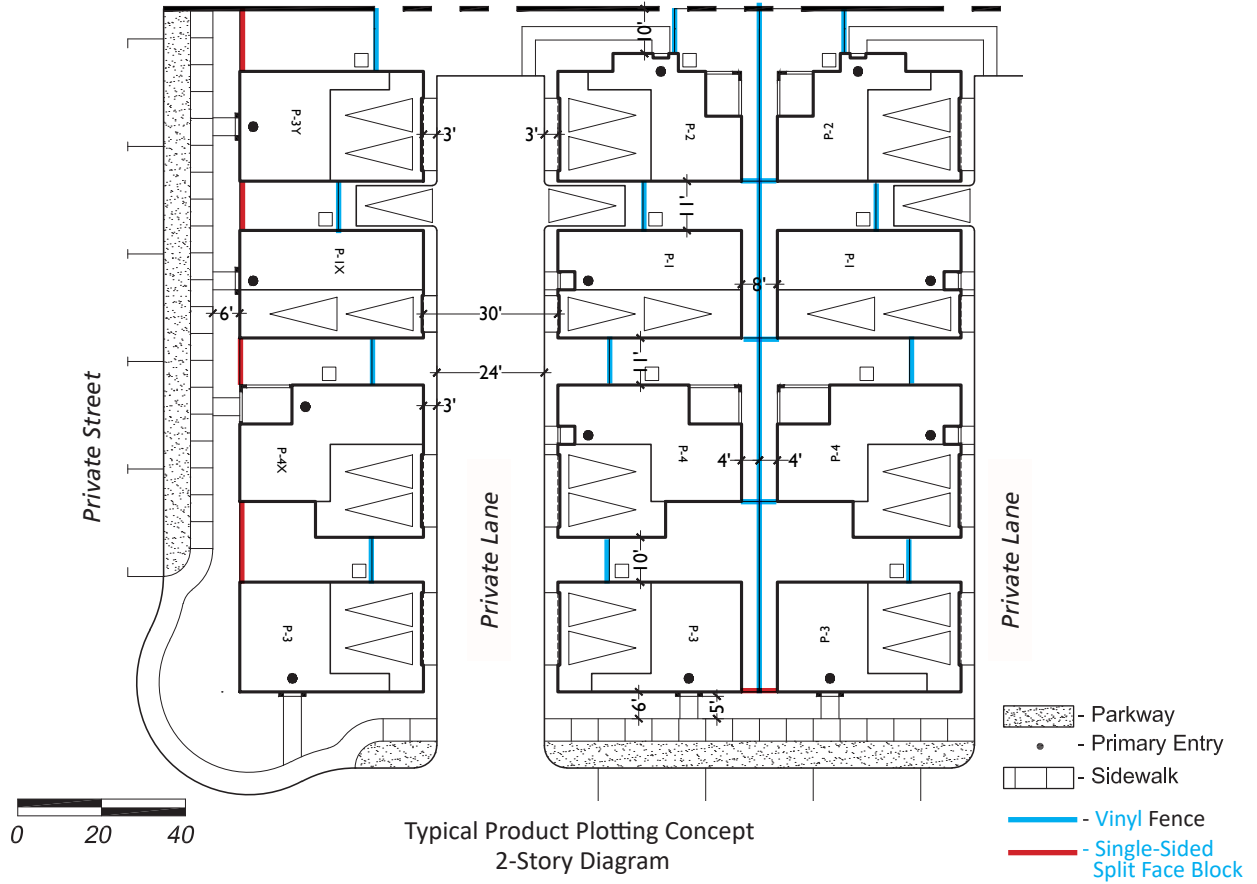
### Building Height

Architectural projections may exceed the maximum building height of 42' by an additional 3'.



# Parkside

## Conventionally-Loaded Lane Homes (up to 12-Plex)- Product 3 (PA 2, 3 & 4)



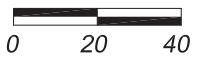
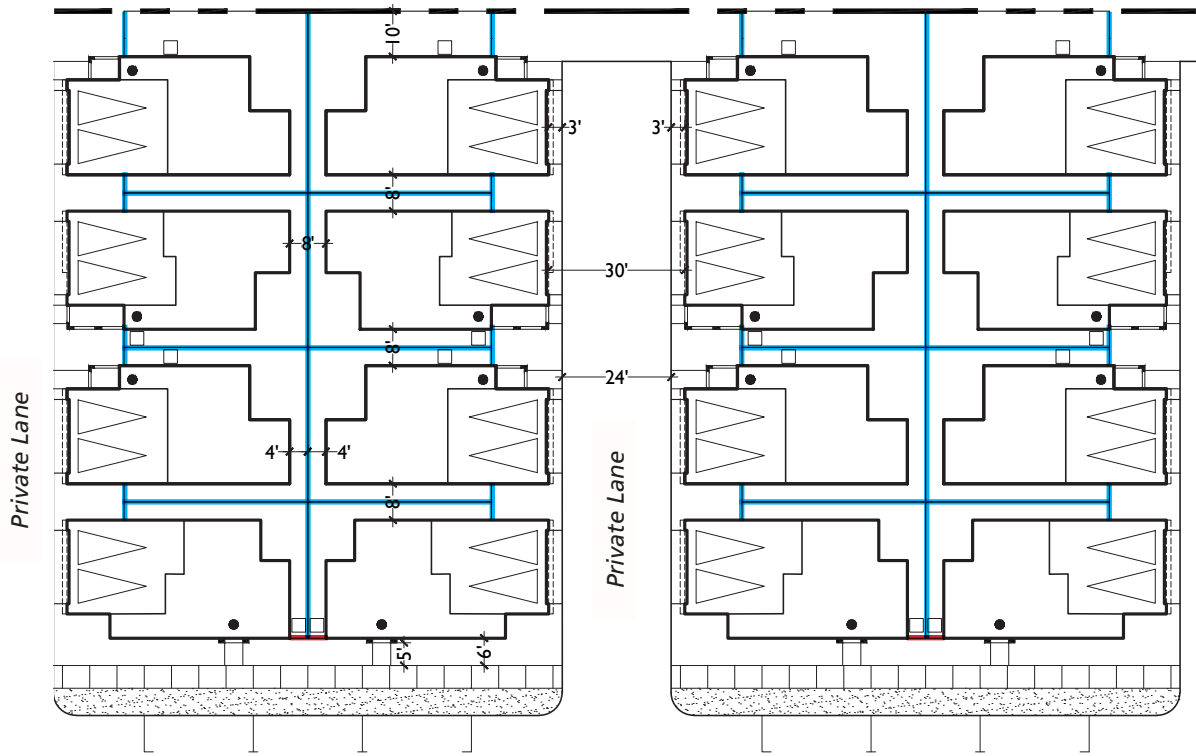
Note: Auto Court and lane-loaded Private Lanes shall be enhanced with a ribbon of either colored concrete, stamped concrete, pavers or other similar decorative materials.



# Parkside

## Conventionally-Loaded Lane Homes- Product 4

(PA 1 & 4)



Private Street

Typical Product Plotting Concept  
2-Story Diagram

- Parkway
- Primary Entry
- Sidewalk
- Vinyl Fence
- Single-Sided Split Face Block

Note: Auto Court and lane-loaded Private Lanes shall be enhanced with a ribbon of either colored concrete, stamped concrete, pavers or other similar decorative materials.



# Parkside

Detached Green Court Cluster (Up to 8 - 10-Plex)

(PA 1 & 4)



## Characteristics

- Single-family detached dwellings clustered along paseo/court;
- Automobile access via lanes (private alleys);
- Primary entries and walks face paseo/court or street;
- Private side yards/patios;
- Parking for residents provided in garages or in uncovered resident parking spaces;
- Guest parking provided on local streets, designated parking areas, or as approved by the City; and
- Reciprocal use easements extend private space.

## Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
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# Parkside

## Detached Green Court Cluster (Up to 8 -10-Plex)- Product 1 (PA 1 & 4)

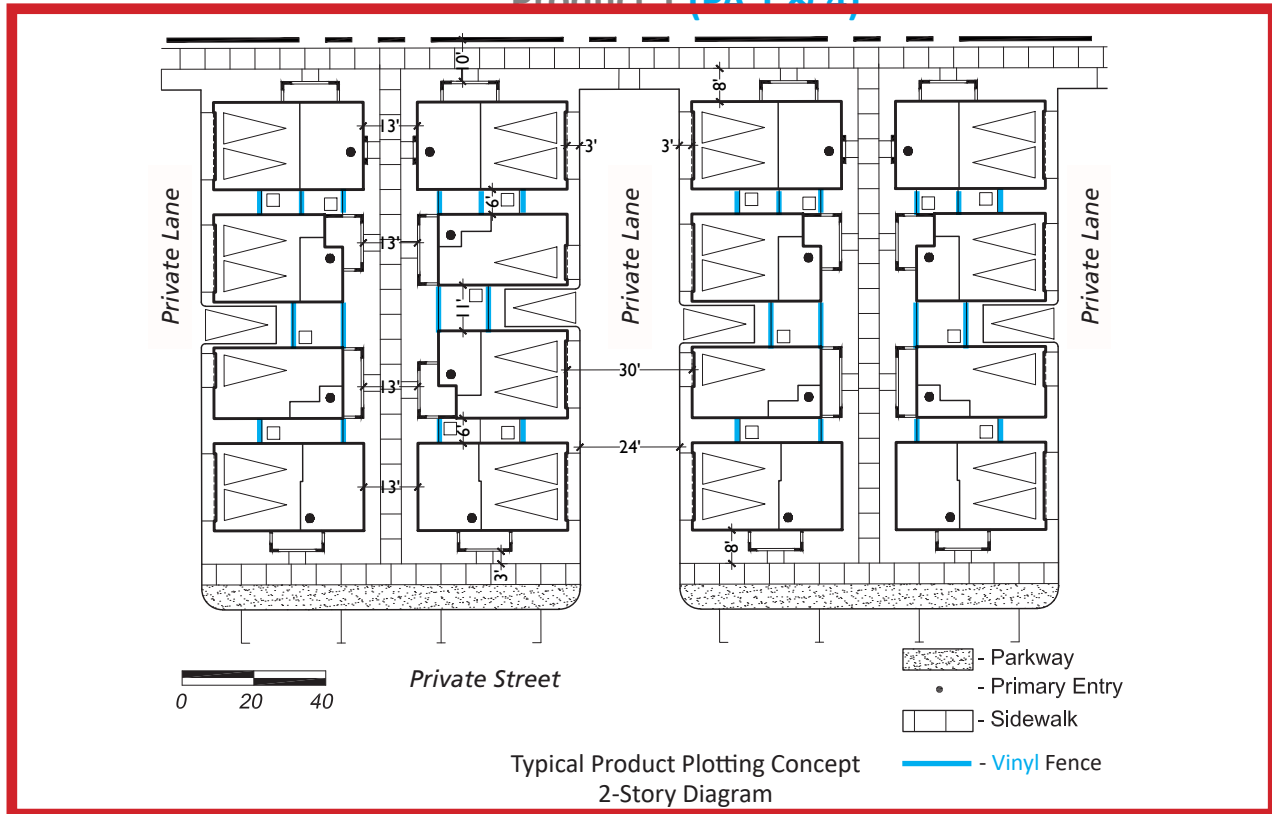


Table 7-2: Detached Green Court Cluster Development Standards

### Building Separation

Front to front living space	13'
Side to side living space	6'

### Front (Private Street-Facing) Setbacks - To Back of Sidewalk

Living space	7'
Porch, balcony	3'
Garden wall	3'

### Side Setbacks - To Lot Edge

Living space to property line	10'
-------------------------------	-----

### Rear Setbacks - To Lot Edge

Living space	10'
--------------	-----

### Garage Setbacks

Garage face to garage face	30'
Garage face to driveway (apron)	3'

Drive aprons shall be 5' or less in length or 18' in length or greater.

### Maximum Encroachments into Building Setbacks

Second/third floor cantilever over garage	3'
Fireplace, media niche (8' max. length), bay windows furr outs, etc.	2'
Potshelves, brackets, eaves, awnings, eyebrows, canopies, cantilever decks, etc.	Additional 36" over porch or furring setback

### Private Open Space Per Unit

Minimum dimension	5'
Total per unit	70 s.f.

### Parking

2 spaces per unit, at least 1 space needs to be covered.

### Building Height

Architectural projections may exceed the maximum building height of 42' by an additional 3'.

**Note:** Green Court pedestrian entries may have decorative arbor (or other similar decorative design feature to enhance pedestrian scale and street scene.



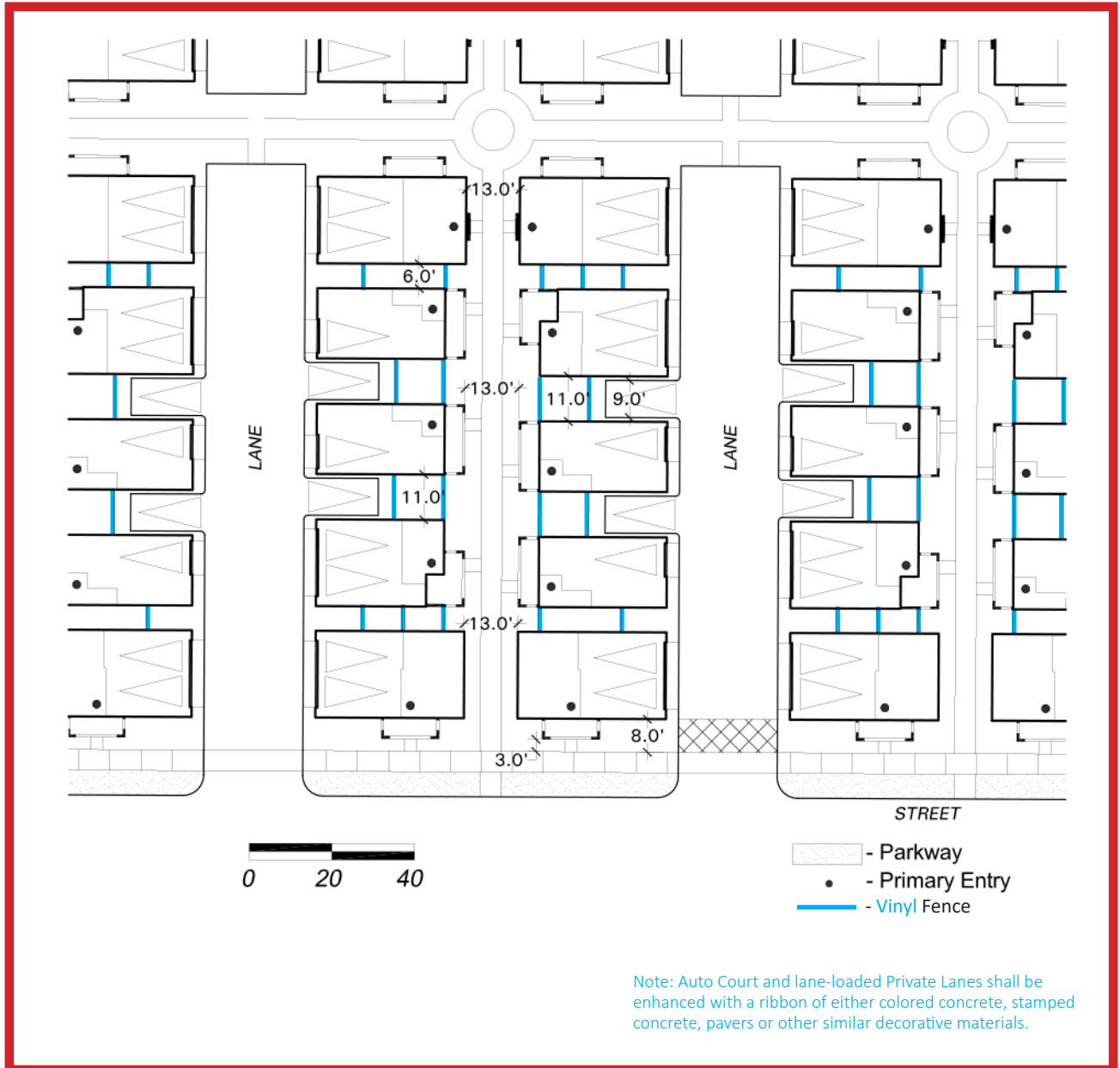
# Parkside

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# Parkside

## Detached Green Court Cluster (Up to 8 -10-Plex)- Product 1 (PA 1 & 4)





# Parkside

## Conventionally Loaded Lane Homes

(PA 1, 7 & 8)\*



### Characteristics

- [Conventionally loaded lane homes](#) are dwellings face street or lanes (private alleys);
- Automobile access via streets or lanes (private alleys);
- Primary entries and walks face streets or lanes (private alleys);
- Private yards/patios;
- Reciprocal use easements may extend private spaces on sides of buildings;
- Parking for residents provided in garages and driveways; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

### Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
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# Parkside

## Conventionally Loaded Lane Home

(PA 1, 7 & 8)\*

\* Proposed product types in each Planning Area are subject to change based on final development submittal.

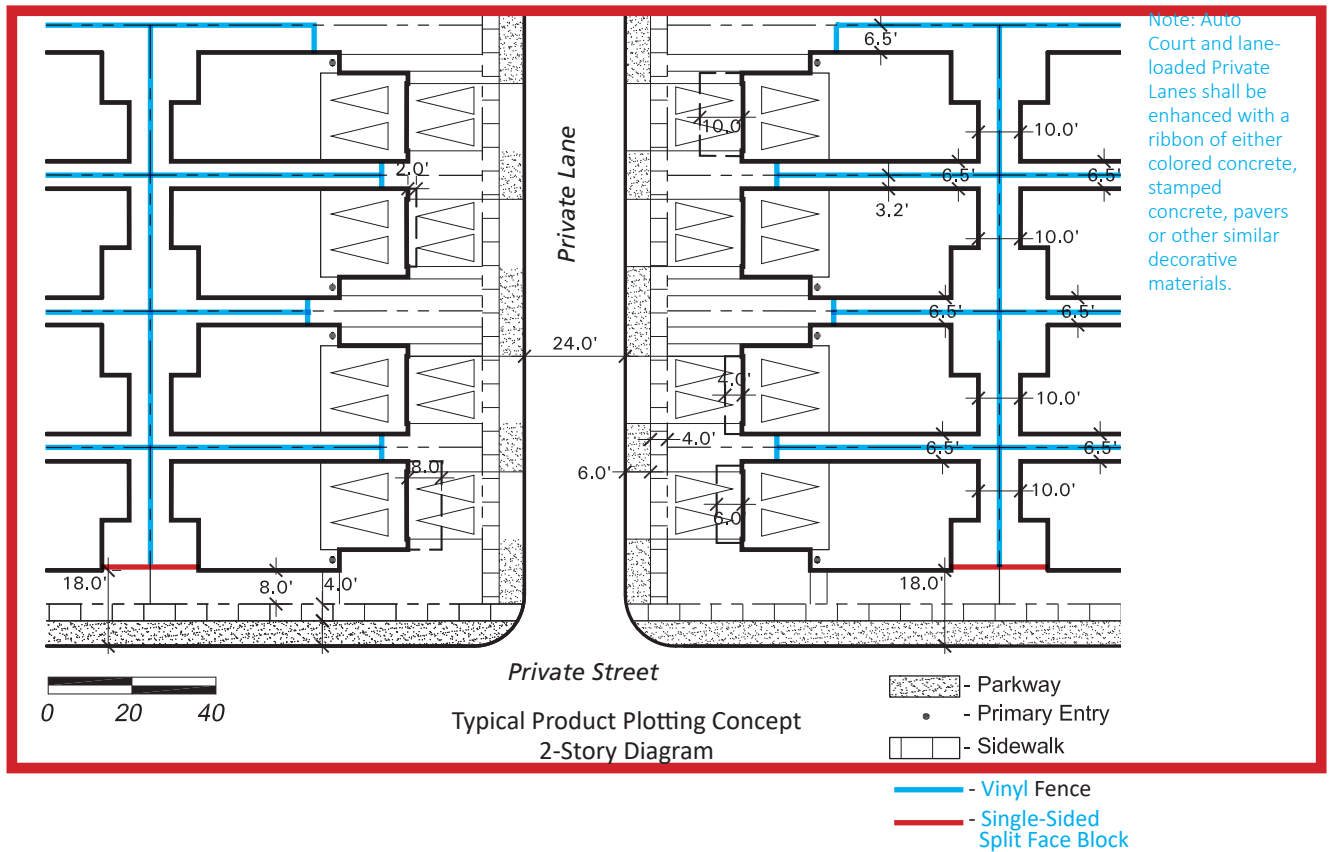


Table 7-3: Conventionally Loaded Lane Home Development Standards

<b>Building Separation</b>		<b>Maximum Encroachments into Building Setbacks</b>	
Front to side living space	N/A	Second floor cantilever over garage	10'
Side to side living space	6.5'	<b>Private Open Space Per Unit</b>	
Rear living to rear living	10'	Minimum dimension	10'
<b>Front Setbacks (Private Street Facing) - To Back of Sidewalk</b>		Total per unit	100 s.f.
Living space	8'	<b>Parking</b>	
Porch, balcony	5'	Per Ontario Development Code	
Garden wall	3'	<b>Building Height</b>	
<b>Garage Setbacks</b>		Architectural projections may exceed the maximum building height of 35' by an additional 5'.	
Garage face to lane (drive apron)	18'		
Drive aprons shall be 5' or less in length or 18' in length or greater. .			



# Parkside

## Conventionally Loaded Lane Home

(PA 1, 7 & 8)\*

\* Proposed product types in each Planning Area are subject to change based on final development submittal.



### Characteristics

- **Conventionally Loaded Lane Homes** are detached dwellings face street or lanes (private alleys);
- Automobile access via streets or lanes (private alleys);
- Primary entries and walks face streets or lanes (private alleys);
- Private yards/patios;
- Reciprocal use easements may extend private spaces on sides of buildings;
- Parking for residents provided in garages and driveways; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

### Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
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# Parkside

## Conventionally Loaded Lane Home

(PA 1, 7 & 8)\*

\* Proposed product types in each Planning Area are subject to change based on final development submittal.

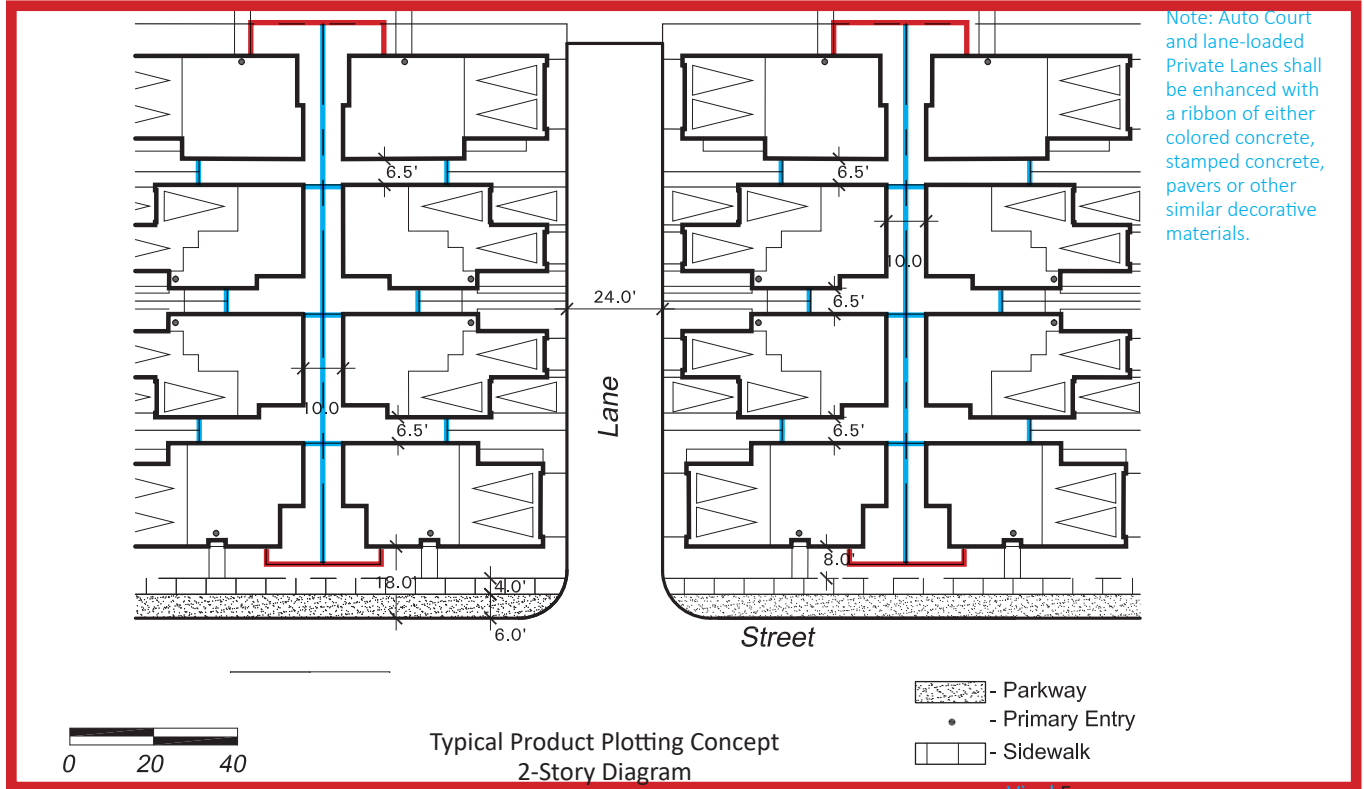


Table 7-4: Conventionally Loaded Lane Home Development Standards

### Building Separation

Front to side living space	6.5'
Side to side living space	6.5'
Rear living to rear living	10'

### Front Setbacks (Private Street Facing) - To Back of Sidewalk

Living space	8'
Porch, balcony	5'
Garden wall	3'

### Garage Setbacks

Garage face to lane (drive apron)	8'
Drive aprons shall be 5' or less in length or 18' in length or greater.	

### Maximum Encroachments into Building Setbacks

Second floor cantilever over garage	2'
-------------------------------------	----

### Private Open Space Per Unit

Minimum dimension	10'
Total per unit	100 s.f.

### Parking

Per Ontario Development Code

### Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 5'



# Parkside

## Detached Green Court Paseo Cluster (6 or 8-Plex)

(PA 1, 7 & 8)\*



### Characteristics

- Single-family detached dwellings clustered along paseo/court;
- Automobile access via lanes (private alleys);
- Primary entries and walks face paseo/court or street;
- Private side yards/patios;
- Reciprocal use easements extend private space;
- Parking for residents provided in garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

### Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
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# Parkside

## Detached Green Court Paseo Cluster (6 or 8-Plex)

(PA 1, 7 & 8)\*

\* Proposed product types in each Planning Area are subject to change based on final development submittal.

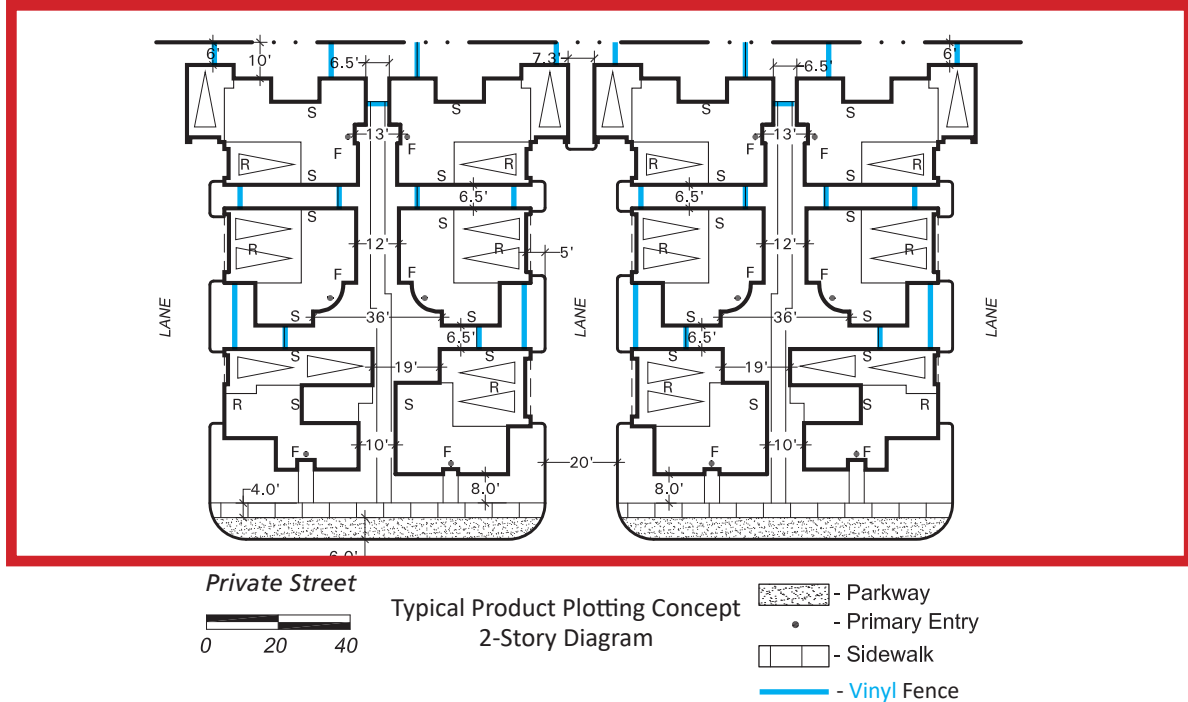


Table 7-5: Detached Green Court Paseo Cluster (6- or 8-Plex) Development Standards

### Building Separation

Front to front living space-paseo/court	6.5' to 13'
Side to side living space	6.5'
Opposing entries—door to door	13'

### Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered) potshelves, brackets, etc.	1.5'*
Second floor cantilever over garage	2'

### Side Setbacks - To Back of Sidewalk - Private Street Facing

Living space	8'
Porch	5'
Garden wall	3'

### Private Open Space Per Unit

Minimum dimension	7'
Total per unit	70 s.f.

### Side Setbacks - To Lot Edge

Living space	10'
Non-living space (garage-single story)	6'

### Parking

Per Ontario Development Code

### Garage Setbacks

Garage face to garage face	30'
Garage face to lane (drive apron)	3'-5'
Living space (to lane edge)	10'

### Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 5'.

Drive aprons shall be 5' or less in length or 18' in length or greater.

Note: Lane width will increase to 24' when the length exceeds 150'

\* 3' min. from PL

**Note:** Green Court pedestrian entries may have decorative arbor (or other similar decorative design feature to enhance pedestrian scale and street scene.



# Parkside

## Detached Green Court Cluster (8 to 10 Plex)

(PA 1, 7 & 8)\*



### Characteristics

- Single-family detached dwellings clustered around a Auto Court;
- Automobile access via common Auto Court driveway;
- Primary entries and walks face Auto Court or street;
- Private side and rear yards;
- Parking for residents provided in garages;
- Guest parking provided on local streets, designated parking areas, or as approved by the City; and
- Reciprocal use easements extend private space.

### Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
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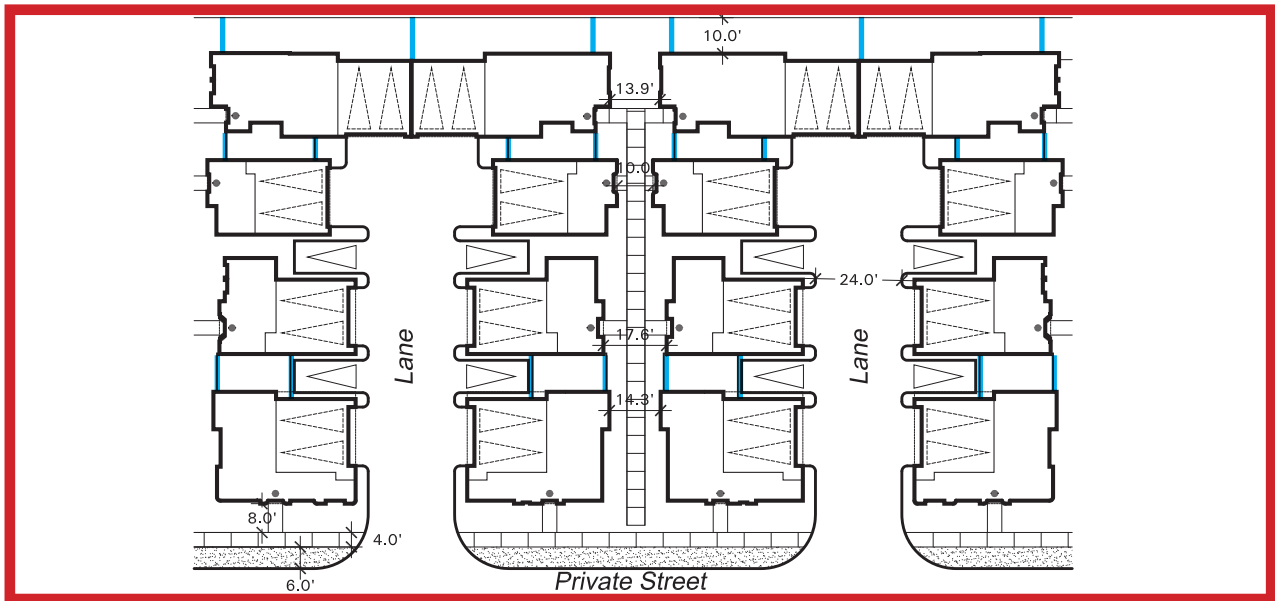


# Parkside

## Detached Green Court Cluster (8 to 10-Plex)

(PA 1, 7 & 8)\*

\* Proposed product types in each Planning Area are subject to change based on final development submittal.



- Parkway
- Primary Entry
- Sidewalk
- Vinyl Fence

Typical Product Plotting Concept  
2-3 Story Diagram

Table 7-6: Detached Green Court Cluster Development Standards

### Building Separation

Front to front living space	10'
Side to side living space	8' or 2" airspace

### Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc.	2'*
Second floor cantilever over garage	2'

### Front (Private Street-Facing) Setbacks - To Back of Sidewalk

Living space	8'
Porch, balcony	5'
Garden wall	3'

### Private Open Space Per Unit

Minimum dimension	7'
Total per unit	70 s.f.

### Side Setbacks - To Lot Edge

Living space to property line	4'
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### Parking

Per Ontario Development Code

### Rear Setbacks - To Lot Edge

Living space	10'
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### Building Height

Architectural projections may exceed the maximum building height of 42' by an additional 3'.

\* 3' min. from PL

### Garage Setbacks

Garage face to garage face	30'
Garage face to driveway (apron)	3'-5'
Drive aprons shall be 5' or less in length or 18' in length or greater.	
Note: Lane width will increase to 24' when the length exceeds 150'	

**Note:** Green Court pedestrian entries may have decorative arbor (or other similar decorative design feature to enhance pedestrian scale and street scene.



# Parkside

## Detached Auto Court Cluster (6 to 8-Plex)

(PA 1. 7 & 8)\*



### Characteristics

- Single-family detached dwellings clustered around Auto Court;
- Automobile access via common Auto Court driveway;
- Primary entries and walks face Auto Court or street
- Private side and rear yards;
- Parking for residents provided in garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.
- Reciprocal use easements extend private space

### Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
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# Parkside

## Detached Auto Court Cluster (6 to 8-Plex)

(PA 1, 7, & 8)\*

\* Proposed product types in each Planning Area are subject to change based on final development submittal.

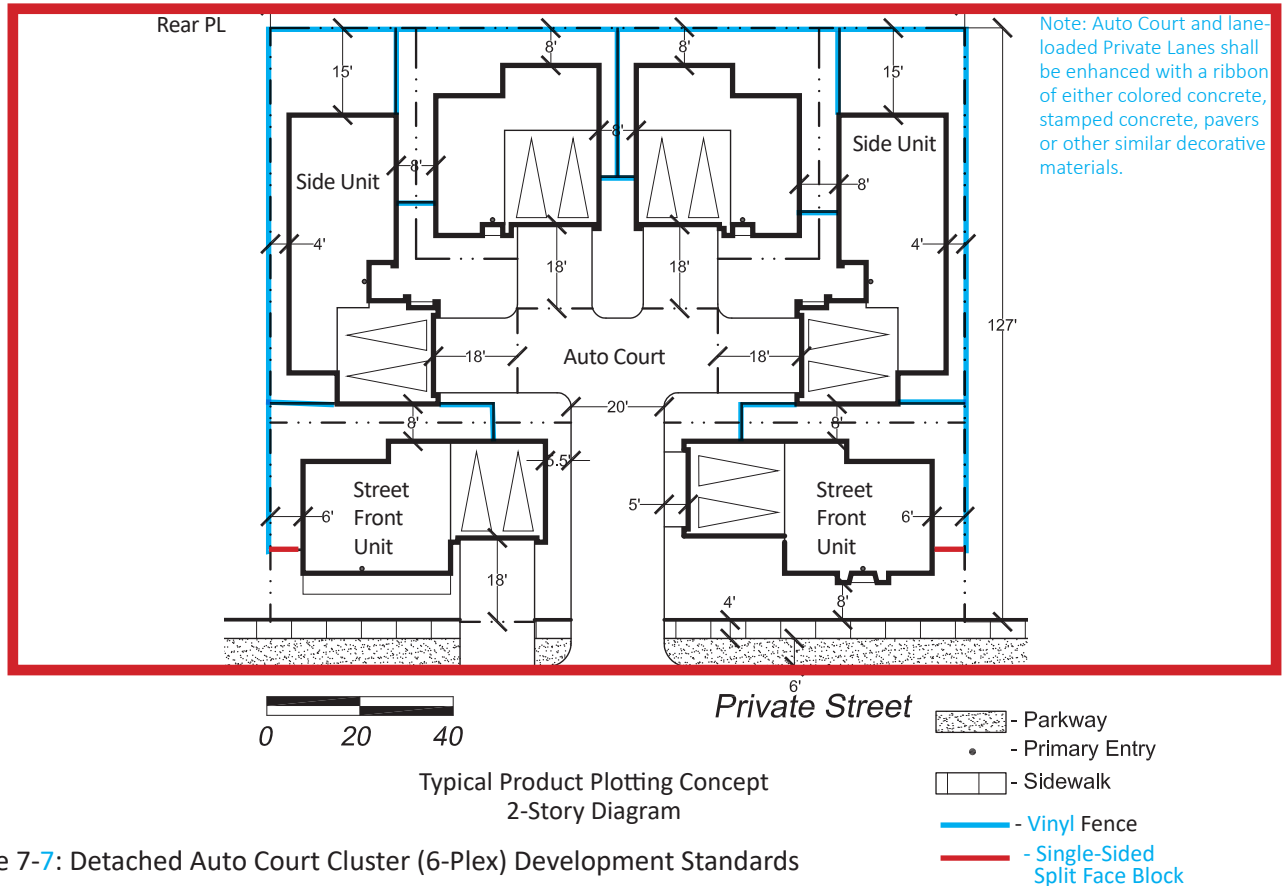


Table 7-7: Detached Auto Court Cluster (6-Plex) Development Standards

### Building Separation

Front to side living space	8' (4'+4')
Side to side living space	8' (4'+4')

### Front Setbacks (Private Street Facing) - To Back of Sidewalk

Living space	8'
Porch, balcony	5'
Garden wall	3'

### Side Setbacks - To Lot Edge

Living space	4'
--------------	----

### Garage Setbacks

Garage facing street	18'
Garage face to garage face	30'
Garage face to lane (drive apron)	5'

Drive aprons shall be 5' or less in length or 18' in length or greater.

Note: Lane width will increase to 24' when the length exceeds 150'

### Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc.	2'*
Second floor cantilever over garage	2'

### Private Open Space Per Unit

Minimum dimension	8'
Total per unit	100' s.f.

### Parking

Per Ontario Development Code

### Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 5'.

\*3' min. from PL



# Parkside

## Detached Auto Court Cluster (6 or 8-Plex)

(PA 1, 7 & 8)\*



### Characteristics

- Single-family detached dwellings clustered around a Auto Court;
- Automobile access via common Auto Court driveway;
- Primary entries and walks face Auto Court or street;
- Private side and rear yards;
- Parking for residents provided in garages;
- Guest parking provided on local streets, designated parking areas, or as approved by the City; and
- Reciprocal use easements extend private space.

### Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
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# Parkside

## Detached Auto Court Cluster (6 or 8-Plex)\*

(PA 1, 7 & 8)\*

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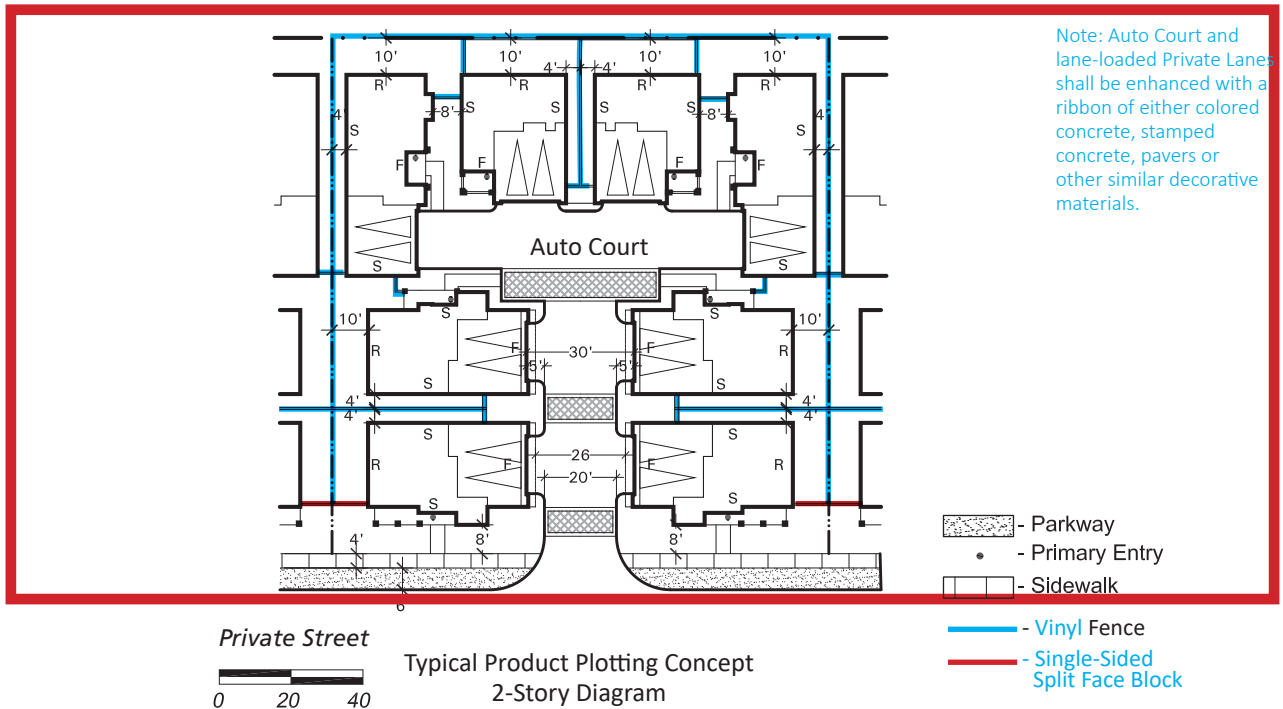


Table 7-8: Detached Auto Court Cluster (6- or 8-Plex) Development Standards

<b>Building Separation</b>		<b>Maximum Encroachments into Building Setbacks</b>	
Front to side living space	8'	Fireplace, media niche (8' max length), bay windows (cantilevered), potselves, brackets, etc.	2'*
Side to side living space with entrance	10'	Second floor cantilever over garage	2'
Side to side living space	8' (4'+4')		
<b>Side Setbacks - To Back of Sidewalk - Private Street Facing</b>		<b>Private Open Space Per Unit</b>	
Living space	8'	Minimum dimension	10'
Porch, balcony	5'	Total per unit	100 s.f.
Garden wall	3'		
<b>Rear Setbacks - To Lot Edge</b>		<b>Parking</b>	
Living space	10'	Per Ontario Development Code	
<b>Garage Setbacks</b>		<b>Building Height</b>	
Garage face to garage face	30'	Architectural projections may exceed the maximum building height of 35' by an additional 5'.	
Garage face to driveway (apron)	3'-5'	* 3' min. from PL	
Drive aprons shall be 5' or less in length or 18' in length or greater.			
Note: Lane width will increase to 24' when the length exceeds 150'			



# Parkside

## Attached Duplex

(PA 14 & 15)\*



FRONT



PLAN 3 FRONT PLAN 1R



PLAN 3 FRONT PLAN 2



REAR



REAR



REAR

Not to Scale

### Characteristics

- Single-family attached dwelling;
- Oriented toward street or paseos;
- Lane (private alley) loaded automobile access;
- Paired homes enter from front;
- Private rear yards;
- Parking for residents provided in garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

### Notes

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# Parkside

## Attached Duplex

(PA 14 & 15)\*

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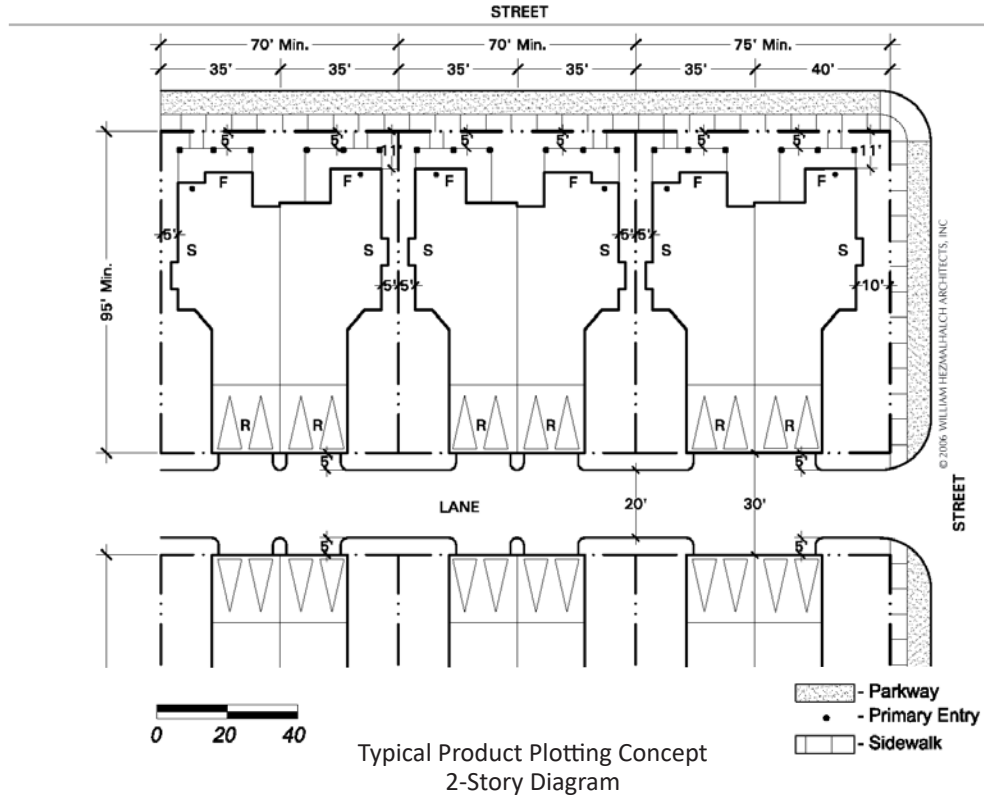


Table 7-9: Attached Duplex Development Standards

### Side Setbacks

Side to side living space 10' (5' + 5')

### Side Setbacks at Corners - To Back of Sidewalk

Living space 11'  
 Living space (2-story) 10'  
 Living space (3-story) 15'  
 Porch or balcony 5'  
 Garden wall 3'

### Rear Setbacks

Living space (ground floor) to property line 25'

Note: Maximum lot coverage is 60%.

### Garage Setbacks

Garage 5'  
 (15' to center line of alley)

Garage face to garage face 30'

Garage face to lane 3'-5'\*

Drive aprons shall be 5' or less in length or 18' in length or greater.

### Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc. 2'

### Private Open Space Per Unit

Minimum dimension 7'  
 Total per unit 150 s.f.

### Parking

Per Ontario Development Code

### Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 5'.

\* Measurement is to be taken from the inside of the curb



# Parkside

## Attached Triplex

(PA 9, 12 & 16)\*



### Characteristics

- Attached multi-family dwellings face street or green court;
- Automobile access via lanes (private alleys);
- Primary entries and walks face green court or street;
- Private courtyards/patios and common open space;
- Parking for residents provided in garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City;

### Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
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- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
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# Parkside

## Attached Triplex

(PA 9, 12 & 16)\*

\* Proposed product types in each Planning Area are subject to change based on final development submittal.

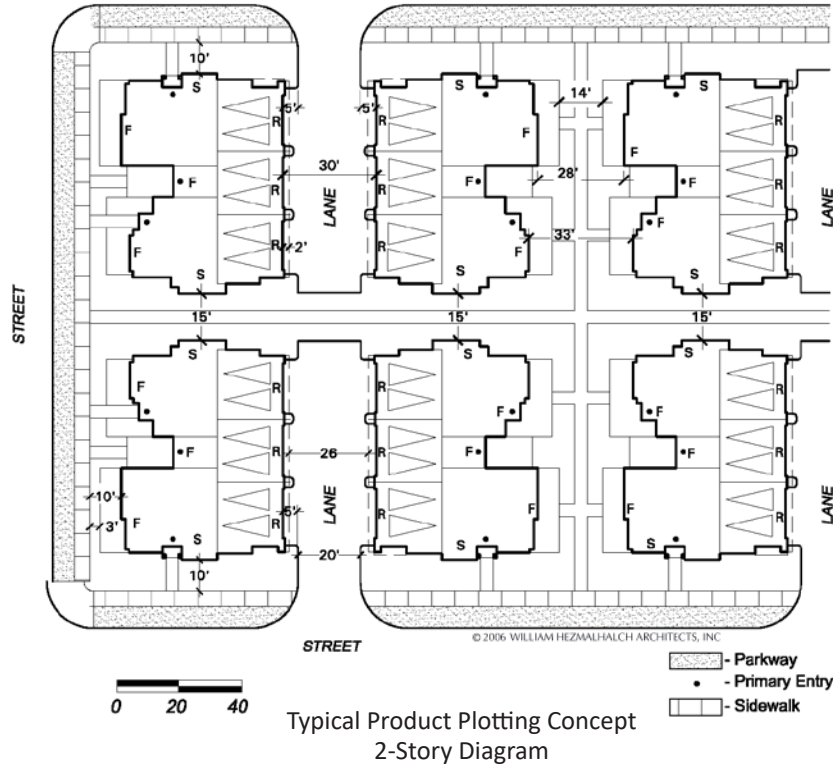


Table 7-10: Triplex Development Standards

### Building Separation

Front to front living space	
28' minimum/35' average- up to 20% of building length	30'- central court
Front to side	25'
Porch/balcony to porch/balcony	14'
Porch/balcony to side	15'
Side to side	15'
Side to front	22'

### Front Setbacks - To Back of Sidewalk- Private Street Facing

Living space	10'
Living space (2-story)	15'
Living space (3-story)	20'
Porch/balcony	5'
Garden wall	3'

### Side Setbacks

Living space to back of sidewalk	10'
Porch/balcony to back of sidewalk	5'
Garden wall to back of sidewalk	3'

### Garage Setbacks

Garage face to garage face	30'
Garage face to lane (drive apron)	3'-5'*
Drive aprons shall be 5' or less in length or 18' in length or greater.	
Living space over garage- min. offset or max. cantilever from face of garage	2'
Note: Lane width may be increased to 24' when the lane length exceeds 150'.	

### Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potselves, brackets, etc.	2'
---	----

### Private Open Space Per Unit

Minimum dimension	7'
Total per unit	150 s.f.

### Parking

Per City of Ontario Development Code

### Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 5'.

\*Measurement is to be taken from the inside of the curb.



# Parkside

## Attached Higher Density Townhomes

(PA 5 & 18)\*



### Characteristics

- Attached multi-family dwellings face street or green court;
- Automobile access via lanes (private alleys);
- Primary entries and walks face green court or street;
- Private courtyards/patios;
- Parking for residents provided in garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

### Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
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# Parkside

## Attached Higher Density Townhomes

(PA 5 & 18)\*

\* Proposed product types in each Planning Area are subject to change based on final development submittal.

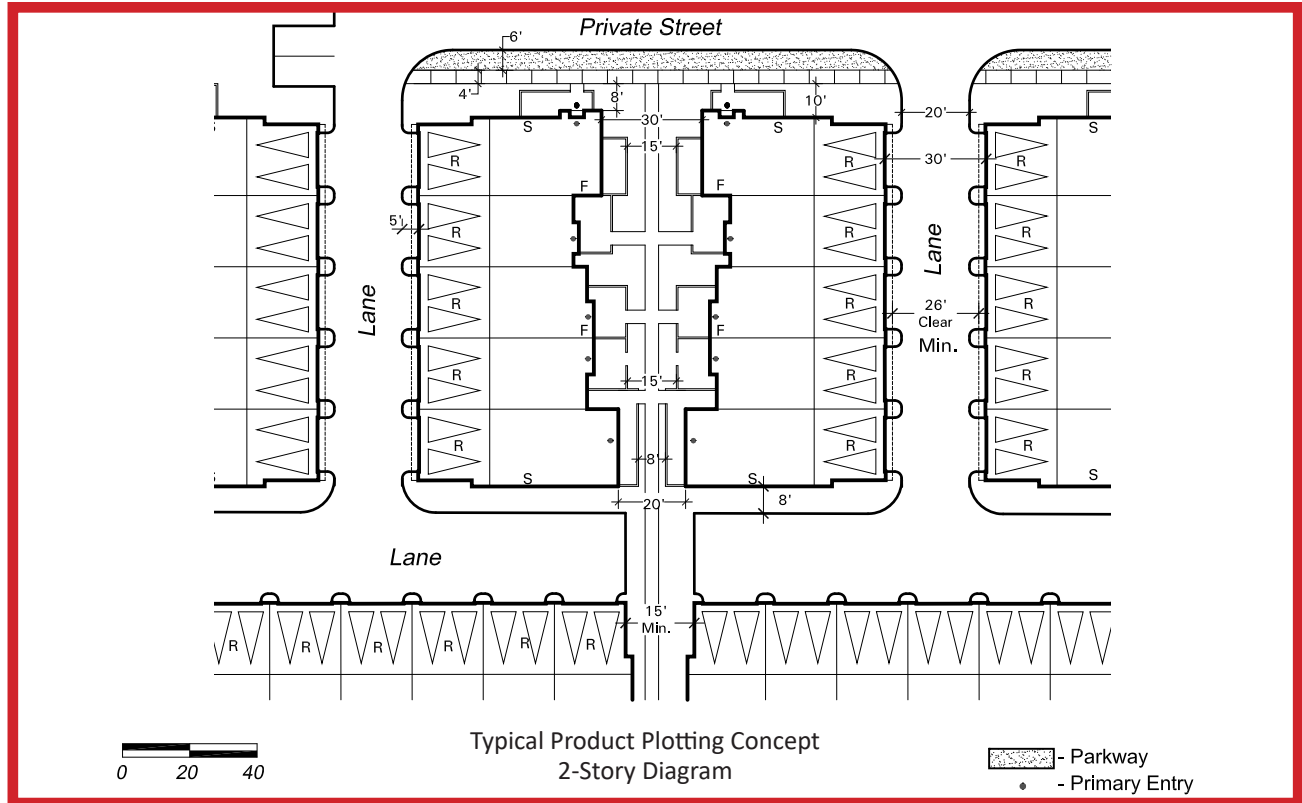


Table 7-11: Attached Higher Density Townhomes Development Standards

### Building Separations

	20' minimum/25' average- up to 20%
Front to front living space	of building length
	30'- central court
Front to side living space	25'
Porch/balcony to porch/balcony	14'
Porch/balcony to side	15'
Side to side living space	15'
Opposing entries- door to door	20'

### Front Setbacks - To Back of Sidewalk - Private Street Facing

Living space	10'
Living space (2-story)	15'
Living space (3-story)	20'
Porch/balcony	5'
Garden wall	3'

### Side Setbacks - To Back of Sidewalk - Street Facing

Living space to back of sidewalk	10'
Porch/balcony to back of sidewalk	5'
Garden wall to back of sidewalk	3'

### Garage Setbacks

Garage face to garage face	30'
Garage face to lane (drive apron)	3'-5'*
<i>Drive aprons shall be 5' or less in length or 18' in length or greater.</i>	
Living space over garage- min. offset or max. cantilever from face of garage	2'
Note: Lane width will increase to 24' when the length exceeds 150'	

### Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc.	2'
--	----

### Private Open Space Per Unit

Minimum dimension	7'
Total per unit	150 s.f.

### Parking

Per Ontario Development Code

### Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 5'.

\*Measurement is to be taken from the inside of the curb.



# Parkside

## Attached Row Townhomes

(PA 10 & 17)\*



### Characteristics

- Attached multi-family dwellings face street or green court;
- Automobile access via lanes (private alleys);
- Primary entries face street or common open space;
- Private patios/courtyards;
- Parking for residents provided in garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

### Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
- Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.

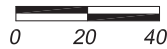
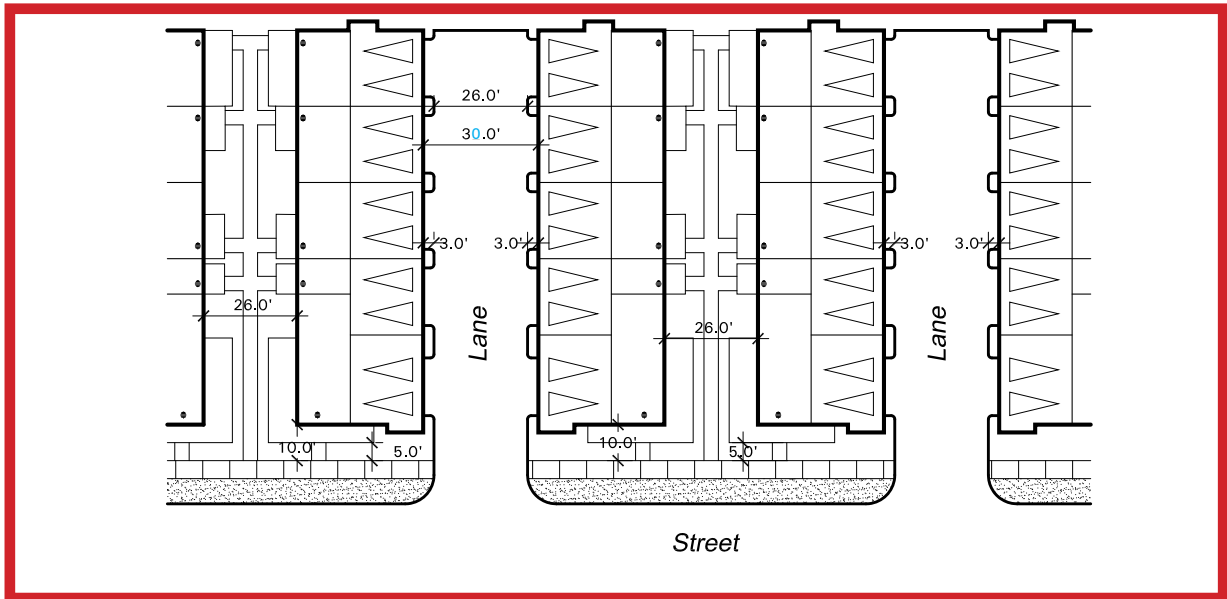


# Parkside

## Attached Townhomes

(PA 10 & 17)\*

\* Proposed product types in each Planning Area are subject to change based on final development submittal.



Typical Product Plotting Concept  
3-Story Diagram with Tandem and Two-Car Garages

- Parkway
- Primary Entry
- Sidewalk

Table 7-12: Attached Townhomes Development Standards

### Building Setbacks

Front to front living space	26' minimum
Side to side	10'

### Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc.	2'*
Second floor cantilever over garage	2'

### Front Setbacks (Private Street Facing) - To Back of Sidewalk

Living space	10'
Porch	5'
Garden wall	5'

### Private Open Space Per Unit

Minimum dimension	5'
Total per unit	50 s.f.

### Garage Setbacks

Garage face to garage face	30'
Garage face to lane (drive apron)	3'-5'
Drive aprons shall be 5' or less in length or 18' in length or greater.	
Note: Lane width will increase to 26' when the length exceeds 150'	

### Parking

Per Ontario Development Code

### Building Height

Architectural projections may exceed the maximum building height of 42' by an additional 3'.

\*3' from PL



# Parkside

Attached Auto Court Cluster (10- to 14-Plex)

(PA 6 & 13)\*



## Characteristics

- Living spaces oriented toward streets or paseos;
- Homes enter from front, side and lane;
- Lane to Auto Court vehicle access;
- Homes have courtyards, porches or balconies;
- Parking for residents provided in 1- or 2-car garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

## Notes:

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
- Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.

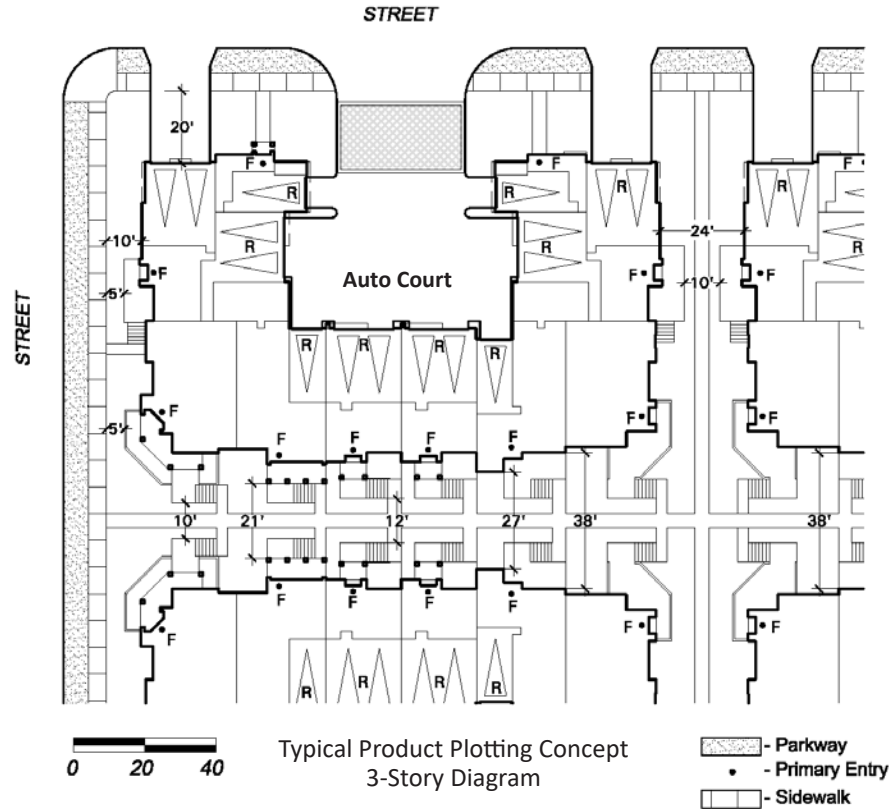


# Parkside

## Attached Auto Court Cluster (10- to 14-Plex)

(PA 6 & 13)\*

\* Proposed product types in each Planning Area are subject to change based on final development submittal.



Note: Auto Court and lane-loaded Private Lanes shall be enhanced with a ribbon of either colored concrete, stamped concrete, pavers or other similar decorative materials.

Table 7-13: Attached Auto Court Cluster (10- to 14-Plex) Development Standards

### Building Separation

Front to front	32' minimum
Porch/balcony to porch/balcony	21'
Opposing entries- door to door	36'

### Front Setbacks - To Back of Sidewalk

Living space	10'
Living space (2-story)	15'
Living space (3-story)	20'
Porch/balcony- street facing	5'
Garden wall	3'

Note: Decorative paving to be provided at the entrance of the motor court area.

### Garage Setbacks

Living space over garage- min. offset or max. cantilever from face of garage	2'
--	----

Drive aprons shall be 5' or less in length or 18' in length or greater.

### Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc.	2.5'
--	------

### Private Open Space Per Unit

Minimum dimension	7'
Total per unit	150 s.f.

### Parking

Per City of Ontario Development Code

### Building Height

Architectural projections may exceed the maximum building height of 42' by an additional 3'.

Note: Lane width may be increased to 24' when the lane length exceeds 150'.



# Parkside

## 7.7 Landscape

### 7.7.1 Introduction

Careful consideration has been given to the design of the community landscape **architectural** character for the Specific Plan area. The following design guidelines are organized to help define the **basic** landscape design principles for the Specific Plan. Observing these guidelines will help to assure the “design vision” and integrity of this planned community.

### 7.7.2 Overall Streetscapes and Entries

All landscape plans, streetscape plans, and graphic designs with regard to community identity, neighborhood identity, or entry monumentation shall conform to the guidelines as set forth herein, and shall be subject to review and approval by the City of Ontario. Conceptual design for all public and private landscaping, streetscape and entries shall maintain a cohesive design character in massing, form, and texture, and provide compatibility with future designs for the Great Park. Landscaped materials (trees, shrubs, vines, and groundcovers) shall be provided per the **NMC Ontario Ranch** Streetscape Master Plan (**Streetscape Master Plan**).

### 7.7.3 Perimeter Streetscape Design

Streetscape design guidelines establish a hierarchy for the landscape development along the surrounding roadways, as well as establishing a framework for consistency of design. All streetscape design **including the neighborhood edges** shall be in substantial conformance with the **Streetscape Master Plan**. The design are subject to change **dues to NPDES standards and must be approved by the City of Ontario’s streetscape master plan**. Four major **New Model Colony Master Planned roadways** arterials

surround the Specific Plan area: Archibald Ave. to the east, **Edison Ave Ontario Ranch Rd.** to the north, **Merrill Eucalyptus Ave.** to the south, and Carpenter Ave. to the west. Landscape easements associated with these roadways have **also** been defined **as noted in the City of Ontario New Model Colony General Plan Amendment**.

Landscape development surrounding this community will help to set the character, while maintaining consistency with the **City of Ontario’s City’s** pedestrian pathway system illustrated on the **Multipurpose Trails and Bikeways section of the New Model Colony Bikeway Corridor Plan of Policy Plan (General Plan Amendment)** component of The Ontario Plan.

**Bus shelter design, if located on perimeter or interior streetscapes, shall incorporate architectural character/design which is consistent with the neighborhood or streetscape theme.**

#### **Archibald Ave.**

Archibald Ave. streetscape along the residential areas shall include the following:

- **A landscaped median (26 feet wide) with a single row of street trees per the Streetscape Master Plan;**
- **Landscaping within the parkway (7-foot wide minimum (west side) - 10-foot wide minimum (east side) with a row of street trees per the City of Ontario Street Tree Streetscape Master Plan along both sides of the street;**
- **A shared 5-foot wide sidewalk /bikeway (13 feet wide) separated from the 8-foot wide multi-purpose trail with a 5-foot wide landscape area along the west side of the street;**



# Parkside

- A lineal sidewalk (5 feet wide) along the other side of the street;
- A landscaped median (26 feet wide) with a single row of street trees per the City of Ontario Street Tree Master Plan;
- A landscaped neighborhood edge (NE) of 50 feet taken measured from face of curb to perimeter wall on the west side of the street with 25 feet of buffer landscape; and
- Background trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant grouping while combining interesting foliage textures and color. Tree species per City of Ontario Street Tree Streetscape Master Plan.
- ~~Monumentation as shown in the Conceptual Landscape Master Plan, Exhibit 20.~~
- Refer to the streetscape cross section Exhibit 7-3: Archibald Ave. Streetscape for illustration of Archibald.

## **Eucalyptus Ave. Merrill**

Eucalyptus Ave. streetscape shall include the following:

- A landscaped parkway strip (7-foot wide minimum) with a single row of street trees per the City of Ontario Street Tree Streetscape Master Plan along the north side of the street;
- ~~A lineal sidewalk~~ (A shared 13-foot wide sidewalk/ multi-purpose trail with an additional 5 feet of landscape along the north side of the street;
- ~~A shared sidewalk/bikeway along one side of the street;~~
- A landscape easement of 35 feet taken measured from face of curb to perimeter wall on the north side of the street;
- Background trees and shrub masses planted in series of layers (foreground, midground, and background) to help define borders and plant groupings while combining interesting foliage textures and color. Tree species per the Streetscape Master Plan;
- Refer to streetscape cross section Exhibit 7-4: Eucalyptus Ave. Streetscape for illustration.

## **Ontario Ranch Rd.**

Ontario Ranch Rd. streetscape along the residential areas shall include the following:

- A landscaped parkway (7-foot wide minimum) with a single row of street trees per the Streetscape Master Plan along both sides of the street;
- A 5-foot sidewalk separated from the 8-foot wide multi-purpose trail with 5 feet of landscape along the south side of the street;
- A Neighborhood Edge of 50 feet measured from face of curb to perimeter wall on the south side of the street with 25 feet of buffer landscape;
- Background trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color. Tree species per the Streetscape Master Plan;
- ~~Monumentation as shown in the Conceptual Landscape Master Plan, Exhibit 20; and~~
- Refer to the streetscape cross section Exhibit 7-5: Ontario Ranch Rd. Section (Merrill Ave.) Exhibit 22 Streetscape for illustration.



# Parkside

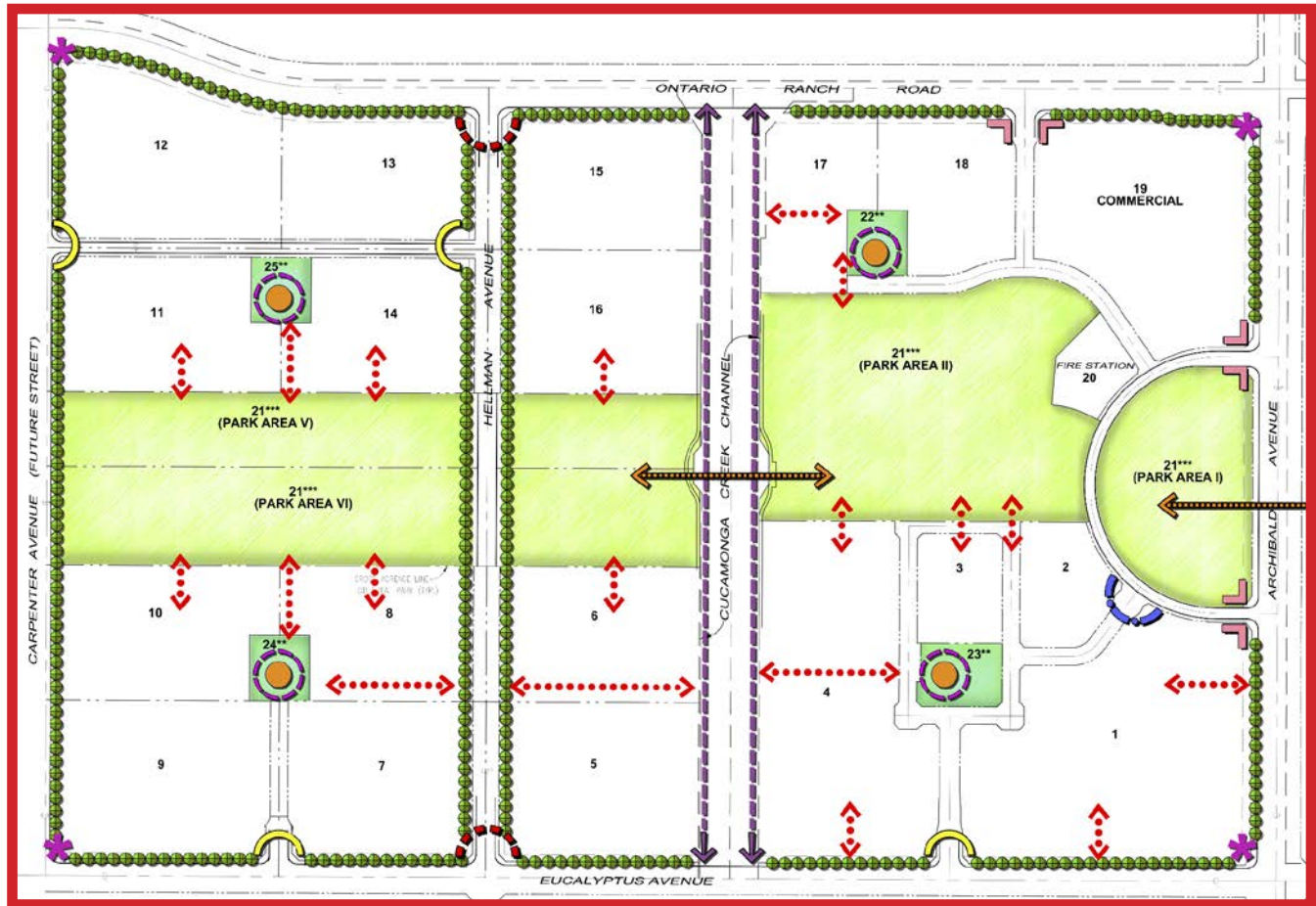














Exhibit 7-2: Conceptual Streetscape Master Plan

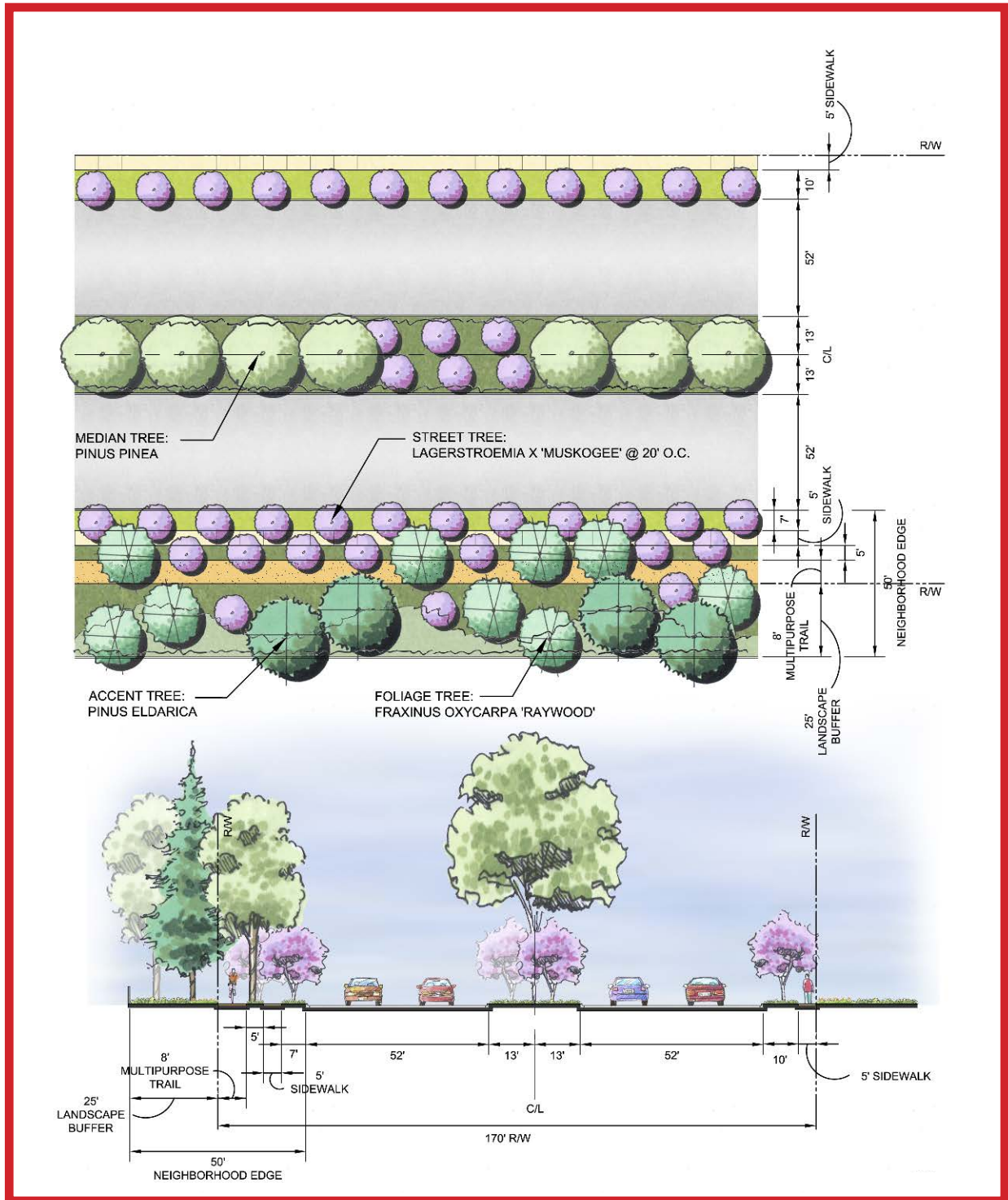
## LEGEND

-  COMMUNITY GATEWAY MONUMENT
-  PRIMARY COMMUNITY GATEWAY ENTRY
-  MAJOR COMMUNITY GATEWAY ENTRY (GATED)
-  SECONDARY COMMUNITY ENTRY (GATED)
-  SECONDARY COMMUNITY ENTRY MONUMENT
-  ENHANCED LANDSCAPE SETBACK
-  MAJOR PEDESTRIAN CONNECTION
-  REGIONAL TRAIL
-  GREAT PARK
-  PRIVATE PARKS
-  REC. AREA
-  PARK AND TRAIL PEDESTRIAN CONNECTIONS





# Parkside

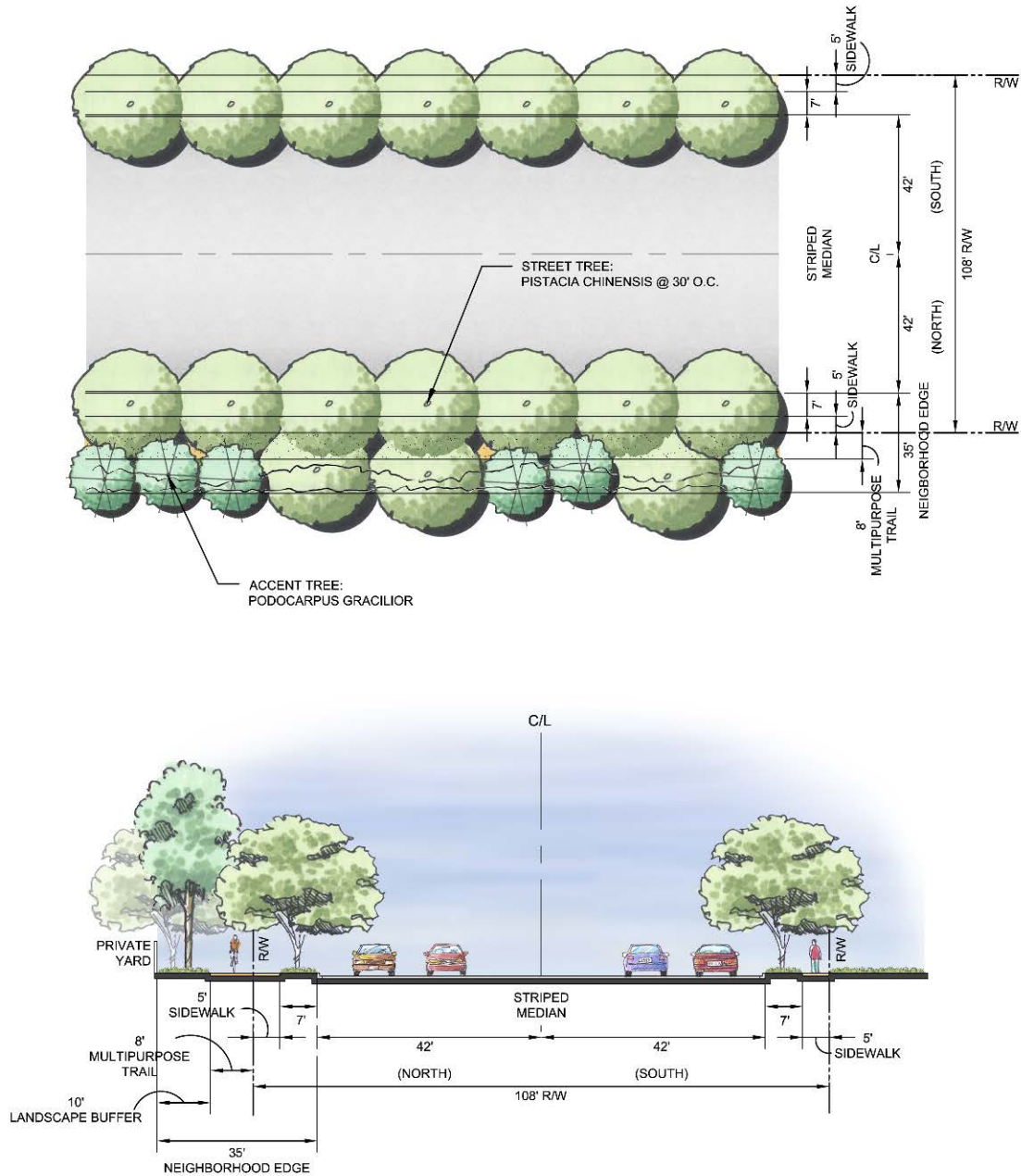


Parkway and median trees shall match the Streetscape Master Plan.

Exhibit 7-3: Archibald Ave. Streetscape



# Parkside



Parkway trees shall match the Streetscape Master Plan .

Exhibit 7-4: Eucalyptus Ave. Streetscape



# Parkside

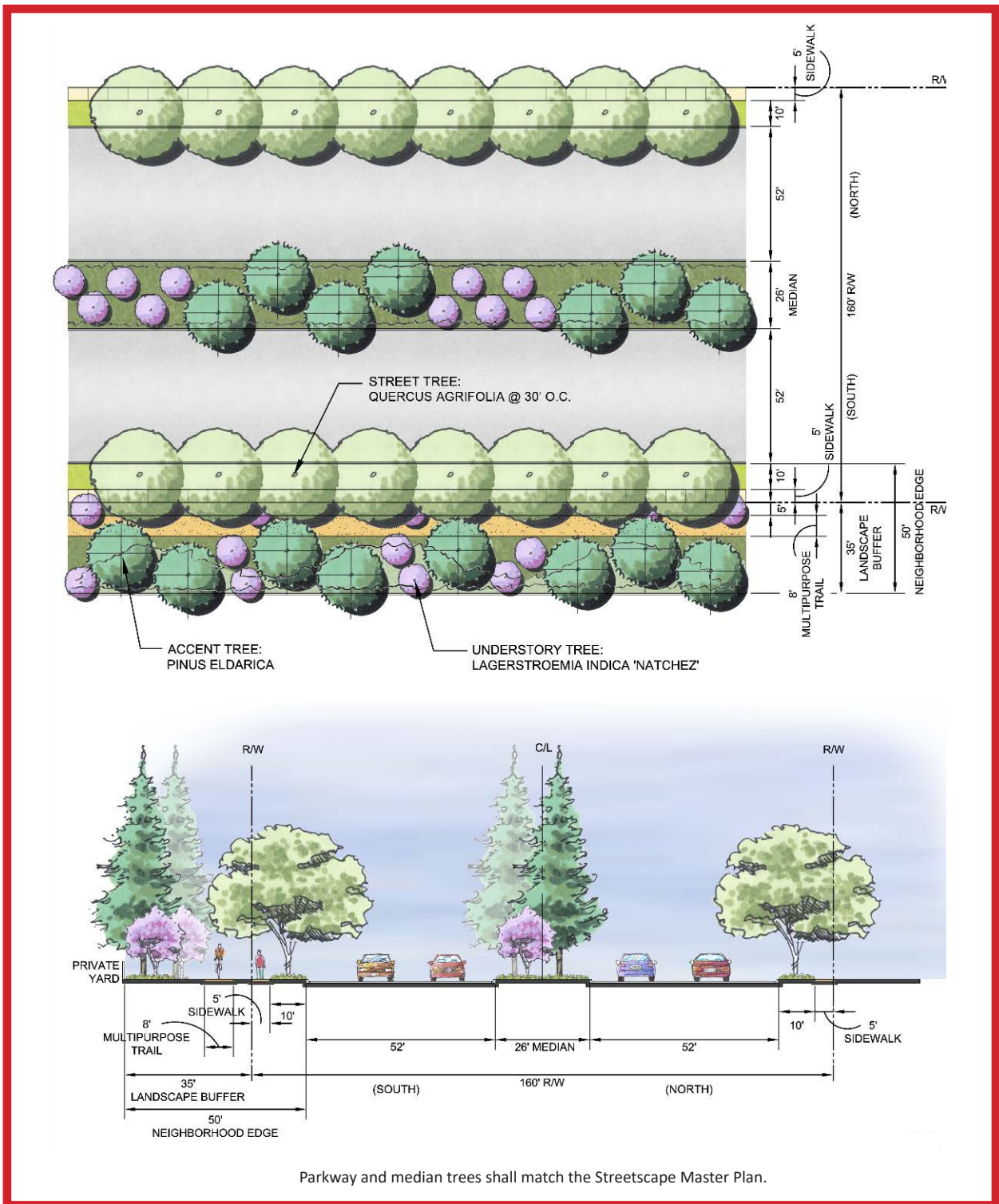


Exhibit 7-5: Ontario Ranch Rd. Streetscape



# Parkside

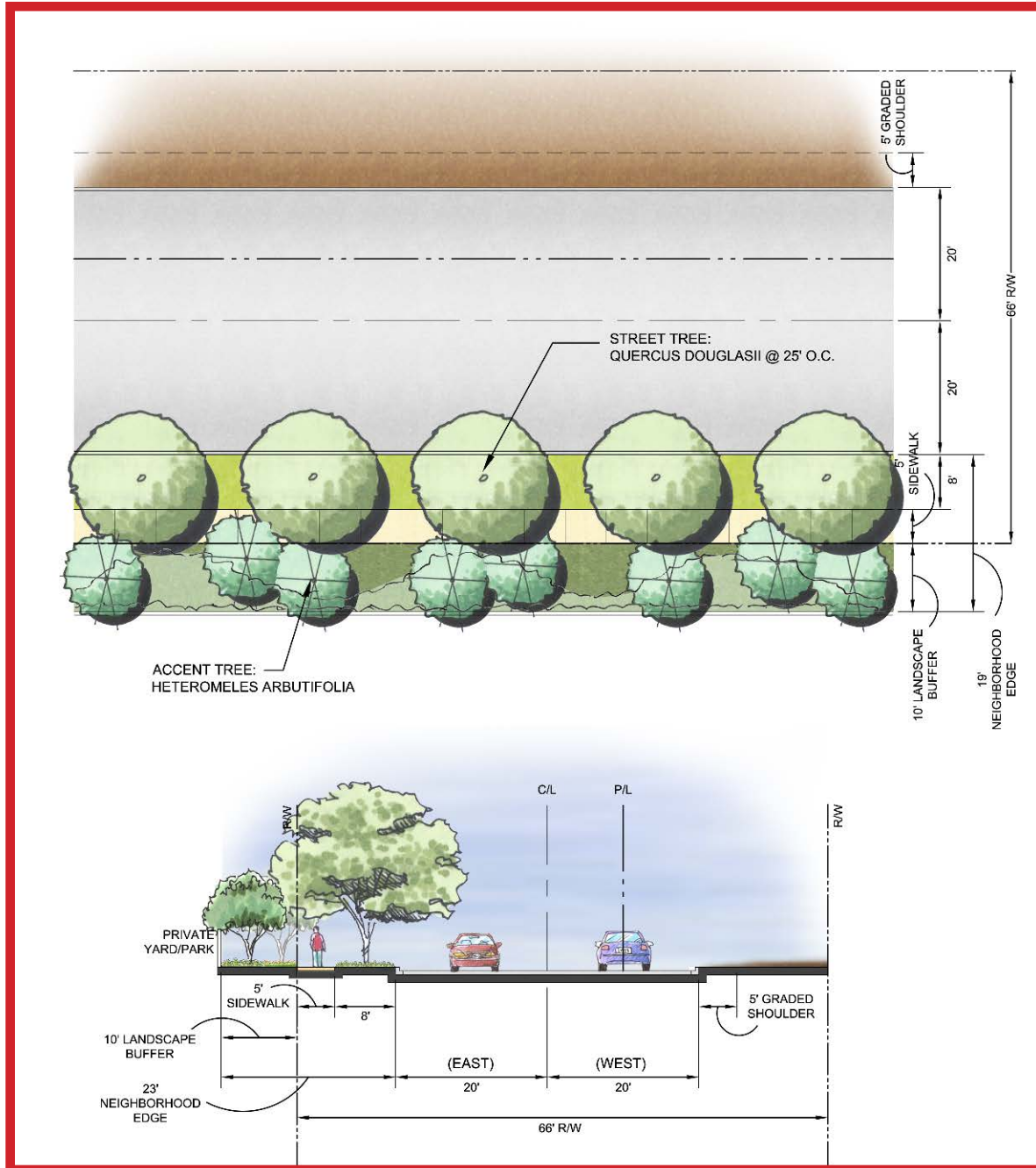


Exhibit 7-6: Carpenter Ave. Streetscape



# Parkside

## Carpenter Ave. Edison Ave.

Carpenter Ave. streetscape shall include the following:

- A landscaped parkway (10 feet 8 feet wide minimum) with a single row of street trees per the City of Ontario Street Tree Master Plan along both sides along east side of the street;
- A lineal 5-foot sidewalk (5 feet wide) along both sides of the street; on the east side of Carpenter Ave.;
- A Neighborhood Edge of 50 23 feet taken measured from face of curb to perimeter wall on the south east side of the street with 10 feet of buffer landscape;
- Background trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color Tree species per City of Ontario Street Tree Master Plan; and
- Monumentation as shown in the Conceptual Landscape Master Plan, Exhibit 20; and
- Refer to the streetscape cross section Exhibit 7-6: Carpenter Ave. Streetscape for illustration.

### 7.7.4 Interior Streetscape Design

Streetscape design within the interior of the Parkside Specific Plan community shall be consistent in character with the perimeter streetscapes and should help to promote pedestrian circulation into the Great Park.

#### Hellman Ave.

While Hellman Avenue is an interior street, the streetscape design is subject to the Streetscape Master Plan and shall include the following:

- A landscaped parkway (7-foot wide minimum) to accommodate shrubs and ground cover with a single row of street trees per the Streetscape Master Plan, along both sides of the street;
- A 5-foot wide sidewalk along both sides of the street that makes additional connections to park and paseo walkways;
- A Neighborhood Edge of 30 feet measured from face of curb to perimeter wall on both sides of the street with 18 feet of buffer landscape. Adjacent to the Park, there is no need for the 30-foot neighborhood edge. Tree species per the Streetscape Master Plan;
- Refer to streetscape section Exhibit 7-7: Hellman Ave. Streetscape for illustration.

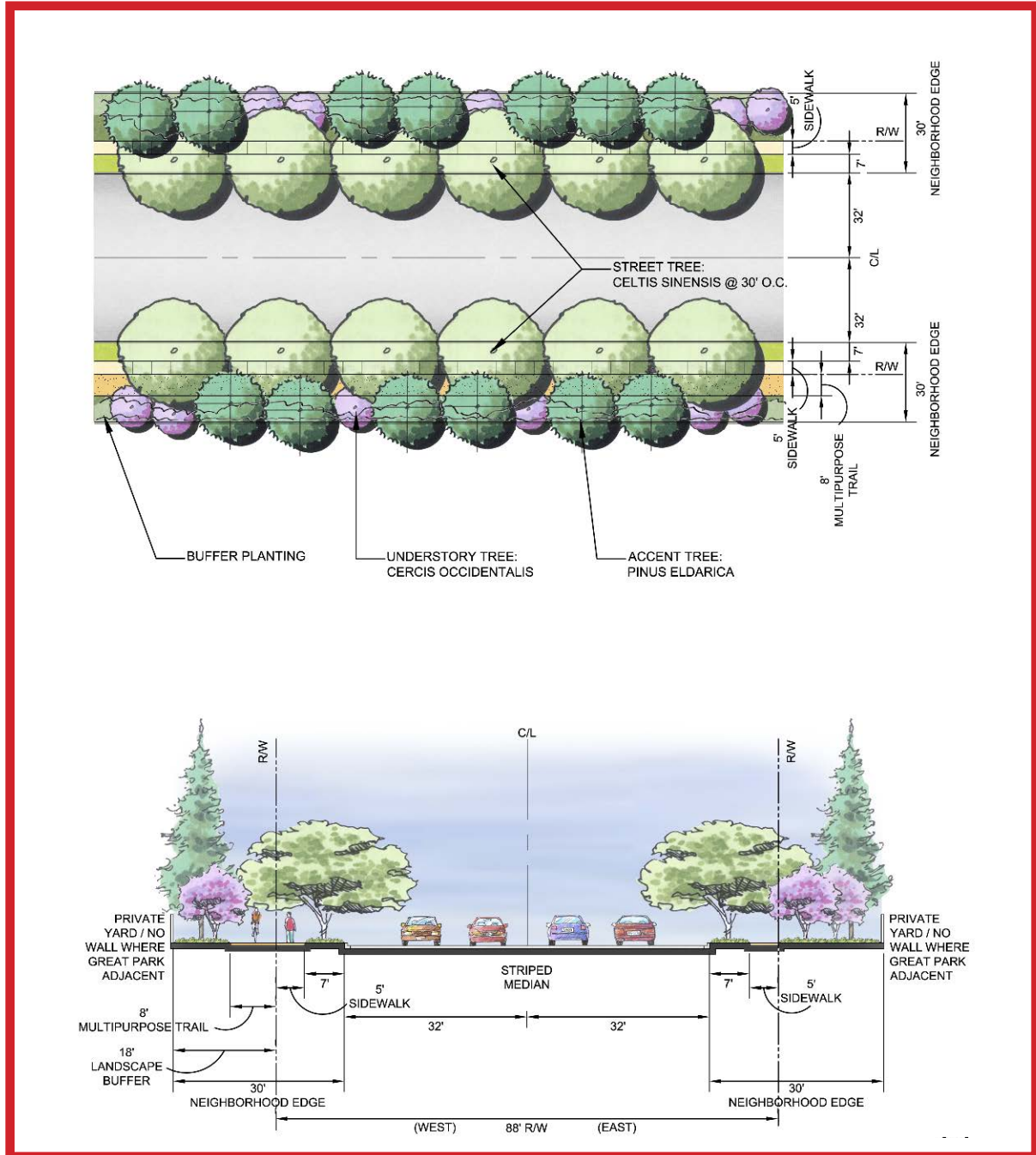
#### Primary Local Street

Where interior streetscapes interface with the Great Park and open space, special consideration should be taken to integrate pedestrian circulation into these areas via a street side pedestrian paseo system that links sidewalks to active walking trails and open space uses.

- A landscaped parkway (7-foot wide minimum) to accommodate shrubs and ground cover, along both sides of the street;
- A lineal sidewalk (5-foot wide) along both sides of the street that makes additional connections to park and paseo walkways;
- Refer to streetscape section Exhibit 7-8: Primary Local Street for illustration.



# Parkside



Parkway trees shall match the Streetscape Master Plan.

Exhibit 7-7: Hellman Ave. Streetscape



# Parkside

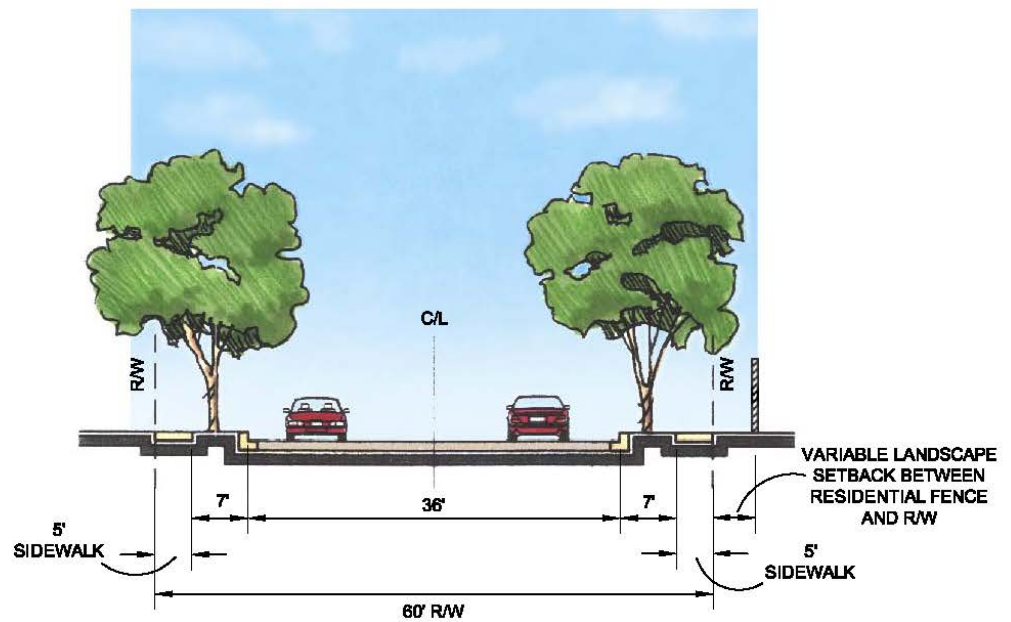
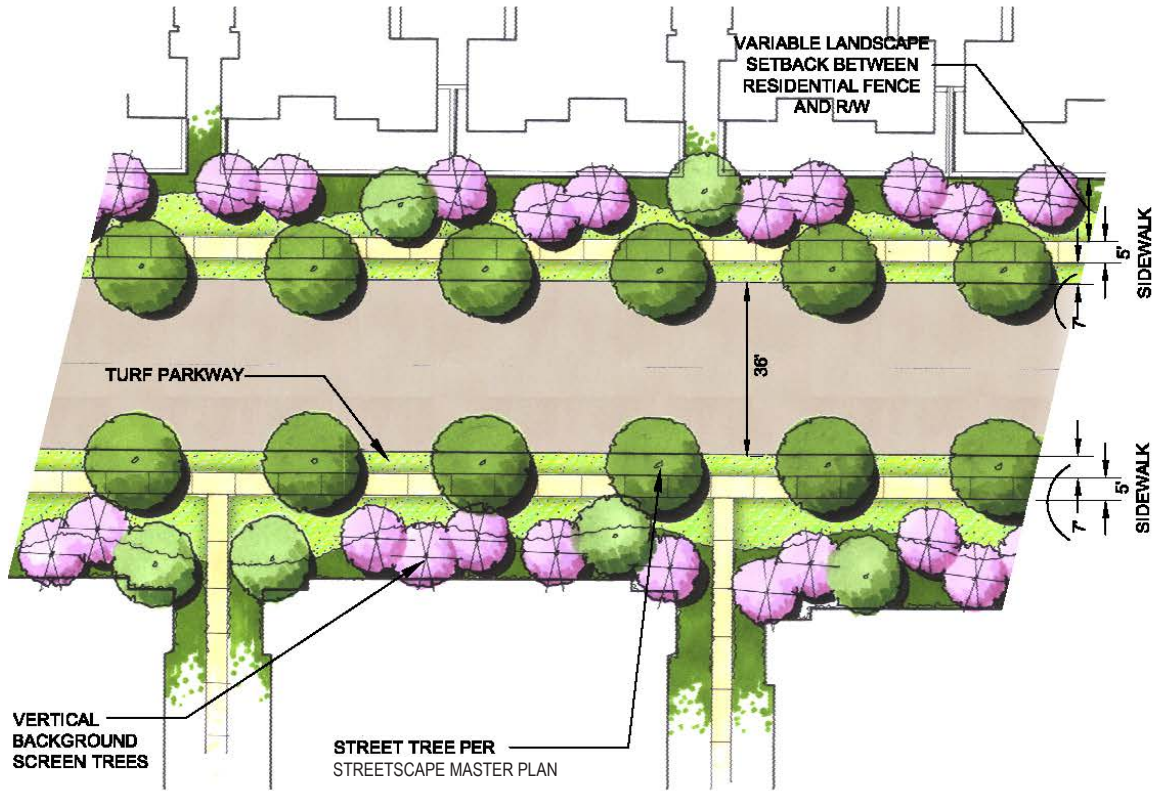
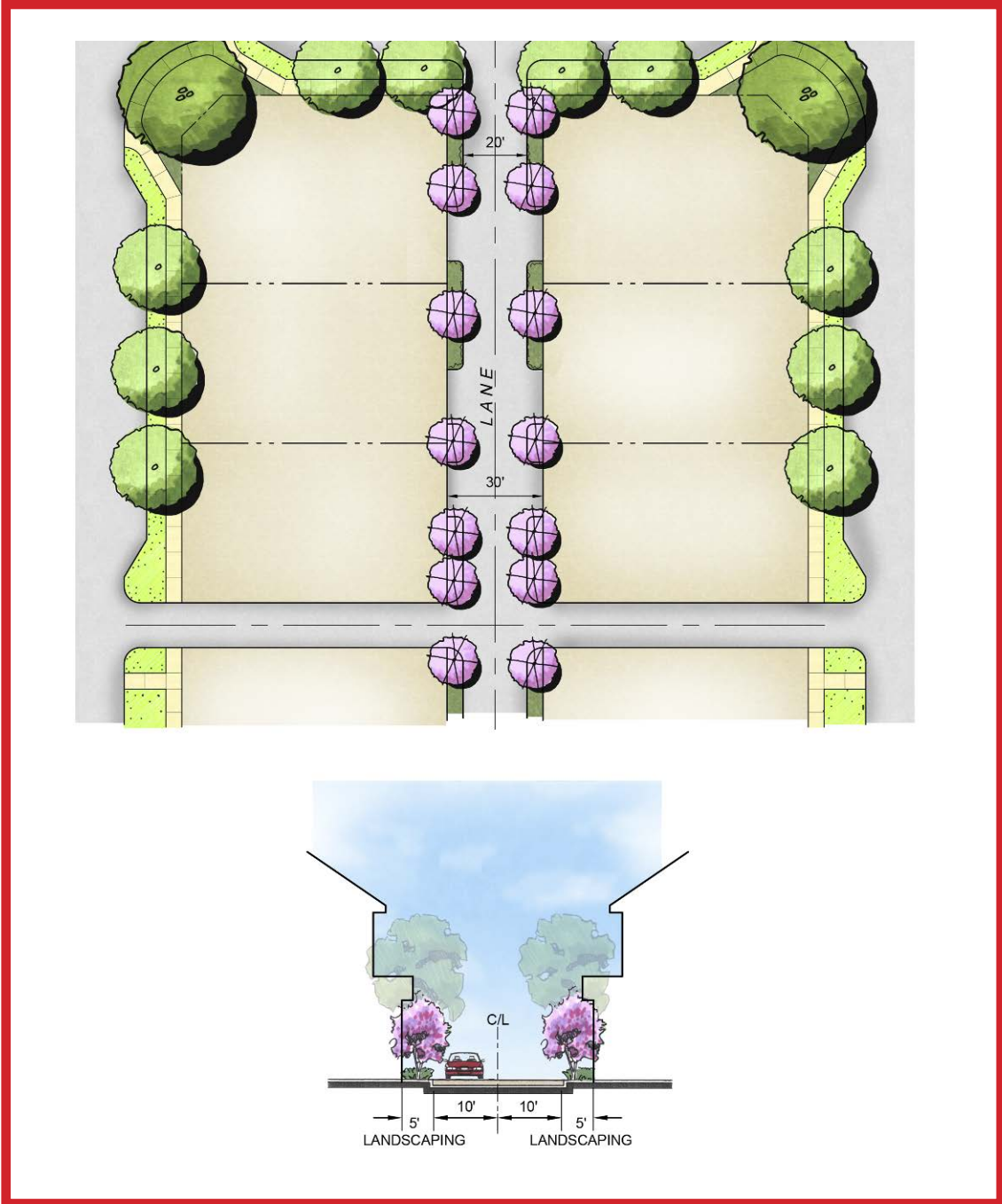


Exhibit 7-8: Primary Local Street



# Parkside



## Lanes (Private Alleys)

20' Paved Section / 30' Width, Garage to Garage

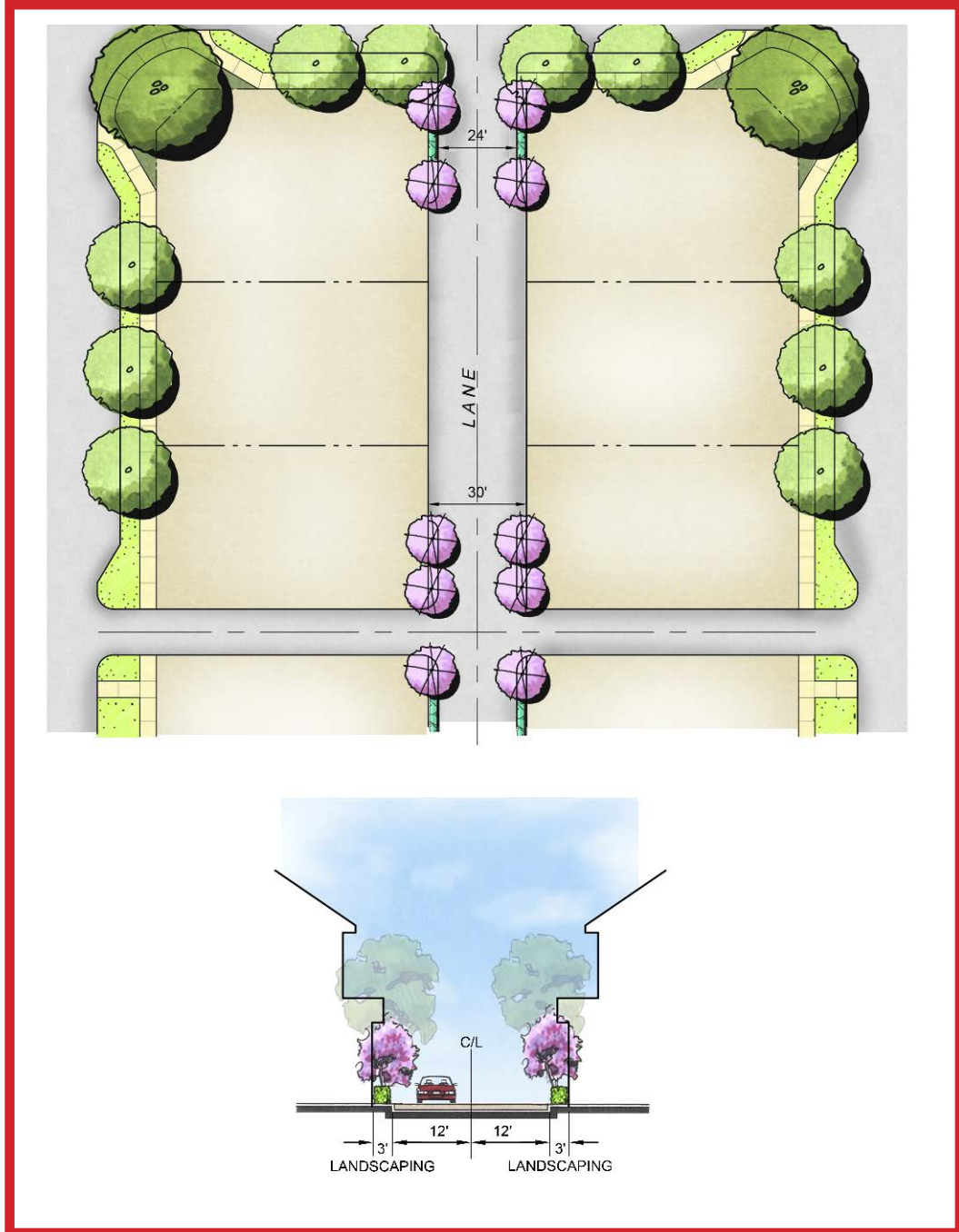
Note: Lane (private alley) paving may increase to 24' if required by the City Fire Department

Exhibit 7-9: Conceptual Lane (Private Alley) Streetscape





# Parkside



Lanes (Private Alleys)

24' Paved Section / 30' Width, Garage to Garage

Exhibit 7-10: Conceptual Lane (Private Alley) Streetscape



# Parkside

## 7.7.5 Lanes (Private Alleys)

Lanes (private alleys) proposed in **detached**, cluster, attached, and multi-family neighborhoods will be landscaped with smaller scale trees, shrubs, vines, and groundcover along pavement edges, to help add a pedestrian scale to the streetscene. Refer to Exhibits 7-9 & 7-10: Conceptual Lane (Private Alley) Streetscape.

## 7.7.6 Entries and Monumentation

Monumentation occurs throughout the Parkside Specific Plan community and is designed to establish a **basic** hierarchy for entering each area of the community. Along the perimeter edges there are several entry points into the community. At key entries, a landscape and monumentation program will be utilized to help identify the community as well as convey a “welcoming” feeling for both vehicular and pedestrian traffic. **Four basic Five entry** and monument treatments are used to set this hierarchy: Community Gateway Monuments, **Primary Community Gateway Entries**, **Major Community Gateway Entries**, **the Neighborhood Entry Secondary Community Entries**, and **Secondary Community Entry Monuments**. Refer to Exhibit 7-11: Community Gateway Monument Plan for illustration of locations.

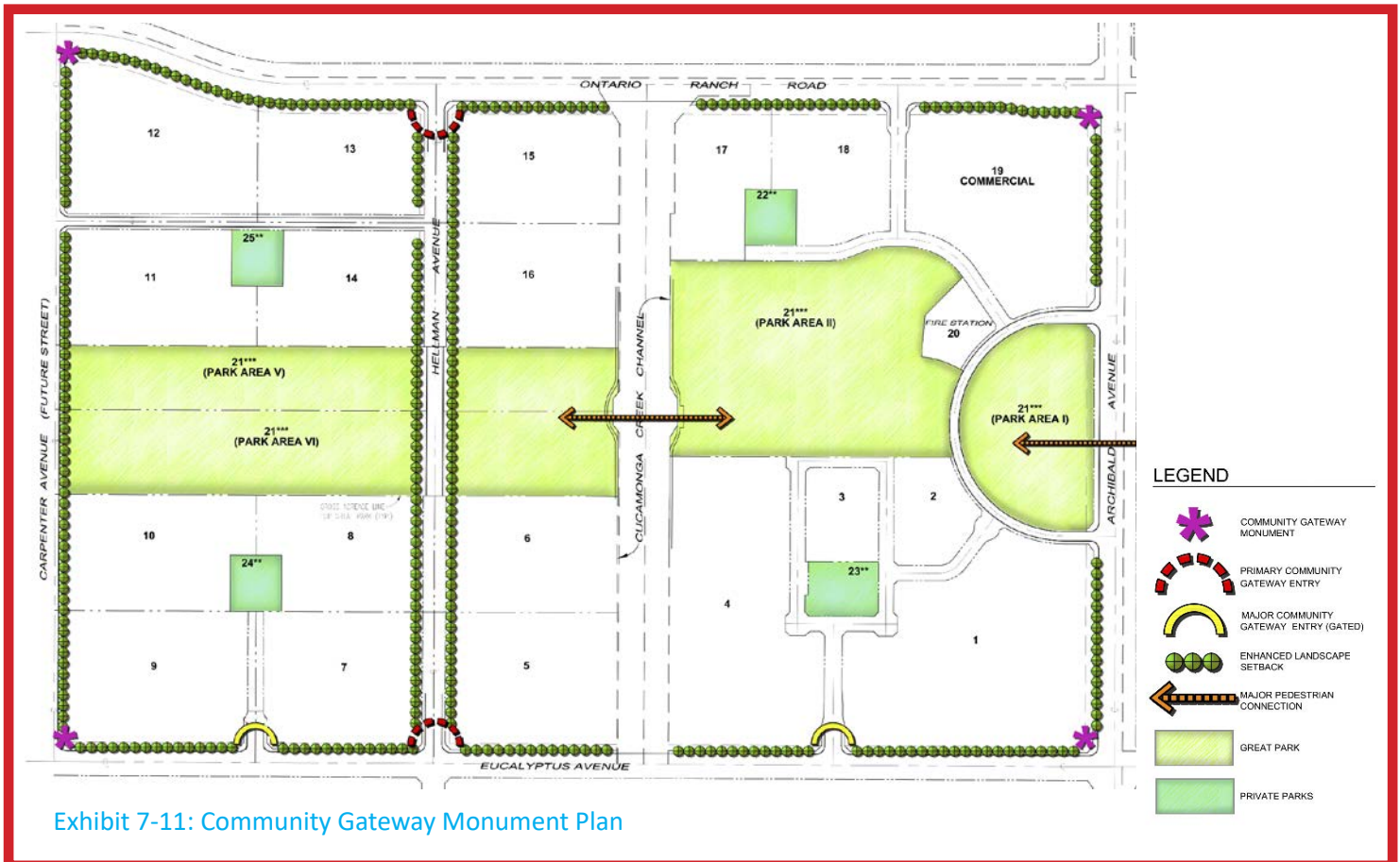


Exhibit 7-11: Community Gateway Monument Plan



# Parkside

## Community Gateway Monuments

The community of Parkside has a strong relationship with the "Great Park." The layout of the community planning areas completely embraces and encompasses the western section of the "Great Park."

As an example of this strong relationship between the community and the Park is the community's Gateway Monuments (Exhibit 20). Here the project's identity will

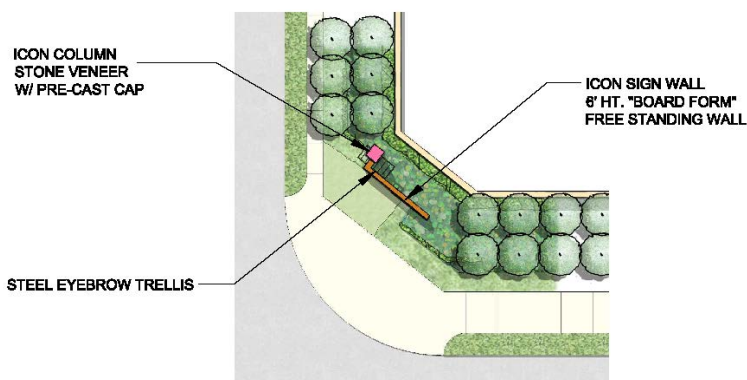
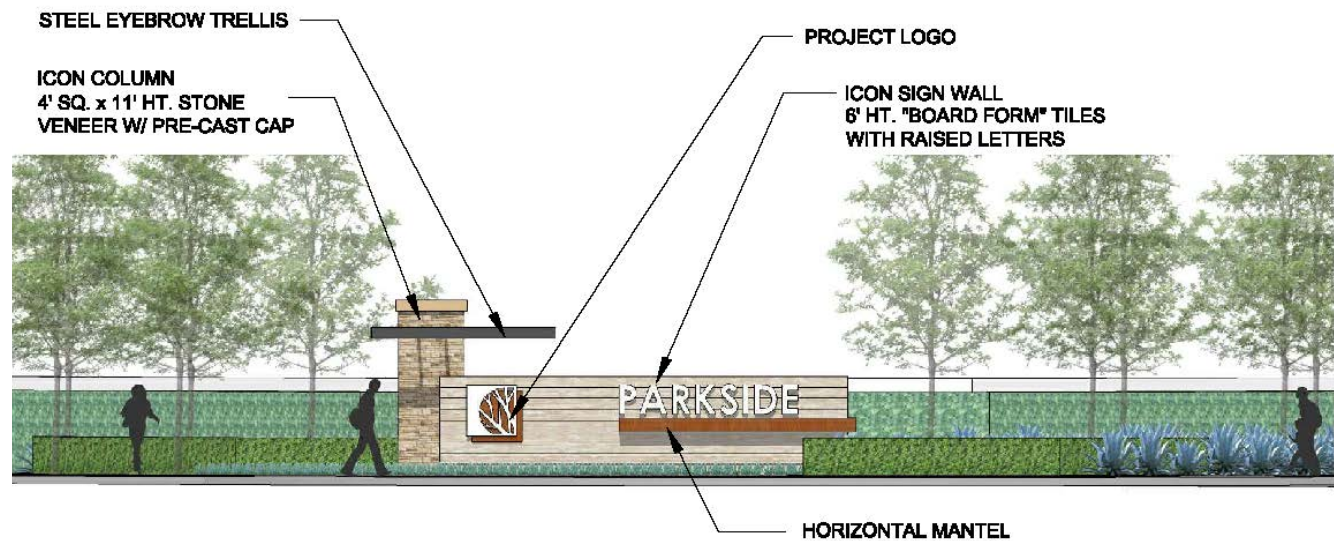
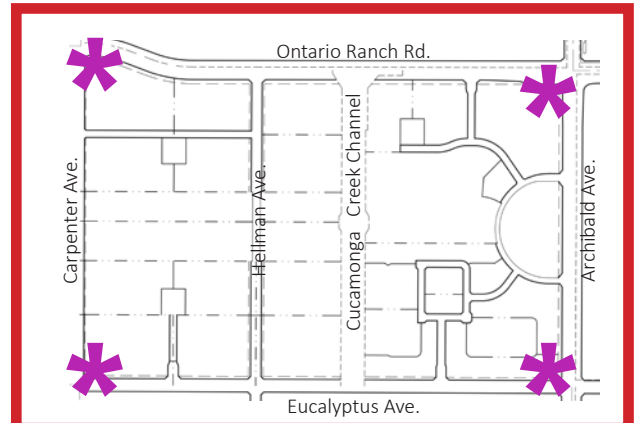


Exhibit 7-12: Community Gateway Monument Concept



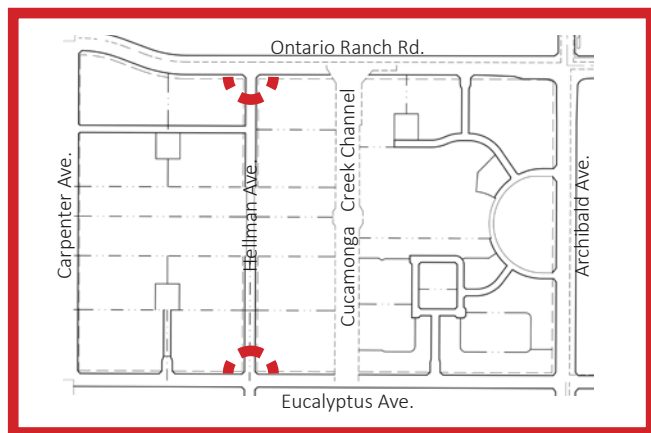
# Parkside

be refined by adopting monument elements similarly found in the Great Park and bring it out to the identity will be refined by adopting monument elements similarly found in the Great Park and bring it out to the defined at the four corners of the community. Here these Specific Plan area. These four icons or “landmarks” will announce the Parkside community as somewhere truly special. Refer to Exhibit 7-12: Community Gateway Monument Concept for illustration.

## Primary Community Gateway Entry

Parkside will have six three primary entries located at the major entry points into this community. Since two of these entries are shared points with the Great Park, the key design elements between the two should be continued and reinforced.

- A scaled-down version of the Community Gateway or park Monument shall be incorporated into the



primary entry statement and located to create a symmetrical formal design;

- The central primary statement shall be back dropped by an elegant 7-foot high curved stucco, brick, stone, or decorative split face block wall. The end of the wall shall be anchored by large, decorative pilasters an icon column;
- Community identification opportunities are located within the base of the primary statement or within a plaque inlaid into the anchoring pilasters columns;
- Architectural concrete caps, columns, trims, and bases help delineate each veneered material used;
- Use of “real” veneer material instead of faux concrete veneers;
- A linear row of vertical specimen screen trees shall be located behind the brick, stone, or decorative split face block wall, and in front of the community perimeter wall;
- Strong use of seasonal perennial and annuals providing for seasonal flowering and interest is encouraged;
- Enhanced pedestrian paving at street crossing and at monument location;
- Accent trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color;
- Accent and up-lighting of landscape/monumentation; and
- Refer to Exhibit 7-13: Primary Community Gateway Entry Concept for illustration.



# Parkside

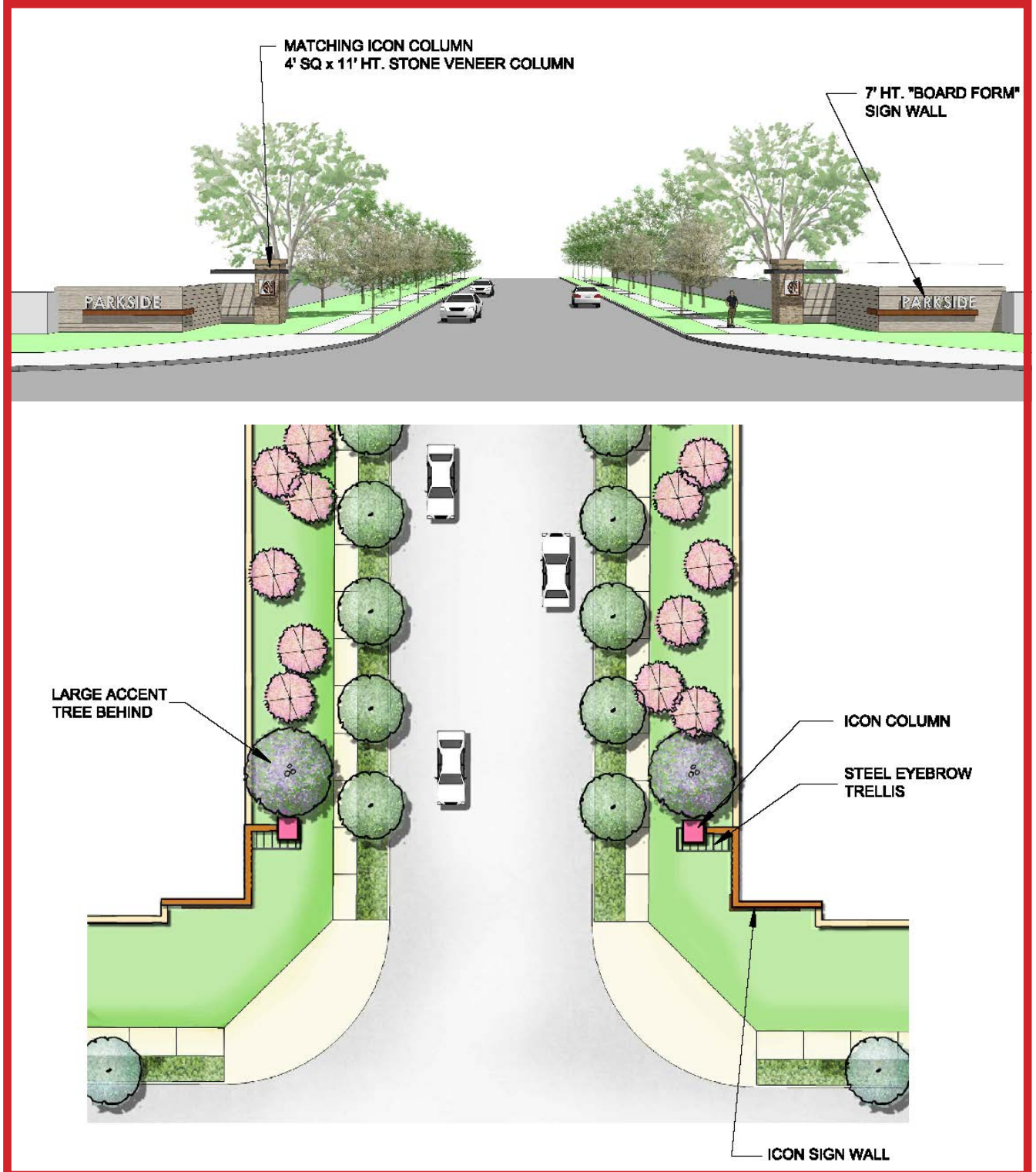


Exhibit 7-13: Primary Community Gateway Entry Concept



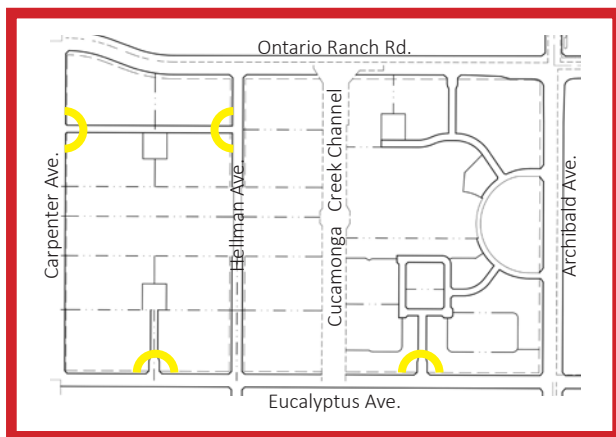
# Parkside

## Major Community Gateway (Gated)

Parkside will have 3 of these types of entry points. These Gateways will echo the same aesthetic as the Community Gateway Monuments located at the four corners of the Specific Plan boundary.

- A scaled-down version of the Community Gateway Monument shall be incorporated into this entry statement and located to create a symmetrical formal design;
- The central primary statement shall be back dropped by an elegant 7-foot high stucco, brick, stone, or decorative split face block wall ;
- Community identification opportunities are located on the monument wall;
- Architectural concrete caps, columns, trims, and bases may be used;

- Vehicular gates shall complement the style of the wall and be consistent with the other community monument characteristics. Gates shall be fully automatic with a directory system and set back a minimum of 100 feet from a public street intersection. Emergency access provided per local agency requirements;
- A linear row of canopy trees shall be located along the entry road leading up to the gate as well as in the median, if a median occurs;
- Strong use of seasonal perennial and annuals providing for seasonal flowering and interest is encouraged;
- Accent trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color;
- Accent and up-lighting of landscape/monumentation; and
- Refer to Exhibit 7-14: Major Community Gateway (Gated) Concept for illustration.



## Secondary Community Entry and Monumentation

The Secondary Community Entry and Monumentation shall include the following:

- Similar in design as the Primary Entries but reduced in scale;
- Freestanding curved walls at each corner with anchoring entry pilasters. The large, elegant entry pilasters shall be brick, stone, or decorative split face block and located adjacent to right-of-way;



# Parkside

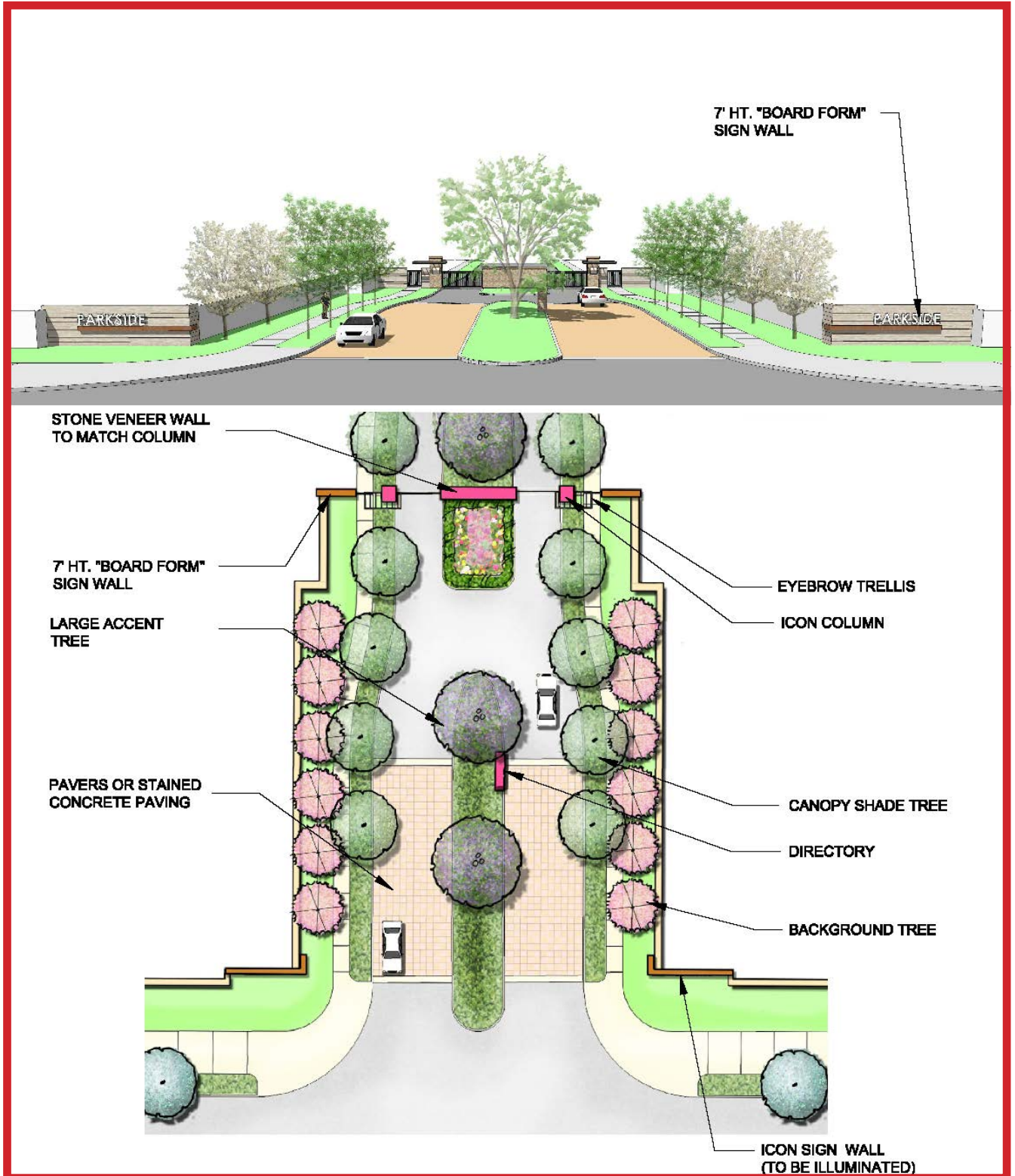


Exhibit 7-14: Major Community Gateway (Gated) Concept



# Parkside

- Identification field for potential sign lettering placement shall be on a plaque inlayed into the two anchoring pilasters;
- Architectural concrete caps, trim, and bases to help delineate each veneered material used;
- Use of “real” veneer materials instead of faux concrete veneers;
- Use of large specimen native trees to flank each side of entry drive at site entry;
- Seasonal perennial flowering to allow for seasonal flowering interest throughout the year;
- Enhanced pedestrian paving at street crossing and at monument location;
- Accent trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color;
- Accent and up-lighting of landscape/monumentation; and

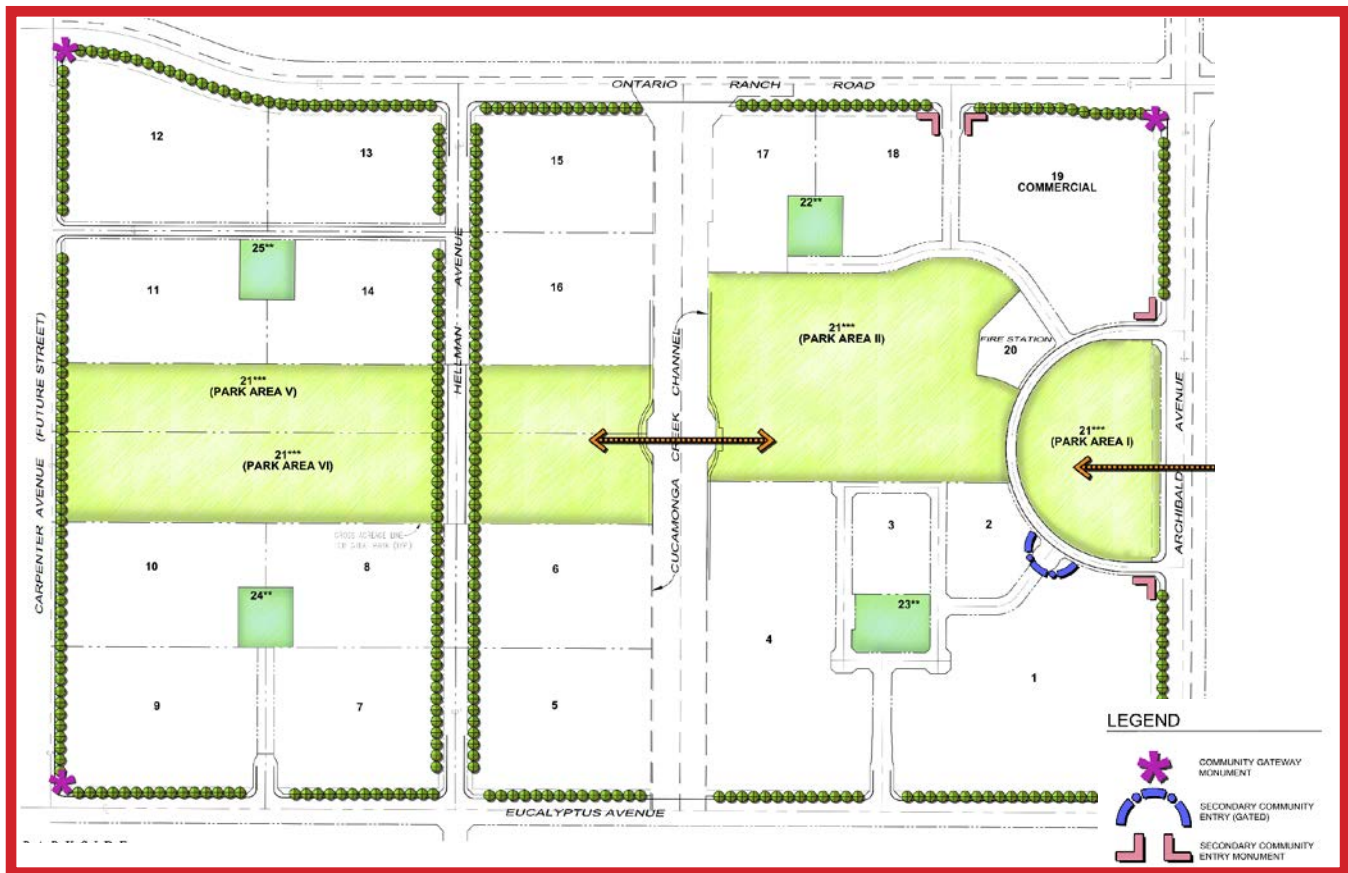


Exhibit 7-15: Secondary Community Entry Monument Plan





# Parkside

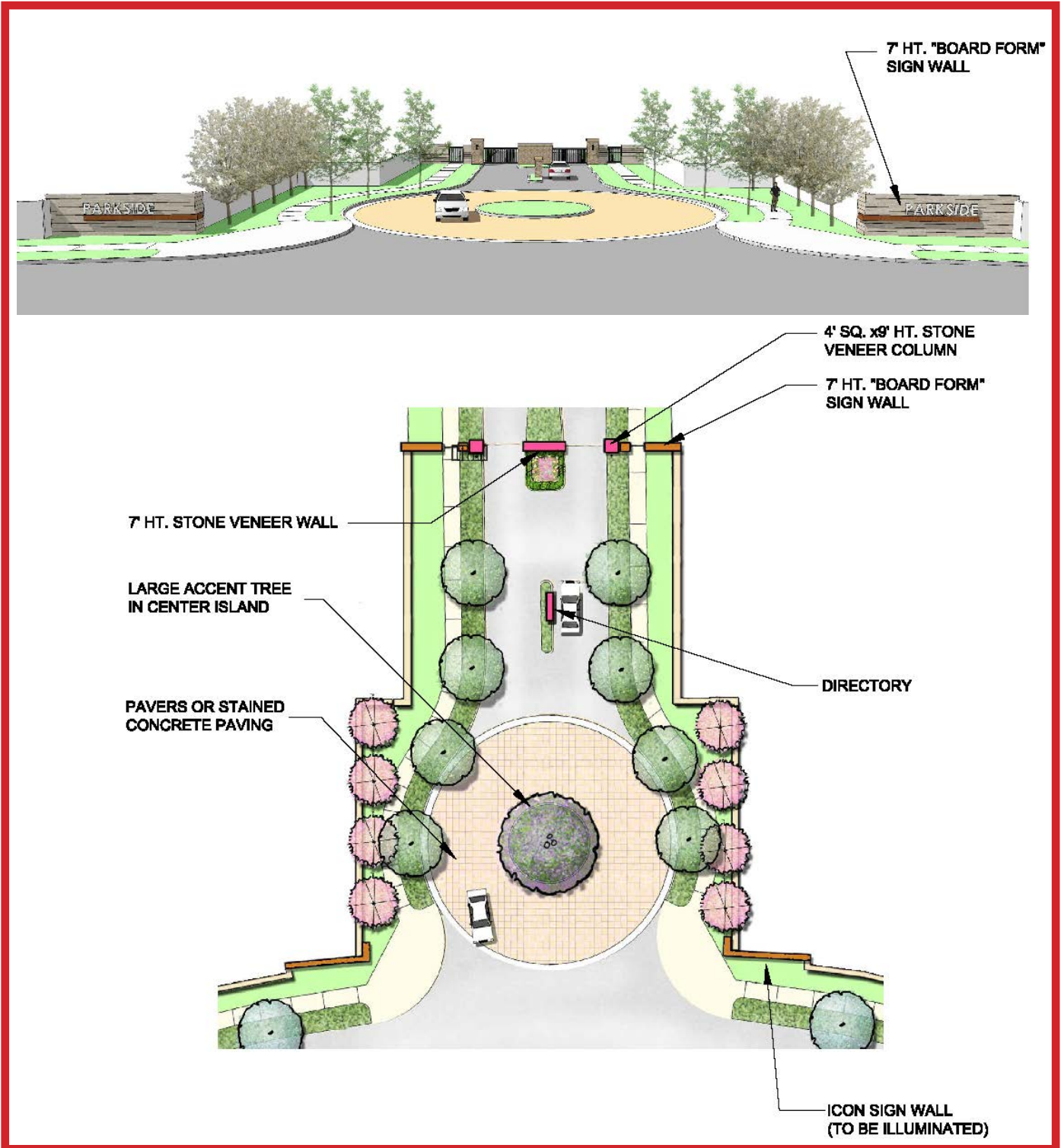


Exhibit 7-16: Secondary Community Entry (Gated) Concept



# Parkside

• Refer to Exhibit 20, Conceptual Landscape Master Plan and Exhibit 30, Secondary Community Gateway and Gated Entry Monument Plan for detailed conceptual illustration, and Exhibit 30a, Secondary Community Gateway Entry.

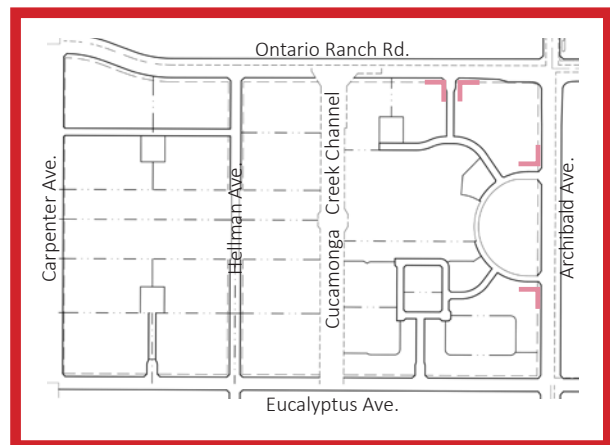
## Secondary Community Entry (Gated)

Parkside will have one gated Secondary Community Entry. This entry will combine several neighborhoods together behind a grand gated entry statement. Beyond these gates each neighborhood would still have its own individual (non-gated) entry. The Secondary Community Gated-This Entry will have the same hierarchy and stature as the other Secondary Community Entry and Monumentation sharing most of the same similar elements and features. Location and final configuration of the Secondary Community Gated Entries shall be reviewed and approved by the City as the other listed entries and monumentation.

The Secondary Community Entry (Gated) shall include the following:

- A large spacious motor court A turnaround with decorative paving shall be provided and sized to accommodate required turn-around clearance and predicted volume of cars. At the center of the motor court, a large heritage tree shall be located in a raised island;
- Two (2) large, elegant entry pilasters Entry sign wall shall be located behind curb announcing entry into the widened motor court. These pilasters turnaround. This wall shall be constructed of stucco, brick, stone, or decorative split face block;

- Identification field for potential sign lettering placement shall may be on a plaque inlayed into the two entry pilasters sign wall;
- Walls enclosing the motor court turnaround and anchoring pilasters columns and wall at the vehicle gates shall be stucco, brick, stone, or decorative split face block with architectural concrete caps, trim and base to help delineate each veneered material used;
- A strong formal linear row of trees is encouraged for the entries entry drive and to frame to motor court;
- Vehicular gates shall be decorative and constructed complement the style of tubular steel the wall and be consistent with the other community monument characteristics. Gates shall be fully automatic with a directory system and set back a minimum of 100 feet from a public street intersection. Gates shall be fully automatic with a "state-of-the-art" directory system. Emergency access provided per local agency requirements;





# Parkside

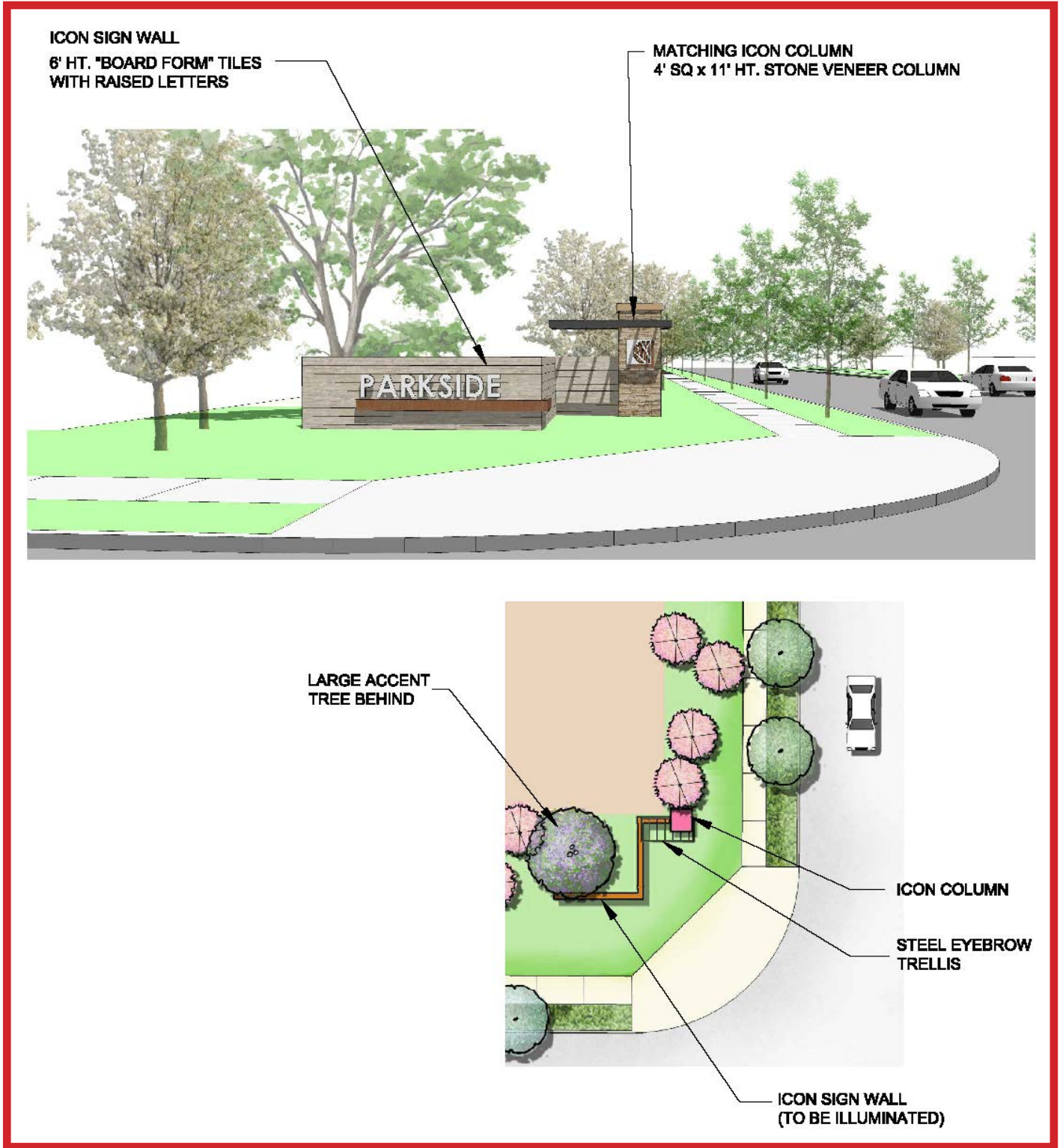


Exhibit 7-17: Secondary Community Entry Monument Concept



# Parkside

- Use of “real” veneer materials instead of faux concrete veneers;
  - Seasonal perennial flowering to allow for seasonal flowering interest throughout the year;
  - Accent trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color;
  - Accent and uplighting of landscape/monumentation; and
  - Refer to Exhibit 7-16: Secondary Community Entry (Gated) Concept for illustration.
- Neighborhood Entry**
- Neighborhood entries shall occur) Concept for each planning area within the Parkside Specific Plan area. These entries shall be used to help continue the landscape character theme to the

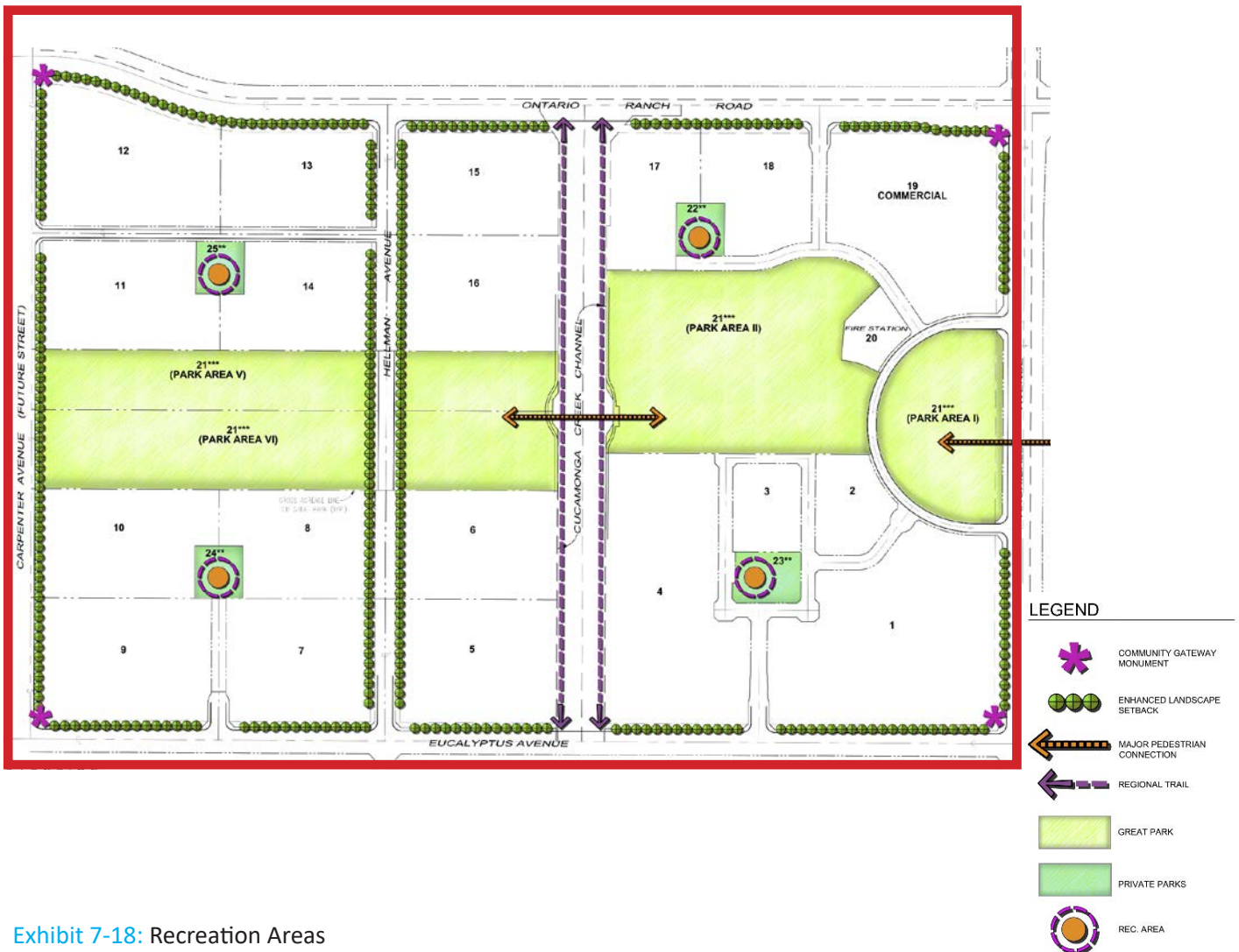


Exhibit 7-18: Recreation Areas



# Parkside

“core” of the community. Each neighborhood built within the project site will have the opportunity to identify their individual project character while providing the basic design features of the other monuments illustration:

- There are two (2) types of neighborhood entries: Gated and Non-gated. Selection of entry type will depend upon space available, local street configuration and residential product type. Location and final configuration of gated entries shall be reviewed and approved by the City.
- The Neighborhood Entry (Gated) shall include the following:
  - Decorative freestanding walls embodying the same character as that of the secondary entries. At the end of the walls, located just behind curb of the entry road, are two large pilasters. These pilasters support two (2) sets of swing gates;
  - Identification for the gate community is located within a plaque inlayed into both pilasters;
  - Vehicular gates shall be decorative and constructed of tubular steel and set back a minimum of 100 feet from a public street intersection. Gates shall be fully automatic with a “state-of-the-art” directory system. Emergency access provided per local agency requirements;
  - A large spacious motor court with decorative paving shall be provided and sized to accommodate required turn-around clearance. At the center of the motor court, a large Heritage Tree shall be located in an island;
  - Use of “real” veneer materials instead of faux concrete veneers;
  - A strong formal linear row of trees is encouraged for the entries drive and along the sides of the motor court;
  - Seasonal perennial flowering to allow for seasonal interest throughout the year; and
  - Refer to Exhibit 20, Conceptual Landscape Master Plan and Exhibit 31, Neighborhood Entry Monument Plan for detailed conceptual illustrations, and Exhibit 31b, Neighborhood Gated Entry Monument.
- The Neighborhood Entry (Non-gated) shall include the following:
  - Freestanding large entry pilaster set within the landscaped parkway. This pilaster should embody the same character as that of the Secondary Community Entry but is slightly

## Secondary Community Entry Monument

The Secondary Community Entry Monument shall include the following:

- Similar in design as the Primary Entries but reduced in scale to create a pedestrian “gateway” into each neighborhood;
- Freestanding walls at each corner with anchoring entry columns. The columns shall be stucco, brick, stone, or decorative split face block and located adjacent to right-of-way;
- Identification field for potential sign lettering placement within a plaque inlayed into pilasters shall be located on the sign wall;



# Parkside

- Architectural concrete caps, trim, and bases to help delineate each veneered material used may be used as well as an eyebrow trellis connected to the icon column;
- Enhancement of corner cut wall and use of accent pilasters Use of large specimen native trees to anchor flank each side of entry drive at site entry;
- Use of “real” veneer materials instead of faux concrete veneers;
- Seasonal perennial flowering to allow for seasonal flowering interest throughout the year;
- Enhanced pedestrian paving at street crossing and at monument location;
- Accent trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color;
- Accent and up-lighting of landscape/monumentation; and
- Refer to Exhibit 7-17: Secondary Community Entry Monument Concept for illustration.



Exhibit 7-19: Pocket Park Concept A



# Parkside



Exhibit 7-20: Pocket Park A Rendering

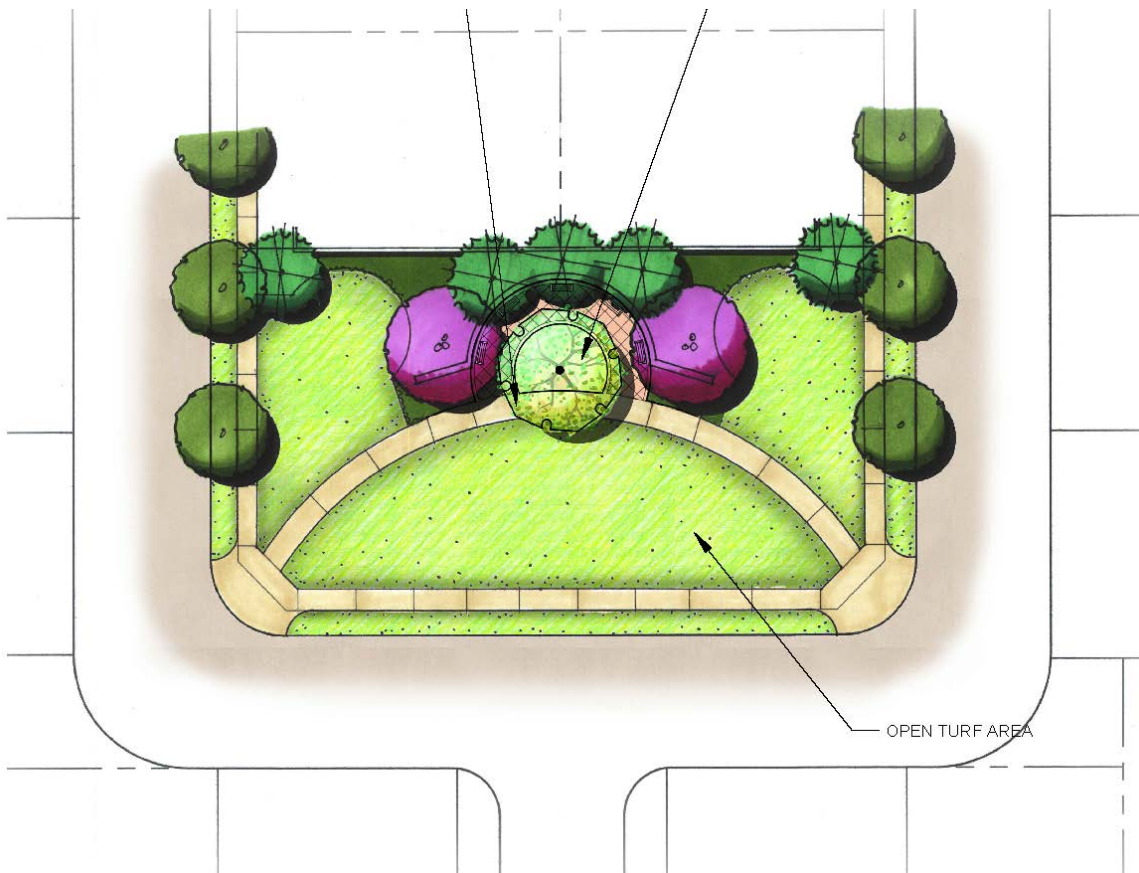


Exhibit 7-21: Pocket Park Concept B



# Parkside

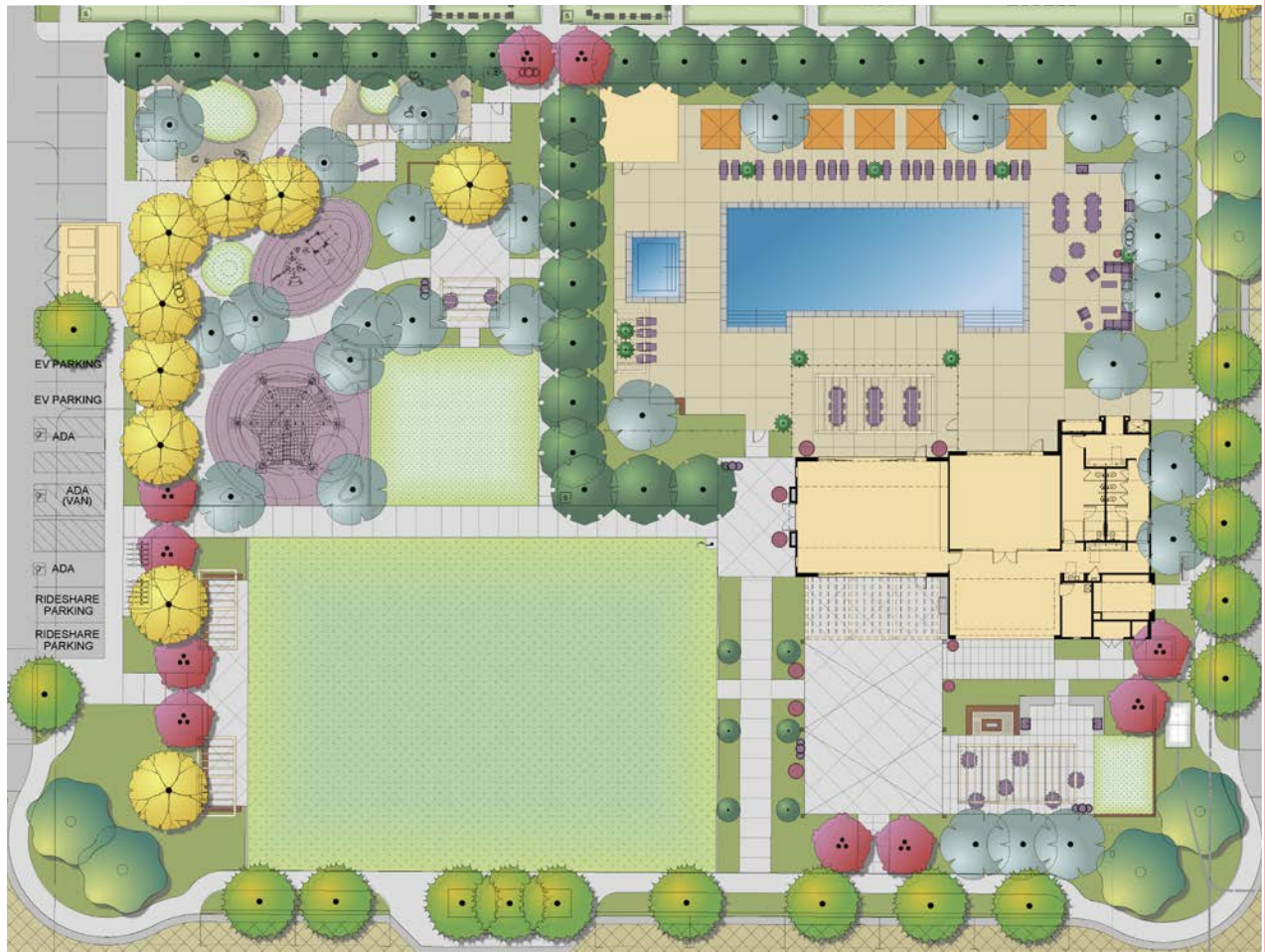
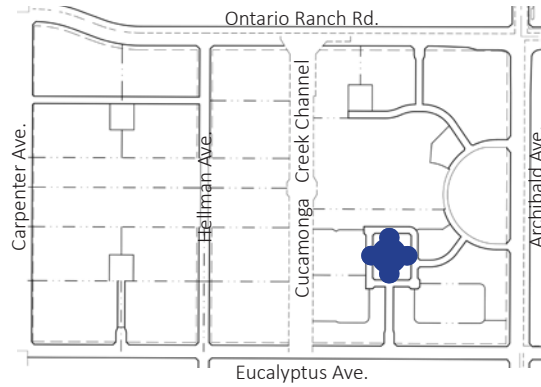


Exhibit 7-22: Recreation Area Concept A





# Parkside



Exhibit 7-23: Recreation Area Concept B



# Parkside

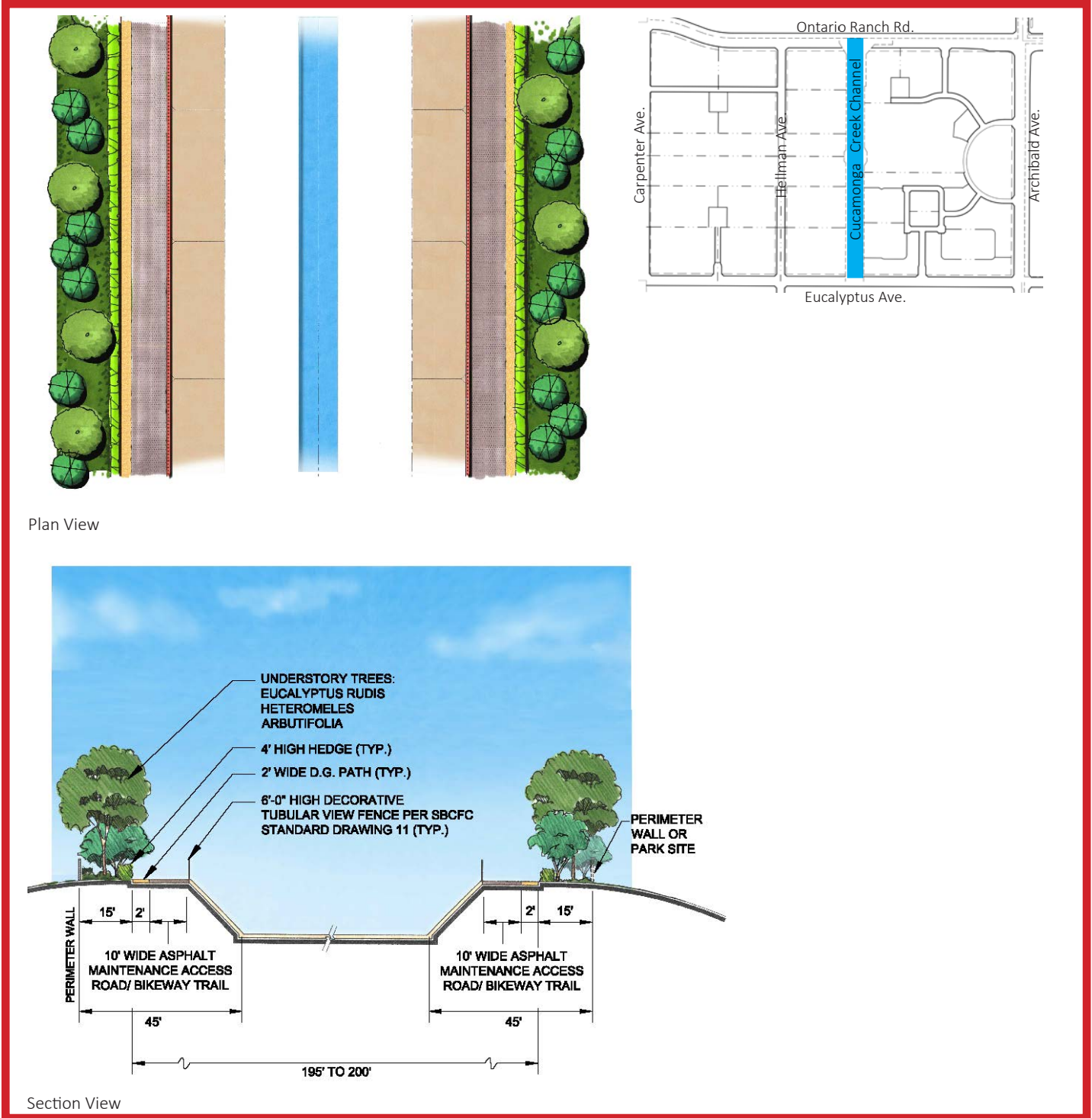


Exhibit 7-24: Cucamonga Creek Channel Regional Trail Easement



# Parkside

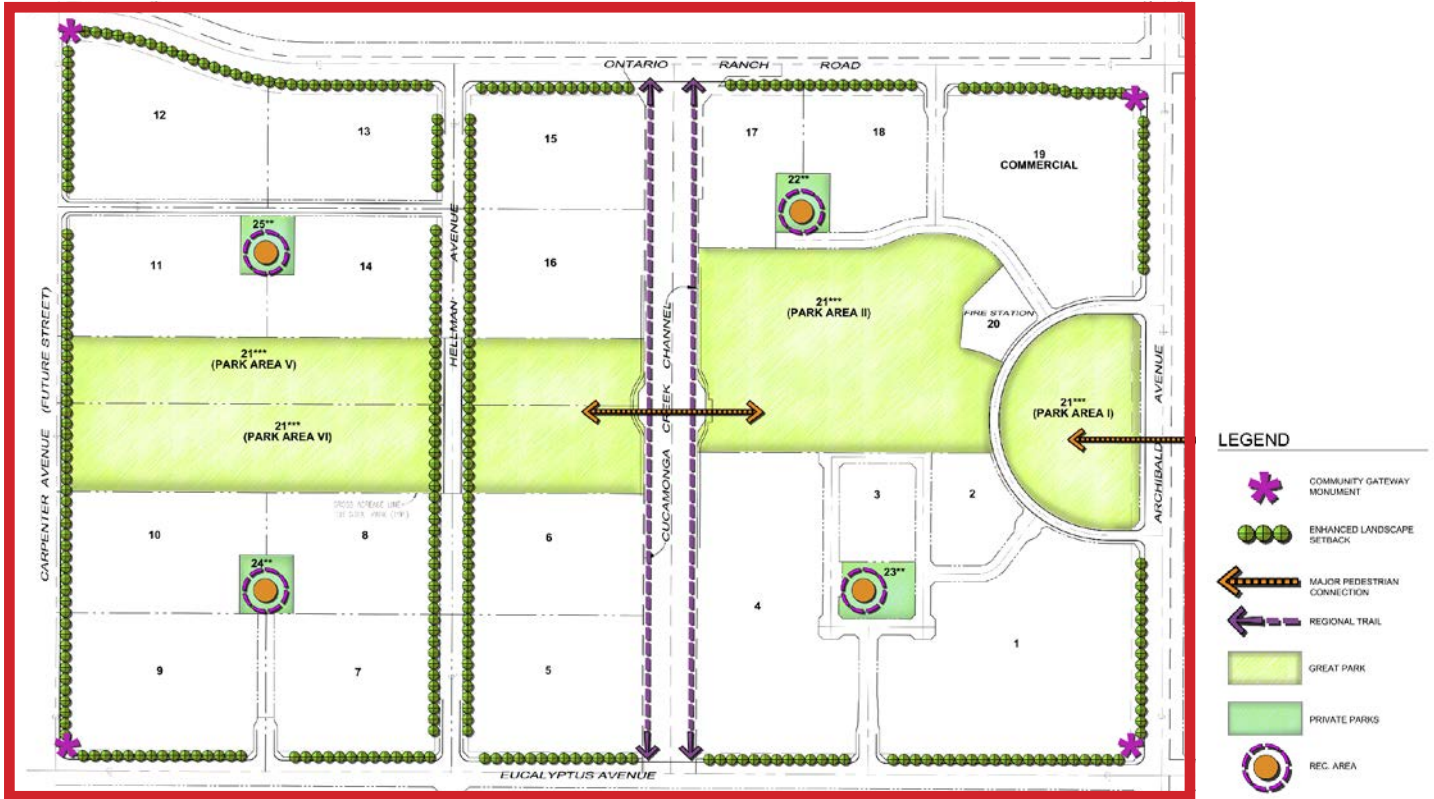


Exhibit 7-25: Channel Regional Trail Easement Plan

## 7.7.7 Parks and Open Space

The Parkside Specific Plan area will have a central Great Park green core that consists primarily of open space for recreation and passive use. Exterior walkways and paseos will lead to this centralized area. Refer to Exhibit 7-18: Recreation Areas for illustration.

The following areas make up the remainder of the Park and Open Space System for Parkside: private community recreation and the paseo trail system.

### A. The Great Park

The Great Park consists of the following:

**Parking**—Parking shall be adequate to accommodate daily use of the park, and will be provided on adjacent streets or within the park area;

**Landscaping**—Landscaping within the Great Park shall harmonize with the surrounding streetscapes. Large specimen trees should be used within the open turf areas to help provide shade and screening of unwanted views. Accent trees should also be used at pedestrian entries for color and seasonal interest.

### B. Pocket Parks

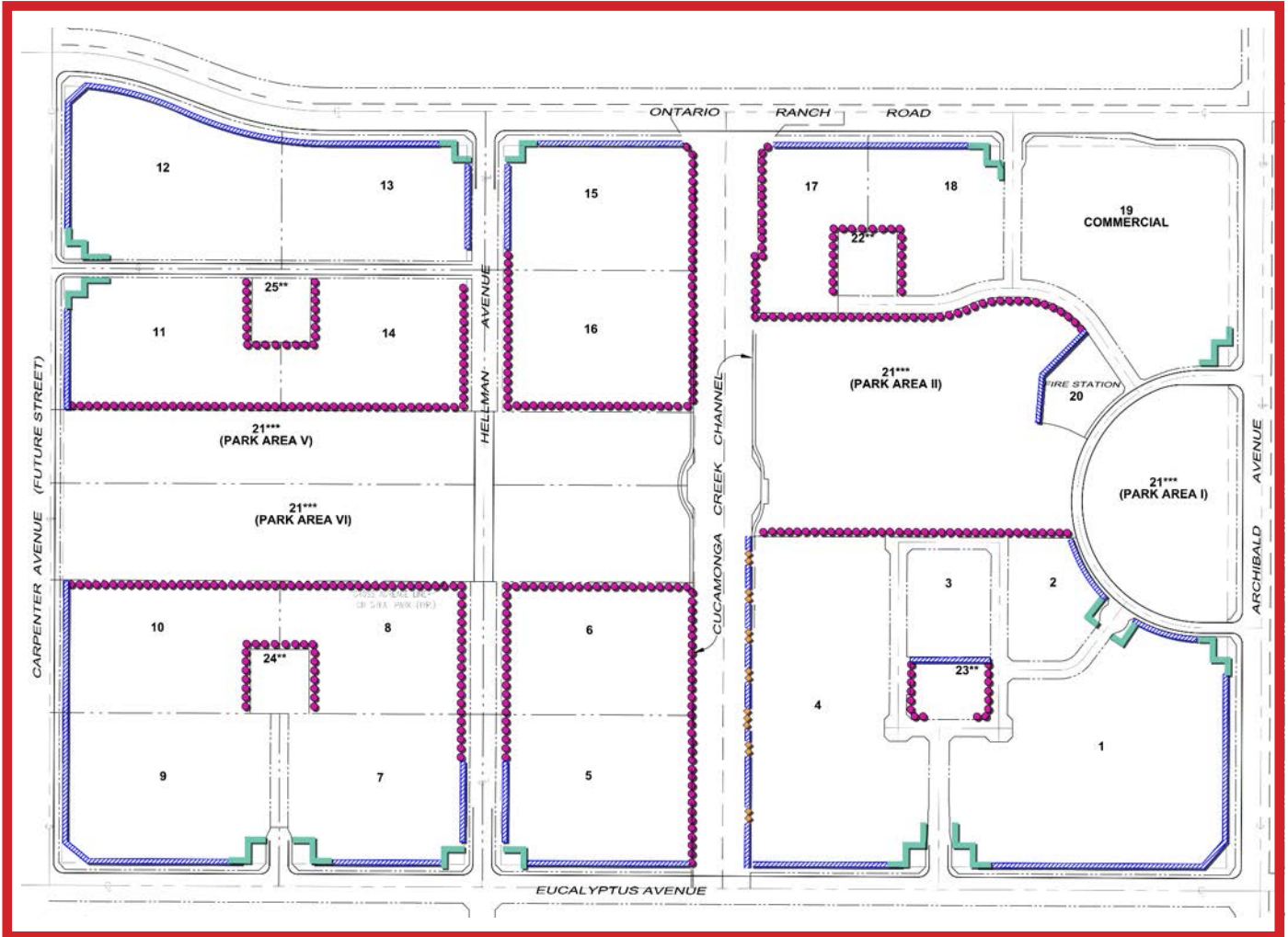
Pocket parks, if any, will consist of the following:

**Parking**—On-street parallel parking will be provided along local streets;

**Open turf**—Open turf, with bench seating, will be provided throughout the pocket parks. Lighting will be included;



# Parkside



## LEGEND





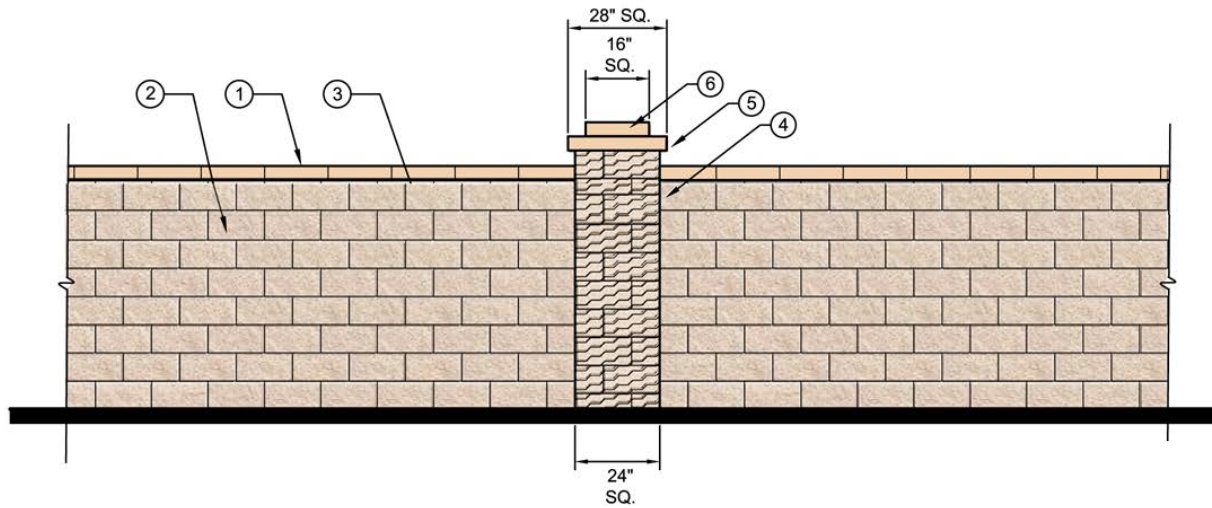
-  SPLIT FACE WALL
-  DECORATIVE ENTRY WALL
-  TUBULAR STEEL VIEW FENCE
-  TUBULAR STEEL VIEW FENCE OR COMBO BLOCK WALL W/ TUBULAR STEEL FENCE

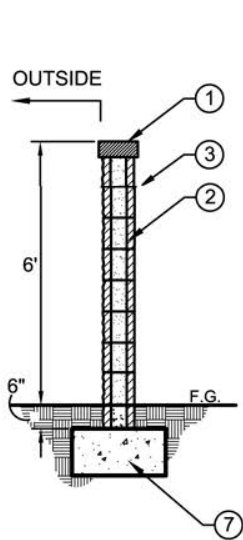
Exhibit 7-26: Fence/Wall Plan



# Parkside



COMMUNITY WALL: 6x8x16 SPLIT FACE  
(OUTSIDE FACING ONLY)  
COLOR: "CANYON BLUFF"  
FROM: ANGELUS BLOCK OR EQUAL CAP: 4" HIGH  
PRECISION CAP BY ANGELUS BLOCK OR EQUAL.  
COLOR TO BE: "HARVEST"



- ① 8X4X16 CMU PRECISION CAP BY ANGELUS BLOCK
- ② 6x8x16 SPLIT FACED BLOCK (OUTSIDE FACE ONLY) BY ANGELUS BLOCK
- ③ 8X4X16 CMU PRECISION BLOCK BY ANGELUS BLOCK.
- ④ PILASTER TO BE CONSTRUCTED FROM SPLIT FACE BLOCK, RIVER ROCK, LEDGE STONE, FLAG STONE, BRICK OR OTHER DECORATIVE MATERIAL APPROVED BY THE CITY. LOCATE AT CORNERS & MAX 150' O.C.
- ⑤ 4" HIGH PRE-CAST CAP SMOOTH FINISH, CUT TO FORM 28" SQ.
- ⑥ 4" HIGH x 16" SQ. PRE-CAST CAP SMOOTH FINISH
- ⑦ CONCRETE FOOTING

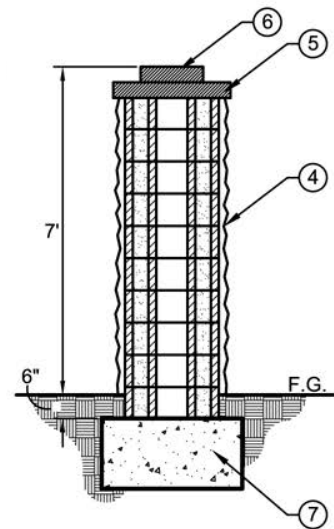
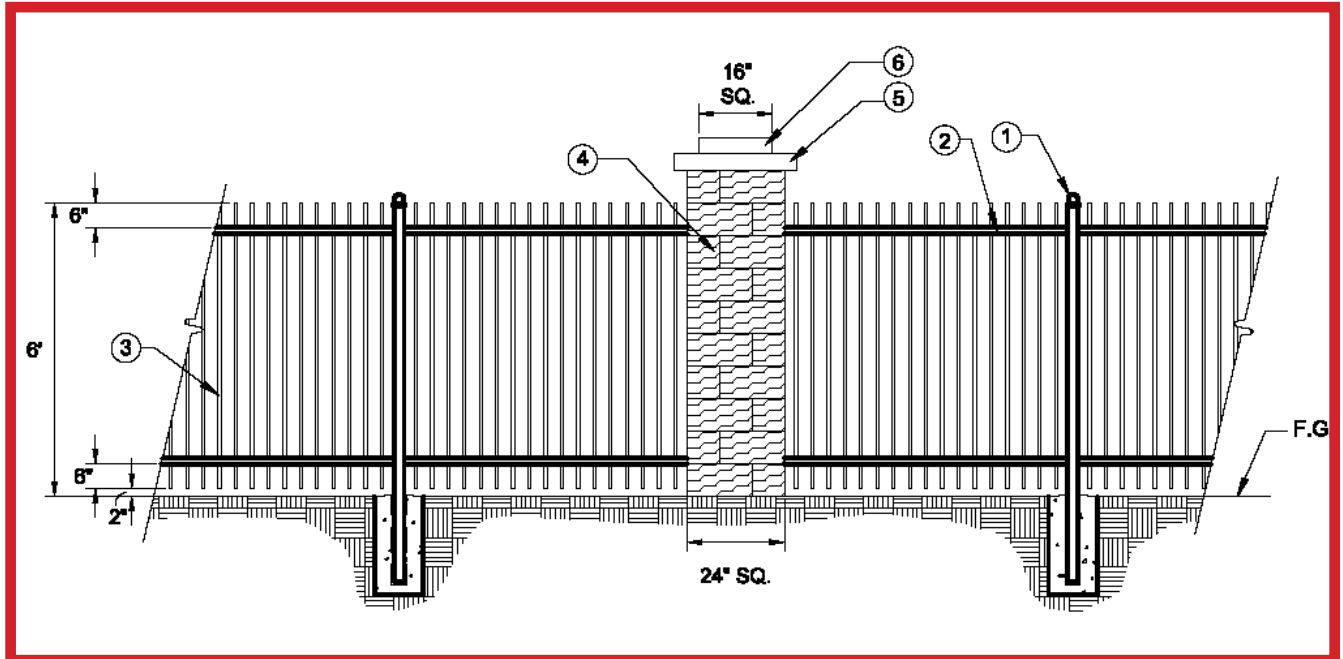


Exhibit 7-27: Wall Detail



# Parkside



- ① 2 1/2" SQ. TUBULAR STEEL POST W/DECORATIVE CAP
- ② 1 1/2" X 1" 12 GA. TUBULAR STEEL TOP & BOTTOM RAILS LAY FLAT
- ③ 1/2" SQ. X 16 GA. TUBULAR STEEL FENCE PICKETS @4" O.C. MAX.
- ④ PILASTER TO BE CONSTRUCTED FROM 8X8X16 COLORED SPLIT FACE BLOCK.
- ⑤ 4" HIGH PRE-CAST CAP SMOOTH FINISH, CUT TO FORM 28" SQ.
- ⑥ 4" HIGH X 16" SQ. PRE-CAST CAP SMOOTH FINISH

\*FENCE TO BE "GENESIS" BY AMERISTAR OR EQUAL.

Exhibit 7-28: View Fence Detail



# Parkside

- ① 8X4X16 CMU PRECISION CAP BY ANGELUS BLOCK
- ② 6X8X16 SPLIT FACED BLOCK (OUTSIDE FACE ONLY) BY ANGELUS BLOCK
- ③ PILASTER TO BE CONSTRUCTED FROM SPLIT FACE BLOCK, RIVER ROCK, LEDGE STONE, FLAG STONE, BRICK OR OTHER DECORATIVE MATERIAL APPROVED BY THE CITY. LOCATE AT CORNERS & MAX 150' O.C.
- ④ 4" HIGH PRE-CAST CAP SMOOTH FINISH, CUT TO FORM 28" SQ.
- ⑤ 4" HIGH x 16" SQ. PRE-CAST CAP SMOOTH FINISH
- ⑥ CONCRETE FOOTING

- ① 2X7 TOP & BOTTOM RAIL
- ② POST CAP
- ③ 4X4 VINYL POST
- ④ 1X6 VINYL PANEL

NOTE:  
 PRIVACY VINYL FENCE IS ONLY PERMITTED WITHIN THE INTERIOR WALLS/FENCES FOR PLANNING AREAS 1-4

COMMUNITY WALL: 6x8x16 SPLIT FACE (OUTSIDE FACING ONLY)  
 COLOR: "CANYON BLUFF"  
 FROM: ANGELUS BLOCK OR EQUAL CAP: 4" HIGH PRECISION CAP BY ANGELUS BLOCK OR EQUAL.  
 COLOR TO BE: "HARVEST"

**Exhibit 7-29: Combo Wall Detail**

**Exhibit 7-30: Privacy Vinyl Wall Detail**



# Parkside

**Focal Points**—Pocket parks may include focal point elements, such as tot lots, fountains, or small gazebos, depending on the size of the tot lot and orientation in the neighborhood:

- Pocket Park tot lots shall contain ~~two~~ either a play ~~areas-constructed-one~~ area for younger children (2-5 years old) ~~and-one~~ or for older children (5-12 years old).

Refer to Exhibit 7-19: Pocket Park Concept A, Exhibit 7-21: Pocket Park Concept B and Exhibit 7-20: Pocket Park A Rendering for detailed conceptual illustrations of proposed Pocket Parks in Parkside.

## 7.7.8 Recreation Area Plan

The Recreation Area Plan for Parkside includes locations for private recreation facilities in local neighborhoods. These recreation areas may include the following:

**Parking**—Parking adequate to accommodate daily use of the recreation areas;

**Recreation Building**—A recreation building should adhere to the architectural design guidelines and should be designed to accommodate the community needs;

**Pool and Spa Facility**—Each neighborhood may have access to a pool (sized adequately to accommodate the needs of the neighborhood) as well as to serve as a summer leisure recreational amenity for adults and children of the community;

- The pool decking area should be sized to allow for lounge chairs and tables. A spa area separate from the pool may also be located within the recreation area. A group barbecue facility and shade structure

may be located within the pool deck area. The private recreational facilities shall be fenced and gated for resident entry only. Hours of operation will be enforced in an effort to avoid evening disturbance to adjacent neighborhoods;

Refer to Exhibit 7-22: Recreation Area Concept A and Exhibit 7-23: Recreation Area Concept B for detailed conceptual illustrations of proposed Recreation Areas in Parkside.

**Landscaping** —Landscaping within the Private recreation areas shall harmonize with the surrounding streetscapes and adjacent Great Park. Large specimen trees should be used within the open turf areas to help provide shade and screening of unwanted views. Accent trees should also be used at pedestrian entries and around the pool area for color and seasonal interest;

- A strong **visual** connection shall be created between the private recreation area and the ~~terminus of one of the four wings of the public Great Park. This connection will be in the form of sight lines, maintaining vistas surrounding neighborhoods; the mutual use of repetitive shade structures;~~ and

**Paseo walkway System**—A paseo walkway system will be designed to provide connections to adjacent neighborhoods, the Great Park, and commercial area.

Refer to Exhibit 7-19: Recreation Area Concept A, and Exhibit 7-21: Recreation Area Concept B.





# Parkside

## 7.7.9 Paseo Trail System

Pedestrian circulation is highly encouraged within the Parkside Specific Plan area. Landscape easements are provided along major roadways and are encouraged within the neighborhood communities.

**Paseo Trail System**—A Paseo walkway system will be designed to provide connections to adjacent neighborhoods, the recreation areas, the Great Park and commercial area. The overall trail system will include:

**Landscaping**—Landscaping along the trails shall harmonize with the surrounding streetscapes;

**Paving**—Meandering sidewalks are encouraged with benches located approximately every 500 lineal feet and enhanced paving at paseo connections where pedestrian circulation crosses roadways;

**Other Elements**—Other elements include:

- Light poles shall be located at every 100 feet along the path;
- Entry points shall have adequate directional signage;
- Provide formal pickup/drop off areas at major entry points;
- ~~Integration of well defined pedestrian connections to the “Great Park.” Refer to Exhibit 38, Trail Connection to “Great Park”;~~
- Integration of the Channel Regional Trails Plan. Refer to the detailed conceptual illustrations Exhibit 7-25: Channel Regional Trail Easement Plan for illustration;
- Integration of well- defined pedestrian connections to the Great Park. Refer to Exhibit 7-24: Trail Connection To Great Park; and

- Connection to recreation areas. Refer to the detailed conceptual illustrations. Refer to Exhibit 7-25: Interior Paseo Concept.

## 7.7.10 Community Walls and Fencing

Walls are a major component in achieving an overall community theme within the Parkside Specific Plan. A strong, cohesive appearance is achieved through the use of “community walls” and general overall wall guidelines.

All walls that adjoin community streetscenes and are along the boundary edges of the Great Park and facing the Cucamonga Creek Channel shall be deemed “community walls.” Non-climbing fencing shall be used around the Cucamonga Creek Channel to prevent children from climbing the fence to gain access. All wall and fencing designs and layout shall be approved by the City of Ontario prior to construction.

### Perimeter Walls and Fencing

Where common or private areas interface with perimeter streets, public open space and/or residential lots, a solid decorative theme wall, a tubular steel view fence with matching themed pilasters, and/or a combo wall shall be constructed. Brick Masonry walls or screening walls shall have climbing plants or anti-graffiti elements to prevent vandalism. Refer to Exhibit 7-26: Fence/Wall Plan and Exhibit 7-29: Combo Wall Detail.

For front yards facing the Great Park the same themed block or view fence shall be used between the structures buildings to secure the project behind. These wall/fences shall be located behind the building setback and arranged to create a project-wide unifying appearance.



# Parkside

## Interior Solid Walls and Fencing

Where privacy or protection of in-tract common and private area views dictates, walls shall either be tubular steel construction or decorative masonry block (~~block sides~~). Reverse frontage walls (both sides) and any wall return that is visible from the street shall be constructed of split-face block or precision block that is plastered or stuccoed ~~and of with~~ a color that blends with the Parkside color palette. Decorative caps and the use of decorative themed pilasters to help enhance the perimeter appeal of the walls are encouraged. Walls visible from the community streets ~~may shall~~ not be ~~made of~~ exposed or painted precision block ~~or slumped block~~. Refer to Exhibit 7-26: Fence/Wall Plan and Exhibit 7-27: Wall Detail.

## View Fencing

These fences allow open views but not physical access; they shall be a minimum of 5 feet high and made of tubular steel construction. Areas where view fencing occurs will be subject to review by the City of Ontario. Refer to Exhibit 7-26: Fence/Wall Plan and Exhibit 7-28: View Fence Detail.

## Privacy Fencing in Cul-De-Sacs

~~Where perimeter walls about a cul-de-sac at the perimeter of a tract, 40 to 60 feet of view fencing will be installed to allow emergency personnel to see within the cul-de-sac area from the street. These areas will also be enhanced with landscaping.~~

These fences provide privacy between homes within the neighborhoods. These fences will be 5 feet- 6 inches tall with a post and cap design. These fences will be vinyl and the posts will be a maximum of 8 feet on center.

This fencing is permitted for all housing types in non-publicly visible locations. Refer to Exhibit 7-30: Privacy Vinyl Wall Detail

## 7.7.11 Outdoor Lighting

Lighting standards within the project site Specific Plan area shall be consistent in style, color, and materials in order to maintain uniformity throughout. Lighting should be subtle, providing a soft wash of light over illuminated objects such as monumentation. A hierarchy shall be established by using a variety of lighting fixtures and illumination levels based off of lighting design intent. Lighting styles shall tie into architectural styles and provide sufficient illumination for the safety and well-being of the community.

Specimen trees may be uplighted with several fixtures into the canopy to avoid creating dark sides of the trees. Providing adequate lighting within the vehicular turn-around areas and lanes (private alleys) is essential. ~~Metal halide lighting to project true colors and clear visibility, and reflective street signs shall be used on all proposed streets.~~ Lighting shall have anti-vandalism fixtures.

Each residential unit shall have illuminated addresses at the front door and the garage door, if the garage faces an alley.

## 7.7.12 Mailboxes

~~Mailbox units may be designed with features to complement the architectural style of the neighborhood with trellises, stone bases, and landscaping. The adjacent space shall be designed to allow frequent pedestrian use and to function as an important neighborhood gathering space. The developer(s)~~



# Parkside

## Entry Monuments

Avoid intensely bright or “hot” lighting of monuments; rather, each should be lit to provide a soft wash of light across the monument signage. Specimen trees should be uplighted with several fixtures into the canopy to avoid creating dark sides of the trees.

## Recreation Center

Uplighting of specimen trees is very important and providing adequate lighting within the vehicular turn-around areas. Recreation center monumentation/kiosk should also be illuminated.

## Community Park/Linear Park/Paseo Lighting

Lighting of the walkways, tot lot, and parking area within the Parks and Paseo Trail System should be considered for safety and security. Utilization of a traditional Globe/Acorn Post mounted light fixture should be considered for both the park open space areas as well as interior street lighting fixtures. Keeping with the character of traditional materials, this will help to create a better sense of scale to the pedestrian.

### 7.7.12 Mailboxes

The developers shall work with the United States Post Office to determine the best possible location for mailboxes within the residential subdivisions. The developer(s) will determine the specifications for the mailbox units.

## 7.8 Landscape Implementation

### 7.8.1 Improvement Responsibility Landscape Maintenance

#### Landscape Maintenance

- All public landscapes, including slopes, street trees, lighting, and irrigation systems, shall be maintained by the City of Ontario Landscape Maintenance District;
- Front yard landscapes shall be installed by the developer, however and maintained by the homeowner shall maintain the front yard landscape in a healthy condition at all times; and
- ~~All manufactured slopes which exceed three (3) feet in height shall be planted by the developer with an effective mixture of groundcover, shrubs, and trees. Such slopes shall also be irrigated as necessary to ensure germination and establishment in conformance with the fuel modification guidelines described later in this document.~~
- All manufactured and cut/fill slopes which exceed 3 feet in height shall be planted by the developer with an effective mixture of groundcover, shrubs, and trees. Such slopes shall also be irrigated as necessary to ensure germination and establishment in conformance with the fuel modification guidelines described later in this document.



# Parkside

## 7.8.2 Homebuilder Landscape Requirements

### Public Landscapes (HOA and City Maintained Areas)

- Landscape plantings in public areas should reflect a commitment to both developing a “sense of place” and maintaining harmony with [the New Model Colony Ontario Ranch](#);
- [A landscape architect licensed in California shall be retained to prepare planting and irrigation plans for all public areas.](#) Arrangement of plants should incorporate the concepts of mass planting; plants should be placed to allow them to grow to their natural sizes and forms, and sheared hedges should be kept to a minimum;
- [Minimum sizes of trees shall conform to Table 7-15: On-Site Tree Size; and](#)
- All [public areas, including HOA designated areas pathways](#), shall be [installed by the developer designed to be barrier free and meet ADA guidelines.](#)

The plant list at the end of this section offers a suggested plant palette for Parkside; while it is by no means all-inclusive, plantings in public areas should draw primarily from this palette for visual community continuity.

Table 7-14: On-Site Tree Size

Trees	Percentage on-site
15 Gallon	60%
24" Box	25%
36" Box	10%
48" Box	5%

### Front Yard Landscapes

Plantings in front yards may vary substantially from the Specific Plan palette, but should retain some of the character and style of the public plantings. No more than ~~70~~25 percent of the total square footage of any front yard shall be lawn; the balance shall be composed of shrubs and groundcovers, with an emphasis on drought tolerant plant species. [Each front yard shall be planted with one 24-gallon tree \(minimum\).](#) [Installation of landscaping within the front yards of single family detached housing products will be provided by the home builder.](#) [Front yard landscaping shall be installed prior to Certificate of Occupancy.](#)

### Soil Testing

Soil samples shall be taken from several locations after the completion of rough grading operations, and a reputable soil-testing laboratory shall perform an agronomic soils test. The test shall assess soil fertility needs for water-wise California native and Mediterranean plant types. No planting shall take place until the soil has been properly prepared based on the recommendations of the soils testing laboratory.

### Slope Landscaping (HOA)

All manufactured and cut/fill slopes which exceed 3 feet in height [\(if they occur\)](#) shall be planted with an effective mixture of groundcover, shrubs, and trees. [These slopes shall be installed before the Certificates of Occupancy.](#) Such slopes shall also be irrigated as necessary to ensure germination and establishment in conformance with the fuel modification guidelines.



# Parkside

## **Private Interior Slopes: Residential Interior**

- Private interior slopes may be more ornamental in character than exterior slopes. They may have a somewhat broader range of plant materials than exterior slopes, but should still be chosen primarily from the plant palette and are subject to the same fuel modification restrictions; and
- All manufactured and cut/fill slopes which exceed 3 feet in height shall be planted by the developer with an effective mixture of groundcover, shrubs, and trees. Such slopes shall also be irrigated as necessary to ensure germination and establishment in conformance with the fuel modification guidelines described later in this document. ~~All manufactured slopes which exceed three~~ All slopes shall be landscaped in accordance with the City standards.

## **Streetscape Landscaping**

### **Streetscape Development Standards**

- Turf grass shall not exceed 30 percent of streetscape planting and shall be located adjacent to the sidewalk or curb line;
- All new plantings within the planned community shall draw substantially from Table 7-16—Community Plant Palette;
- All streetscape landscaping within the Parkside planned community will be implemented by the Developer in accordance with this Specific Plan;

- A balance of evergreen and deciduous trees shall be considered during the planning process for the selection of street trees and understory trees between them (if any). Street trees shall be sized per City standards;
- The Developer shall install all primary and secondary improvements concurrently with the construction of the roadway on which they front. Neighborhood intersections shall be constructed as each neighborhood street is built;
- The Developer shall provide site inspection of all construction and installation of entries and intersections in accordance with City of Ontario requirements; Self-attached vines are to be added against block walls;
- Along community walls, the use of 5-gallon plant material for hedgegrows is preferred; and
- Closely spaced plant materials (to horticultural practices) are expected on this project.

### **Irrigation Design**

Irrigation for both public and private landscapes should be designed to be as water-efficient as possible. All irrigation systems shall ~~have automatic controllers designed to properly water plant materials given the site's soil conditions, and irrigation systems for all public landscapes shall have automatic rain shut-off devices. Drip irrigation is encouraged. Spray systems shall have low-gallonage, matched-precipitation heads. Separate irrigation systems with bubblers or spray heads are required for trees and palms~~ comply with Subsection 6.05.035C Irrigation



# Parkside

Requirements of the Ontario Development Code. All City maintained landscapes (CFD areas) are to be controlled with a central controlled system as approved by the City's Parks and Maintenance Department, and trees plantings are to consist of root barriers and a pop up stream bubbler system on a separate valve. Irrigation design shall meet the requirements of the Landscape Development Standards.

## **Addresses**

Each single family home will be required to have the address numbers painted on the curb with black numerals on a white reflective paint background. Each homeowner will be responsible for the maintenance of these numbers. ~~Illuminated addresses shall also be installed on the residential units as required by the City of Ontario Fire Department.~~

## **7.9 Design Guideline Implementation**

### **7.9.1 Design Review Submittal Requirements**

The master developer shall review all plans prior to submittal to the City for Plan Check. This design review process will include these steps: conceptual review, refined review and construction document review prior to plans being submitted for City processing. The design review process will ensure the vision of Parkside will be implemented.



# Parkside

Table 7-15: Community Plant Palette

Note: The landscape materials (trees, shrubs, vines, and groundcovers) will be provided per the Streetscape Master Plan.v

BOTANICAL NAME	COMMON NAME													
		Archibald Avenue	Eucalyptus Avenue	Ontario Ranch Road	Hellman Avenue	Carpenter Avenue	Cucamonga Creek Channel	Primary Street / Park Loop Street	Interior Neighborhood Streets	Lanes (Private Alleys)	Community Gateway Monuments & Entries	Secondary Community Entries	Interior Paseo & Pocket Parks	Commercial Area
<b>Shrubs</b>														
Acacia redolens	Creeping Acacia						•							
Agapanthus africanus	Lily-of-the-Nile				•					•				•
Agave attenuata	Foxtail Agave										•	•	•	
Agave desmettiana 'Variegata'	Variegated Dwarf Century Plant												•	•
Agave vilmoriniana	Octopus Agave										•	•		
Agave weberi	Weber's Century Plant										•	•		
Aloe species	Aloe										•		•	
Alyogene huegelii	Blue Hibiscus					•				•				
Arctostaphylos species	Manzanita						•							
Baccharis pilularis	Coyote Brush						•							
Buxus m. japonica	Japanese Boxwood										•	•	•	•
Callistemon viminalis 'LittleJohn'	Dwarf Bottlebrush							•	•					
Carex Specs	Sedge							•	•	•	•	•	•	
Carissa grandiflora 'Green Carpet'	Natal Plum							•	•					
Ceanothus species	California Lilac						•							
Chondropetalum tectorum	Cape Rush								•					•
Cistus purpureus	Rockrose					•	•							
Cistus salvifolius 'Prostratus'	Sageleaf Rockrose				•									
Cotoneaster species	Cotoneaster						•				•		•	
Dianella revoluta 'DR5000'	Little Rev Flax Lily							•	•	•	•	•	•	•
Dianella tasmanica 'Silver Streak'	Silver Streak Flax Lily							•	•	•	•	•	•	•
Dietes bicolor	Fortnight Lily							•		•	•	•	•	•
Echium fastuosum	Pride of Madiera													•
Eleagnus pungens	Silverberry						•							•
Eschscholzia californica	California Poppy						•							
Euryops pectinatus	Grey-Leafed Euryops													•
Fremontodendron californicum	Flannel Bush						•							



# Parkside

Table 7-15: Community Plant Palette

Note: The landscape materials (trees, shrubs, vines, and groundcovers) will be provided per the Streetscape Master Plan.v

BOTANICAL NAME	COMMON NAME	Archibald Avenue	Eucahyptus Avenue	Ontario Ranch Road	Hellman Avenue	Carpenter Avenue	Cucamonga Creek Channel	Primary Street / Park Loop Street	Interior Neighborhood Streets	Lanes (Private Alleys)	Community Gateway Monuments & Entries	Secondary Community Entries	Interior Paseo & Pocket Parks	Commerical Area
<b>Shrubs</b>														
Gardenia jasminoides	Gardenia											•	•	
Grevillea species	N.C.N				•								•	
Hemerocallis species	Day Lily									•	•	•	•	
Heteromeles arbutifolia	Toyon						•							
Hibiscus rosa-sinensis	Golden Dust Hibiscus													
Ilex species	Holly										•			
Juniperus chinensis & CVS	Juniper									•				
Kniphofia uvaria	Red-Hot Poker												•	•
Lantana montevidensis	Lantana										•	•	•	
Lavandula species	Lavendar										•	•	•	•
Leptospermum scoparium 'Snow White'	New Zealand Tea Tree												•	
Leymus condensatus 'Canyon Prince'	Canyon Prince Wild Rye							•	•	•	•	•	•	•
Ligustrum japonicum 'Texanum'	Glossy Privet									•	•	•	•	•
Limonium perezii	Statice						•							
Liriope species	Lily Turf												•	
Muhlenbergia rigens	Deer Grass						•							•
Myroporum parvifolium	N.C.N.							•	•		•			•
Myrtus communis 'Compacta'	Dwarf Myrtle											•	•	
Pittosporum tobira & CVS	Mock Orange							•	•		•	•	•	•
Pittosporum tobira variegated	Tobira Variegated									•				•
Plumbago auriculata	Cape Plumbago												•	
Prunus caroliniana 'Bright & Tight'	Carolina Cherry	•												
Pyracantha species	Fire Thorn							•	•		•	•		
Rhaphiolepis indica	India Hawthorne		•					•	•		•	•	•	•
Rhus integrifolia	Lemonade Berry									•				•
Rosa species	Rose		•	•							•	•		
Rosmarinus officianalis	Rosemary							•	•		•	•	•	•





# Parkside

**Table 7-15: Community Plant Palette**

Note: The landscape materials (trees, shrubs, vines, and groundcovers) will be provided per the Streetscape Master Plan.v

BOTANICAL NAME	COMMON NAME													
		Archibald Avenue	Eucalyptus Avenue	Ontario Ranch Road	Hellman Avenue	Carpenter Avenue	Cucamonga Creek Channel	Primary Street / Park Loop Street	Interior Neighborhood Streets	Lanes (Private Alleys)	Community Gateway Monuments & Entries	Secondary Community Entries	Interior Paseo & Pocket Parks	Commercial Area
<b>Shrubs</b>														
Salvia clevelandii	Cleveland Sage													•
Salvia species	Sage													•
Senecio mandraliscae	Blue Chalk Sticks							•						•
Strelizia reginae	Bird-of-Paradise													• •
Tecoma stans 'Sierra Apricot'	Sierra Apricot						•	•						•
Trachelospermum jasminoides	Star Jasmine									•				• •
Viburnum plicatum	Japanese Snowball			•										
Viburnum tinus	Laurustinus Viburnum			•										• •
Westringia fruticosa	Westringia													•
Xylosma congestum	Shiny Xylosma							•	•		•	•		
Yucca recurvifolia	Pendulous Yucca										•	•		

Note: Landscape slopes shall not be greater than 3:1.



# Parkside

Table 7-15: Community Plant Palette

Note: The landscape materials (trees, shrubs, vines, and groundcovers) will be provided per the Streetscape Master Plan.v

BOTANICAL NAME	COMMON NAME	Archibald Avenue	Eucahyptus Avenue	Ontario Ranch Road	Hellman Avenue	Carpenter Avenue	Cucamonga Creek Channel	Primary Street / Park Loop Street	Interior Neighborhood Streets	Lanes (Private Alleys)	Community Gateway Monuments & Entries	Secondary Community Entries	Interior Paseo & Pocket Parks	Commercial Area
		Trees												
<i>Albizia julibrissin</i> 'Rosa'	Silk Tree									•				
<i>Alnus rhombifolia</i>	White Alder						•							
<i>Bauhinia purpurea</i>	Orchid Tree										•	•	•	•
<i>Cedrus deodara</i>	Deodar Cedar											•		
<i>Celtis sinensis</i>	Chinese Hackberry				•									
<i>Cercidium hybrid</i> 'Desert Museum'	Desert Museum Palo Verde							•	•					
<i>Cercis occidentalis</i>	Western Redbud				•									
<i>Chitalpa tashkentensis</i>	Chitalpa Tree						•			•			•	
<i>Cinnamomum camphora</i>	Camphor Tree			•				•						
<i>Citrus sinensis</i>	Orange Tree										•		•	
<i>Cupaniopsis anacardioides</i>	Carrotwood								•					•
<i>Cupressus sempervirens</i>	Italian Cypress										•	•	•	
<i>Eucalyptus sideroxylon</i> 'Rosea'	Pink Iron Bark						•							
<i>Fraxinus angustifolia</i>	Raywood Ash	•												
<i>Geijera parvifolia</i>	Australian Willow									•				
<i>Gleditsia t. inermis</i> 'Shademaster'	Honey Locust								•				•	
<i>Heteromeles arbutifolia</i>	Toyon					•	•						•	
<i>Jacaranda mimosifolia</i>	Jacaranda											•	•	
<i>Koelreuteria bipinnata</i>	Chinese Flame Tree							•					•	
<i>Lagerstroemia farnesia</i>	Crape Myrtle								•		•		•	•
<i>Lagerstroemia indica</i> 'Nanchez'	White Crape Myrtle			•										
<i>Lagerstroemia x</i> 'Muskogee'	Muskogee Crape Myrtle	•												
<i>Magnolia grandiflora</i> 'Little Gem'	Southern Magnolia											•	•	
<i>Magnolia grandiflora</i> 'Magestic Beauty'	Magesitc Beautify Magnolia		•											
<i>Magnolia grandiflora</i> 'Russet'	Southern Magnolia								•				•	•
<i>Melaleuca quinquenervia</i>	Cajeput Tree												•	
<i>Michelia champaca</i>	Michelia												•	



# Parkside

Table 7-15: Community Plant Palette

Note: The landscape materials (trees, shrubs, vines, and groundcovers) will be provided per the Streetscape Master Plan.v

BOTANICAL NAME	COMMON NAME	Archibald Avenue	Eucalyptus Avenue	Ontario Ranch Road	Hellman Avenue	Carpenter Avenue	Cucamonga Creek Channel	Primary Street / Park Loop Street	Interior Neighborhood Streets	Lanes (Private Alleys)	Community Gateway Monuments & Entries	Secondary Community Entries	Interior Paseo & Pocket Parks	Commercial Area
<b>Trees</b>														
<i>Olea europaea</i>	Olive										•	•		•
<i>Phoenix dactylifera</i>	Date Palm										•	•		•
<i>Pinus canariensis</i>	Canary Island Pine							•					•	•
<i>Pinus eldarica</i>	Afghan Pine	•		•	•						•	•	•	•
<i>Pinus pinea</i>	Italian Stone Pine	•									•			
<i>Pistacia Chinensis</i>	Chinese Pistache		•											
<i>Pittosporum undulatum</i>	Vicorian Box								•				•	
<i>Platanus acerfolia</i> 'Bloodgood'	London Plane Tree								•				•	
<i>Podocarpus gracilior</i>	Fern Podocarpus										•	•	•	•
<i>Prosopis glandulosa</i> 'Thornless'	Thornless Honey Mesquite								•				•	
<i>Pyrus calleryana</i> 'Chanticleer'	Chanticleer Pear								•	•			•	
<i>Pyrus kawakamii</i>	Evergreen Pear								•					•
<i>Quercus agrifolia</i>	Coast Live Oak							•			•			
<i>Quercus douglasii</i>	Blue Oak					•								
<i>Quercus ilex</i>	Holly Oak								•					•
<i>Raphiolepis</i> 'Majestic Beauty'	N.C.N.									•				
<i>Schinus molle</i>	California Pepper Tree							•					•	
<i>Syagrus romanzoffianum</i>	Queen Palm									•				
<i>Tabebuia impetiginosa</i>	Pink Trumpet Tree									•			•	
<i>Tipuana tipu</i>	Tipu Tree							•						
<i>Tristania conferta</i>	Brisbane Box								•				•	•
<i>Ulmus parvifolia</i> 'Ture Green'	Evergreen Elm							•	•					•



# Parkside

**Table 7-15: Community Plant Palette**

Note: The landscape materials (trees, shrubs, vines, and groundcovers) will be provided per the Streetscape Master Plan.v

BOTANICAL NAME	COMMON NAME	Vines													
		Archibald Avenue	Eucalyptus Avenue	Edison Avenue	Hellman Avenue	Carpenter Avenue	Cucamonga Creek Channel	Primary Street / Park Loop Street	Interior Neighborhood Streets	Lanes (Private Alleys)	Community Gateway Monuments & Entries	Primary & Secondary Community Entries	Neighborhood Entries	Interior Paseo & Pocket Parks	Commercial Area
<i>Clematis ligusticifolia</i>	Clematis	•													•
<i>Clytostoma callistegiodes</i>	Violet Trumpet Vine		•												
<i>Distictis buccinatoria</i>	Blood Red Trumpet Vine			•											
<i>Ficus repens</i>	Creeping Fig						•	•	•						
<i>Gelsemium sempervirens</i>	Carolina Jessamine												•	•	
<i>Jasminum polynthum</i>	Pink Jasmine													•	
<i>Macfadyana unguis-cati</i>	Cat's Claw						•								
<i>Pandorea jasminoides</i>	Bower Vine														
<i>Parthenocissus tricuspidata</i>	Boston Ivy	•	•	•	•	•		•	•	•					
<i>Passiflora species</i>	Passion Flower							•	•						•
<i>Rosa banksiae</i>	Lady Banks Rose							•	•						•
<i>Solanum jasminoides</i>	Potato Vine														•
<i>Wisteria sinensis</i>	Chinese Wisteria												•	•	

## Chapter 8: Commercial Design Guidelines



New Orleans, Louisiana: City Park





# Parkside

## 8.0 Commercial Design Guidelines

### 8.1 Introduction

The Parkside Specific Plan area will include a commercial area in the northeastern portion of the project site, adjacent to Edison Ontario Ranch Rd. and Archibald Ave. These non-residential uses will provide the residents of Parkside with shopping conveniently located nearby. Residents will be able have the ability to access services by foot or ride their bicycles to shop via a pedestrian trail connecting the residential uses proposed in Parkside to at the commercial center.

The commercial site is included in Planning Area 21, amounting to 11.3 net acres.

Throughout the commercial development, appropriate landscaping, site planning, and architectural design should be considered to create friendly and welcoming places to shop. These non-residential uses will continue the vision established in the residential community, including integrating pedestrian-friendly design.

In order to maintain the character of Parkside's residential community, the commercial area is expected to be developed with a compatible and harmonious quality. The Parkside Commercial Design Guidelines will help guide the development of the area to enhance the pedestrian experience and innovative design that contributes to the character of the entire Parkside Specific Plan area. These guidelines are not intended to be strictly enforced rules, but rather as a tool to aid the design process. Additionally, these guidelines have been generalized for the Parkside Specific Plan. Potential developers will be required to submit their own supplemental design guidelines for the Commercial development area to the City of Ontario for approval.

The Commercial Design Guidelines Section is organized into the following components:

- Site Planning;
- Building Design;
- Architectural Details; and
- Open Space and Landscaping

#### 8.1.1 Guiding Principles

Guiding Principles for commercial design for Parkside include:

Buildings should contribute to the qualitative nature of the overall neighborhood.

- Proper scale and proportions in massing and details should be used;
- Pedestrian open spaces should be implemented;
- Massing and articulation should be varied;
- Buildings should be designed at a human-scale; and
- Parking lots should be de-emphasized as much as possible.

Buildings should have an enduring quality.

- Design should draw inspiration from the rich and diverse Southern California architectural heritage;
- Materials and colors typical to Southern California should be used;
- Details and ornamentation should be authentic, as much as possible; and
- Design should create an inviting place to shop.



# Parkside

## 8.1.2 Review Process

These Commercial Design Guidelines are to be used as a general tool to guide the development of the northeastern portion of Parkside. Final designs will be subject to ~~the City of Ontario's~~ approval. Additionally, more detailed Design Guidelines for each developer may be required by the City prior to final approval. Refer to the ~~City of Ontario~~ Development Code for further information regarding the review process.

## 8.2 Commercial Design Guidelines

### 8.2.1 Introduction

Commercial development within Parkside will continue the pedestrian-friendly character established in the residential community. By reducing the impact of the automobile on the appearance of the commercial development, appropriate site planning can de-emphasize the vehicle, creating a pleasant outdoor environment without ignoring the necessary needs of vehicular traffic. Through well executed planning, architecture, and landscape design, the commercial uses within Parkside will complement and, to some extent, integrate the residential community ~~rather than turn its back completely to the Parkside community residents.~~

### 8.2.2 Site Planning

#### Orientation

Buildings should be oriented for best visibility from the surrounding streets. Windows and entries should face the streets, avoiding blank walls which could dominate public views. Stores with entries not visible from the street may be oriented towards pedestrian open space.

All primary entries should face onto a street or a connecting internal walkway.

#### Driveways and Sidewalks

The ~~project~~ site shall be provided with a limited number of driveways to reduce the traffic impact along ~~Ontario Ranch Rd.~~ and Archibald Ave. Pedestrian crossing distances should be minimized at driveways. To increase safety for pedestrians, a raised median as a pedestrian island should be ~~provided~~ ~~considered~~ between travel lanes, especially those entries with more than one lane in any direction. Driveways should be paved with asphalt, concrete, or an enhanced paving material such as brick or concrete block.

Sidewalks should be located along natural travel paths for pedestrians. Sidewalks should be a minimum of 6 feet wide along pedestrian pathways and a minimum of 8 feet in width along more heavily traveled commercial areas.

#### Street Frontage and Buildings

~~Street side~~ buildings, or those with no parking separating them from the street, should use faux windows and entry features to soften the building's appearance from the street. These buildings should not be more than 100 feet long without a pedestrian plaza or walkway breaking the building mass and connecting the parking lot with the street. On corners, buildings should utilize massing elements to anchor the corner.

#### Street Side Setbacks and Buildings

The sides of buildings along street edges should be landscaped within the setback to soften the building's appearance to the street, as well as provide additional windows or design elements. Refer to the ~~City of Ontario~~ Development Code for specific setback requirements.





# Parkside

## **Street Frontage and Parking Lots**

Parking lots should generally be placed away from streets. No street frontages should consist of a continuous parking lot, but should be broken up with buildings, landscaping, plazas, and other pedestrian features. Any parking along street edges should be set back with a landscaped buffer to minimize the dominant feeling of the automobile along the street. Continuous parking stalls may be interrupted by landscaped islands, no more than 10 stalls apart.

## **Service and Storage Areas**

Loading docks, garages, and storage areas are to be located behind or to the side of buildings, siding onto secondary access and not primary streets. These features must be screened with walls and landscaping as much as possible from public view of streets, residences, ~~pedestrians walkways, and connecting trails~~ and [sidewalks](#). Loading docks and storage areas should not conflict with pedestrian walkways.

## **Refuse Containers, Utility and Mechanical Equipment**

Refuse containers and equipment should be easily accessed by service vehicles but screened from view of the streets, parking lots, and connecting walkways through roof forms, walls, and/or landscaping. Screening details should incorporate elements that are compatible with the architectural style of the building. Proper landscaping, including trellises, also may help to screen these elements. Equipment and enclosures shall not be located near pedestrian walkways.

## **8.2.3 Building Design**

### **Massing**

With larger buildings, appropriate massing becomes more important to maintain the human-scale of the development and create a visually pleasing environment. Bay windows, stepped buildings, height changes, and setback variations between stores helps to break up large buildings as well as indicate entries and store locations to customers. Tower elements or monumental features are encouraged at focal points, such as corners, plazas, major entrances, or where walkways meet streets. All elevations of a tower element or [monumental](#) feature shall be completely constructed. In no instance shall a feature of this nature be left unfinished and exposed to public view.

### **Roof Forms**

Hipped and gable roofs are encouraged. Roof pitches should be compatible with the architectural style of the building. Flat roofs should vary in height and be accompanied by cornice, trim, or other accent features.

### **Entry Design**

Building entries should be prominent and easily identified. Various elements can be used to enhance the entry features including massing variation, materials and color change, change in roof form, and awnings. For smaller retail buildings, as part of a cluster or strip, arcades, awnings, and simple signage may be acceptable as entries.

### **Arcades, Trellises, and Awnings**

Outdoor arcades along store fronts are encouraged over pedestrian walkways. Arcades may be used to connect separate buildings, or to provide a more pleasing



# Parkside

experience for pedestrians. Trellises or awnings may also be used to create a covered walkway to protect pedestrians from the sun and rain.

## 8.2.4 Architectural Details

### Architectural Styles

A consistent architectural style should be used throughout the commercial development in Planning Area 21 to create a sense of continuity between the buildings. Related elements, such as trellises, planters, light-standards, windows, doors, etc. should also adopt detailing that is compatible with the selected architectural style. The selected style of the commercial development does not necessarily need to be of any one of those available for the residential community but should be compatible with them.

Architectural styles should derive or be adapted from Southern California and Ontario's history. ~~Modern and "high-tech" styles are not acceptable style choices.~~ Acceptable styles generally respond to the region's climate including such features as shade windows, outdoor circulation, and outdoor courtyards or plazas, recessed window frames, awnings, arcades, loggias, and trellises.

### Building Wall Treatment

No wall should have a blank, uninterrupted length exceeding 20 feet without including one of the following:

- Change in texture;
- Change in material;
- Change in plane;
- Lattice; and
- A tree or equivalent element.

Facades that are visible from adjacent streets or walkways should display even greater visual interest by using architectural elements that break up the massing of large buildings, such as windows, arcades, awnings, porticos, and other architectural features.

### Base and Top Treatments

Base and top treatments help to balance the "weight" of the building visually. Bases should appear to "ground" the building, while tops create a defined edge to the roofline. Possible treatment techniques are as follows:

- Base;
- Thicker walls;
- Natural materials;
- Different colored materials; and
- Enriched landscaping with a mature height of at least 18 inches.

Special materials, such as ceramic tile, granite, stone, and marble, are encouraged on the base of buildings that face streets or connecting walkways, especially adjacent to major entries.

- Tops;
- Cornice treatments;
- Roof overhangs with brackets;
- Stepped parapets;
- Textured materials; and
- Different colored materials.

### Exterior Materials

Recommended materials include stucco, exterior plaster, brick, wood siding, tile, or stone. Exterior materials that appear pre-fabricated are not recommended. Selected materials and detailing should have an enduring appearance.



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## Roof Materials

Roofing materials that are generally acceptable include metal standing seam, asphalt shingles, **concrete** tile, ceramic tile, and slate or slate-like materials.

## Colors

Color selection should be consistent with the selected architecture style.

## 8.2.5 Open Space and Landscaping

### Site Accessories

Site accessories, such as recycling bins, bike racks, litter cans, planters, benches, and transit shelters, should **employ** use materials and **provide have** an architectural character consistent with the overall **project development**. These accessories should be graffiti-resistant with materials that are easy to clean and paint.

### Connecting Walkways

Walkways throughout **Parkside Specific Plan area** the commercial development should connect the various buildings to each other and to the sidewalks along **Edison Ave-Ontario Ranch Rd.** and Archibald **Ave**. Use of trellises, landscaping, sitting areas, and the site planning of buildings adjacent to one another will encourage pedestrian activity within the commercial area. A typical walkway should be a minimum of 6-feet wide. In large parking lots, pedestrian walkways should be provided within the median of at least one or two parking aisles depending on the size of the commercial center and/or design of the center.

Pedestrian walkways that may also be used for emergency access must allow a minimum clearance of 30-feet **wide**

and 14 **feet** in vertical clearance.

### Off-site Connections

Off-site connections should be provided from the commercial area to Parkside's residential development. Access to the residential community will be available by special pedestrian walkways. **adjacent to the common entry areas**

### Plazas

Plazas are encouraged as much as possible to emphasize a pedestrian-friendly environment by creating locations within the commercial development that allow for people to gather. Plazas create a more inviting feeling to pedestrians, allowing them to feel safe from the vehicular dominated parking lot. These outdoor places may include water features, landscaping, and other elements to create a comfortable place to sit. Plazas are encouraged where high levels of pedestrian activity are expected, such as adjacent to major entrances and nearby uses that allow for more interactivity with the plaza such as delis, cafes, restaurants, bakeries, and other food services.

### Landscaping

Landscaping helps to soften the feel of the buildings and parking lots while enhancing the visual quality of the site. The following landscaping techniques may be used:

- Provide special landscaping treatment at all project site entries and building entries;
- Provide shade/canopy trees within parking areas;
- Use plants to define outdoor spaces such as edges, outdoor plazas, or movement paths between parking



# Parkside

and building entrances; and

- Choose plantings that will provide a continuity of form across the entire project site.

Shrubs and tree canopies shall not be higher than 3 feet or lower than 7 feet respectively to allow police officers the ability to patrol the project site without visual obstructions. Plants near low-lying windows shall have thorns as a deterrent for suspects hiding and removing glass panes as a point of entry.

Where a parking lot contains 10 or more parking spaces and is visible from a street, not less than 5 percent of the total area, excluding any perimeter landscaping, shall be landscaped. Such landscaping shall be concentrated in any one location. A minimum of 50 percent of the plant material shall be canopy or shade trees.

All rows of parking spaces shall be provided with landscape islands at each row terminus, minimum 5-foot wide exclusive of curbs, to protect parked vehicles, ensure visibility, confine moving traffic to drive aisles and driveways, and provide within each row of parking spaces, landscaped islands, minimum 5-foot wide, exclusive of curbs, located so as to prevent more than 10 vehicles from being parked side-by-side in an abutting configuration.

Landscaped areas shall be delineated with a concrete curb, minimum 6-inch high by 6-inch wide; except where a landscape area is parallel and adjacent to a parking stall, the curb shall be a minimum 6-inches high by 12-inches wide to provide an area for persons to step when entering or exiting a motor vehicle.

## Outdoor Storage Areas

Outdoor storage areas should be incorporated into the design of the project to avoid visual impacts on the project site.

Outdoor storage areas should be located away from the street, behind, or to the side of buildings. Walls and landscaping should be used to screen stored materials.

## Transit Shelters

Transit shelters should be incorporated into the design of the commercial project and will be designed according to guidelines provided by the transit agency of Ontario. The commercial developer should explore, with the transit agency and the City, possibilities for a structure building that is integrated architecturally with the project through its color, materials, and architectural style.

Pedestrian walkways should be provided for easy accessibility by pedestrians moving to and from the transit stop to the commercial development.

## Fence and Wall Design

Chain-link fencing, barbed wire, corrugated metal fencing, and “tennis windscreens” are not permitted. Fences and walls should be built with attractive, durable materials, including (but not limited to), wrought iron, textured concrete block, brick, or formed concrete with reveals. Fences or walls should be consistent with architectural style, materials, and designs used throughout the project site. Walls and fences should not exceed a height of 6 feet without being made of texture concrete block, interlocking “diamond” blocks, formed concrete with reveals, or similar materials to a maximum of 8 feet



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from grade.

Along streets, walls and fences should not exceed 42-inch in height. Avoid long expanses of uninterrupted fences and walls. Use of an opening, planter box, material change, pilaster, or post is acceptable. Openings shall be provided to connect walkways directly to the street and to allow pedestrians to avoid long, inconvenient walking paths. These pedestrian portals should be announced by pilasters, trellis, special landscaping, or other special features.

## **Lighting**

Lighting standards under 32 feet high should provide adequate illumination throughout the project site during the nighttime hours. Lighting fixtures should be designed to be compatible with the architectural styles selected for the project site. At a minimum, all light standards should have an attractive base and top; overhead “cobra head” standards are not permitted. Buildings and landscaping can be illuminated indirectly to create a strong positive image. Along pedestrian walks, lighting should provide appropriate illumination at a human scale without glare. Lighting within larger parking lots should provide safe visibility. Service area lighting should be positioned to not be seen from public view.

## **8.2.6 Sign Program**

Refer to Section 6.18, Signage.



# Parkside



Exhibit 8-1: Retail Site Plan

## Chapter 9: General Plan Consistency



Portland, Maine; Deering Oaks Memorial







# Parkside

## **9.0 General Plan Consistency**

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits the adoption and administration of specific plans as an implementation tool for elements contained in the local General Plan. Specific plans must demonstrate consistency in regulations, guidelines, and programs with the goals and policies set forth in the General Plan.

The [Subarea 29 Specific Plan \(SP\)](#) has been prepared in conformance with the goals and policies of The Ontario [Plan \(TOP\) Policy Plan](#) (General Plan). The [policy analysis in this Chapter](#) describes the manner in which the [SP](#) complies with [TOP Goals and Polices applicable to the project](#).

~~THIS CHAPTER HAS BEEN FULLY UPDATED FROM ORIGINAL 2006 APPROVED DOCUMENT. 2006 TEXT IS LOCATED AT THE END OF THIS CHAPTER, ALL IN RED TEXT.~~



# Parkside

Policy Plan Goals and Policies	Specific Plan Consistency
<b>Land Use Element</b>	
<b>GOAL LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.</b>	
LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identify, maximizes available and planned infrastructure, and foster the development of transit.	The Specific Plan area is within the Ontario Ranch as identified in The Ontario Plan. The Specific Plan will adhere to current approved City Infrastructure Master Plans.
LU1-2 Sustainable Community Strategy. We integrate state, regional and local Sustainable Community/ Smart Growth principles into the development and entitlement process.	Sustainable Community/Smart Growth principles are incorporated into Land Use Plan. Pedestrian sidewalks and multipurpose trails provide connectivity.
LU1-3 Adequate Capacity. We require adequate infrastructure and services for all development.	The Specific Plan establishes an infrastructure and public facilities plan to ensure that adequate roadways and public utilities including sewer, water and drainage facilities along with parks and other public facilities are provided to serve the Specific Plan area.
LU1-4 Mobility. We require development and urban design, where appropriate, that reduces reliance on the automobile and capitalizes on multi-modal transportation opportunities. (Link to Mobility Element Policy M3-3)	Multipurpose trails are provided within the Specific Plan area as shown on the City’s Multipurpose Trails and Bikeway Corridor Plan.
LU1-6 Complete Community. We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Link to Complete Community Section of Community Economics Element)	The Specific Plan plans for a complete community with a mix of residential, commercial, and parks where residents can live, work, shop and recreate.
<b>GOAL LU2: Compatibility between a wide range of uses.</b>	
LU2-6 Infrastructure Compatibility. We require infrastructure to be aesthetically pleasing and in context with the community character.	The Specific Plan provides for an aesthetically pleasing streetscape concept.
LU2-9 Methane Gas Sites. We require sensitive land uses and new uses on former dairy farms or other methane-producing sites be designed to minimize health risks.	The project will comply with appropriate mitigation measures identified in the project EIR for soil remediation and proper venting to address the potential existence of methane gases within the project. Per development requirements, soil that is potentially methane producing will be removed from the site to minimize risks.
<b>GOAL LU4: Development that provides short-term value only when the opportunity to achieve our Vision can be preserved.</b>	
LU4-3 Infrastructure Timing. We require that the necessary infrastructure and services be in place prior to or concurrently with development.	The Specific Plan establishes an infrastructure and public facilities plan to ensure that adequate roadways and public utilities including sewer, water and drainage facilities along with parks and other public facilities are provided to serve the Specific Plan area.
<b>GOAL LU5: Integrated airport facilities that minimize negative impacts to the community and maximize economic benefits.</b>	



# Parkside

Policy Plan Goals and Policies	Specific Plan Consistency
<p>LU5-7 ALUCP Consistency and Land Use Regulations. We comply with state law that requires general plans, specific plans and all new development to be consistent with the policies and criteria set forth within an Airport Land Use Compatibility Plan for any public use airport.</p>	<p>The Specific Plan is located within the Airport Influence Areas of Chino Airport and Ontario International Airport. The Specific Plan is considered to be an “existing land Use” as defined by the Riverside county ALUCP for the Chino Airport and the ONT ALUCP.</p>
<p><b>Housing Element</b></p>	
<p><b>GOAL H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.</b></p>	
<p>H2-4 New Model Colony. We support a premier lifestyle community in the New Model Colony distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.</p>	<p>The Specific Plan features detached and attached single-family and multi-family housing types in a variety of plotting configurations distinguished by the highest design quality and cohesive and highly amenitized neighborhoods.</p>
<p>H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.</p>	<p>The Specific Plan has its own design guidelines to achieve architectural excellence.</p>
<p><b>GOAL H4: Increased opportunities for low and moderate income households and families to afford and maintain quality ownership and rental housing opportunities, including move-up opportunities.</b></p>	
<p>H4-2 : Homeownership Opportunities. We increase and expand homeownership rates for lower and moderate income households by offering financial assistance, low-interest loans and educational resources, and by working in collaboration with partnerships.</p>	<p>The Housing Element has identified 1,510 dwelling units (at 10-24 du. ac) within the Specific Plan area to meet the Regional Housing Need Assessment (RHNA) for Moderate Income residents. Recently adopted Senate Bill 166, prohibits cities from allowing their inventory of available sites to be insufficient to meet their remaining unmet RHNA share for lower and moderate-income housing. California Government Code Section 65863 requires cities to make certain findings that the remaining housing element sites can accommodate the RHNA requirements by income level. The City has identified a surplus of 3,248 Moderate Income units with land availability to meet the requirements. Therefore, even if all the units are not constructed, the Specific Plan is still consistent with the Housing Element.</p>
<p>H4-4: Mixed-income Housing. We encourage the integration of affordable housing in the New Model Colony, Ontario Airport Metro Center Area, and existing neighborhoods.</p>	<p>The Specific Plan provides for a wide range of home ownership opportunities for many different lifestyles.</p>
<p><b>GOAL H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.</b></p>	
<p>H5-1 Senior Housing. We support the development of accessible and affordable senior housing and provide financial assistance for seniors to maintain and improve their homes.</p>	<p>All development will meet the required accessibility requirements of either the California Residential Code or the California Building Code.</p>



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Policy Plan Goals and Policies	Specific Plan Consistency
H5-2 Family Housing. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation and other amenities.	See comments under H4-2 above.
H5-3 Disabled People. We increase the supply of permanent, affordable and accessible housing for people with disabilities, and provide assistance to allow them to maintain and improve their homes	All development will meet the required accessibility requirements of either the California Residential Code or the California Building Code.
<b>Parks and Recreation Element</b>	
<b>Goal PR1: A system of safe and accessible parks that meets the needs of the community.</b>	
PR1-1 Access to Parks. We strive to provide a park and/or recreational facility within walking distances (1/4 mile) of every residence.	The Specific Plan provides for parks, a multipurpose trails, and sidewalks that allow all homes to be within walking distance of a recreational facility.
PR1-5 Acreage Standard. We strive to provide 5 acres of parkland (public and private) per 1,000 residents.	The Specific Plan provides for park land, park improvements and/or park fees or a combination thereof in order to meet and comply with City park standards.
PR1-6 Private Parks. We expect development to provide a minimum of 2 acres of developed private park space per 1,000 residents.	The Specific Plan will provide a minimum of 2 acres of developed private and/or public park space per 1,000 residents.
PR1-9 Phased Development. We require parks be built in new communities before a significant proportion of residents move in.	Development within the Specific Plan is required to be reviewed and approved pursuant to the City’s development review process which requires the approval of tentative and final subdivision maps for the project. Conditions of approval associated with the City’s approval of tentative subdivision maps will provide for the timing of construction of parks as part of the development.
PR1-11 Environmental Function of Parks. We require new parks to meet environmental management objectives.	The Specific Plan requires all new development to be reviewed and approved pursuant to the provisions of the City’s development review process which provides for review by the City’s Parks and Maintenance Department which may require the development to incorporate environmental management objectives into the design of parks.
PR1-12 Trails. We promote connections between parks and local trails including those managed by other public agencies.	Multipurpose trails are provided within the Specific Plan area as shown on the City’s Multipurpose Trails and Bikeway Corridor Plan.
PR1-14 Multi-family Residential Developments. We require that new multi-family residential developments of five or more units provide recreational facilities or open space, in addition to paying adopted impact fees.	The Specific Plan requires that all condominium and multi-family developments within the project provide private recreational areas and/or pocket parks for residents of the development in addition to the payment by the developer of adopted impact fees.



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Policy Plan Goals and Policies	Specific Plan Consistency
<b>Environmental Resources Element</b>	
<b>GOAL ER1: A reliable and cost effective system that permits the City to manage its diverse water resources and needs.</b>	
ER1-3 Conservation. We require conservation strategies that reduce water usage.	All Specific Plan development will meet CALGreen low flow fixture requirements. All irrigation systems meet the City’s water conservation requirements.
ER1-5 Groundwater Management. We protect groundwater quality by incorporating strategies that prevent pollution, require remediation where necessary, capture and treat urban run-off, and recharge the aquifer.	The Specific Plan development will comply with all applicable regulations in regards to protecting groundwater quality.
ER1-6 Urban Run-off Quantity. We encourage the use of low impact development strategies to intercept run-off, slow the discharge rate, increase infiltration and ultimately reduce discharge volumes to traditional storm drain systems.	Specific Plan development shall be designed to detain, filter and treat surface runoff, in a manner and combination which is practical, to comply with the most recent requirements of the San Bernardino County NPDES Stormwater Program’s Water Quality Management (WQMP) for significant new development projects.
ER1-7 Urban Run-off Quality. We require the control and management of urban run-off, consistent with Regional Water Quality Control Board regulations.	The project is obligated to control urban run-off, consistent with Regional Water Quality Control Board regulations as part of the regional NPDES Permit.
ER1-8 Wastewater Management. We require the management of wastewater discharge and collection consistent with waste discharge requirements adopted by the Regional Water Quality Control Board.	The project is obligated to comply with wastewater discharge and collection requirements adopted by the Regional Water Quality Control Board.
<b>GOAL ER3: Cost-effective and reliable energy system sustained through a combination of low impact building, site and neighborhood energy conservation and diverse sources of energy generation that collectively helps to minimize the region’s carbon footprint.</b>	
ER3-1 Conservation Strategy. We require conservation as the first strategy to be employed to meet applicable energy-saving standards.	All development within the Specific Plan will meet California’s stringent CALGreen and Energy Codes.
ER3-2 Green Development- Communities. We require the use of best practices identified in green community rating systems to guide the planning and development of all new communities.	The Specific Plan contains a broad mix of land uses, density, multi-purpose trails and proximity to planned transit as identified in the best practices of several rating systems.
R3-3 Building and Site Design. We require new construction to incorporate energy efficient building and site design strategies, which could include appropriate solar orientation, maximum use of natural daylight, passive solar and natural ventilation.	All development within the Specific Plan will meet California’s stringent CALGreen and Energy Codes.
<b>GOAL ER4: Improved indoor and outdoor air quality and reduced locally generated pollutant emissions.</b>	
ER4-4 Indoor Air Quality. We will comply with State Green Building Codes relative to indoor air quality.	Specific Plan development will be required to comply with all State laws in regards to indoor air quality.
<b>GOAL ER5: Protected high value habitat and farming and mineral resource extraction activities that are compatible with adjacent development.</b>	



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Policy Plan Goals and Policies	Specific Plan Consistency
ER5-2 Entitlement and Permitting Process. We comply with state and federal regulations regarding protected species.	The project will comply with all mitigation measures identified in the project EIR with regard to biological resources.
<b>Community Economics Element</b>	
<b>GOAL CE1: A complete community that provides for all incomes and stages of life.</b>	
CE1-7 Retail Goods and Services. We seek to ensure a mix of retail businesses that provide the full continuum of goods and services for the community.	The Specific Plan contains an approximately <b>11.3-acre</b> neighborhood commercial site to provide goods and services to the community as well as surrounding areas.
CE1-12 Circulation. We continuously plan and improve public transit and non-vehicular circulation for the mobility of all, including those with limited or no access to private automobiles. (Link to Mobility Public Transit)	The Specific Plan multipurpose trails throughout the community. Bus turnouts are provided on Archibald Ave and Ontario Ranch Rd.
<b>GOAL CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.</b>	
CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community. (Link to Community Design Element)	The Specific Plan sets forth criteria for creating high-quality places.
CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.	The Specific Plan provides a framework for high quality architecture and urban design.
<b>Safety Element</b>	
<b>Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.</b>	
S1-1: Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.	All habitable buildings within the Specific Plan will be required to meet the California Building Code.
<b>Goal S3: Reduced risk of death, injury, property damage and economic loss due to fires, accidents and normal everyday occurrences through prompt and capable emergency response.</b>	
S3-8: Fire Prevention through Environmental Design. We require new development to incorporate fire prevention consideration in the design of streetscapes, sites, open space and buildings. (Link to Community Design Element)	All development within the Specific Plan incorporates fire prevention considerations in the design of streetscapes, sites, open spaces and buildings and will be verified for compliance during the development review process.
<b>Goal S5: Reduced risk of injury, property damage and economic loss resulting from windstorms and wind- related hazards.</b>	
S5-2 Dust Control Measures. We require the implementation of Best Management Practices for dust control at all excavation and grading project.	Construction within the Specific Plan will comply with all mitigation measures identified in the project EIR with regard to dust control.
<b>Goal S6: Reduced potential for hazardous materials exposure and contamination.</b>	



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Policy Plan Goals and Policies	Specific Plan Consistency
<p>S6-9 Remediation of Methane. We require development to assess and mitigate the presence of methane, per regulatory standards and guidelines.</p>	<p>The project will comply with all mitigation measures identified as part of the recommend methane mitigation requirements to address the potential existence of methane gases within the Specific Plan area.</p>
<p><b>Goal S7: Neighborhoods and commercial and industrial districts that are kept safe through a multi-faceted approach of prevention, suppression, community involvement and a system of continuous monitoring.</b></p>	
<p>S7-4 Crime Prevention through Environmental Design (CPTED). We require new development to incorporate CPTED in the design of streetscapes, sites, open spaces and buildings.</p>	<p>The Specific Plan requires all new development to be reviewed and approved pursuant to the provisions of the City’s development review process which provides for review by the Police Department which may require the development to incorporate CPTED in the design of streetscapes, sites, open spaces and buildings.</p>
Mobility Element	
<p><b>GOAL M1: A system of roadways that meets the mobility needs of a dynamic and prosperous Ontario.</b></p>	
<p>MI-1: Roadway Design and Maintenance. We require our roadways to:</p> <ul style="list-style-type: none"> <li>* Comply with federal, state and local design and safety standards.</li> <li>* Meet the needs of multiple transportation modes and users.</li> <li>* Handle the capacity envisioned in the Functional Roadway Classification Plan.</li> <li>* Maintain a peak hour Level of Service (LOS) E or better at all intersections.</li> <li>* Be compatible with the streetscape and surrounding land uses.</li> <li>* Be maintained in accordance with best practices and our Right-of-Way Management Plan.</li> </ul>	<p>The Specific Plan is consistent with the requirements of the City’s Functional Roadway Classification Plan. The City will maintain arterial streets and curb-to-curb internal streets through the creation of an Community Facilities District. Parkway improvements within the ROW along interior streets and alleys shall be maintained by an HOA.</p>
<p>MI-2: Mitigation of Impacts. We require development to mitigate its traffic impacts.</p>	<p>Development within the Specific Plan will implement all traffic mitigation measures, standard conditions and project design features identified in the project EIR.</p>
<p><b>GOAL M2: A system of trails and corridors that facilitate and encourage bicycling and walking.</b></p>	
<p>M2-2 Bicycle System. We provide off-street multipurpose trails and Class II bikeways as our primary paths of travel and use the Class III for connectivity in constrained circumstances</p>	<p>Multi-purpose trails and connections are shown on Exhibit 5-10 promoting safe and convenient travel between destination points.</p>
<p>M2-3: Pedestrian Walkways. We require walkways that promote safe and convenient travel between residential areas, businesses, schools, parks, recreation areas, and other key destination points. (Link to Community Design CD3-1)</p>	<p>Pedestrian walkways, as shown in the roadway plans and sections, exist throughout the Specific Plan area promoting safe and convenient travel between destination points.</p>
<p><b>GOAL M3: A public transit system that is a viable alternative to automobile travel and meets the basic transportation needs of the transit dependent.</b></p>	
<p>M3-2 Transit Facilities at New Development. We require new development to provide transit facilities, such as bus shelters, transit bays and turnouts, as necessary.</p>	<p>Turnouts are location on Archibald Ave. and Ontario Ranch Road. Bus shelters are located on Eucalyptus Ave.</p>



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Policy Plan Goals and Policies	Specific Plan Consistency
M3-4 Bus Rapid Transit (BRT) Corridors. We work with regional transit agencies to implement BRT service to target destinations and along corridors, as shown in the Transit Plan.	Ontario Ranch Rd. is designated as a BRT Corridor. The design for the street meets the City’s requirements and includes an additional landscape buffer on the south side of the road.

**GOAL M4: An efficient flow of goods through the City that maximizes economic benefits and minimizes negative impacts**

M4-1 Truck Routes. We designate and maintain a network of City truck routes that provide for the effective transport of goods while minimizing negative impacts on local circulation and noise-sensitive land uses, as shown in the Truck Routes Plan.	Ontario Ranch Rd. and Archibald Ave. are designated as a Truck Route. The design for the street meets the City’s requirements and includes an additional landscape buffer on the south side of the road.
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**Community Design Element**

**GOAL CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.**

CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.	The Design Guidelines set forth a clear and cohesive design theme by providing design criteria for site planning, architectural styles, landscaping, and walls and fences.
CD1-4 Transportation Corridors. We will enhance our major transportation corridors within the City through landscape, hardscape, signage and lighting.	The Design Guidelines set forth a clear and cohesive design theme by providing design criteria for landscape, walls and fences, signage and lighting.
CD1-5 View Corridors. We require all major north- south streets be designed and redeveloped to feature views of the San Gabriel Mountains, which are part of the City’s visual identity and a key to geographic orientation. Such views should be free of visual clutter, including billboards and may be enhanced by framing with trees.	The major north-south streets are designed to feature views of the San Gabriel Mountains and comply with City standards for these streets.

**GOAL CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and district.**

CD2-1: Quality Architecture. We encourage all development projects to convey visual interest and character through: * building volume, massing, and height to provide appropriate scale and proportion; * a true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and * exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.	The Design Guidelines set forth a clear and cohesive design theme by providing design criteria for architecture, landscaping, and walls and fences.
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# Parkside

Policy Plan Goals and Policies	Specific Plan Consistency
<p>CD2-2 Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:</p> <ul style="list-style-type: none"> <li>* a pattern of smaller, walkable blocks that promote access, activity and safety;</li> <li>* variable setbacks and parcel sizes to accommodate a diversity of housing types;</li> <li>* traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;</li> <li>* floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and</li> <li>* landscaped parkways, with sidewalks separated from the curb.</li> </ul>	<p>The Design Guidelines provide for distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places.</p>
<p>CD2-3 Commercial Centers. We desire commercial centers to be distinctive, pedestrian friendly, functional and vibrant with a range of businesses, places to gather, and connectivity to the neighborhoods they serve.</p>	<p>The Design Guidelines set forth a clear and cohesive design theme by providing design criteria for architecture, landscaping, and walls and fences.</p>
<p>CD2-5: Streetscapes. We design new and, when necessary, retrofit existing streets to improve walkability, bicycling and transit integration, strengthen connectivity, and enhance community identity through improvements to the public right of way such as sidewalks, street trees, parkways, curbs, street lighting and street furniture.</p>	<p>The Circulation Plan is designed to accommodate pedestrians, bicycles and autos equally.</p>
<p>CD2-6: Connectivity. We promote development of local street patterns and pedestrian networks that create and unify neighborhoods, rather than divide them, and create cohesive and continuous corridors, rather than independent “islands” through the following means:</p> <p>local street patterns that provide access between subdivisions and within neighborhoods and discourage through traffic;</p> <p>a local street system that is logical and understandable for the user. A grid system is preferred to avoid circuitous and confusing travel paths between internal neighborhood areas and adjacent arterials;</p> <p>and neighborhoods, centers, public schools, and parks that are linked by pedestrian greenways open space networks. These may also be used to establish clear boundaries between distinct neighborhoods and/or centers.</p>	<p>The Circulation Plan is designed for connectivity that unifies neighborhoods.</p>



# Parkside

Policy Plan Goals and Policies	Specific Plan Consistency
CD 2-7: Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.	All buildings will meet or exceed California’s stringent CALGreen and California Energy Codes reducing energy demand.
CD2-8: Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.	The Specific Plan encourages “eyes on the street” for neighborhood safety and security.
CD2-9: Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.	The Specific Plan landscape enhances the aesthetics of buildings, created and defines public and private spaces and provides shade.
CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.	The Specific Plan provides for landscape buffer and landscaped islands within commercial surface parking areas.
CD2-11: Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.	The Specific Plan contains guidelines for neighborhood entries and monumentation.
CD2-12: Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.	The Specific Plan contains guidelines for neighborhood entries and monumentation.

**GOAL CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing, and safe during all hours.**

CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics. (Link to Bicycle and Pedestrians Section of the Mobility Element and Policies M2-3 and M2-4)	The Specific Plan is designed for bicycle and pedestrian accessibility throughout the community through a network of off-street bike and pedestrian trails.
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# Parkside

Policy Plan Goals and Policies	Specific Plan Consistency
CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.	Landscaping and paving are used to maintain a visual connectivity between streets, sidewalks, and walkways for pedestrians.
CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces. (Link to Safety Element Policy S7-4)	The Specific Plan guidelines for doors and entries promote the interface between public and private spaces.
CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.	Sidewalks and road surfaces contribute to the appearance of streets and public spaces.
CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.	The Design Guidelines set forth a clear and cohesive design theme by providing design criteria for site planning, architectural styles, landscaping, and walls and fences.
<b>GOAL CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages public and private investments.</b>	
CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.	A master Home Owner Association (HOA) will be established to maintain common areas and other areas as described in Table 6-1 Maintenance Responsibilities..
CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.	The HOA will maintain infrastructure elements as described in Table 6-1 Maintenance Responsibilities and in accordance with City rules and regulations.
<b>Community Economics Element</b>	
<b>GOAL CE1: A complete community that provides for all incomes and stages of life.</b>	
CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.	The Specific Plan provides a wide array of residential product types, including single family detached, single family attached and multi-family homes to provide housing opportunities for a variety of stages of life and price points to foster a socially balanced community.
<b>GOAL CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.</b>	
CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.	The Specific Plan provides for unique, high-quality places that add value to the community.
CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.	The Specific Plan requires all new development to be reviewed and approved pursuant to the provisions of the City's development review process.



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Policy Plan Goals and Policies	Specific Plan Consistency
CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.	The Specific Plan sets forth criteria for creating high-quality places.
CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.	Maintenance will be ensured by the HOA.
CE2-6 Public Maintenance. We require the establishment and operation of maintenance districts or other vehicles to fund the long-term operation and maintenance of the public realm whether on private land, in rights-of-way, or on publicly-owned property.	A Community Facilities District (CFD) will be established in conjunction with the Master Homeowners Association to maintain the public realm.



# Parkside

## 9.0 General Plan Consistency

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits the adoption and administration of Specific Plans as an implementation tool for elements contained in the local General Plan. Specific plans must demonstrate consistency in regulations, guidelines, and programs with the goals and policies set forth in the General Plan.

Prior to the approval of this Specific Plan, for portions of Subareas 22 and 23, the City will adopt a General Plan Amendment changing the General Plan Land Use Policy Map to reassign the locations of high density residential uses, eliminate all low density designations, create new medium density residential designations, add a Neighborhood Commercial Center designation at Archibald and Edison Avenues, and add a community facility designation (fire station), west of the Neighborhood Center. This Specific Plan has been prepared in conformance with the goals and policies of the City of Ontario General Plan, as amended. The City of Ontario New Model Colony Specific Plan Policy Matrix, which follows, describes the manner in which this Specific Plan complies with the General Plan.

## Ontario New Model Colony Specific Plan Policy Matrix

*Following are policies from the Ontario NMC General Plan; they are intended to achieve the “vision” for the New Model Colony planning area as outlined in Chapter 2 of the NMC General Plan. The policy matrix is divided into five (5) parts, consisting of the five (5) elements of the NMC General Plan Amendment: I. Community Development; II Infrastructure; III Aesthetic, Cultural, Open Space, and Recreational Resources; IV Natural Resources and V Hazards. The following matrix is intended to determine the adequacy of the proposed Specific Plan in meeting the policies of the NMC General Plan. The checklist shall be completed and submitted with the Specific Plan application. Check if the Specific Plan fully complies or partially complies with each respective policy, or if not applicable. A complete explanation is required in instances whereby the proposed plan does not fully comply with a policy or if not applicable.*

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<b>PART I: COMMUNITY DEVELOPMENT</b>					
<b>1.1 Role of New Model Colony General Plan</b>					
1.1.1, 1.1.5	X			2.4, 2.6, 4.2	
<p>The proposed Specific Plan reflects the types, organization and distribution of land uses depicted on the New Model Colony General Plan Land Use Plan (Figure 3-5) and Development Capacity Table (Table 3-3). The Plan incorporates the minimum and does not exceed the maximum amount of development prescribed in Table 3-4 of the NMC General Plan, where applicable, including other uses such as greenways that are specified by the NMC General Plan policies, but not tabulated in Table 3-4</p>					
1.1.4	X			2.2, 2.4, 4.2	
<p>The Proposed Specific encompasses sub-area boundaries designated on Figure 3-6 of the New Model Colony General Plan and sufficient land to constitute a significant portion of a distinct and cohesive neighborhood or district, as depicted on Figure 3-7 of the New Model Colony General Plan.</p> <p>Sub-area boundaries may be modified on the basis of the following conditions (check all that apply if sub-area boundaries are not entirely reflected by proposed Specific Plan):</p> <p><i>Note adjustments to sub-area boundaries shall be accompanied by an equivalent adjustment of the development capacity, maintaining the overall coverage densities depicted on the NMC General Plan Land Use Plan.</i></p> <ul style="list-style-type: none"> <li>• to achieve unified districts and neighborhoods;</li> <li>• to account for defined market demands regarding the types and scale/acreage of development that should be accommodated;</li> <li>• to reflect the location of major roads, utility corridors and easements, flood control improvements, natural resource areas, and public properties;</li> <li>• to reflect property subdivisions and ownerships; and</li> <li>• to consider appropriate infrastructure and public service phasing.</li> </ul>					

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<b>1.2 Structure and Organization of Land Use:</b>					
1.2.1 The proposed plan provides for integrated development of distinct residential neighborhoods and major regional serving activity centers and corridors that are differentiated by their functional role, uses, and character. Each is conceived as a uniquely identifiable place, rather than a fragment of a continuous/undifferentiated urban area.	X			2-2 4-2	
1.2.2 The plan provides for a balance of land uses within each neighborhood and district in accordance with phasing policies of the NMG General Plan.	X			2-2 4-2	
1.2.3 The plan links neighborhoods, centers, and corridors by transportation, utility, and greenways/open space networks. These may also be used to establish clear boundaries between distinct neighborhoods and/or centers.	X			5-1.4, 4.2, 5-1.3	
1.2.4 The plan includes major amenities that provide identity and enhances character including a large-scale park ("Village Green"), lake / streams, and golf courses. The Parkside Land Use Summary (Table 4-1) includes a portion of the Great Park, but it does not meet the criteria to be deemed a "Village Green". There are no additional major amenities included in the plan.					X
1.2.5 The plan includes an educational campus as the "centerpiece" of community identity and organization. The Parkside Specific Plan does not include an educational campus.					- X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.2.6 The plan includes elements that provide linkages and continuity with uses abutting the New Model Colony, including:</p> <ul style="list-style-type: none"> <li>continuity and linkage of transportation and utility infrastructure</li> <li>continuity of street trees and landscaped median along Euclid Avenue</li> </ul> <p>The Parkside Specific Plan area does not border Euclid Avenue:</p> <ul style="list-style-type: none"> <li>Pedestrian linkages to residential areas north of Riverside Drive</li> </ul> <p>The Parkside Specific Plan area does not border Riverside Drive:</p> <ul style="list-style-type: none"> <li>continuity of commercial centers at the Riverside Drive/ Euclid Avenue and Riverside Drive/ Milliken Avenue intersections; and</li> </ul> <p>The Parkside Specific Plan area does not border Riverside Drive, Euclid Avenue, or Milliken Avenue:</p> <ul style="list-style-type: none"> <li>relationship to existing and planned land uses on the west side of Euclid Avenue (City of Chino), north side of Riverside Drive (City of Ontario), and east side of Milliken Avenue (Riverside County)—by the use of complementary and/or transitional land uses and densities and streetscape improvements.</li> <li>The surrounding land use is dominantly agricultural, which may not be considered to be a transitional land use for residential development.</li> </ul>	X			5.1, 5.2	
	X			5.3, 5.4	X
					X
					X
<p>1.2.7 The Specific Plan establishes clearly defined “edges” for the New Model Colony, including landscape setbacks, transitional densities, and other suitable techniques that avoid the use of walls and creation of “walled” enclaves.</p>	X			5.1.1, 7.0	
<p>1.2.8 The Specific Plan includes measures to minimize potential impacts of environmental hazards including flooding, and noise. The Plan provides for the use of electrical, transmission corridors, flood channels, and similar elements to form “edges” for residential neighborhoods and centers and/or accommodates public greenways / corridors.</p>	X			4.4	
<b>1.3 Residential Neighborhoods</b>					
<p>1.3.1 The Specific Plan accommodates single and multi-family housing, local serving commercial services (e.g., grocery stores, barber and beauty shops, small dining establishments, etc.), community and religious facilities, schools, parks, open spaces, and similar uses within “Residential Neighborhoods.”</p>	X			4.0	



Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.3.2 The Specific Plan includes priority areas for executive housing as provided for in the NMC-General Plan (refer to Figure 3-8, NMC-General Plan). Of the target 500 acres of executive housing provided in the General Plan, 20% or 100 acres shall be provided in the areas north of Schaefer Avenue. The following standards must be met in establishing an area large enough to achieve a distinctive identity and critical mass for executive housing (check all that apply):</p> <ul style="list-style-type: none"> <li>• <i>Development with lots of 10,000 square feet or greater.</i></li> <li>• 80 contiguous acres minimum</li> <li>• <i>Development adjacent to a golf course.</i></li> <li>• 50 contiguous acres minimum</li> <li>• <i>Developments adjacent to a large open space, lake, park, or Village Green.</i></li> <li>• 20 contiguous acres minimum</li> </ul> <p>The Parkside Specific Plan housing program does not include provision for executive housing.</p>					X
1.3.3 The Plan accommodates a mix of land uses and a diversity of housing types and parcel sizes within each "Residential Neighborhood" with each designed to create a unified and distinct character. Each neighborhood may incorporate one or more planning sub areas depicted in Figure 3-7 of the NMC-General Plan. Refer to Figures 3-9 and 3-10 for examples on the use of Residential Neighborhoods.	X			4.2	
1.3.4 The Plan incorporates a focal point of activity and identity within each "Residential Neighborhood" that consolidates local serving retail and services, public meeting facilities, schools, parks, common areas (for community events, farmers markets, and other celebrations), religious facilities, and other pertinent uses.	X			4.2, 4.3	
1.3.5 The plan incorporates supporting recreational, educational, retail, cultural, and institutional uses within each "Residential Neighborhood" based on appropriate service standards. <i>To the extent feasible, these uses shall be integrated in "Neighborhood Centers"</i>	X			4.5	
1.3.6 The Plan includes provisions to increase residential densities and reduce block sizes adjacent to "Neighborhood Centers" and other key activity centers, as appropriate.		X			
1.3.7 The Plan includes provisions for greenways for pedestrians and biking that links housing with parks, "Neighborhood Centers", amenities, and adjacent neighborhood and activity centers	X			4.4, 5.1.3, 5.1.4	
1.3.8 The Plan establishes clearly defined "edges" and "entries" that contribute to a distinct neighborhood identity (refer to Figures 3-11, 3-12, and 3-13 of the NMC-General Plan)	X			7.0, 5.1	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<b>1.4 Housing Diversity/Quality</b>					
1.4.1 The Plan accommodates single and multi-family housing, parks, and open spaces in areas designated as "Residential-Low Density" by the NMG General Plan.	X			4.2	
1.4.2 The Plan provides for a variety of housing types to serve the needs of residents, including large lot, estate, executive, affordable, live/work housing.		X		6.14, 4.2	
1.4.3 The Plan provides for an overall average density of 4.6 units per gross acre in areas designated as "Residential-Low Density" by the NMG General Plan Land Use Plan, within which a variety of parcel sizes and housing types area allowed. Densities may be varied throughout a planning sub area, as long as the average density is not exceeded.	X			4.2	
1.4.4 The plan provides design standards emphasizing neighborhood livability, and socialization, through such elements as (refer to Figures 3-7, 3-9 and 3-14, NMG General Plan):	X			4.1, 4.2, 5.1.1	
• Establishment of a pattern of blocks that promotes access and neighborhood activity, while maintaining safety;	X				
• use of variable setbacks and parcel sizes to accommodate a diversity of housing unit types and respond to changing market conditions and resident needs;	X				
• minimizing the width of streets to slow traffic and promote "intimacy," while maintaining acceptable fire protection and traffic flows;	X				
• orientation of housing toward the street and reducing the visual and physical dominance of garages (introducing the street frontage as the "outdoor living room"); and	X			7.0	
• use of extensively landscaped parkways, with sidewalks separated from the curb.	X			7.0	
1.4.5 The Plan includes standards to achieve variable residential densities and housing types along a block where appropriate (e.g., single family attached units placed adjacent to duplexes, which could be adjacent to townhomes).	X			—4.0	
1.4.6 The Plan includes provisions for small clusters of multi-family housing (generally, of a maximum of 2-5 acres) within areas designated as "Residential-Low Density" by the NMG General Plan Land Use Plan, that are integrated within the overall pattern of residential development, avoiding large-scale, segregated concentrations of high density uses (refer to Figures 3-9 and 3-10 -NMG General Plan)	X			4.0	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p><b>1.4.7</b> The plan includes standards for multi-family housing in areas designated as “Residential Low Density” by the NMC General Plan Land Use Plan to convey a high level of visual quality and distinctive low density neighborhood including, but not limited to:</p> <ul style="list-style-type: none"> <li>• modulation of building volumes, masses, and heights and articulation of elevations to convey the sense of individual units or smaller structures (precluding “big stucco boxes”);</li> <li>• orientation of the building elevation and use of clear visual and physical linkages to the primary street frontages;</li> <li>• inclusion of visually well-defined entries that promote individual identity, while maintaining adequate security;</li> <li>• integration of balconies and stairways as distinctive architectural elements, rather than “appendages”;</li> <li>• inclusion of common open space and recreational facilities that are accessible to all residents (with requirements for on-going maintenance of these by residents);</li> <li>• use of landscaped setbacks along primary street frontages that maintains the predominant single family neighborhood character; and</li> <li>• linkage of the development with adjacent sites (avoiding “walled enclaves”).</li> </ul>	X			4.0, 7.0	
<p><b>1.4.8</b> The proposed plan includes provisions to avoid the use of walls to separate residential areas from arterial and other high traffic volume streets by the use of expanded landscaped setbacks, frontage roads with landscaped medians, natural drainage swales, and other appropriate techniques (refer to Figure 3-13, NMC General Plan). Where used, walls shall be attractively designed and landscaped and setbacks used to promote a high level of visual quality.</p>	X			4.2, 7.0, 5.1	
<p><b>1.4.9</b> The Plan includes measures addressing all of the use and development restrictions as specified by Referral Areas A, B, and C, and Safety Zones I, II, and III, and of the adopted Chino Airport Comprehensive Land Use Plan. This includes prohibited land uses, interior noise levels, maximum persons per acre, aviation easements, and non-hazardous operations and materials. In addition, a noise impact notification area shall be implemented for all land uses within two miles of the Chino Airport. These measures are consistent with Federal Aviation Administration (FAA) rules, will help protect the long-term operation of the Chino Airport, and will prevent the kind of aircraft noise and other impacts created by flight patterns such as have been experienced by the City around the Ontario International Airport.</p> <p>The Parkside Specific Plan does not address that a portion of the plan area is within Referral Area C and Safety Zone III of the Chino Airport Comprehensive Land Use Plan.</p>			X		

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
1.4.10 The proposed plan includes provisions for the implementation of a Noise Impact Notification Area (NINA) and a requirement for aviation easements for all schools within two miles of the China Airport.					X
The Parkside Specific Plan does not include any schools.					
1.4.11 The plan includes provisions requiring residential units to be set back from power transmission line easements in accordance with the California Department of Education's limits for locating school sites.					X
The Parkside Specific Plan does not include any schools, nor does it include a power transmission line easement.					
<b>1.5 Neighborhood Centers</b>					
1.5.1 The proposed plan accommodates local serving commercial (food and beverage sales, small retail shops, personal services, etc.), small restaurants, community/public meeting facilities, libraries, government facilities, cultural facilities, parks, schools (refer to Policy 1.6.1 below), religious facilities, public plazas and performance spaces, and similar uses in areas designated as "Neighborhood Center."	X			4.5	
1.5.2 The plan accommodates multi-family housing and mixed use structures that incorporate housing units above ground level retail shops, offices, or community facilities, in areas designated as "Neighborhood Center" by the NMG General Plan Land Use Plan.					X
The Parkside Specific Plan does not include mixed use development.					
<b>Density/Intensity The plan provides for the following densities/intensities for Neighborhood Centers (Policies 1.5.1 to 1.5.6):</b>					
1.5.3 The Plan includes provisions insuring neighborhood centers contain 50,000 to 100,000 square feet of local serving commercial uses for each 3,500 to 4,000 residential units.	X			4.5	
1.5.4 The Plan includes provisions for the development of commercial and public uses at intensities that allow for the use of surface parking, generally at a maximum floor area ratio of 0.35:1.	X			4.5	
1.5.5 The Plan includes provisions for the development of multi-family housing at a maximum overall average density of 12 units per gross acre.					X
The Parkside Specific Plan includes a wide variety of multi-family housing product types with this density included at the low end of the density range.					

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.5.6 The Plan includes provisions for the development of mixed use structures at a maximum floor area ratio of 1.5:1.</p> <p>The Parkside Specific Plan does not include mixed-use development.</p>					X
<p><b>Design and Development Principles for Neighborhood Centers (policies 1.5.7 to 1.5.13):</b></p>					
<p>1.5.7 Provisions insuring “Neighborhood Centers” are designed and developed as pedestrian oriented “villages” including the following policies: (refer to Figures 3-10 and 3-16 – NMG General Plan)</p> <ul style="list-style-type: none"> <li>• integration of multiple uses into a cohesive place;</li> <li>• use of multiple building volumes, masses, and heights and extensively articulated facades;</li> <li>• siting of buildings around common pedestrian areas, plazas, and public open spaces;</li> <li>• development of public spaces that support casual gatherings, outdoor dining and retail, entertainment, artistic exhibitions and performances, community events, and similar functions;</li> <li>• establishment of a continuous or semi-continuous business frontage along pedestrian spaces and design of ground elevations to be “pedestrian-friendly” (extensive windows, projecting signage, lighting, etc.);</li> <li>• siting of a portion of development along the primary street frontage, with parking partially or fully screened by the buildings;</li> <li>• development of clearly defined walkways from parking areas (paving materials, landscape, signage, etc.); and</li> <li>• design of buildings and sites to complement adjacent low density residential areas.</li> </ul>	<p>X</p> <p>X</p>	<p>X</p> <p>X</p>		<p>4.5</p> <p>6.15</p>	
<p>1.5.8 The Plan includes provisions insuring integration of public uses, such as community meeting rooms and auditoriums, with commercial uses in the designated “Neighborhood Centers”.</p> <p>The Parkside Specific Plan “Neighborhood Center” does not include detail for these types of uses.</p>					X
<p>1.5.9 The Plan includes provisions insuring linkages between multi-family residential development (single family attached, townhomes, or apartments) to “Neighborhood Centers”. Include design standards insuring multi-family residential developments complement the “village” character in accordance with Policy 1.4.7.</p>	X			4.2.2, 7.1	
<p>1.5.10 The Plan includes provisions insuring “Neighborhood Centers” are developed as a low rise environment, 1 to 2 stories, except mixed use structures that may be developed to a maximum height of 3 stories.</p>	X			6.15	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
1.5.11 The Plan includes provisions to promote the consolidation of school campuses within or linked to “Neighborhood Centers” sharing common facilities such as play fields, gymnasiums, auditoriums, and other facilities, where feasible. The Parkside Specific Plan does not include any schools.					X
1.5.12 The Plan includes provisions to integrate public libraries and neighborhood parks with “Neighborhood Centers” and schools.	X			4.5.1	
1.5.13 The Plan includes provisions to link “Neighborhood Centers” with adjacent residential areas by the use of greenways, pedestrian walkways, and bicycle paths.	X			4.3.2	
<b>1.6 Schools</b>					
1.6.1 The plan includes provisions to accommodate the development of school administration, classrooms, athletic facilities, and supporting uses in areas designated as “Elementary,” “Middle,” and “High School” in addition to those accommodated in designated “Neighborhood Centers.” <i>The number of schools depicted on the NMC General Plan Land Use Plan are based on the use of current (1997) school standards and may vary to reflect changes in these over the NMC’s development period. Locations are conceptualized and may be varied, provided that the intent of the NMC General Plan policies for consolidation with parks, other schools, and community facilities are attained.</i>	X			5.5	
1.6.2 The plan provides schools to meet resident needs in accordance with pertinent School District standards.	X			5.5	
1.6.3 The plan meets the policy to distribute schools throughout the New Model Colony to promote accessibility from all “Residential Neighborhoods.”	X			4.6	
1.6.4 The plan includes provisions to promote the consolidation of school campuses sharing common facilities such as play fields, gymnasiums, auditoriums, and other facilities, where feasible. The Parkside Specific Plan does not include any schools.					X
1.6.5 The plan includes provisions to integrate public schools with other community-serving uses such as parks, libraries, public meeting rooms, day care, and similar uses (see Figure 3-10 – NMC GPA).		X		5.5	
1.6.6 The plan includes provisions to link the public schools with adjacent housing, parks, and other schools by a pedestrian greenways network (see Figure 3-10 – NMC GPA).	X			5.1.3	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.6.7 The Plan includes provisions requiring educational facilities to be set back from power transmission line easements in accordance with the California Department of Education's limits for locating school sites.</p> <p>The Parkside Specific Plan does not include any schools.</p>					X
<b>1.7 Multi-Family Residential Corridors and Centers</b>					
1.7.1 The plan accommodates multi-family residential units (small lot detached, single family attached, townhomes, and apartments) in areas designated as "Residential-Medium Density" and "Residential-High Density" by the NMC General Plan Land Use Plan.	X			4-2	
1.7.2 The plan requires the inclusion of sufficient on-site recreational amenities in higher density developments to meet resident needs.	X			4-3	
1.7.3 The plan encourages the inclusion of community oriented uses such as public meeting rooms, plazas/courtyards, day-care facilities, and similar uses. These may be developed as single purpose buildings or incorporated with residential uses.	X				
The Parkside Specific Plan does not include community amenities such as public meeting rooms, plazas, day-care facilities, etc.					
1.7.4 The Plan accommodates an overall average density of 12 units per gross acre in areas designated as "Residential Medium Density" and 18 units per gross acre in areas designated as "Residential-High Density" by the NMC General Plan, within which a variety of parcel sizes and housing types would be allowed. Densities may be varied throughout a planning sub area, as long as the average density is not exceeded.		X			
The Parkside Specific Plan application includes a General Plan Amendment, which proposes a wide variety of housing types, with an average of 12 du/gross acre of residential use.					
1.7.5 The Plan includes provisions to establish visual and physical linkages among individually developed multi-family sites to create a cohesive and continuous corridor, rather than independent "islands." This may be achieved through the fronting of buildings adjacent or in immediate proximity to the street, use of a consistent landscaped setback incorporating common street trees or other vegetation, minimization of side property setbacks to achieve building continuity (a common "building wall") along the street frontage, location of all parking to the rear of street-facing buildings, use of interconnecting pedestrian walkways, and similar techniques (see Figure 3-17 in the NMC-GPA).	X			7.1, 7.5	
1.7.6 The Plan includes standards and guidelines to promote visual interest in the design of building elevations in accordance with Policy 1.4.7 (except for the use of extensive landscaped setbacks).	X			7.0	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
1.7.7 The Plan includes provisions to link community serving facilities in multi-family corridors with adjacent residential neighborhoods.	X			7.7.9	
1.7.8 The Plan includes provisions insuring sidewalks are developed in multi-family corridors as per City Standards to facilitate and be attractive for pedestrian activity.	X			7.5, 7.7	
1.7.9 The Plan includes provisions requiring residential units be set back from power transmission line easements in accordance with the California Department of Education's limits for locating school sites. The Parkside Specific Plan does not include any power transmission lines.					X
<b>1.8 Town Center</b>					
1.8.1 The plan includes provisions for the development of retail, commercial, professional offices, entertainment, art galleries, dining establishments, hotels, and similar uses in areas designated as "Town Center" by the NMG General Plan Land Use Plan. The Parkside Specific Plan does not include a "Town Center"					X
1.8.2 The Plan includes provisions requiring integration of multi-family housing (townhomes and apartments) with retail, office, and other uses within the Town Center. The Parkside Specific Plan does not include a "Town Center"					X
1.8.3 The Plan includes provisions for mixed-use buildings that integrate housing with retail commercial, office, and similar uses and special housing types (e.g., artist live/work facilities and home occupations) within the Town Center. The Parkside Specific Plan does not include a "Town Center"					X
1.8.4 The Plan includes provisions for government office, cultural uses (libraries, museums, performance venues, etc.), religious facilities, schools, recreational facilities, multi-modal transportation hub, and similar public and quasi-public uses, within the Town Center area. The Parkside Specific Plan does not include a "Town Center"					X
1.8.5 The Plan includes provisions for community meeting rooms, day-care facilities, and public and private plazas, courtyards, and open spaces within the Town Center area. The Parkside Specific Plan does not include a "Town Center"					X



Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.8.6 The Plan accommodates development within the Town Center at building heights, generally, of 1 to 3 stories and a maximum floor area ratio of 0.5, except for mixed-use structures that integrate housing with retail, office, public, and other uses that may be developed up to four stories and a floor area ratio of 2.0:1.</p> <p>The Parkside Specific Plan does not include a "Town Center."</p>					X
<p>1.8.7 The Plan accommodates free-standing residential uses at a maximum overall average density of 18 units per gross acre.</p> <p>The Parkside Specific Plan does not include a "Town Center."</p>					X
<p>1.8.8 The Plan includes standards to integrate development of individual parcels to create a cohesive pedestrian-oriented center. The following design considerations are included (as illustrated by <b>Figures 3-18 and 3-19 of the NMC GPA</b>):</p> <ul style="list-style-type: none"> <li>• integration of multiple buildings and uses into a cohesive development pattern;</li> <li>• siting of buildings to create a common/semi-continuous "building wall" directly fronting sidewalks, plazas, and other open spaces—areas may be exempted to incorporate courtyards and corridors;</li> <li>• location of parking to the rear of street facing structures, subterranean, or in structures;</li> <li>• use of multiple building volumes, masses, heights, and highly articulated facades to promote visual interest and convey the sense of individual storefronts/businesses;</li> <li>• use of vertical setbacks above the second level to reduce visual impacts along sidewalks and street frontages;</li> <li>• design of the ground floor of structures along the street frontage of commercial and mixed use buildings to enhance pedestrian activity (extensive windows, clearly defined entries, modulation of facades, outdoor patios and seating, etc.) (as illustrated by Figures 3-20, 3-21, and 3-22 of the NMC GPA);</li> <li>• integration of consistent and well-designed building and public signage;</li> <li>• develop sidewalks and other public spaces to support casual gatherings, outdoor dining and retail, entertainment, arts exhibitions and performances, community events, and similar functions; and</li> <li>• incorporate streetscape amenities along the street frontages (benches, trees, planters, decorative paving, pedestrian-oriented signage and lighting, trash receptacles, fountains, information kiosks, telephones, news racks, banners, public art, and similar elements).</li> </ul> <p>The Parkside Specific Plan does not include a "Town Center."</p>					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
1.8.9 The plan incorporates at least one major public plaza/square as a centerpiece of community activity and identity of sufficient size to accommodate events and celebrations, outdoor performances, community meetings, picnics, farmers markets, and similar functions. The Parkside Specific Plan does not include a "Town Center"					X
1.8.10 The plan includes provisions to consolidate parking facilities, with retail uses located along the street frontage. The Parkside Specific Plan does not include a "Town Center"					X
1.8.11 The Plan includes standards to site and design multi-family housing to create a cohesive and continuous corridor, rather than independent "islands" in accordance with Policy 1.7.5; and promote a high quality of design in accordance with Policy 1.4.7 (except for the use of extensive landscaped setbacks). The Parkside Specific Plan does not include a "Town Center"					X
1.8.12 The plan includes provisions to integrate a diversity of housing types within any block rather than a singular type (e.g., duplexes, townhomes, and courtyard units). The Parkside Specific Plan does not include a "Town Center"					X
1.8.13 The plan includes provisions to integrate public meeting facilities and other community services with retail and other high activity uses. The Parkside Specific Plan does not include a "Town Center"					X
1.8.14 The design of internal local streets emphasizes pedestrian activity (15' plus width sidewalks) and reduces traffic speed using such techniques as reduced width, angled parking, landscaped "pullouts"/chokers, or traffic circles. The Parkside Specific Plan does not include a "Town Center"					X
1.8.15 The plan includes provisions to establish clearly defined "entries" to the Town Center, including the use of signage, art (banners, sculptures, etc.) landscape, fountains, lighting, building massing, and other techniques. The Parkside Specific Plan does not include a "Town Center"					X
1.8.16 The plan establishes pedestrian, bicycle, greenways, and other corridors that link the Town Center with adjacent "Residential Neighborhoods" and, if developed, educational campus. The Parkside Specific Plan does not include a "Town Center"					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<b>1.9 Regional Centers</b>					
1.9.1 The plan accommodates regional serving retail commercial, professional offices, entertainment, dining, hotel/motel, conference facilities, hospitals and supporting medical offices, research laboratories, and facilities, financial institutions, sports stadiums/arenas and facilities, and similar uses. The Parkside Specific Plan does not include a "Regional Center"					X
1.9.2 The Plan accommodates multi-family housing, mixed uses that incorporate housing and retail/offices, and live/work facilities that is integrated with other regional serving uses. The Parkside Specific Plan does not include a "Regional Center"					X
1.9.3 The Plan encourages the inclusion of community meeting rooms, day care, and other public facilities. The Parkside Specific Plan does not include a "Regional Center"					X
1.9.4 The Plan accommodates buildings at a height of 1 to 3 stories, with 4 stories allowed for mixed use structures. This would accommodate a maximum floor area ratio of 0.5:1 for commercial/office uses and 2.0:1 for mixed use buildings. Sports stadium/arena, hotel/conference facilities, and other "specialty" uses may require additional height. The Parkside Specific Plan does not include a "Regional Center"					X
1.9.5 The Plan accommodates free-standing multi-family residential uses at an overall average density of 18 units per gross acre. The Parkside Specific Plan does not include a "Regional Center"					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.9.6 The Plan includes provisions to integrate development of individual parcels to create a cohesive center, which considers the (see Figure 3-16, NMG General Plan):</p> <ul style="list-style-type: none"> <li>• development of sidewalks, plazas, and other public spaces around which individual buildings and businesses are grouped and incorporate pedestrian oriented amenities (benches, public art, fountains, pedestrian-sealed lighting and signage, and similar);</li> <li>• inclusion of one or more public “square” to serve as a gathering place for public activity and events;</li> <li>• use of modulated building volumes, masses, and heights and articulated facades to create the sense of individual businesses, rather than undifferentiated “boxes”;</li> <li>• integration of consistent and well-designed building and public signage;</li> <li>• siting of a portion of buildings on the site’s peripheral streets to provide identity and connectivity with adjacent uses;</li> <li>• inclusion of sidewalks of sufficient width to accommodate pedestrian activity and outdoor restaurants, news stands, and other uses;</li> <li>• opening of the street and sidewalk/plaza facing building elevations to promote visual interest and pedestrian activity (extensive windows, clearly defined entries, and so on);</li> <li>• extensive use of landscape to enhance the environmental character; and</li> <li>• design of parking structures to be visually integrated with and continue the principal design character of commercial buildings.</li> </ul> <p><del>The Parkside Specific Plan does not include a “Regional Center”.</del></p>					X
<p>1.9.8 The Plan includes provisions to link Regional Centers with surrounding residential neighborhoods and centers through the street and highway network, distinctively paved pedestrian walkways and crosswalks, common “palette” of street trees and landscape and street furniture, well designed signage, and similar elements.</p> <p><del>The Parkside Specific Plan does not include a “Regional Center”.</del></p>					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<b>1.10 Community Centers</b>					
1.10.1 The Plan accommodates large scale retail commercial (e.g., garden supply, furniture warehouses, discount retail, "big box," and similar uses), professional offices, entertainment, dining, hotel and conference facilities, and similar uses in areas designated as "Community Commercial" by the NMG General Plan. The Parkside Specific Plan includes approximately 11.9 net acres designated for development of neighborhood commercial uses, but not Community Commercial uses.					X
1.10.2 The Plan accommodates multi-family housing and live/work facilities that is integrated with other region serving uses. The Parkside Specific Plan does not include live/work facilities.					X
1.10.3 The plan accommodates a community center of approximately 150,000 to 225,000 square feet of commercial uses for each 9,000 to 15,000 residents. The Parkside Specific Plan includes approximately 115,000 square feet designated for development of neighborhood commercial uses, but not a Community Center.					X
1.10.4 The plan includes provisions limiting buildings to 1 to 2 stories and a maximum floor area ratio of 0.35:1 with surface parking. The Parkside Specific Plan includes approximately 115,000 square feet designated for development of neighborhood commercial uses, but not a Community Center.					X
1.10.5 The plan includes provisions to accommodate free-standing residential uses at an overall average density of 18 units per gross acre. The Parkside Specific Plan does not include live/work facilities.					X
1.10.6 The plan includes provisions to integrate development of individual parcels to create a cohesive center considering the design and development principles defined by Policy 1.9.6. The Parkside Specific Plan does not include a Community Center.					X
1.10.7 The plan includes provisions to site and design development of multi-family housing to convey a high level of visual quality in accordance with Policy 1.4.7. The Parkside Specific Plan does not include live/work facilities.					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
1.10.8 The plan includes provisions to link Community Centers with surrounding residential neighborhoods and centers through the street and highway network, distinctively paved pedestrian walkways and crosswalks, common "palette" of street trees and landscape and street furniture, well designed signage, and similar elements. The Parkside Specific Plan does not include a Community Center.					X
<b>1.11 Educational Campus</b>					
1.11.1 The plan accommodates educational, administrative, academic, vocational, medical, research, athletic, student housing, services, cultural (library, museum, performance, etc.), and other supporting uses. The Parkside Specific Plan does not include an educational campus.					X
1.11.2 The plan includes research and educational uses that reflect the agricultural history of the New Model Colony and promotes water fowl habitat restoration and management. The Parkside Specific Plan does not include an educational campus.					X
1.11.3 The plan includes provisions for public meeting rooms, auditoriums, concert facilities, museums, libraries, and recreational facilities (e.g., sports fields) that are accessible to and serve adjacent residential neighborhoods. The Parkside Specific Plan does not include an educational campus.					X
1.11.4 The Plan includes provisions to site and design development to convey a "campus" environment including the clustering of buildings on common plazas and courtyards, linkage of all areas by walkways, and extensive use of landscape. The Parkside Specific Plan does not include an educational campus.					X
1.11.5 The plan includes provisions to site buildings and design the campus to promote continuity with the development fabric of the adjacent Town Center and business parks (e.g., continuity of a "Main Street" and overall street pattern, location of administrative offices as a visual terminus for the Town Center, continuity of greenways, etc.). Avoid the establishment of a isolated, "walled" environment. The Parkside Specific Plan does not include an educational campus.					X
1.11.6 The plan includes provisions for walkways, bicycle paths, greenways, and other elements that link the campus with surrounding uses. The Parkside Specific Plan does not include an educational campus.					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<b>1.12 Business and Industrial Parks</b>					
1.12.1 The plan accommodates light manufacturing, research and development, technology development, medical, entertainment facilities and production, innovative technology production, and similar primary uses. <del>The Parkside Specific Plan does not include a business park or an industrial park.</del>					*
1.12.2 The plan accommodates limited supporting uses such as retail sales, product exhibition, galleries and arts exhibition, financial institutions, restaurants, health clubs, personal services, day-care, and professional offices. <del>The Parkside Specific Plan does not include a business park or an industrial park.</del>					*
1.12.3 The plan accommodates "high impact" entertainment and commercial recreation uses that are not suitable in commercial districts due to their noise, traffic, or other characteristics that may conflict with residential or commercial uses. <del>The Parkside Specific Plan does not include a business park or an industrial park.</del>					*
1.12.4 The plan accommodates live/work facilities for artists and professionals associated with other uses in the business park. <del>The Parkside Specific Plan does not include a business park or an industrial park.</del>					*
1.12.5 The plan discourages the development of extensive warehouse development. <del>The Parkside Specific Plan does not include a business park or an industrial park.</del>					*
1.12.6 The plan includes provisions limiting building heights at 1 to 3 stories and a maximum floor area ratio of 0.4:1. <del>The Parkside Specific Plan does not include a business park or an industrial park.</del>					*

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.12.7 The plan includes site and design provisions to establish a cohesive and integrated district considering the following (see Figure 3-19 of the NMC-GPA):</p> <ul style="list-style-type: none"> <li>• development of a network of pedestrian walkways and plazas around buildings sited and concentrated to connect a site with abutting properties;</li> <li>• location of parking to the rear of buildings fronting pedestrian plazas or in structures; inclusion of restaurants, retail shops, services, and other “high activity” uses in the ground floor of structures facing pedestrian areas;</li> <li>• inclusion of restaurants, retail shops, services, and other “high activity” uses in the ground floor of structures facing pedestrian areas;</li> <li>• inclusion of design elements that visually tie individual buildings and sites together such as walkways, trellises, arcades, landscape, signage, and lighting; and</li> <li>• incorporation of monuments, signs, landscape, and other elements that clearly define entry points and activity locations.</li> </ul> <p>The Parkside Specific Plan does not include a business park or an industrial park.</p>					X
<p>1.12.8 The plan includes site and design provisions requiring business and industrial park projects to convey visual interest and character considering:</p> <ul style="list-style-type: none"> <li>• modulation of building volumes, masses, and heights to reduce bulk;</li> <li>• architectural treatment of all visible elevations;</li> <li>• use of consistent and well-designed building and site signage; and</li> <li>• incorporation of extensive landscape along street frontages, common open spaces, and parking lots.</li> </ul> <p>The Parkside Specific Plan does not include a business park or an industrial park.</p>					X
<b>1.13 Lake/Water Element</b>					
<p>1.13.1 The plan includes provisions for a lake and/or streams for visual and recreational use that are abutted by commercial, residential, and other appropriate uses.</p> <p>The Parkside Specific Plan does not include a lake or water element. These elements may be included in the Great Park Master Plan by the City.</p>					X
<p>1.13.2 The plan includes provisions to site and design development to take advantage of lake views and access including the location of buildings to “open onto” the lake and/or streams—for example, the Town-Center’s public plazas and outdoor restaurants should extend to the waterfront.</p> <p>The Parkside Specific Plan does not include a lake or water element.</p>					X



Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
1.13.2 The plan incorporates pedestrian walkways/promenades and public parks along the lake frontage. The Parkside Specific Plan does not include a lake or water element.					X
1.13.3 The plan provides visual "windows" to the lake from surrounding streets and public places. The Parkside Specific Plan does not include a lake or water element.					X
1.13.4 The plan links residential neighborhoods adjacent to the lake by a network of pedestrian walkways, greenways, and bike paths. The Parkside Specific Plan does not include a lake or water element.					X
1.14.1 The plan includes provisions for one or more golf courses (minimum of 150 acres each) with supporting club houses, retail services, and overnight accommodations within "Residential Neighborhoods." The Parkside Specific Plan does not include a golf course.					X
<b>1.14 Golf Course(s)</b>					
1.14.2 The plan includes provisions for neighborhood-oriented facilities, such as public meeting rooms, within the club house. The Parkside Specific Plan does not include a golf course.					X
1.14.3 The plan includes provisions for executive homes and other residential uses in concert with golf courses. The Parkside Specific Plan does not include a golf course.					X
1.14.4 The plan integrates golf courses with surrounding residential neighborhoods, orienting supporting facilities and housing to the course. The Parkside Specific Plan does not include a golf course.					X
1.14.5 The plan links adjacent residential neighborhoods to the golf course by a network of pedestrian walkways, greenways, and bike paths. The Parkside Specific Plan does not include a golf course.					X
1.14.6 The plan incorporate ponds and streams that assist in flood control and, as appropriate, are linked to the lake. The Parkside Specific Plan does not include a golf course.					X
1.14.7 The plan provides visual "windows" to the golf course(s) from surrounding streets and public places. The Parkside Specific Plan does not include a golf course.					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<b>1.15 Major Community Park – “Village Green”</b>					
1.15.1 Require that a minimum of 160 acres be designated by pertinent sub-area Specific Plans for the development of a “Village Green” that accommodates a diversity of active and passive recreational uses (athletic fields, picnic areas, performance venues, and botanical gardens), and that these lands be dedicated for this purpose through the development approval process. The Parkside Specific Plan includes a portion of the “Great Park” but does not meet the criteria of a “Village Green”.					X
1.15.2 Require that the land acquisition and development of the “Village Green” be equitably funded by all development within the New Model Colony. The Parkside Specific Plan “Great Park” is 56.7 gross acres and does not meet the criteria for a “Village Green”.					X
1.15.3 Allow the acreage allocated for the development of a “Village Green” to contribute to the total parks and recreational requirement of 5 acres per thousand residents.	X				
1.15.4 The plan uses the “village green” as an organizational element for the siting and design of development, such as the orientation and linkage of multi-family housing and commercial uses to the park (e.g., restaurants “opening” to the park) (see <b>Figure 3-17 of the NMG-GPA</b> )	X				
1.15.5 The plan links residential neighborhoods adjacent to the “village green” by a network of pedestrian walkways, greenways, and bike paths.	X				
1.15.6 The plan incorporates ponds and streams as part of the “village green”, that are linked to the lake, as appropriate. The Parkside Specific Plan does not include a lake or water element. These elements may be included in the Great Park Master Plan by the City.					X
<b>1.16 Greenways and Open Spaces</b>					
1.16.1 The plan includes provisions for greenway corridors containing pedestrian paths, bicycle trails, and other recreational/open space amenities.	X			4.4	
1.16.2 The plan includes an area-wide greenway network that links all residential neighborhoods, activity centers, and amenities and directly connect to and abut parks and schools as nodes along its length.	X			4.3, 5.1, 5.1.3	
1.16.3 The plan includes provisions for joint use of SCE easement-owned corridors, storm drainage, and other infrastructure rights-of-way for greenways, where feasible and compatible with the intended primary use.	X			7.7.9	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
1.16.4 The plan includes provisions for joint use of SCE fee-owned rights-of-way for greenways, where feasible and compatible with SCE's secondary land use licensing program. The Parkside Specific Plan does not include an S.C.F. corridor.	X			7.7.9	X
1.16.5 The plan includes provisions for drainage channels to be designed as an open space amenity, maintaining them as natural open landscaped swales where feasible. When concrete channels are used, consider the development of landscaped greenways along their length.	X			5.0	
1.16.6 The plan includes provisions for drainage detention basins to accommodate passive and active recreational uses during dry periods, locating these adjacent to and extending local park uses. The Parkside Specific Plan "Great Park" description does not call out a drainage detention basin.	X			5.1	
1.16.7 The plan includes provisions for principal roadways including Grove Avenue, Vineyard Avenue, Archibald Avenue, Haven Avenue, Euclid Avenue, Milliken Avenue, and Edison Avenue to be designed as "parkways" with expanded right-of-ways containing landscaped medians and frontages, pedestrian paths, and other elements.	X				X
1.16.8 The plan incorporates extensive landscaping along street frontages in "urbanized" areas. (1-7 & 1-10) The Parkside Specific Plan is not in an urbanized area.	X			4.3, 4.4, 5.1, 7.0	
1.16.9 The plan includes design provisions for public greenways and open spaces to ensure public safety through the avoidance of physically and visually isolated spaces, maintenance of visibility and accessibility, use of lighting, and other "defensible" space concepts.	X				
<b>1.17 Electrical Energy Transmission Corridors</b>					
1.17.1 The plan includes pedestrian and bicycle trails in electrical energy transmission corridors to link neighborhoods and districts. The Parkside Specific Plan area does not include an electrical energy transmission corridor nor does it adjacent to one.	X			4.4	X
1.17.2 The plan accommodates such uses as parking lots, storage, commercial recreation, nurseries, greenways/trails, and complementary and compatible uses in electrical energy transmission corridors in areas designated as "Neighborhood Centers," "Community Commercial," "Town Center," and "Industrial/Business Parks" by the NMG General Plan. The Parkside Specific Plan area does not include an electrical energy transmission corridor nor does it adjacent to one.					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p><del>1.17.3 The plan accommodates development in electrical energy transmission corridors in areas abutting the Village Green, parks, lake, and other major open space/recreational amenities of uses that promote continuity with the amenity such as extensive landscape, tree farms and nurseries, buffers, and similar uses.</del></p> <p><del>The Parkside Specific Plan area does not include an electrical energy transmission corridor nor does it is adjacent to one.</del></p>					X
<p><del>1.17.4 The plan accommodates development in electrical energy transmission corridors in areas traversing “Residential Neighborhoods” of uses that serve as transitions among neighborhoods, pedestrian and biking trails, landscaped buffers, recreational facilities, and similar uses that are feasible and compatible with Southern California Edison’s secondary land use licensing program. At the corridor’s intersection with arterials and collectors, consider the accommodation of uses that support the primary residential activity (e.g. parking lots for churches), which shall be designated by the Specific Plan.</del></p> <p><del>The Parkside Specific Plan area does not include an electrical energy transmission corridor nor does it is adjacent to one.</del></p>					X
<p><del>1.17.5 The plan includes provisions for development located in electrical energy transmission corridors be designed to complement, be compatible with, and promote continuity and cohesiveness with abutting land uses. They should not be designated to create “walls” or barriers among uses.</del></p> <p><del>The Parkside Specific Plan area does not include an electrical energy transmission corridor nor does it is adjacent to one.</del></p>					X
<p><del>1.17.6 The Plan includes provisions encouraging Southern California Edison Company and private owners to visually improve, respectively, fee-owned and easement-owned rights-of-way that are retained for electrical transmission purposes to ensure their compatibility with abutting uses. This should emphasize the incorporation of landscape, parklands, and greenways.</del></p> <p><del>The Parkside Specific Plan area does not include an electrical energy transmission corridor nor does it is adjacent to one.</del></p>					X
<b>1.18 Phasing</b>					
<p><del>1.18.1 The plan includes a phasing plan to ensure adequate supporting public services, retail, parks, schools, and other uses are in place to support residential uses. These should establish increments of residential development that “trigger” and cannot be exceeded until the construction of appropriate improvements has been initiated. Standards for schools, infrastructure, and other public services should be determined in collaboration with pertinent service agencies. Standards for local serving retail, parks, and other uses should be confirmed by the City and reflect other policies contained in this Plan.</del></p>	X			5-8	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
1.18.2	X				
1.18.3	X			5.0, 6.12	
<b>2.1</b>					
2.1.1				Per proj. EIR	X
2.1.2					X
2.2.1					X
2.2.2					X
2.2.3					X
2.3.1					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>2.3.2 The plan includes a Transitional Roadway Plan that minimizes farm product transport/farm equipment conflicts with urban use related transport. Said plan includes the following:</p> <ul style="list-style-type: none"> <li>existing routes essential to the transportation of farm products through remaining agricultural areas and through non-agricultural areas as needed to access regional transportation routes;</li> <li>prioritize those roads that will be first to convert to primarily serving urban uses; and</li> <li>roadway signage and markings to inform drivers that farm transport vehicles and machinery may be using the roads.</li> </ul> <p>The Parkside Specific Plan land use designation does not include agricultural uses.</p>					X
<p>2.3.3 The plan provides measures to prevent urban runoff flooding and siting from impacting agricultural operations.</p> <p>The Parkside Specific Plan land use designation does not include agricultural uses.</p>					X
<p>2.3.4 Inform new residents and property owners that existing agricultural uses may create nuisances such as flies, odors, dust, noise, night light, and chemical spraying.</p> <p>The Parkside Specific Plan land use designation does not include agricultural uses.</p>					X
<p>2.3.5 The Plan includes provisions to protect agricultural lands from trespass, theft, vandalism, roaming dogs, and comparable impacts from urban uses.</p> <p>The Parkside Specific Plan land use designation does not include agricultural uses.</p>					X
<p>2.4.1 Oppose regulations that impact agricultural operations unless they are justified on the basis of sound environmental concerns.</p> <p>The Parkside Specific Plan land use designation does not include agricultural uses.</p>					X
<p>2.4.2 Assist farmers and agricultural land owners in their efforts to understand and abide by regulations and to process applications for permits and licenses.</p> <p>The Parkside Specific Plan land use designation does not include agricultural uses.</p>					X
<b>3.1 Housing</b>					
<p>3.1.1 Provide for the development of an estimated 20,996 single family and 10,792 multi-family dwelling units to sustain sound economic development (applies to entire 8,069 acre New Model Colony).</p>	X			4.2	
<p>3.1.2 Ensure that the densities and characteristics of residential projects fulfill the intent of applicable land use designations.</p>	X			4.2	
<p>3.2.1 The plan includes provisions for infrastructure needed to support anticipated residential development and ensures the proper integration of all services.</p>	X			5.1, 5-8	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
3.2.2 Maintain internal consistency among the General Plan's elements to provide the necessary services and infrastructure for urban development.	X			5.1-5.8	
3.3.1 The plan allows flexibility in the type of units developed on vacant, residentially designated properties in master-planned communities and other planned developments.	X				
3.3.2 The plan includes a mix of housing types.	X			4.2	
3.3.3 The plan encourages the development of residential uses in association with compatible nonresidential uses (i.e. mixed-use).	X			4.2, 4.3	
3.3.4 The plan provides housing opportunities for upper-income residents by designating sites in selected areas for large-lot development or sites adjacent to significant amenities. The Parkside Specific Plan promotes the development of high density housing. No land is allocated for large-lot development in the Parkside housing program.					X
3.4.1 The plan includes comprehensive design guidelines and development standards ensuring residential design that is functional, people and pedestrian-oriented, aesthetically pleasing, and contributes to a sense of community through the sensitive arrangement of buildings, open space (public and private), and circulation (vehicular and pedestrian).	X			7.0	
3.5.1 The plan includes provisions to integrate development with neighborhoods in a way that minimizes adverse impacts on privacy, noise levels, and traffic.	X			5.1	
3.5.2 The plan includes provisions for physical design elements of development (e.g., scale, street and circulation design, architectural elements) to complement and respect that of surrounding residential neighborhoods.	X			5.1, 6.41	
3.5.3 Where the new development's site design characteristics and standards will differ from those within an adjacent neighborhood, use design features (street trees, sidewalk siting and materials, etc.) that are common in both the neighborhoods as measures to transition from the surrounding neighborhood to the new neighborhood. The Parkside Specific Plan area is not located adjacent to any other existing neighborhoods.					X
<b>3.6 &amp; 3.7 Affordable Housing</b>					
3.6.1 Designate sufficient vacant land with maximum densities high enough to facilitate the development of housing affordable to lower-income households. A development agreement with the City of Ontario will in order that will satisfy all requirements of affordable housing.	X				

Policy	Fully Complies	Partially Complies	Docs-Not	S.P. Sec.-No.	N/A
<p><b>3.7.1</b> The Plan includes the provisions for affordable to very low, low, and moderate income households: A development agreement with the City of Ontario will be in order that will satisfy all requirements of affordable housing.</p>	X				
<p><b>3.7.2</b> The plan implements housing programs that comply with the State of California Housing and Community Development requirements, and ensures compliance and attainment of the regional housing need assessment "affordable" unit target. A development agreement with the City of Ontario will be in order that will satisfy all requirements of affordable housing.</p>	X				
<p><b>3.8 Housing for Special Needs</b></p>					
<p><b>3.8.1</b> The plan includes provisions for multi-family units specifically designed for the elderly located near Neighborhood Centers that provide access to transit and pedestrian access to amenities, goods, and services (i.e., recreational areas, commercial districts, and medical services). Age restricted housing product, potentially adjacent to the Neighborhood Center, will be selected by the Master Developer of builders at a future date.</p>		X			
<p><b>3.8.2</b> The plan includes provisions for rental units specifically designed for students to be located near the educational campus area. The Parkside Specific Plan includes provisions for residential units, but not adjacent to an educational campus.</p>					X
<p><b>PART II INFRASTRUCTURE AND PUBLIC SERVICE</b></p>					
<p><b>5.1 Water Sources</b></p>					
<p><b>5.1.3</b> Require Specific Plans and large development projects to prepare a water system planning study.</p>	X			5.2	
<p><b>5.1.4</b> Consider requiring the planning and construction of a dual pipe system to supply reclaimed water throughout the New Model Colony.</p>	X			5.2	
<p><b>5.2.1</b> Require new development to construct and dedicate water supply facilities.</p>	X			5.2, 6.13.1	
<p><b>5.2.2</b> Designate, preserve, and acquire land, as necessary, for water wells and storage facilities.</p>	X			5.2	
<p><b>5.3.1</b> The Plan requires improvements to the water supply facilities necessitated by new development be borne by the new development benefiting from the improvements, either through the payment of fees, or the actual construction of the improvements.</p>	X			5.2	



Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<b>6.1 Wastewater System and Treatment</b>					
6.1.1 Enable the planning for and construction of a wastewater system to support new development, as shown in <b>Figure 4-11 of the NMC-GPA</b> .	X			5.3	
6.2.1 The Plan requires new developments to connect to the sewer system.	X			5.3	
6.2.2 The Plan includes provisions requiring that sewer capacity and facilities are available before building permits are issued for new development.	X			5.3	
6.3.1 The Plan includes provisions requiring the costs of improvements to the existing wastewater collection facilities necessitated by new development be borne by the new development benefiting from the improvements; either through the payment of fees, or by the actual construction of the improvements.	X			5.3	
<b>7.1 Storm Drainage and Flood Control</b>					
7.1.2 Require Specific Plan and development projects to prepare a storm drainage planning study for the affected drainage area.	X			5.4	
7.1.4 Permit the joint use of detention basins for non-intensive recreational purposes, in accordance with City policy.	X			5.4	
7.1.5 Require that the refined backbone infrastructure plan will be used in the development process to ensure that each project will construct adequate drainage facilities. A detailed drainage master plan must be in place and must have San Bernardino County Flood Control District concurrence prior to any major development approval.	X			5.4	
7.1.6 Strategically locate detention basins to mitigate downstream drainage concern as development occurs. Siting of these facilities will take into consideration potential impacts to flight paths of the crosswind runways of the Chino Airport. Detention basins shall be located as required to bring storm water flows to a safe and acceptable level downstream of Merrill Avenue. However, basins shall not be located within Referral Areas A and B and Safety Zones I and II around the Chino Airport so as to prevent potentially significant wildlife hazards to aircraft operations.	X			5.4	
7.1.7 The plan includes provisions requiring development plans to be consistent with and implement the Master Plan of drainage for the area as finally adopted.	X			5.4	
7.2.1 The Plan requires new development to control surface run-off through onsite measures.	X			5.4	
7.2.2 The Plan requires new development to construct and dedicate flood control and storm drainage facilities.	X			5.4, 6.6	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
7.2.3 Designate, preserve, and acquire land, as necessary, for storm drainage and flood control facilities.	X			5.4	
7.2.6 The Plan requires developers of each proposed project to submit a final drainage plan for the City Engineer's review and approval.	X			5.4, 6.9	
7.2.8 The plan requires developers to recommend measures which ensure that all structures located within the boundaries of the New Model Colony, subject to flooding from 100-year storm events, are constructed on a pad of earth elevated at least one foot above 100-year flood elevations. The recommended measures must be approved, monitored, and enforced by the City Engineer.	X			5.4, 6.6	
7.2.9 The Plan requires developers to provide evidence to the City Engineer that a National Pollutant Discharge Elimination System (NPDES) permit has been obtained from the State Water Resources Control Board (SWRCB) prior to moving construction equipment onto a New Model Colony site. Once obtained, the NPDES permit shall be retained on the construction site throughout the construction period, and a copy shall be filed with the City Engineer.	X			5.4, 6.6	
7.2.10 The plan includes provisions to ensure compliance with all the terms and conditions outlined in the National Pollutant Discharge Elimination System (NPDES) permit, including the implementation of Best Management Practices (BMPs).	X			5.4	
7.2.11 The Plan includes provisions requiring developers to prepare a Storm Water Pollution Prevention Plan (SWPPP) for individual proposed projects prior to the issuance of grading permits. These plans shall be submitted to the City Engineer for review and comment prior to implementing any SWPPP provisions or starting any construction activity. A copy of the SWPPP shall be held by the construction contractor(s) on the construction site throughout development of each project. The City Engineer will monitor and enforce the provisions of the SWPPP.	X			5.4.1	
7.2.12 The Plan includes provisions requiring the developer to prepare a Water Quality Management Plan in compliance with the Regional NPDES Permit requirements for New Development projects. The plan shall be submitted to the City Engineer for review and comment prior to issuance of any grading permits or any construction activity. A copy of the approved WQMP shall be held by the developer at the business office or at the construction site.					
<b>8.1 Schools</b>					
8.1.2 The Plan accommodates sufficient schools to meet School District criteria as shown in <b>Table 4-10 of the NMC GPA</b> . Based on these, the Land Use Plan's buildout will necessitate an estimated 20 elementary school sites, 5 middle school sites, and 3 high school sites within the total New Model Colony, with the number of schools in each planning sub-area indicated in <b>Table 3-4 of the NMC GPA</b> . The required number of schools to be constructed may vary due to modifications of the school district's standards to account for demographic changes, the number and types of dwelling units, resident characteristics, and other pertinent criteria.	X			5.5	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p><b>8.1.3</b> Locate schools within a designated Neighborhood Center to the extent feasible, as illustrated in <b>Figures 3-8 and 3-9 of the NMC GPA</b>.  The elementary, middle, and high schools planned for in the New Model Colony master plan will serve the Parkside Specific Plan, but currently there are no schools planned within the Specific Plan area, or within the Neighborhood Center in the Specific Plan.</p>					X
<p><b>8.1.4</b> Encourage schools to be located as neighborhood focal points, with linkage to recreational uses, pedestrian paths, and bike ways, when locating schools in a Neighborhood Center is not feasible.  The elementary, middle, and high schools planned for in the New Model Colony master plan will serve the Parkside Specific Plan, but currently there are no schools planned within the Specific Plan area.</p>					X
<p><b>8.1.5</b> Encourage the consolidation or location of school campuses adjacent to one another, thereby enabling a joint use of recreational space, library, auditoriums, and other facilities.  The elementary, middle, and high schools planned for in the New Model Colony master plan will serve the Parkside Specific Plan, but currently there are no schools planned within the Specific Plan area.</p>					X
<p><b>8.1.6</b> Locate schools to avoid impacts attributable to Chino Airport. To ensure efficient accessibility of schools from residential neighborhoods, work with the California Department of Transportation, Division of Aeronautics, and Department of Education to permit schools to be developed at acceptable locations within the Airport's two-mile radius.  The elementary, middle, and high schools planned for in the New Model Colony master plan will serve the Parkside Specific Plan, but currently there are no schools planned within the Specific Plan area.</p>					X
<p><b>8.1.8</b> Encourage the school districts to design schools so that their periphery maintains an appropriate transition with adjoining residential neighborhoods; avoiding the use of walls, fences, and landscape which establish a visual barrier.  There are no schools within or adjacent to the Parkside Specific Plan area.</p>					X
<p><b>8.1.11</b> Locate preschool and day-care facilities in appropriate areas throughout the New Model Colony to meet the needs of a growing population. Require specific planned areas to designate sites for day-care facilities.</p>	X			6.15	
<b>9.1 Police Protection</b>					

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>9.1.3 The plan identifies appropriate sites for the construction of a police station within the neighborhood Centers, Town Center, or other activity nodes.</p> <p>Police Department services for the Parkside Specific Plan are provided by the City of Ontario. The nearest police station is approximately 2.2 miles northeast of the Specific Plan area.</p>	X			5.5.3	
<p>9.1.4 The plan encourages consolidation of public facilities, thereby creating mini civic centers comprised of police stations, libraries, fire stations, schools, parks, and administrative offices, etc.</p> <p>As part of the Parkside Specific Plan, the nearest police station (2500 Archibald Ave.) was relocated to be more centrally located to the New Model Colony.</p>	X			5.5.3	
<p>9.2.1 The plan incorporates defensible space designs. These designs should help ensure maximum visibility and security for entrances, pathways, and corridors, as well as open space (both public and private) and parking lots/structures.</p>	X			4.2, 6.1 46.15	
<p>9.2.2 The plan includes lighting provisions providing adequate exterior illumination to facilitate security surveillance around commercial, industrial, multi-family, and public structures.</p>	X			6.14, 6.15	
<b>10.1 Fire Protection and Emergency Medical Service</b>					
<p>10.1.2 Require that a total of three fire stations be incorporated within the Neighborhood centers, Town center, or other activity nodes.</p> <p>Fire Department services for the Parkside Specific Plan are provided by the City of Ontario. Currently, the nearest fire station is approximately 2.5 miles northeast of the Specific Plan area, and a new fire station will be built in Planning Area 11.</p>	X			5.5.2	
<p>10.1.3 The plan encourages the consolidation of public facilities thereby creating mini civic centers comprised of police stations, libraries, fire stations, schools, parks, and administrative offices, etc.</p> <p>The Parkside Specific Plan includes a new fire station in Planning area 11, which is adjacent to the commercial component of the plan, and the linear park.</p>	X			5.5.2	
<b>11.1 Circulation</b>					
<p>11.1.1 The plan promotes the timely development of the backbone circulation plan, as shown in <b>Figure 4-14</b> – NMC GPA that is phased and based on projected needs generated by the implementation of the NMC GPA Land Use Plan and the surrounding uses.</p>	X			5.1	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
11.1.2 Develop a detailed master plan to phase the backbone transportation infrastructure in a logical manner. The master plan should address coordination with each of the Specific Plan's master planning efforts and funding mechanisms including cost sharing and future credits.	X			5.1, 6.12	
11.1.3 Implement hierarchy of roadways and roadway classifications, as shown in <b>Figure 4-15</b> , <b>Figure 4-16</b> , and <b>Figure 4-17 of the NMC GPA</b> , that provides for efficient movement of regional through traffic and also protects the residential neighborhoods from intrusion of through traffic.	X			5.1	
11.1.4 Reserve adequate rights-of-way for roadways to implement the hierarchy of local roads within the New Model Colony that is consistent with the planned land uses.	X			5.1	
11.1.5 The plan establishes safe and efficient truck routes that minimize exposure to noise sensitive land uses and reduce other adverse impacts to adjacent land uses. (see <b>Figure 4-18 — NMC GPA</b> .)	X			5.1	
11.1.6 The plan consider the development of high-capacity, regional traffic serving facilities in the east-west and north-south direction through the New Model Colony, such as Edison Avenue, Euclid Avenue, Archibald Avenue, and Milliken Avenue.	X			5.1	
11.1.7 The plan identifies appropriate intelligent transportation system (ITS) elements to be incorporated into the New Model Colony's transportation infrastructure. The ITS elements include such technologies as changeable message signs, closed circuit television, highway advisory radio, loop detectors, fiber optic communications connections, and links to regional traffic management centers.	X			Per proj. EIR	
11.1.8 The plan incorporates a transitional Roadway Plan that minimizes agricultural vehicle-urban vehicle conflicts, minimizes agricultural vehicles impact on non-agricultural uses, and transitions the New Model Colony's roadways from agricultural oriented to urban oriented.	X			Per proj. EIR	
11.1.9 The plan augments and implements the comprehensive Citywide Traffic Model within the New Model Colony area to reflect the New Model Colony land uses and the proposed circulation system.	X			Per proj. EIR	
11.2.1 The plan maintains a level of service not to exceed LOS D for intersections during the peak hours.	X			Per proj. EIR	
11.2.2 The plan maintains a peak period level of service not to exceed LOS D for collector and arterial roadways.	X			Per proj. EIR	
11.2.3 The plan maintains a peak period level of service not to exceed LOS C for residential streets.	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
11.3.1 Coordinate with Riverside County and Caltrans to ensure the timely design and implementation of the Galena Interchange on the I-15 Freeway.	X			Per proj. EIR	
11.3.2 Coordinate with Caltrans and monitor traffic growth around freeway interchanges to determine the need, timing, and design for ramp improvements and additional right-of-way needs at the Route 60 Freeway interchanges.	X			Per proj. EIR	-
11.3.3 Actively support the County of San Bernardino's Congestion Management Plan.	X			Per proj. EIR	
11.3.4 Support and implement locally applicable portions of the Regional Mobility Plan and Air Quality Management Plan.	X			Per proj. EIR	
11.3.5 Coordinate the development of the New Model Colony's circulation plan with adjacent jurisdictions and regional agencies, including City of Chino to the west and Riverside County to the east and south, for proper future connectivity and service to regional trips.	X			Per proj. EIR	
11.3.6 Specific Plans for proposed developments within the NMC shall not be approved and adopted until such time as the San Bernardino County Congestion Management Program Traffic Impact Analysis (CMP TIA) for the New Model Colony Congestion Management Program Traffic Impact Analysis. This CMP TIA shall identify improvement measures to mitigate impacts of the traffic generated by the New Model Colony on transportation systems in the City of Ontario, the unincorporated San Bernardino County and all neighboring cities, which fall within the five-mile radius (designated impact area) of the New Model Colony boundaries, per CMP TIA guidelines.	X			Per proj. EIR	
11.3.7 The Plan includes a separate traffic and circulation studies to: <ul style="list-style-type: none"> <li>• Assess internal circulation system need and to develop a traffic circulation plan for the Specific Plan area;</li> <li>• Identify regional transportation infrastructure connectivity requirements; and</li> <li>• Identify specific traffic impacts related to the buildout of the Specific Plan on the surrounding areas (outside the Specific Plan) and to identify traffic improvement measures to mitigate these impacts.</li> </ul>	X			Per proj. EIR	
11.3.8 Circulate all relevant traffic and circulation studies prepared for proposed Specific Plan developments within the New Model Colony area to all neighboring local jurisdictions that may be potentially impacted by the traffic generated from the buildout of the Specific Plan's land uses.	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>11.3.9 For the following arterial segments outside the New Model Colony:</p> <ul style="list-style-type: none"> <li>• Euclid Avenue — Riverside Drive to Philadelphia Street</li> <li>• Grove Avenue — Saint Andrews Street to 4<sup>th</sup> Street</li> <li>• Vineyard Avenue — Riverside Drive to Route 60</li> <li>• Archibald Avenue — Riverside Drive to Philadelphia Street</li> <li>• Haven Avenue — Creekside to Route 60; Mission Boulevard to Airport Drive</li> <li>• Milliken Avenue — Riverside Drive to Mission Boulevard</li> </ul> <p>Provide additional through-traffic lanes by:</p> <ul style="list-style-type: none"> <li>• widening the roads to accommodate the ultimate number of lanes designated in the General Plan;</li> <li>• modifying functional classification for arterials to accommodate additional traffic lanes;</li> <li>• restriping; and</li> <li>• using parking restrictions and other measures, where feasible.</li> </ul>	X			Per proj. EIR	
11.4.1 Require each major development phase or Specific Plan to develop a master plan of streets and conduct a comprehensive traffic impact study, as appropriate.	X			5.1, Per proj. EIR	
11.4.2 As condition of development for each planning area, require that proper connecting facilities and capacity be provided to the regional transportation system to minimize impacts to existing roadways.	X			5.1, Per proj. EIR	
11.4.3 Require that development within the New Model Colony be consistent with the provisions of the County-wide Congestion Management Program.	X			4.2, per proj. EIR	
11.4.4 The plan prohibits future development from providing backout driveways onto arterial roadways.	X			4.2, 5.1	
11.4.5 The plan discourages future development from providing backout driveways onto collector roadways.	X			4.2, 5.1	
11.4.6 The plan considers the use of alley ways (20 feet recommended width) to add flexibility to frontage designs and assist in the creation of more pedestrian-oriented front areas.	X			4.2, 5.1	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
11.4.7 The plan promotes development of local street patterns that create and unify neighborhoods, rather than divide them, through the following means: <ul style="list-style-type: none"> <li>local street patterns should provide access between subdivisions within a neighborhood, with the exception of through traffic which should be directed onto major and secondary arterials; and</li> <li>the local street system should be logical and understandable for the user. Creation of circuitous and confusing travel paths between internal neighborhood areas and adjacent arterials should be avoided.</li> </ul>	X			4.2, 5.1	
11.4.8 The plan includes measures requiring developers to provide all required onsite infrastructure and contribute their proportional share to offsite improvements. (1-30, 1-31, and 1-32)	X			5.1-5.8	
11.4.9 The plan includes an impact fee system in order to offset the cost of transportation required by new development.	X			Per City-DF	
11.4.10 Consider whether it is appropriate to establish a transportation benefits district. The Parkside Specific Plan may not be a candidate for a transportation benefits district.	X			Per proj.-EIR	
11.5.1 The plan includes a comprehensive Transportation Mobility Plan to improve the movement of pedestrians, bicyclists, public transit, truck and automobile, freight, passenger rail, etc., within the New Model Colony.	X			4.2, 5.1	
11.5.2 The plan integrates the Transportation Mobility Plan with the Congestion Management Planning process to identify and develop necessary transportation services, and assist land use and urban design decision making.	X			Per proj.-EIR	
11.5.3 The plan promotes a variety of transportation modes.	X			4.2, 4.4, 5.1	
11.5.4 The plan explores the possibility of developing transit centers within the appropriate Specific Plan areas such as the Town Center, educational campus, and adjacent to the Regional Centers along Milliken Avenue and Euclid Avenue. The Parkside Specific Plan does not include a Town Center, and educational campus, or a Regional Center.					X
11.5.5 Analyze cost/benefit ratios in determining future publicly financed transportation improvements. A feasibility study has not been done for financing transportation improvements.					X
11.5.6 Consult and coordinate with the various transit agencies in order to bring about improved transit service to the New Model Colony.	X			Per proj.-EIR	



Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
11.5.7 Through coordination with transit agencies, the plan provide transit friendly design with the objective of maximizing the potential for transit use.	X			Per proj. EIR	
11.5.8 The plan includes provisions requiring development to fund transit facilities, such as transit centers, bus shelters and turnouts, where feasible.	X			Per proj. EIR	
11.5.9 The plan includes provisions requiring that development provide for transit-friendly features such as buildings located close to streets, bus amenities, etc.	X			4.2	
11.5.10 The plan encourages the development of connections with commuter rail stations in the vicinity of the New Model Colony. There are no commuter rail stations in the vicinity of the Parkside Specific Plan area.					X
11.6.1 The plan encourages nonresidential developments to provide employee incentives for using alternatives to the conventional single-occupant automobile (i.e., carpools, vanpools, buses, bicycles, walking, telecommuting, etc.)	X			Per proj. EIR	
11.6.2 The plan supports national, state, and regional legislation directed at encouraging the use of carpools and vanpools.	X			Per proj. EIR	
11.6.3 The plan encourages the establishment of Transportation Management Associations (TMAs) within the commercial and industrial areas of the New Model Colony. The Parkside Community Plan commercial area is not large enough to merit a TMA.					X
11.6.4 The plan promotes the use of alternative transportation modes to the single-occupancy vehicle within the New Model Colony, such as carpooling, bus and rail transit and explore the feasibility of linking such systems with other regional transportation systems.	X			4.2, 5.1	
11.6.5 The plan promotes ridesharing through publicity and provision of information to the public.	X			Per proj. EIR	
11.6.6 The plan encourages the preservation and development of freight and passenger/commuter rail that occur or would occur in the region. The Parkside Specific Plan region does not include a freight or passenger commuter rail.					X
11.6.7 The plan encourages a telecommunications center within the planning area to reduce vehicle miles traveled.	X			5.6, 6.14	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No. Per proj. EIR	N/A
11.6.8 Develop a program for traffic signal coordination and synchronization for the arterial street system in coordination with and connecting to the City's signal system.	X				
11.7.1 The plan requires parking to be consolidated along parkways, arterials, and pertinent collectors, eliminating the number of ingress and egress points. Prime candidate locations for consolidated parking are the "Neighborhood Centers", "Town Center", "Business Parks", and "Regional Centers". (1-7 and 1-10)	X			4.2, 5.1	
11.7.2 The plan requires that sufficient and secure bicycle parking be provided in all parking areas.	X				
11.7.3 The plan promotes sharing of off-street parking facilities.				6.15	
<b>PART III AESTHETIC, CULTURAL, OPEN SPACE &amp; RECREATIONAL RESOURCES</b>					
<b>12.1 Parks &amp; Open Space</b>					
12.1.1 Provide a minimum 509 acres of parkland consisting of a mix of mini, neighborhood, community and "village green" facilities to achieve a standard of 5 acres of parkland per 1,000 residents. The actual park acreage may vary as a result of future modifications of the City standards.	X			4.3	
12.1.2 Update the Parks and Bike Trail Master Plan to provide specific criteria and guidelines for the siting, design and programming of parks and recreational facilities. Consider the following when creating the criteria for acquiring or receiving dedicated parkland: <ul style="list-style-type: none"> <li>The usability of proposed parklands and the ease of accessibility for future users; and</li> <li>Balance of passive and active recreational opportunities and facilities designed to meet the existing and future needs of all user groups.</li> </ul> The Parks and Bike Trail Master Plan does not include an update to the Parks and Bike Trail Master Plan.					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>12.1.3 The plan incorporate a comprehensive and unified parks and recreation plan that:</p> <ul style="list-style-type: none"> <li>• identifies mini, neighborhood, and community park sites in accordance with the service standards and updated Parks and Bike Trail Master Plan criteria;</li> <li>• integrates neighborhood parks with Neighborhood Centers and schools (see <b>Figure 5-2</b>);</li> <li>• links parks by pedestrian greenway and bike trail networks (see <b>Figure 5-3</b>);</li> <li>• incorporates passive and active recreational uses as specified in the Parks and Bike Trail Master Plan; and</li> <li>• defines a park acquisition and improvement financing plan. (I-3 and I-34)</li> </ul>	X			4.2 4.3 4.4 5.1 5.1.3 5.1.4	
<p>12.1.4 The plan encourages:</p> <ul style="list-style-type: none"> <li>• golf course site(s) be developed in the New Model Colony;</li> </ul> <p>The Parkside Specific Plan does not include a golf course:</p> <ul style="list-style-type: none"> <li>• school and park facilities be located in a manner that permits shared use of auditoriums, playing fields, and so on;</li> <li>• public facilities (auditoriums, amphitheaters, day-care centers, public meeting rooms) be integrated into park design; and</li> <li>• The Parkside Specific Plan does not include public facilities;</li> <li>• drainage channels be developed as an open space amenity. (I-3, I-7, and I-47)</li> </ul>	X	X		4.1, 4.3 5.4	X X
<p>12.1.5 The plan requires Sub area 22 (Town Center) Specific Plan to locate park(s) and/or public open space along the lake, if the lake is developed.</p> <p>The Parkside Specific Plan does not incorporate park/public open space by a lake.</p>					X
<p>12.1.6 The Plan requires that Sub area 16 and Sub area 17 Specific Plans identify sites for the development of a linear “village green” park. The Village Green will act as a recreational focal point serving the entire New Model Colony. The Village Green shall incorporate a diversity of passive and active uses including athletic fields, picnic areas, botanical gardens, museums, and performance venues.</p>	X			4.3.2, 4.3.3	
<p>12.1.7 The plan requires that residential neighborhoods located next to a park provide pedestrian greenway and bike trails to the park.</p>	X				

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
12.1.8 The plan encourages storm drainage detention basins to be designed to accommodate passive and active recreational uses during the dry periods (see <b>Figure 5-4 NMC GPA</b> ).	X			5.4	
12.1.9 The plan requires the use of extensive landscaping along street frontages. Landscaping elements should include tree and plant specimens currently found within the New Model Colony. (1-7 and 1-10)	X			5.1, 7.0	
12.1.10 The plan requires the dedication and development of future community and neighborhood park sites before a significant proportion of the new population to be served by the park exists. (1-7 and 1-10)  The Parkside Specific Plan provides a guideline for the development of the park sites located within the plan area, and the remaining specific plan area is dedicated for other land uses and therefore could not be dedicated for future park sites.					X
12.1.11 The plan requires that new multi-family residential developments of five or more units provide recreational or open space facilities onsite and contribute appropriate fees that aid in the public development of other facilities to offset additional demands generated by their resident population. (1-7 and 1-10)	X			4.2, 4.3	
12.1.12 The plan requires that large-scale commercial developments, such as the Town Center, Community Centers, Regional Centers, provide open space facilities within the development for passive or active recreation or contribute fees for the public development of such uses. (1-7 and 1-10)  The Parkside Specific Plan does not include a Town Center, a Community Center, or a Regional Center.					X
12.1.13 The plan encourages industrial and business parks to incorporate employee recreational facilities within their developments or contribute fees for the public development of such uses. (1-7 and 1-10)  The Parkside Specific Plan does not include an industrial park or a business park.					X
12.1.14 The plan encourages agricultural industry representatives and local university (ies) to provide educational and recreational programs that reflect the New Model Colony's agricultural history. (1-50)  The Parkside Specific Plan does not specify any programs to reflect the New Model Colony's agricultural history.					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
13.1.1 The plan incorporates a comprehensive network of greenways, pedestrian paths, and bike trails (see <b>Figures 5-3 and 5-5 - NMC GPA</b> ), linking the Town Center, educational campus (if developed), Regional Centers, Community Commercial, Neighborhood Centers, "Village Green," neighborhood parks, and schools. (1-7 and 1-10)	X			4.3.2	
13.1.2 Require developments to link their internal trail system with the surrounding New Model Colony network.	X			4.4, 5.1.3, 5.1.4	
13.1.3 The plan encourages infrastructure rights-of-ways or easements to be designed and developed to accommodate trails and bikeways where feasible and where compatible with the intended primary use. (1-47)	X			4.4	
13.1.4 The plan includes provisions providing for joint use of SCE easement-owned rights-of-way for trails and bikeways where feasible and compatible with the intended primary use. (1-10 and 1-47)  The Parkside Specific Plan area does not provide for joint use of an SCE easement; none exists.					X
13.1.5 The plan includes provisions providing for joint use SCE's fee-owned rights-of-way for trails and bikeways where feasible and compatible with SCE's secondary land use licensing program. (1-10 and 1-47)	X			4.4	
13.1.6 Work with the surrounding jurisdictions to ensure trail connections between the New Model Colony and other regional recreational destinations or amenities. (1-51)	X			4.3.2, 4.4	
14.1.1 The plan includes measures to extensively landscape Euclid Avenue, Grove Avenue, Vineyard Avenue, Archibald Avenue, Milliken Avenue, and Edison Avenue by the use of "enhanced parkways" with landscaped medians and rights-of-way (see <b>Figure 5-3 - NMC GPA</b> ). (1-7 and 1-10)	X			5.1	
14.1.2 The plan requires that comprehensive street tree and landscape plans be established to uniquely identify parkways, neighborhoods, centers, and districts. (1-7)	X			Per city Master Plan	
14.1.3 The plan requires that the landscaping within the public medians and rights-of-way as well as along the private developments' street frontage is well designed and properly maintained, minimizes water usage, and maximizes visual continuity while permitting individual expression. (1-25)	X			6.13.1	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
14.1.4	X			7.0	
14.1.5		X		4.2	
<b>15.1 Historic and Cultural Resources</b>					
15.1.1	X			Per proj. EIR	
15.1.2	X			Per proj. EIR	
15.1.3	X			Per proj. EIR	
15.1.4	X			Per proj. EIR	
15.1.5	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
15.1.6 Conduct an intensive archaeological field survey under the supervision of a Society of Professional Archaeologists (S.O.P.A.) certified archaeologist for each proposed project not previously surveyed within the past ten years. A technical report following format and content guidelines proposed by the Office of Historic Preservation must be completed. (1-10)	X			Per proj.-EIR	
15.1.7 The plan includes provisions requiring that each project resulting in an adverse impact on a known significant resource prepare an appropriate planning approach to reduce the impact to a level of insignificance. (1-10)	X			Per proj.-EIR	
15.1.8 The plan includes provisions requiring that a qualified archaeologist be retained in order to monitor the grading, identify any cultural resources that may be exposed, complete a preliminary evaluation of the resource, and recommend appropriate resource management for the treatment of the resource. (1-10)	X			Per proj.-EIR	
<b>16.1 Libraries</b>					
16.1.1 The plan includes provisions requiring that library site(s) be developed within the Neighborhood Centers and Town Centers, and near transit facilities, bike paths, and pedestrian paths. (1-7)	X			Per proj.-DIF	
16.1.2 The plan encourages consolidation of public facilities thereby creating "mini-civic centers" comprised of police station(s), library(ies), fire stations, schools, parks, and administrative offices, etc. (1-7) The Parkside Specific Plan does not include any public facilities (with the exception of the park and the fire station).					X
16.1.3 Construct a minimum of 61,107 square feet library facilities in order to provide the estimated 101,845 New Model Colony residents with adequate library facilities based on the City's current standard of 0.6 square feet per resident, or as modified to reflect technology. (1-10 and 1-20)	X			Per proj.-DIF	
16.1.4 The plan encourages the installation of new technology including a fiber optic network for telecommunications. (1-22)	X			5-6	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
16.1.5 The plan encourages the joint use of school libraries and public library facilities. (1-47)The Parkside Specific Plan does not include a library or a school.					X
<b>PART IV NATURAL RESOURCES</b>					
<b>17.1 Aggregate Resources</b>					
17.1.1 Identify the locations of aggregate resources and coordinate their location with the appropriate land use designations. (1-19 and 1-23)	X			Per proj. EIR	
<b>18.1 Biological Resources</b>					
18.1.1 Work with and encourage the Army Corps of Engineers, Chino Basin SAPA, and County Flood Control to retain a minimum of 160 acres of surface water in the El Prado Basin. This water shall serve as the habitat mitigation for waterfowl to offset the discontinued use of State-mandated dairy manure-water retention basins. (1-36 and 1-47)	X			Per proj. EIR	
18.1.2 Ensure that development projects proposed for areas that support and retain waterfowl and raptor habitat:	X			Per proj. EIR	
<ul style="list-style-type: none"> <li>• incorporate fences, walls, vegetative cover, or other measures to buffer waterfowl areas from the built environment;</li> <li>• locate and design roads and utilities in such a manner that they will not conflict or impact habitat areas; and</li> <li>• permit only the following uses: <ul style="list-style-type: none"> <li>• educational and research facilities (excluding buildings);</li> <li>• passive (non-motorized, non-consumptive) recreation;</li> <li>• trails on public land(s) if located outside of a undeveloped buffer zone;</li> <li>• fish and wildlife management activities; and</li> <li>• flood control projects where no other viable methods are available to protect the public safety. (1-7 and 1-10)</li> </ul> </li> </ul>					



Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>18.1.3 The plan includes measures to ensure that development projects that incorporate onsite mitigation for the loss of existing waterfowl and raptor habitat:</p> <ul style="list-style-type: none"> <li>• are reviewed by a City-approved biologist who conducts an onsite assessment to determine whether the mitigation is sufficient based upon the quality, location, or size of the area to be lost;</li> <li>• include a Biological Assessment Report that addresses the proposed project's impact on state and federally listed and candidate plants and animals; California Department of Fish and Game Special Animals; waterfowl or raptor habitat and any other special interest species or communities identified in the General Plan Analyses of Existing Conditions and Trends Report, or those hereafter named by state or federal trustee agencies;</li> <li>• assess project conformance to specific habitat objectives described above;</li> <li>• identify mitigation measures necessary to eliminate significant adverse impacts to sensitive biological resources; and</li> <li>• define a program for monitoring and evaluating the effectiveness of the specified mitigation measures. (1-7 and 1-10)</li> </ul>	X			Per proj.-EIR	
18.1.4 Periodically update the Biological Habitat Map (see Figure 6-5 -- NMC GPA) and data base to maintain current information regarding waterfowl and raptor habitat. (1-6 and 1-11)	X			Per proj.-EIR	
18.1.5 The plan includes sufficient technical data to enable an adequate assessment of the potential for impacts on biological resources. Such technical data shall include species lists, habitat use, acreage of habitat, and descriptions of any vegetation. (1-7 and 1-10)	X			Per proj.-EIR	
18.1.6 The plan assesses impacts on local biological resources and recommend appropriate mitigation measures, if necessary to account for specific development characteristics or site conditions that are not adequately addressed by the New Model Colony General Plan Environmental Impact Report. (1-7 and 1-10)	X			Per proj.-EIR	
18.1.7 The plan includes provisions encouraging development plans take all reasonable measures to avoid creating significant impacts. (1-7 and 1-10)	X			Per proj.-EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
18.1.8 The plan includes provisions encouraging development plans take all reasonable measures to avoid altering the biological integrity. Reasonable measures may include but are not limited to vegetative restoration, control of alien plants and animals, and use of landscaping buffers. (1-7 and 1-10)	X			Per proj. EIR	
18.1.9 The plan includes provisions for enhancing the ponded areas adjacent to the flood control channel (i.e., planting native trees to serve as buffers). (1-36)	X			Per proj. EIR	
18.1.10 The plan includes provisions requiring dedicated open space easements as a condition of approval for development projects proposed adjacent to waterfowl and raptor habitat areas that are voluntarily retained in the New Model Colony. These open space easements shall assure that the biological habitats are buffered and protected from degradation and shall be used only for passive recreational/educational purposes. (1-7 and 1-10)	X			Per proj. EIR	
18.1.11 Ensure that development projects located adjacent to waterfowl or raptor habitat areas that are voluntarily retained in the New Model Colony: <ul style="list-style-type: none"> <li>Maintain a 100-foot buffer (minimum) from the edge of the habitat on the subject property;</li> <li>maintain connection to other preserved habitats, where they exist; and</li> <li>dedicate open space easements to the City, homeowner associations, or other responsible party. (1-7 and 1-10)</li> </ul> The Parkside Specific Plan area does not contain waterfowl or raptor habitat.	X			Per proj. EIR	
18.1.12 Establish an onsite "Wildlife Habitat" or Waterfowl and Raptor Conservation Area (WRCA) in the areas adjacent to the Chino Basin Flood Control Ponds located to the south of Chino Avenue, west of Archibald Avenue, north of Seaefer and east of Whispering Lakes Lane consisting of approximately 145 acres adjacent to the 85 acres of existing county-owned detention basins. (1-6) Refer to p. 6-27, NMGCPA for complete discussion on the two approaches to establishing the WRCA.	X			Per proj. EIR	
<b>18.2 Recovery of Delhi sands flower-loving fly</b>					
18.2.1 Monitor the efforts of the Department of Fish and Wildlife to study the recovery of the Federally listed Delhi sands flower-loving fly. Assess the impacts of recovery recommendations for development in the New Model Colony. (1-5 and 1-47)	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>18.2.2 The City shall cooperate with the U.S. Fish and Wildlife Service in the following ways to mitigate potential impact to the DSFLF:</p> <ul style="list-style-type: none"> <li>cooperate with USFWS to ensure, through the Specific Plan Process, that potential recovery areas are appropriately mitigated;</li> <li>establish a standard for buffers for protecting DSFLF restoration areas, in cooperation with USFWS; and</li> <li>work with rights-of-way owners and USFWS to explore the possibility of creating DSFLF habitat within these undeveloped strips. (1-5 and 1-47)</li> </ul>	X			Per proj. EIR	
<b>18.3 Increased public awareness of local biological resources</b>					
<p>18.3.1 Sponsor or co-sponsor "balanced" educational programs that:</p> <ul style="list-style-type: none"> <li>promote awareness of local biological resources;</li> <li>inform about protection and preservation programs;</li> <li>foster community attitudes and behaviors that help protect local plants and wildlife; and</li> <li>encourage community involvement in protection programs. (1-50)</li> </ul>	X			Per proj. EIR	
<b>PART IV HAZARDS</b>					
<b>19.1 Geologic and Seismic Hazards</b>					
19.1.1 The Plan includes requirements to validate the presence of unstable NMGIs, areas of liquefaction potential, unique geological features, and shallow water tables.	X			Per proj. EIR	
19.2.1 Identifies the location and type of utilities within the area and identifies emergency procedures in case of breaks, rupture, or other disruption.	X			Per proj. EIR	
19.2.2 Identifies the location and type of utilities within the area and identify emergency procedures in case of breaks, rupture, other disruption.	X			Per proj. EIR	
19.3.1 Determine portions of the New Model Colony are subject to subsidence.	X			Per proj. EIR	
19.3.2 Update planning (including setbacks requirements), building (including design considerations), grading, and engineering requirements, as necessary, to protect life and property located near potential subsidence and fissure zones.	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<b>20.1 Protection from Fire Hazards</b>					
20.1.1 Requires two viable points of ingress and egress from development projects.	X			4.2, 5.1	
20.2.1 The Plan includes procedures to ensure that manure storage practices are not conducive to the generation of fires due to spontaneous combustion.	X			Per proj. EIR	
<b>21.1 Dust, Wind and Blow Sand</b>					
21.1.1 The Plan requires that structures be sited and designed to prevent adverse funneling of wind onsite and on adjacent properties.	X			Per proj. EIR	
21.2.1 The Plan requires provisions to construction for adherence to NMCI Erosion Control Area of City-mandated dust control programs.	X			Per proj. EIR	
21.2.2 The Plan requires that technical information and mitigation methods examples, regarding wind blown sand, available to the general public.	X			Per proj. EIR	
21.2.3 The Plan provides provisions to coordinate with other jurisdictions to maximize wind blown sand control planning.	X			Per proj. EIR	
<b>22.1 Soils</b>					
22.1.1 The Plan requires that geologic deposit technical information, erosion potential identification methods, and erosion control examples are made available to developers.	X			Per proj. EIR	
22.1.2 The Plan requires that NMCI's engineering problem information, identification methods, and mitigation method examples are made available to developers.	X			Per proj. EIR	
22.1.3 The Plan includes provisions requiring proposed development projects to determine if the project would be located in or near areas with significant erosion potential or NMCI engineering problems. Require proposed project applications to include a detailed discussion regarding the types of NMCI and locations, erosion potential of NMCI engineering problems, and erosion control plans. Mitigation plans must address methods to be used during all phases of project development, implementation, and operation.	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
22.1.4	X			Per proj. EIR	
22.1.5	X			Per proj. EIR	
22.1.6	X			Per proj. EIR	
22.2.1	X			Per proj. EIR	
<b>23.1 Hazardous Materials &amp; Waste</b>					
23.1.1	X			Per proj. EIR	
23.1.3	X			Per proj. EIR	
<b>24.1 Noise</b>					
24.1.1	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
24.2.1 Site housing, health care facilities, schools, libraries, religious facilities, and other "noise sensitive" land uses in areas where existing or future noise levels are below an Ldn of 60 (dB(A) exterior and an Ldn of 45 dB(A) interior limits.	X			4.2, Per proj. EIR	
24.2.2 The Plan permits the locations of commercial, industrial, and recreational uses in areas where the ambient noise levels exceed an Ldn of 60 dB(A), if they area able to reduce the interior noise levels to within the 45 dB(A) Ldn level.	X			4.2	
24.2.3 The Plan requires that <ul style="list-style-type: none"> <li>Dining and other high noise-generating entertainment uses in commercial areas are not located directly adjacent to any residences, schools, health-care facilities, or other noise sensitive" land uses; and</li> <li>The entertainment and restaurant/bar uses' outdoor dining areas, waiting areas, entries, and kitchens are located in such a manner that the noise levels emanating from these uses are not discernible from ambient noise levels at a distance of fifty (50) feet from the establishment in which it is being conducted or within ten (10) feet of any residence (whichever is more restrictive).</li> </ul>	X			6.15	
24.2.4 The Plan requires that the facilities for active recreational uses that are likely to draw cheering crowds, elicit loud play, or have amplified game announcements (i.e., stadiums, soccer fields, tennis courts, basketball courts, etc) be located within the park's interior and away from surrounding residential and "noise sensitive" uses. <ul style="list-style-type: none"> <li>The Parkside Specific Plan does not include any active recreational uses that would draw cheering crowds.</li> </ul>					X
24.2.5 The Plan requires educational, recreational, commercial, and industrial land uses (including educational campuses, parks, stadiums, and public event facilities) be designed in such a manner that: <ul style="list-style-type: none"> <li>Locate and orient vehicle access points away from residential and/or noise sensitive parcels.</li> <li>Locate loading and shipping facilities away from adjacent noise sensitive uses;</li> <li>Incorporate structural building materials that mitigate sound transmission;</li> <li>Minimize the use of outside speakers and amplifiers;</li> <li>Configure interior spaces to minimize sound amplification and transmission; and</li> <li>Incorporate fences, walls, landscaping and other noise buffers and barriers between incompatible uses, as appropriate.</li> </ul>	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>24.2.6 The Plan requires mixed-use buildings:</p> <ul style="list-style-type: none"> <li>• Be constructed soundly to prevent adverse noise transmission between differing uses or tenants located in the same commercial structure and individual dwelling units in multi-family residential structures; and</li> <li>• Designed for concurrent commercial and residential and uses to minimize to the greatest degree practicable (through design and construction techniques) the transfer or transmission of noise and vibration from the commercial land use to the residential land use;</li> </ul>	X			Per proj. EIR	
<p>24.2.7 The Plan requires that, prior to the issuance of grading permits, an Acoustical Analysis Report be submitted to the City Engineer by the project developer. The report shall:</p> <ul style="list-style-type: none"> <li>• Describe the cumulative effect of road noise on surrounding land uses and recommend mitigation measures;</li> <li>• describe in detail the interior and exterior noise levels for residential uses on the site and the specific design and mitigation features to ensure compliance with the City's noise criteria of 65 dB(A) CNEL for outdoor living areas and 45dB(A) CNEL in habitable rooms;</li> <li>• specify the noise barriers' height, location, and types capable of achieving the desired mitigation affect; and</li> <li>• identify those residential lots that may require mechanical ventilation to achieve interior noise standards. When the operable doors and windows are open for homes facing these roadways, the interior 45 dB(A) CNEL interior noise limit for these units may be exceeded. Therefore, a "windows closed" condition may be required for these units. Any proposed mechanical ventilation must meet the requirements of the Uniform Building Code (UBC) standard. It should be noted that the windows facing some roadways may be operable windows, but the homeowners would have the option to close the windows and still obtain adequate ventilation through the use of a mechanical ventilation system. This mechanical ventilation system shall supply two air changes per hour to each habitable room, including 20 percent (one-fifth) fresh make-up air obtained directly form the outdoors. The fresh air inlet duct shall be of sound attenuating construction and shall consist of a minimum of ten feet of straight or curved duct or six feet plus one sharp 90 degree bend. The City Engineer shall ensure that the Acoustical Analysis Report identifies any requirements for mechanical ventilation for individual onsite residential units. (1-7 and 1-10)</li> </ul>	X			Per proj. EIR	
<p>24.2.8 The Plan requires that all prospective owners and occupants of residential units be formally notified prior to purchase, lease, or rental, that certain units (without windows and doors closed), and outdoor areas could be subject to noise levels above City standards for residential uses. Such notification shall be in language approved by the City Planning Department, and shall be formalized in written Covenants, Conditions, and Restrictions (CC&amp;Rs) recorded on the title of each residential lot in the project. In addition, each advertisement, solicitation, and sales brochure or other literature regarding the project shall contain the approved notification language. (1-7 and 1-10)</p>	X				

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
24.2.9 The Plan limits construction in the New Model Colony to the hours of 7:00 AM to 7:00 PM Monday through Saturday, and prohibited on Sundays and Federal holidays. (1-7 and 1-10)	X			Per proj. EIR	
24.2.10 The Plan requires all project construction vehicles or equipment, fixed or mobile, be equipped with properly operating and maintained muffler. (1-7 and 1-10)	X			Per proj. EIR	
24.2.11 The Plan requires that stockpiling and/or vehicle staging areas be located as far as practical from existing residential units on and off the project site. (1-7 and 1-10)	X			Per proj. EIR	
24.2.12 The Plan requires to, whenever feasible, schedule the noisiest construction operations to occur together to avoid continuing periods of the greatest annoyance. (1-7 and 1-10)	X			Per proj. EIR	
<b>25.1 Air Quality</b>					
25.1.1 Continue to enforce the following:	X				
<ul style="list-style-type: none"> <li>• During all construction activities, construction contractors shall use low emission mobile construction equipment where feasible to reduce the release of undesirable emissions.</li> <li>• During all construction activities, construction contractors shall encourage rideshare and transit programs for project construction personnel to reduce automobile emission.</li> <li>• During all grading and site disturbance activities, construction contractors shall water active sites at least twice a day, and clean construction equipment in the morning and/or evening to reduce particulate emissions and fugitive dust.</li> <li>• During all construction activities, construction contractors shall, as necessary, wash truck tires leaving the site to reduce the amount of particulate matter transferred to paved streets as required by SCAG/MD Rule 403.</li> <li>• During all construction activities, construction contractors shall sweep on and offsite streets if silt is carried to adjacent public thoroughfares, as determined by the City Engineer to reduce the amount of particulate matter on public streets.</li> <li>• During all construction activities, construction contractors shall limit traffic speeds on all unpaved road surfaces to 15 miles per hour or less to reduce fugitive dust.</li> <li>• During grading and all site disturbance activities, at the discretion of the City Planner, construction contractors shall suspend grading operations during first and second stage smog alerts to reduce fugitive dust.</li> <li>• During grading and all site disturbance activities, at the discretion of the City Planner, construction contractors shall suspend all grading operations when wind speeds (including instantaneous gusts) exceed 25 miles per hour to reduce fugitive dust.</li> <li>• During all construction activities, the construction contractors shall maintain construction equipment engines by keeping them tuned.</li> </ul>				Per proj. EIR	



Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<ul style="list-style-type: none"> <li>• During all construction activities, the construction contractors shall use low sulfur fuel for stationary construction equipment as required by AQMD Rules 431.1 and 431.2 to reduce the release of undesirable emissions.</li> <li>• During all construction activities, the construction contractors shall use existing onsite electrical power sources to the maximum extent practicable. Where such power is not available, the Contractor shall use clean fuel generators during the early stages of construction to minimize or eliminate the use of portable generators and reduce the release of undesirable emissions.</li> <li>• During all construction activities, the construction contractors shall use low emission, on-site stationary equipment (e.g., clean fuels) to the maximum extent practicable to reduce emissions, as determined by the City Engineer.</li> <li>• During all construction activities, the construction contractors, in conjunction with the City Engineer, shall locate construction parking to minimize traffic interference on local roads.</li> <li>• During all construction activities, the construction contractors shall ensure that all trucks hauling dirt, sand, NMG, or other loose materials are covered or should maintain at least two feet of freeboard (i.e., minimum vertical distance between top of the load and the top of the trailer) in accordance with the requirements of the California Vehicle Code Section 23114 to reduce spilling of material on area roads. (1-14)</li> </ul>					



City of Ontario  
Planning Department  
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## **Planning Department Land Development Division Conditions of Approval**

**Meeting Date:** May 26, 2020

**File No:** PSPA19-007

**Related Files:** N/A

**Project Description:** An Amendment to the Parkside Specific Plan (File No. PSPA19-007) to: [1] Reconfigure residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 21; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). The project site is bounded by Ontario Ranch Road to the north, Eucalyptus Avenue to south, Archibald Avenue to the east and Carpenter Avenue to the west. (APN's): 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, and 0218-221-10); **submitted by SC Ontario Development Company, LLC.**

**Prepared By:** Rudy Zeledon, Assistant Planning Director  
Phone: 909.395.2422 (direct)  
Email: rzeledon@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

**1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

**2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

**2.1 Specific Plan/Specific Plan Amendment.** The following shall be submitted to the Planning Department within 30 days following City Council approval of the Specific Plan/Specific Plan Amendment:

- (a) Five copies of the final Specific Plan document;
- (b) One CD or USB Flash Drive containing a complete Microsoft Word copy of the final Specific Plan document, including all required revisions;
- (c) Five CDs or USB Flash Drives, each containing a complete PDF copy of the final Specific Plan document, including all required revisions; and
- (d) One CD or USB Flash Drives containing a complete electronic website version of the final Specific Plan document, including all required revisions.

## **2.2** Environmental Review.

(a) The environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

**2.3** Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

## **2.4** Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

## **2.5** Additional Requirements - Tribal Consultation

At the request of The Gabrieleno Band of Missions Indians – Kizh Nation, all proposed future development entitlements within the Specific Plan boundary shall consult with The Gabrieleno Band of Missions Indians – Kizh Nation (Chairman, Andrew Sales @ <http://www.gabrielenoindians.org/>), prior to grading activities. In addition, the project shall be subject to following conditions:

(a) The project developer shall retain a Native American Monitor of Gabrieleño Ancestry to conduct a Native American Indian Sensitivity Training for construction personnel prior to commencement of any excavation activities. The training session shall include a handout and focus on how to identify Native American resources encountered during earthmoving activities and the procedures followed if resources are discovered, the duties of the Native American Monitor of Gabrieleño Ancestry and the general steps the Monitor would follow in conducting a salvage investigation.

(b) The project developer shall retain a Native American Monitor of Gabrieleño Ancestry to be on-site during all project-related, ground-disturbing construction activities (e.g., pavement removal, auguring, boring, grading, excavation, potholing, trenching, and grubbing) of previously

undisturbed native soils to a maximum depth of 30 feet below ground surface. At their discretion, a Native American Monitor of Gabrieleño Ancestry can be present during the removal of dairy manure to native soil, but not at the developers' expense.

**(c)** A qualified archaeologist and a Native American Monitor of Gabrieleño Ancestry shall evaluate all archaeological resources unearthed by project construction activities. If the resources are Native American in origin, the Tribe shall coordinate with the developer regarding treatment and curation of these resources. Typically, the Tribe will request reburial or preservation for educational purposes. If archeological features are discovered, the archeologist shall report such findings to the Ontario Planning Director. If the archeological resources are found to be significant, the archeologist shall determine the appropriate actions, in cooperation with the City that shall be taken for exploration and/or salvage in compliance with CEQA Guidelines Section 15064.5(f).

**(d)** Prior to the start of ground disturbing activities, the developer shall arrange a designated site location within the footprint of the project for the respectful reburial of Tribal human remains and/or ceremonial objects. All human skeletal material discoveries shall be reported immediately to the County Coroner. The Native American Monitor shall immediately divert work a minimum of 50 feet from the discovery site and place an exclusion zone around the burial. The Native American Monitor shall notify the construction manager who shall contact the San Bernardino County Coroner. All construction activity shall be diverted while the San Bernardino County Coroner determines if the remains are Native American. The discovery shall be confidential and secure to prevent further disturbance. If Native American, the San Bernardino County Coroner shall notify the Native American Heritage Commission (NAHC) as mandated by state law who will then appoint a Most Likely Descendent. In the case where discovered human remains cannot be documented and recovered on the same day, the remains shall be covered with muslin cloth and a steel plate that can be moved by heavy equipment placed over the excavation opening to protect the remains. If this type of steel plate is not available, a 24-hour guard shall be posted outside working hours. The Tribe shall make every effort to recommend diverting the project and keep the remains in situ and protected. If the project cannot be diverted, it may be determined that burials will be removed. If data recovery is approved by the Tribe, documentation shall be taken, which includes at a minimum detailed descriptive notes and sketches. Additional types of documentation shall be approved by the Tribe for data recovery purposes. Cremations will either be removed in bulk or means necessary to ensure complete recovery of all material. If the discovery of human remains includes four (4) or more burials, the location is considered a cemetery and a separate treatment plan shall be created. The project developer shall consult with the Tribe regarding avoidance of all cemetery sites. Once complete, a final report of all activities shall be submitted to the NAHC.

**(e)** No scientific study or the utilization of any invasive diagnostics shall be allowed to any Native American human remains.

**(f)** If the San Bernardino County Coroner determines the remains represent a historic non-Native American burial, the burial shall be treated in the same manner of respect with agreement of the San Bernardino County Coroner. Reburial will be in an appropriate setting. If the San Bernardino County Coroner determines the remains to be modern, the San Bernardino County Coroner shall take custody of the remains.

**(g)** Each occurrence of human remains and associated funerary objects shall be stored using opaque cloth bags. All human remains, funerary objects, sacred objects and objects of cultural patrimony shall be removed to a secure container on site if possible. These items shall be retained and reburied within six months of recovery. The site of reburial/repatriation shall be on the project site, but at a location agreed upon between the Tribe and the developer and protected in perpetuity. There shall be no publicity regarding any cultural materials recovered.



# PLANNING COMMISSION STAFF REPORT

May 26, 2020

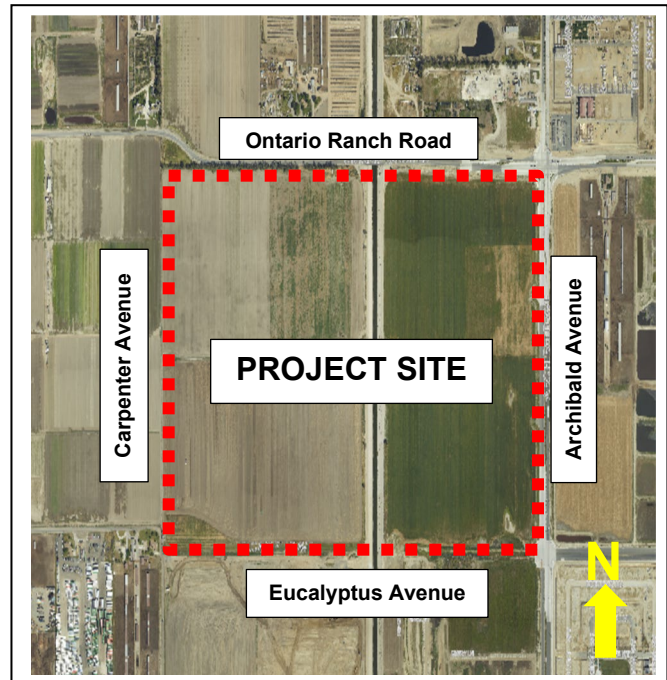
**FILE NO.:** PDA05-002

**SUBJECT:** A Fourth Amendment to the Development Agreement (File No. PDA05-002) between the City of Ontario and SC Ontario Development Company, LLC, to modify the commencement of certain specific infrastructure associated with the development of Tentative Tract Map 20316 (File No. PMTT19-020), and conform to the revised development standards established by the Parkside Specific Plan Amendment (File No. PSPA19-007), located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning Areas 1 through 26 of the Parkside Specific Plan (APNs: 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, and 0218-221-10). **Submitted by SC Ontario Development Company, LLC. City Council action is required.**

**PROPERTY OWNER:** SC Ontario Development Company, LLC, a Delaware limited liability company

**RECOMMENDED ACTION:** That the Planning Commission consider and recommend City Council adoption of an ordinance approving the Fourth Amendment to the Development Agreement (File No. PDA05-002) between the City of Ontario and SC Ontario Development Company, LLC, pursuant to the facts and reasons contained in the staff report and attached resolution.

**PROJECT SETTING:** The project site, depicted in Figure 1: Project Location, is comprised of approximately 250.89 acres of land located north of Eucalyptus Avenue, south of Ontario Ranch Road, east of Carpenter Avenue, and west of Archibald Avenue within Planning Areas



**Figure 1: Project Location**

Case Planner:	Derrick Womble, Administrative Officer
Planning Director Approval:	
Submittal Date:	November 11, 2019

Hearing Body	Date	Decision	Action
DAB	N/A	N/A	N/A
PC	05/26/2020		Recommend
CC	06/16/2020		Final

1 through 26 of the Parkside Specific Plan. The properties to the north, south, east, and west, are developed with agriculture and dairy uses and currently vacant. The property also encompasses approximately a half-mile portion of the Cucamonga Creek Channel from north to south.

**PROJECT ANALYSIS:**

[1] Background — On July 25, 2006, the Planning Commission approved Tentative Tract Map 18048 (File No. PMTT06-039), pending approval of the Parkside Specific Plan, File No. PSP03-002 (“Specific Plan”). Tract Map 18048 proposed to subdivide 104.13 acres of land into 25 lots, facilitate the backbone infrastructure improvements (major streets, sewer, water, storm drain facilities) and the creation of residential neighborhoods, the commercial center, community facilities (fire station), and parks for the eastern portion of the Specific Plan. On August 15, 2006, the City Council approved the Specific Plan and certified the Environmental Impact Report (EIR).

On September 19, 2006, the City Council approved a Development Agreement, File No. PDA05-002 (“Development Agreement”), between the City of Ontario and SC Ontario Development Company, LLC (“Applicant”) to provide the funding for additional City services required to support the Specific Plan residential development of up to 1,947 residential units and the infrastructure improvements required to support the related Tract Map No. 18048.

On July 7, 2009, the City Council approved an amendment to the Development Agreement (“First Amendment”) to allow Tract Map 18048 to be recorded for the purpose of financing.

On February 3, 2015, the City Council approved a second amendment to the Development Agreement (“Second Amendment”), to update the Development Agreement to conform with the current Construction Agreement with NMC Builders, LLC, and to provide for the phasing of the construction of public infrastructure for 250.89 acres of the Specific Plan.

On June 18, 2019, the City Council approved a third amendment to the Development Agreement (“Third Amendment”) to remove approximately 2.43 acres of land from the Development Agreement and change the legal description in conjunction with the lot line adjustment (File No. LLA18-010) and sale of a portion of the property to Ontario Land Ventures, LLC, for the design, construction, and completion of street improvements necessary for the ultimate alignment of Eucalyptus Avenue.

The main points of the original Development Agreement continue to address Development Impact Fees (DIF), public service funding, Community Facilities District (CFD) for maintenance of public facilities, park/open space requirements, affordable housing fees, school facilities requirements, and remain in full force and effect. State law and Section 2.5 of the original Development Agreement provide that amendments may

be made to the Agreement upon the mutual consent of both parties, using the same process and procedures as for the consideration and approval of the original Development Agreement.

[2] Staff Analysis – On October 16, 2019, the Applicant submitted Tentative Tract Map 20316 (PMTT19-020) for condominium purposes to subdivide 56.99 acres of land into 4 numbered lots and 12 lettered lots, within Planning Areas 1 through 4 of the Specific Plan. In addition, the Applicant has proposed the Parkside Specific Plan Amendment (File No. PSPA19-007) to: [a] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [b] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [c] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [d] Update and revise Residential Design Guidelines to introduce new housing types and architectural styles; [e] Update and revise Landscape Standards.

The new Tract Map and proposed revisions to the Specific Plan necessitates a fourth amendment to the Development Agreement (“Fourth Amendment”) to modify the commencement of certain specific infrastructure associated with the development of Tentative Tract Map 20316 (File No. PMTT19-020), and conform to the updated development standards established by the Parkside Specific Plan Amendment (File No. PSPA19-007).

Key points addressed in the Fourth Amendment are the following:

- Payment of the Public Service Funding Fee (PSFF) for residential and non-residential uses; and
- Participation in a cost-sharing agreement with other developers for the construction of the Eucalyptus Avenue Bridge Improvements, or deposit funds with the City for the proportional share of the cost to construct the bridge; and
- Complete construction of the traffic signal at Eucalyptus Avenue and Parkhouse Privado (required with Phase 1), prior to the request for the first occupancy permit for Phase 1 Production Units, or prior to completion of the Eucalyptus Bridge Improvements, whichever occurs first; and
- Complete construction of the Cucamonga Creek Channel Trail Improvements and Neighborhood Edge Improvements in phases, in conjunction with the development of the respective Phase (Phases 1 through 4); and
- Continuing requirement to design, construct, and complete significant portions of the Great Park and allow for the City to acquire such portions, subject to the terms of a DIF Credit and Reimbursement Agreement; and

- Extension of the term of the Development Agreement another five (5) years until September 19, 2026.

Staff finds that the Fourth Amendment to the Development Agreement is consistent with State law, The Ontario Plan, and the City's Development Agreement policies and staff is recommending approval of the Fourth Amendment. If the Planning Commission finds the Fourth Amendment acceptable, a recommendation of approval to the City Council would be appropriate.

**COMPLIANCE WITH THE ONTARIO PLAN:** The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)
  - Encourage, Provide or Support Enhanced Recreational, Educational, Cultural and Healthy City Programs, Policies and Activities
  - Ensure the Development of a Well Planned, Balanced, and Self-Sustaining Community in the New Model Colony

[2] Vision.

**Distinctive Development:**

- Commercial and Residential Development
  - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

[3] Governance.

**Decision Making:**

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
  - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision



[4] Policy Plan (General Plan)

**Land Use Element:**

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
  - LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.
  - LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).
- Goal LU2: Compatibility between a wide range of uses.
  - LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

**Housing Element:**

- Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.
  - H2-4 New Model Colony. We support a premier lifestyle community in the New Model Colony distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.
  - H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.
- Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.
  - H5-2 Family Housing. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation and other amenities.

### **Community Economics Element:**

- Goal CE1: A complete community that provides for all incomes and stages of life.

- CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

- CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

- CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

- CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

- CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

### **Safety Element:**

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.

- S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

### **Community Design Element:**

- Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➤ CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

➤ CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

➤ CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

▪ Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

➤ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➤ CD2-2 Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb.

➤ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural

daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➤ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

➤ CD2-12 Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

▪ Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

➤ CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

➤ CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.

- CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.
- CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.
- CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.
  - Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.
- CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.
- CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

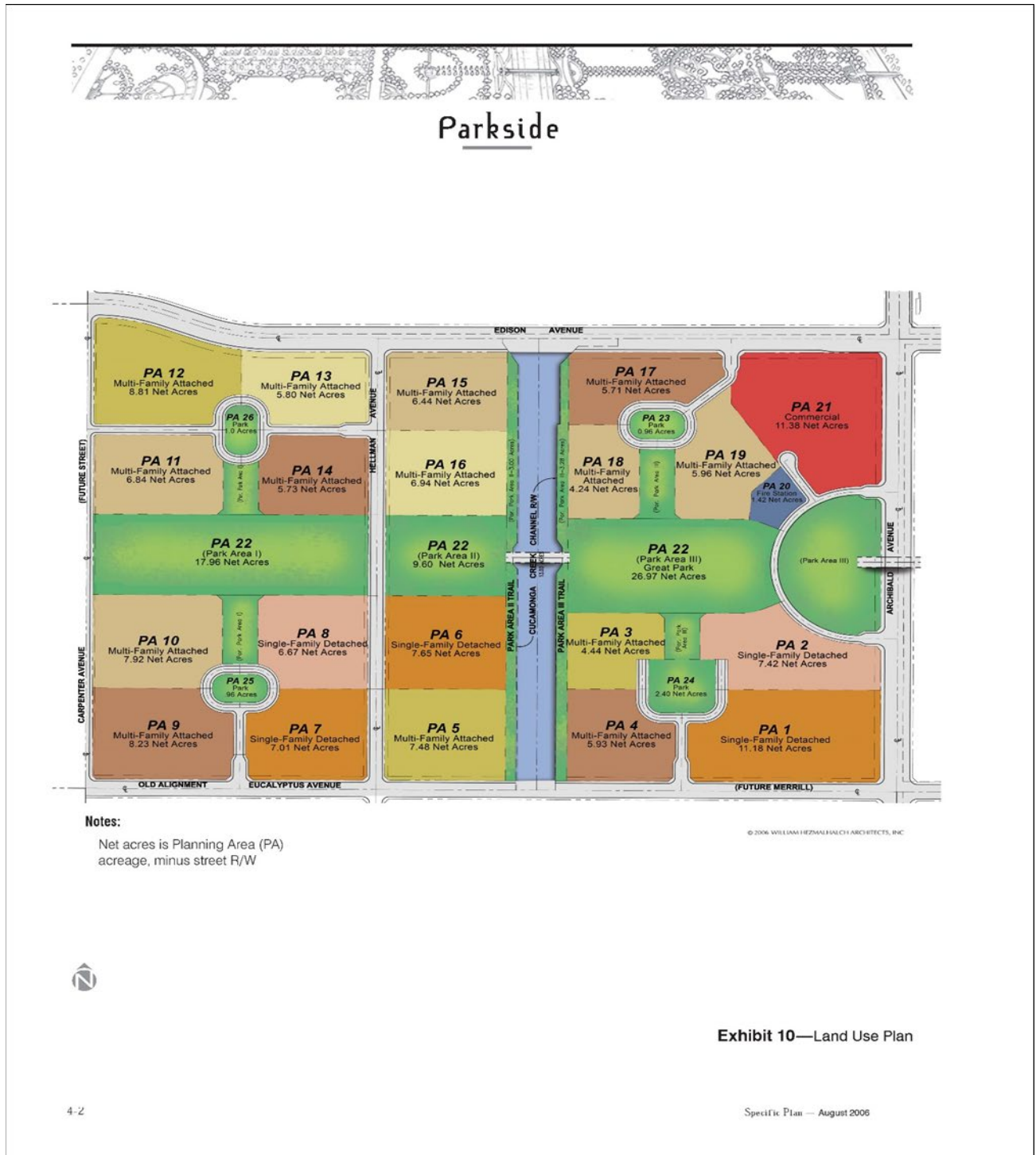
**HOUSING ELEMENT COMPLIANCE:** The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project is consistent with the number of dwelling units (1,977) and density (15.73 DU/AC) specified in the Available Land Inventory.

**AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE:** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

**ENVIRONMENTAL REVIEW:** The environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental



## Exhibit "B" Existing Approved Parkside Specific Plan Land Use Map



**Exhibit "C"**  
**Proposed Parkside Specific Plan Land Use Map**

Parkside



Exhibit 1-3: Land Use Plan

Legend	
<span style="display:inline-block; width:15px; height:15px; background-color:orange; border:1px solid black;"></span>	Medium Density Residential (11.1 - 25 du/ac)
<span style="display:inline-block; width:15px; height:15px; background-color:darkred; border:1px solid black;"></span>	Neighborhood Commercial (0.4 FAR)
<span style="display:inline-block; width:15px; height:15px; background-color:lightblue; border:1px solid black;"></span>	Public Facility
<span style="display:inline-block; width:15px; height:15px; background-color:green; border:1px solid black;"></span>	Open Space - Parkland
<span style="display:inline-block; width:15px; height:15px; background-color:lightgreen; border:1px solid black;"></span>	Open Space - Private/Village Edges
<span style="display:inline-block; width:15px; height:15px; background-color:grey; border:1px solid black;"></span>	Roads - Master Planned
<span style="display:inline-block; width:15px; height:15px; background-color:lightgrey; border:1px solid black;"></span>	Roads - Local
<span style="display:inline-block; width:15px; height:15px; background-color:lightblue; border:1px solid black;"></span>	Cucamonga Creek Channel



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL APPROVE THE FOURTH AMENDMENT TO THE DEVELOPMENT AGREEMENT (FILE NO. PDA05-002), BETWEEN THE CITY OF ONTARIO AND SC ONTARIO DEVELOPMENT COMPANY, LLC, TO MODIFY THE COMMENCEMENT OF CERTAIN SPECIFIC INFRASTRUCTURE ASSOCIATED WITH THE DEVELOPMENT OF TENTATIVE TRACT MAP 20316 (FILE NO. PMTT19-020), AND CONFORM TO THE REVISED DEVELOPMENT STANDARDS ESTABLISHED BY THE PARKSIDE SPECIFIC PLAN AMENDMENT (PSPA19-007), LOCATED AT THE NORTHWEST CORNER OF EUCALYPTUS AVENUE AND ARCHIBALD AVENUE, WITHIN PLANNING AREAS 1 THROUGH 26 OF THE PARKSIDE SPECIFIC PLAN, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, AND 0218-221-10.

WHEREAS, SC Ontario Development Company, LLC ("Applicant") has filed an Application for the approval of a Fourth Amendment to the Development Agreement, File No. PDA05-002, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 250.89 acres of land generally located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning areas 1 through 26 of the Parkside Specific Plan; and

WHEREAS, the properties to the north, south, east, and west, are developed with agriculture and dairy uses and currently vacant. The property also encompasses approximately a half-mile portion of the Cucamonga Creek Channel from north to south; and

WHEREAS, on September 19, 2006, the City Council of the City of Ontario ("City"), adopted Ordinance No. 2841, approving the original Development Agreement between the City and Applicant, to provide the funding for additional City services required to support the Parkside Specific Plan (File No. PSP03-002) residential development of up to 1,947 residential units and the infrastructure improvements required to support the related Tract Map 18048 (File No. PMTT06-039); and

WHEREAS, on July 7, 2009, the City adopted Ordinance No. 2909, approving a First Amendment to the Development Agreement, to allow Tract Map 18048 to be recorded for the purpose of financing; and

WHEREAS, on February 3, 2015, the City adopted Ordinance No. 3012, approving a Second Amendment to the Development Agreement, to update the Development Agreement to conform with the Construction Agreement with NMC Builders, LLC, and to provide for the phasing of the construction of public infrastructure for 250.89 acres of the Parkside Specific Plan; and

WHEREAS, on June 18, 2019, the City adopted Ordinance No. 3134, approving the Third Amendment to the Development Agreement, to change the legal description in conjunction with the lot line adjustment (File No. LLA18-010) and sale of a portion of the property to Ontario Land Ventures, LLC, for the design, construction, and completion of street improvements necessary for the ultimate alignment of Eucalyptus Avenue; and

WHEREAS, on October 16, 2019, the Applicant submitted a new Tentative Tract Map 20316 (File No. PMTT19-020) for condominium purposes to subdivide 56.99 acres of land into 4 numbered lots and 12 lettered lots, within Planning Areas 1 through 4 of the Parkside Specific Plan. In addition, the Applicant has proposed the Parkside Specific Plan Amendment (File No. PSPA19-007) to: [a] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [b] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [c] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [d] Update and revise Residential Design Guidelines to introduce new housing types and architectural styles; [e] Update and revise Landscape Standards; and

WHEREAS, the new Tract Map and proposed Specific Plan Amendment necessitates a fourth amendment to the Development Agreement ("Fourth Amendment") to modify the commencement of certain specific infrastructure associated with the development of Tentative Tract Map 20316 (File No. PMTT19-020), and conform to the updated development standards established by the Parkside Specific Plan Amendment (PSPA19-007); and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an initial study has been prepared to determine possible environmental impacts; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. This application is consistent with the previously adopted EIR and introduces no new significant environmental impacts. All previously adopted mitigation measures shall be a condition of project approval and are incorporated herein by reference; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make recommendations to the City Council on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 26, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

**SECTION 1: Environmental Determination and Findings.** As the recommending authority for the Project, the Planning Commission has reviewed and considered the information contained in the previous Certified EIR and supporting

documentation. Based upon the facts and information contained in the previous Certified EIR and supporting documentation, the Planning Commission finds as follows:

(1) The environmental impacts of this project were previously reviewed in conjunction with File No. PSP03-002, an addendum to the Parkside Specific Plan for which a Certified EIR was adopted by the City Council on September 5, 2006; and

(2) The previous Certified EIR contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous Certified EIR was completed in compliance with CEQA and the Guidelines promulgated thereunder; and

(4) The previous Certified EIR reflects the independent judgment of the Planning Commission; and

(5) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the previous Certified EIR, and all mitigation measures previously adopted with the Certified EIR, are incorporated herein by this reference.

**SECTION 2: Subsequent or Supplemental Environmental Review Not Required.** Based on the information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

**SECTION 3: *Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the recommending authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project is consistent with the number of dwelling units (1,977) and density (15.73 DU/AC) specified in the Available Land Inventory.

**SECTION 4: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

**SECTION 5: *Concluding Facts and Reasons.*** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 4, above, the Planning Commission hereby concludes as follows:

(a) The Fourth Amendment applies to approximately 250.89 acres of land generally located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning Areas 1 through 26 of the Parkside Specific Plan; and

(b) The properties to the north, south, east, and west, are developed with agriculture and dairy uses and currently vacant. The property also encompasses approximately one-half mile portion of the Cucamonga Creek Channel from north to south; and

(c) The main points of the original Development Agreement continue to address Development Impact Fees (DIF), public service funding, Community Facilities District (CFD) for maintenance of public facilities, park/open space requirements, affordable housing fees, school facilities requirements, and remain in full force and effect. State law and Section 2.5 of the original Development Agreement provide that amendments may be made to the Agreement upon the mutual consent of both parties, using the same process and procedures as for the consideration and approval of the original Development Agreement; and

(d) The new Tract Map and proposed Specific Plan Amendment necessitates a fourth amendment to the Development Agreement (“Fourth Amendment”) to modify the commencement of certain specific infrastructure associated with the development of Tentative Tract Map 20316 (File No. PMTT19-020), and conform to the updated development standards established by the Parkside Specific Plan Amendment (File No. PSPA19-007).

(e) Key points addressed in the Fourth Amendment are the following:

- i. Payment of the Public Service Funding Fee (PSFF) for residential and non-residential uses; and
- ii. Participation in a cost-sharing agreement with other developers for the construction of the Eucalyptus Avenue Bridge Improvements, or deposit funds with the City for the proportional share of the cost to construct the bridge; and
- iii. Complete the construction of the traffic signal at Eucalyptus Avenue and Parkhouse Privado (required with Phase 1), prior to the request for the first occupancy permit for Phase 1 Production

Units, or prior to completion of the Eucalyptus Bridge Improvements, whichever occurs first; and

- iv. Construct the Cucamonga Creek Channel Trail Improvements and Neighborhood Edge Improvements in phases, in conjunction with the development of the respective Phase (Phases 1 through 4); and
- v. Design, construct, and complete significant portions of the Great Park and allow for the City to acquire such portions, subject to the terms of a DIF Credit and Reimbursement Agreement; and
- vi. Extension of the term of the Development Agreement another five (5) years until September 19, 2026.

(f) This Amendment will not be materially injurious or detrimental to the adjacent properties and will not have a significant impact on the environment or the surrounding properties. The environmental impacts of this project were previously reviewed in conjunction with addendum to the Parkside Specific Plan EIR (SCH#20040111008) that was adopted by the City Council on September 5, 2006. This application introduces no new significant environmental impacts; and

(g) All adopted mitigation measures of the related EIR shall be a condition of project approval and are incorporated herein by reference.

**SECTION 6: Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL approve the Fourth Amendment to the Development Agreement (File No. PDA05-002), the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

**SECTION 7: Indemnification.** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

**SECTION 8: Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

**SECTION 9: Certification to Adoption.** The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26<sup>th</sup> day of May 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

---

Jim Willoughby  
Planning Commission Chairman

ATTEST:

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Cathy Wahlstrom  
Planning Director and  
Secretary to the Planning Commission



STATE OF CALIFORNIA                    )  
COUNTY OF SAN BERNARDINO        )  
CITY OF ONTARIO                        )

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. \_\_\_\_\_ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 26, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

---

Gwen Berendsen  
Secretary Pro Tempore

**ATTACHMENT A:**

**File No. PDA05-002**

**Fourth Amendment to the Development Agreement**

**By and Between**

**City of Ontario  
a California municipal corporation**

**and**

**SC Ontario Development Company, LLC  
a Delaware limited liability company**

*(Document to follow this page)*

**RECORDING REQUESTED BY AND  
WHEN RECORDED RETURN TO:**

CITY OF ONTARIO  
CITY CLERK / RECORDS MANAGEMENT  
303 EAST "B" STREET  
ONTARIO, CA 91764-4196

---

Space above this line for Recorder's Use

Exempt from Fees Per Gov. Code § 6103

**File No. PDA05-002**

**FOURTH AMENDMENT TO THE DEVELOPMENT AGREEMENT**

**By and Between**

**City of Ontario  
a California municipal corporation**

**and**

**SC Ontario Development Company, LLC  
a Delaware limited liability company**

\_\_\_\_\_, 2020

**San Bernardino County, California**

**FOURTH AMENDMENT TO THE DEVELOPMENT AGREEMENT  
BY AND BETWEEN THE CITY OF ONTARIO AND  
SC ONTARIO DEVELOPMENT COMPANY, LLC  
(File No. PDA05-002)**

This Fourth Amendment (hereinafter “**Fourth Amendment**”) is entered into as of the \_\_\_\_\_ day of \_\_\_\_\_ 2020 by and among the CITY OF ONTARIO, a California municipal corporation (hereinafter “**CITY**”), and SC ONTARIO DEVELOPMENT COMPANY LLC, a Delaware limited liability company (hereinafter “**OWNER**”).

**RECITALS**

WHEREAS, the CITY and OWNER’s predecessor have previously entered into a (i) Development Agreement dated September 19, 2006 and recorded in San Bernardino County, California on November 14, 2006 as Instrument No. 2006-0774531 pursuant to Section 65864, *et seq.*, of the Government Code, (hereinafter the “**Original Development Agreement**”), as amended by (ii) that First Amendment to Development Agreement dated June 16, 2009, and recorded on September 14, 2009 as Instrument No. 2009-0403692 (the “**First Amendment**”), (iii) that Second Amendment to the Development Agreement dated April 4, 2017, and recorded on May 15, 2017 as Instrument No. 2017-0199310 (the “**Second Amendment**”), and (iv) that Third Amendment to Development Agreement dated June 18, 2019, and recorded on June 27, 2019 as Instrument No. 2019-0211740 (the “**Third Amendment**”) all with respect to the real property described in Exhibit “A-R” to this Agreement.

WHEREAS the CITY and OWNER now intend to amend the Original Development Agreement, as amended by the First, Second, and Third Amendments respectively (collectively the “**Existing Development Agreement**”), as set forth herein.

WHEREAS, Section 2.5 of the Existing Development Agreement specifies that the Development Agreement may be amended in whole or in part only in the manner provided for in Government Code Section 65868.1 and the procedure for adopting and entering into an amendment to the Existing Development Agreement shall be the same as the procedure for adopting and entering into the Original Development Agreement; and

WHEREAS, the CITY and OWNER agree to modify the commencement of certain specific infrastructure improvements to reflect the new Tentative Tract Map 20316 (File No. PMTT19-020), and proposed development standards in Ontario Ranch as it relates to the Parkside Specific Plan Amendment (PSPA19-007); and

WHEREAS, the CITY and OWNER agree that execution of this Fourth Amendment shall constitute Certification of Agreement Compliance under Section 6.4 of the Existing Development Agreement; and

WHEREAS, the term of the Original Development Agreement was for a ten (10) year term, expiring on September 19, 2016. The term of the Original Development Agreement was extended an additional five (5) years by the execution of the Second Amendment, expiring September 19, 2021. CITY and OWNER now agree to extend the term of the Existing Development Agreement for an additional five (5) year period, expiring on September 19, 2026.

## AGREEMENTS

NOW, THEREFORE, in consideration of the above recitals and of the mutual agreements hereinafter contained, the parties agree as follows:

### 1. DEFINITIONS AND EXHIBITS.

1.1. Existing Definitions. The following terms when used in this Fourth Amendment shall be defined as in the Existing Development Agreement: CITY; Construction Agreement; Construction Agreement Amendment; Deferred Infrastructure; Development; Development Approvals; Development Exaction; Development Impact Fee; Development Plan; General Plan; Land Use Regulations; Model Units; OWNER; Project; Property; Production Units; Specific Plan; Storm Water Capacity Availability Equivalents; Subsequent Development Approvals; Subsequent Land Use Regulations; and Water Availability Equivalent (WAE). Any capitalized terms not expressly defined in Section 1.2 below or otherwise expressly defined or modified in this Fourth Amendment shall have the meaning given to those terms in the Existing Development Agreement.

1.2. Additional and Modified Definitions. The following terms used in this Fourth Amendment shall be defined as follows:

**“Effective Date”** means the date that the ordinance approving this Fourth Amendment becomes effective.

**“Development Agreement”** is revised to mean, collectively, the Original Development Agreement, First Amendment, Second Amendment, Third Amendment and this Fourth Amendment.

**“Existing Development Approvals”** is revised to mean all Development Approvals approved or issued prior to the Effective Date. Existing Development Approvals includes the Development Approvals incorporated herein as Exhibit “C-R” and all other Development Approvals that are a matter of public record on the Effective Date.

**“Existing Land Use Regulations”** is revised to mean all Land Use Regulations in effect on the Effective Date and all other Land Use Regulations that are in effect and a matter of public record on the Effective Date. Existing Land Use Regulations includes the Regulations incorporated herein as Exhibit “D-R”.

**“Improvement”, “Improvements”, or “Infrastructure”** is revised to mean those public improvements and public infrastructure required to support the development of the Project as described in the Tract Map conditions for the “B” Tract Map No. 20316 and any and all future “A” and “B” Tract Maps for the Property. Improvement, Improvements, or Infrastructure shall also mean those public improvements required to support the Phase 1 Units, Phase 2 Units, Phase 3 Units and Phase 4 Units as further described in the respective Exhibit “F” for each respective Phase (the **“Infrastructure Improvements Exhibits”**).

**“Phase”** means the four (4) development phases of the Project as shown on Exhibit “F”.

**“Phase 1 Improvements”** means the Improvements (as defined in Section 1.2, below) that shall be designed, constructed and completed by OWNER prior to, and as a condition precedent to, CITY’s issuance of the first building permit for any Production Unit and as shown in “Exhibit F - Phase 1.”

**“Phase 1 Units”** means the first five hundred forty (540) units for which the CITY issues building permits to OWNER and shall include up to thirty-six (36) Model Units.

**“Phase 2 Improvements”** means the Improvements that shall be designed, constructed and completed by OWNER prior to, and as a condition precedent to, CITY’s issuance of the first building permit for any Production Unit in the Phase 2 area and as shown in “Exhibit F – Phase 2”.

**“Phase 2 Units”** means the next two hundred ten (210) units for which the CITY issues building permits to OWNER after the issuance of building permits for the Phase 1 Units.

**“Phase 3 Improvements”** means the Improvements that shall be designed, constructed and completed by OWNER prior to, and as a condition precedent to, CITY’s issuance of the first building permit for any Production Unit in the Phase 3 area and as shown in “Exhibit F - Phase 3”.

**“Phase 3 Units”** means the next six hundred thirteen (613) units for which the CITY issues building permits to OWNER after the issuance of building permits for the Phase 2 Units.

**“Phase 4 Improvements”** means the Improvements that shall be designed, constructed and completed by OWNER prior to, and as a condition precedent to, CITY’s issuance of the first building permit for any Production Unit in the Phase 4 area and as shown in “Exhibit F- Phase 4”.

**“Phase 4 Units”** means the next six hundred fourteen (614) units for which the CITY issues building permits to OWNER after the issuance of the building permits for the Phase 3 Units.

1.3. Exhibits. The following documents are attached to, and by this reference made a part of, this Fourth Amendment and revise and replace Exhibits A, B, C-R, D-R, E-R, F, F-Phase 1, F-Phase 2, F-Phase 3 and F-Phase 4 in the Existing Development Agreement:

Exhibit “A-R” – Revised Legal Description of Property

Exhibit “B-R” – Revised Parkside Specific Plan

Exhibit “C-R” – Revised Existing Development Approvals.

Exhibit “D-R” – Revised Existing Land Use Regulations.

Exhibit “E-R” — Revised Conceptual Phasing Plan

Exhibit “F” — Infrastructure Improvements for all Phases

Exhibit “F-Phase 1” - Phase 1 Improvements

Exhibit “F-Phase 2” - Phase 2 Improvements

Exhibit “F-Phase 3” - Phase 3 Improvements

Exhibit “F-Phase 4” – Phase 4 Improvements

2. MODIFICATIONS TO DEVELOPMENT AGREEMENT TO CONFORM TO CONSTRUCTION AGREEMENT AMENDMENT.

A. The provisions of Section 2.3 of the Second Amendment shall be removed and replaced with the following:

2.3 Modifications of the Amounts of the CFD to Finance City Services. Prior to, and as a condition precedent to, the recordation of each final subdivision creating buildable lots, such map shall be included in a Community Facilities District (“**CFD**”) to finance CITY services through annual special taxes. The amounts contained in Section 5.1 in the Development Agreement shall be modified as follows:

- Single Family Detached Dwelling Unit from \$1,387.00 to \$1,687.00
- Multiple Family Dwelling Unit from \$1,202.00 to \$1,462.00

- Gated Apartment Community Dwelling Unit from \$1,008.00 to \$1,226.00.
- The amount for Non-Residential building shall be modified to \$0.32 per square foot.

These modified amounts shall be subject to an automatic increase, not to exceed four (4%) percent per year, beginning on January 1, 2021.

B. Section 2.4 of the Second Amendment shall be removed and replaced with the following:

2.4 Retention of Public Services Funding Fee Amounts and Payment Terms. CITY and OWNER acknowledge and agree that the modifications to the amount and payment terms included in Section 3.7.4 of the Construction Agreement Amendment shall not apply to OWNER's Public Services Funding Fee Amounts and payment terms as OWNER entered into the Original Development Agreement with the City prior to the effective date of the Construction Agreement Amendment. Additionally, CITY agrees that OWNER is in compliance with the requirements for payment of the first installment of the Public Services Funding Fees as specified in Section 4.6 in the Original Development Agreement. Furthermore, OWNER acknowledges and agrees to pay the CITY the Second and Third Installments as specified in Section 4.6 in the Original Development Agreement.

If OWNER receives approval for any non-residential uses for the Project, OWNER shall pay the Public Services Funding Fee in a Single Installment for non-residential uses, and such Single Installment shall be due and payable on a building-by-building basis prior to the issuance of a building permit for each non-residential building. The amount of the Single-Installment for non-residential uses shall be the fee in effect at the time of payment and such fee shall automatically increase (but no decrease) in the Consumer Price Index (Los Angeles-Anaheim-Riverside County), 1950-2001 (1982-84=100) over the preceding year on January 1<sup>st</sup> of each year, beginning on January 1, 2021. OWNER may exercise the option to pay any single installment amounts for the remainder of the non-residential square footage within the Project on or before December 31<sup>st</sup>, before the Single Installment amount is automatically increased.

3. MODIFICATIONS TO DEVELOPMENT AGREEMENT TO AMEND CERTAIN REQUIREMENTS FOR CONSTRUCTION OF SPECIFIC PUBLIC IMPROVEMENTS.

A. The following provisions shall be added to Section 3.7 of the Development Agreement:

3.7.3 Eucalyptus Avenue Bridge Improvements. To the extent the Eucalyptus Avenue Bridge Improvements have not been commenced by other



developers, OWNER shall be responsible to commence construction of the Eucalyptus Avenue Bridge Improvements prior to the first building for Phase 1 Production Units, and complete construction prior to requesting the last building permit for Phase 1 Production Units. If OWNER has not commenced construction of the Eucalyptus Avenue Bridge Improvements prior to requesting the first building permit for Phase 1 Production Units, OWNER shall provide proof to the satisfaction of the CITY that OWNER has exercised one (1) of the following two (2) options:

3.7.3.1 Enter into a cost sharing agreement with other developer(s) for the construction and completion of the Eucalyptus Avenue Bridge Improvements and OWNER has fully funded OWNER'S obligations under the cost sharing agreement; or

3.7.3.2 OWNER shall deposit their proportional share at thirty-three percent (33%) of the cost to construct and complete the Eucalyptus Avenue Bridge Improvements into an Escrow Account with the City.

3.7.4 Traffic Signal at Eucalyptus Avenue and Parkhouse Privado. OWNER shall design, construct and complete the Traffic Signal at Eucalyptus Avenue and Parkhouse Privado, prior to and as a condition precedent to OWNER'S request for the first occupancy permit for Phase 1 Production Units or prior to completion of the Eucalyptus Bridge Improvements, whichever occurs first.

3.7.5 Cucamonga Creek Channel Trail Improvements. CITY and OWNER mutually agree that the development of the Cucamonga Creek Channel Trail Improvements (the "**Trail Improvements**") shall be constructed and completed in phases in conjunction with the development of the respective Phase (Phases 1 through 4) adjacent to the Trail Improvements.

3.7.5.1 Phase 1 Trail Improvements. OWNER shall design and commence construction of the Trail Improvements adjacent to Phase 1 prior to and as a condition precedent to, the request of the 305<sup>th</sup> building permit for Phase 1 Production Units or, upon completion of the Eucalyptus Avenue Bridge Improvements, whichever occurs first. OWNER shall complete construction of the Trail Improvements adjacent to Phase 1, prior to the request of the last building permit for Phase 1 Production Units.

3.7.5.2 Phase 2 Trail Improvements. OWNER shall design and commence construction of the Trail Improvements adjacent to Phase 2 prior to and as a condition precedent to, the request of the first building permit for Phase 2 Production Units. OWNER shall complete construction of the Trail Improvements adjacent to Phase 2, prior to the request of the last building permit for Phase 2 Production Units.

3.7.5.3 Phase 3 Trail Improvements. OWNER shall design and commence construction of the Trail Improvements adjacent to Phase 3 prior to and as a condition precedent to, the request of the first building permit for Phase 3 Production Units. OWNER shall complete construction of the Trail Improvements adjacent to Phase 3, prior to the request of the last building permit for Phase 3 Production Units.

3.7.5.4 Phase 4 Trail Improvements. OWNER shall design and commence construction of the Trail Improvements adjacent to Phase 4 prior to and as a condition precedent to, the request of the first building permit for Phase 4 Production Units. OWNER shall complete construction of the Trail Improvements adjacent to Phase 4, prior to the request of the last building permit for Phase 4 Production Units.

- 3.7.6 OWNER shall provide periodic written progress reports to the City commencing thirty (30) days after the OWNER initiates construction of the Trail Improvements, and each sixty (60) days thereafter regarding the progress of construction of the Trail Improvements.
- 3.7.7 If OWNER is unable to complete the construction of the Trail Improvements adjacent to the respective Phase prior to requesting the last building permit for Production Units within the respective Phase of the Project, due to either the design, construction and completion of the Great Park or, the construction and completion of the Eucalyptus Avenue and Ontario Ranch Road (Edison) Bridges, the City Manager or designee shall have the administrative authority to establish alternative and/or additional requirements of the OWNER for the release of any remaining building permits for Production Units for the respective Phase within the Project, at his/her reasonable discretion.
- 3.7.8 Neighborhood Edge Improvements along Archibald Avenue, Eucalyptus Avenue, Ontario Ranch Road, and Hellman Avenue. OWNER shall design, construct, and complete all neighborhood edge improvements in conjunction with the development of the respective Phase (Phases 1 through 4), prior to requesting the first building permit for Production Units for the respective Phase. If OWNER has not completed the neighborhood edge improvements with the development of the respective Phase prior to the request for the first building permit for Production Units within that Phase, the City Manager or designee shall have the authority to reasonably consider and approve the release of Production Permits respective to each Phase before the completion of all of the neighborhood edge improvements within such Phase at his/her discretion.

4. CONSTRUCTION AND ACQUISITION OF GREAT PARK.

A. Section 4.2.4 of the Development Agreement shall be removed and replaced with the following:

4.2.4 OWNER agrees that the Great Park areas as identified in the Parkside Specific Plan are to be constructed and transferred to the CITY by OWNER in four (4) Phases and shall be developed in accordance with the City's park standards and open to the public. CITY and OWNER shall meet periodically and/or as needed from the Effective Date of this Amendment, to jointly develop a plan for the timing and phasing of the development, funding, completion and acquisition of each Phase of design and construction of the Great Park area. Subject to the successful development of the aforementioned plan and OWNER's and CITY's approval and execution of a separate DIF Credit and Reimbursement Agreement, the design and construction of the Great Park areas shall entitle OWNER to a credit towards its obligations under the Quimby Act (Gov. Code § 64477) and the CITY's implementing ordinance and/or resolution (collectively "**Quimby Act Obligations**"), and to the extent OWNER's Quimby Act Obligations are satisfied, OWNER shall be entitled to have the CITY acquire such developed and public available parks as Non-Program Interests as set forth in Section 3.6.2 of the Construction Agreement Amendment. CITY acknowledges that the development of the Great Park areas by OWNER exceeds OWNER's requirements for the development of parkland and open space and that OWNER's requirements for the development of parkland and open space and that OWNER may be entitled to further consideration for the development and dedication of the Great Park area in form of reimbursements to OWNER from Quimby Act Fees collected by CITY and paid by other development within the Ontario Ranch area of the New Model Colony. The specific terms of the issuance of DIF Credit and DIF Reimbursement to OWNER will be included in the separate DIF Credit and Reimbursement Agreement between CITY and OWNER as provided for in Section 4.2.6.

5. OTHER MODIFICATIONS. Section 4.2. of the Second Amendment shall be removed and replaced with the following:

4.2 Extension of Term of the Development Agreement. CITY and OWNER acknowledge that the Term of the Development Agreement will expire on September 19, 2021. CITY and OWNER also agree that the OWNER will not meet all requirements for the extension of the term of the Development Agreement as it is not anticipated that OWNER will obtain fifty percent (50%) of the building permits for Project prior to the end of the ten (10) year term of the Development Agreement. Notwithstanding this requirements, CITY and OWNER agrees that OWNER has met the remaining requirements for the extension of the term of the Development Agreement

and that OWNER is not in default of the Development Agreement. CITY and OWNER agree that the term of the Development Agreement shall be extended for an additional five (5) year period to September 19, 2026 for a total term of twenty (20) years following the Effective Date of the Development Agreement.

6. INTEGRATION.

6.1 Integration of Previous Understands and Clarifications. This Fourth Amendment reflects the complete understanding of the parties with respect to the subject matter hereof. To the extent this Fourth Amendment conflicts with the Original Development Agreement, First Amendment, Second Amendment, and Third Amendment, this Fourth Amendment supersedes such previous document(s). In all other aspects, the parties hereto re-affirm and ratify all other terms, conditions, provisions, and obligations under the Original Development Agreement, First Amendment, Second Amendment, and Third Amendment. The Property covered by this Fourth Amendment is as described in the legal description of the Property attached hereto as revised Exhibit "A-R". This Fourth Amendment shall be recorded against the Property.

[Signature Immediately on Following Page]

**SIGNATURE PAGE  
TO FOURTH AMENDMENT TO DEVELOPMENT AGREEMENT BY AND BETWEEN  
THE CITY OF ONTARIO AND SC ONTARIO DEVELOPMENT COMPANY, LLC.  
(File No. PDA05-002)**

**IN WITNESS WHEREOF**, the parties hereto have executed this Fourth Amendment as of the Effective Date.

**"OWNER"**

**SC ONTARIO DEVELOPMENT COMPANY, LLC,**  
a Delaware limited liability company

By: **LEWIS MANAGEMENT CORP.,**  
a Delaware corporation - Its Sole Manager

By: \_\_\_\_\_  
Name: \_\_\_\_\_  
Its: Authorized Agent

**"CITY"**

**CITY OF ONTARIO**  
a California municipal corporation

By: \_\_\_\_\_  
Scott Ochoa, City Manager

Date: \_\_\_\_\_

**ATTEST:**

\_\_\_\_\_  
City Clerk, Ontario

**APPROVED AS TO FORM:**  
COLE HUBER, LLP

\_\_\_\_\_  
City Attorney

**ACKNOWLEDGEMENT**

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

STATE OF CALIFORNIA )  
COUNTY OF \_\_\_\_\_ )

On \_\_\_\_\_, 20\_\_\_\_, before me, \_\_\_\_\_,  
*Date* *Insert Name and Title of the Officer*

personally appeared \_\_\_\_\_  
*Name(s) of Signer(s)*

who proved to me on the basis of satisfactory evidence to be the person whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity, and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

Signature \_\_\_\_\_  
*Signature of Notary Public*

*Place Notary Seal Above*

**ACKNOWLEDGEMENT**

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

STATE OF CALIFORNIA )  
COUNTY OF \_\_\_\_\_ )

On \_\_\_\_\_, 20\_\_\_\_, before me, \_\_\_\_\_,  
*Date* *Insert Name and Title of the Officer*

personally appeared \_\_\_\_\_  
*Name(s) of Signer(s)*

\_\_\_\_\_ who proved to me on the basis of satisfactory evidence to be the person whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity, and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

Signature \_\_\_\_\_  
*Signature of Notary Public*

*Place Notary Seal Above*

**EXHIBIT "A-R"**

**LEGAL DESCRIPTION**

THE LAND HEREINAFTER DESCRIBED IS SITUATED IN THE CITY OF ONTARIO, STATE OF CALIFORNIA, COUNTY OF SAN BERNARDINO AND IS DESCRIBED AS FOLLOWS:

PARCEL 1 (SUNKIST)

GOVERNMENT LOTS 3 AND 4 AND THE SOUTHEAST ONE-QUARTER OF SECTION 15, TOWNSHIP 2 SOUTH, RANGE 7 WEST, SAN BERNARDINO MERIDIAN IN THE COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF.

EXCEPTING THEREFROM THAT PORTION LYING WITHIN THE LAND DESCRIBED IN THAT CERTAIN EASEMENT IN FAVOR OF THE SAN BERNARDINO COUNTY FLOOD CONTROL DISTRICT RECORDED APRIL 20, 1944, IN BOOK 1678 PAGE 154 OF OFFICIAL RECORDS.

ALSO EXCEPTING THEREFROM THAT PORTION CONVEYED TO THE SAN BERNARDINO COUNTY FLOOD CONTROL DISTRICT BY DEED RECORDED SEPTEMBER 29, 1977, IN BOOK 9273 PAGE 254 OF OFFICIAL RECORDS.

ALSO EXCEPTING THEREFROM THAT PORTION CONVEYED TO THE COUNTY OF SAN BERNARDINO BY DEED RECORDED AUGUST 29, 1990, AS INSTRUMENT NO. 90-344004 OF OFFICIAL RECORDS.

ALSO EXCEPT ALL COUNTY ROADS AND HIGHWAYS.



ALSO EXCEPTING THE FOLLOWING:

**EXHIBIT "A"**

SHEET 1 OF 1

GRANT DEED FOR LLA18-010  
SC ONTARIO DEVELOPMENT TO ONTARIO LAND VENTURES

THAT PORTION OF GOVERNMENT LOT 4, SECTION 15, TOWNSHIP 2 SOUTH, RANGE 7 WEST, SAN BERNARDINO BASE AND MERIDIAN, IN THE CITY OF ONTARIO, COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, ACCORDING TO THE UNITED STATES GOVERNMENT TOWNSHIP PLAT THEREOF, APPROVED BY THE SURVEYOR GENERAL ON AUGUST 30, 1873, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID GOVERNMENT LOT 1 AS SHOWN ON RECORD OF SURVEY 02-139, RECORDED IN BOOK 118, PAGES 82 THROUGH 95 INCLUSIVE OF RECORD OF SURVEYS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, SAID CORNER BEING ON THE CENTERLINE OF CARPENTER AVENUE AS SHOWN ON SAID RECORD OF SURVEY, SAID CORNER ALSO BEING ON THE EAST LINE OF RANCHO SANTA ANA DEL CHINO, AS PER MAP RECORDED IN BOOK 6, PAGE 15 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY;

THENCE ALONG SAID CENTERLINE AS SHOWN ON RECORD OF SURVEY, RECORDED IN BOOK 25, PAGE 15 OF RECORD OF SURVEYS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, NORTH 00° 02' 44" WEST, 187.76 FEET TO THE CENTERLINE OF EUCALYPTUS AVENUE, AS SHOWN ON LAST MENTIONED RECORD OF SURVEY;

THENCE ALONG THE EASTERLY PROLONGATION OF SAID CENTERLINE OF EUCALYPTUS AVENUE, NORTH 89° 19' 34" EAST, 144.43 FEET TO THE BEGINNING OF A TANGENT CURVE, CONCAVE SOUTHWESTERLY AND HAVING A RADIUS OF 905.00 FEET;

THENCE SOUTHEASTERLY ALONG SAID CURVE, THROUGH A CENTRAL ANGLE OF 27° 08' 53" AN ARC LENGTH OF 428.81 FEET TO THE BEGINNING OF A TANGENT REVERSE CURVE, CONCAVE NORTHEASTERLY AND HAVING A RADIUS OF 905.00 FEET, A RADIAL LINE TO SAID BEGINNING OF TANGENT REVERSE CURVE BEARS SOUTH 26° 28' 27" WEST;

THENCE SOUTHEASTERLY ALONG SAID CURVE, THROUGH A CENTRAL ANGLE OF 26° 27' 26" AN ARC LENGTH OF 417.90 FEET TO A POINT OF TANGENCY WITH THE NORTHERLY LINE OF SAID GOVERNMENT LOT 1, A RADIAL LINE TO SAID POINT OF TANGENCY BEARS SOUTH 00° 01' 02" WEST;

THENCE ALONG SAID NORTHERLY LINE OF GOVERNMENT LOT 1, NORTH 89° 58' 58" WEST, 961.54 FEET TO THE POINT OF BEGINNING.

CONTAINING: 105,724 SQUARE FEET OR 2.43 ACRES MORE OR LESS (GROSS)

EXHIBIT "B" ATTACHED HERETO AND BY THIS REFERENCE MADE A PART HEREOF.

SUBJECT TO: COVENANTS, CONDITIONS, RESTRICTIONS, RESERVATIONS, EASEMENTS AND RIGHTS-OF-WAY, IF ANY.

SURVEYOR:

PREPARED UNDER THE SUPERVISION OF:

Last Update: 12/04/18  
O:\3290\LLA\GRANT DEED\3290\_DEED.dwg

PREPARED BY:

**Thienes Engineering, Inc.**  
CIVIL ENGINEERING • LAND SURVEYING  
14349 FIRESTONE BOULEVARD  
LA MIRADA, CALIFORNIA 90638  
PH.(714)521-4811 FAX(714)521-4173

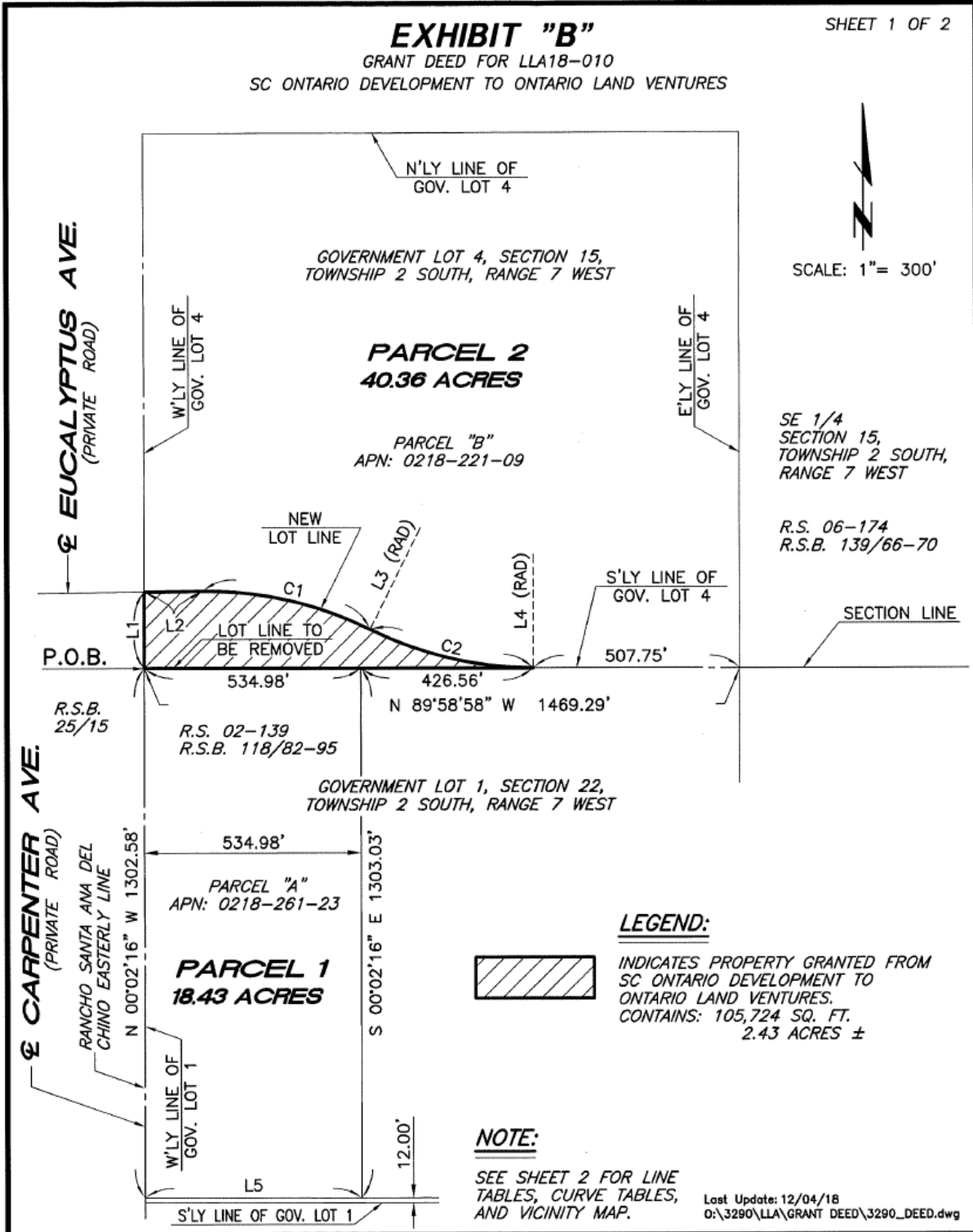
BRIAN L. THIENES \_\_\_\_\_ DATE \_\_\_\_\_  
P.L.S. NO. 5750  
REG. EXP. DEC. 31, 2019



**EXHIBIT "B"**

SHEET 1 OF 2

GRANT DEED FOR LLA18-010  
SC ONTARIO DEVELOPMENT TO ONTARIO LAND VENTURES



**EXHIBIT "B-R"**  
**PARKSIDE SPECIFIC PLAN**

Parkside



Exhibit 1-3: Land Use Plan

Legend	
<span style="display:inline-block; width:15px; height:15px; background-color:orange; border:1px solid black;"></span>	Medium Density Residential (11.1 - 25 du/ac)
<span style="display:inline-block; width:15px; height:15px; background-color:darkred; border:1px solid black;"></span>	Neighborhood Commercial (0.4 FAR)
<span style="display:inline-block; width:15px; height:15px; background-color:lightblue; border:1px solid black;"></span>	Public Facility
<span style="display:inline-block; width:15px; height:15px; background-color:green; border:1px solid black;"></span>	Open Space - Parkland
<span style="display:inline-block; width:15px; height:15px; background-color:yellow; border:1px solid black;"></span>	Open Space - Private/Village Edges
<span style="display:inline-block; width:15px; height:15px; background-color:grey; border:1px solid black;"></span>	Roads - Master Planned
<span style="display:inline-block; width:15px; height:15px; background-color:lightgrey; border:1px solid black;"></span>	Roads - Local
<span style="display:inline-block; width:15px; height:15px; background-color:lightblue; border:1px solid black;"></span>	Cucamonga Creek Channel

## EXHIBIT "C-R"

### EXISTING DEVELOPMENT APPROVALS

#### On July 25, 2006, the Planning Commission:

- a) Issued Resolution No. PC06-060 Recommending City Council Adopt and certify the Parkside Specific Plan Environmental Impact Report.
- b) Issued Resolution No. PC06-061 Recommending City Council approval of the General Plan Amendment (File No. PGPA04-003).
- c) Issued Resolution No. PC06-062 Recommending City Council approval of the Parkside Specific Plan (File No. PSP03-002).
- d) Issued Resolution No. PC06-063 approving Tentative Tract Map 18048 (File No. PMTT06-039).

#### On August 15, 2006, the City Council:

- a) Adopted and certified the Parkside Specific Plan Environmental Impact Report and issued Resolution No. 2006-068.
- b) Approved General Plan Amendment (File No. PGPA04-003) and issued Resolution No. 2006-068A.
- c) Adopted Ordinance No. 2838 approving the Parkside Specific Plan (File No. PSP03-002).

#### On September 19, 2006, the City Council:

- a) Adopted Ordinance No. 2841, approving the original Development Agreement between the City of Ontario and SC Ontario Development Company, LLC, to provide the funding for additional City services required to support the Parkside Specific Plan (File No. PSP03-002) residential development of up to 1,947 residential units and infrastructure improvements required to support the related Tract Map 18048 (File No. PMTT06-039).

#### On July 7, 2009, the City Council:

- a) Adopted Ordinance No. 2909, approving the First Amendment to the Development Agreement (File No. PDA09-002) between the City of Ontario and SC Ontario Development Company, LLC, to allow Tract Map 18048 to be recorded for the purposes of financing.

**EXHIBIT “C-R” Continued**

**EXISTING DEVELOPMENT APPROVALS**

On December 15, 2014, the Planning Commission:

- a) Adopted Resolution No. PC14-116 recommending City Council approval of the SC Ontario Development Company, LLC Second Amendment to the Development Agreement (File No. PDA14-007); and
- b) Issued Resolution No. PC14-117 approving Tentative Tract Map No. 18999 (File No. PMTT14-021).

On February 3, 2015, the City Council:

- a) Adopted Ordinance No. 3012 approving the Second Amendment to the Development Agreement (File No. PDA14-007) between the City of Ontario and SC Ontario Development Company, LLC.

On April 23, 2019, the Planning Commission:

- a) Adopted Resolution No. PC19-025 recommending City Council approval of the Third Amendment to the Development Agreement (File No. PDA05-002) between the City of Ontario and SC Ontario Development Company, LLC.

On June 18, 2019, the City Council:

- a) Adopted Ordinance No. 3134 approving the Third Amendment to the Development Agreement (File No. PDA05-002) between the City of Ontario and SC Ontario Development Company, LLC.

## **EXHIBIT “D-R”**

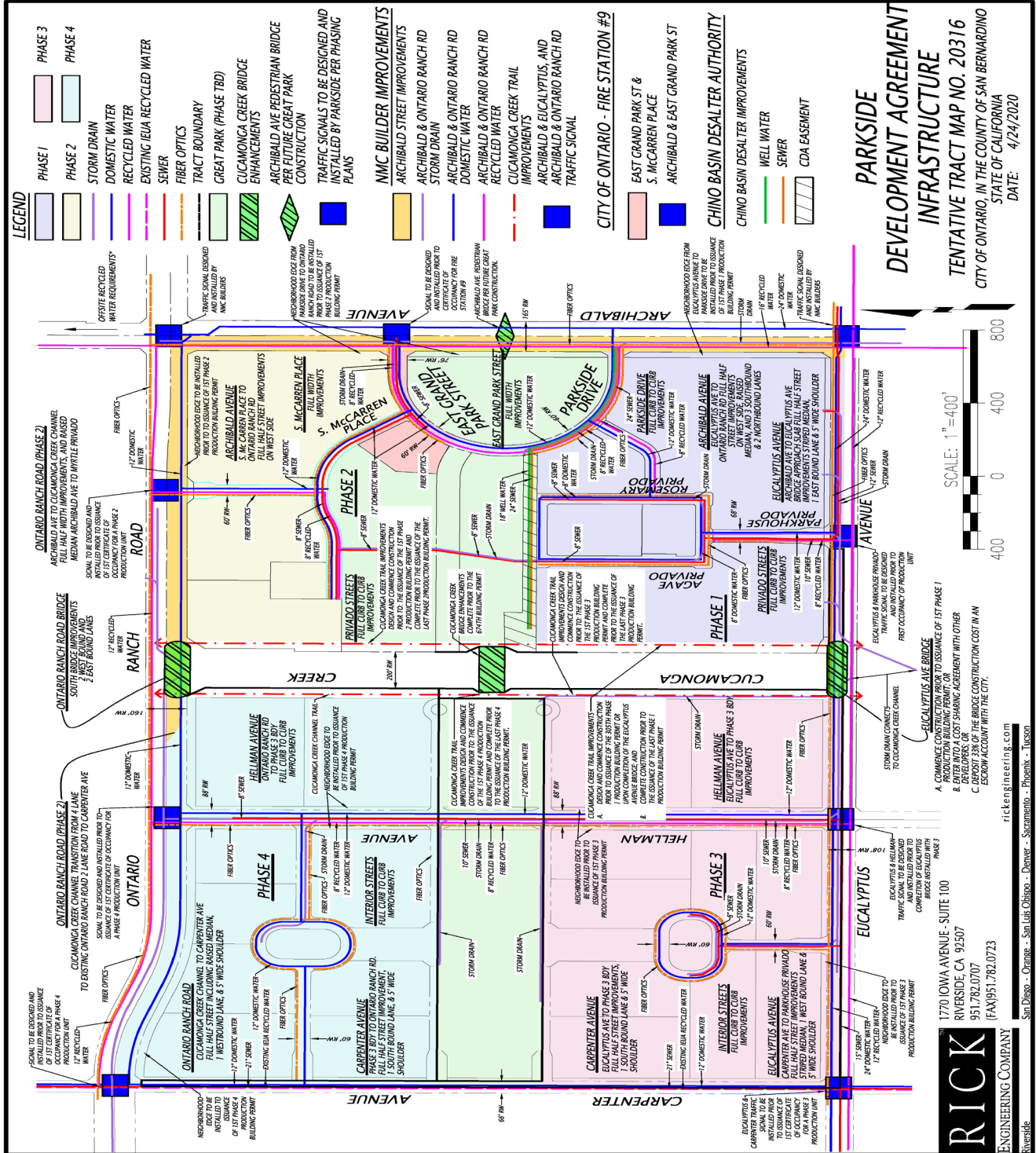
### **EXISTING LAND USE REGULATIONS**

The Documents are listed for reference only:

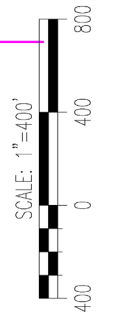
1. Parkside Specific Plan Environmental Impact Report, Resolution No. 2006-068.
2. General Plan Amendment (File No. PGPA04-003), Resolution No. 2006-068A.
3. Parkside Specific Plan (File No. PSP03-002), Ordinance No. 2838.
4. City of Ontario Municipal Code:
  - a. Six – Sanitation & Health
  - b. Seven – Public Works
  - c. Eight – Building Regulations
  - d. Nine – Development Code
  - e. Ten – Parks and Recreation

# EXHIBIT "E-R"

## CONCEPTUAL PHASING PLAN



**PARKSIDE DEVELOPMENT AGREEMENT INFRASTRUCTURE**  
**TENTATIVE TRACT MAP NO. 20316**  
 CITY OF ONTARIO, IN THE COUNTY OF SAN BERNARDINO  
 STATE OF CALIFORNIA  
 DATE: 4/24/2020



A. COMMENCE CONSTRUCTION PRIOR TO ISSUANCE OF 1ST PHASE 1 PRODUCTION BUILDING PERMIT; OR  
 B. SHARING AGREEMENT WITH OTHER DEVELOPERS; OR  
 C. DEPOSIT 33% OF THE BRIDGE CONSTRUCTION COST IN AN ESCROW ACCOUNT WITH THE CITY.

1770 IOWA AVENUE, SUITE 100  
 RIVERSIDE, CA 92507  
 951.782.0707  
 (FAX) 951.782.0723



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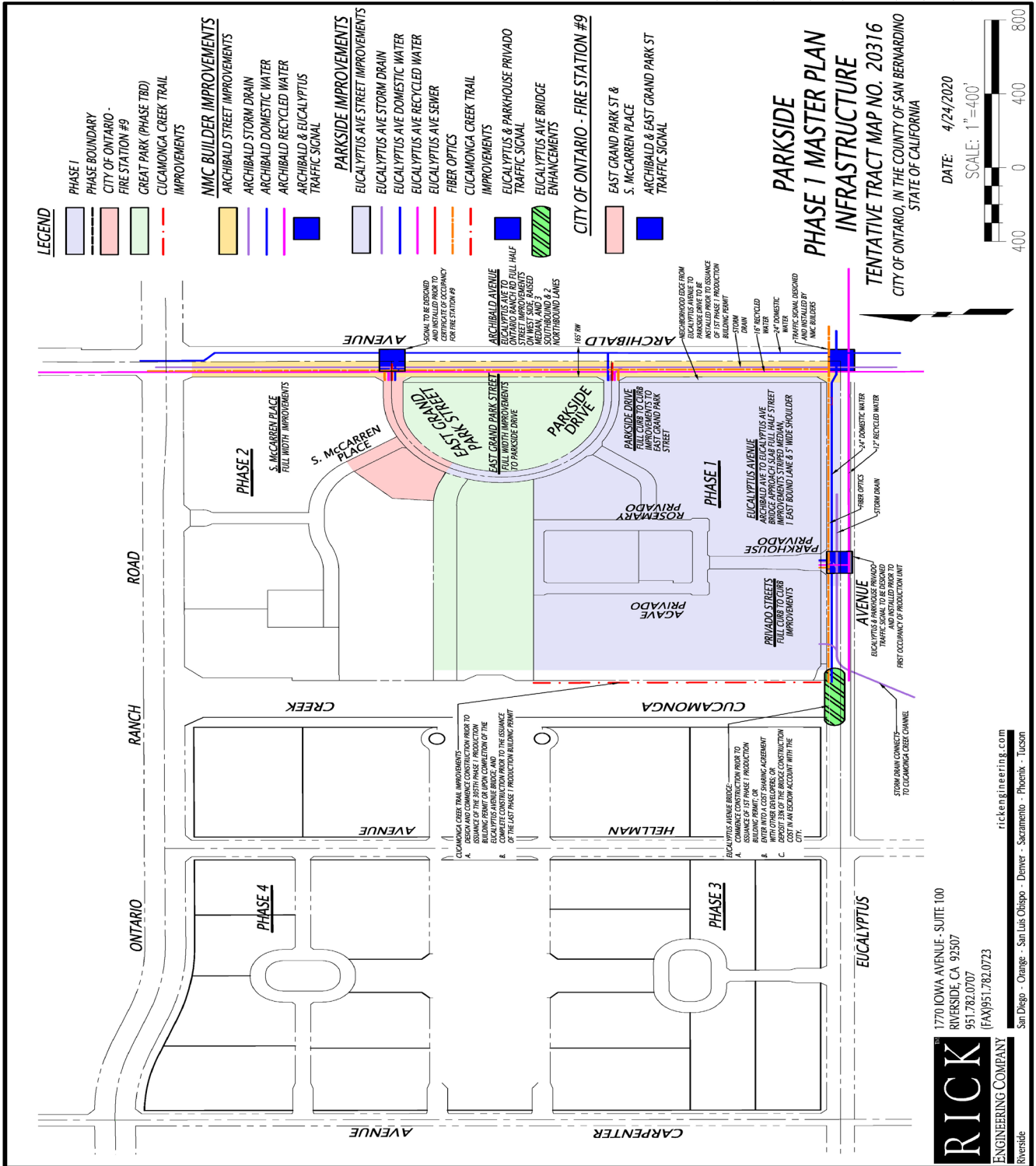
**EXHIBIT “F”**

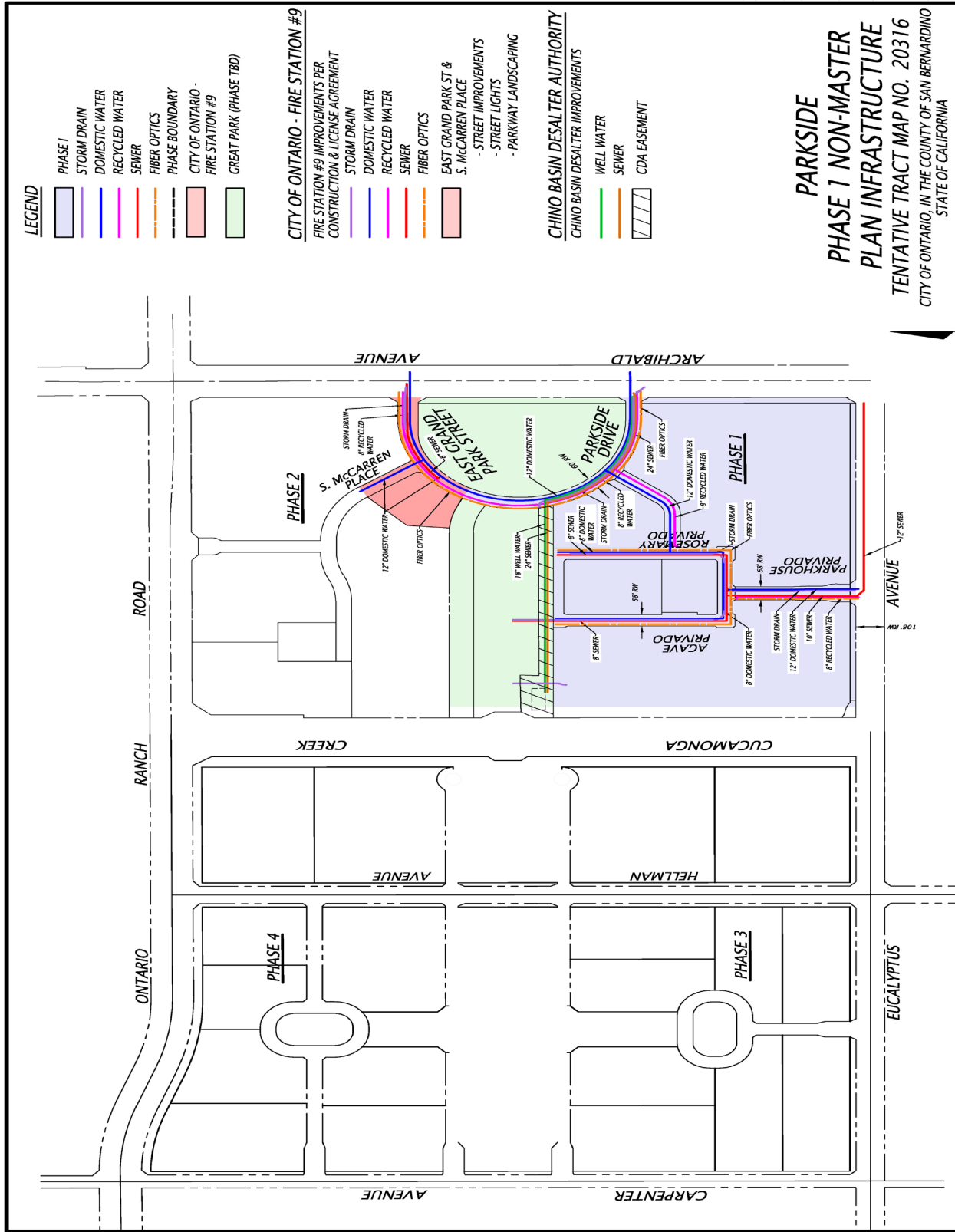
**INFRASTRUCTURE IMPROVEMENTS EXHIBIT FOR ALL PHASES**

**[SEE ATTACHED]**



# Exhibit "F-Phase 1" - Phase 1 Improvements





**LEGEND**

- PHASE 1
- STORM DRAIN
- DOMESTIC WATER
- RECYCLED WATER
- SEWER
- FIBER OPTICS
- PHASE BOUNDARY
- CITY OF ONTARIO -
- FIRE STATION #9
- GREAT PARK (PHASE TBD)

**CITY OF ONTARIO - FIRE STATION #9**

- FIRE STATION #9 IMPROVEMENTS PER CONSTRUCTION & LICENSE AGREEMENT
- STORM DRAIN
- DOMESTIC WATER
- RECYCLED WATER
- SEWER
- FIBER OPTICS
- EAST GRAND PARK ST & S. MCCARRREN PLACE
- STREET IMPROVEMENTS
- STREET LIGHTS
- PARKWAY LANDSCAPING

**CHINO BASIN DESALTER AUTHORITY**

- CHINO BASIN DESALTER IMPROVEMENTS
- WELL WATER
- SEWER
- CDA EASEMENT

**PARKSIDE  
PHASE 1 NON-MASTER  
PLAN INFRASTRUCTURE  
TENTATIVE TRACT MAP NO. 20316  
CITY OF ONTARIO, IN THE COUNTY OF SAN BERNARDINO  
STATE OF CALIFORNIA**

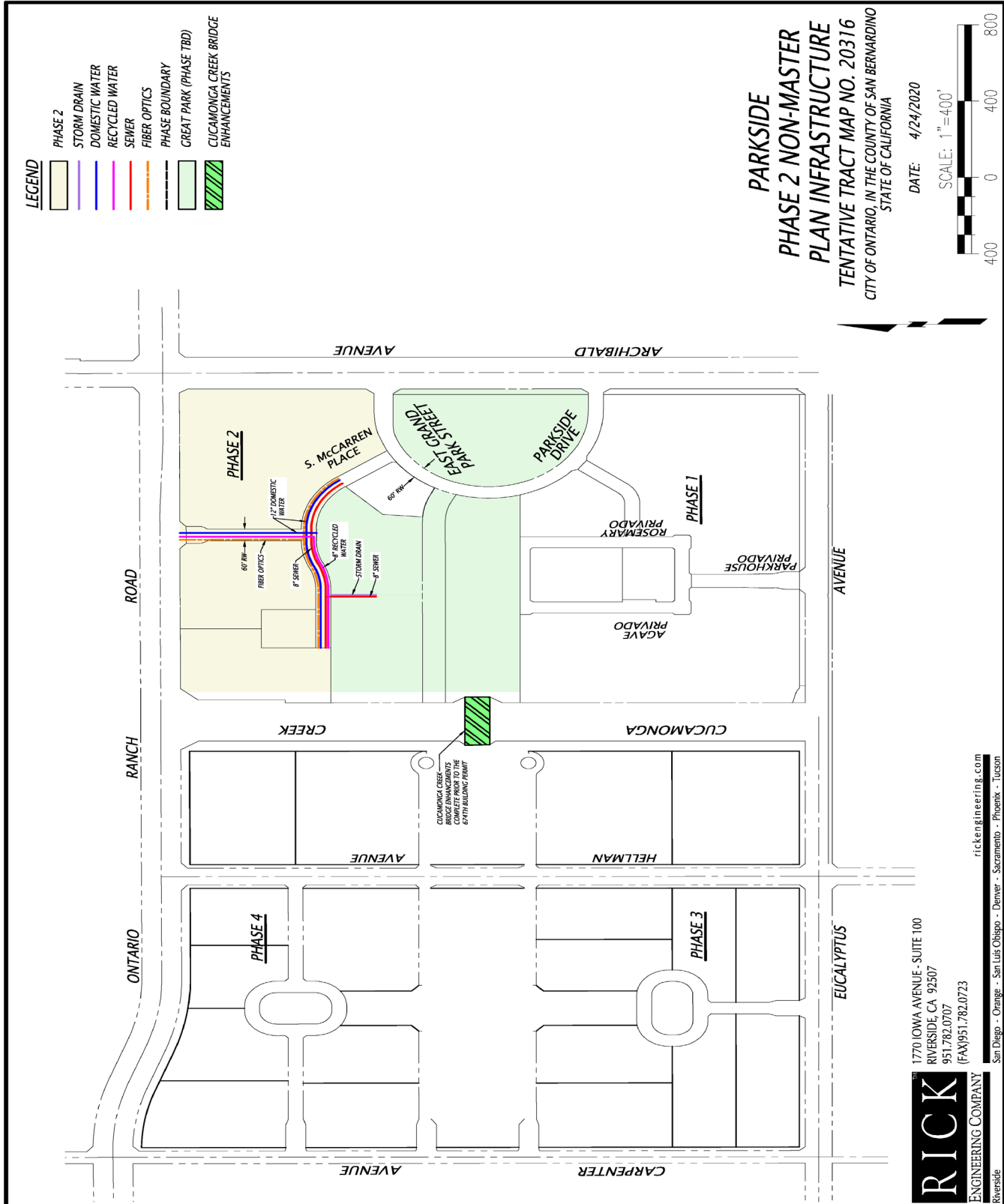
DATE: 4/24/2020  
SCALE: 1"=400'  
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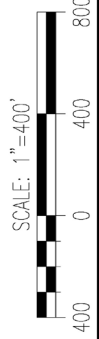


- LEGEND**
- PHASE 2
  - STORM DRAIN
  - DOMESTIC WATER
  - RECYCLED WATER
  - SEWER
  - FIBER OPTICS
  - PHASE BOUNDARY
  - GREAT PARK (PHASE TBD)
  - CUCAMONGA CREEK BRIDGE ENHANCEMENTS

**PARKSIDE  
PHASE 2 NON-MASTER  
PLAN INFRASTRUCTURE  
TENTATIVE TRACT MAP NO. 20316**

CITY OF ONTARIO, IN THE COUNTY OF SAN BERNARDINO  
STATE OF CALIFORNIA

DATE: 4/24/2020

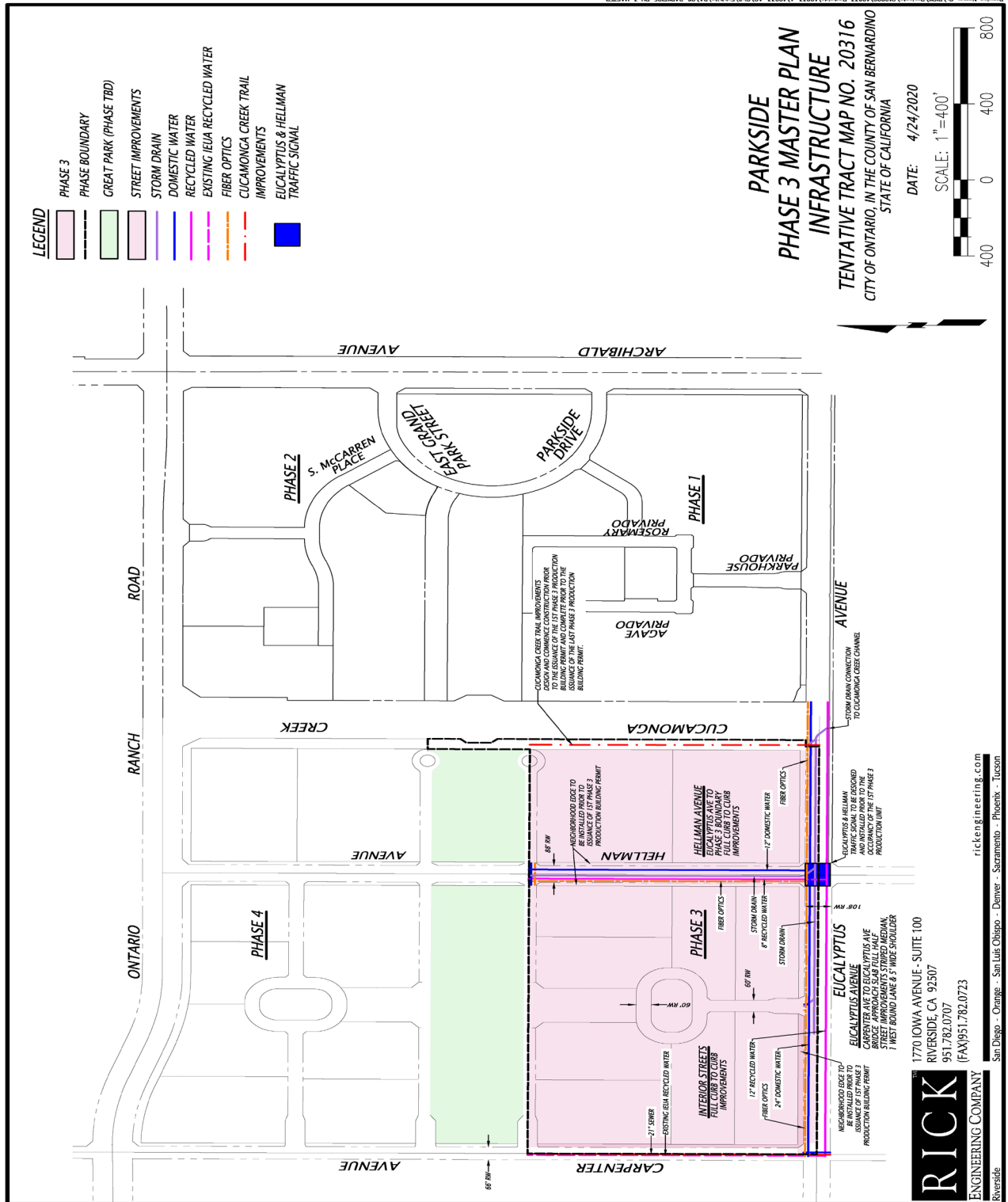


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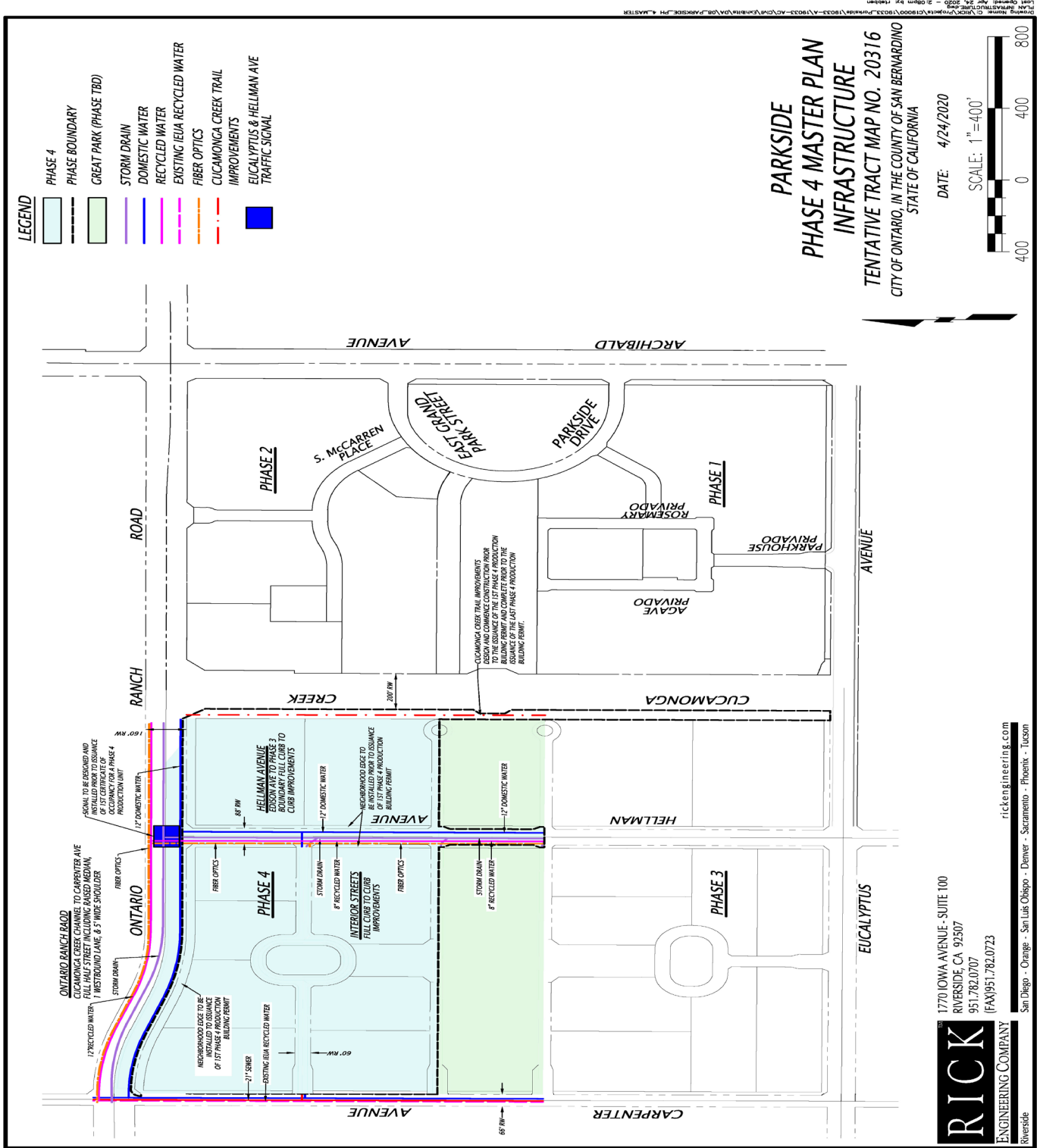
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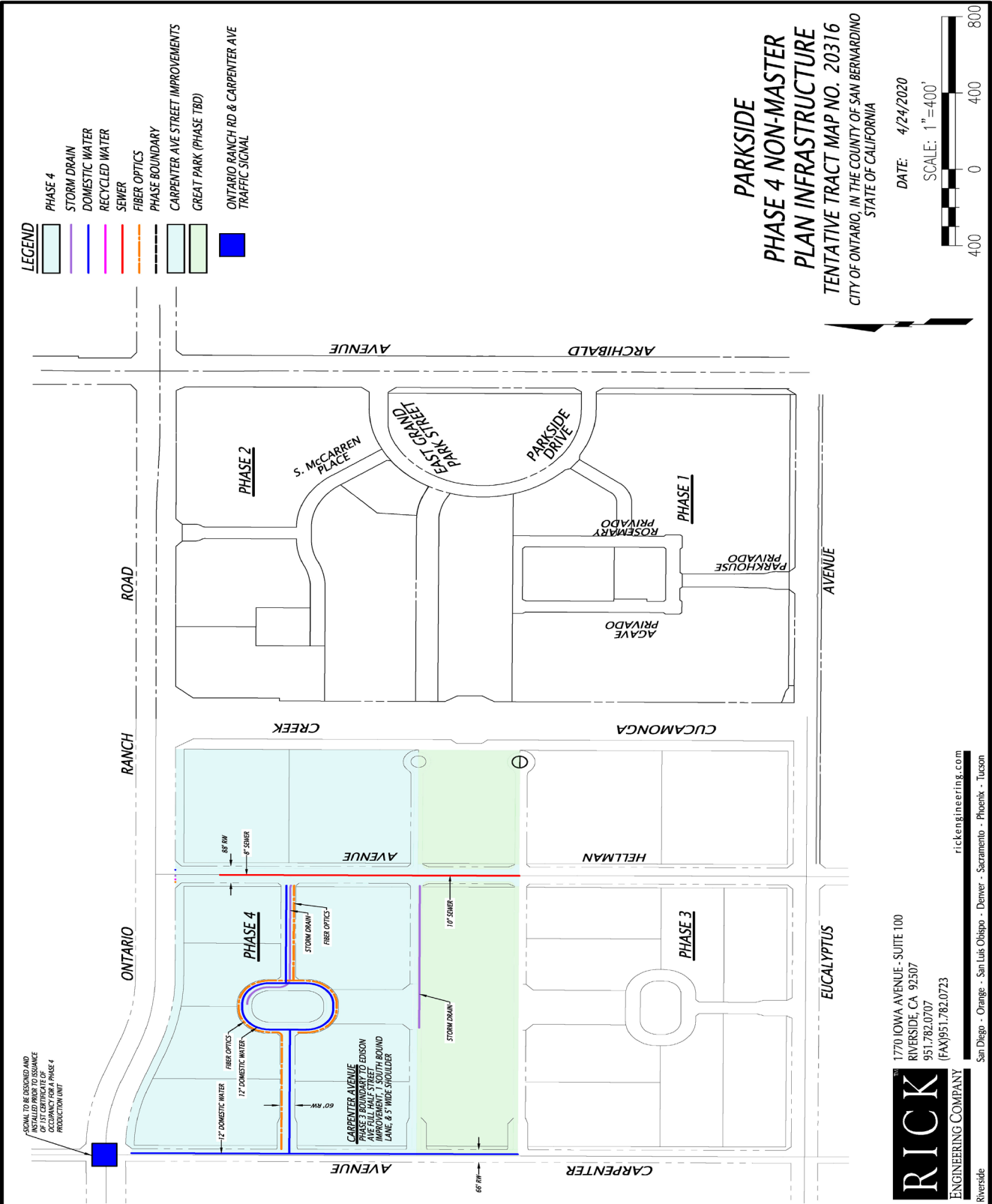
# Exhibit "F-Phase 3" - Phase 3 Improvements





# Exhibit "F-Phase 4" - Phase 4 Improvements





- LEGEND**
- PHASE 4
  - STORM DRAIN
  - DOMESTIC WATER
  - RECYCLED WATER
  - SEWER
  - FIBER OPTICS
  - PHASE BOUNDARY
  - CARPENTER AVE STREET IMPROVEMENTS
  - GREAT PARK (PHASE TBD)
  - ONTARIO RANCH RD & CARPENTER AVE TRAFFIC SIGNAL

**PARKSIDE  
PHASE 4 NON-MASTER  
PLAN INFRASTRUCTURE  
TENTATIVE TRACT MAP NO. 20316**  
CITY OF ONTARIO, IN THE COUNTY OF SAN BERNARDINO  
STATE OF CALIFORNIA

DATE: 4/24/2020

SCALE: 1"=400'



SIGNAL TO BE DESIGNED AND  
INSTALLED PRIOR TO ISSUANCE  
OF TENTATIVE TRACT MAP NO. 20316  
PHASE 4  
PRODUCTION UNIT

1770 IOWA AVENUE - SUITE 100  
RIVERSIDE, CA 92507  
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# PLANNING COMMISSION STAFF REPORT

May 26, 2020

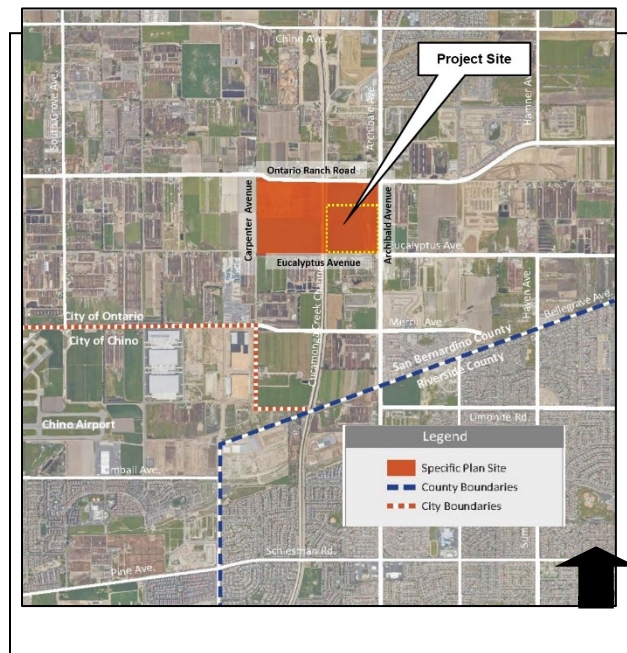
**FILE NO.:** PMTT19-020

**SUBJECT:** A Tentative Tract Map (File No. PMTT19-20/TT20316) for condominium purposes to subdivide 56.99 acres of land into 4 numbered residential lots and 12 lettered lots for common areas, portions of the Great Park, private streets, public utility easements and neighborhood landscape edges, located at the northwest corner of Archibald Avenue and Eucalyptus Avenue, within Planning areas 1 thru 4 (Conventionally-Loaded Lane Homes/Detached Green Court Cluster Homes) of the Parkside Specific Plan (APNs: 0218-231-12, 14, 17-21, 28-30, 33, 35 and 38); **submitted by SC Ontario Development Company, LLC.**

**PROPERTY OWNER:** SC Ontario Development Company, LLC

**RECOMMENDED ACTION:** That the Planning Commission consider and approve File No. PMTT19-020, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval contained in the attached departmental reports.

**PROJECT SETTING:** The project site is comprised of 56.99 acres of land located at northwest corner of Archibald Avenue and Eucalyptus Avenue, within the Planning areas 1 through 4 (Conventionally-Loaded Lane Homes/Detached Green Court Cluster Homes) of the Parkside Specific Plan, and is depicted in Figure 1: Project Location, below. The project site has historically been used for agricultural purposes, primarily for field crop farming. The project site is presently vacant and used for crop production. The natural vegetation and soil conditions that once occurred throughout the project area have been significantly altered through agricultural uses, leaving little to no native vegetation. In addition, the project area



**Figure 1: Project Location**

Case Planner:	Rudy Zeledon
Planning Director Approval:	
Submittal Date:	October 16, 2019

Hearing Body	Date	Decision	Action
DAB	05/18/2020	Approval	Recommend
PC	05/26/2020		Final
CC			

generally slopes southwest at a slope of approximately 0.8% to 2.0%.

## **PROJECT ANALYSIS:**

[1] Background — The Parkside Specific Plan and the Environmental Impact Report (EIR) were approved by the City Council in September 2006. The Parkside Specific Plan (**See attached Exhibit A: Parkside Specific Plan Land Use Plan**) established the land use designations, development standards, design guidelines and development capacity of 1,947 residential units and 115,000 square feet of commercial uses for the Specific Plan area. The Specific Plan is comprised of twenty-six (26) land use districts, incorporating nineteen (19) distinctive neighborhoods and offering a variety of residential products.

On July 25, 2006, the Planning Commission approved Tentative Tract Map 18048 (“A” Map) to facilitate the backbone infrastructure improvements (major streets, sewer, water and storm drain facilities) and the creation of a parcel lot for the future City Fire Station 9, recreational facility, parks and residential neighborhoods within the eastern portion of the Specific Plan area. Subsequently, on December 15, 2014, the Planning Commission approved Tentative Tract Map 18998 (File No. PMTT14-021), that revised the original approved Tentative Tract Map 18048 (“A” Map) to allow for the phasing of the map into two phases, reconfigure the previous loop street into two separate streets (Parkside Drive and Park Vista Drive), and merge various lots into single lots. The phasing of the Tentative Tract Map provided for the orderly build-out of the backbone infrastructure and laid the groundwork for the future “B” maps, further subdividing the parcels into residential neighborhoods.

The Applicant, SC Ontario Development Company, LLC, is requesting approval of Tentative Tract Map 20316 (File No. PMTT19-020) for condominium purposes to subdivide 56.99 acres of land into four (4) numbered residential lots and twelve (12) lettered lots for common areas, portions of the Great Park, private streets, public utility easements and neighborhood landscape edges. The project is located at the northwest corner of Archibald Avenue and Eucalyptus Avenue, within Planning areas 1 thru 4 (Conventionally-Loaded Lane Homes/Detached Green Court Cluster Homes) of the Parkside Specific Plan. In conjunction, with the Tentative Tract Map Application, the Applicant has submitted an Amendment to the Parkside Specific Plan (File No. PSPA19-007) to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). Approval of the proposed Tentative Tract Map (File No. PMTT19-020/TT20316) will be contingent upon Planning Commission and City Council Approval.

In addition, the proposed Tentative Tract Map 20316 will facilitate a Development Plan (File No. PDEV19-064), submitted by the New Home Company, to construct 540 conventional land loaded single family homes. The Development Plan will require Planning Commission review and approval.

[2] Tract Map Subdivision — Tentative Tract Map 20316 proposes to subdivide 56.99 acres of land into four (4) numbered residential lots for condominium purposes and twelve (12) lettered lots for common areas, portions of the Great Park, private streets, public utility easements and neighborhood landscape edges (**See attached Exhibit “B” Tentative Tract Map 20316**). The 4 residential lots will provide for (Lots 1-4) for detached single-family conventional lane loaded and detached green court condominium homes. The lots range in size from 2.40 to 14.59 acres, consistent with the development regulations and standards of the Parkside Specific Plan. The twelve (12) lettered lots will accommodate the following:

- Lot A: 1.32-acre private recreational facility.
- Lots B, C and D: 16.84 acres of the Great Park.
- Lots G and H: 50-foot wide landscaped “Neighborhood Edge” along the west side of Archibald Avenue.
- Lots E and F: 35-foot wide Landscape “Neighborhood Edge” along the north side of Eucalyptus Avenue.
- Lots J, K and L: Private interior tract streets with public utility easements.
- Lot I: Provide an access road from Eucalyptus Avenue to the Cucamonga Creek Channel to the future regional trail.

The Tentative Tract Map will also implement the required infrastructure improvements to serve the site. These improvements include the backbone infrastructure and the internal street circulation for the residential neighborhoods (Private Streets A, B, C, E, F, G, H, I and J). The portion of the Great Park will be constructed at a future time. As part of the related Development Agreement (File No. PDA05-002), the Parkside Specific Plan will be developed in four phases. The first phase of the Development Agreement includes the flowing Infrastructure improvements:

- Archibald Avenue: The NMC builders have constructed the street improvements along the projects Archibald Avenue frontage that include, curb/gutter, four south bound lanes, a 26-foot-wide median, two north bound lanes and signalized in intersection at Archibald Avenue and Eucalyptus Avenue. The project will be required to construct the 50 wide neighborhood edge, along the projects Archibald Avenue frontage, that will include a 7-foot parkway, 5-foot sidewalk, an 8-foot wide multipurpose trail and 35-foot-wide landscape edge.
- Eucalyptus Avenue: Full half street improvements (two lanes, curb, gutter, sidewalk/parkway and neighborhood edge), from Archibald Avenue to Eucalyptus Avenue bridge approach slab. This includes two west bound lanes, curb, gutter,

sidewalk, neighborhood edge, traffic signed at the primary community entrance, stripped median, 1 east bound lane and a 5 -foot shoulder on the south side of Eucalyptus along the project frontage. In addition, Eucalyptus Avenue Cucamonga Creek Channel bridge improvements (widening) will required.

- Parkside Drive: The project will construct Parkside Drive to its full width of 60 feet, that includes a 12-foot-wide parkway (7-foot landscape parkway and 5-foot sidewalk) on each side of the drive. Parkside Drive will be improved north to the City's future Fire Station 9. The City will be responsible to complete Parkside Drive as part of the development of the Fire Station 9.

Pursuant to the Development Agreement the City and the Applicant will jointly develop a plan for the timing and phasing of the development, funding, completion, and acquisition of each phase of design and construction of the Great Park.

[3] Site Access/Circulation — The project will have primary access from Eucalyptus Avenue, through signalized intersection. Secondary access will be provided from Parkside Drive, located on the northeast area of site, with access to Archibald Avenue.

[4] Open Space — The Tentative Tract Map will facilitate the construction of a 2.78-acre neighborhood park, sidewalks, parkways, and open space areas within the tract. TOP Policy PR1-1 requires new developments to provide a minimum of 2 acres of Private Park per 1,000 residents. The proposed project is required to provide 4.1 acres parkland to meet the minimum TOP private park requirement. To satisfy the park requirement, as part of the Development Agreement, the applicant will be required construct and transfer the portions of the Great Park identified in the Parkside Specific Plan in four (4) phases to the City. In addition, the applicant (Owner) will enter a Development Impact Fee (DIF) credit and Reimbursement Agreement with City for the design and construction of the Great Park areas.

[5] CC&R's — As a Condition of Approval, CC&R's will be required to be prepared and recorded with the final map. The CC&R's will outline the maintenance responsibilities for the open space areas, recreation amenities, drive aisles, utilities and upkeep of the entire site to ensure the on-going maintenance of the common areas and facilities.

**COMPLIANCE WITH THE ONTARIO PLAN:** The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner

- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)
- Ensure the Development of a Well Planned, Balanced, and Self-Sustaining Community in Ontario Ranch

[2] Vision.

**Distinctive Development:**

- Commercial and Residential Development
  - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

[3] Governance.

**Decision Making:**

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
  - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

[4] Policy Plan (General Plan)

**Land Use Element:**

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
  - LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.
  - LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).
- Goal LU2: Compatibility between a wide range of uses.

➤ LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

### **Housing Element:**

▪ Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

➤ H2-4 New Model Colony. We support a premier lifestyle community in the New Model Colony distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.

➤ H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.

Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.

➤ H5-2 Family Housing. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation and other amenities.

### **Community Economics Element:**

▪ Goal CE1: A complete community that provides for all incomes and stages of life.

➤ CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

▪ Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

➤ CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

➤ CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

**Community Design Element:**

▪ Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➤ CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

➤ CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

➤ CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

▪ Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

➤ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➤ CD2-2 Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and

- Landscaped parkways, with sidewalks separated from the curb.
- CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.
- CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.
- CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.
- CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.
- CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.
- CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.
  - Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.
- CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.
  - Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.



➤ CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.

➤ CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

**HOUSING ELEMENT COMPLIANCE:** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project is consistent with the number of dwelling units (540) and density (12 to 18 DU/Acre) specified in the Available Land Inventory. Per the Available Land Inventory, the Parkside Specific Plan is required to provide 1,947 dwelling units with an overall density range of between 9 and 21 DU/AC.

**AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE:** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

**ENVIRONMENTAL REVIEW:** The environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference. All

previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.

**CONDITIONS OF APPROVAL:** See attached department reports.

**TECHNICAL APPENDIX:**

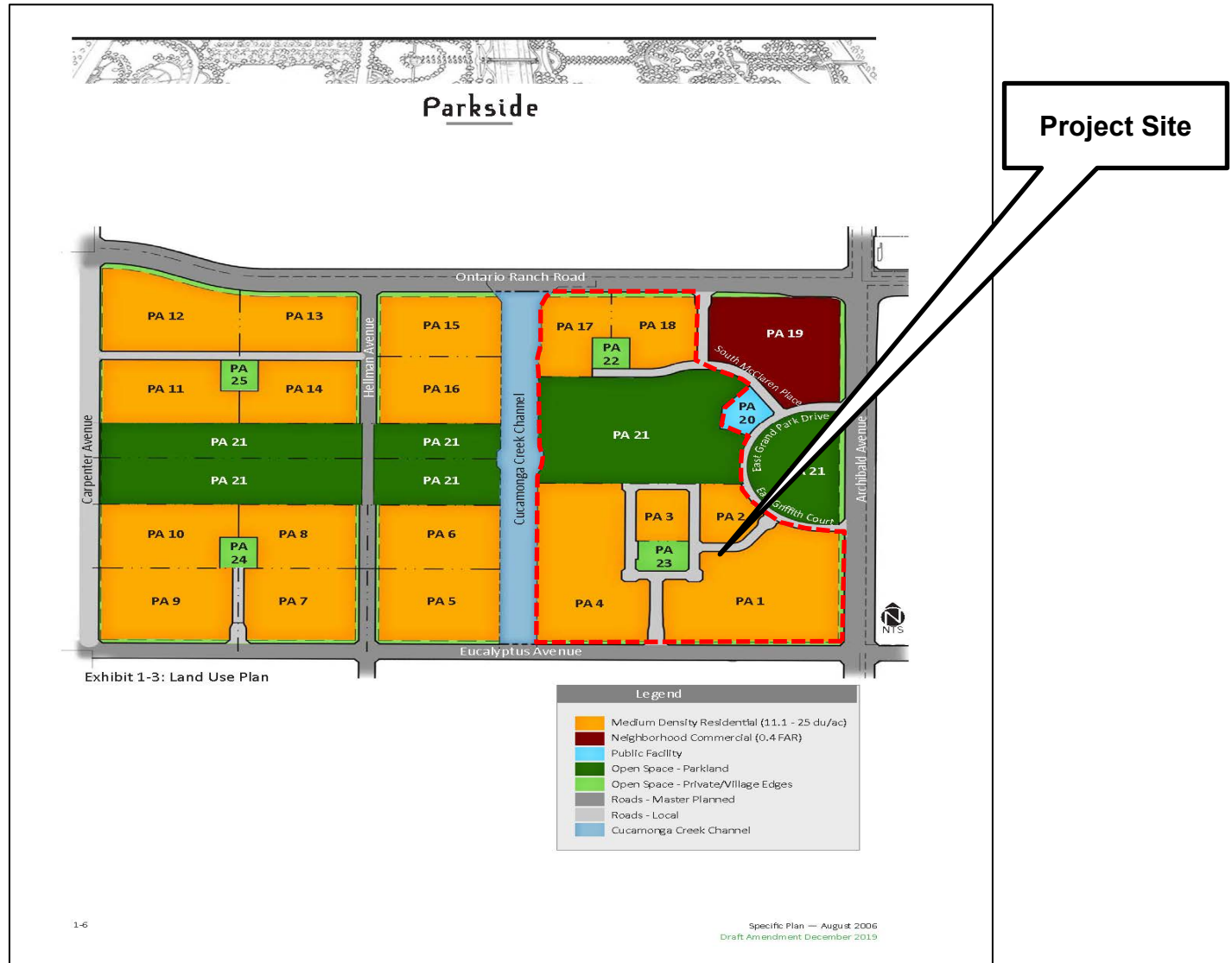
**Surrounding Zoning and Land Use:**

	<i>Existing Land Use</i>	<i>General Plan Designation</i>	<i>Zoning Designation</i>	<i>Specific Plan Land Use</i>
Site:	Agricultural Corp Production	Medium Density Residential (11.1 – 25 du/ac)	Parkside Specific Plan	Planning Areas 1 thru 4 (Conventionally Loaded Lane Homes), 17 and 18 (Townhome), 21 (Great Park), and 22 and 23 (Private Parks)
North:	Agricultural Corp Production	Low Density Residential (2.1 – 5 du/ac)	The Avenue Specific Plan	Low Density Residential
South:	Mass Graded Vacant Land	Low Density Residential (2. Commercial (0.40 FAR) 1 – 5 du/ac)	Subarea 29 Specific Plan	Neighborhood Commercial Low Density Residential
East:	Vacant	Medium Density Residential (11.1 – 25 du/ac) and Open Space-Parkland	Grand Park Specific Plan	High Density Residential Great Park
West:	Cucamonga Creek Channel	Open Space-Non-Recreation	Cucamonga Creek Channel	N/A

**Tentative Tract Map Summary:**

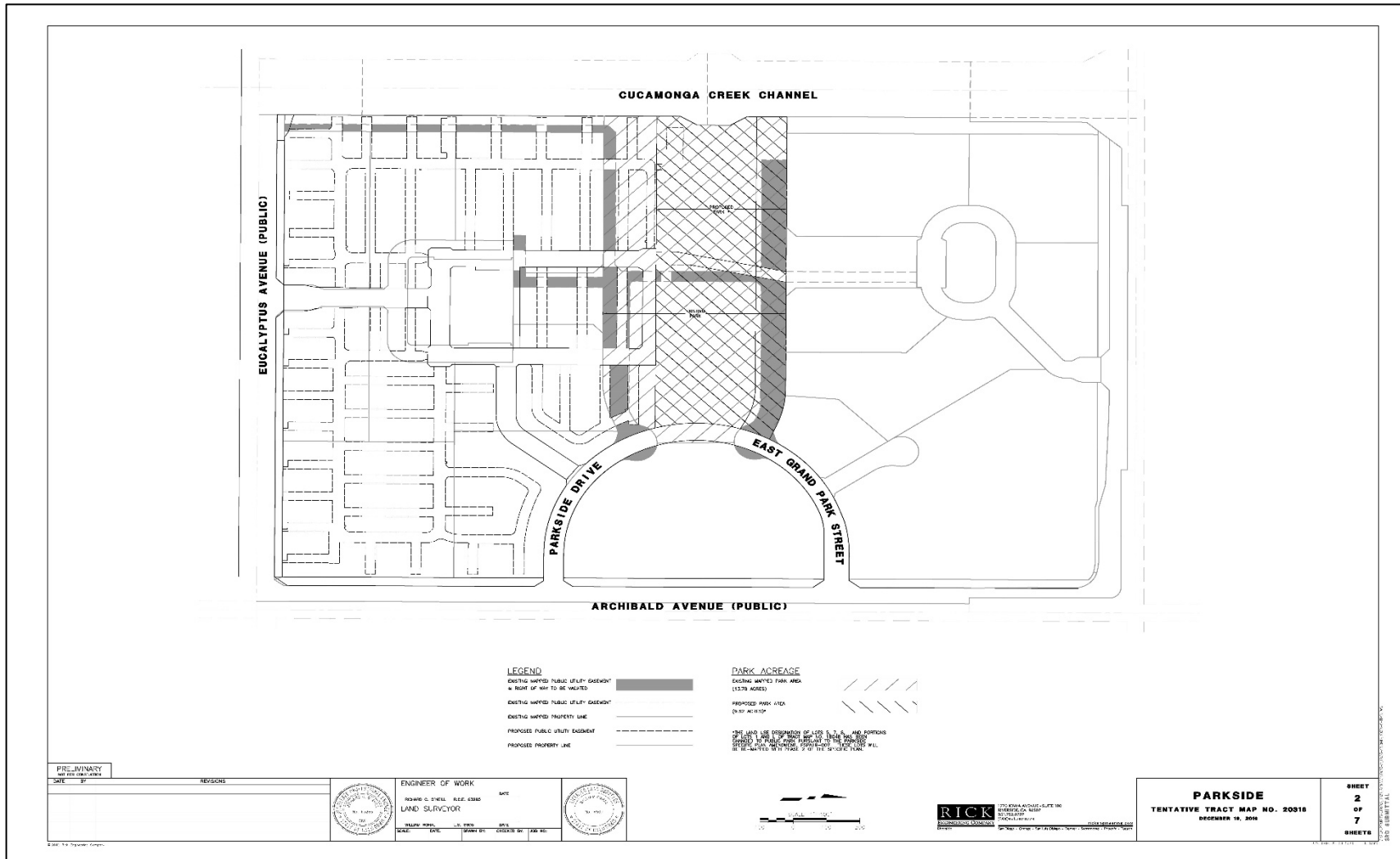
<i>Item</i>	<i>Proposed</i>
<i>Project area (in acres):</i>	56.99
<i>Numbered Lots:</i>	4
<i>Number of Lettered Lots:</i>	12
<i>Minimum Lot Size:</i>	2.40 Acres
<i>Maximum Lot Size:</i>	14.59 Acres
<i>Average Lot Size:</i>	8.25 Acres

**Exhibit A—Parkside Specific Plan Land Use Plan**

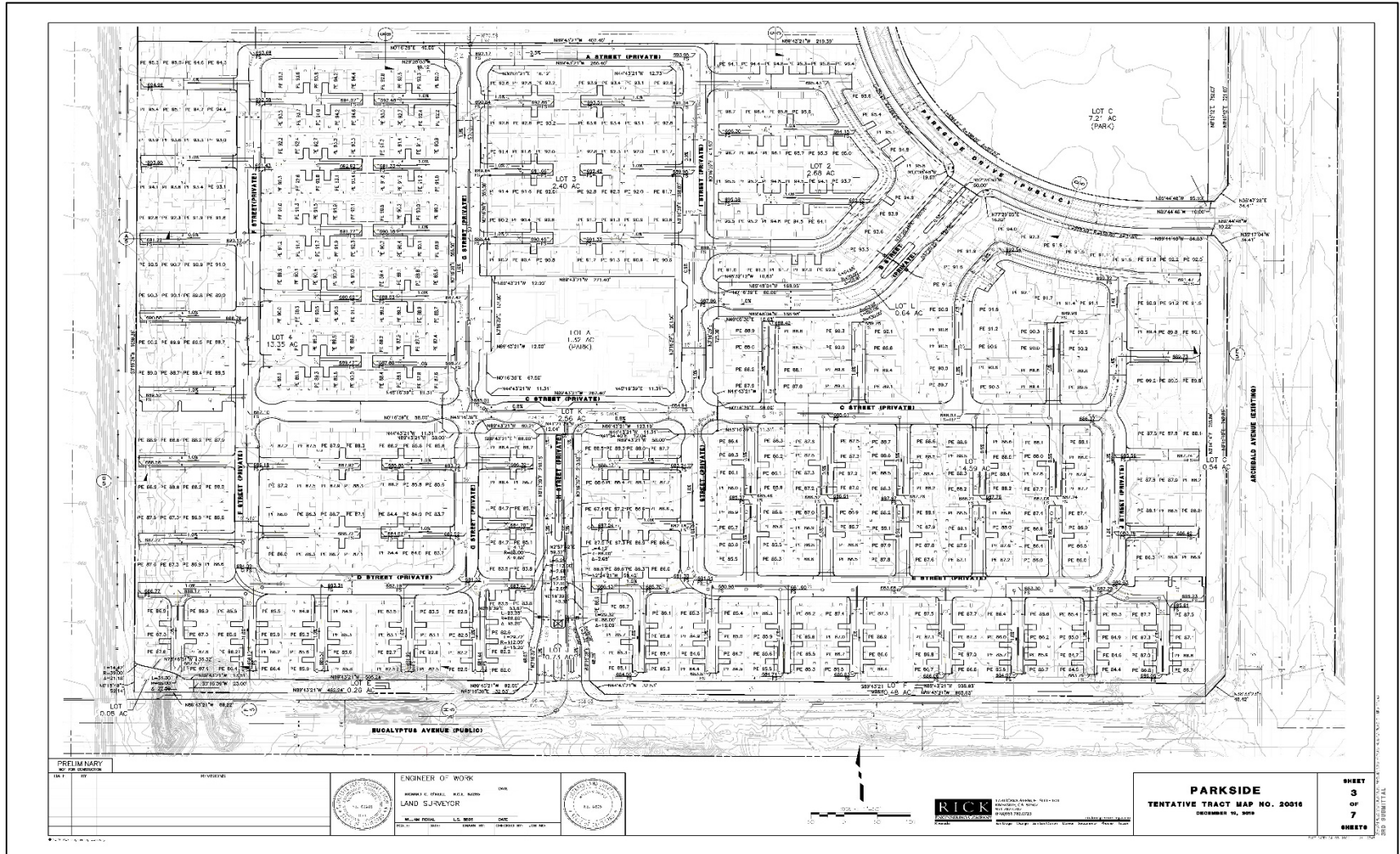




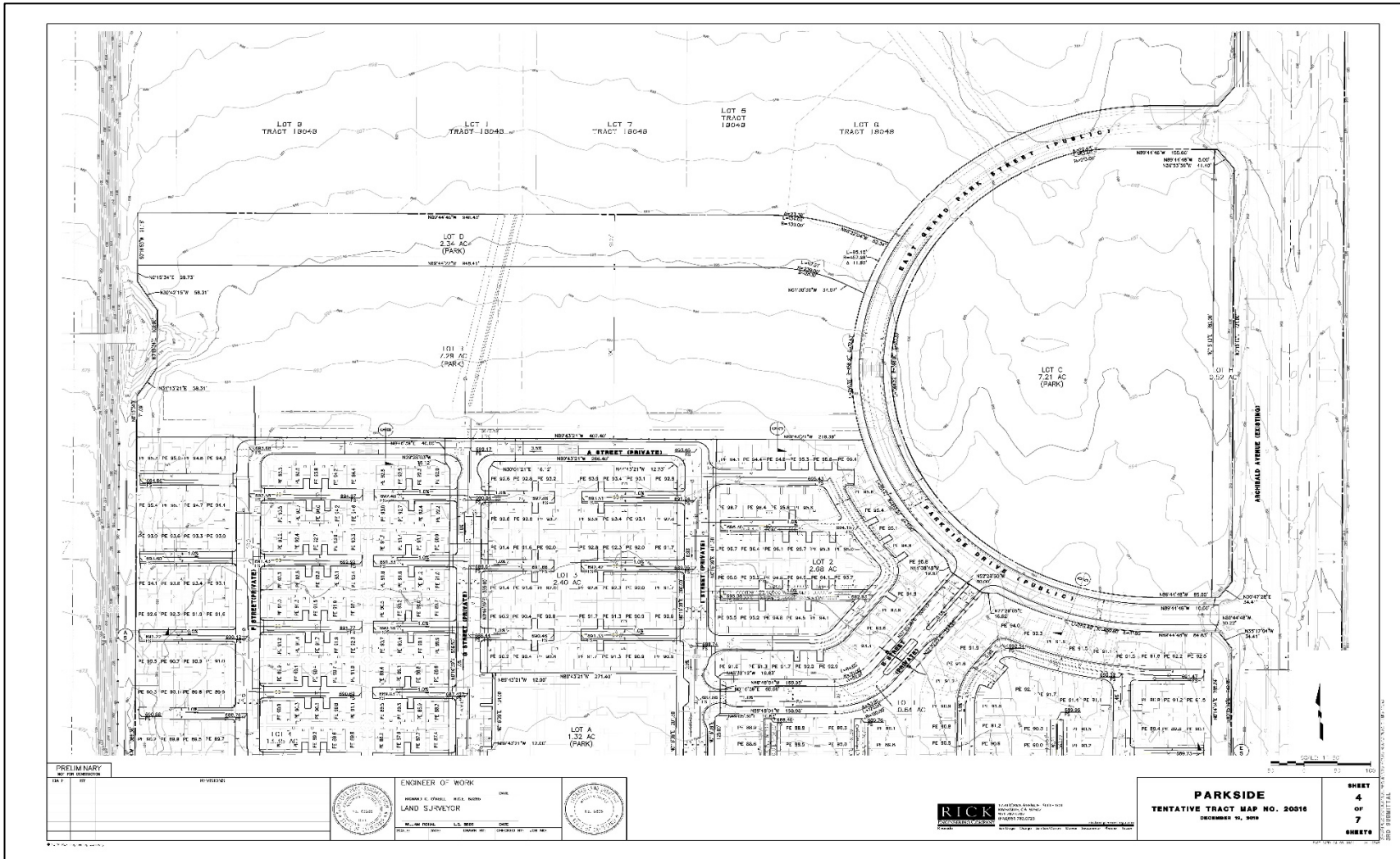
**Exhibit B—Tentative Tract Map 20316 Continued**



**Exhibit B—Tentative Tract Map 20316 Continued**



**Exhibit B—Tentative Tract Map 20316 Continued**



PRELIMINARY  
 NOT FOR CONSTRUCTION

DATE	BY	REVISIONS



ENGINEER OF WORK  
 HERMAN O. SPYRALL, REG. NO. 10486  
 LAND SURVEYOR  
 11, 2020



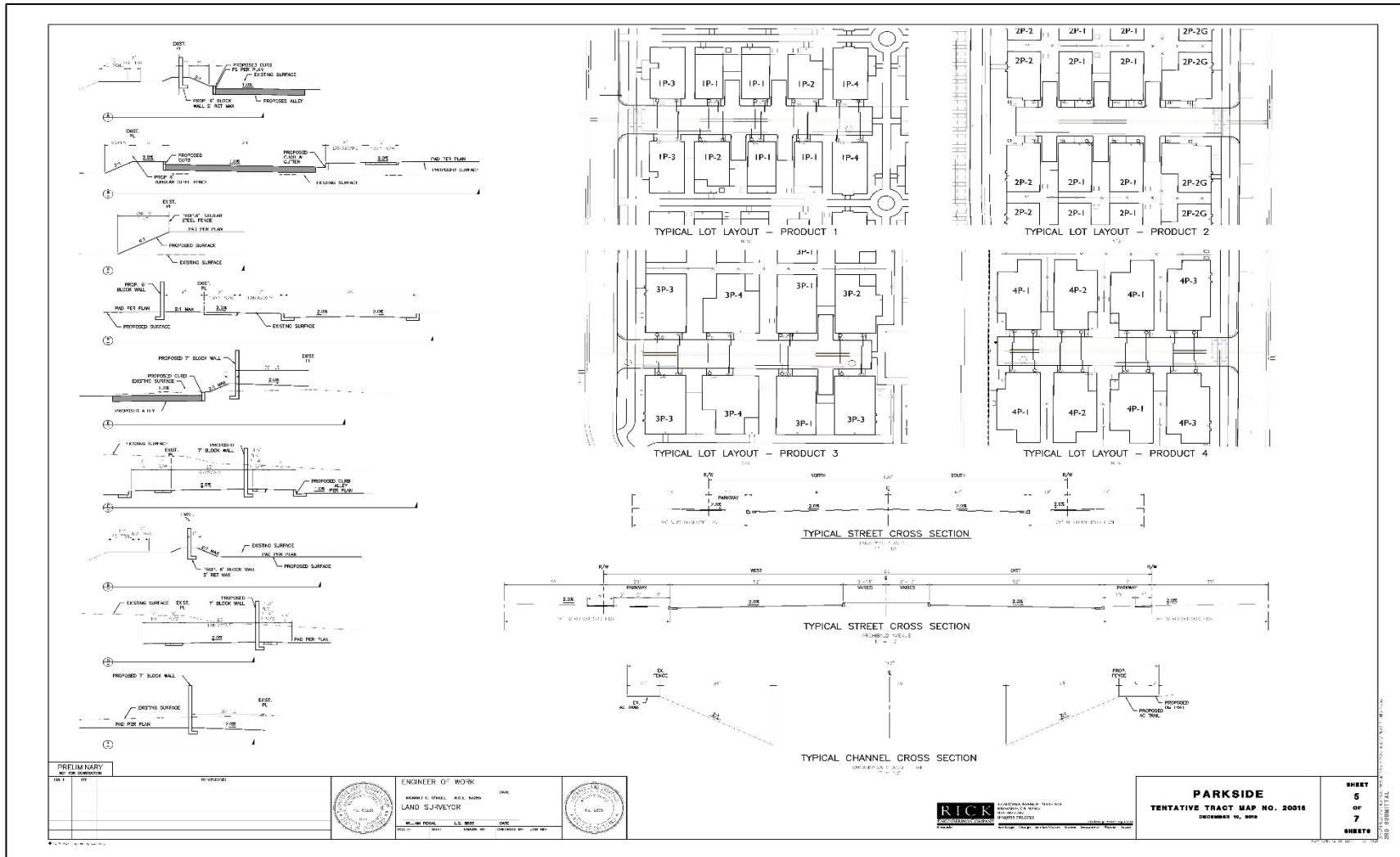
**RICK**  
 ADMINISTRATION  
 12000 W. 12TH AVENUE, SUITE 200  
 DENVER, CO 80202  
 PHONE: (303) 440-1100  
 FAX: (303) 440-1101  
 WWW.RICKADMINISTRATION.COM

**PARKSIDE**  
**TENTATIVE TRACT MAP NO. 20016**  
 DECEMBER 16, 2019

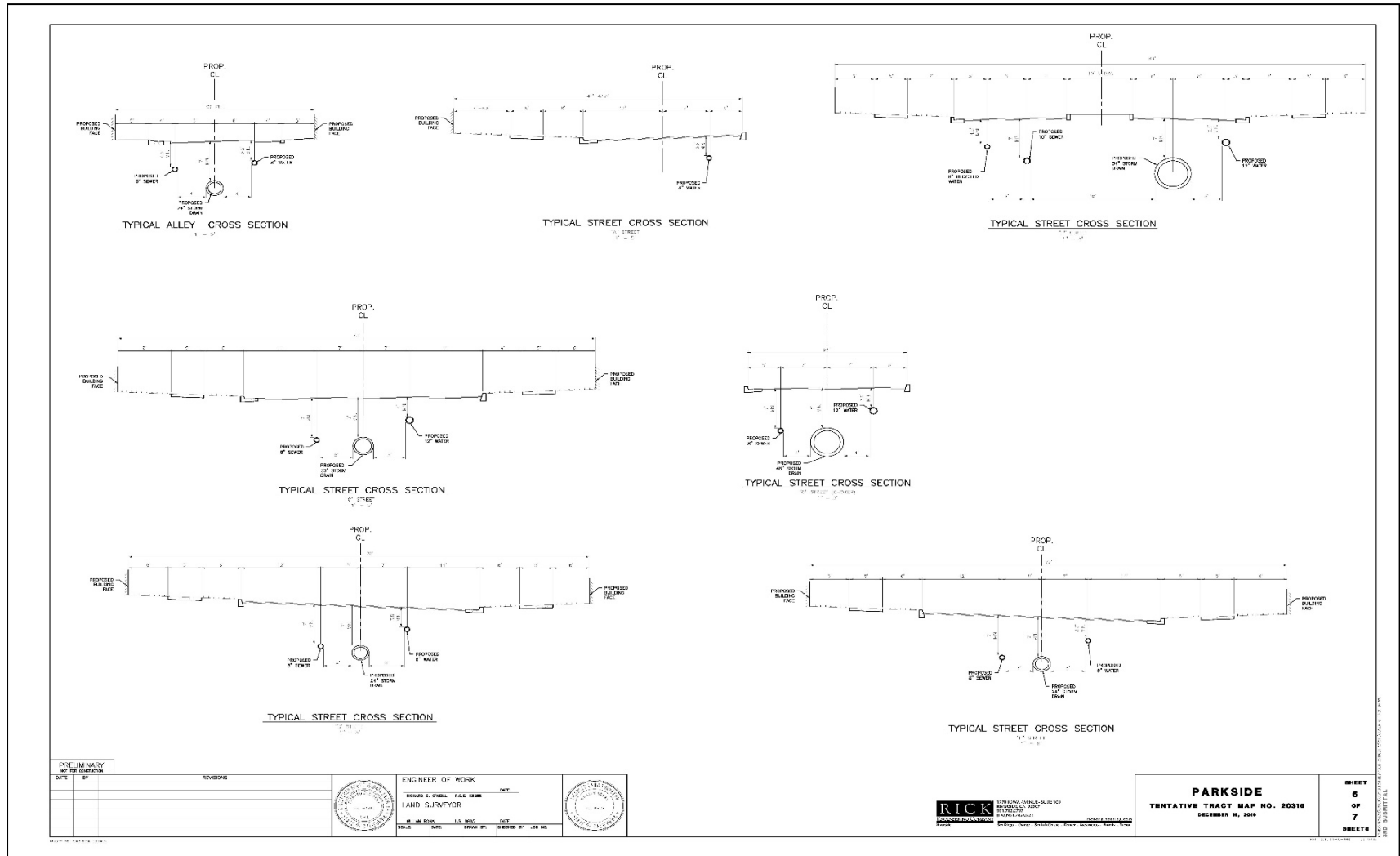
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**4**  
 OF  
**7**  
 SHEETS



**Exhibit B—Tentative Tract Map 20316 Continued**



**Exhibit B—Tentative Tract Map 20316 Continued**



DATE	BY	REVISION

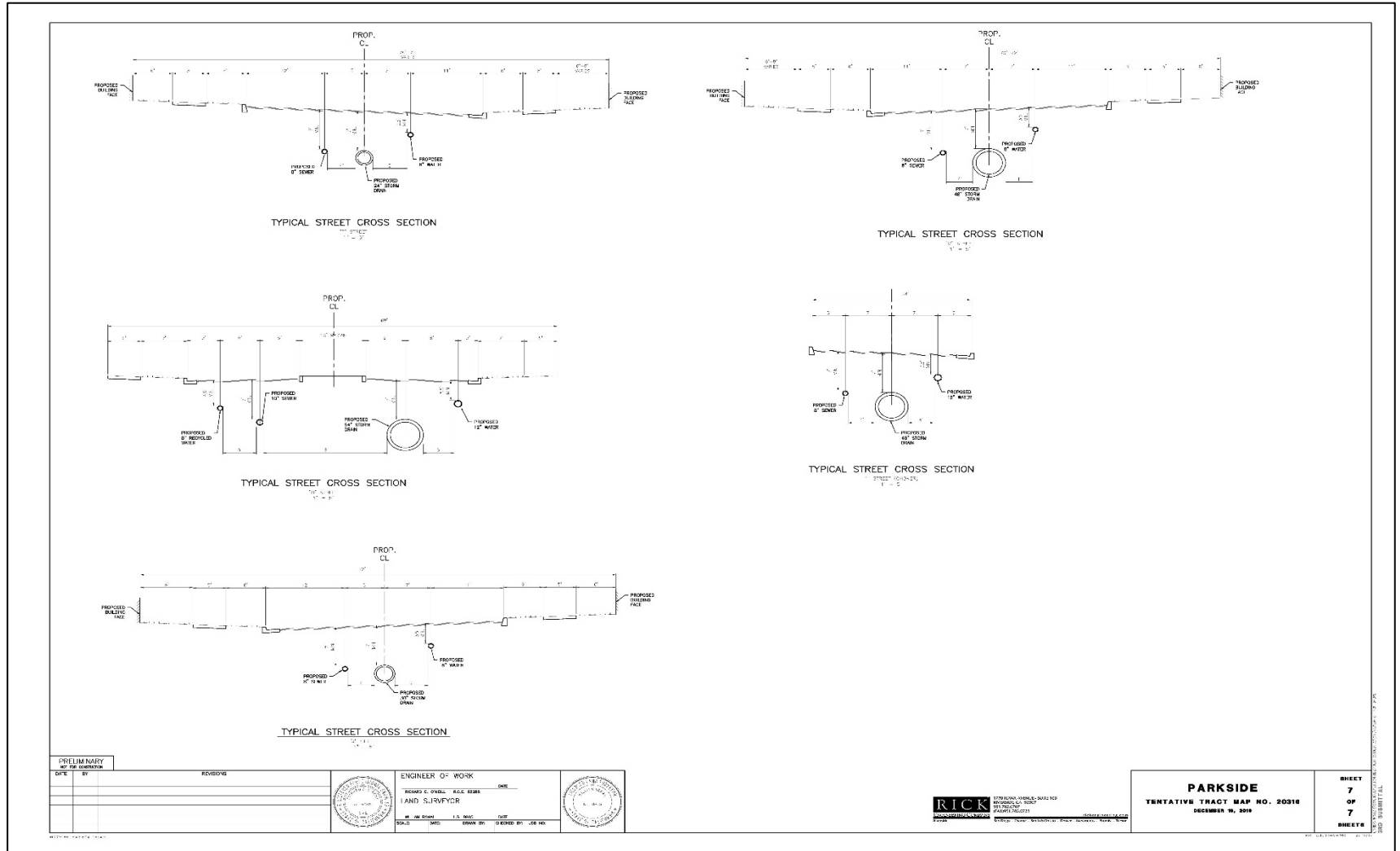
  

		<b>ENGINEER OF WORK</b> EDWARD C. ORRELL, P.E. 44528 JAND S.JRVFYCR	
<small>SCALE</small> <small>DATE</small>	<small>U.S. MAPS</small> <small>DATE</small>	<small>DATE</small> <small>JOB NO.</small>	<small>DATE</small> <small>JOB NO.</small>

**RICK**  
 17700 BROADWAY, SUITE 100  
 BAYVIEW, CA 94026  
 (415) 338-1111  
 WWW.RICKENGINEERS.COM

<b>PARKSIDE</b> TENTATIVE TRACT MAP NO. 20316 DECEMBER 19, 2019	SHEET <b>6</b> OF <b>7</b> SHEETS
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**Exhibit B—Tentative Tract Map 20316 Continued**



PRELIMINARY	
DATE	BY

REVISIONS



ENGINEER OF WORK  
 RICK J. ...  
 LAND SURVEYOR  
 ...  
 ...



**RICK**  
 ...  
 ...

**PARKSIDE**  
 TENTATIVE TRACT MAP NO. 20316  
 DECEMBER 19, 2019

SHEET  
 7  
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 7  
 SHEETS

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING A TENTATIVE TRACT MAP (FILE NO. PMTT19-020/TT20316) FOR CONDOMINIUM PURPOSES TO SUBDIVIDE 56.99 ACRES OF LAND INTO 4 NUMBERED RESIDENTIAL LOTS AND 12 LETTERED LOTS FOR COMMON AREAS, PORTIONS OF THE GREAT PARK, PRIVATE STREETS, PUBLIC UTILITY EASEMENTS AND NEIGHBORHOOD LANDSCAPE EDGES, LOCATED AT THE NORTHWEST CORNER OF ARCHIBALD AVENUE AND EUCALYPTUS AVENUE, WITHIN PLANNING AREAS 1 THRU 4 (CONVENTIONALLY LOADED LANE HOMES/DETACHED GREEN COURT CLUSTER HOMES), 17 AND 18 (TOWNHOME), 21 (GREAT PARK), 22 AND 23 (PRIVATE PARKS) OF THE PARKSIDE SPECIFIC PLAN, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 0218-231-12, 14,17-21, 28-30, 33, 35 AND 38.

WHEREAS, SC ONTARIO DEVELOPMENT COMPANY, LLC, (hereinafter referred to as "Applicant") has filed an Application for the approval of a Tentative Tract Map, File No. PMTT19-020, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 56.99 acres of land generally located, at the northwest corner of Archibald Avenue and Eucalyptus Avenue, Planning areas 1 thru 4 (Conventionally Loaded Lane Homes), 17 and 18 (Townhome), 21 (Great Park), 22 and 23 (Private Parks) of the Parkside Specific Plan, and is presently improved with an agriculture crop production use; and

WHEREAS, the property to the north of the Project site is within the Low-Density Residential zoning district of The Avenue Specific Plan and is developed with agriculture uses. The property to the east is within the High Density Residential and Great Park zoning districts of the Grand Park Specific Plan and is developed with agriculture uses. The property to the south is within the Conventional Small single-family residential and Neighborhood Commercial zoning districts of the Subarea 29 Specific Plan and is Mass graded. The property to the west is developed with Cucamonga Creek Flood Control Channel; and

WHEREAS, the Tentative Tract Map 20316 proposes to subdivide 56.99 acres of land into four (4) numbered residential lots for condominium purposes and twelve (12) lettered lot for common areas, portions of the Great Park, private streets, public utility easements and neighborhood landscape edges. The 4 residential lots will provide for (Lots 1-4) for detached single-family conventional lane loaded and detached green court condominium homes. The lots range in size from 2.40 to 14.59 acres, consistent with the

development regulations and standards of the Parkside Specific Plan. The twelve (12) lettered lots will accommodate the following:

- Lot A: 1.32-acre private recreational facility.
- Lots B, C and D: 16.84 acres of the Great Park.
- Lots G and H: 50-foot wide landscaped “Neighborhood Edge” along the west side of Archibald Avenue.
- Lots E and F: 35-foot wide Landscape “Neighborhood Edge” along the north side of Eucalyptus Avenue.
- Lots J, K and L: Private interior tract streets with public utility easements.
- Lot I: Provide an access road from Eucalyptus Avenue to the Cucamonga Creek Channel to the future regional trail; and

WHEREAS, the Tentative Tract Map will facilitate the construction of a 2.78-acre neighborhood park, sidewalks, parkways, and open space areas within the tract. TOP Policy PR1-1 requires new developments to provide a minimum of 2 acres of Private Park per 1,000 residents. The proposed project is required to provide 4.1 acres parkland to meet the minimum TOP private park requirement. To satisfy the park requirement, as part of the Development Agreement, the applicant will be required construct and transfer the portions of the Great Park identified the Parkside Specific Plan in four (4) phases to the City. In addition, the applicant (Owner) will enter a Development Impact Fee (DIF) credit and Reimbursement Agreement with City for the design and construction of the Great Park areas; and

WHEREAS, the Tentative Tract Map will implement infrastructure improvements to serve the site. These improvements include the backbone infrastructure to serve the site, and the internal street circulation for the residential neighborhoods (Private Streets A, B, C, E, F, G, H, I and J); and

WHEREAS, the project will have primary access from Eucalyptus Avenue, through signalized intersection. Secondary access will be provided from Parkside Drive, located on the northeast area of site, with access to Archibald Avenue; and

WHEREAS, as a Condition of Approval, staff will require that CC&R’s be prepared and recorded with the final map. The CC&R’s will outline the maintenance responsibilities for the open space areas, recreation amenities, drive aisles, utilities and upkeep of the entire site to ensure the on-going maintenance of the common areas and facilities; and

WHEREAS, the project will have primary access from Eucalyptus Avenue, through signalized intersection. Secondary access will be provided from Parkside Drive, located on the northeast area of site, with access to Archibald Avenue; and

WHEREAS, the environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an EIR Addendum has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 18, 2020, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision Nos. DAB20-021, respectively, recommending that the Planning Commission approve the Application; and

WHEREAS, on May 26, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

**SECTION 1: Environmental Determination and Findings.** As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the previous Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006 and supporting documentation. Based upon the facts and information contained in the previous Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) and supporting documentation, the Planning Commission finds as follows:

(1) The environmental impacts of this project were previously reviewed in conjunction with File No. Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006 and supporting documentation.

(2) The previous Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) was completed in compliance with CEQA and the Guidelines promulgated thereunder; and

(4) The previous Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) reflects the independent judgment of the Planning Commission; and

(5) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the previous Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008), and all mitigation measures previously adopted with the Addendum to the Parkside

Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008), are incorporated herein by this reference.

**SECTION 2: Subsequent or Supplemental Environmental Review Not Required.** Based on the information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) that will require major revisions to the Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) was prepared, that will require major revisions to the Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) was certified/adopted, that shows any of the following:

1. The project will have one or more significant effects not discussed in the Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008); or

2. Significant effects previously examined will be substantially more severe than shown in the Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008); or

3. Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or



4. Mitigation measures or alternatives considerably different from those analyzed in the Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

**SECTION 3: *Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project is consistent with the number of dwelling units (540) and density (12 to 18 DU/Acre) specified in the Available Land Inventory. Per the Available Land Inventory, the Parkside Specific Plan is required to provide 1,947 dwelling units with an overall density range of between 9 and 21 DU/AC.

**SECTION 4: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

**SECTION 5: *Concluding Facts and Reasons.*** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 and 2, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed Tentative Tract/Parcel Map is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable area and specific plans, and planned unit developments.*** The proposed Tentative Tract/Parcel Map is located within the Medium Density Residential (11.1 – 25 du/ac) land use district of the Policy Plan Land Use Map, and the Planning areas 1 thru 4 (Conventionally Loaded Lane Homes), 17 and 18 (Townhome), 21 (Great Park), and 22 and 23 (Private Parks) of the Parkside Specific Plan. The proposed subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will contribute to providing “a spectrum of housing types and price ranges that match the jobs in the City, and that make it possible for people to live and work in Ontario and maintain a quality of life” (Goal LU1). Furthermore, the project will promote the City’s policy to “incorporate a variety of land uses and building types that contribute to a complete community where residents at all stages of life, employers, workers, and visitors, have a wide spectrum of choices of where they can live, work, shop, and recreate within Ontario” (Policy LU1-6 *Complete Community*).

(2) ***The design or improvement of the proposed Tentative Tract/Parcel Map is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable specific plans and planned unit developments.*** The proposed Tentative Tract is located within the Medium Density Residential (11.1 – 25 du/ac) land use district of the Policy Plan Land Use Map, and the Planning areas 1 thru 4 (Conventionally Loaded Lane Homes), 17 and 18 (Townhome), 21 (Great Park), and 22 and 23 (Private Parks) of the Parkside Specific Plan. The proposed design or improvement of the subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will contribute to providing “[a] high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct” (Goal CD2). Furthermore, the project will promote the City’s policy to “create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety; and
- Variable setbacks and parcel sizes to accommodate a diversity of housing types; and
  - Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows; and
  - Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor

living room”), as appropriate; and

▪ Landscaped parkways, with sidewalks separated from the curb.” (Policy CD2-2 *Neighborhood Design*).

(3) ***The site is physically suitable for the type of development proposed.***

The project site meets the minimum lot area and dimensions of Planning areas 1 thru 4 (Conventionally Loaded Lane Homes), 17 and 18 (Townhome), 21 (Great Park), and 22 and 23 (Private Parks) of the Parkside Specific Plan, and is physically suitable for the type of residential and open space development proposed in terms of zoning, land use and development activity proposed, and existing and proposed site conditions.

(4) ***The site is physically suitable for the density/intensity of development proposed.*** The project site is proposed for residential and open space development at [a density of 13.5 DUs/acre (Planning Areas 1-4). The project site meets the minimum lot area and dimensions of the Planning areas 1 thru 4 (Conventionally-Loaded Lane Homes\ Detached Green Court Cluster Homes) and 17 and 18 (Townhome), of the Parkside Specific Plan, and is physically suitable for this proposed density / intensity of development.

(5) ***The design of the subdivision or the proposed improvements thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.*** The project site is not located in an area that has been identified as containing species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations or by the California Department of Fish and Wildlife or the U.S. Fish and Wildlife Service, nor does the site contain any riparian habitat or other sensitive natural community, and no wetland habitat is present on site; therefore, the design of the subdivision, or improvements proposed thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.

(6) ***The design of the subdivision, or the type of improvements thereon, are not likely to cause serious public health problems.*** The design of the proposed subdivision, and the infrastructure improvements existing or proposed on the project site, are not likely to cause serious public health problems, as The project is not anticipated to involve the transport, use, or disposal of hazardous materials during either construction or project implementation, include the use of hazardous materials or volatile fuels, nor are there any known stationary commercial or industrial land uses within close proximity to the subject site that use/store hazardous materials to the extent that they would pose a significant hazard to visitors or occupants to the project site.

(7) ***The design of the subdivision, or the type of improvements thereon, will not conflict with easements acquired by the public at large for access through, or use of property within, the proposed subdivision.*** The proposed subdivision has

provided for all necessary public easements and dedications for access through, or use of property within, the proposed subdivision. Furthermore, all such public easements and dedications have been designed pursuant to: (a) the requirements of the Policy Plan component of The Ontario Plan and applicable area plans; (b) applicable specific plans or planned unit developments; (c) applicable provisions of the City of Ontario Development Code; (d) applicable master plans and design guidelines of the City; and (e) applicable Standard Drawings of the City.

**SECTION 6: Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

**SECTION 7: Indemnification.** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

**SECTION 8: Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

**SECTION 9: Certification to Adoption.** The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26<sup>th</sup> day of May 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

---

Jim Willoughby  
Planning Commission Chairman

ATTEST:

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Cathy Wahlstrom  
Planning Director and  
Secretary to the Planning Commission

STATE OF CALIFORNIA                    )  
COUNTY OF SAN BERNARDINO        )  
CITY OF ONTARIO                        )

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. \_\_\_\_\_ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 26, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

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Gwen Berendsen  
Secretary Pro Tempore

**ATTACHMENT A:**

**File No. PMTT19-020 (TT20316)  
Departmental Conditions of Approval**

*(Departmental conditions of approval to follow this page)*



City of Ontario  
Planning Department  
303 East B Street  
Ontario, California 91764  
Phone: 909.395.2036  
Fax: 909.395.2420

## **Planning Department Land Development Division Conditions of Approval**

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**Meeting Date:** May 26, 2020  
**File No:** PMTT19-020 (TT20316)  
**Related Files:** PSPA19-007 and PDEV19-064

**Project Description:** A Tentative Tract Map (File No. PMTT19-20/TT20316) for condominium purposes to subdivide 56.99 acres of land into 4 numbered residential lots and 12 lettered lots for common areas, portions of the Great Park, private streets, public utility easements and neighborhood landscape edges, located at the northwest corner of Archibald Avenue and Eucalyptus Avenue, within Planning areas 1 thru 4 (Conventionally-Loaded Lane Homes) of the Parkside Specific Plan (APN(s): 0218-231-12, 14, 17-21, 28-30, 33, 35 and 38); **submitted by SC Ontario Development Company, LLC.**

**Prepared By:** Rudy Zeledon, Assistant Planning Director  
Phone: 909.395.2422 (direct)  
Email: rzeledon@ontarioca.gov

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The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

**1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

**2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

**2.1** The approval of Tentative Tract Map 20316 is contingent upon City Council approval the Amendment to the Parkside Specific Plan, File No. PSPA19-007.

**2.2** The approval of Tentative Tract Map 20316 is contingent upon City Council approval of the 4<sup>th</sup> Amendment the Development Agreement, File No. PDA05-002.

**2.3** Time Limits.

**(a)** Tentative Tract Map approval shall become null and void 2 years following the effective date of application approval or as specified in the related Development Agreement (File No. PDA05-002), unless the final parcel/tract map has been recorded, or a time extension has been approved by the Planning Commission pursuant to Development Code Section 2.02.025 (Time Limits and Extensions). This Permit does not supersede any individual time limits specified herein for performance of specific conditions or improvements.

**(b)** Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced,



and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

**2.4**     Subdivision Map.

**(a)**     The Final Tract Map shall be in conformance with the approved Tentative Tract Map on file with the City. Variations from the approved Tentative Tract Map may be reviewed and approved by the Planning Department. A substantial variation from the approved Tentative Tract Map may require review and approval by the Planning Commission, as determined by the Planning Director.

**(b)**     Tentative Tract Map approval shall be subject to all conditions, requirements and recommendations from all other departments/agencies provided on the attached reports/memorandums.

**(c)**     The subject Tentative Tract Map for condominium purposes shall require the recordation of a condominium plan concurrent with the recordation of the Final Tract Map and CC&Rs.

**(d)**     Pursuant to California Government Section 66474.9, the subdivider agrees that it will defend, indemnify, and hold harmless the City of Ontario or its agents, officers and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer of this subdivision, which action is brought within the time period provided for in Government Code Section 66499.37. The City of Ontario shall promptly notify the subdivider of any such claim, action or proceeding and the City of Ontario shall cooperate fully in the defense.

**2.5**     General Requirements. The Project shall comply with the following general requirements:

**(a)**     All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

**(b)**     The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

**(c)**     The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

**2.6**     Landscaping.

**(a)**     The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

**(b)**     Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

**(c)**     Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

**(d)**     Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be

resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

**2.7** Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions) and Parkside Specific Plan standards and design criteria.

**2.8** Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements.

(a) CC&Rs shall be prepared for the Project and shall be recorded prior to the issuance of a building permit.

(b) The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owners association and the CC&Rs shall be reviewed and approved by the City.

(c) CC&Rs shall ensure reciprocal parking and access between parcels.

(d) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:

(i) Landscaping and irrigation systems within common areas;

(ii) Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02;

(iii) Shared parking facilities and access drives; and

(iv) Utility and drainage easements.

(e) CC&Rs shall include authorization for the City's local law enforcement officers to enforce City and State traffic and penal codes within the project area.

(f) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.

(g) The CC&Rs shall include a parking (garage kept clear for vehicles) and solid waste enforcement plan.

(h) A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

**2.9** Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

(ii) Some of the property adjacent to this tract is zoned for agricultural uses and there could be fly, odor, or related problems due to the proximity of animals.

(iii) The area south of Riverside Drive lies within the San Bernardino County Agricultural Preserve. Dairies currently existing in that area are likely to remain for the foreseeable future.

(iv) This tract is part of a Landscape Maintenance District. The homeowner(s) will be assessed through their property taxes for the continuing maintenance of the district.

#### **2.10** Environmental Review.

(a) The environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval, and are incorporated herein by this reference. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

**2.11** Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

#### **2.12** Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee (\$50.00) shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

#### **2.13** Additional Requirements.

(a) The applicant shall contact the Ontario Post Office to determine the size and location of mailboxes for this project. In addition, the location and design of the mailboxes shall be reviewed and approved by the Planning Department.

(b) Street naming and address numbering shall be consistent with Article 32, Section 9-13280 of the Development Code and subject to Planning, Building and Fire Department review and approval.

(c) Prior to the issuance of grading permits, (Rough or Precise Grading). Mitigation Measures (MM), from the Parkside Specific Plan EIR, pertaining to Grading Activities must be met prior to issuance of grading permits.

(d) Dairy Separation Requirement for Residential Development:

The following separation requirements from existing dairies/feed lots shall apply to new residential development or structures used for public assembly purposes from existing dairies/feed lots. A minimum 100' separation shall be required between a new residential, commercial or industrial development or structure used for public assembly and an existing animal feed trough, corral/pen or an existing dairy/feed lot including manure stockpiles and related wastewater detention basins. The 100-foot separation requirement may be satisfied by an off-site easement acceptable to the Planning Director with adjacent properties, submitted with the initial final map and recorded prior to or concurrent with the final map.

## 2.14 Tribal Consultation

At the request of The Gabrieleno Band of Missions Indians – Kizh Nation, the applicant shall contact the tribal Chairman, Andrew Sales (<http://www.gabrielenoindians.org/>), prior to grading activities and shall be subject to following conditions:

(a) The project developer shall retain a Native American Monitor of Gabrieleño Ancestry to conduct a Native American Indian Sensitivity Training for construction personnel prior to commencement of any excavation activities. The training session shall include a handout and focus on how to identify Native American resources encountered during earthmoving activities and the procedures followed if resources are discovered, the duties of the Native American Monitor of Gabrieleño Ancestry and the general steps the Monitor would follow in conducting a salvage investigation.

(b) The project developer shall retain a Native American Monitor of Gabrieleño Ancestry to be on-site during all project-related, ground-disturbing construction activities (e.g., pavement removal, auguring, boring, grading, excavation, potholing, trenching, and grubbing) of previously undisturbed native soils to a maximum depth of 30 feet below ground surface. At their discretion, a Native American Monitor of Gabrieleño Ancestry can be present during the removal of dairy manure to native soil, but not at the developers' expense.

(c) A qualified archaeologist and a Native American Monitor of Gabrieleño Ancestry shall evaluate all archaeological resources unearthed by project construction activities. If the resources are Native American in origin, the Tribe shall coordinate with the developer regarding treatment and curation of these resources. Typically, the Tribe will request reburial or preservation for educational purposes. If archeological features are discovered, the archeologist shall report such findings to the Ontario Planning Director. If the archeological resources are found to be significant, the archeologist shall determine the appropriate actions, in cooperation with the City that shall be taken for exploration and/or salvage in compliance with CEQA Guidelines Section 15064.5(f).

(d) Prior to the start of ground disturbing activities, the developer shall arrange a designated site location within the footprint of the project for the respectful reburial of Tribal human remains and/or ceremonial objects. All human skeletal material discoveries shall be reported immediately to the County Coroner. The Native American Monitor shall immediately divert work a minimum of 50 feet from the discovery site and place an exclusion zone around the burial. The Native American Monitor shall notify the

construction manager who shall contact the San Bernardino County Coroner. All construction activity shall be diverted while the San Bernardino County Coroner determines if the remains are Native American. The discovery shall be confidential and secure to prevent further disturbance. If Native American, the San Bernardino County Coroner shall notify the Native American Heritage Commission (NAHC) as mandated by state law who will then appoint a Most Likely Descendent. In the case where discovered human remains cannot be documented and recovered on the same day, the remains shall be covered with muslin cloth and a steel plate that can be moved by heavy equipment placed over the excavation opening to protect the remains. If this type of steel plate is not available, a 24-hour guard shall be posted outside working hours. The Tribe shall make every effort to recommend diverting the project and keep the remains in situ and protected. If the project cannot be diverted, it may be determined that burials will be removed. If data recovery is approved by the Tribe, documentation shall be taken, which includes at a minimum detailed descriptive notes and sketches. Additional types of documentation shall be approved by the Tribe for data recovery purposes. Cremations will either be removed in bulk or means necessary to ensure complete recovery of all material. If the discovery of human remains includes four (4) or more burials, the location is considered a cemetery and a separate treatment plan shall be created. The project developer shall consult with the Tribe regarding avoidance of all cemetery sites. Once complete, a final report of all activities shall be submitted to the NAHC.

**(e)** No scientific study or the utilization of any invasive diagnostics shall be allowed to any Native American human remains.


**(f)** If the San Bernardino County Coroner determines the remains represent a historic non-Native American burial, the burial shall be treated in the same manner of respect with agreement of the San Bernardino County Coroner. Reburial will be in an appropriate setting. If the San Bernardino County Coroner determines the remains to be modern, the San Bernardino County Coroner shall take custody of the remains.

**(g)** Each occurrence of human remains and associated funerary objects shall be stored using opaque cloth bags. All human remains, funerary objects, sacred objects and objects of cultural patrimony shall be removed to a secure container on site if possible. These items shall be retained and reburied within six months of recovery. The site of reburial/repatriation shall be on the project site, but at a location agreed upon between the Tribe and the developer and protected in perpetuity. There shall be no publicity regarding any cultural materials recovered.

**CITY OF ONTARIO**  
**LANDSCAPE PLANNING DIVISION**  
 303 East "B" Street, Ontario, CA 91764

**DAB CONDITIONS OF APPROVAL**

**Sign Off**

  
 Jamie Richardson, Landscape Planner

4/20/2020  
 Date

Reviewer's Name:  
**Jamie Richardson, Landscape Planner**

Phone:  
**(909) 395-2615**

D.A.B. File No.:  
 PMTT19-020

Related Files:  
 PSPA19-007, PDEV19-064

Case Planner:  
 Rudy Zeledon

Project Name and Location:

Parkside  
 TM 20316

Applicant/Representative:

Damon Gascon – [damon.gascon@lewismc.com](mailto:damon.gascon@lewismc.com)  
 1156 N. Mountain Ave.  
 Upland, CA 91785

- |                                     |  |
|-------------------------------------|--|
| <input checked="" type="checkbox"/> | <b>A Tentative Tract Map (dated 3/30/2020) has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.</b> |
| <input type="checkbox"/>            | <b>A Tentative Tract Map (dated) has not been approved. Corrections noted below are required prior to DAB approval.</b>  |

**CORRECTIONS REQUIRED**

1. Storm water infiltration devices located in landscape areas shall be reviewed and approved by the Landscape Planning Division prior to installation.
2. Northwest (Lot H) and southwest (Lot G) corners of Archibald Ave and Parkside Drive and northwest corner (Lots G and F) of Archibald Ave and Eucalyptus Ave and northeast and northwest of Eucalyptus Ave and H Street (Lots F, J and E); verify dimension and grade for required monumentation (see Parkside Specific Plan for detail). Adjacent walls shall not interfere with required monumentation.

**On Grading or Utility Construction Plans:**

3. Storm water infiltration devices located in parkways or other landscape areas shall be routed to this department to be reviewed and approved prior to permit approval or installation.
4. Note for decorative paving for all motor courts including the lots facing the parking rows aisles.
5. Note for compaction to not be greater than 85% at landscape areas; all finished grades 1 ½" below finished surfaces; landscaped slopes to be max 3:1.
6. Show or note transformers shall be located in planter areas, and set back 3' from paving for small transformers less than 4' high and 5' setback for large transformer greater than 4' high. Locate on level grade. Coordinate with landscape plans.
7. Show or note backflow devices shall be located in planter areas, and set back min 3' from paving Locate on level grade. Coordinate with landscape plans.
8. Provide a utility clear space 8' wide in parkways 30' apart for street trees. Move water meters, drain lines, light standards to the minimum spacing to allow space for street trees.
9. Show light standards 15' away from required tree locations.
10. Wall footings shall not restrict landscape; max 12" in front of footing with of 12" of cover.

11. Show on plans step outs at parking spaces adjacent to planters; 12" wide monolithic curb, 12" compacted decomposed granite or pavers adjacent to the 6" curb.
12. Wall openings for drainage overflow shall be max 4" wide.
13. Provide a solid surface path from driveway to side yard gate for entry and trash bin access.

# CITY OF ONTARIO

## MEMORANDUM

**TO:** PLANNING DEPARTMENT, Rudy Zeledon  
**FROM:** BUILDING DEPARTMENT, Kevin Shear  
**DATE:** October 23, 2019  
**SUBJECT:** PMTT19-020

- 
- The plan **does** adequately address the departmental concerns at this time.  
 No comments  
 Report below.

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### Conditions of Approval

1. Standard Conditions of Approval apply.

KS:lr



# AIRPORT LAND USE COMPATIBILITY PLANNING

## CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV19-064, PMTT19-020, PSPA19-007

Address: Northwest corner of Archibald Avenue and Eucalyptus Avenue

APN: 0218-231-12, 14, 17-21, 28-30, 33, 35 & 38

Existing Land Use: Vacant

Proposed Land Use: Parkside SPA to reconfigure Planning Areas and revise Residential/landscape standards; Subdivision for 56.99 acres into 16 lots; DP to construct 540 SF units

Site Acreage: 56.99 Proposed Structure Height: N/A

ONT-IAC Project Review: n/a

Airport Influence Area: ONT

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Rudy Zeledon

Date: 5/7/2020

CD No.: 2019-087

PALU No.: n/a

### The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="radio"/> High Terrain Zone	<input type="radio"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input type="checkbox"/> FAA Notification Surfaces	<input type="radio"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input type="radio"/> 65 - 70 dB CNEL	<input type="checkbox"/> Airspace Obstruction Surfaces	<input checked="" type="checkbox"/> Real Estate Transaction Disclosure
<input type="radio"/> Zone 3	<input type="radio"/> 60 - 65 dB CNEL	<input type="radio"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: <u>200 FT +</u>	
<input type="radio"/> Zone 5			

### The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1   
  Zone 2   
  Zone 3   
  Zone 4   
  Zone 5   
  Zone 6

Allowable Height: 110- 200 FT

## CONSISTENCY DETERMINATION

This proposed Project is:  Exempt from the ALUCP     Consistent     Consistent with Conditions     Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Real Estate Disclosure Required.

Airport Planner Signature: \_\_\_\_\_

# AIRPORT LAND USE COMPATIBILITY PLANNING

## CONSISTENCY DETERMINATION REPORT

CD No.: 2019-087  
PALU No.: \_\_\_\_\_

### PROJECT CONDITIONS

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the Airport Land Use Compatibility Plan (ALUCP) for ONT. The applicant is required to meet the Real Estate Transaction Disclosure in accordance with California Codes (Business and Professions Code Section 11010-11024). New residential subdivisions within an Airport Influence Area are required to file an application for a Public Report consisting of a Notice of Intention (NOI) and a completed questionnaire with the Department of Real Estate and include the following language within the NOI:

**NOTICE OF AIRPORT IN VICINITY:** This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Rudy Zeledon, Assistant Planning Director  
Planning Department

**FROM:** Paul Ehrman, Deputy Fire Chief/Fire Marshal  
Fire Department

**DATE:** October 23, 2019

**SUBJECT:** PMTT19-020 – A Tentative Tract Map for Common Interest Subdivision purposes, to subdivide 53.03 acres of land into 4 numbered lots and 12 common lots, located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning Areas 1 thru 4 of the Parkside Specific Plan (APN(s): 0218-231-12, 14, 17-21, 28-30, 33, 35 &38). Related File(s): [insert File #s].

- 
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
- 

### **SITE AND BUILDING FEATURES:**

- A. 2013 CBC Type of Construction: Type V-B wood frame
- B. Type of Roof Materials: non-rated
- C. Ground Floor Area(s): Various
- D. Number of Stories: Two Story
- E. Total Square Footage: Various
- F. 2013 CBC Occupancy Classification(s): R-3, U

## **CONDITIONS OF APPROVAL:**

### **1.0 GENERAL**

- ☒ 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at [www.ontarioca.gov](http://www.ontarioca.gov), click on “Fire Department” and then on “Standards and Forms.”
- ☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

### **2.0 FIRE DEPARTMENT ACCESS**

- ☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- ☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- ☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- ☒ 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services.

### **3.0 WATER SUPPLY**

- ☒ 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 1500 gallons per minute (g.p.m.) for 2 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- ☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300’) apart, per Engineering Department specifications.
- ☒ 3.4 The public water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

### **4.0 FIRE PROTECTION SYSTEMS**

- ☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 13 D. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

## **5.0 BUILDING CONSTRUCTION FEATURES**

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Homes that do not front street shall be provided with an address entry sign at the street. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.
- ☒ 5.5 All residential chimneys shall be equipped with an approved spark arrester meeting the requirements of the California Building Code.



# CITY OF ONTARIO MEMORANDUM

TO: Scott Murphy, Development Director  
 Cathy Wahlstrom, Planning Director (Copy of memo only)  
 Diane Ayala, Advanced Planning Division (Copy of memo only)  
 Charity Hernandez, Economic Development  
 Kevin Shear, Building Official  
 Khoi Do, City Engineer  
 Jamie Richardson, Landscape Planning Division  
 Ahmed Aly, Municipal Utility Company  
 Emily Hernandez, Police Department  
 Paul Ehrman, Deputy Fire Chief/Fire Marshal  
 Jay Bautista, T. E., Traffic/Transportation Manager  
 Lorena Mejia, Airport Planning  
 Eric Woosley, Engineering/NPDES  
 Joe De Sousa, Code Enforcement (Copy of memo only)  
 Jimmy Chang, IT Department

FROM: Rudy Zeledon, Assistant Planning Director

DATE: October 22, 2019

SUBJECT: FILE #: PMTT19-020 Finance Acct#:

The following project has been submitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by .

- Note:
- Only DAB action is required
  - Both DAB and Planning Commission actions are required
  - Only Planning Commission action is required
  - DAB, Planning Commission and City Council actions are required
  - Only Zoning Administrator action is required

**PROJECT DESCRIPTION:** A Tentative Tract Map for Common Interest Subdivision purposes, to subdivide 53.03 acres of land into 4 numbered lots and 12 common lots, located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning Areas 1 thru 4 of the Parkside Specific Plan (APN(s): 0218-231-12, 14, 17-21, 28-30, 33, 35 & 38). Related File(s): [insert File #s].

- The plan does adequately address the departmental concerns at this time.
  - No comments
  - Report attached (1 copy and email 1 copy)
  - Standard Conditions of Approval apply
- The plan does not adequately address the departmental concerns.
  - The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

POLICE  
Department

E. HERNANDEZ  
Signature

POLICE OFFICER  
Title

11/13/19  
Date



**ENGINEERING DEPARTMENT  
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Information Technology & Management Services Department Conditions incorporated)

<input type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input checked="" type="checkbox"/> TRACT MAP <input checked="" type="checkbox"/> FOR CONDOMINIUM PURPOSES
<b>PROJECT FILE NO. TTM-20316</b>  <b>RELATED FILE NO(S). PMTT19-20</b>	
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: __/__/__	

**CITY PROJECT ENGINEER & PHONE NO:** Jesus Plasencia, 909-395-2128

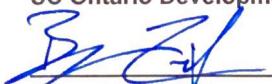
**CITY PROJECT PLANNER & PHONE NO:** Diane Ayala, 909-395-2428

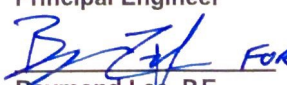
**DAB MEETING DATE:** May 18, 2020

**PROJECT NAME / DESCRIPTION:** TM-20316, a Tentative Tract Map to subdivide 53.03 acres of land into 4 numbered lots and 12 lettered lots for Condominium Purposes within the Parkside Specific Plan

**LOCATION:** Northwest corner of Archibald Avenue and Eucalyptus Avenue

**APPLICANT:** SC Ontario Development Company, LLC

**REVIEWED BY:**  5/13/20  
 Bryan Lirley, P.E.      Date  
 Principal Engineer

**APPROVED BY:**  FOR 5/13/20  
 Raymond Lee, P.E.      Date  
 Assistant City Engineer



**THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.**

<b>1. PRIOR TO FINAL MAP APPROVAL, APPLICANT SHALL:</b>	<b>Check When Complete</b>
---	----------------------------

- 1.01 Dedicate to the City of Ontario in fee simple, the right-of-way, described below:** 
  - Eucalyptus Avenue to the ultimate ½ right-of-way of 54 feet from CL north along tract frontage
  - Archibald Avenue to the ultimate ½ right-of-way of 85 feet from CL west along tract frontage.
  - Parkside Drive/East Grand Park Street to the ultimate full right-of-way of 60 feet along tract frontage.
  - Lots B-D for public parks at a later date per the development agreement.
  - Lots E-H for landscape Neighborhood Edge and public utility purposes.
  - Corner P/L radius (corner cut-offs) throughout the tract per City standards.
- 1.02 Dedicate to the City of Ontario, the following easement(s):** 
  - Public utility easement (PUE) on public park (Lots B & D) for sewer and storm drain purposes.
  - Blanket PUE on all private streets/alleys (Lots 1-4 and Lettered Lots J-L) for storm drain, potable water, recycled water, sewer, and fiber optic purposes.
  - Public access easement for Cucamonga Channel trail on Lot I.
- 1.03 Restrict vehicular access to the site as follows: Only approved access points per the approved Parkside specific plan.**
- 1.04 Vacate the following street(s) and/or easement(s): Existing right-of-way and easements as identified in the approved tentative tract map.**
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.**
- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, solid waste collection etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards. The Solid Waste Handling Plan shall be included in the CC&R's with a provision that the HOA will enforce can collection placement requirements of the Plan.**
- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).**





- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Management Services Department at (909) 395-2124 regarding this requirement.
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.
- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: [www.ci.ontario.ca.us](http://www.ci.ontario.ca.us)) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Management Services at (909) 395-2353 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments: 
  - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
  - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
  - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: 
  - Lettered Lots J-L shall be private streets and labeled accordingly in the final map.
  - Public and private improvements shall be maintained in accordance with the Maintenance Responsibility matrix in the Parkside Specific Plan.
  - Dedicate a public utility easement on Lot B to the Chino Basin Desalter Authority
  - The applicant/developer shall obtain all right of way necessary to construct the required public improvements identified in Section 2.

**2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:**

**A. GENERAL  
 ( Permits includes Grading, Building, Demolition and Encroachment )**

- 2.01 Record Tract Map No. 20316 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a duplicate photo mylar of the recorded map to the City Engineer's office.
- 2.03 Note that the subject parcels are recognized parcels in the City of Ontario per Tract Map 18048.
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of \_\_\_\_\_.



**2.05 Apply for a:**  Certificate of Compliance with a Record of Survey;  Lot Line Adjustment

**Make a Dedication of Easement.**

**2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, solid waste collection, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards. The Solid Waste Handling Plan shall be included in the CC&R's with a provision that the HOA will enforce can collection placement requirements of the Plan.**

**2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).**

**2.08 Submit a soils/geology report.**

**2.09 Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:**

- State of California Department of Transportation (Caltrans)
- San Bernardino County Road Department (SBCRD)
- San Bernardino County Flood Control District (SBCFCD) – See Condition No. 2.43**
- Federal Emergency Management Agency (FEMA)
- Cucamonga Valley Water District (CVWD) for sewer/water service
- United States Army Corps of Engineers (USACE) – See Condition No. 2.43**
- California Department of Fish & Game
- Inland Empire Utilities Agency (IEUA) – See Condition No. 2.23**
- Other: Chino Basin Desalter Authority – See Condition No. 1.14**

**2.10 Dedicate to the City of Ontario the right-of-way described below:**

\_\_\_\_\_ feet on \_\_\_\_\_

Property line corner 'cut-back' required at the intersection of \_\_\_\_\_ and \_\_\_\_\_.

**2.11 Dedicate to the City of Ontario the following easement(s):** \_\_\_\_\_

\_\_\_\_\_



- 2.12 New Model Colony (NMC) Developments:** 
  - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
  - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.
  - 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall.**
- 2.13 Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein on Archibald Avenue and Eucalyptus Avenue valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.
- 2.14 The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.**
- 2.15 Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.**
- 2.16 Other conditions: \_\_\_\_\_



**B. PUBLIC IMPROVEMENTS**

(See attached Exhibit 'A' for plan check submittal requirements.)

- 2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	EUCALYPTUS AVENUE	ARCHIBALD AVENUE	PARKSIDE DRIVE	ONTARIO UTILITY EASEMENTS
<b>Curb and Gutter</b>	<input checked="" type="checkbox"/> <b>New; 42 ft. from C/L</b> <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input checked="" type="checkbox"/> <b>Replace damaged</b> <input checked="" type="checkbox"/> <b>Remove and replace</b>	<input checked="" type="checkbox"/> <b>New; 18 ft. from C/L both sides</b> <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
<b>AC Pavement</b>	<input type="checkbox"/> Replacement <input checked="" type="checkbox"/> <b>Widen 40 ft from CL north and 26 feet from CL south along project frontage</b>	<input checked="" type="checkbox"/> <b>Replacement</b> <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input checked="" type="checkbox"/> <b>Widen 16 ft from CL both sides along project frontage</b>	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
<b>PCC Pavement (Truck Route Only)</b>	<input type="checkbox"/> New <input checked="" type="checkbox"/> <b>Modify existing</b>	<input type="checkbox"/> New <input checked="" type="checkbox"/> <b>Modify existing</b>	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Drive Approach</b>	<input checked="" type="checkbox"/> <b>New</b> <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> <b>New</b> <input checked="" type="checkbox"/> <b>Remove and replace</b>	<input checked="" type="checkbox"/> <b>New</b> <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Sidewalk</b>	<input checked="" type="checkbox"/> <b>New</b> <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> <b>New</b> <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> <b>New</b> <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>ADA Access Ramp</b>	<input checked="" type="checkbox"/> <b>New</b> <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> <b>New</b> <input checked="" type="checkbox"/> <b>Remove and replace</b>	<input checked="" type="checkbox"/> <b>New</b> <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Parkway</b>	<input checked="" type="checkbox"/> <b>Trees</b> <input checked="" type="checkbox"/> <b>Landscaping (w/irrigation)</b>	<input checked="" type="checkbox"/> <b>Trees</b> <input checked="" type="checkbox"/> <b>Landscaping (w/irrigation)</b>	<input checked="" type="checkbox"/> <b>Trees</b> <input checked="" type="checkbox"/> <b>Landscaping (w/irrigation)</b>	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
<b>Raised Landscaped Median</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> <b>Remove and replace</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Fire Hydrant</b>	<input checked="" type="checkbox"/> <b>New / Upgrade</b> <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input checked="" type="checkbox"/> <b>Relocation</b>	<input checked="" type="checkbox"/> <b>New / Upgrade</b> <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> <b>New / Upgrade</b> <input type="checkbox"/> Relocation



<b>Sewer</b> (see Sec. 2.C)	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral
<b>Water</b> (see Sec. 2.D)	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service
<b>Recycled Water</b> (see Sec. 2.E)	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service
<b>Traffic Signal System</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input checked="" type="checkbox"/> <b>Modify existing</b>	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Traffic Signing and Striping</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input checked="" type="checkbox"/> <b>Modify existing</b>	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Street Light</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> <b>New / Upgrade</b> <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> <b>New / Upgrade</b> <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> <b>New / Upgrade</b> <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
<b>Bus Stop Pad or Turn-out</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Storm Drain</b> (see Sec. 2G)	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral
<b>Fiber Optics</b> (see Sec. 2K)	<input checked="" type="checkbox"/> <b>Conduit / Appurtenances</b>	<input checked="" type="checkbox"/> <b>Conduit / Appurtenances</b>	<input checked="" type="checkbox"/> <b>Conduit / Appurtenances</b>	<input checked="" type="checkbox"/> <b>Conduit / Appurtenances</b>
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____	_____	_____
<b>Other Improvements</b>	<b>Construct Eucalyptus Bridge at Cucamonga Creek Channel</b>	_____	_____	_____

**Specific notes for improvements listed in item no. 2.17, above:**

- AC pavement replacement, PCC modification, and landscape median replacement may be required on Archibald Avenue to accommodate proposed utility connections.
- Location of public utility systems in private streets/alleys shall be approved during final design review.
- Construct all interior private lettered street improvements per the approved Parkside specific plan.
- The off-site public improvements associated with the construction of Fire Station No. 9,



including but not limited to, improvements on Parkside Drive/East Grand Park Street and the traffic signal at the Archibald Avenue intersection, shall be constructed pursuant to the terms, conditions, and obligations under the Purchase and Sale Agreement and the License and Reimbursement Agreement, between the City of Ontario and the applicant.

- If Fire Station No. 9 street improvements on Parkside Drive/East Grand Park Street are not complete, the applicant/developer will be required to construct interim street improvements at the northern tract boundary (e.g. temporary turnaround).

- 2.18 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s): \_\_\_\_\_
- 2.19 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.
- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide  water service  sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately \_\_\_\_\_, for undergrounding of utilities in accordance with Section 7-7.303.e of the City's Municipal Code.
- 2.22 Other conditions: \_\_\_\_\_

**C. SEWER**

- 2.23 **The IEUA Eastern Trunk Sewer main is available for connection by this project in Archibald Avenue.**
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 **Other conditions:** 
  1. **Submit a Final Sewer Sub-Area Master Plan (SSAMP) for review and approval. The Final SSAMP shall refine and optimize the sewer design to the meet Sewer Master Plan Design Criteria. The Final SSAMP shall also demonstrate that portions of TM-18048 north of the project site can sewer through the project site and still meet Sewer Master Plan Design Criteria.**
  2. **Construct the following sewer improvements:**
    - 12-inch sewer main in Eucalyptus Avenue between "H" Street and Archibald Avenue.
    - 10-inch sewer main in "H" Street from Eucalyptus Avenue to "C" Street
    - 8-inch sewer main in "C" Street from "H" Street to "G" Street
    - 8-inch sewer main in "G" Street from "C" Street to "A" Street
    - 8-inch sewer main from "A" Street to northern tract boundary through future Great Park lots.



**D. WATER**

- 2.27 A 24-inch water main is available for connection by this project in Archibald Avenue. (Ref: Water plan bar code: W13403-W13404)
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.
- 2.29 Other conditions: 
  - 1. The units within this map exceed the equivalent water use of 500 residential units. Pursuant to the requirements of SB221 (Subdivision Map/California Government Code §66473.7 and Water Code §10910), this subdivision is required to have a Written Verification of Water Supply (WV). Prior to the approval of the Final Map for recordation a WV shall be prepared for this Tract Map and the WV must make the findings that there is sufficient water supply for the Tract Map in order for the Final Map to record. The applicant shall make a deposit with the City in order to authorize the preparation of the required WV and pay for the full costs of the preparation of the WV.
  - 2. Construct the following potable water improvements (unless completed by others):
    - 24-inch main in Eucalyptus Avenue from just east of the Cucamonga Creek Channel to Archibald Avenue.
    - 12-inch main in “H” Street from Eucalyptus Avenue to “C” Street.
    - 12-inch main in “C” Street from “H” Street to “I” Street.
    - 12-inch main in “I” Street from “C” Street to “B” Street
    - 12-inch main in “B” Street from “I” Street to Parkside Drive
    - 12-inch main in Parkside Drive/ East Grand Park Street from “B” Street to norther tract boundary
    - 8-inch mains within in-tract private streets, drives, and alleys connecting to proposed 12” mains

**E. RECYCLED WATER**

- 2.30 A 16-inch recycled water main is available for connection by this project in Archibald Avenue. (Ref: Recycled Water plan bar code: P10147-10148)
- 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
- 2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.
- 2.33 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval. 

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.
- 2.34 Other conditions: 
  - 1. The development shall comply with City Ordinance 2689 and make use of recycled water for all approved uses, including but not limited to landscape irrigation for HOA maintained areas and parks.
  - 2. Construct the following recycled water improvements (unless completed by others):
    - 16-inch main in Eucalyptus Avenue, from just east of Cucamonga Creek Channel to Archibald Avenue.
    - 8-inch main in Parkside Drive/East Grand Park Street form Archibald to northern tract boundary.
    - 8-inch main in “H” Street from Eucalyptus Avenue to “C” Street.
    - 8-inch main in “B” Street from Parkside Drive to “I” Street.



**F. TRAFFIC / TRANSPORTATION**

- 2.35 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer: 
  - 1. On-site and off-site circulation
  - 2. Traffic level of service (LOS) at 'build-out' and future years
  - 3. Impact at specific intersections as selected by the City Engineer
  
- 2.36 **New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.**
  
- 2.37 **Other conditions:** 
  - 1. **The applicant/developer shall construct the following improvements as a part of the map:**
    - Archibald Avenue ultimate frontage improvements (last lane, curb & gutter) from Ontario Ranch Road to Parkside Drive.
    - Archibald Avenue ultimate frontage improvements (last lane, curb & gutter, sidewalk, landscaping and street lights) from Parkside Drive to Eucalyptus Avenue.
    - Eucalyptus Avenue ultimate half-street plus one lane in the eastbound direction (curb & gutter, sidewalk, landscaping and street lights). This shall include pavement transitions to accommodate the construction of the Eucalyptus Avenue Bridge.
    - Parkside Drive/Grand Park Street ultimate curb-to-curb street along project frontage.
    - New Traffic Signal at Eucalyptus Avenue and H Street (centerline of H Street must align with tract entry on southside of Eucalyptus Avenue)
    - Traffic Signal Modification at Archibald Avenue and Eucalyptus Avenue as necessary to accommodate tract frontage improvements.
  - 2. **The applicant/developer shall be responsible to design and construct the pavement and striping transitions from existing conditions to the widened portions along the Archibald Avenue and Eucalyptus Avenue project frontages. Existing interim signing/striping beyond project frontages will need to be adjusted to accommodate ultimate widening being provided by the project on Archibald Avenue and Eucalyptus Avenue. Provide separate exhibit showing limits of existing signing/striping being revised.**
  - 3. **The applicant/developer shall sign Parkside Drive/Grand Park Street, Eucalyptus Avenue and Archibald Avenue No Parking/No Stopping in accordance with the City's Traffic and Transportation Design Guidelines. Parking restrictions on Parkside Drive/Grand Park Street may be evaluated as necessary to accommodate parking for the Park.**
  - 4. **The intersection of Parkside Drive (south loop) and Archibald Avenue shall be limited to right-turn ingress/egress only.**
  - 5. **The applicant/developer shall be responsible to design and construct the Cucamonga Creek Channel Trail improvements along the tract boundary consistent with the Parkside Specific Plan in accordance with the Development Agreement.**
  - 6. **The applicant/developer shall be responsible to design and construct a bus pad on the north side of Eucalyptus Avenue west of Archibald Avenue to the satisfaction of the City Engineer and Omnitrans. The design shall be in accordance with Omnitrans' Bus Stop Design Guidelines.**
  - 7. **Design/Construct the Eucalyptus Avenue Bridge or exercise the options specified in the development agreement.**





**G. DRAINAGE / HYDROLOGY**

- 2.38 A \_\_\_\_\_ inch storm drain main is available to accept flows from this project in \_\_\_\_\_.  
 (Ref: Storm Drain plan bar code: \_\_\_\_\_)
- 2.39 **Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.**
- 2.40 An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100-year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.
- 2.41 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.
- 2.42 **Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.**
- 2.43 **Other conditions:**
  - 1. **Construct the following Storm Drain improvements (unless constructed by others):**
    - 60-inch storm drain on Eucalyptus Avenue from the Cucamonga Channel to approximately 860 feet east in accordance with the City Master Plan of Drainage (ECLP-XI-1). Obtain any necessary permits/approvals from the U.S. Army Corps of Engineers and the San Bernardino County Flood Control District for the proposed connection to the Cucamonga Channel.
    - 36-inch storm drain from Eucalyptus to in-tract private streets/alleys in accordance with the City Master Plan of Drainage (ECLP-XI-1).
    - 24 to 36-inch storm drain on Parkside Drive from Lot B to Archibald Avenue.
    - Storm drain for in-tract private streets/alleys and across Lots B & D. Storm drain located in alleys shall be owned and maintained by the HOA.

**H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)**

- 2.44 **401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels. If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant’s engineer shall be submitted.  
 Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.**
- 2.45 **Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.**
- 2.46 **Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch**



basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.

2.47 Other conditions: \_\_\_\_\_

#### J. SPECIAL DISTRICTS

2.48 File an application, together with an initial payment deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community facilities District Act of 1982. The application and fee shall be submitted a minimum four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact the Management Services Department at (909) 395-2353 to initiate the CFD application process.

2.49 Other conditions: \_\_\_\_\_

#### K. FIBER OPTIC

2.50 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole, generally located on Archibald Avenue. The City requires conduit infrastructure within a public utility easement on private streets/alleyways as may be necessary to provide service to all residential units.

2.51 Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Information Technology Department at (909) 395-2000, regarding this requirement.

#### L. Solid Waste

2.52 A final Solid Waste Handling Plan Sheet shall be submitted with the Precise Grading Plan submittal for review and approval. Any park with a recreation center, building, or amenities that generate refuse shall require solid waste collection service with a trash enclosure and bins, unless otherwise approved by the City.

2.53 Other conditions:   
1. The applicant/developer shall submit a Final Integrated Waste Management Report (IWMMR) for review and approval with the Precise Grading Plan. This report shall address the management of all integrated waste (Refuse, Recycling, Organics, etc.). The IWMMR shall demonstrate compliance with the "Integrated Waste Management Report Requirements" document.  
2. The Developer shall provide the buyers of all residential units an informational disclosure with map exhibit showing the designated can placement locations for collections for their lot, based upon the designated collections locations on the Final Solid Waste Handling Plan. This informational disclosure with map exhibit shall be submitted with Precise Grading Plan for review and approval of Ontario Municipal Utility Company.



**3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:**

- 3.01 Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.
- 3.02 Complete all requirements for recycled water usage. 
  - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
  - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
  - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.
- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 Confirm payment of all Development Impact Fees (DIF) to the Building Department.
- 3.06 Submit electronic copies (PDF and Auto CAD format) of all approved improvement plan studies and reports (i.e. hydrology, traffic, WQMP, etc.).

**4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:**

- 4.01 Complete all Conditions of Approval listed under Sections 1-3 above.
- 4.02 Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.
- 4.03 The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.



**EXHIBIT 'A'**  
**ENGINEERING DEPARTMENT**  
**First Plan Check Submittal Checklist**

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**Project Number: Tract Map No. 20316**

**The following items are required to be included with the first plan check submittal:**

1.  **A copy of this check list**
2.  **Payment of fee for Plan Checking**
3.  **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4.  **One (1) copy of project Conditions of Approval**
5.  **Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).**
6.  **Three (3) sets of Public Street improvement plan with street cross-sections**
7.  **Three (3) sets of Private Street improvement plan with street cross-sections**
8.  **Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
9.  **Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)**
10.  **Four (4) sets of Public Sewer improvement plan**
11.  **Five (5) sets of Public Storm Drain improvement plan**
12.  **Three (3) sets of Public Street Light improvement plan**
13.  **Three (3) sets of Signing and Striping improvement plan**
14.  **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15.  **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
16.  **Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.**
17.  **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
18.  **One (1) copy of Hydrology/Drainage study**
19.  **One (1) copy of Soils/Geology report**
20.  **Payment for Final Map/Parcel Map processing fee**
21.  **Three (3) copies of Final Map/Parcel Map**



22.  **One (1) copy of approved Tentative Map**
23.  **One (1) copy of Preliminary Title Report (current within 30 days)**
24.  **One (1) copy of Traverse Closure Calculations**
25.  **One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.**
26.  **Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use**
27.  **Other: As part of the precise grading plan submittal, provide a Final Utilities Systems Map (USM) that shows all existing and proposed Utilities (Potable Water, Recycled Water, Sewer, Storm Drain, and other utilities) including each of the City's public utilities' points of connection to the existing systems. A Copy of the Final USM shall be submitted with all public improvement plan submittals.**



# PLANNING COMMISSION STAFF REPORT

May 26, 2020

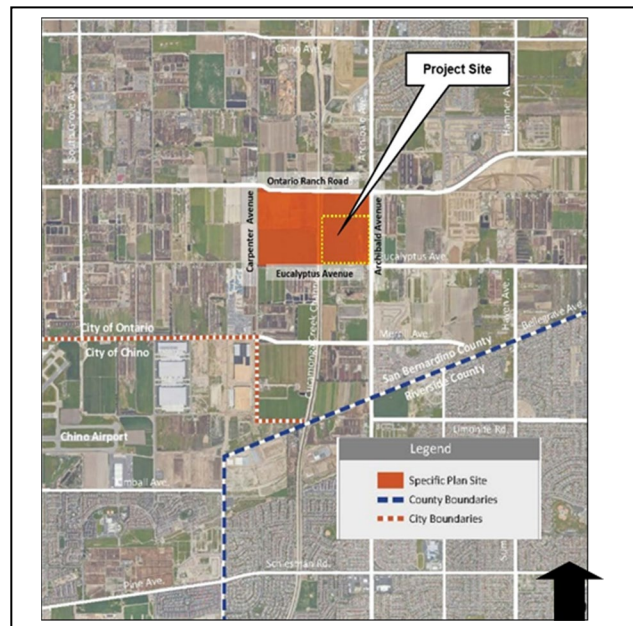
**FILE NO.:** PDEV19-064

**SUBJECT:** A Development Plan to construct 540 single-family detached homes and a private recreational center within a gated community on 34.30 acres of land located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning Areas 1 through 4 and Planning Area 23 of the Parkside Specific Plan. (APNs: 0218-231-35, 0218-231-33, 0218-231-30, 0218-231-29, 0218-231-28, 0218-231-22, 0218-231-21, 0218-231-20, 0218-231-19, 0218-231-18, 0218-231-17, 0218-231-14, and 0218-231-12) **submitted by The New Home Company Southern California, LLC.**

**PROPERTY OWNER:** The New Home Company Southern California, LLC.

**RECOMMENDED ACTION:** That the Planning Commission consider and approve File No. PDEV19-064, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval contained in the attached departmental reports.

**PROJECT SETTING:** The Project site is comprised of 34.30 acres of land located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning Areas 1 through 4 (Detached Green Court Cluster Homes and Conventionally-Loaded Lane Homes), and Planning Area 23 (Private Open Space) of the Parkside Specific Plan, as depicted in Figure 1: Project Location. The properties to the north and west of the Project site are within Planning Area 21 (Open Space Parkland/Great Park), Planning Area 26 (CDA Well Site/ Public Facility), Planning Area 5 (2 and 3-Story Townhomes/MDR), and Planning Area 6 (Motorcourt/MDR) of the Parkside Specific Plan, and include vacant land and the Cucamonga Creek Flood Control Channel (west). The properties to the



**Figure 1: Project Location**

Case Planner:	Diane Ayala
Planning Director Approval:	
Submittal Date:	11/18/2019

Hearing Body	Date	Decision	Action
DAB	5/18/20	Approval	Recommend
PC	5/26/20		Final
CC			

east are within Planning Area 8 (High Density Residential and Great Park) of the Grand Park Specific Plan and are currently vacant. The property to the south of the project site is within Planning Area 2 (Neighborhood Center) and Planning Area 1 (Conventional Small Lot) of the Subarea 29 Specific Plan and is currently vacant. The surrounding existing land uses, Policy Plan (General Plan), and zoning information are tabulated in the Technical Appendix of this report.

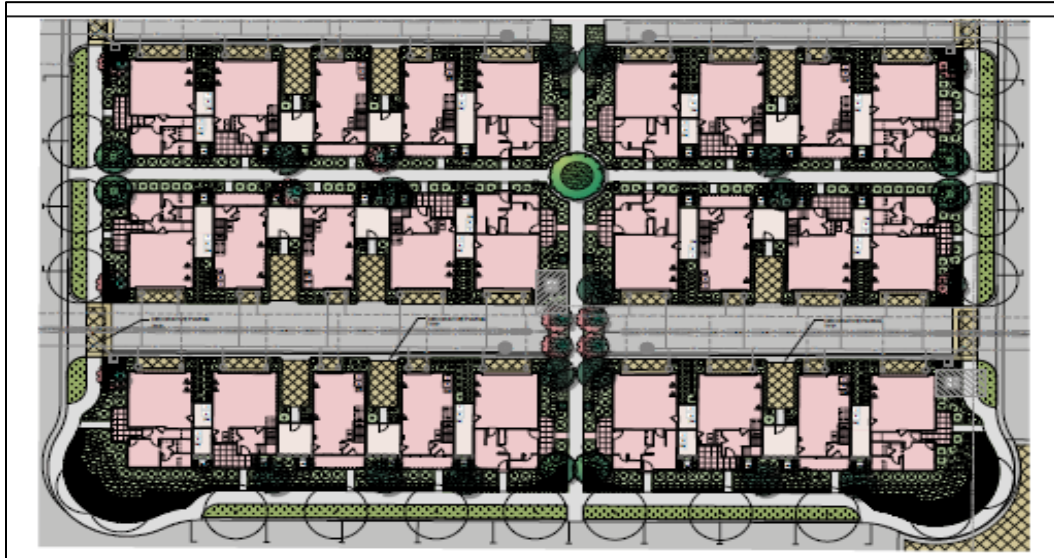
## **PROJECT ANALYSIS:**

[1] Background —The Applicant is requesting the approval of a Development Plan to construct 540 detached single-family homes (170 8-Plex Detached Green Court Cluster Homes and 370 Conventionally-Loaded Lane Homes) and a 2,800 square foot recreational center within a gated community on 34.30 acres of land located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning Areas 1 through 4 (Medium Density Residential) and Planning Area 23 (Private Open Space) of the Parkside Specific Plan.

The Parkside Specific Plan and Environmental Impact Report (EIR) were approved by the City Council on September 5, 2006. The Parkside Specific Plan established the land use designations, development standards, and design guidelines for 250 acres of land, which includes the potential development of 1,947 dwelling units (Exhibit B: The Parkside Specific Plan Land Use Map - Proposed Amendment, attached). Related to the Development Plan request, are the following discretionary actions to be considered concurrently by the City:

- An Amendment to the Parkside Specific Plan (File No. PSPA19-007) to: [1] reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] update and revise Landscape Standards (Section 7.7).
- A Tentative Tract Map 20316 (File No. PMTT19-020) for condominium purposes, subdividing 56.99 acres of land into 4 numbered residential lots and 12 lettered lots for common areas, portions of the Great Park, private streets, public utility easements, and neighborhood landscape edges. The Project is located at the northwest corner of Archibald Avenue and Eucalyptus Avenue, within Planning areas 1 through 4 (Detached Green Court Cluster Homes and Conventionally-Loaded Lane Homes) of the Parkside Specific Plan.
- A Fourth Amendment to the Development Agreement (File No. PDA05-002) to modify the commencement of certain specific infrastructure associated with the development of Tentative Tract Map 20316 (File No. PMTT19-020), and

conform to the revised development standards established by the Parkside Specific Plan Amendment (File No. PSPA19-007), located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning Areas 1 through 4 of the Parkside Specific Plan.



**Figure 3A: Conceptual Detached Green Court Cluster Homes (8-Plex)**

[2] Site Design/Building Layout — The Project proposes the development of 540 detached single-family homes (170 8-Plex Detached Green Court Cluster Homes and 370 Conventionally-Loaded Lane Homes) and a 2,800 square foot recreational center within a gated community (see Exhibit B—Site Plan, attached).

Product 1, the Detached Green Court Cluster, allows for a cluster of 8 single-family homes, with smaller and more efficient home footprints. The single-family homes are clustered around a green paseo, with primary entry access from a paseo or street for corner lots. Parking for residents are provided in garages or uncovered parking spaces. The products feature private side yards and patio areas. All driveways, including on-site parking and private lane (alley) entrances, will be treated with decorative paving. Landscaped paseos run horizontal and vertical, connecting pedestrian access throughout the site.

Product 1 is comprised of 170 three-story dwelling units (DUs) and includes 4 floor plans with three architectural styles per plan. The 4 floor plans are characterized as follows:

- Plan 1 (50 DUs): 1,090 square feet, 2 bedrooms, 2.5 baths, patio, deck, and one-car garage.
- Plan 2 (40 DUs): 1,458 square feet, 3 bedrooms, 2.5 baths, patio, deck, and 2-car garage.

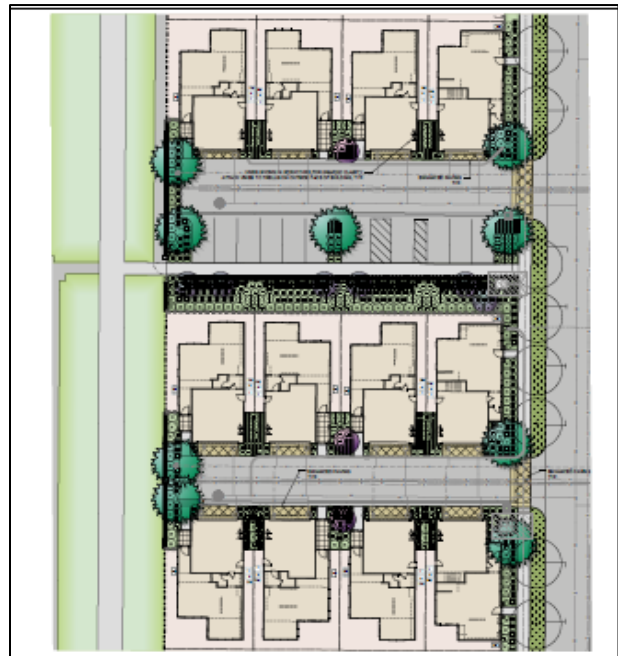


- Plan 3 (40 DUs): 1,688 square feet, 4 bedrooms, 3.5 baths, patio, deck, and 2-car garage.
- Plan 4 (40 DUs): 1,783 square feet, 4 bedrooms, 3.5 baths, patio, deck, and 2-car garage.

Products 2 through 4 consist of conventionally-loaded lane, small lot single-family homes, which are characterized by a private lane (Alley) that provides garage access (a second driveway to access a one-car parking stall is provided on select units) and primary entry access into each unit, with the exception of street facing units that have primary entry access directly from a street. The products also feature private yards and patio areas.

Product 2 (Conventionally-Loaded Lane Homes) will provide 136 two-story DUs and includes 3 floor plans with three architectural styles per plan. The 3 floor plans are characterized as follows:

- Plan 1 (68 DUs): 1,020 square feet, 2 bedrooms, 2.5 baths, and one-car garage.
- Plan 2 (31 DUs): 1,046 square feet, 2 bedrooms, 2.5 baths, loft, and one-car garage.
- Plan 2G (37 DUs): 1,046 square feet, 2 bedrooms, 2.5 baths, loft, and 2-car garage.



**Figure 3A: Conceptual Conventionally-Loaded Lane Homes**

Product 3 (Conventionally-Loaded Lane Homes) consists of 111 two-story DUs and includes 4 floor plans with three architectural styles per plan. The 4 floor plans are characterized as follows:

- Plan 1 (40 DUs): 1,351 square feet, 3 bedrooms, 2.5 baths, and 2-car tandem garage (option 4 bedroom/3 bathroom in lieu of tandem garage).
- Plan 2 (18 DUs): 1,404 square feet, 3 bedrooms, 2.5 baths, and 2-car garage.
- Plan 3 (39 DUs): 1,429 square feet, 3 bedrooms, 2.5 baths, and 2-car garage.
- Plan 4 (14 DUs): 1,550 square feet, 3 bedrooms, 2.5 baths, and 2-car garage.

Product 4 (Conventionally-Loaded Lane Homes) consists of 123 two-story DUs and includes 3 floor plans with three architectural styles per plan. The 3 floor plans are characterized as follows:

- Plan 1 (54 DUs): 1,595 square feet, 3 bedrooms, 2.5 baths, loft, porch, and 2-car garage.
- Plan 2 (35 DUs): 1,647 square feet, 4 bedrooms, 2.5 baths, loft, porch, and 2-car garage.
- Plan 3 (34 DUs): 1,790 square feet, 4 bedrooms, 2.5 baths, loft, and 2-car garage.

[3] Site Access/Circulation — The Project street frontage improvements along Eucalyptus Avenue, Archibald Avenue, Parkside Drive, and the Cucamonga Creek Channel will be constructed as a condition of Tentative Tract Map 20316 (File No. PMTT19-020), once approved. The Project will have a major community gateway with access from Eucalyptus Avenue and a secondary community gateway with access from Parkside Drive (see Exhibit D – Conceptual Entry Gate Renderings, attached). Additional pedestrian access gates will be located on Eucalyptus Avenue, Archibald Avenue, and on the north end of the property connecting the neighborhood to the Great Park. Vehicular circulation through the Project is provided by a series of 26-foot wide main loop streets, which connect to 24-foot wide private lanes. The Project will also have a network of landscaped paseos and walkways providing east-west and north-south pedestrian connectivity throughout the site.

[4] Parking — As demonstrated in the Parking Summary Table below, the Project requires a total 1,080 parking spaces and 1,320 parking spaces have been provided. All proposed single-family homes provide 2 parking spaces, with at least one space enclosed, which is consistent with the requirements of the Parkside Specific Plan. Two-car garages will be constructed for 391 units and the remaining 141 units will have a one-car garage and a second driveway to access a one-car parking space. In addition, 240 on-street guest parking spaces are distributed throughout the neighborhood community.

<b>Parking Summary Table</b>					
<b>Product</b>	<b>Number of Units</b>	<b>Required 2 Parking Spaces</b>	<b>On-Street Parking</b>	<b>Total Provided</b>	<b>+/- Parking</b>
SF Lane Loaded\Cluster	540	1,080	240	1,320	+240
				2.4 spaces per unit	

[5] Architecture — The Parkside Specific Plan allows for a variety of traditional American styles found throughout California and Ontario. These styles include American Traditional, Craftsman, and Mediterranean. The Project proposes 3 architectural styles from Parkside Specific Plan’s Millennium Collection. This collection represents subtle differences in the architectural vocabulary using creative interpretation of traditional styles utilizing embellished statements and details to express an iconic style in a new vocabulary. Elevations can be abstracted architectural expressions of a recognizable style. These elevations are encouraged to push the artistic envelope of design to

incorporate new, modern or alternative forms, details, and materials in the modern context of architecture. Styles within this series include the following (see Exhibit C – Floor Plans and Elevations, attached):

- Adaptive Farmhouse: This style blends the sleek clean lines of contemporary design with warm farmhouse charm, to create a uniquely fresh take on this country living inspired style. Elements include hipped and gabled roofs, board and batten siding and stucco finishes, metal awnings, raised panel garage doors, and horizontal wood railing on balconies.
- Adaptive Mediterranean: Simplified details borrowed from traditional Mediterranean styles include streamline forms which move toward a modern and unpretentious aesthetic. Elements include hipped and gable roofs, a combination of smooth and sand stucco finishes, recessed multiple paned windows, arched porch entries, raked eaves, and decorative foam frieze board under eave.
- Contemporary: Clean lines and ordered appearance best describes this particular style. Elements include hipped, gable, and shed roofs, tight eaves, stucco and Cementous siding, metal awnings over entries, horizontal siding, and vertical banding of windows.

[6] Landscaping/Open Space — Contingent upon approval of the Tentative Tract Map 20316 (File No. PMTT19-020), the Project will facilitate the construction of a centrally located recreation center, which includes a 2,800 square foot clubhouse, swimming pool and spa, barbeque area, dog park, yoga lawn, a larger event lawn on 1.32-acres of land, paseos, and parkways within the neighborhood community (see Exhibit E – Clubhouse/Recreation Center Renderings, attached). TOP Policy PR1-1 requires new developments to provide a minimum of 2 acres of Private Park per 1,000 residents. The proposed Project is required to provide 4.1 acres parkland to meet the minimum TOP private park requirement. To satisfy the park requirement, as part of the Development Agreement, the applicant will be required to construct and transfer the portions of the Great Park identified the Parkside Specific Plan in four phases, to the City. In addition, the applicant (Owner) will enter a Development Impact Fee (DIF) credit and Reimbursement Agreement with City for the design and construction of the Great Park areas.

**COMPLIANCE WITH THE ONTARIO PLAN:** The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals.

- Operate in a Businesslike Manner

- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)
- Ensure the Development of a Well Planned, Balanced, and Self-Sustaining Community in the New Model Colony

[2] Vision.

**Distinctive Development:**

- Commercial and Residential Development
  - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

[3] Governance.

**Decision Making:**

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
  - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

[4] Policy Plan (General Plan)

**Land Use Element:**

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
  - LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.
  - LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).
- Goal LU2: Compatibility between a wide range of uses.

➤ LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

### **Housing Element:**

▪ Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

➤ H2-4 New Model Colony. We support a premier lifestyle community in the New Model Colony distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.

➤ H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.

▪ Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.

### **Community Economics Element:**

▪ Goal CE1: A complete community that provides for all incomes and stages of life.

➤ CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

▪ Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

➤ CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

➤ CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

➤ CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

➤ CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

**Safety Element:**

▪ Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.

➤ S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

**Community Design Element:**

▪ Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➤ CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

➤ CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

▪ Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

➤ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➤ CD2-2 Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb.

➤ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➤ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

**HOUSING ELEMENT COMPLIANCE:** The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The Project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed Project is consistent with the maximum number of dwelling units (540) and density (18 DUs/AC) specified within the Parkside Specific Plan. Per the Available Land Inventory, the Parkside Specific Plan is required to provide 1,947 dwelling units with an overall density range of 9 to 21 DUs/AC.

**AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE:** The project site is located within the Airport Influence Area of the Ontario International Airport and has been found to be consistent with the policies and criteria set forth within the Ontario International Airport Land Use Compatibility Plan.

Planning Commission Staff Report

File No.: PDEV19-064

May 26, 2020

**ENVIRONMENTAL REVIEW:** The environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures shall be a condition of project approval and are incorporated herein by reference.

**CONDITIONS OF APPROVAL:** See attached department reports.



**TECHNICAL APPENDIX:**

**Surrounding Zoning and Land Use:**

	<i>Existing Land Use</i>	<i>General Plan Designation</i>	<i>Zoning Designation</i>	<i>Specific Plan Land Use</i>
<i>Site</i>	Vacant Prev. Ag/Dairy Uses	Medium Density Residential	Parkside Specific Plan	PA 1 (8-10 Plex Detached Green Court Cluster Homes), PAs 2-4 (Conventionally-Loaded Lane Homes), and PA 23 Private Open Space
<i>North</i>	Vacant Prev. Ag/Dairy Uses	Open Space-Recreational	Parkside Specific Plan	PA 21 (Open Space Parkland/Great Park) and PA 26 (CDA Well Site/ Public Facility)
<i>South</i>	Vacant Prev. Ag/Dairy Uses	Low Density Residential and Neighborhood Commercial	Subarea 29 Specific Plan	PA 2 (Neighborhood Center) and PA 1 (Conventional Small Lot)
<i>East</i>	Vacant Prev. Ag/Dairy Uses	Open Space-Recreational	Grand Park Specific Plan	PA 8 High Density Residential and Great Park
<i>West</i>	Cucamonga Creek Channel	Medium Density Residential	Parkside Specific Plan	PA 5 (2 and 3-Story Townhomes/MDR) PA 6 (Motorcourt/MDR)

**General Site & Building Statistics – Detached Green Court Cluster Homes (Product 1):**

<i>Item</i>	<i>Required Min./Max.</i>	<i>Provided (Ranges)</i>	<i>Meets Y/N</i>
<i>Building Separation:</i>			Y
<i>Front to front living space:</i>	13'	13'	Y
<i>Side to side living space:</i>	6'	6'-11'	Y
<i>Garage to garage face:</i>	30'	30'	Y
<i>Garage face to drive apron:</i>	3'	3'	
<i>Maximum height:</i>	42'	36'11" - 40'	Y

**General Site & Building Statistics – Conventionally-Loaded Lane Homes (Products 2 through 4):**

<i>Item</i>	<i>Required Min./Max.</i>	<i>Provided (Ranges)</i>	<i>Meets Y/N</i>
<i>Building Separation</i>			
<i>Front to side living space:</i>	8'	8'	Y

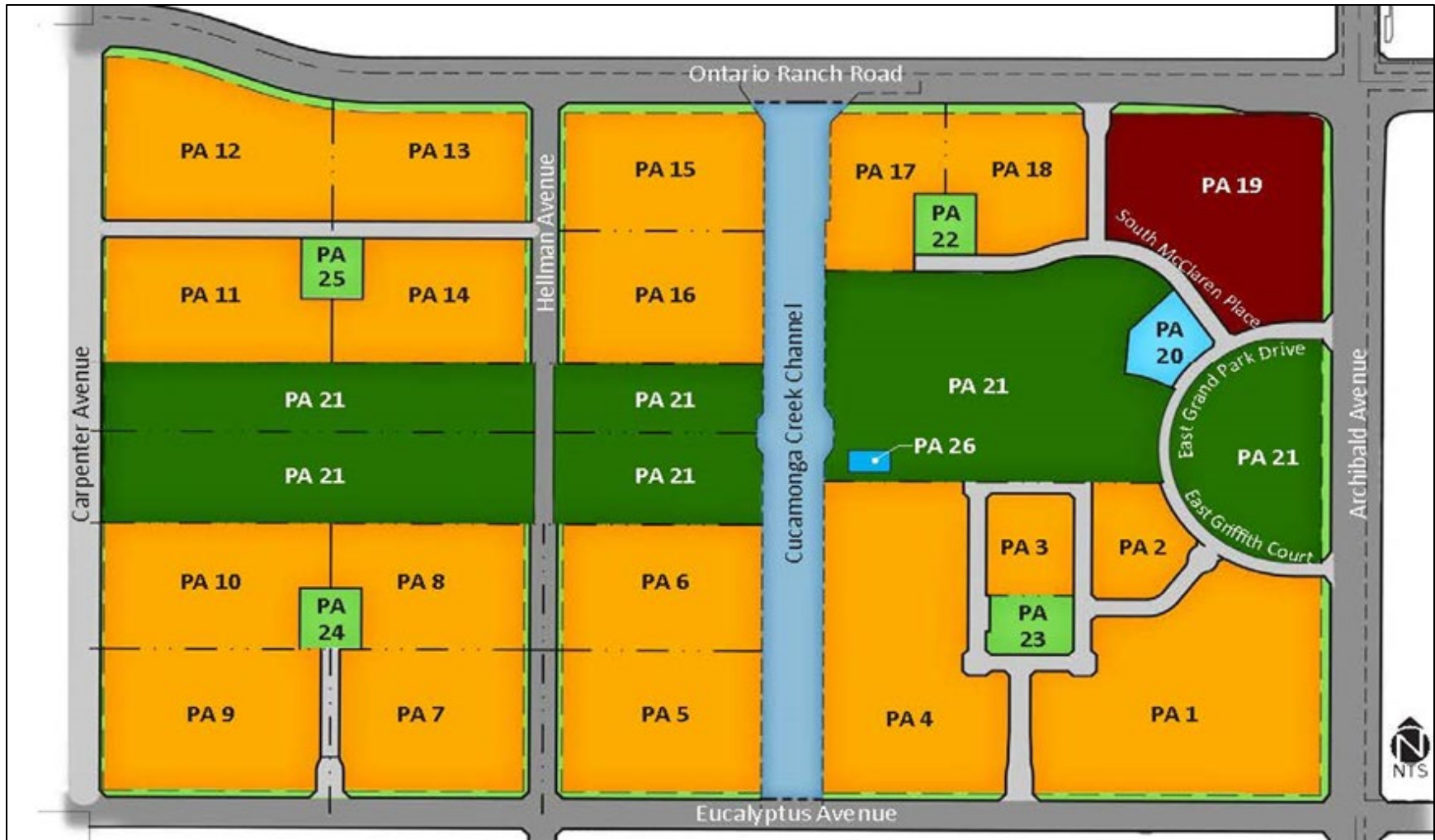
Planning Commission Staff Report

File No.: PDEV19-064

May 26, 2020

<i>Item</i>	<i>Required Min./Max.</i>	<i>Provided (Ranges)</i>	<i>Meets Y/N</i>
<i>Side to side living space:</i>	8'	8'	Y
<i>Side to side living with parking space:</i>	11'	11'	Y
<i>Rear living to rear living:</i>	8'	8'	Y
<i>Garage to garage face:</i>	30'	30'	Y
<i>Garage face to drive apron:</i>	3'	3'	Y
<i>Front Setbacks- Street Facing</i>			
<i>Living space:</i>	5'	5'	Y
<i>Porch, balcony:</i>	3'	3'	Y
<i>Garden wall:</i>	3'	3'	Y
<i>Front setback (lane facing):</i>	3'	3'	Y
<i>Maximum height:</i>	42'	27'5"- 29'3"	Y

**Exhibit A—Parkside Specific Plan Land Use Map, Proposed Amendment**



**Exhibit B—SITE PLAN**



**Exhibit C—FLOOR PLANS AND ELEVATIONS – PRODUCT 1**



PLAN 3R ALT.

PLAN 1

PLAN 2

PLAN 4R

**PASEO**  
2019-19



PLAN 3

PLAN 3R ALT.

**MOTOR ENTRY**  
2019-19

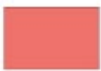


**STREET SCENE**  
Adaptive Mediterranean Elevation  
**NUVO PARKSIDE - PRODUCT 1**  
Ontario, California  
752.19016

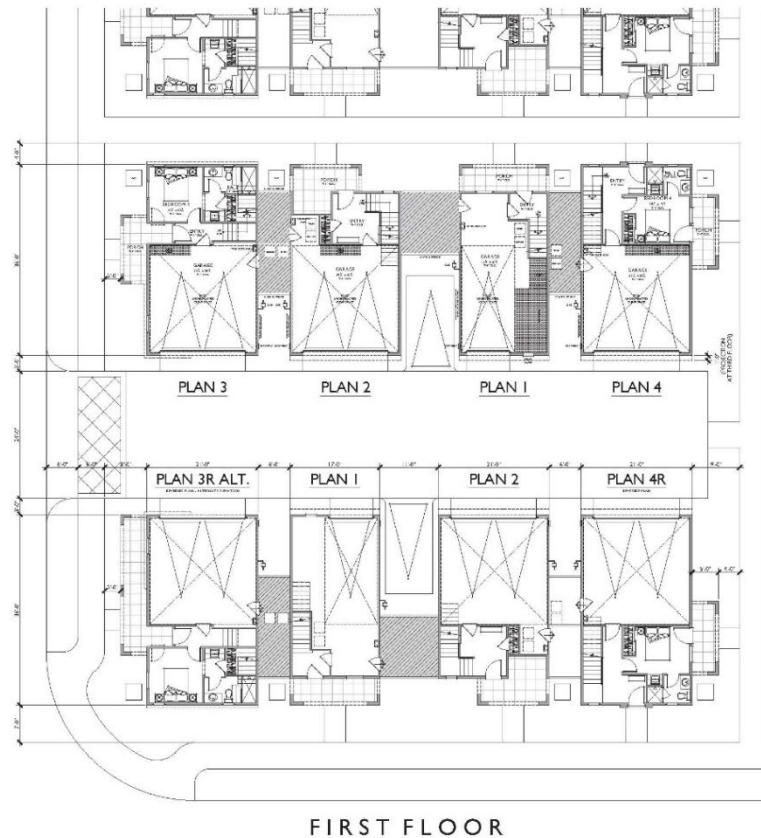
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**Exhibit C—FLOOR PLANS AND ELEVATIONS – PRODUCT 1**



**Exhibit C—FLOOR PLANS AND ELEVATIONS – PRODUCT 1**



**PLAN 1**  
 1,090 SQ. FT.  
 3 BEDROOMS / 2.5 BATHS  
 1 - CAR GARAGE / OWNER'S PARKING

FLOOR AREA TABLE	
1ST FLOOR	73 SQ. FT.
2ND FLOOR	473 SQ. FT.
3RD FLOOR	544 SQ. FT.
TOTAL	1,090 SQ. FT.
1 - CAR GARAGE	455 SQ. FT.
PORCH	85 SQ. FT.
DECK	85 SQ. FT.
PRIVATE OPEN SPACE	132 SQ. FT.

NOTE: SQUARE FOOTAGES ARE BASED ON METRIC OF CALCULATION.

**PLAN 2**  
 1,458 SQ. FT.  
 3 BEDROOMS / 2.5 BATHS  
 2 - CAR GARAGE

FLOOR AREA TABLE	
1ST FLOOR	164 SQ. FT.
2ND FLOOR	354 SQ. FT.
3RD FLOOR	740 SQ. FT.
TOTAL	1,458 SQ. FT.
2 - CAR GARAGE	469 SQ. FT.
PORCH	101 SQ. FT.
DECK	101 SQ. FT.
PRIVATE OPEN SPACE	60 SQ. FT.

NOTE: SQUARE FOOTAGES ARE BASED ON METRIC OF CALCULATION.

**PLAN 3**  
 1,688 SQ. FT.  
 4 BEDROOMS / 3.5 BATHS  
 2 - CAR GARAGE

FLOOR AREA TABLE	
1ST FLOOR	294 SQ. FT.
2ND FLOOR	673 SQ. FT.
3RD FLOOR	721 SQ. FT.
TOTAL	1,688 SQ. FT.
2 - CAR GARAGE	443 SQ. FT.
PORCH	89 SQ. FT.
DECK	60 SQ. FT.
PRIVATE OPEN SPACE	58 SQ. FT.

NOTE: SQUARE FOOTAGES ARE BASED ON METRIC OF CALCULATION.

**PLAN 4**  
 1,783 SQ. FT.  
 4 BEDROOMS / 3.5 BATHS  
 2 - CAR GARAGE

FLOOR AREA TABLE	
1ST FLOOR	319 SQ. FT.
2ND FLOOR	686 SQ. FT.
3RD FLOOR	777 SQ. FT.
TOTAL	1,783 SQ. FT.
2 - CAR GARAGE	437 SQ. FT.
PORCH	70 SQ. FT.
DECK	70 SQ. FT.
PRIVATE YARD	130 SQ. FT.

NOTE: SQUARE FOOTAGES ARE BASED ON METRIC OF CALCULATION.

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**COMPOSITE**  
**NUVO PARKSIDE - PRODUCT 1**  
 Ontario, California  
 752.19016

SCALE: 1/8" = 1'-0"

02.14.20

**Exhibit C— FLOOR PLANS AND ELEVATIONS – PRODUCT 1**



SECOND FLOOR

**PLAN 1**  
 1,090 SQ. FT.  
 2 BEDROOMS / 2.5 BATHS  
 1 - CAR GARAGE / OWNER'S PARKING

FLOOR AREA TABLE	
1ST FLOOR	72 SQ. FT.
2ND FLOOR	473 SQ. FT.
3RD FLOOR	544 SQ. FT.
TOTAL	1,090 SQ. FT.
1 - CAR GARAGE	455 SQ. FT.
PORCH	85 SQ. FT.
DECK	85 SQ. FT.
PRIVATE OPEN SPACE	132 SQ. FT.

NOTE: SQUARE FOOTAGE DOES NOT INCLUDE THE AREA OF CALCULATED.

**PLAN 2**  
 1,458 SQ. FT.  
 3 BEDROOMS / 3.5 BATHS  
 2 - CAR GARAGE

FLOOR AREA TABLE	
1ST FLOOR	164 SQ. FT.
2ND FLOOR	554 SQ. FT.
3RD FLOOR	740 SQ. FT.
TOTAL	1,458 SQ. FT.
2 - CAR GARAGE	489 SQ. FT.
PORCH	121 SQ. FT.
DECK	121 SQ. FT.
PRIVATE OPEN SPACE	60 SQ. FT.

NOTE: SQUARE FOOTAGE DOES NOT INCLUDE THE AREA OF CALCULATED.

**PLAN 3**  
 1,688 SQ. FT.  
 4 BEDROOMS / 3.5 BATHS  
 2 - CAR GARAGE

FLOOR AREA TABLE	
1ST FLOOR	294 SQ. FT.
2ND FLOOR	673 SQ. FT.
3RD FLOOR	721 SQ. FT.
TOTAL	1,688 SQ. FT.
2 - CAR GARAGE	443 SQ. FT.
PORCH	89 SQ. FT.
DECK	60 SQ. FT.
PRIVATE OPEN SPACE	58 SQ. FT.

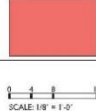
NOTE: SQUARE FOOTAGE DOES NOT INCLUDE THE AREA OF CALCULATED.

**PLAN 4**  
 1,783 SQ. FT.  
 4 BEDROOMS / 3.5 BATHS  
 2 - CAR GARAGE

FLOOR AREA TABLE	
1ST FLOOR	319 SQ. FT.
2ND FLOOR	686 SQ. FT.
3RD FLOOR	777 SQ. FT.
TOTAL	1,783 SQ. FT.
2 - CAR GARAGE	437 SQ. FT.
PORCH	70 SQ. FT.
DECK	70 SQ. FT.
PRIVATE YARD	120 SQ. FT.

NOTE: SQUARE FOOTAGE DOES NOT INCLUDE THE AREA OF CALCULATED.

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**COMPOSITE**  
**NUVO PARKSIDE - PRODUCT 1**  
 Ontario, California  
 752.19016

02.14.20





**Exhibit C— FLOOR PLANS AND ELEVATIONS – PRODUCT 1**



THIRD FLOOR

**PLAN 1**

1,090 SQ. FT.  
 2 BEDROOMS / 2.5 BATHS  
 1 - CAR GARAGE / OWNER'S PARKING

**FLOOR AREA TABLE**

1ST FLOOR	775 SQ. FT.
2ND FLOOR	473 SQ. FT.
3RD FLOOR	344 SQ. FT.
TOTAL	1,592 SQ. FT.
1 - CAR GARAGE	435 SQ. FT.
PORCH	85 SQ. FT.
DECK	85 SQ. FT.
PRIVATE OPEN SPACE	122 SQ. FT.

NOTE: SQUARE FOOTAGE DOES NOT INCLUDE GARAGE DECK

**PLAN 2**

1,458 SQ. FT.  
 3 BEDROOMS / 3.5 BATHS  
 2 - CAR GARAGE

**FLOOR AREA TABLE**

1ST FLOOR	164 SQ. FT.
2ND FLOOR	554 SQ. FT.
3RD FLOOR	740 SQ. FT.
TOTAL	1,458 SQ. FT.
2 - CAR GARAGE	449 SQ. FT.
PORCH	161 SQ. FT.
DECK	161 SQ. FT.
PRIVATE OPEN SPACE	493 SQ. FT.

NOTE: SQUARE FOOTAGE DOES NOT INCLUDE GARAGE DECK

**PLAN 3**

1,688 SQ. FT.  
 4 BEDROOMS / 3.5 BATHS  
 2 - CAR GARAGE

**FLOOR AREA TABLE**

1ST FLOOR	294 SQ. FT.
2ND FLOOR	673 SQ. FT.
3RD FLOOR	721 SQ. FT.
TOTAL	1,688 SQ. FT.
2 - CAR GARAGE	443 SQ. FT.
PORCH	89 SQ. FT.
DECK	60 SQ. FT.
PRIVATE OPEN SPACE	58 SQ. FT.

NOTE: SQUARE FOOTAGE DOES NOT INCLUDE GARAGE DECK

**PLAN 4**

1,783 SQ. FT.  
 4 BEDROOMS / 3.5 BATHS  
 2 - CAR GARAGE

**FLOOR AREA TABLE**

1ST FLOOR	319 SQ. FT.
2ND FLOOR	686 SQ. FT.
3RD FLOOR	777 SQ. FT.
TOTAL	1,783 SQ. FT.
2 - CAR GARAGE	427 SQ. FT.
PORCH	70 SQ. FT.
DECK	70 SQ. FT.
PRIVATE YARD	122 SQ. FT.

NOTE: SQUARE FOOTAGE DOES NOT INCLUDE GARAGE DECK



**COMPOSITE**  
**NUVO PARKSIDE - PRODUCT 1**  
 Ontario, California  
 752.19016



**Exhibit C— FLOOR PLANS AND ELEVATIONS – PRODUCT 2**



**Exhibit C— FLOOR PLANS AND ELEVATIONS – PRODUCT 2**



FIRST FLOOR

**PLAN 1**

1,020 SQ. FT.  
 2 BEDROOMS / 2.5 BATHS / TECH  
 1 - CAR GARAGE / OWNER'S PARKING

**FLOOR AREA TABLE**

1ST FLOOR	463 SQ. FT.
2ND FLOOR	557 SQ. FT.
TOTAL	1,020 SQ. FT.
1 - CAR GARAGE	217 SQ. FT.
PRIVATE OPEN SPACE	289 SQ. FT.

NOTE: SQUARE FOOTAGE MAY VARY DUE TO THE METHOD OF CALCULATION.

**PLAN 2**

1,046 SQ. FT.  
 2 BEDROOMS / 2.5 BATHS / LOFT  
 1 - CAR GARAGE / OWNER'S PARKING

**FLOOR AREA TABLE**

1ST FLOOR	448 SQ. FT.
2ND FLOOR	598 SQ. FT.
TOTAL	1,046 SQ. FT.
1 - CAR GARAGE	218 SQ. FT.
PRIVATE OPEN SPACE	289 SQ. FT.

NOTE: SQUARE FOOTAGE MAY VARY DUE TO THE METHOD OF CALCULATION.

**PLAN 2G**

1,046 SQ. FT.  
 2 BEDROOMS / 2.5 BATHS / LOFT  
 2 - CAR GARAGE

**FLOOR AREA TABLE**

1ST FLOOR	448 SQ. FT.
2ND FLOOR	598 SQ. FT.
TOTAL	1,046 SQ. FT.
2 - CAR GARAGE	429 SQ. FT.
PRIVATE OPEN SPACE	249 SQ. FT.

NOTE: SQUARE FOOTAGE MAY VARY DUE TO THE METHOD OF CALCULATION.

STORAGE  
 PRIVATE OPEN SPACE

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**COMPOSITE**  
**NUVO PARKSIDE - PRODUCT 2**  
 Ontario, California  
 752.19013

SCALE: 1/8" = 1'-0"

02.14.20



**Exhibit C— FLOOR PLANS AND ELEVATIONS – PRODUCT 2**



**PLAN 1**  
 1,020 SQ. FT.  
 2 BEDROOMS / 2.5 BATHS / TERR.  
 1 - CAR GARAGE / OWNER'S PARKING

FLOOR AREA TABLE	
1ST FLOOR	463 SQ. FT.
2ND FLOOR	557 SQ. FT.
TOTAL	1,020 SQ. FT.
1 - CAR GARAGE	217 SQ. FT.
PRIVATE OPEN SPACE	289 SQ. FT.

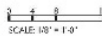
**PLAN 2**  
 1,046 SQ. FT.  
 2 BEDROOMS / 2.5 BATHS / LOFT  
 1 - CAR GARAGE / OWNER'S PARKING

FLOOR AREA TABLE	
1ST FLOOR	446 SQ. FT.
2ND FLOOR	596 SQ. FT.
TOTAL	1,046 SQ. FT.
1 - CAR GARAGE	216 SQ. FT.
PRIVATE OPEN SPACE	289 SQ. FT.

**PLAN 2G**  
 1,046 SQ. FT.  
 2 BEDROOMS / 2.5 BATHS / LOFT  
 2 - CAR GARAGE

FLOOR AREA TABLE	
1ST FLOOR	446 SQ. FT.
2ND FLOOR	596 SQ. FT.
TOTAL	1,046 SQ. FT.
2 - CAR GARAGE	432 SQ. FT.
PRIVATE OPEN SPACE	249 SQ. FT.

SECOND FLOOR



**Exhibit C— FLOOR PLANS AND ELEVATIONS – PRODUCT 3**



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**STREET SCENE**  
Contemporary Elevation  
**NUVO PARKSIDE - PRODUCT 3**  
Ontario, California  
752.19014

02.14.20



**Exhibit C— FLOOR PLANS AND ELEVATIONS – PRODUCT 3**



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**STREET SCENE**  
Contemporary Elevation  
**NUVO PARKSIDE - PRODUCT 3**  
Ontario, California  
752.19014

02.14.20  
**NEW HOME**  
COMPANY

**Exhibit C— FLOOR PLANS AND ELEVATIONS – PRODUCT 3**



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**COMPOSITE**  
 NUVO PARKSIDE - PRODUCT 3  
 Ontario, California  
 752.19014

SCALE: 1/8" = 1'-0"

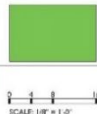
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**Exhibit C— FLOOR PLANS AND ELEVATIONS – PRODUCT 3**



SECOND FLOOR





**Exhibit C— FLOOR PLANS AND ELEVATIONS – PRODUCT 4**



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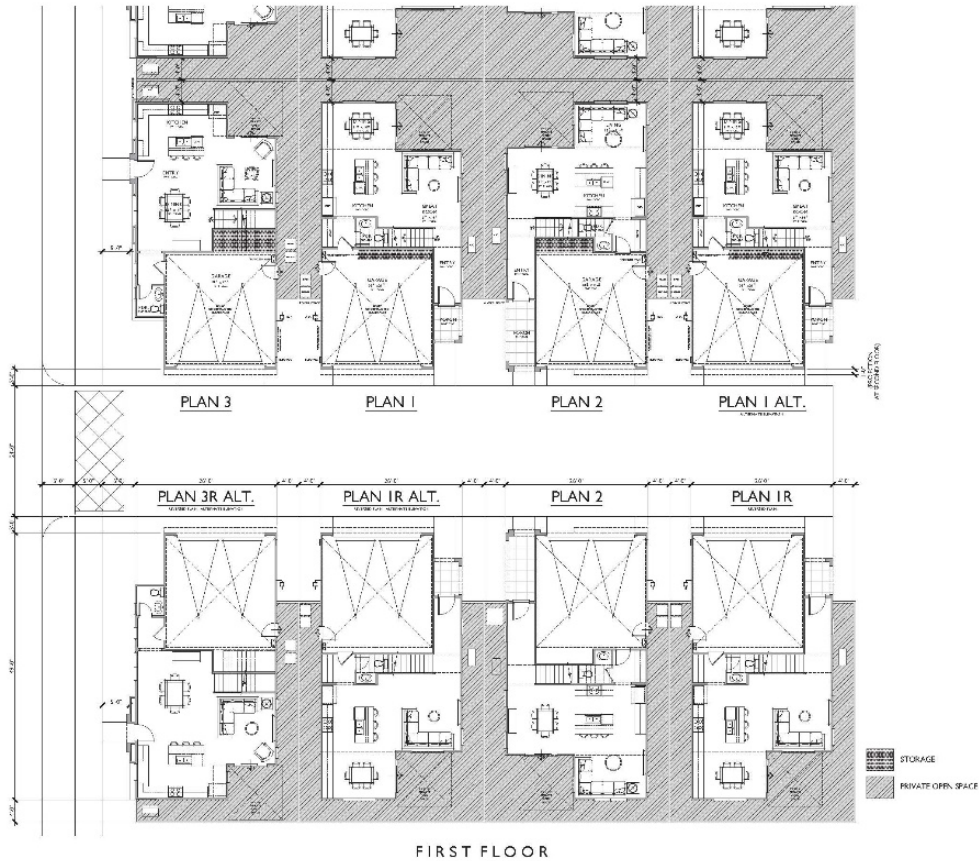


**STREET SCENE**  
Adaptive Farmhouse Elevation  
**NUVO PARKSIDE - PRODUCT 4**  
Ontario, California  
752.19015

02.14.20



**Exhibit C— FLOOR PLANS AND ELEVATIONS – PRODUCT 4**



**PLAN 1**

1,595 SQ. FT.  
 3 BEDROOMS / 2.5 BATHS / LOFT  
 2 - CAR GARAGE

**FLOOR AREA TABLE**

1ST FLOOR	653 SQ. FT.
2ND FLOOR	942 SQ. FT.
TOTAL	1,595 SQ. FT.
2 - CAR GARAGE	457 SQ. FT.
PORCH	35 SQ. FT.
PRIVATE OPEN SPACE	532 SQ. FT.

**PLAN 2**

1,647 SQ. FT.  
 4 BEDROOMS / 2.5 BATHS / LOFT  
 2 - CAR GARAGE

**FLOOR AREA TABLE**

1ST FLOOR	639 SQ. FT.
2ND FLOOR	1,008 SQ. FT.
TOTAL	1,647 SQ. FT.
2 - CAR GARAGE	466 SQ. FT.
PORCH	67 SQ. FT.
PRIVATE OPEN SPACE	529 SQ. FT.

**PLAN 3**

1,790 SQ. FT.  
 4 BEDROOMS / 2.5 BATHS / LOFT  
 2 - CAR GARAGE

**FLOOR AREA TABLE**

1ST FLOOR	681 SQ. FT.
2ND FLOOR	1,109 SQ. FT.
TOTAL	1,790 SQ. FT.
2 - CAR GARAGE	486 SQ. FT.
PRIVATE OPEN SPACE	322 SQ. FT.

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 Fax: 714.947.8101

**COMPOSITE**  
**NUVO PARKSIDE - PRODUCT 4**  
 Ontario, California  
 752.19015  
 SCALE: 1/8" = 1'-0"

02.14.20  

 NEW HOME COMPANY

**Exhibit C— FLOOR PLANS AND ELEVATIONS – PRODUCT 4**



SECOND FLOOR

**PLAN 1**  
 1,598 SQ. FT.  
 3 BEDROOMS / 2.5 BATHS / LOFT  
 2 - CAR GARAGE

FLOOR AREA TABLE	
1ST FLOOR	653 SQ. FT.
2ND FLOOR	942 SQ. FT.
TOTAL	1,595 SQ. FT.
2 - CAR GARAGE	457 SQ. FT.
PORCH	35 SQ. FT.
PRIVATE OPEN SPACE	532 SQ. FT.

**PLAN 2**  
 1,647 SQ. FT.  
 4 BEDROOMS / 2.5 BATHS / LOFT  
 2 - CAR GARAGE

FLOOR AREA TABLE	
1ST FLOOR	639 SQ. FT.
2ND FLOOR	1,008 SQ. FT.
TOTAL	1,647 SQ. FT.
2 - CAR GARAGE	466 SQ. FT.
PORCH	67 SQ. FT.
PRIVATE OPEN SPACE	529 SQ. FT.

**PLAN 3**  
 1,790 SQ. FT.  
 4 BEDROOMS / 2.5 BATHS / LOFT  
 2 - CAR GARAGE

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TOTAL	1,790 SQ. FT.
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PRIVATE OPEN SPACE	322 SQ. FT.

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**COMPOSITE**  
**NUVO PARKSIDE - PRODUCT 4**  
 Ontario, California  
 752.19015

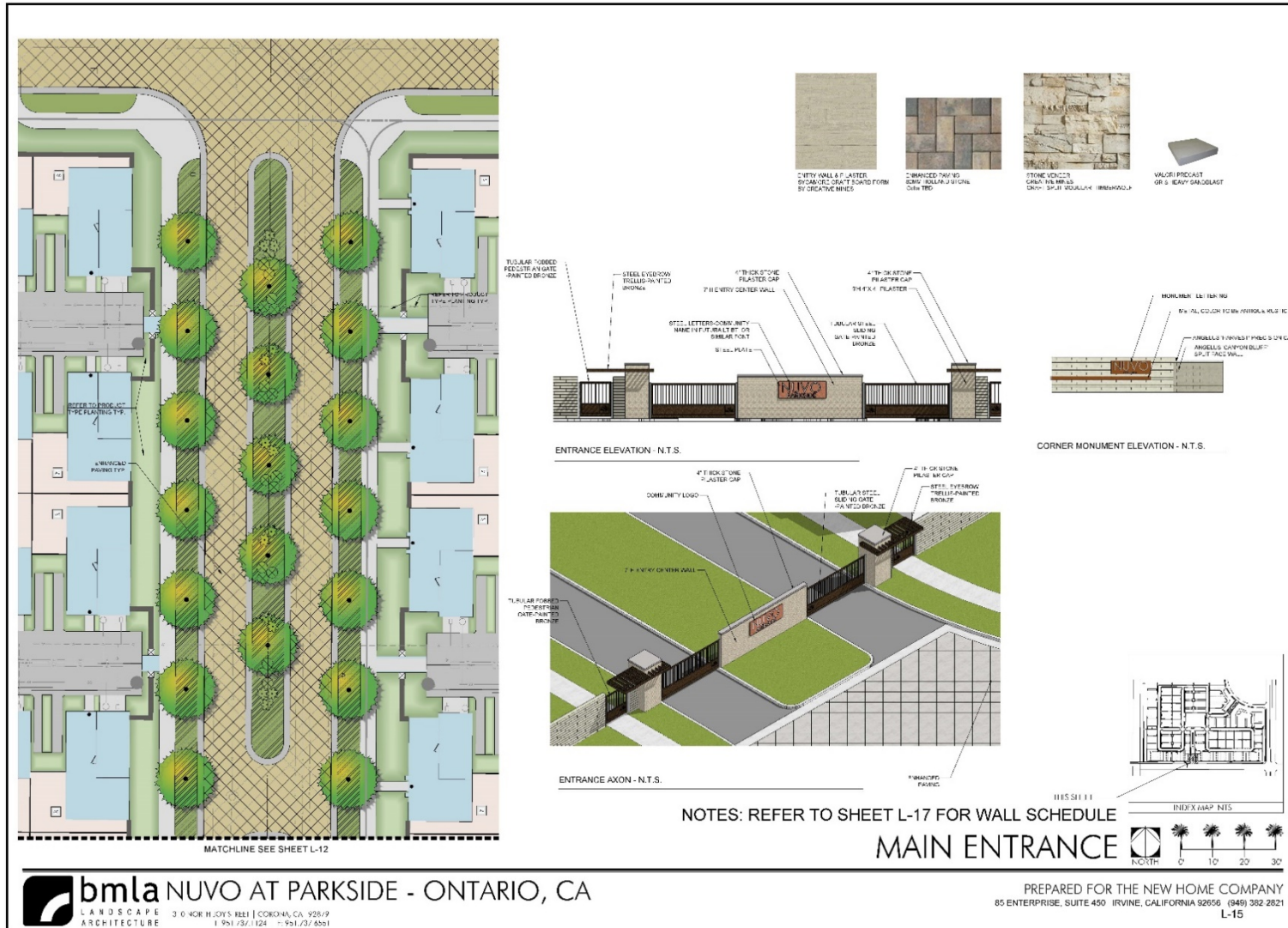


SCALE: 1/8" = 1'-0"

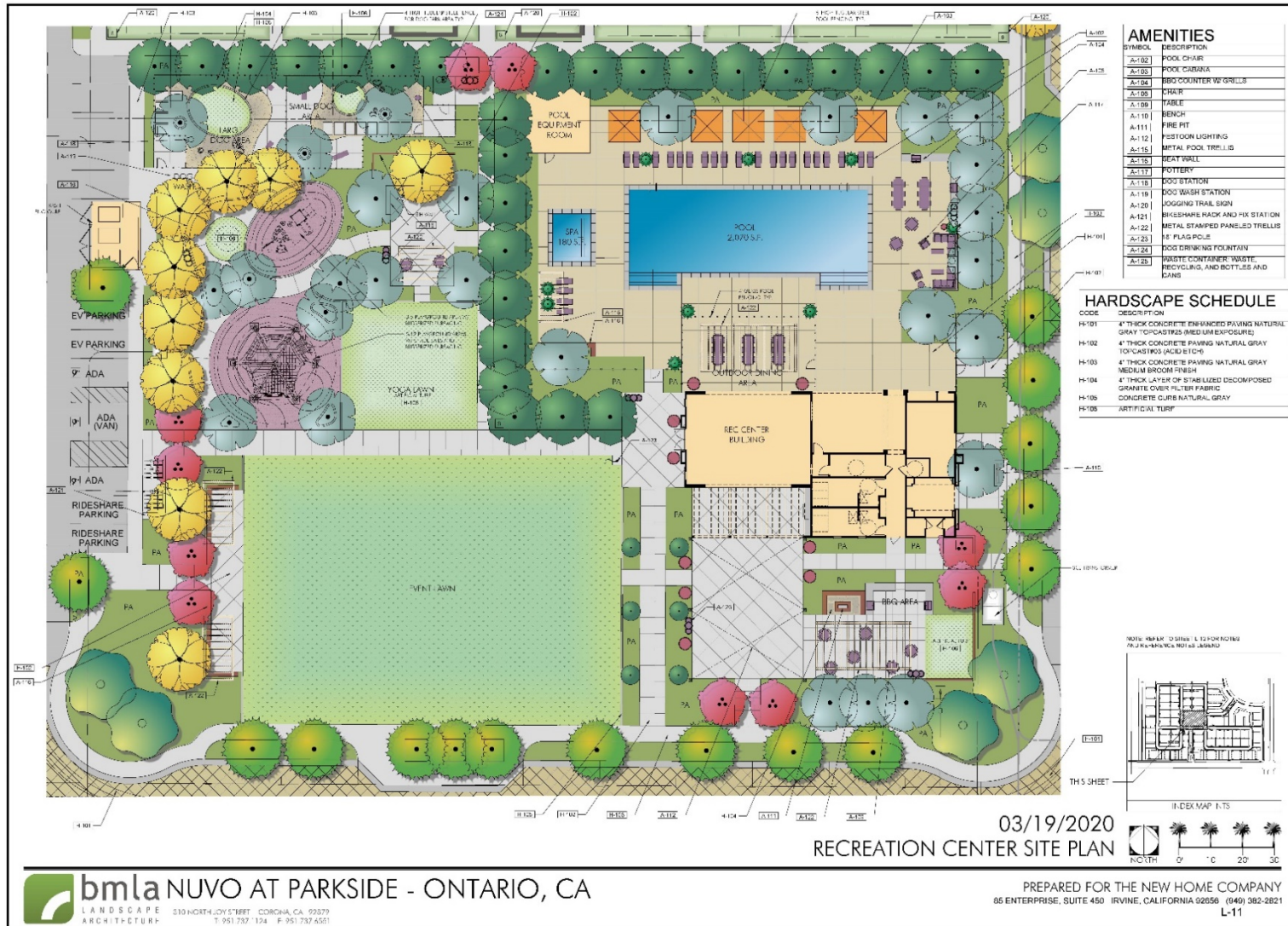
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**Exhibit D — CONCEPTUAL ENTRY GATE RENDERINGS**



**Exhibit E—CLUBHOUSE/RECREATION CENTER SITE PLAN**



**Exhibit E—CLUBHOUSE/RECREATION CENTER RENDERINGS**



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TEL: 951.221.1111  
WWW.BLARCHITECT.COM

CONCEPTUAL ELEVATION  
NUVO PARKSIDE - RECREATION CENTER  
Ontario, California  
752.19243

12.04.19



**Exhibit E—CLUBHOUSE/RECREATION CENTER RENDERING**



REAR  
SECONDARY ENTRY ELEVATION



LEFT  
POOL DECK ELEVATION

## RESOLUTION NO. PC

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV19-064, A DEVELOPMENT PLAN TO CONSTRUCT 540 SINGLE-FAMILY DETACHED HOMES AND A PRIVATE RECREATIONAL CENTER WITHIN A GATED COMMUNITY ON 34.30 ACRES OF LAND LOCATED AT THE NORTHWEST CORNER OF EUCALYPTUS AVENUE AND ARCHIBALD AVENUE, WITHIN PLANNING AREAS 1 THROUGH 4 AND PLANNING AREA 23 OF THE PARKSIDE SPECIFIC PLAN, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 0218-231-35, 0218-231-33, 0218-231-30, 0218-231-29, 0218-231-28, 0218-231-22, 0218-231-21, 0218-231-20, 0218-231-19, 0218-231-18, 0218-231-17, 0218-231-14.

WHEREAS, The New Home Company of Southern California, LLC. (hereinafter referred to as "Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV19-064, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 34.30 acres of land located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning Area 1 (8-10 Plex Detached Green Court Cluster Homes), Planning Areas 2 through 4 (Conventionally-Loaded Lane Homes), and Planning Area 23 (Private Open Space) of the Parkside Specific Plan; and

WHEREAS, properties to the north and west of the Project site are within Planning Area 21 (Open Space Parkland/Great Park), Planning Area 26 (CDA Well Site/ Public Facility), Planning Area 5 (2 and 3-Story Townhomes/MDR), and Planning Area 6 (Motorcourt/MDR) of the Parkside Specific Plan, and include vacant land and the Cucamonga Creek Flood Control Channel (west). The properties to the east are within Planning Area 8 (High Density Residential and Great Park) of the Grand Park Specific Plan and is currently vacant. The property to the south of the project site is within Planning Area 2 (Neighborhood Center) and Planning Area 1 (Conventional Small Lot) of the Subarea 29 Specific Plan and is currently vacant; and

WHEREAS, the Project is contingent upon City Council approval of an Amendment to the Parkside Specific Plan (File No. PSPA19-007) to: [1] reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] update and revise Landscape Standards (Section 7.7); and



WHEREAS, the Project is contingent upon Planning Commission approval of the related Tentative Tract Map TT 20316 (File No. PMTT19-020) for condominium purposes, subdividing 56.99 acres of land into 4 numbered residential lots and 12 lettered lots for common areas, portions of the Great Park, private streets, public utility easements, and neighborhood landscape edges. The Project is located at the northwest corner of Archibald Avenue and Eucalyptus Avenue, within Planning areas 1 through 4 (Detached Green Court Cluster Homes and Conventionally-Loaded Lane Homes) of the Parkside Specific Plan; and

WHEREAS, the Development Plan proposes to construct 540 single-family homes (170 8-Plex Detached Green Court Cluster Homes and 370 Conventionally-Loaded Lane Homes) and a 2,800 square foot recreational center within an a gated community. The development meets the Development Standards of the 8-10 Plex Detached Green Court Cluster Homes and Conventionally-Loaded Lane Homes of the Parkside Specific Plan; and

WHEREAS, four product types with three to four floor plans are proposed, with three architectural styles (Adaptive Farmhouse, Adaptive Mediterranean and Contemporary) per floor plan; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an initial study has been prepared to determine possible environmental impacts; and

WHEREAS the environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed and this Application introduces no new significant environmental impacts; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element

law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (“ALUCP”), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 18, 2020, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB20-022, recommending that the Planning Commission approve the Application; and

WHEREAS, on May 26, 2019, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

**SECTION 1: Environmental Determination and Findings.** As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the previous Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006, and supporting documentation. Based upon the facts and information contained in the previous Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) and supporting documentation, the Planning Commission finds as follows:

(1) The environmental impacts of this project were previously reviewed in conjunction with File No. Addendum to the Parkside Specific Plan (File No. PSP03-002)

Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006 and supporting documentation; and

(2) The previous Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) was completed in compliance with CEQA and the Guidelines promulgated thereunder; and

(4) The previous Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) reflects the independent judgment of the Planning Commission; and

(5) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the previous Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008), and all mitigation measures previously adopted with the Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008), are a condition of Project approval and are incorporated herein by this reference.

**SECTION 2: Subsequent or Supplemental Environmental Review Not Required.** Based on the information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

**SECTION 3: *Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The Project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed Project is consistent with the maximum number of dwelling units (540) and density (18 DUs/AC) specified within the Parkside Specific Plan. Per the Available Land Inventory, the Parkside Specific Plan is required to provide 1,947 dwelling units with an overall density range of 9 to 21 DUs/AC.

**SECTION 4: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the Planning Commission, therefore, finds and determines that the Project, when

implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

**SECTION 5: Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the Medium Density Residential land use district of the Policy Plan Land Use Map, and the Parkside Specific Plan. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. The Development Plan has been required to comply with all provisions of the Parkside Specific Plan. Future neighborhoods within the Parkside Specific Plan and surrounding area will provide for diverse housing and highly amenitized neighborhoods that will be compatible in design, scale and massing to the proposed development; and

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the Parkside Specific Plan, including standards relative to the particular land use proposed (single-family residential), as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions; and

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Parkside Specific Plan are maintained; [ii] the Project will not endanger the public health, safety or general welfare; [iii] the Project will not result in any significant environmental impacts; [iv] the Project will be in harmony with the area in which it is located; and [v] the Project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan, and the Parkside Specific Plan. Additionally, the environmental impacts of this Project were previously reviewed in conjunction with the Parkside Specific Plan Environmental Impact

Report (SCH# 2004011008). This application is consistent with the previously adopted EIR and introduces no new significant environmental impacts; and

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.*** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Parkside Specific Plan that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (single-family residential homes). As a result of this review, the Planning Commission has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the Parkside Specific Plan.

**SECTION 6: Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

**SECTION 7: Indemnification.** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

**SECTION 8: Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

**SECTION 9: Certification to Adoption.** The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day May 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

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Jim Willoughby  
Planning Commission Chairman

ATTEST:

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Cathy Wahlstrom  
Planning Director and  
Secretary to the Planning Commission

Planning Commission Resolution  
File No. PDEV19-064  
May 26, 2020  
Page 9

STATE OF CALIFORNIA            )  
COUNTY OF SAN BERNARDINO )  
CITY OF ONTARIO                )

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. \_\_\_\_ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 26, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

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Gwen Berendsen  
Secretary Pro Tempore



**ATTACHMENT A:**

**File No. PDEV19-064  
Departmental Conditions of Approval**

*(Departmental conditions of approval follow this page)*



City of Ontario  
Planning Department  
303 East B Street  
Ontario, California 91764  
Phone: 909.395.2036  
Fax: 909.395.2420

*Planning Department  
Land Development Division  
Conditions of Approval*

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**Meeting Date:** May 26, 2020  
**File No:** PDEV19-064  
**Related Files:** PSPA19-007 and PMTT19-020

**Project Description:** A Development Plan to construct 540 single-family detached homes and a private recreational center within a gated community on 34.30 acres of land located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning Areas 1-4 and Planning Area 23 of the Parkside Specific Plan. (APNs: 0218-231-35, 0218-231-33, 0218-231-30, 0218-231-29, 0218-231-28, 0218-231-22, 0218-231-21, 0218-231-20, 0218-231-19, 0218-231-18, 0218-231-17, 0218-231-14, and 0218-231-12) **submitted by The New Home Company Southern California, LLC.**

**Prepared By:** Diane Ayala, Senior Planner  
Phone: 909.395.2428 (direct)  
Email: dayala@ontarioca.gov

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The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

**1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

**2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

**2.1 Contingent Approvals**

**(a)** The project is contingent on City Council approval of an amendment to the Parkside Specific Plan, File No. PSPA19-007 and Planning Commission approval of Tentative Tract Map (TT20316), File No. PMTT19-020. All conditions of approval adopted with referenced projects shall be incorporated herein by this reference.

**2.2 Time Limits**

**(a)** Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

**2.3 General Requirements** - The Project shall comply with the following general requirements:

**(a)** All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

**(b)** The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

**(c)** The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

#### **2.4** Landscaping

**(a)** The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

**(b)** Comply with the conditions of approval of the Planning Department, Landscape Planning Division.

**(c)** Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

**(d)** Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

#### **2.5** Walls and Fences

**(a)** All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions) and Parkside Specific Plan standards.

**(b)** Maximum wall heights are 4' along paseo trails and pedestrian pathways.

**(c)** Walls and fences shall be adequately setback from the front facades of buildings.

#### **2.6** Parking, Circulation and Access

**(a)** The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

**(b)** The gated vehicular entryways shall be constructed with enhanced pavement.

**(c)** Pedestrian pathways that cross driveways and drive aisles shall be delineated by enhanced paving treatments.

**(d)** All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(e) All residential driveways, including parking spaces located on driveways shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(f) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(g) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(h) The number of parking spaces shall comply with the requirements of the Parkside Specific Plan.

(i) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(j) Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

## **2.7** Site Lighting

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise daily and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

## **2.8** Mechanical and Rooftop Equipment

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls. Ground mounted AC units, which are located within the front or side yards of residential units, shall be "slim-line" dimensioned AC units unless it can be demonstrated that the AC unit can be adequately screened through landscape.

**2.9** Security Standards - The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

**2.10** Signs - All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

**2.11** Sound Attenuation - The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noise levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

**2.12** Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements

(a) CC&Rs shall be prepared for the Project pursuant to Tentative Tract Map, File No. PMTT19-020 (TT20316) and shall be recorded prior to the issuance of a building permit.

(b) The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owners' association and the CC&Rs shall be reviewed and approved by the City.

(c) CC&Rs shall ensure reciprocal parking and access between parcels.

(d) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:

- i. Landscaping and irrigation systems within common areas
- ii. Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02
- iii. Shared parking facilities and access drives
- iv. Utility and drainage easements

(e) CC&Rs shall include authorization for the City's local law enforcement officers to enforce City and State traffic and penal codes within the project area.

(f) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.

(g) A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

**2.13** Disclosure Statements

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

- i. This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.
- ii. Some of the property adjacent to this tract is zoned for agricultural uses and there could be fly, odor, or related problems due to the proximity of animals.
- iii. The area south of Riverside Drive lies within the San Bernardino County Agricultural Preserve. Dairies currently existing in that area are likely to remain for the foreseeable future.
- iv. This tract is part of a Landscape Maintenance District. The homeowner(s) will be assessed through their property taxes for the continuing maintenance of the district.

## **2.14** Environmental Review

**(a)** The environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. The previously adopted mitigation measures shall be a condition of project approval and are incorporated herein by this reference.

**(b)** If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

**(b)** If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

**2.15** Indemnification - The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

## **2.16** Additional Fees

**(a)** Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

**(b)** After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

**2.17** Additional Requirements - Tribal Consultation - At the request of The Gabrieleno Band of Missions Indians – Kizh Nation, all proposed future development entitlements within the Specific Plan boundary shall consult with The Gabrieleno Band of Missions Indians – Kizh Nation (Chairman, Andrew Sales @ <http://www.gabrielenoindians.org/>), prior to grading activities. In addition, the project shall be subject to following conditions:

**(a)** The project developer shall retain a Native American Monitor of Gabrieleño Ancestry to conduct a Native American Indian Sensitivity Training for construction personnel prior to commencement of any excavation activities. The training session shall include a handout and focus on how to identify Native American resources encountered during earthmoving activities and the procedures followed if resources are discovered, the duties of the Native American Monitor of Gabrieleño Ancestry and the general steps the Monitor would follow in conducting a salvage investigation.

**(b)** The project developer shall retain a Native American Monitor of Gabrieleño Ancestry to be on-site during all project-related, ground-disturbing construction activities (e.g., pavement removal, auguring, boring, grading, excavation, potholing, trenching, and grubbing) of previously undisturbed native soils to a maximum depth of 30 feet below ground surface. At their discretion, a Native American Monitor of Gabrieleño Ancestry can be present during the removal of dairy manure to native soil, but not at the developers' expense.

**(c)** A qualified archaeologist and a Native American Monitor of Gabrieleño Ancestry shall evaluate all archaeological resources unearthed by project construction activities. If the resources are Native American in origin, the Tribe shall coordinate with the developer regarding treatment and curation of these resources. Typically, the Tribe will request reburial or preservation for educational purposes. If archeological features are discovered, the archeologist shall report such findings to the Ontario Planning Director. If the archeological resources are found to be significant, the archeologist shall determine the appropriate actions, in cooperation with the City that shall be taken for exploration and/or salvage in compliance with CEQA Guidelines Section 15064.5(f).

**(d)** Prior to the start of ground disturbing activities, the developer shall arrange a designated site location within the footprint of the project for the respectful reburial of Tribal human remains and/or ceremonial objects. All human skeletal material discoveries shall be reported immediately to the County Coroner. The Native American Monitor shall immediately divert work a minimum of 50 feet from the discovery site and place an exclusion zone around the burial. The Native American Monitor shall notify the construction manager who shall contact the San Bernardino County Coroner. All construction activity shall be diverted while the San Bernardino County Coroner determines if the remains are Native American. The discovery shall be confidential and secure to prevent further disturbance. If Native American, the San Bernardino County Coroner shall notify the Native American Heritage Commission (NAHC) as mandated by state law who will then appoint a Most Likely Descendent. In the case where discovered human remains cannot be documented and recovered on the same day, the remains shall be covered with muslin cloth and a steel plate that can be moved by heavy equipment placed over the excavation opening to protect the remains. If this type of steel plate is not available, a 24-hour guard shall be posted outside working hours. The Tribe shall make every effort to recommend diverting the project and keep the remains in situ and protected. If the project cannot be diverted, it may be determined that burials will be removed. If data recovery is approved by the Tribe, documentation shall be taken, which includes at a minimum detailed descriptive notes and sketches. Additional types of documentation shall be approved by the Tribe for data recovery purposes. Cremations will either be removed in bulk or means necessary to ensure complete recovery of all material. If the discovery of human remains includes four (4) or more burials, the location is considered a cemetery and a separate treatment plan shall be created. The project developer shall consult with the Tribe regarding avoidance of all cemetery sites. Once complete, a final report of all activities shall be submitted to the NAHC.

**(e)** No scientific study or the utilization of any invasive diagnostics shall be allowed to any Native American human remains.

**(f)** If the San Bernardino County Coroner determines the remains represent a historic non-Native American burial, the burial shall be treated in the same manner of respect with agreement of the San Bernardino County Coroner. Reburial will be in an appropriate setting. If the San Bernardino County Coroner determines the remains to be modern, the San Bernardino County Coroner shall take custody of the remains.

**(g)** Each occurrence of human remains and associated funerary objects shall be stored using opaque cloth bags. All human remains, funerary objects, sacred objects and objects of cultural patrimony shall be removed to a secure container on site if possible. These items shall be retained and reburied within six months of recovery. The site of reburial/repatriation shall be on the project site, but at a location agreed upon between the Tribe and the developer and protected in perpetuity. There shall be no publicity regarding any cultural materials recovered.

**2.18** Additional Requirements

**(a)** The Ontario Climate Action Plan (CAP) requires new development to be 25% more efficient. The applicant has elected to utilize the Screening Tables provided in the CAP instead of preparing separate emissions calculations. By electing to utilize the Screening Tables the applicant shall be required to garner a minimum of 100 points to be consistent with the reduction quantities outlined in the CAP. The applicant shall identify on the construction drawings the items identified in the industrial Screening Tables.

**(b)** The following separation requirements from existing dairies/feed lots shall apply to new residential development or structures used for public assembly purposes from existing dairies/feed lots. A minimum 100' separation shall be required between a new residential, commercial or industrial development or structure used for public assembly and an existing animal feed trough, corral/pen or an existing dairy/feed lot including manure stockpiles and related wastewater detention basins. The 100-foot separation requirement may be satisfied by an off-site easement acceptable to the Planning Director with adjacent properties, submitted with the initial final map and recorded prior to or concurrent with the final map.

**(c)** Prior to issuance of the 151<sup>st</sup> home occupancy within TT 20316, the Recreation Center and park located on 1.32 acres of land on Lot A of TT 20316 shall be fully constructed.

**(c)** Prior to the first home occupancy, the applicant shall submit an HOA parking and solid waste pick-up enforcement plan for the community to be codified within the CC&R's, subject to Planning Department review and approval

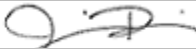
**(d)** All applicable conditions of approval of Development Agreement (File No. PDA05-002) shall apply to this development.



**CITY OF ONTARIO**  
**LANDSCAPE PLANNING DIVISION**  
 303 East "B" Street, Ontario, CA 91764

**DAB CONDITIONS OF APPROVAL**

**Sign Off**

	4/20/20
Jamie Richardson, Sr. Landscape Planner	Date

Reviewer's Name: <b>Jamie Richardson, Sr. Landscape Planner</b>	Phone: <b>(909) 395-2615</b>
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D.A.B. File No.: PDEV19-064	Case Planner: Diane Ayala
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Project Name and Location:

Nuvo at Parkside  
 TM20316

Applicant/Representative:

The New Home Company – Holly Slevcove  
 85 Enterprise, Ste. 450  
 Aliso Viejo, CA 92656

**A Preliminary Landscape Plan (dated 4/2/20) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.**

**A Preliminary Landscape Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.**

**A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE.**  
 Landscape construction plans with plan check number may be emailed to: [landscapeplancheck@ontarioca.gov](mailto:landscapeplancheck@ontarioca.gov)  
**DIGITAL SUBMITTALS MUST BE 10MB OR LESS.**

**Civil/ Site Plans**

1. Callout decorative paving for all motor courts and alleys.
2. Storm water infiltration devices located in landscape areas shall be reviewed and plans approved by the Landscape Planning Division prior to permit issuance. Any storm water devices in parkway areas shall not displace street trees.
3. Show backflow devices set back 4' from paving all sides. Locate on level grade
4. Locate utilities including light standards, fire hydrants, water, drain and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans.
5. Note for compaction to be no greater than 85% at landscape areas. All finished grades at 1 ½" below finished surfaces. Landscape slopes shall be maximum 3:1; change all 2:1 landscape slopes to meet the required 3:1.
6. Dimension, show and call out for step-outs at parking spaces adjacent to planters; a 12" wide monolithic concrete curb, DG paving or pavers with edging.
7. Typical lot drainage shall include a catch basin with gravel sump below each before exiting property; or connect to underground drainage system.

**Landscape Plans**

8. Callout decorative paving for all motor courts and alleys.
9. Vinyl fencing shall only be used where unseen from the public view.
10. Replace artificial turf at Hollywood Drives use stamped and/or colored concrete or decorative pavers. Replace artificial turf at community/rec center and dog parks; inland summers can reach temperatures of over 100 degrees; artificial turf increases surrounding temperatures up to 30 degrees making these spaces unusable to the community.
11. Show backflow devices with 36" high strappy leaf shrub screening and trash enclosures and transformers, a 4'-5' high evergreen hedge screening. Do not encircle utility, show as masses and

duplicate masses in other locations on regular intervals. Transformers shall be located in landscape areas that allow for screening on all sides; groundcover in front of access and shrubs on 3 sides. Coordinate with utility specialist.

12. Locate light standards, fire hydrants, water and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans.
13. Show all utilities on the landscape plans. Coordinate so utilities are clear of tree locations.
14. Coordinate with civil plans: Landscape slopes to be maximum 3:1; change all 2:1 landscape slopes to max. 3:1.
15. Provide larger canopy trees in larger open spaces.
16. Additional trees required; each lot shall have a minimum of one tree for front yards and one tree at alleys and private drives.
17. Provide a recycled/potable water meter exhibit showing what will be irrigated with what type of water, show meter type (recycled/potable) and locations; the Landscape Division will require a colored exhibit prior to plan check.
18. Relocate the "dog wash station" away from the park entries.
19. Typical lot drainage shall include a catch basin with gravel sump below each before exiting property; or connect to underground drainage system.
20. Residential projects shall include a 30" wide solid surface walkway (concrete, pavers, etc.) on at least one side to access the back yard and to move equipment or trash receptacles.
21. Provide phasing map for multi-phase projects.
22. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council. Fees are:

Plan Check—5 or more acres.....	\$2,326.00
Inspection—Construction (up to 3 inspections per phase).....	\$278.00
Total.....	\$2,604.00
Inspection—Field – any additional.....	\$83.00

Landscape construction plans with building permit number for plan check may be emailed to:

[landscapeplancheck@ontarioca.gov](mailto:landscapeplancheck@ontarioca.gov)

# AIRPORT LAND USE COMPATIBILITY PLANNING

## CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV19-064, PMTT19-020, PSPA19-007

Address: Northwest corner of Archibald Avenue and Eucalyptus Avenue

APN: 0218-231-12, 14, 17-21, 28-30, 33, 35 & 38

Existing Land Use: Vacant

Proposed Land Use: Parkside SPA to reconfigure Planning Areas and revise Residential/landscape standards; Subdivision for 56.99 acres into 16 lots; DP to construct 540 SF units

Site Acreage: 56.99 Proposed Structure Height: N/A

ONT-IAC Project Review: n/a

Airport Influence Area: ONT

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Rudy Zeledon

Date: 5/7/2020

CD No.: 2019-087

PALU No.: n/a

### The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="radio"/> High Terrain Zone	<input type="radio"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input type="checkbox"/> FAA Notification Surfaces	<input type="radio"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input type="radio"/> 65 - 70 dB CNEL	<input type="checkbox"/> Airspace Obstruction Surfaces	<input checked="" type="checkbox"/> Real Estate Transaction Disclosure
<input type="radio"/> Zone 3	<input type="radio"/> 60 - 65 dB CNEL	<input type="radio"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: <u>200 FT +</u>	
<input type="radio"/> Zone 5			

### The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1   
  Zone 2   
  Zone 3   
  Zone 4   
  Zone 5   
  Zone 6

Allowable Height: 110- 200 FT

## CONSISTENCY DETERMINATION

This proposed Project is:  Exempt from the ALUCP     Consistent     Consistent with Conditions     Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Real Estate Disclosure Required.

Airport Planner Signature: \_\_\_\_\_

# AIRPORT LAND USE COMPATIBILITY PLANNING

## CONSISTENCY DETERMINATION REPORT

CD No.: 2019-087  
PALU No.: \_\_\_\_\_

### PROJECT CONDITIONS

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the Airport Land Use Compatibility Plan (ALUCP) for ONT. The applicant is required to meet the Real Estate Transaction Disclosure in accordance with California Codes (Business and Professions Code Section 11010-11024). New residential subdivisions within an Airport Influence Area are required to file an application for a Public Report consisting of a Notice of Intention (NOI) and a completed questionnaire with the Department of Real Estate and include the following language within the NOI:

**NOTICE OF AIRPORT IN VICINITY:** This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Diana Ayala, Senior Planner

**FROM:** Officer Emily Hernandez, Police Department

**DATE:** December 3, 2019

**SUBJECT:** PDEV19-064 – A DEVELOPMENT PLAN TO CONSTRUCT 506 SINGLE-FAMILY DWELLING DETACHED HOMES LOCATED AT THE NORTHWEST CORNER OF EUCALYPTUS AVENUE AND ARCHIBALD AVENUE.

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The “Standard Conditions of Approval” contained in Resolution No. 2017-027 for “Ontario Ranch Projects” apply. The applicant shall read and be thoroughly familiar with these conditions, including but not limited to, the requirements listed below.

- Required lighting for all walkways, driveways, doorways, parking areas, and other areas used by the public shall be provided and operate on photosensor at the prescribed foot-candle levels. Photometrics shall be provided to the Police Department. Photometrics shall include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- The Applicant shall comply with all construction site security requirements as stated in the Standard Conditions. This includes the provisions for perimeter lighting, site lighting, fencing and/or uniformed security.

The Applicant is invited to contact Officer Emily Hernandez at (909)408-1755 with any questions or concerns regarding these conditions.



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Diane Ayala, Senior Planner  
Planning Department

**FROM:** Paul Ehrman, Deputy Fire Chief/Fire Marshal  
Fire Department

**DATE:** November 26, 2019

**SUBJECT:** PDEV19-064 – A Development Plan to construct 540 single-family homes on approximately 58 acres of land located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning Area 2 (PA-2) of the Parkside Specific Plan (APNs: 0218-231-12, 0218-231-14, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-29, 0218-231-30, 0218-231-33, and 0218-231-35). Related File: TT 18048

- 
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
- 

### **SITE AND BUILDING FEATURES:**

- A. 2016 CBC Type of Construction: Type V-B
- B. Type of Roof Materials: Ordinary
- C. Ground Floor Area(s): Varies
- D. Number of Stories: 2 - 3
- E. Total Square Footage: 1,090 to 1,790 Sq. Ft.
- F. 2016 CBC Occupancy Classification(s): R3

## **CONDITIONS OF APPROVAL:**

### **1.0 GENERAL**

- ☒ 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at [www.ontarioca.gov](http://www.ontarioca.gov), click on “Fire Department” and then on “Standards and Forms.”
- ☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

### **2.0 FIRE DEPARTMENT ACCESS**

- ☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- ☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- ☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- ☒ 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- ☒ 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- ☒ 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- ☒ 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services.

### **3.0 WATER SUPPLY**

- ☒ 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 1500 gallons per minute (g.p.m.) for 2 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- ☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- ☒ 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

### **4.0 FIRE PROTECTION SYSTEMS**

- ☒ 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- ☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.4 Wood frame buildings that are to be sprinkled shall have these systems in service (but not necessarily finalized) before the building is enclosed.

### **5.0 BUILDING CONSTRUCTION FEATURES**

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.
- ☒ 5.5 All residential chimneys shall be equipped with an approved spark arrester meeting the requirements of the California Building Code.





# CITY OF ONTARIO MEMORANDUM

**TO:** Scott Murphy, Development Director  
 Cathy Wahlstrom, Planning Director (Copy of memo only)  
 Diane Ayala, Advanced Planning Division (Copy of memo only)  
 Charity Hernandez, Economic Development  
 Kevin Shear, Building Official  
 Khoi Do, City Engineer  
 Jamie Richardson, Landscape Planning Division  
 Ahmed Aly, Municipal Utility Company  
 Emily Hernandez, Police Department  
 Paul Ehrman, Deputy Fire Chief/Fire Marshal  
 Jay Bautista, T. E., Traffic/Transportation Manager  
 Lorena Mejia, Airport Planning  
 Eric Woosley, Engineering/NPDES  
 Joe De Sousa, Code Enforcement (Copy of memo only)  
 Jimmy Chang, IT Department

**FROM:** Diane Ayala, Senior Planner

**DATE:** November 18, 2019

**SUBJECT:** FILE #: PDEV19-064 Finance Acct#:

The following project has been submitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by **Monday, December 2, 2019**.

- Note:**
- Only DAB action is required
  - Both DAB and Planning Commission actions are required
  - Only Planning Commission action is required
  - DAB, Planning Commission and City Council actions are required
  - Only Zoning Administrator action is required

**PROJECT DESCRIPTION:** A Development Plan to construct 540 single-family detached homes, on approximately 58-acres of land, located on the northwest corner of Merrill Ave & Archibald Ave, within Planning Area 2 (PA-2) of Parkside Specific Plan (APNs: 0218-231-12, -14, -17, -18, -19, -20, -21, -22, -28, -29, -30, -33 & -35).

**RELATED FILE:** Tract Map (TM 18048)

- The plan does adequately address the departmental concerns at this time.
  - No comments
  - Report attached (1 copy and email 1 copy)
  - Standard Conditions of Approval apply
- The plan does not adequately address the departmental concerns.
  - The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

Broadband Operations      *Anna Vaca*      Sr. Systems Analyst      12/06/2019  
 Department                      Signature                      Title                      Date

1. The City of Ontario is developing a fiber-optic telecommunications system throughout the city commonly known as OntarioNet. The fiber-optic telecommunications system is capable of providing advanced Internet/data services to homes and businesses in feasible areas within the city. OntarioNet will provide community related services including: traffic management; online civic services; meter reading; educational services; and a variety of other community services. OntarioNet and the high-speed data services it provides will keep the city on par with the modern workforce and ever changing lifestyles of the people and the community.
2. Communication systems proposed on-site facilities will be placed underground within a duct and structure system to be installed by the developer, as illustrated in Exhibit A, "Fiber Optics Plan". Maintenance of the installed system will be the responsibility of the City and/or Special District fiber optic entity and not that of the developer, private homeowners association or private homeowners. Development of the project requires the installation by the developer of all fiber optic infrastructure necessary to service the project as a standalone development.
3. The City requires public utility easement for fiber optics on all private aisles/alley ways.
4. Trenching, joint trenching, and boring shall be used to install the fiber-optic conduits. Fiber-optic conduit placement will generally be in a joint trench with Street Light conduits or in a separate trench/bore and in the Right-of-Way (ROW) generally placed behind the sidewalk. Resulting conduit placement will be on the north side of street and the east side of street based on the direction of the street. Properly sized handholes shall be placed along the conduit path no greater than 500-feet apart in major streets and no greater than 300-feet apart within in-tract community streets. Handholes shall be strategically placed to allow for efficient entrance into commercial buildings, and residential properties and multi-dwelling units.
5. Structured Wiring – An integrated structured wiring system (low-voltage wiring) provides infrastructure for today's technology applications and the framework for the future technology advances. Requirements and benefits of a structured wiring system include:
  - Residential (single-family and multi-family), commercial and industrial developments shall adhere to the City's Structured Wiring ordinance
  - Allows for uniform receipt & distribution of technology services
  - Ensures scalability of wiring for future technology advances
  - Provides consistent & identical wiring protocols throughout developments
  - Enables the property infrastructure to interface efficiently with broadband networks for highest bandwidth capacity
  - Adoption of these standards will minimize retrofitting required to ensure new property owners are capable of the latest technologies and services
6. Building Entrance (Multi-family) - Design and install fiber optic conduit at a minimum depth of 36 inches. Trenching shall be per City Standard for Commercial Buildings. (1) 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct. Install locate/tracer wires minimum 12AWG within conduit bank and fiber warning tape 18-inch above the uppermost duct.
7. Multi-family and commercial properties shall terminate conduit in an electrical room adjacent to the wall no less than five inches above the finished floor. A 20" width X length 36" space shall be reserved on the plywood wall for OntarioNet equipment. This space shall be labeled "OntarioNet Only". Ontario Conduit shall be labeled "OntarioNet"
8. A minimum 1.5-inch joint use telecommunications conduit with pull-rope from the multi-family or commercial building communal telecom/electrical room/closet to each multi-family or commercial building unit shall be installed. See Structured Wiring Checklist on City's website for additional details.
9. A Fiber Optics Improvement Design Plan sheet should be part of the Design Plan submission and should be provided in digital format (PDF) as well, on future revisions

# CITY OF ONTARIO

## MEMORANDUM

**TO:** PLANNING DEPARTMENT, Diane Ayala  
**FROM:** BUILDING DEPARTMENT, Kevin Shear  
**DATE:** November 19, 2019  
**SUBJECT:** PDEV19-064

- 
- The plan **does** adequately address the departmental concerns at this time.  
 No comments  
 Report below.

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### Conditions of Approval

1. Standard Conditions of Approval apply.

KS:lr



# CITY OF ONTARIO

## MEMORANDUM

### ENGINEERING DEPARTMENT CONDITIONS OF APPROVAL

(Environmental Section, Information Technology & Management Services Department, Ontario Municipal Utilities Company and Traffic & Transportation Division Conditions incorporated)

**DATE:** May 13, 2020

**PROJECT:** PDEV19-064 - A Development Plan to construct 540 single-family homes on approximately 58 acres of land within Planning Areas 1-4 of the Parkside Specific Plan; Related File: TT20316, TM-18048, PMTT19-020

**APN:** 0218-231-12; 0218-231-14; 0218-231-17 to 0218-231-22; 0218-231-28 to 0218-231-22; 0218-231-33; 0218-231-35

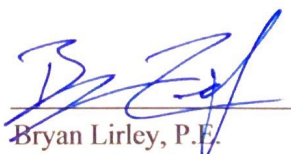
**LOCATION:** Northwest Corner of Eucalyptus Avenue and Archibald Avenue

**PROJECT ENGINEER:** Jesus Plasencia, Senior Associate Civil Engineer (909) 395-2128

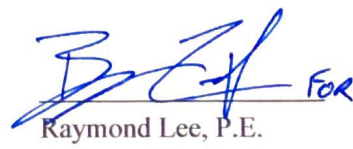
**PROJECT PLANNER:** Diane Ayala, Senior Planner (909) 395-2428

The following items are the Conditions of Approval for the subject project:

1. Project shall comply with the requirements as set forth in the Amendment to the Standard Conditions of Approval for New Development Projects adopted by the City Council (Resolution No. 2017-027) on April 18, 2017.
2. The project shall be subject to completion of the required public improvements as specified in the Parkside Specific Plan, the Development Agreement, and the Conditions of Approval for TM-20316 and TM-18048.
3. The Applicant/Developer shall pay all Development Impact Fees (DIF) to the Building Department.
4. The applicant/developer shall prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required.

  
Bryan Lirley, P.E.  
Principal Engineer

5/13/20  
Date

 FOR  
Raymond Lee, P.E.  
Assistant City Engineer

5/13/20  
Date



# PLANNING COMMISSION STAFF REPORT

May 26, 2020

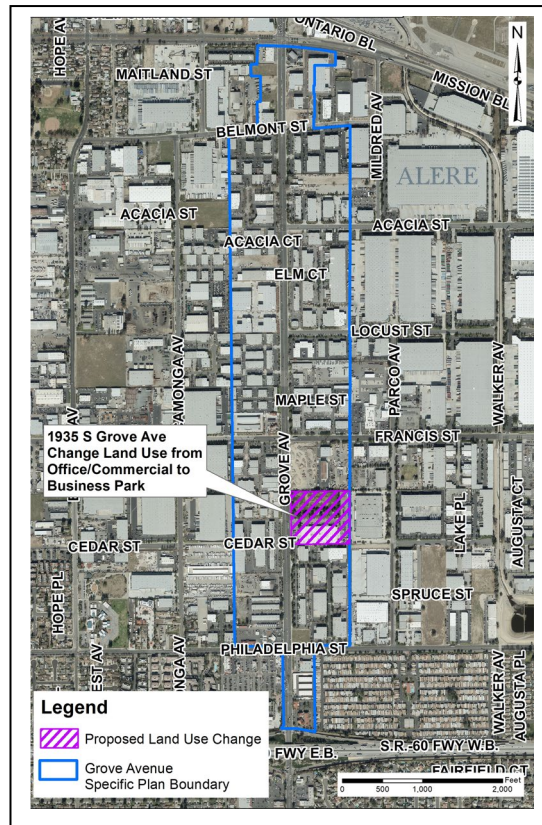
**FILE NO.:** PSPA20-004

**SUBJECT:** An Amendment to the Grove Avenue Specific Plan to: (1) establish consistency with the Ontario International Airport Land Use Compatibility Plan; (2) change the land use designation on 11.1 acres of land located at 1935 South Grove Avenue, from Office/Commercial to Business Park, to establish consistency with The Ontario Plan Policy Plan (Exhibit LU-01, Land Use Plan); and (3) Amend the Specific Plan’s Business Park development standards to be consistent with the Ontario Development Code development standards for the Business Park zoning district (APN: 113-451-14); **City Initiated. City Council action is required.**

**PROPERTY OWNER:** Multiple Property Owners

**RECOMMENDED ACTION:** That the Planning Commission recommend that the City Council approve File No. PSPA20-004, pursuant to the facts and reasons contained in the staff report and attached resolution.

**PROJECT SETTING:** The Grove Avenue Specific Plan encompasses approximately 250 acres of land located along both sides of Grove Avenue, bounded by Mission Boulevard to the north and the Pomona Freeway (State Route 60) on the south. The majority of the Specific Plan is presently built-out with a combination light industrial, office and commercial uses. There are a few vacant parcels and underutilized lots remaining within the Specific Plan. The property, located at 1935 South Grove Avenue, is presently developed with an approximate 100,000 square foot prefabricated metal building developed in the 1970s and an employee open space area. The Grove Avenue Specific Plan boundary and the location of 1935 South Grove Avenue is depicted in Figure 1: Project Location, right.



**Figure 1: Project Location**

**PROJECT ANALYSIS:**

Case Planner:	Lorena Mejia
Planning Director Approval:	
Submittal Date:	April 9, 2020

Hearing Body	Date	Decision	Action
DAB	n/a	n/a	
PC	5/26/2020		Recommend
CC			Final

[1] Background — On May 18, 1993, the City Council approved the Grove Avenue Specific Plan (File No. 4388-SP) and certified the Grove Avenue Corridor Specific Plan Environmental Impact Report (EIR 90-2). The Grove Avenue Specific Plan established the land use designations, development standards, and design guidelines for approximately 250 acres of land, which included the potential development of 2.9 million square feet of light industrial, office/commercial and commercial land uses. Furthermore, the Specific Plan included an Airport Environs Element that established height limitations, floor area ratio restrictions, land use restrictions, safety zones, and policies/standards to protect businesses and residents from Ontario International Airport safety and noise impacts.

In 2010, The Ontario Plan (“TOP”) was adopted by the City Council, approving the Policy Plan (General Plan) Land Use Plan (Exhibit LU-01) that changed the land use designations of several properties within certain areas of the Grove Avenue Specific Plan, to Business Park. To bring the Grove Avenue Specific Plan into conformance with the Policy Plan, a land use amendment to the Grove Avenue Specific Plan is needed to establish land use consistency with the Policy Plan.

On April 19, 2011, the Ontario City Council adopted the Airport Land Use Compatibility Plan for Ontario International Airport (“ONT ALUCP”). The ONT ALUCP established citywide policies and criteria to mitigate ONT safety, noise, airspace, and overflight impacts for new development.

[2] Amendment to the Grove Avenue Specific Plan — The proposed amendment to the Grove Avenue Specific Plan (SPA) proposes the following:

[a] *Establish consistency between the Grove Avenue Specific Plan and the Ontario International Airport Land Use Compatibility Plan (ONT ALUCP).*

[i] State Law requires that General Plans and Specific Plans must be made consistent with an adopted airport land use compatibility plan. The City adopted the ONT ALUCP in 2011, at which time it became the governing document for new development within the City. Furthermore, all discretionary and legislative projects are required to have a Consistency Determination that evaluates and determines whether a project is consistent with the policies and criteria of the ONT ALUCP. The City of Ontario updated the General Plan for consistency with the ONT ALUCP on June 21, 2011 and has updated several specific plans to be consistent with the ONT ALUCP. The programmatic updates to Specific Plans that are necessary to achieve ONT ALUCP consistency, eliminates any inconsistent or outdated language regarding ONT impacts and/or incorporates policy language into the Specific Plans to refer to the ONT ALUCP for additional development criteria and restrictions. By incorporating policy language referencing the ONT ALUCP in Specific Plans, it eliminates the need to amend any Specific Plan in the future should the ONT ALUCP be amended.

[ii] The amendment to the Grove Avenue Specific Plan includes the elimination of the Airport Environs Element, the Business Park Airport Approach (AA) Overlay Zone Regulations, and associated text changes, exhibits and figures. The amendment also includes policy language referencing the ONT ALUCP for additional development criteria and restrictions, where applicable (see Attachment A: Grove Avenue Specific Plan Amendment Document).

[b] *Change the land use designation for 11.1 acres of land located at 1935 South Grove Avenue from Office/Commercial to Business Park to establish consistency with The Ontario Plan Policy Plan Land Use Plan (Exhibit LU-01).*

The adoption of TOP changed the land use designation of 1935 South Grove Avenue from Commercial to Business Park. To date, the City has been processing land use changes within Specific Plans to achieve consistency with the Policy Plan Land Use Plan on an as needed basis, when new development projects are proposed and land use inconsistencies must be addressed. A Development Plan application for the subject site (1935 South Grove Avenue) has been submitted to the City and will be brought forward at a future date for Planning Commission review and action. The subject site presently has a Specific Plan land use designation of Office/Commercial, which is inconsistent with the Business Park land use designation assigned to the property by the Policy Plan Land Use Plan. To establish consistency between the two land use plans and facilitate the proposed development, the Grove Avenue Specific Plan land use designation on the subject site will be changed from Office/Commercial to Business Park (see Exhibit A—Grove Avenue Specific Plan Map, attached).

[c] *Revise the Specific Plan's Business Park development standards to be consistent with the Ontario Development Code's Business Park zoning district.*

Development along the Grove Avenue Specific Plan has resulted in several Variance requests since the Ontario Development Code was comprehensively revised in 2016, which in part, modified the industrial building standards to allow for less restrictive building setback standards and increased floor area ratios ("FARs") than those required by the Grove Avenue Specific Plan. The majority of the Grove Avenue Specific Plan is presently built-out and the remaining undeveloped/underutilized parcels are smaller in size and have narrow lot width conditions. The restrictive nature of the Specific Plan's development standards limits the viability of development projects within the Specific Plan area, making portions of the Grove Avenue Specific Plan less desirable for new development, as compared to industrial properties outside of the Specific Plan area. As such, Staff is proposing to amend the Specific Plan's Business Park development standards to align with the Ontario Development Code's development standards for the Business Park zoning district (see Attachment A: Grove Avenue Specific Plan Amendment Document). Proposed changes to the Specific Plan's Business Park development standards are as follows:

[i] Change the 10-foot rear and side yard building setback to a zero-foot building setback.

[ii] Change the local street building setback from 30-feet to 20-feet. The Development Code requires properties within the Business Park zoning district to maintain a 10-foot setback from the front property line for local streets and a 20-foot setback for arterial streets.

[iii] Increase the maximum FAR from 0.25 and 0.35, to 0.40, which is consistent with the Policy Plan's assumed build-out intensity. The restrictive FAR (0.25 and 0.35) standards were established to limit the number of people within the vicinity, due to safety impacts from ONT. The adoption of the ONT ALUCP established new standards for FAR restrictions within the City that supersedes the Grove Avenue Specific Plan standards. The new FAR restrictions limit the number of people in close proximity to the airport, based on land use. Lower intensity uses, such as warehouses, are allowed to have a higher FAR, while higher intensity land uses, such manufacturing, are required to have a lower FAR.

[iv] Various text and exhibit changes throughout the Specific Plan document, as required to reflect the aforementioned modified development standards.

All changes and additions to the Specific Plan (exhibits, tables and development standards) are contained within the revised Specific Plan document accompanying this report. All additions to the Specific Plan have been highlighted in red.

**COMPLIANCE WITH THE ONTARIO PLAN:** The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods

[2] Governance.

**Decision Making:**

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.



➤ G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

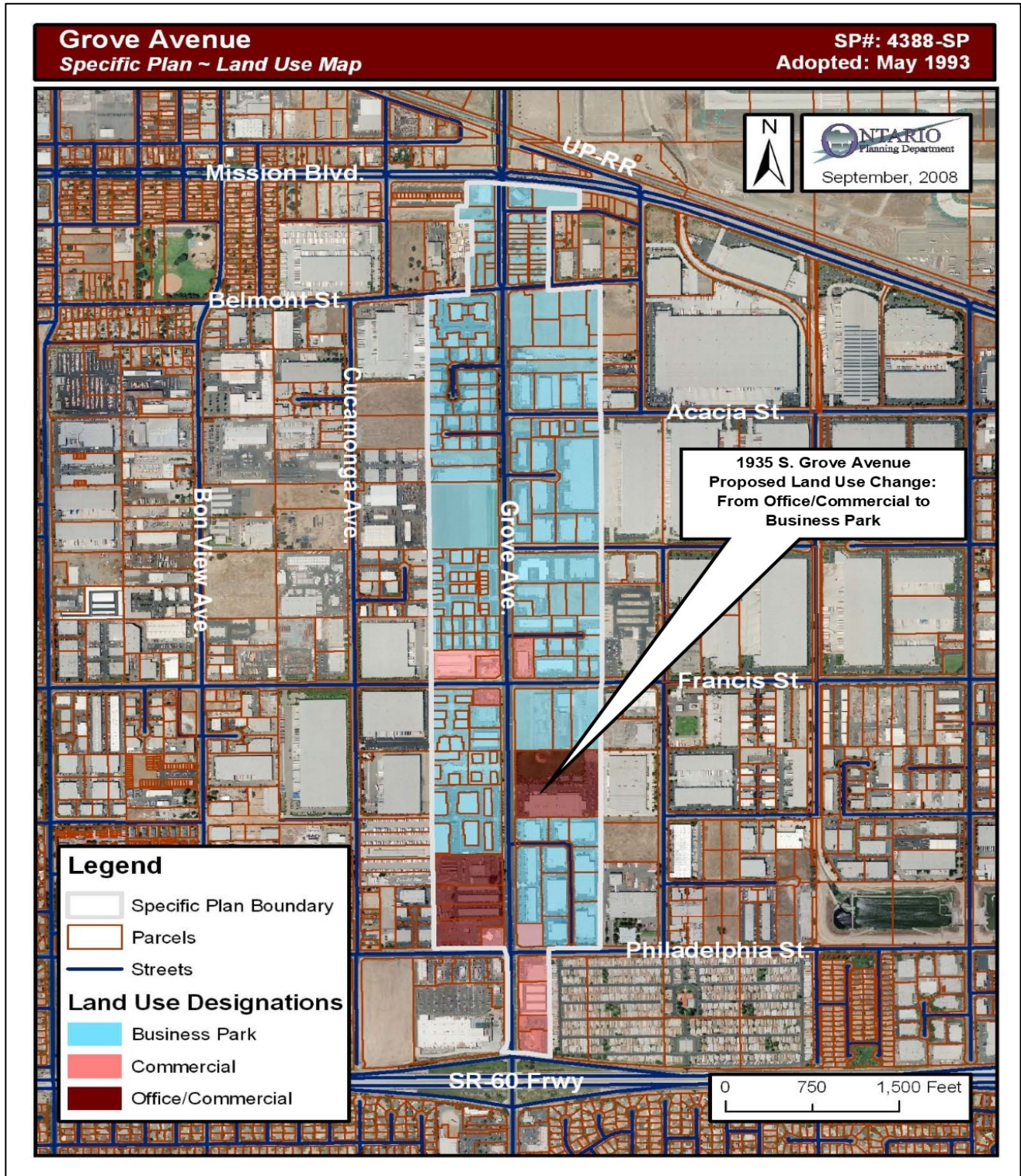
**HOUSING ELEMENT COMPLIANCE:** The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

**AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE:** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

**ENVIRONMENTAL REVIEW:** The environmental impacts of this Project were previously analyzed with The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140), certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts.

The ONT ALUCP-related environmental impacts of this project were previously reviewed in conjunction with File No. PADV07-008, for which a Negative Declaration (State Clearinghouse No. 2011011081) was adopted by the Ontario City Council on April 19, 2011. This application introduces no new significant environmental impacts.

**Exhibit A—Grove Avenue Specific Plan Map**



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL APPROVE FILE NO. PSPA20-004, AN AMENDMENT TO THE GROVE AVENUE SPECIFIC PLAN TO: (1) ESTABLISH CONSISTENCY WITH THE ONTARIO INTERNATIONAL AIRPORT LAND USE COMPATIBILITY PLAN; (2) CHANGE THE LAND USE DESIGNATION ON 11.1 ACRES OF LAND LOCATED AT 1935 SOUTH GROVE AVENUE, FROM OFFICE/COMMERCIAL TO BUSINESS PARK, TO ESTABLISH CONSISTENCY WITH THE ONTARIO PLAN POLICY PLAN (EXHIBIT LU-01, LAND USE PLAN); AND 3) AMEND THE SPECIFIC PLAN'S BUSINESS PARK DEVELOPMENT STANDARDS TO BE CONSISTENT WITH THE ONTARIO DEVELOPMENT CODE DEVELOPMENT STANDARDS FOR THE BUSINESS PARK ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 113-451-14.

WHEREAS, the City of Ontario has initiated a Specific Plan Amendment, File No. PSPA20-004, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 250 acres of land generally located along the east and west sides of Grove Avenue, between Mission Boulevard to the north and the Pomona Freeway (CA 60) to the south; and

WHEREAS, the majority of the Grove Avenue Specific Plan is presently built-out with a combination light industrial, office, and commercial land uses, and a few vacant and underutilized parcels remaining within the Specific Plan; and

WHEREAS, on May 18, 1993, the City Council approved the Grove Avenue Specific Plan (File No. 4388-SP) and certified the Grove Avenue Corridor Environmental Impact Report (EIR 90-2); and

WHEREAS, in 2010, The Ontario Plan ("TOP") was adopted by the City Council, approving the. TOP Policy Plan (General Plan) Land Use Plan (Policy Plan Exhibit LU-01) that changed the land use designations of several properties within certain areas of the Grove Avenue Specific Plan; and

WHEREAS, on April 19, 2011 the Ontario City Council adopted the Airport Land Use Compatibility Plan for Ontario International Airport ("ONT ALUCP"). The ONT ALUCP established citywide policies and criteria to mitigate ONT safety, noise, airspace, and overflight impacts for new development; and

WHEREAS, the Amendment to the Grove Avenue Specific Plan (SPA) proposes to establish consistency between the Grove Avenue Specific Plan and the Ontario International Airport Land Use Compatibility Plan; and

WHEREAS, State Law requires General Plans and Specific Plans must be made consistent with an adopted airport land use compatibility plan; and

WHEREAS, the amendment to the Grove Avenue Specific Plan includes the elimination of the Airport Environs Element, the Business Park Airport Approach (AA) Overlay Zone Regulations, and associated text changes, exhibits, and figures. The amendment includes policy language referencing the ONT ALUCP for additional development criteria and restrictions where applicable; and

WHEREAS, the Amendment to the Grove Avenue Specific Plan proposes to change the land use designation on 11.1 acres of land located at 1935 South Grove Avenue, from Office/Commercial to Business Park, to be consistent with TOP Policy Plan; and

WHEREAS, the Amendment to the Grove Avenue Specific Plan proposes to revise the Specific Plan's Business Park development standards to be consistent with the Ontario Development Code's development standards for the Business Park zoning district. The amendment includes changing the 10-foot rear and side yard building setback to a zero-foot building setback, changing the 30-foot local street building setback to 20-feet and an increase in the maximum allowed FAR in the Business Park land use district, from 0.25 and 0.35, to 0.40, which is consistent with the Policy Plan's assumed buildout intensities. All changes and additions to the Specific Plan (exhibits, tables, development standards and design guidelines) are contained within the revised Grove Avenue Specific Plan document accompanying this Resolution (Attachment "A"). All additions to the Specific Plan have been highlighted in red; and

WHEREAS, the Amendment to the Grove Avenue Specific Plan proposes text and exhibit changes throughout the document to reflect the modified development standards; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — ("CEQA"); and

WHEREAS, the environmental impacts of this project were previously reviewed in conjunction with The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the City Council on January 27, 2010, in conjunction with File No. PGPA06-001 (hereinafter referred to as "Certified EIR"); and

WHEREAS, the environmental impacts of this project were previously reviewed in conjunction the ONT ALUCP (File No. PADV07-008), for which a Negative Declaration (State Clearinghouse No. 2011011081) was adopted by the Ontario City Council on April 19, 2011 (hereinafter referred to as "ND"); and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make recommendation to City Council on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan, which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 26, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

**SECTION 1: *Environmental Determination and Findings.*** As the recommending authority for the Project, the Planning Commission has reviewed and considered the information contained in the previous Certified EIR, ND, and supporting documentation. Based upon the facts and information contained in the previous Certified EIR, ND, and supporting documentation, the Planning Commission finds as follows:

(1) The environmental impacts of this project were previously reviewed in conjunction with The Ontario Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001, and the ONT ALUCP, for which a Negative Declaration (State Clearinghouse No. 2011011081) was adopted by the Ontario City Council on April 19, 2011, in conjunction with File No. PADV07-008; and

(2) The previous Certified EIR and ND contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous Certified EIR and ND were completed in compliance with CEQA and the Guidelines promulgated thereunder; and

(4) The use of the previous Certified EIR and ND reflect the independent judgment of the Planning Commission; and

(5) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the previous Certified EIR and ND, and all mitigation measures previously adopted with the Certified EIR and ND, are incorporated herein by this reference.

**SECTION 2: *Subsequent or Supplemental Environmental Review Not Required.*** Based on the information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR and ND is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR and ND that will require major revisions to the Certified EIR and ND due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR and ND was prepared, that will require major revisions to the Certified EIR and ND due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR and ND was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the Certified EIR and ND; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR and ND; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR and ND would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

**SECTION 3: *Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the recommending authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

**SECTION 4: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP

Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

**SECTION 5: Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed Specific Plan, or amendment thereto, is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Amendment to the Grove Avenue Specific Plan will bring the Specific Plan in to conformance with TOP's Policy Plan Land Use Plan (Policy Plan Exhibit LU-01) and the ONT ALUCP.

(2) ***The proposed Specific Plan, or amendment thereto, would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City.*** The proposed amendments to the Grove Avenue Specific Plan establish conformance with the Policy Plan Land Use Plan and Policy Plan goals and policies, in accordance with State law. Furthermore, the proposed amendment to the Grove Avenue Specific Plan will not be detrimental to the public interest, health, safety, convenience, or general welfare of the City because it will provide for additional commercial and industrial within the Grove Avenue Specific Plan, which is consistent with the type and intensity of development specified in The Ontario Plan and evaluated by The Ontario Plan Environmental Impact Report.

(3) ***In the case of an application affecting specific property(ies), the proposed Specific Plan, or amendment thereto, will not adversely affect the harmonious relationship with adjacent properties and land uses.*** The Project site, 1935 South Grove Avenue, is located in an area that will be developed with industrial uses, which will be complimentary and harmonious to the surrounding area.

(4) ***In the case of an application affecting specific property(ies), the subject site is physically suitable, including, but not limited to, parcel size, shape, access, and availability of utilities, for the request and anticipated development.*** The proposed amendment to the Grove Avenue Specific Plan Business Park development standards will facilitate future development within the Business Park land use district by providing greater site design flexibility through the introduction of reduced setback standards and increased FAR allowances, which are consistent with those required by the Ontario Development Code, within the Business Park zoning district. With the approval of the proposed amendment, future projects will be developed with adequate lot size, access, and utilities to serve the projects.



**SECTION 6: *Planning Commission Action.*** Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL APPROVE the herein described Specific Plan Amendment, as specified in "Attachment A," and incorporated herein by this reference.

**SECTION 7: *Indemnification.*** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

**SECTION 8: *Custodian of Records.*** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

**SECTION 9: *Certification to Adoption.*** The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of May 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

---

Jim Willoughby  
Planning Commission Chairman

ATTEST:

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Cathy Wahlstrom  
Planning Director and  
Secretary to the Planning Commission

STATE OF CALIFORNIA                    )  
COUNTY OF SAN BERNARDINO        )  
CITY OF ONTARIO                        )

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. \_\_\_\_\_ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 26, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

---

Gwen Berendsen  
Secretary Pro Tempore

**ATTACHMENT A:**

**File No. PSPA20-004  
Grove Avenue Specific Plan**

*(Document to follow this page)*

# **GROVE AVENUE SPECIFIC PLAN**

**FILE NO. 4388- SP**

**(Amended by File No., 4663-SPA)**

**(Amended by File No. PSPA17-002)**

**Prepared for:**

**City of Ontario  
303 East B Street  
Ontario, California 91764-4196**

**Prepared by:**

**The PRS Group  
27132-B Paseo Espada, Suite 1222  
San Juan Capistrano, California 93675**

## CITY COUNCIL

James R. Fatland, Mayor  
Gus Skropos, Mayor Pro Tempore  
Jim Bowman  
Rudy Favila  
Gary Ovitt

## PLANNING COMMISSION

Sheila Mautz, Chairman  
Frank J. Lizarraga, Jr., Vice Chairman  
Kate Cerda-Núñez  
Matthew John Fertal  
Reverend Charles Mabry  
James D. Maletic  
William C. McCaughey

## CITY MANAGER

G. Michael Milhiser

## DEVELOPMENT ADVISORY BOARD

Community Economic Development/Redevelopment	Norm Priest
Community Development	Byron Ely
Building Department	Tony Lopez
Planning Department	Otto Kroutil
Engineering Department	LeRoy Bender
Public Facilities Development	Sam Gutierrez
Police Department	Patrick Sanford
Fire Department	Bill Alexander
Transportation Projects Manager	Kim Shultz
Code Enforcement	Fred Stillions

CITY OF ONTARIO, FILE NO. 4388-SP  
(AMENDED BY FILE NO. 4663-SPA)

ACCEPTED BY THE DEVELOPMENT ADVISORY BOARD

Community Development \_\_\_\_\_

Building \_\_\_\_\_

Engineering \_\_\_\_\_

Planning \_\_\_\_\_

Public Services \_\_\_\_\_

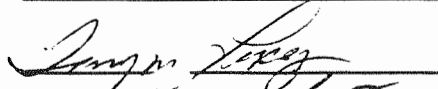
Redevelopment \_\_\_\_\_

Fire \_\_\_\_\_

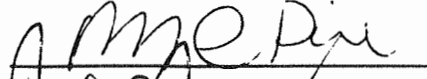
Police \_\_\_\_\_

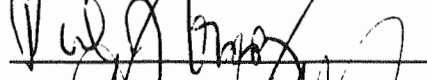
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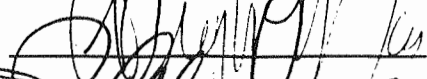
Code Enforcement \_\_\_\_\_








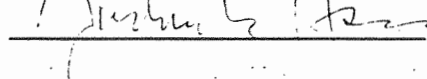












## ADOPTION

### **Resolution No. 93-50 (Adopted May 18, 1993)**

*The Specific Plan was established with adoption of Resolution No. 93-50 setting forth the development intensities, land uses, development standards, design guidelines, and infrastructure requirements for the approximately 250 acre project area.*

## AMENDMENTS

### **Resolution No. 93-103 (Adopted September 7, 1993)**

*The Specific Plan was amended to permit various minor adjustments to the regulations regarding floor area ratios, land uses, development standards, and design guidelines.*

### **Resolution No. 2017-092 (Adopted July 18, 2017)**

The Specific Plan was amended to change the land use designation from Business Park to Commercial for approximately one acre of land located at the northeast corner of Grove Avenue and Philadelphia Street and amendments to the Commercial Land Use Table.



**RESOLUTION NO. 93-50**

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA APPROVING A SPECIFIC PLAN KNOWN AS THE GROVE AVENUE SPECIFIC PLAN SETTING FORTH THE DEVELOPMENT INTENSITIES, LAND USES, DEVELOPMENT STANDARDS, DESIGN GUIDELINES, AND INFRASTRUCTURE REQUIREMENTS FOR AN APPROXIMATELY 250 ACRE SITE LOCATED ALONG BOTH SIDES OF GROVE AVENUE, BOUNDED BY MISSION BOULEVARD ON THE NORTH AND THE POMONA FREEWAY (STATE ROUTE 60) ON THE SOUTH**

**WHEREAS**, the City Council on this date held a duly noticed public hearing to consider and review a proposed Specific Plan, File No. 4388-SP, and related applications, which have been recommended for approval by the Planning Commission at its public hearing on March 23, 1993; and

**WHEREAS**, the Specific Plan would establish development intensities, allowable land uses, development standards, design guidelines, and infrastructure requirements for the project site; and

**WHEREAS**, the Specific Plan would permit development of a 2.9 million square foot Business Park with light industrial, office/commercial, and commercial land uses; and

**WHEREAS**, the Specific Plan would implement the goals and policies of the Ontario General Plan by establishing superior design guidelines and development standards for the Grove Avenue Corridor, while permitting greater flexibility in the allowable land uses; and

**WHEREAS**, the Specific Plan would result in the establishment of regulations and requirements in conformance with the spirit, intent, and provisions of the Ontario Municipal Code; and

**WHEREAS**, the properties within the project area would have to be developed in conformance with the Specific Plan and in compliance with the conditions of approval, as recommended by the Planning Commission and the Development Advisory Board; and

**WHEREAS**, development of the properties within the Specific Plan would be an economic benefit to the City because of the estimated 1.58 revenue - cost ratio as determined in the fiscal impact report prepared for this project; and


WHEREAS, the City Council has reviewed and certified Environmental Impact Report No. 90-2 and has required incorporation of all of its mitigation measures as part of the project, thereby reducing all of the projects effects on the public health, safety, and welfare to acceptable levels (including public education which is to receive all legally established school impact fees to off-set any effect the project may have on school-facilities).

**NOW, THEREFORE, BE IT RESOLVED, THAT:**

1. The Grove Avenue Specific Plan is beneficial to the City; and
2. The public health, safety, welfare (including public education), and quality of life is not adversely impacted through approval of the Grove Avenue Specific Plan; and
3. The Grove Avenue Specific Plan, File No. 4388-SP, is approved.

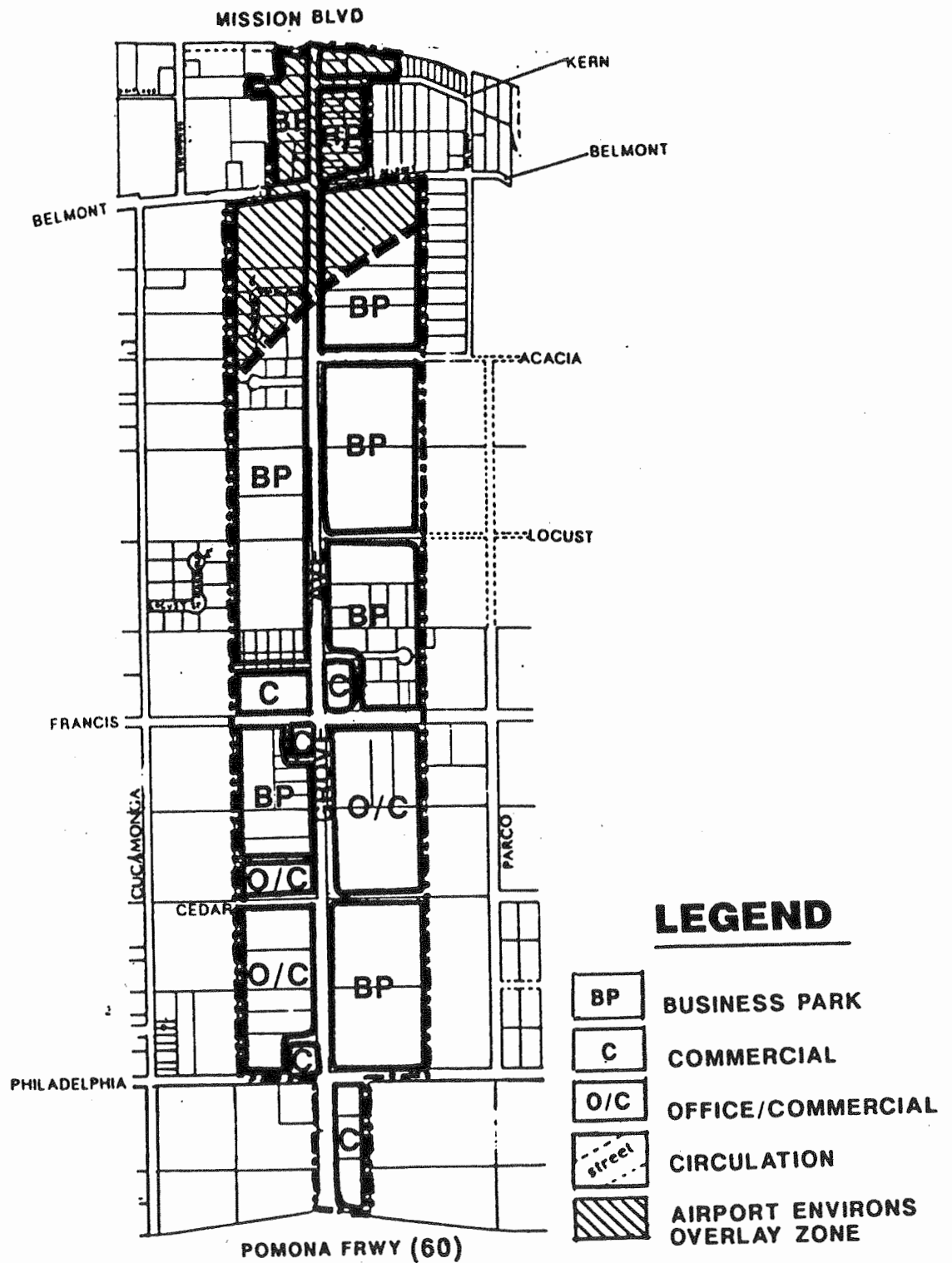
I hereby certify that this resolution was duly passed and approved by the City Council of the City of Ontario at a regular meeting held on the 18th day of May, 1993.



  
City Clerk of the City of Ontario

# ONTARIO PLANNING DEPARTMENT

## EXHIBIT 4: LAND USE PLAN



FILE NO. 4388-SP

**RESOLUTION NO. 93-103**

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA APPROVING AN AMENDMENT TO THE GROVE AVENUE SPECIFIC PLAN RELATING TO DEVELOPMENT INTENSITIES, ALLOWABLE LAND USES, DEVELOPMENT STANDARDS, DESIGN GUIDELINES, AND LAND USE DESIGNATIONS ON AN APPROXIMATELY 250 ACRE SITE LOCATED ALONG BOTH SIDES OF GROVE AVENUE, BOUNDED BY MISSION BOULEVARD ON THE NORTH AND THE POMONA FREEWAY (STATE ROUTE 60) ON THE SOUTH**

**WHEREAS**, the City Council on this date held a duly noticed public hearing to review and consider amending certain items of the Grove Avenue Specific Plan, File No. 4663-SPA, which the Planning Commission has recommended for approval at its public hearing on July 27, 1993; and

**WHEREAS**, this Specific Plan Amendment would permit the following:

1. An increase in the maximum floor area ratio in the Business Park category, subject to a traffic generation analysis;
2. Service stations with mini-marts as conditional uses in the Commercial category;
3. Warehousing and light distribution if it is ancillary to a permitted or conditionally permitted use in the Business Park category;
4. Individual projects to vary from the prescribed rear and interior side yard setbacks with the approval of the Planning Commission if it can be demonstrated that the variations would result in a superior design;
5. Limited outdoor storage if it is ancillary to a permitted or conditionally permitted business, subject to certain design requirements; and
6. Redesignation of the Lee property on the south of Francis Street and east of Grove Avenue as "Business Park"; and

**WHEREAS**, a provision would be added to the Specific Plan requiring that all freestanding walls be finished with graffiti resistant paint or otherwise be graffiti-proofed and maintained graffiti free; and

**WHEREAS**, the Specific Plan Amendment affects the entire 2.9 million square foot Grove Avenue Corridor Business Park which is to include light industrial, office/commercial, and commercial land uses; and

**WHEREAS**, the Specific Plan Amendment is consistent with the goals and policies of the Ontario General Plan because the superior design guidelines and development standards for the Grove Avenue Corridor are being maintained, while greater flexibility in the allowable land uses is being permitted; and

**WHEREAS**, the Specific Plan Amendment is in conformance with the spirit, intent, and provisions of the Ontario Municipal Code; and

**WHEREAS**, the properties within the project area would have to be developed in conformance with the Specific Plan and in compliance with the conditions of approval, as recommended by the Planning Commission and the Development Advisory Board with the establishment of the Specific Plan in May of 1993; and

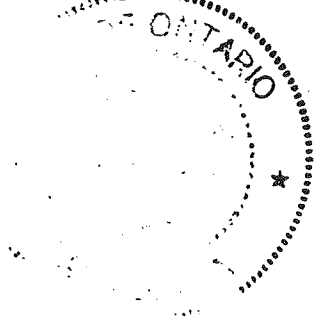
**WHEREAS**, a program Environmental Impact Report, EIR #90-2, was completed and certified for the Grove Avenue Specific Plan which adequately describes the proposed Specific Plan Amendment for the purposes of the California Environmental Quality Act; and


**WHEREAS**, because no new effects would occur as a result of the proposed Specific Plan Amendment besides those already evaluated in EIR #90-2, no additional environmental documentation is required.

**NOW, THEREFORE, BE IT RESOLVED**, that the City Council hereby approves the Specific Plan Amendment, File No. 4663-SPA. It is further resolved that automobile service centers and used automobile sales and auctions are not appropriate land uses for the Grove Avenue Corridor.

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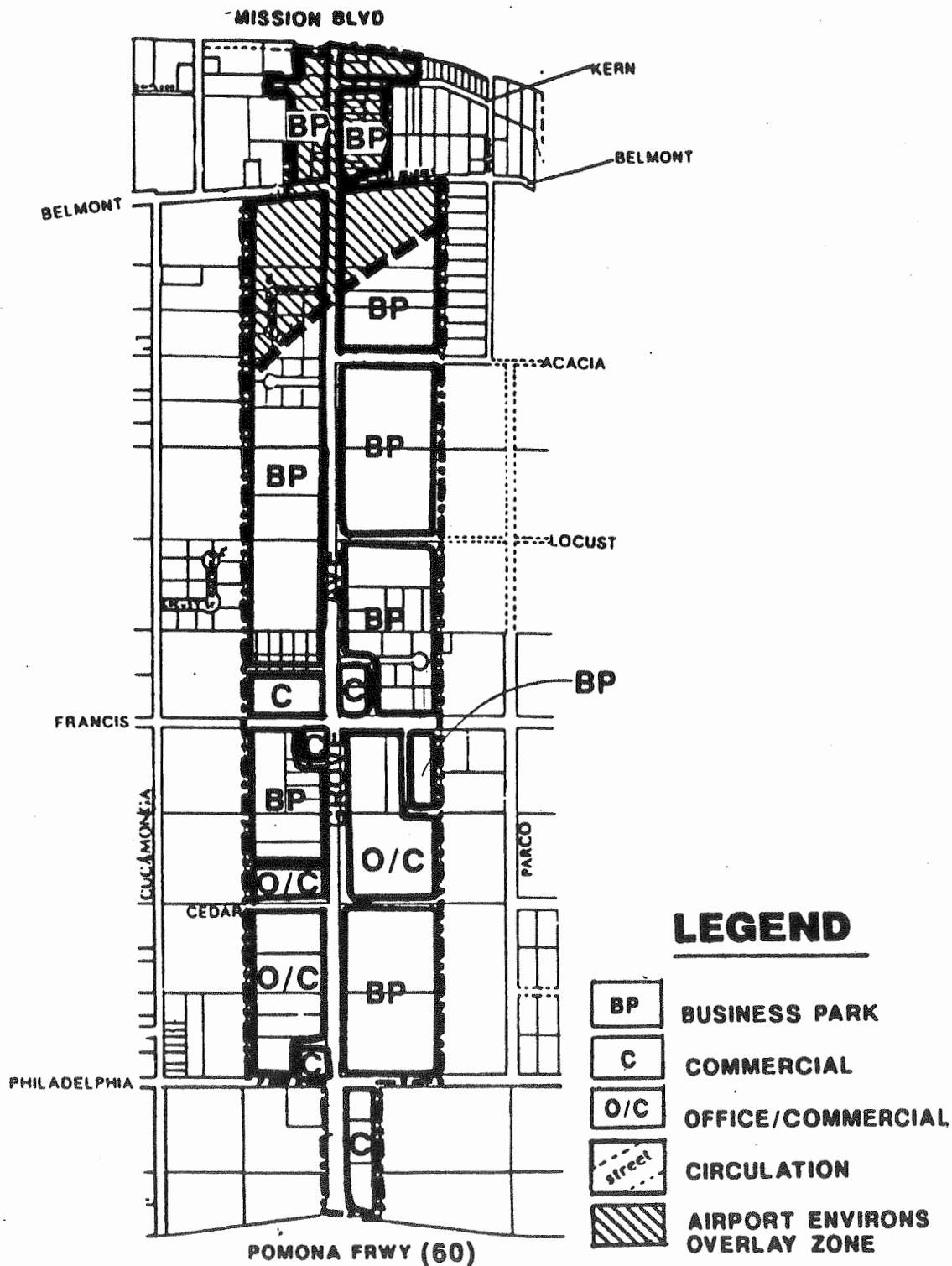
I hereby certify that this resolution was duly passed and approved by the City Council of the City of Ontario at a regular meeting held on the 7th day of September, 1993.



  
City Clerk of the City of Ontario

# ONTARIO PLANNING DEPARTMENT

## LAND USE PLAN



FILE NO. 4633-SPA

RESOLUTION NO. 2017-092

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PSPA17-002, AN AMENDMENT TO THE GROVE AVENUE SPECIFIC PLAN (FILE NO. PSPA17-002) TO: 1) CHANGE THE LAND USE DESIGNATION FROM BUSINESS PARK TO COMMERCIAL FOR APPROXIMATELY ONE ACRE OF LAND LOCATED AT THE NORTHEAST CORNER OF GROVE AVENUE AND PHILADELPHIA STREET; 2) AMEND THE COMMERCIAL DISTRICT PERMITTED AND CONDITIONALLY PERMITTED USES TO ALLOW RESTAURANTS, CONDITIONALLY PERMIT RESTAURANTS WITH A DRIVE-THRU FACILITY AND CONDITIONALLY PERMIT CAR WASH FACILITIES (FULL AND SELF-SERVICE) WITH THE EXCEPTION OF THE COMMERCIAL CORNERS OF GROVE AVENUE AND PHILADELPHIA STREET AND GROVE AVENUE AND FRANCIS STREET WHERE FULL AND SELF-SERVICE CAR WASH FACILITIES WILL NOT BE PERMITTED; AND 3) UPDATE ALL APPLICABLE SPECIFIC PLAN SECTIONS TO REFLECT THE PROPOSED AMENDMENTS. THE GROVE AVENUE SPECIFIC PLAN IS GENERALLY LOCATED ON THE EAST AND WEST SIDES OF GROVE AVENUE AND BETWEEN MISSION BOULEVARD TO THE NORTH AND THE I-60 FREEWAY TO THE SOUTH, AND MAKING FINDINGS IN SUPPORT THEREOF — APN: 0113-641-13.

WHEREAS, A&E Leasing, LLC ("Applicant") has filed an Application for the approval of an Amendment to the Grove Avenue Specific Plan, File No. PSPA17-002, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to approximately one acre of land located at the northeast corner of Grove Avenue and Philadelphia Street within the Business Park land use designation of the Grove Avenue Specific Plan and is presently vacant; and

WHEREAS, the properties to the north and east of the project site are developed with an existing business park development and located within the Business Park Planning Areas of the Grove Avenue Specific Plan. The property to the south is developed with an existing Office Depot and located within the Commercial Planning Area of the Grove Avenue Specific Plan. The property to the west is developed with an existing Arco gas station and is located within the Commercial Planning Area of the Grove Avenue Specific Plan; and

WHEREAS, on January 27, 2010, the City Council adopted The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) and a related Mitigation Monitoring and Reporting Program in conjunction with File No. PGPA06-001; and

c. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts.

d. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference.

e. The Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the City Council; and

f. There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts; and

**SECTION 2. *Additional Environmental Review Not Required.*** Based on the Addendum, all related information presented to the City Council, and the specific findings set forth in Section 1, above, the City Council finds that the preparation of a subsequent or supplemental The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) is not required for the Project, as the Project:

a. Does not constitute substantial changes to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) that will require major revisions to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

b. Does not constitute substantial changes with respect to the circumstances under which The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was prepared, that will require major revisions to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

c. Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified/adopted, that shows any of the following:

1. The project will have one or more significant effects not discussed in The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140); or

2. Significant effects previously examined will be substantially more severe than shown in The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140); or



the public interest, health, safety, convenience, or general welfare of the City because it would add additional commercial and retail services that would support business operations along the Grove Avenue corridor, the needs of airport-bound visitors and all the new residential development south of the 60 Freeway and west of Grove Avenue along Philadelphia Street. The proposed zone change to Commercial would allow the site to develop and provide additional retail and commercial to the surrounding area.

c. In the case of an application affecting specific property(ies), the proposed Specific Plan, or amendment thereto, will not adversely affect the harmonious relationship with adjacent properties and land uses. The project site is located in an area that will be developed with commercial land uses that will be complimentary and harmonious to the surrounding area. Furthermore, the project site will provide additional commercial and retail services that would support business operations along the Grove Avenue corridor, the needs of airport-bound visitors and all the new residential development south of the 60 Freeway and west of Grove Avenue along Philadelphia Street. The proposed zone change to Commercial would allow the site to develop and provide additional retail and commercial to the surrounding area.

d. In the case of an application affecting specific property(ies), the subject site is physically suitable, including, but not limited to, parcel size, shape, access, and availability of utilities, for the request and anticipated development. The proposed amendment to the Grove Avenue Specific Plan will change the land use of the project site to Commercial and any future development will be required to meet the Commercial development standards of the Grove Avenue Specific Plan. With the approval of the proposed amendment, the proposed project area will be developed with adequate lot size, access and utilities to serve the project.

**SECTION 6. City Council Action.** Based upon the findings and conclusions set forth in Sections 1 through 5, above, the City Council hereby APPROVES an Amendment to the Grove Avenue Specific Plan, as described herein and included as Exhibits "A" thru "C" of this Resolution.

**SECTION 7. Indemnification.** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

**SECTION 8. Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

**SECTION 9. Certification to Adoption.** The Secretary shall certify to the adoption of the Resolution.



***Exhibit B: Revised Commercial Land Use Table***

**Commercial (C) District Regulations**

**Purpose:** This land use designation is intended for commercial sales and retail facilities which support business operations within the Grove Avenue Corridor and/or serve the needs of airport-bound visitors. Those commercial uses which can take advantage of high traffic volumes along Grove Avenue will be permitted in the Commercial District.

**Permitted Uses:** The uses permitted in the Commercial (C) District shall include those uses listed below when developed in compliance with the purpose and intent of this Specific Plan.

1. **Commercial Uses, including:**

- Apparel shops, shoe stores and shoe repair
- Art and artists supply stores
- Bakery
- Barber shops and beauty shops
- Blueprint and photocopy services; printing, publishing, lithography and engraving
- Camera and photography shops and studios, photographic developing and printing
- Catering services
- Cleaning establishments and laundries
- Department stores and variety stores
- Drugstores and pharmacies
- Electrical equipment sales and repair shops, electronic parts and equipment sales
- Employment agencies
- Financial institutions
- Florists
- Food stores, supermarkets, delicatessens, and specialty food stores, candy stores and confectioneries
- Furniture stores
- Hardware stores
- Health studios and reducing salons
- Household appliance sales and repair shops
- Interior decorating shops

**Exhibit B: Revised Commercial Land Use Table (cont'd)**

- Financial institutions (without drive-in teller facilities)
- Glass replacement and repair shops
- Music and dance studios
- Radio and television broadcasting studios
- Restaurants with Drive-Thru
- Service stations with mini-markets
- Tailor schools
- Trade schools
- Veterinarians' offices and small animal hospitals
- Car Washes—Full Service and Self Service (excludes facilities ancillary to fueling stations)<sup>1</sup>

**Prohibited Uses:** The following uses are prohibited in the Commercial District.

- Adult businesses
- Agricultural uses
- Amusement enterprises, such as fairs, merry-go-rounds, ferris wheels, and similar uses operated on a temporary basis
- Automobile dismantling, and junk, rag, metal salvage
- Automobile service centers
- Cocktail lounges
- Contractors storage yard
- Outdoor storage (excluding outdoor storage areas that are associated with permitted and/or conditionally permitted uses)
- Recreational vehicle parks
- Residential uses
- Second hand stores and thrift shops
- Tattoo parlors
- Wholesale business establishments
- Sandblasting establishments

**Other Uses:** All other uses not listed as Permitted or Conditional Use are prohibited unless a finding is made by the Planning Commission that the use is similar to, and not more objectionable than that is permitted or conditionally permitted in the Commercial District.

<sup>1</sup>Car Washes—Full Service and Self Service are not permitted on the Commercial zoned corners at Grove Avenue and Philadelphia and Grove Avenue and Frank Street.

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## **APPENDIX A**

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**Mitigation Monitoring Program for Environmental Impact Report No. 90-2**



# 1. INTRODUCTION

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## PURPOSE AND INTENT

The purpose of the Grove Avenue Specific Plan is to implement the goals and policies of the Ontario General Plan by establishing design guidelines, development standards, and infrastructure requirements to aid in the development of the Grove Avenue Corridor as a light industrial and limited, commercial Business Park.

## AUTHORITY

California Government Code (Section 65450 et. seq.) contains provisions for the preparation of specific plans. The Grove Avenue Specific Plan has been prepared in conformance with the Code, as well as all applicable ordinances and resolutions of the City of Ontario. The Code requires that specific plans contain text and diagrams which specify:

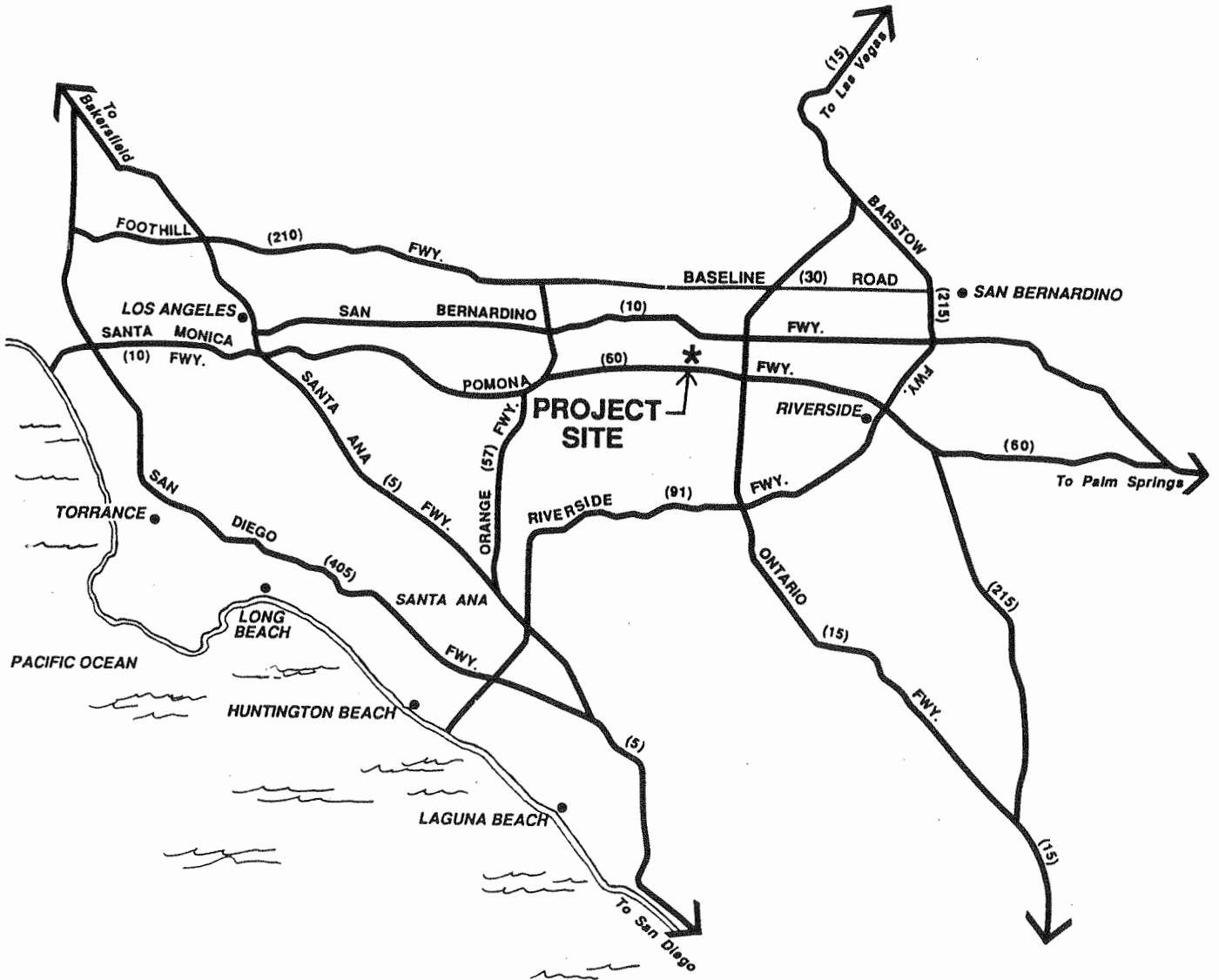
- A. The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan;
- B. The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan;
- C. Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable;
- D. A program of implementation measures including regulations, programs, public work projects, and financing measures necessary to carry out paragraphs A, B and C; and
- E. A statement of the relationship of the Specific Plan to the General Plan.

## PROJECT LOCATION

### Regional Context

The Grove Avenue Specific Plan area (**Exhibit 1**) is located in the City of Ontario, in San Bernardino County. Orange County and Riverside County lie south of the project area, and Los

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Angeles County lies to the west. The Grove Avenue Specific Plan area is adjacent to the Pomona Freeway (State Route 60). It is approximately 51 miles from downtown Los Angeles and 20 miles from downtown San Bernardino. Several major freeways are within 20 miles of the Specific Plan area, including the Corona Freeway (State Route 71), the Ontario Freeway (Interstate 15), the Barstow Freeway (Interstate 215), the Orange Freeway (State Route 57), the San Bernardino Freeway (Interstate 10), the Riverside Freeway (State Route 91), and Interstate 210.

#### Area Context

Grove Avenue is the primary southern entrance to the Ontario International Airport via the Pomona Freeway. The Ontario International Airport is adjacent the northeast corner of the Specific Plan area, and the Chino Airport is located approximately 4 miles south of the Specific Plan area (Exhibit 2).

### **BACKGROUND AND HISTORY**

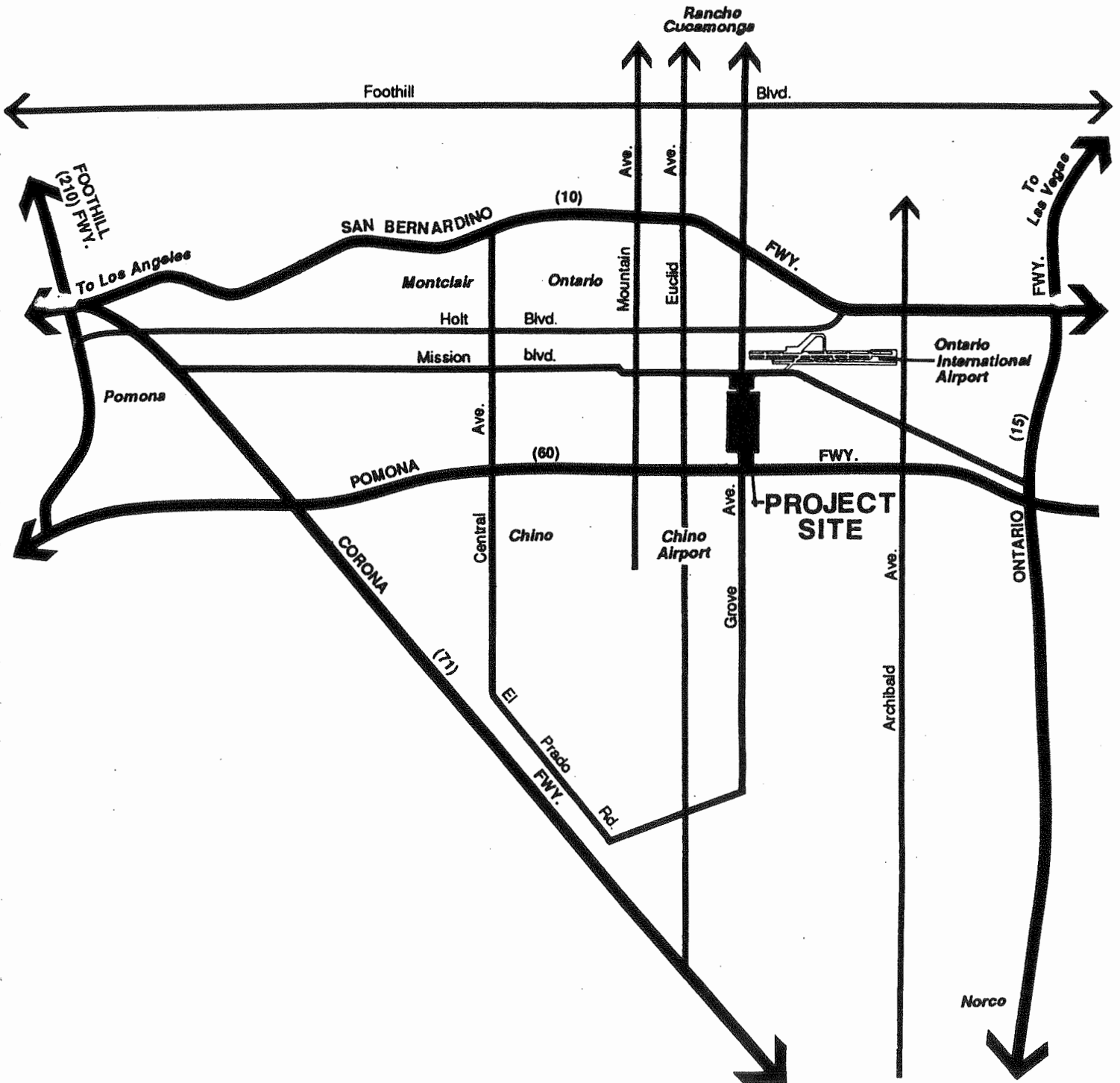
#### Involvement of the Grove Avenue Property Owners in the Planning Process

For the past couple of years, several of the property owners in the Grove Avenue Corridor have been involved in the planning for future development along Grove Avenue. A property owners committee was formulated at the inception of the planning process for the Grove Avenue Specific Plan. This property owners committee provided input throughout the specific plan process.

#### Ontario International Airport Ground Access and Terminal Expansion

A Ground Access and Terminal Expansion Study was prepared by the Southern California Association of Governments in 1982. The purpose of the study was to develop a plan and implementation program to improve those elements of the off-airport ground access system which can best mitigate the anticipated increase in surface traffic generated by the planned expansion of the existing passenger terminal facilities at Ontario International Airport and by non-airport growth in the area.

The expansion will serve a forecasted demand of 12 million annual passengers by 1995. It includes a new 700,000 square foot terminal, parking for an additional 9,000 vehicles, structural pavement for taxiways and parking positions for 36 aircraft, and an internal airport roadway system.



Grove Avenue is a major component of the ground access program. As the major access to/from Ontario International Airport from the south, Grove Avenue will have substantial increases in traffic volume associated with the airport expansion program.

#### Area Market Overview

In March of 1989, a feasibility study for Grove Avenue was prepared by Wallace, Roberts and Todd. It contained an extensive evaluation of the market conditions in the Specific Plan area. The major findings of the feasibility study are summarized below.

The Ontario International Airport has become the center of a large and rapidly growing business core in Southern California. Building data comparing the valuation of commercial and industrial permits to those in the five county Southern California area demonstrate the strength of the local economy.

In 1985, the number of industrial building permits issued by the City of Ontario equaled about 0.3% of those in the five county area. Similarly, the valuation of commercial permits was about 0.1%. The climb in industrial building activity has been virtually spectacular, ranging between 6% and nearly 10% of all industrial permits in Southern California between 1985 and 1989.

The growth in commercial permits has been slower reaching nearly 1% of the total five county area for the last several years. This figure is more impressive than it seems because commercial permits include both retail and office structures.

Most recently, the valuation of commercial permits in the City of Ontario jumped to over 2% of the Southern California market because of the number of new office buildings being constructed in the vicinity of the Ontario International Airport.

The Ontario International Airport has been a major factor in the concentration of growth in the City of Ontario and surrounding region. It is a center of business activity, and it has promoted Ontario to the rest of the Southern California. Ontario's proximity to Los Angeles, the freeway system, and low land prices have been other factors in the concentration of growth in the City and surrounding region.

The Ontario International Airport presently carries 5.5 million passengers a year or just over twelve percent of the number of passengers accommodated by the Los Angeles International Airport (LAX). In 1977-1978, the total number of passengers at Ontario were less than 6 percent of those at LAX. Similarly, the cargo traffic at the Ontario International Airport is now 20 percent of that at LAX, up from less than one half of one percent in 1977-78.

Large planned developments in Ontario and neighboring communities opened at land prices well below those in Los Angeles and Orange Counties. This comparative advantage still exists.

## Recent Development Activity

Several projects have been recently (1986-1989) built, approved, or are pending within the Grove Avenue Specific Plan area, and the immediate vicinity. Recent, proposed development is of an industrial/business park character.

In recent years, Grove Avenue has become the location of several industrial parks including: the Pepper Grove Business Park, Woodlawn Industrial Park, Acacia court, and portions of Grove Avenue Business Park. Together these developments account for approximately 25 acres.

Approved and pending projects account for 5% of all land and 7% of vacant land within the Specific Plan area. Approved projects span six parcels and eleven acres. A total of twelve new buildings are anticipated.

In addition to the development activity within the Specific Plan area, several projects have been approved in the immediate vicinity. Nine parcels have been approved for industrial development and are expected to contain approximately nine new buildings. Just north of the Specific Plan area, twelve industrial buildings and a request for subdivision are pending approval.

## PLANNING OBJECTIVES

Below is a synopsis of the existing opportunities that affected the development of the Grove Avenue Specific Plan:

- \* A significant portion of the study area is vacant (54%).
- \* The vacant parcels tend to be relatively large in size (5 acres or greater) and have significant frontages adjacent to Grove Avenue or other arterial thoroughfares.
- \* Several industrial park projects recently constructed within the Specific Plan area are of high quality and would be compatible with higher intensity business park, retail or office uses.
- \* Basic utilities (sewer, water, storm drainage) have already been provided by the City of Ontario.
- \* The Grove Avenue Corridor area offers excellent regional access to the Ontario International Airport via an interchange at the Pomona Freeway. As part of the Ground Access Program for the Ontario International Airport, access will be improved with the completion of the Grove Avenue/Union Pacific Railroad grade separation in the early 1990's.

The objectives of the Grove Avenue Specific Plan are:

- A. To provide opportunities for the establishment of airport-serving light industrial land uses, airport-related businesses and offices, and retail and support commercial development aimed at serving the needs of airport-bound visitors and nearby workers;
- B. To create a distinctive identity for Grove Avenue in recognition of its importance as the primary transportation link between the Pomona Freeway (State Route 60) and the Ontario International Airport by specifying design standards, as well as landscape and streetscape treatments, for development within the Grove Avenue Corridor; and
- C. To obtain the highest and best use of the properties within the Grove Avenue Specific Plan area by maximizing the opportunities to intensify the existing and proposed land uses without compromising Grove Avenue's function as the primary southern access to the Ontario International Airport via the Pomona Freeway.

## **RELATIONSHIP TO THE ONTARIO GENERAL PLAN**

California State Law (Government Code Section 65450 et. seq.) requires that a specific plan include a discussion of its relationship to the City's General Plan. The Government Code also stipulates that no specific plan may be adopted or amended unless the proposed plan or amendment is consistent with the General Plan. The following discussion contains a list of the goals and policies of the City of Ontario General Plan that apply to the Grove Avenue Specific Plan. For each identified goal or policy, a statement regarding its consistency with the Grove Avenue Specific Plan is provided.

### **Hazards Element**

#### Flooding:

Goal 2.0: Identify and reduce flood-related hazards and risks.

The intermittent flooding problems on Francis Street at Grove Avenue have been reduced by the installation of storm drainage improvements associated with the Grove Avenue Improvement Plan.

### **Infrastructure Element**

Goal 5.0: Provide for the adequate, timely and economically sound development of flood protection facilities for Ontario residents and businesses.

Policy 5.2: Continue to give priority to improvement of significant deficiencies in the existing system over new facilities to serve development areas of the City.

All development proposals within the Grove Avenue Specific Plan area will be required to prepare on-site hydrology studies and to make the required improvements per the City's Master Plan of Drainage. An assessment of the ability of the existing and planned facilities to accommodate the proposed development shall be completed and any necessary modifications shall be identified.

Dust, Wind and Blowsand:

Goal 4.0: Reduce damage to life and property from dust, wind, and blow-sand.

Policy 4.1: Require new development to demonstrate permits from the Agricultural Commissioner's office and comply with their provisions before issuing permits for new construction within the soil erosion control area.

The entire Specific Plan area is located in an area identified by the General Plan as a soil erosion control area. As such, development within the Specific Plan area will be required to adhere to dust control measures during construction.

~~Airport Environs Element~~

~~Goal 3.0: Protect residents and workers within air safety zones from excessive exposure to accident potential.~~

~~Goal 5.0: Encourage and accelerate the orderly and systematic replacement of incompatible uses by industrial development within action area H west of the airport.~~

~~Policy 5.2: Purchase nonconforming residential uses from willing sellers.~~

~~Policy 5.3: In areas of extreme noise impact, vacate and demolish acquired residential units and relocate occupants to safer, quieter housing.~~

~~Policy 5.4: Land bank and assemble developed residential parcels for future industrial development.~~

~~Airport Land Use Compatibility Planning~~

~~The Grove Avenue Specific Plan is located within the Airport Influence Area (AIA) of the Ontario International Airport and is required to be consistent with the Airport Land Use Compatibility Plan.~~



- ~~Policy 5.5: Acquire and land bank vacant land for future industrial development. Offer acquired properties for sale with development controls and aviation easements.~~
- ~~Policy 5.6: Under the administration of the City of Ontario Redevelopment Agency, assist willing sellers participating in the acquisition program.~~
- ~~Policy 5.7: Give priority in City housing programs to relocating renters from Action Area II in need of housing assistance.~~

~~A portion of the Grove Avenue Specific Plan area (that area north of Francis Street) is identified as Airport Action Area II (Exhibit 3). No residential uses are proposed in the area north of Francis Street. Existing residential uses are currently nonconforming uses. The Grove Avenue Specific Plan contains an overlay zone for the purpose of specifying appropriate restrictions to protect workers within air safety zones from excessive exposure to aircraft crash hazards. A separate program, the Ontario International Airport Part 150 Study, is responsible for minimizing exposure to aircraft noise. Options to address the existing residences along Grove Avenue north of Belmont Street, including relocation, is part of the City of Ontario's implementation program for the Ontario International Airport Part 150 Study.~~

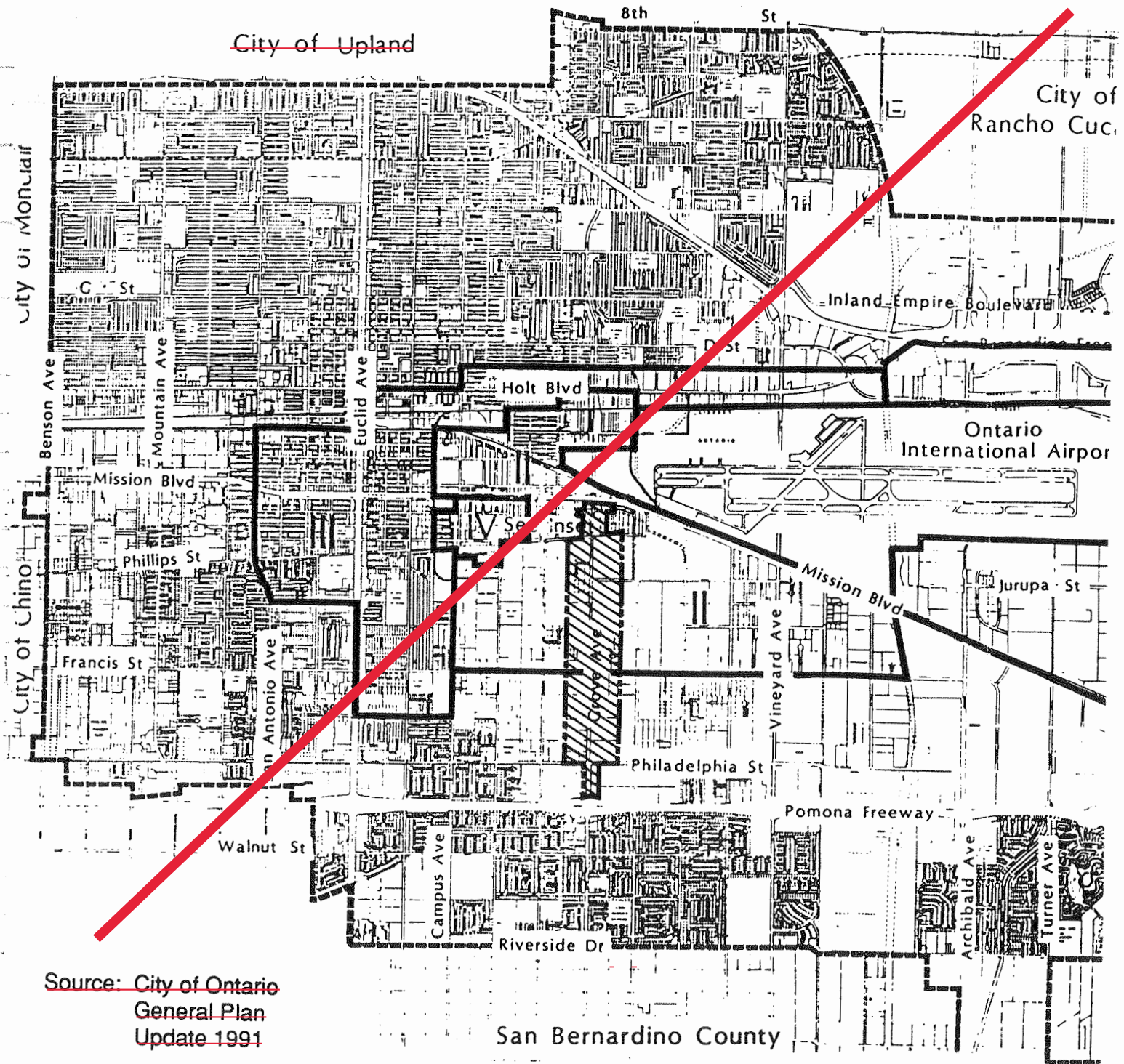
#### **Natural Resources Element**

- Goal 2.0: Support and reinforce regional air quality plans and programs.
- Policy 2.4: Promote the growth of "clean" industry which does not increase pollution from point services.
- Policy 2.5: Work with Omnitrans to expand bus services. Require bus-related improvements (shelters, turnouts, etc.) as part of new specific plan developments.

The Grove Avenue Specific Plan land use plan allows industrial land uses which are of a limited nature such as those contained in business parks. Noxious industrial uses are not allowed. Bus-related improvements are discussed under the circulation guidelines contained within the design guidelines of the Grove Avenue Specific Plan.

#### **Aesthetics, Cultural, Open Space and Recreational Resources Element**

- Goal 4.0: Protect and expand the city system of trails and bikeways.



Source: City of Ontario  
General Plan  
Update 1991

Goal 4.2: Include new pedestrian and equestrian trails and bikeways in new development under development code and specific plan procedures.

The General Plan includes a proposed Class 3 bikeway along Mission Boulevard and along Philadelphia Street. These have been incorporated into the Circulation Plan for the Grove Avenue Specific Plan and will be implemented as part of site plan review.

Goal 5.0: Preserve and protect Ontario's scenic highways and vistas as community assets.

Policy 5.2: Designate Euclid Corridor and Mission Boulevard as City scenic landmarks.

Policy 5.5: Complete proposed landscape improvements to Mission Boulevard.

The Specific Plan includes a discussion of Mission Boulevard as a scenic landmark and identifies the proposed landscape improvements to Mission Boulevard. These improvements have been incorporated into the Specific Plan, to help preserve and protect the scenic character of Mission Boulevard.

#### **Community Development Element**

Goal 1.0: Develop and maintain a balance of residential, commercial, industrial, open space and recreational land uses which will encourage a healthy variety of economic, social and cultural opportunities.

Policy 1.1: Permit only such new development, as is compatible with the existing and planned growth of Ontario International Airport, consistent with the Airport Environs Element of this General Plan.

The Airport Environs Element of the General Plan designates that portion of the Grove Avenue Specific Plan area north of Francis Street as General Industrial. The Grove Avenue Specific Plan land use plan allows for business park uses, consistent with the Airport Environs Element. The development of industrial (business park), office and limited commercial land uses will encourage economic opportunities in the City.

Goal 6.0: Utilize Ontario's proximity to the airport and its inventory of vacant industrial and commercial land to develop uses which maximize employment opportunities.

Policy 6.1: Encourage a pattern of land uses to establish an economic base which provides sufficient jobs for those who choose to both live and work in Ontario.

Policy 6.2: Require new Specific Plans and revisions to existing Specific Plans which include commercial and/or industrial development and demonstrate compatibility with the goals and implementation policies of the General Plan, and in particular with Policy 6.1, immediately above.

The Grove Avenue Specific Plan includes predominantly commercial and business park uses. The Specific Plan is compatible with the goals and implementation policies of the General Plan, including those of the Airport Environs Element, and embodies a pattern of land uses to supplement the City of Ontario's economic base.

### **Infrastructure Element**

Goal 1.0: Ensure an adequate supply of safe water for Ontario residents and businesses.

Policy 1.5: Require financing plan for water system capital improvements in large developments as a condition of approval.

As development occurs along Grove Avenue, the individual development will be conditioned to build its portion of required water lines. The Specific Plan also requires that parcels of ten acres or more prepare a planning area plan prior to subdivision approval. This planning area plan would address, among other things, water system requirements.

Goal 2.0: Provision of adequate wastewater lines and treatment facilities which serve Ontario residents and businesses and protect the environment.

Policy 2.6: Require financing plans for sewerage system capital improvements in large developments as a condition of approval.

As development occurs along Grove Avenue, the individual development will be conditioned to build its portion of required sewerage improvements. The Specific Plan also requires that parcels of ten acres or more prepare a planning area plan prior to subdivision approval. This planning area plan would address, among other things, sewerage system requirements.

Goal 3.0: Continue to provide for the environmentally sound collection and disposal of solid waste from Ontario's residents and businesses.

Policy 3.1: Expand the recycling program to include multi-family residences, commercial and industrial uses. Establish and maintain incentives which encourage residents and businesses to participate.

Businesses within the Specific Plan area will participate in any City-required recycling program and will be a part of the City's system for collection and disposal of solid waste.

Goal 4.0: Provide for the adequate, timely and economically sound development of flood protection facilities for Ontario residents and businesses.

Policy 4.2: Continue to give priority to improvements of significant deficiencies in the existing system over new facilities to serve developing areas of the City.

As part of the Grove Avenue assessment district, storm drain facilities were installed in Francis Street and Grove Avenue north of Francis. As precise plans for development along Grove Avenue are submitted to the City, complete hydrology studies and storm drain designs will be required.

Goal 8.0: Provide a system of streets that meets the needs of current and future residents of Ontario and facilitates the safe and efficient movement of people and goods throughout the City.

Policy 8.7: Maintain and improve circulation to and from Ontario International Airport by carrying out the recommendations of the Ground Access Study.

Grove Avenue is a major component of the ground access program. The Grove Avenue Specific Plan was prepared to respond to the role of Grove Avenue as the major access to Ontario International Airport from the south.

Goal 10.0: Support and pursue transit service expansion and other alternatives to automobile travel, including pedestrian and bicycle facilities.

Policy 10.5: Require provision of an accessible and secure areas for bicycle storage at all new commercial and industrial developments.

As part of the site plan review process, all proposed developments along Grove Avenue will be reviewed for their provision of pedestrian and bicycle facilities, (including bicycle storage areas), and other facilities to encourage use of alternative travel modes.

## **Housing Element**

- Goal 4.0:** Promote and encourage the rehabilitation of deteriorated dwelling units and conservation of the currently sound housing stock.
- Objective 4.1:** Promote the rehabilitation of deteriorated dwellings at an average annual rate of 150 units over the next five years.
- Policy 4.1:** Continue to pursue housing programs offered by the State and Federal governments.

Options to address the existing residences along Grove Avenue north of Belmont Street, including relocation, is part of the City of Ontario's implementation program for the Ontario International Airport Part 150 Study.

## 2. ENVIRONMENTAL SETTING

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### EXISTING LAND USES

Existing land uses in the Grove Avenue Specific Plan area are shown on **Exhibit 4**. Approximately one-half of the Specific Plan area is vacant. Industrial land uses are located throughout the specific plan area. There are scattered commercial land uses located in the southern half of the Specific Plan area. Additionally, a limited amount of governmental/public land uses are located north of Philadelphia Street on the west side of Grove Avenue. **Table 1** provides a statistical summary of the existing land uses in the Grove Avenue Specific Plan area.

The Grove Avenue Specific Plan includes the majority of parcels which are adjacent to Grove Avenue in the area between the Pomona Freeway on the south and Mission Boulevard on the north. The length of the Grove Avenue Corridor is approximately 1.5 miles. Existing land uses in the 250.15 acre Specific Plan area are summarized below:

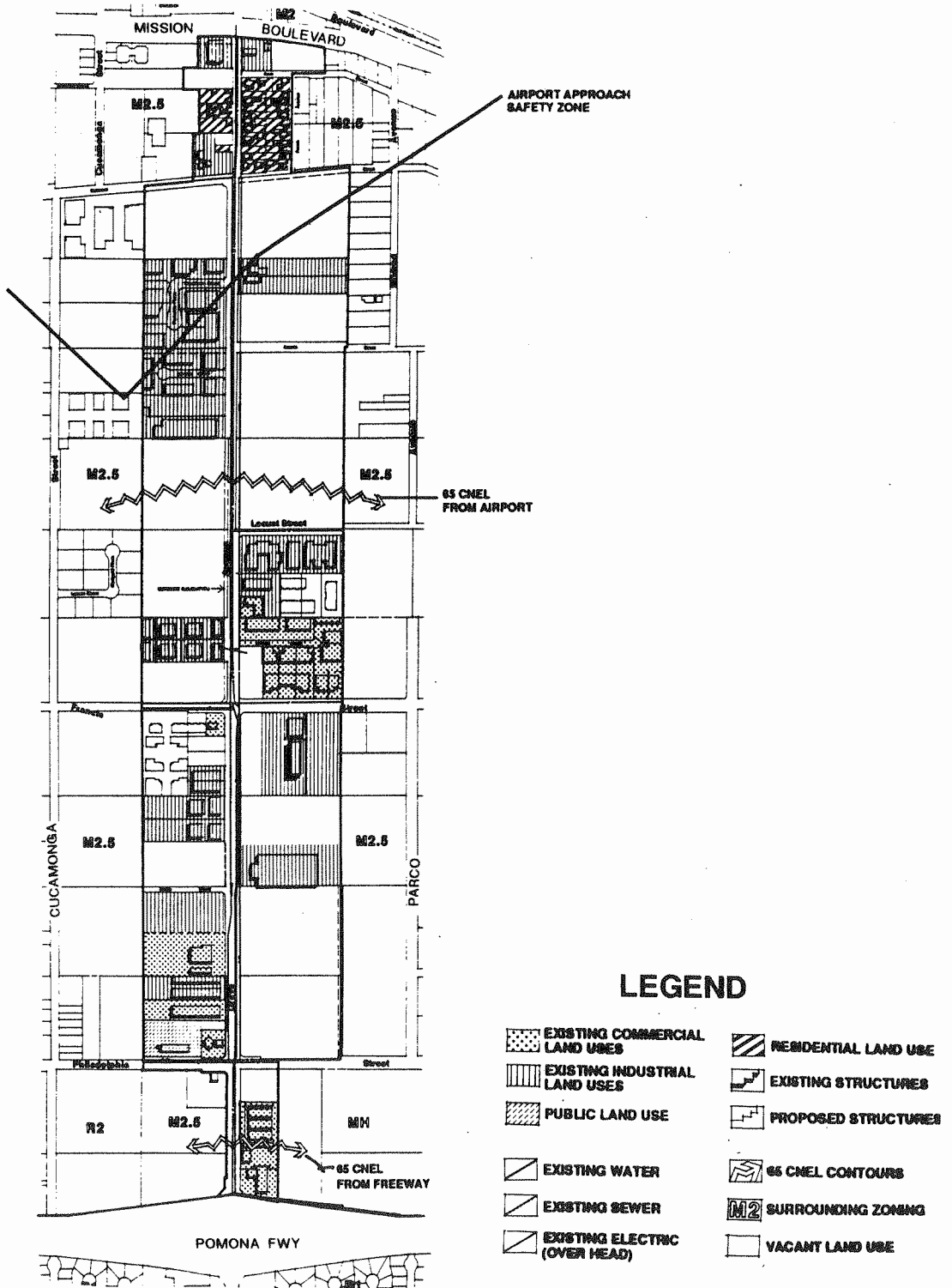
**TABLE 1  
EXISTING LAND USE SUMMARY**

Commercial . . . . .	27.84 acres
Industrial . . . . .	61.74 acres
Public . . . . .	3.43 acres
Vacant . . . . .	119.85 acres
Residential . . . . .	7.56 acres
Circulation . . . . .	29.72 acres
Total: . . . . .	250.15 acres

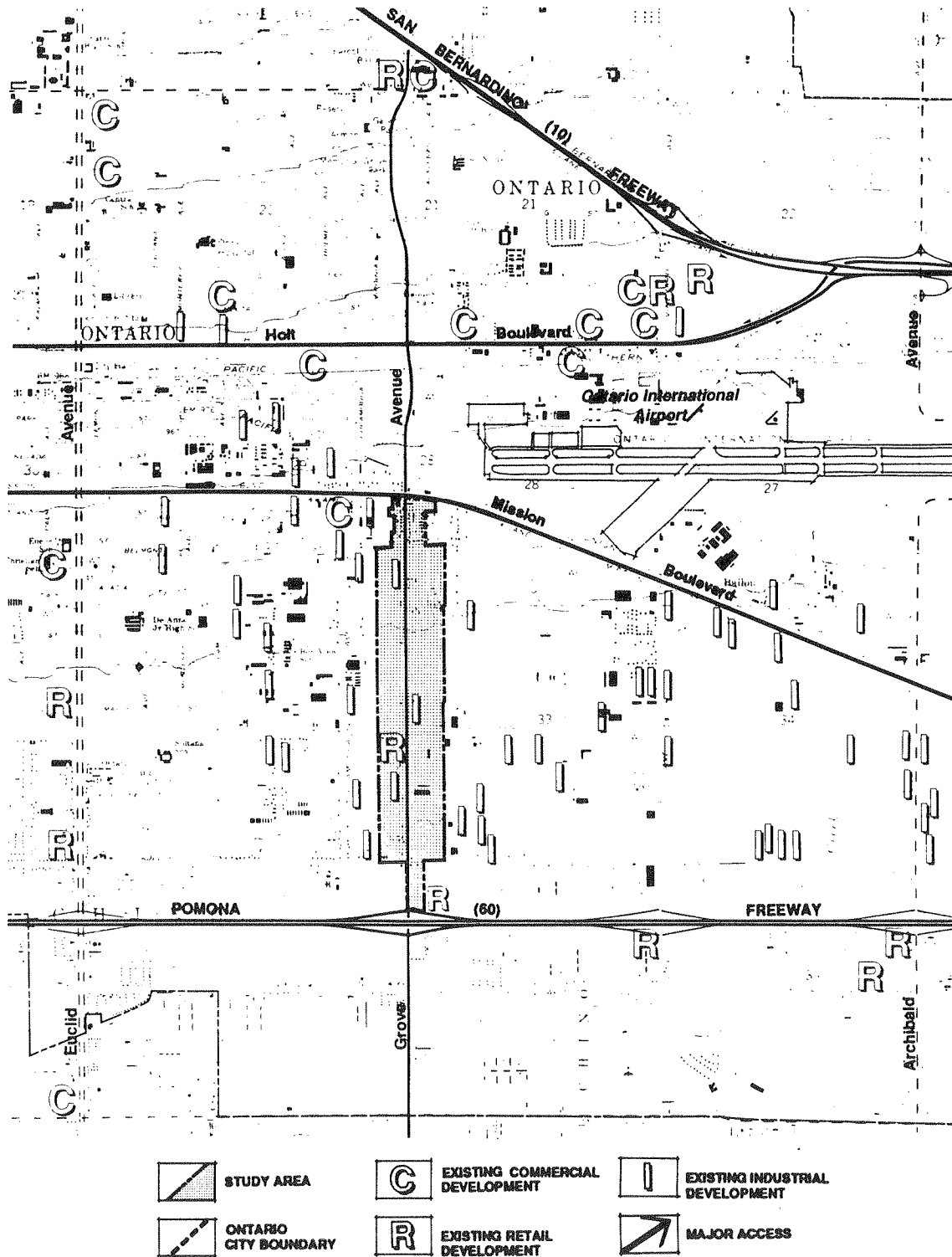
The majority of the land uses surrounding the Specific Plan area are industrial, although there are some residential uses to the southwest and southeast (mobile home park) of the Specific Plan area. **Exhibit 5** shows the industrial/commercial and retail development that is currently underway in the vicinity of the Specific Plan area.

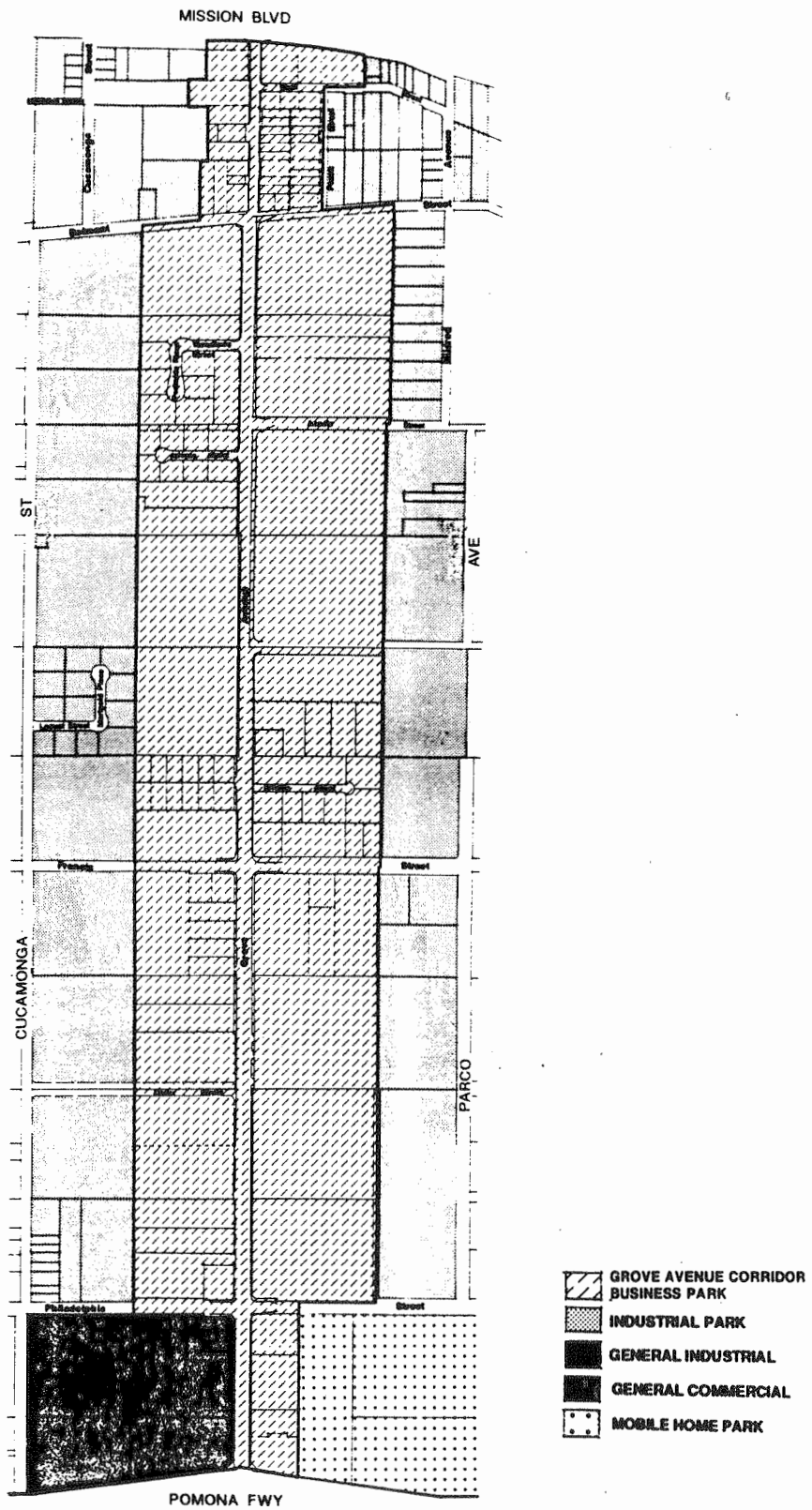
### GENERAL PLAN AND ZONING DESIGNATIONS

The City of Ontario General Plan was updated in **January 2010**. ~~September of 1992. Under the General Plan, the entire Grove Avenue Corridor is designated "Grove Avenue Corridor Business Park"~~









~~(Exhibit 6). The General Plan contains the following information on the Grove Avenue Corridor Business Park:~~

~~"The Grove Avenue Corridor Business Park is the subject of a special planning effort that has been undertaken by the City. This effort on development of a Specific Plan is to establish superior design guidelines and development standards for the Grove Avenue Corridor. These guidelines and standards are intended to create a distinctive identity for the Corridor in recognition of its premiere location as the main entrance into the Ontario International Airport from the Pomona Freeway. A variety of light industrial uses with an FAR of 0.35 and professional/administrative offices with an FAR of 0.30 will be permitted. In addition, commercial uses with an FAR of 0.30 will be established at Philadelphia Street and Frances Street. For those northern portions of the Grove Avenue Corridor that are located in the Approach Safety Zones of the Ontario International Airport, building intensity is limited to an FAR of 0.25 and a building height of one story."~~

The zoning map for the City designates the Grove Avenue Specific Plan area as SP (Specific Plan). ~~The majority of land uses surrounding the Grove Avenue Specific Plan area to the west, north and east are zoned M2.5, except for MH (Mobile Home Park) and R2 (Multiple Dwelling) designations adjacent to the Grove Avenue Corridor at the Pomona Freeway. The Pomona Freeway is located south of the Specific Plan area. The land uses south of the freeway are zoned C1 (Shopping Center Commercial) and R1 (One Family Dwellings).~~

## **GEOLOGY AND SOILS**

### **Geology**

No geologic faults are known to occur in the Specific Plan area. Ontario, however, is located near six active or potentially active fault zones. These faults are the San Jacinto, Chino, Cucamonga, San Andreas, Red Hill, and Central Avenue faults. The closest known active faults are located less than ten miles from the City. The primary dangers associated with seismic activity are surface rupture, ground failure, liquefaction and ground shaking. The risk of seismic activity within the Specific Plan area is not any greater than that found elsewhere throughout Southern California.

The City of Ontario is situated on the alluvial fan composed of unconsolidated course to medium grained soil. This loosely compacted silty, sandy, alluvial soil will cause magnification of ground shaking. The anticipated intense ground shaking generated by an earthquake presents a hazard to the structural integrity of buildings. The Uniform Building Code, however, contains provisions to mitigate impacts of ground shaking on buildings to an acceptable level.

## Soils

The soils within the City of Ontario are derived from the alluvial valley floors, fans and terraces which cross areas of San Bernardino County. The major soils in the Specific Plan area are identified as Tujunga-Loamy Sand. There are no soil stability problems associated with these soils, and they are suitable for development.

## HYDROLOGY

According to the Flood Insurance Rate Map prepared by the Federal Emergency Management Agency, there are no portions of the Specific Plan area that are within the areas of 100 year flood. There have, however, been problems with periodic flooding on Grove Avenue at Francis Street due to a low spot in the roadway. This localized and periodic problem has been substantially alleviated as part of the Grove Avenue Improvement Plan, which provided a catch basin to convey water into a storm drainage system in Francis Avenue.

## DUST, HIGH WIND AND BLOWSAND

The City of Ontario is subject to strong winds between September and April and at unexpected times throughout the year. Hazards resulting from these winds include property damage, wind tunneling and channeling effects of buildings, and blowing sand and soil.

A Soil Erosion Control Area covers much of eastern Ontario, including the entire Grove Avenue Specific Plan area. Grading, plowing or other soil disturbance within this area is regulated by the City of Ontario and a permit is required. Non-agricultural projects larger than fifteen acres must submit a comprehensive dust control plan to be granted a permit for new construction.

## NOISE

A noise compatibility study for the Ontario International Airport (ONT), the Part 150 Study, was conducted as part of a collective effort among the City of Los Angeles, the City of Ontario, and other affected agencies. It was undertaken in 1986, and completed four years thereafter to address the problems and concerns of aircraft noise around ONT under Federal Aviation Regulation Part 150 Guidelines. The Part 150 Study identified actions to be taken to achieve noise reduction and land use compatibility in noise impacted areas of Ontario.

A portion of the Grove Avenue Specific Plan area is affected by ONT in an area located north of Acacia Street and south of Mission Boulevard. This area was included in the Part 150 Study as part of Action Area II. For Action Area II, the Part 150 Study recommended the orderly and systematic replacement of incompatible land uses (i.e. residential) with industrial development.

This recommendation has already been implemented because the noise affected residential neighborhood north of Acacia Street is zoned for industrial development. This Specific Plan is a refinement of the allowable industrial land uses for the affected property.

Another method to minimize exposure to noise impacts is via acoustical treatment and building design. All new construction is to be reviewed by an acoustical engineer in an independent report, which is to specify any appropriate noise attenuation measures. These recommendations are to become conditions of approval to be applied to new construction.

## **HAZARDOUS MATERIALS**

The City of Ontario contains many industries which use, produce, and store a variety of hazardous materials. The most prevalent toxic and hazardous materials in Ontario include chemicals used in some manufacturing plants, pesticides and animal waste from agricultural uses, and aviation fuel in storage at Ontario International Airport. In addition, hazardous materials pass through Ontario en route to other destinations, via the City's freeway, rail and air transportation corridors. There are several sites within the Grove Avenue Specific Plan Area which currently (County of San Bernardino, Department of Environmental Health Services, May 2, 1990) have hazardous waste permits, including:

1. 915 South Grove Avenue, Carny Chemical Corporation
2. 1842 South Grove Avenue, Maaco Auto Painting
3. 2124 South Grove Avenue, #A, Gary's Grinding and Hardware
4. 2124 South Grove Avenue, #F, Anesthesia Equipment Service
5. 2124 South Grove Avenue, #H, House of Printing
6. 2124 South Grove Avenue, #N, RVC Shell Company
7. 2156 South Grove Avenue, ARCO Petroleum Prod #5252
8. 2520 South Grove Avenue, 24K Cleaners

No new land uses or changes of use may be initiated on a site where hazardous materials and/or hazardous waste(s) have been stored, used, handled, dispensed, mixed, generated, treated or refined until clearance has been obtained from all appropriate and responsible agencies (including, but not limited to, the Ontario Fire Department and the County of San Bernardino, Department of Environmental Health Services), that the site has been properly remediated and cleaned-up.

### 3. DEVELOPMENT PLAN

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#### LAND USE PLAN

The Land Use Plan for the Grove Avenue Specific Plan area is shown on **Exhibit 7**. The Land Use Plan emphasizes Grove Avenue's connection with the Ontario International Airport. Airport-related industrial uses are designated in the Specific Plan area north of Francis Street. South of Francis Street, office/commercial uses predominate with a small concentration of commercial use at Philadelphia Street.

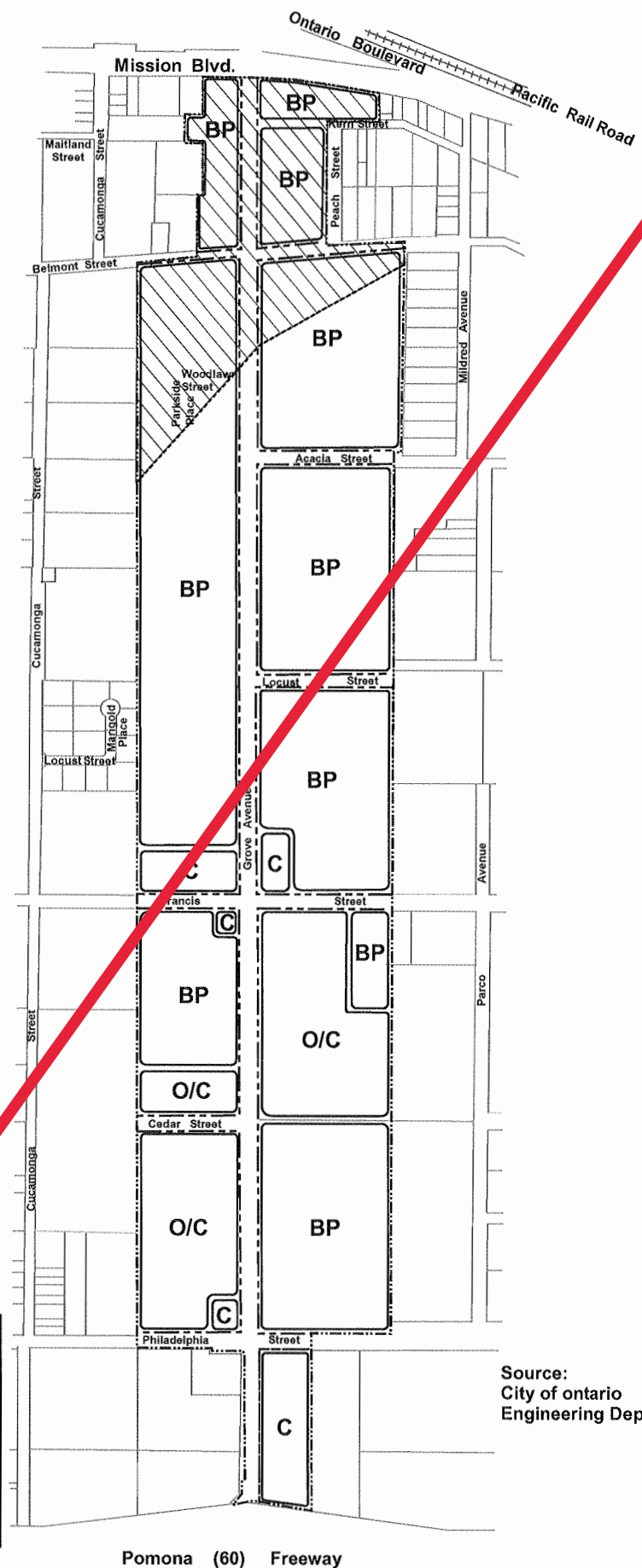
At build-out, there could be almost three million square feet of development within the Specific Plan Area, as shown in the statistical summary below.

**TABLE 2  
GROVE AVENUE SPECIFIC PLAN  
STATISTICAL SUMMARY**

<u>Land Use District</u>	<u>Acres</u>	<u>Square Feet</u>
Business Park ( <del>0.35 FAR</del> ) <sup>1</sup> <b>(0.40 FAR)<sup>1</sup></b>	127.17 <b>160.82</b>	1,938,833 <b>2,305,282</b>
<del>Business Park, Airport Approach Overlay<sup>2</sup> (0.25 FAR)</del>	<del>33.65</del>	<del>366,449</del>
Commercial (0.30 FAR)	13.74	179,544
Office/Commercial (0.30 FAR)	<u>37.93</u>	<u>495,669</u>
<b>Subtotal:</b>	<b>212.49</b>	<b>2,980,505</b>
Circulation	<u>37.66</u>	-----
<b>TOTAL</b>	<b>250.15</b>	<b>2,980,505</b>

1. An increase in the FAR may be permitted if the traffic generating characteristics of a specific project, as based on a report from a traffic engineer, do not exceed the number of trips that EIR 90-2 assumed would occur for the site.
2. ~~Airport Approach Overlay (consistent with land use compatibility guidelines within the City of Ontario General Plan)~~

Refer to the Ontario International Airport Land Use Compatibility Plan for additional criteria and policies that may affect but not limited to the restriction of allowable land uses, allowable FAR, overall site design and building/structure heights.



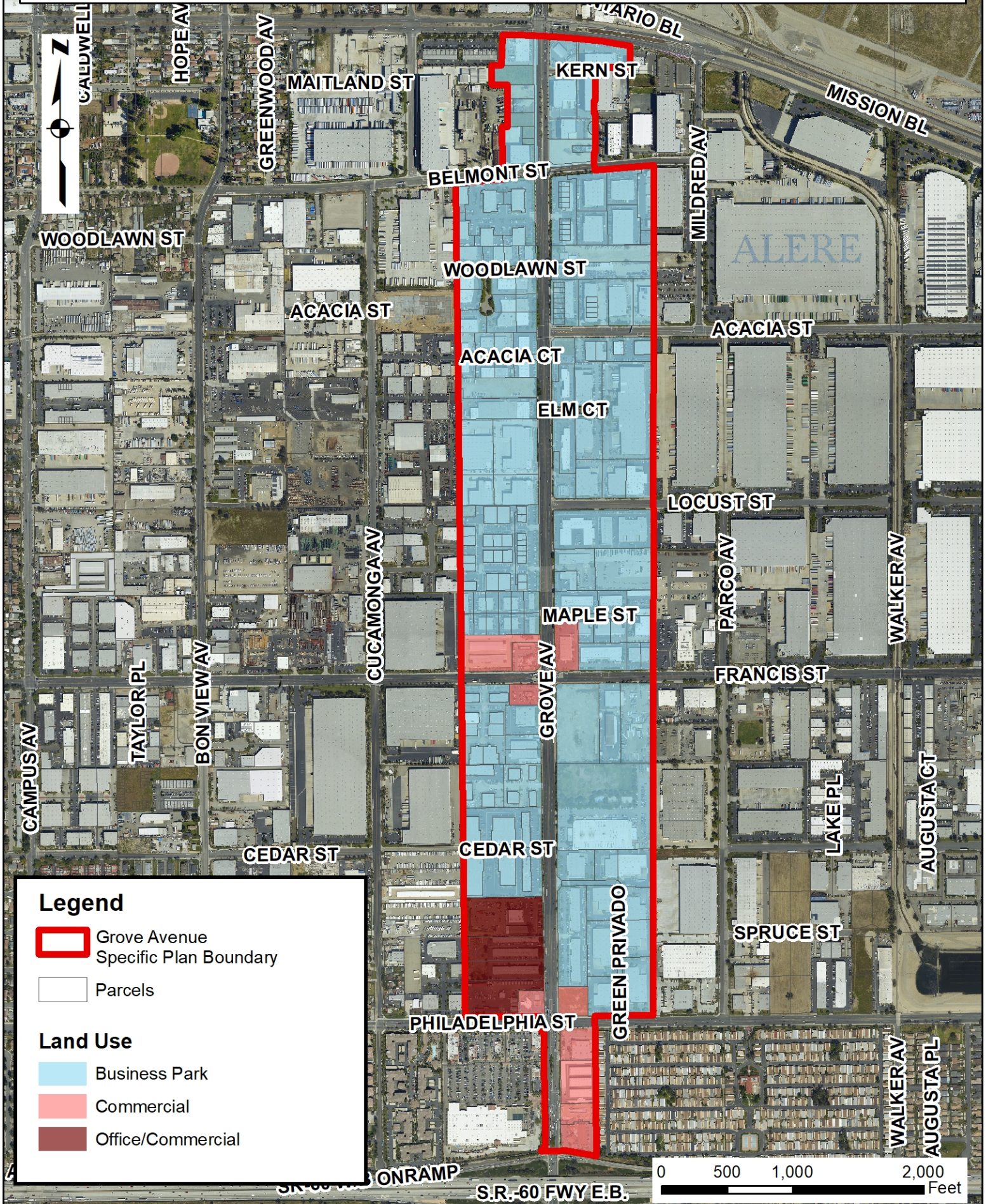
**LEGEND**

<b>BP</b>	BUSINESS PARK
<b>C</b>	COMMERCIAL
<b>O/C</b>	OFFICE/COMMERCIAL
	CIRCULATION
	AIRPORT ENVIRONS OVERLAY ZONE

Source:  
City of Ontario  
Engineering Department



# Grove Avenue Specific Plan - Land Use Plan (Exhibit 7)





## ~~Airport Approach Overlay Zone~~

~~The City of Ontario General Plan includes an Airport Environs Element. The objectives of this Element are:~~

- ~~\* To safeguard the general welfare of the inhabitants within the vicinity of the airport and the public in general by minimizing public exposure to excessive noise and safety hazards; and~~
- ~~\* To provide for the orderly growth of Ontario International Airport by promoting the overall goals and objectives of California airport noise standards and by preventing the creation of new noise and safety problems.~~

~~Exhibit 8 identifies the air safety zones of the Ontario International Airport. A portion of the Grove Avenue Specific Plan area lies within an "approach safety zone" and a portion lies within "clear zone". Within this area, land uses and land use intensities are restricted in conformance with the Land Use Compatibility Guidelines of the City's General Plan.~~

~~The Airport Environs Land Use Plan designates that Grove Avenue Specific Plan area north of Francis Street as General Industrial. As shown on the Land Use Plan, the land uses which are part of the Grove Avenue Specific Plan are consistent with the Airport Environs Element of the Ontario General Plan, as industrial business park uses are designated north of Francis Street on Grove Avenue.~~

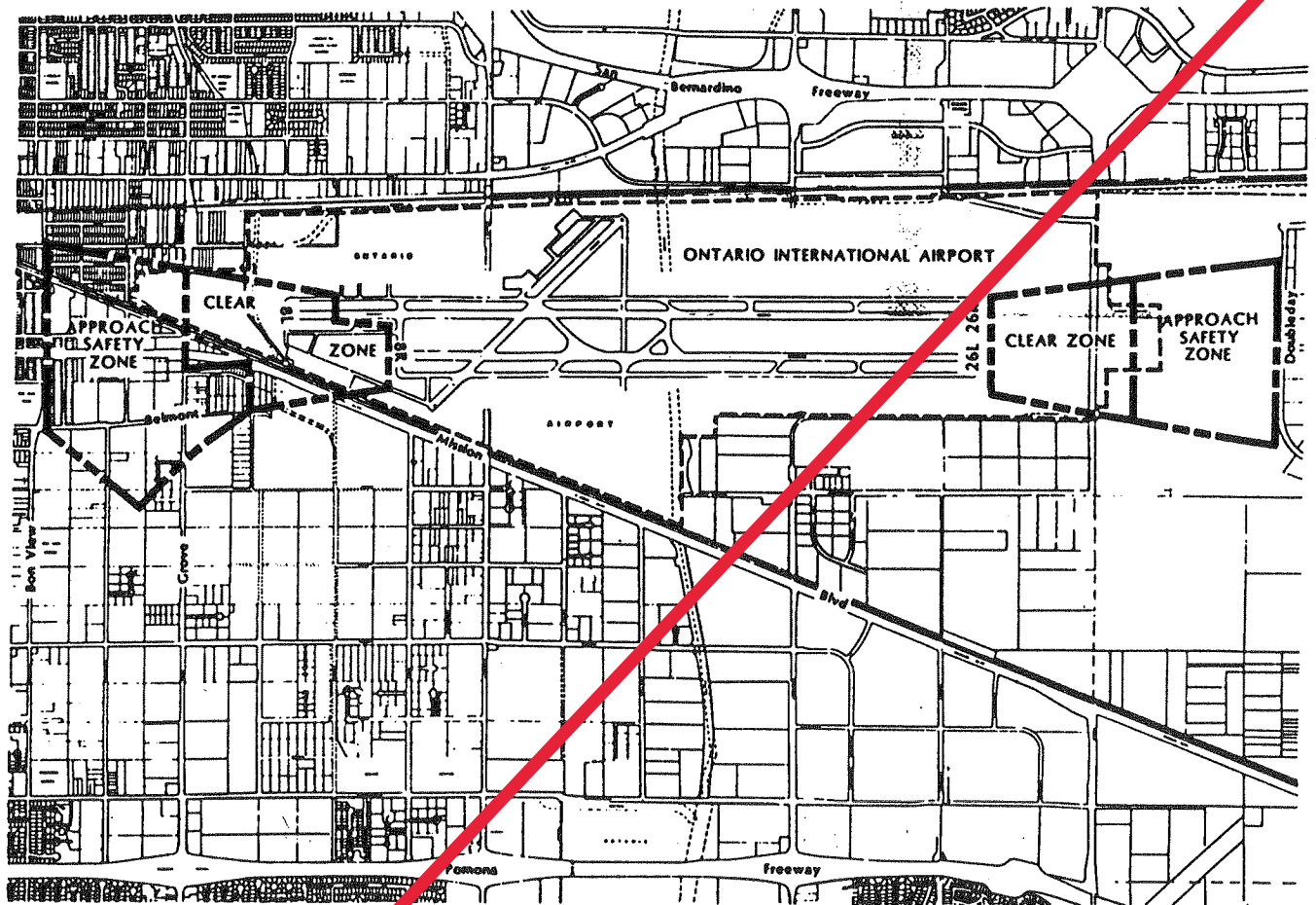
~~Land use intensities are restricted to a floor area ratio of 0.25 within the airport approach overlay zone. This restriction on floor area ratio is adapted from the Airport Land Use Planning Handbook and is often cited as a method to minimize the number of people exposed to aircraft crash hazards. The development standards contained in the Grove Avenue Specific Plan for the Airport Approach Overlay Zone reflect these use and intensity restrictions.~~



~~The Airport Land Use Commission will review future amendments to the Grove Avenue Specific Plan which affect the areas within the Airport Safety Zone because the Commission has adopted the Airport Environs Element as their guideline for development of the properties within the Airport Safety Zones.~~

## **CIRCULATION PLAN**

### **Existing Circulation System**

Grove Avenue: Grove Avenue is a north-south street which runs from the southerly City limits of Ontario to the northerly City limits, including an interchange with the Pomona Freeway (State



-  Air Safety Zones
-  Airport Boundary

Source:  
City of Ontario General Plan  
update, 1991. Figure derived  
from Ontario International  
Airport Layout Plan:  
Airport Land Use handbook.

Route 60). Grove Avenue also passes by the west end of the Ontario International Airport. It is designated by the City of Ontario Master Plan of Streets and Highways as a six-lane divided arterial (108 feet right-of-way, 94 feet curb-to-curb) between the Pomona Freeway and Mission Boulevard (Exhibit 9).

A raised median, to be maintained by the City of Ontario, is included north of Belmont Street and south of Mission Boulevard.

Grove Avenue has a diamond interchange at the Pomona Freeway and has a traffic signal at the Mission Boulevard intersection. The Grove Avenue intersections at Philadelphia Street and Francis Avenue are signalized and there is an at-grade crossing of the Union Pacific Railroad tracks immediately north of Mission Boulevard. Grove Avenue serves as a major access route to the Ontario International Airport as it provides a direct link from Airport Drive to the Pomona Freeway.

Mission Boulevard: Mission Boulevard is an east-west four-lane divided arterial with an existing pavement width of 94 feet and an existing right-of-way width at Grove Avenue of 150 feet. It serves as a major Inland Empire link and runs from Riverside to Pomona (Exhibits 9, 10, and 11).

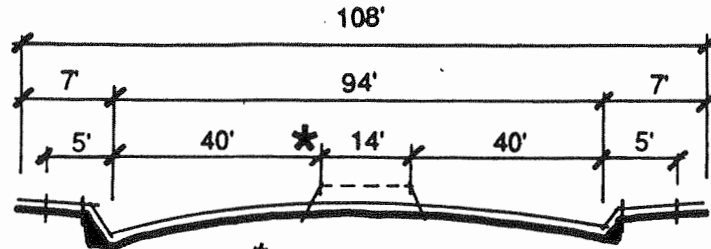
Francis Street: Francis Street is designated by the Master Plan of Streets as a four-lane arterial, which currently has two lanes west of Grove Avenue and four lanes east of Grove Avenue (Exhibit 10).

Philadelphia Street: Philadelphia Street is designated as a standard arterial with a 100 foot right-of-way and it is 76 feet wide from curb-to-curb (Exhibits 10 and 11).

### **Circulation Plan**

There are two major roadway improvement plans for the Grove Avenue Corridor. The first is a widening project for Grove Avenue between the Pomona Freeway and the Union Pacific Railroad tracks. Grove Avenue has been widened to its fully planned width of 94 feet from the westbound Pomona Freeway ramps to Belmont Street, and the widening of Grove Avenue between Belmont Street and Mission Boulevard is anticipated to be completed by the summer of 1995. These widths will accommodate 3 traffic lanes in each direction plus a dual left turn median lane in the 94-foot cross section. The widening of Grove Avenue has been funded by an assessment district.

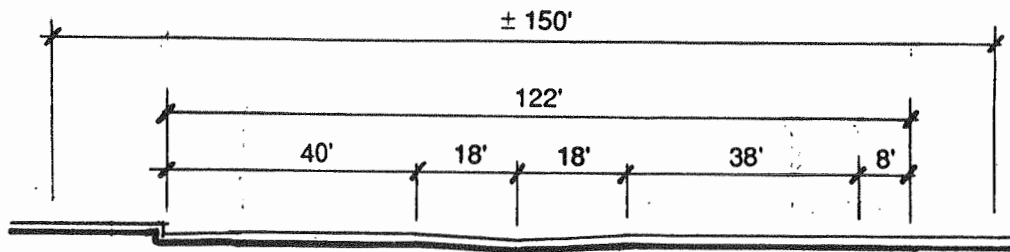
The second major roadway improvement planned for Grove Avenue is an underpass at the Union Pacific Railroad tracks which would separate the grades at this railroad crossing. The project would also result in a 94-foot pavement width for Grove Avenue between Belmont Street and



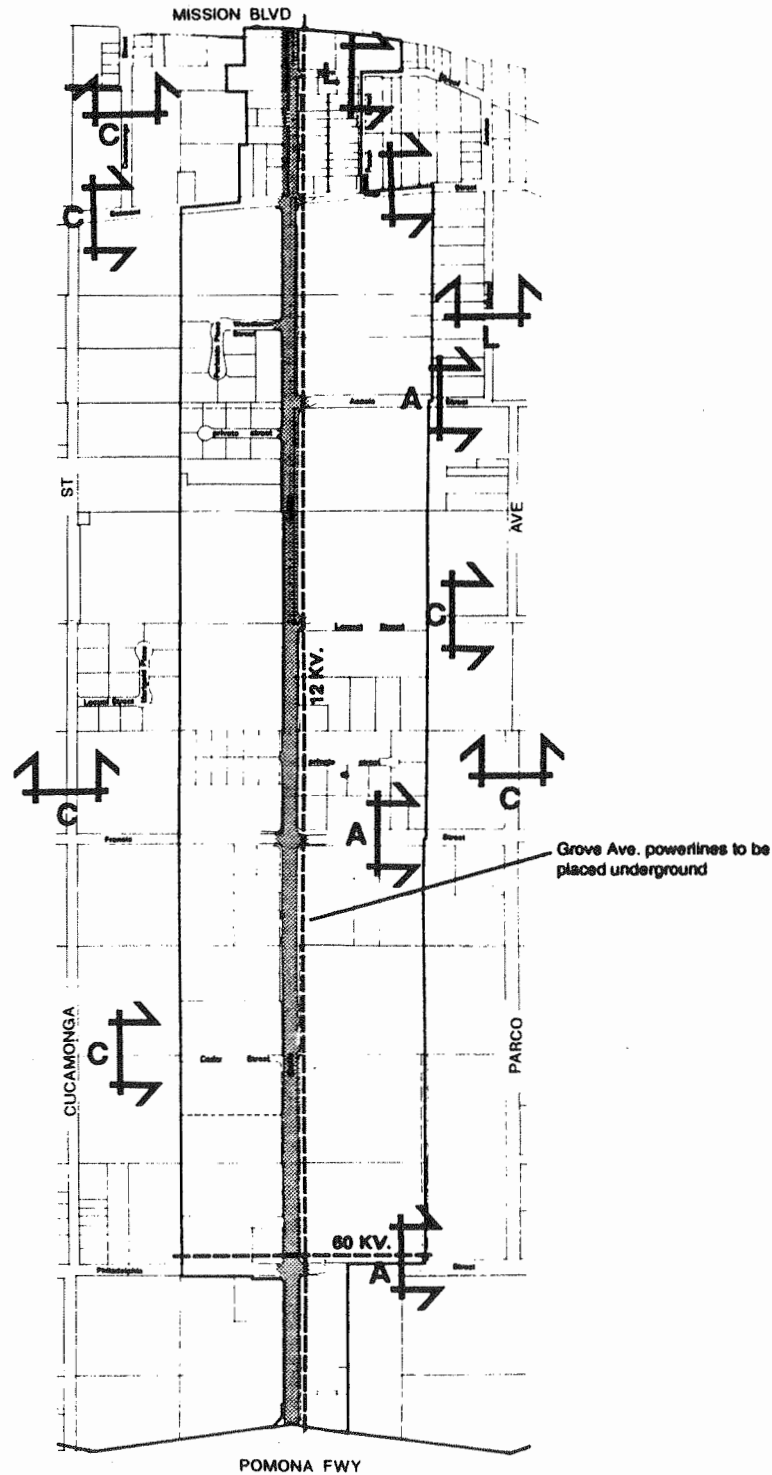
\* TWO - WAY  
TURN LANE

Raised median to be provided between Belmont and Mission Avenue

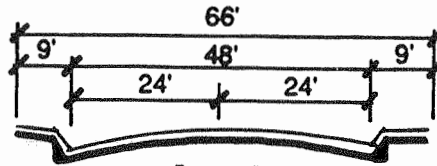
**Grove Avenue**



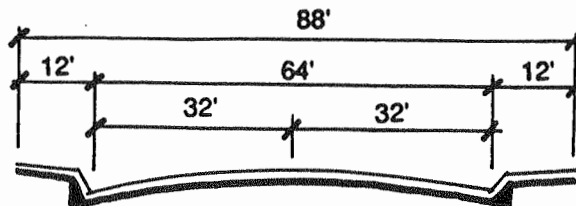
**Mission Boulevard**



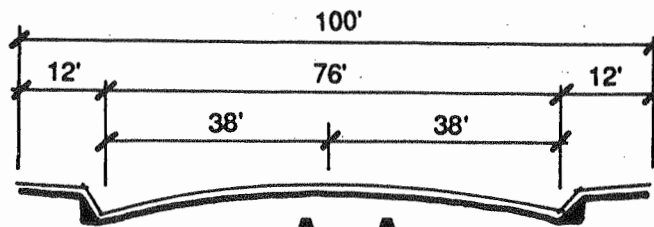
Source:  
City of Ontario  
Engineering Department



**L - L**  
Belmont Street  
Kern Street  
Mildred Avenue



**C - C**  
Cucamonga Street  
Locust Street  
Cedar Street  
Parco Avenue



**A - A**  
Philadelphia Street  
Francis Street  
Acacia Street  
Belmont Street

Airport Drive, which would accommodate three lanes in each direction plus a median turn lane. It is estimated that this project will be completed in four to six years.

The Circulation Plan for the Grove Avenue Specific Plan is shown in **Exhibit 12**. In addition to the improvements discussed above, roadway improvements will be required for several additional roadways, including improvements to Grove Avenue north of Belmont Street. The proposed General Plan designates two Class III bikeways to be constructed within the Grove Avenue Specific Plan area. Class III bikeways are designated by signs or surface marking and shared by motorists and pedestrians. The planned bikeways, shown on **Exhibit 12**, run along Mission Boulevard and Philadelphia Street, and are to be installed by the City of Ontario.

### **Transportation Demand Management Programs**

Transportation Demand (TDM) programs, required by the South Coast Air Quality Management District's Regulation XV, should reduce traffic volumes within the project vicinity. Proposed developments along Grove Avenue will participate in TDM programs designed to implement Regulation XV and other air quality mandates.

### **INFRASTRUCTURE PLAN**

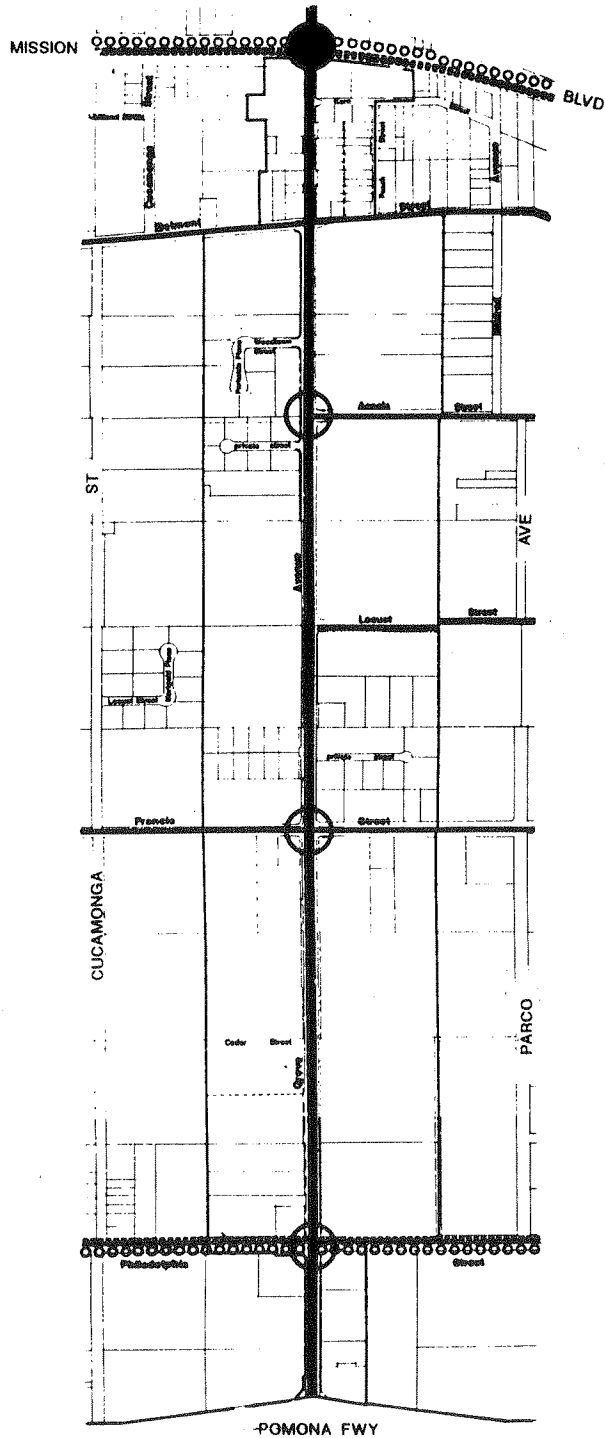
Note: For all facilities including, but not limited to street, water recycled water and sewer, development plans shall refer to the current Master Plans for proposed improvements.

#### **Water Concept Plan**

Ontario is served by its City Water Department, with most of its water provided by the Chino Basin Municipal Water District (CBMWD). The CBMWD is the court-designated Watermaster for the Chino Basin and is responsible for maintaining the quantity of groundwater underlying Ontario and much of western San Bernardino County. (Groundwater quality is the responsibility of the Santa Ana Regional Water Quality Control Board).

The basin has an estimated storage capacity of 13 million acre-feet. The legally designated annual safe yield from the Chino Groundwater Basin is 140,000 acre feet, which is the amount of groundwater that can be pumped from the basin while maintaining safe levels. This is not enough water to completely supply the growing population of Ontario and the surrounding area. Supplementary water is imported to make up the shortfall.

The Concept Water Plan for the Grove Avenue Specific Plan is shown on **Exhibit 13**. The Specific Plan area is within the Fourth Street water system except for the northwest corner. The area fronting on the west side of Grove Avenue between Mission Boulevard and Belmont Street is in the Eighth Street system. The Fourth Street system is basically a grid distribution system with a twelve-inch water main in Grove Avenue and twelve-inch water mains in Francis and Philadelphia Streets. To complete the grid water system, an eight-inch water line is proposed





**LEGEND**

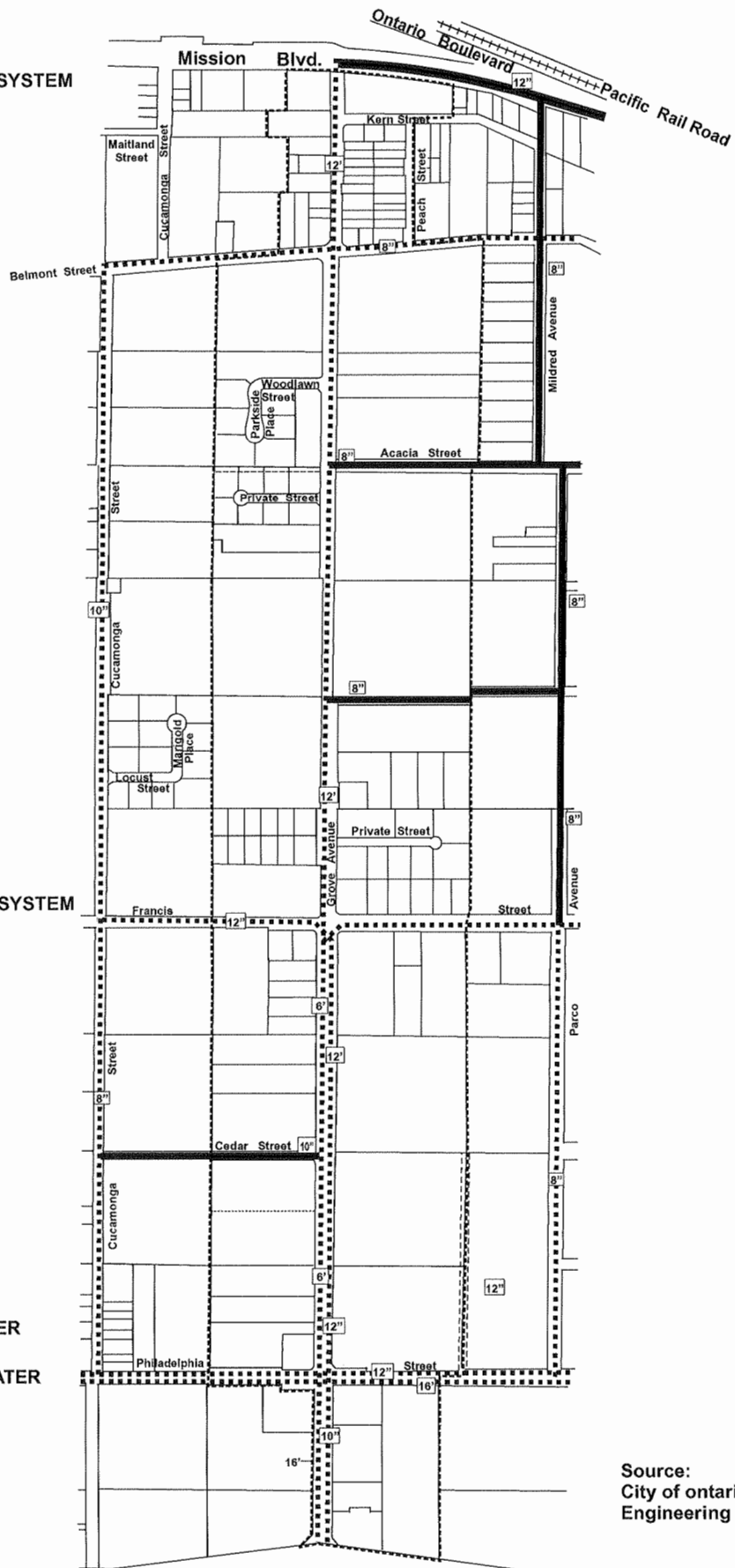
- DIVIDED ARTERIAL (6 LANES DIVIDED)
- STANDARD ARTERIAL (4 LANES DIVIDED)
- LOCAL STREET (2 LANES UNDIVIDED)
- PROPOSED CLASS 3 BIKEWAY
- EXISTING SIGNALIZATION
- PROPOSED SIGNALIZATION



8TH STREET SYSTEM

4TH STREET SYSTEM

-  EXISTING WATER
-  PROPOSED WATER



Source:  
City of Ontario  
Engineering Department

Pomona (60) Freeway



in Acacia Street, from Grove Avenue to Mildred Avenue. An eight-inch water line needs to be extended in Locust from its terminus easterly to Parco Avenue. A ten-inch water line needs to be constructed in Cedar Street. In addition, a ten-inch water line needs to be constructed in the Parco/Mildred alignment from Francis Street to Mission Boulevard.

As development occurs along the above streets, the development will be conditioned to build its portion of the water line. During the design review stage of a development project, if an off-site water line is needed to complete the loop system for fire protection, the development will qualify for a reimbursement agreement with the City.

Projected water demand for the build-out of the Grove Avenue Specific Plan area is as follows:

**TABLE 3  
PROJECTED WATER CONSUMPTION**

<b>Land Use</b>	<b>Usage Factors</b>	<b>Square Feet</b>	<b>Total Demand (gallons)</b>
Business Park	100 gal./day/1,000 sq. ft.	1,952,403	195,240
Office/Commercial	100 gal./day/1,000 sq. ft.	1,060,207	106,020
<b>Total:</b>		<b>3,012,610</b>	<b>301,260</b>

Source: Ontario General Plan Environmental Impact Report, October 1991.

### **Wastewater System and Treatment**

Wastewater (sewage) is collected primarily by lines owned and maintained by the City of Ontario. The collected sewage is then treated by the Chino Basin Municipal Water District (CBMWD), which provides primary, secondary and tertiary treatment. The District owns and maintains all interceptor systems and water reclamation plants to use/dispose of wastewater. The expansion or construction of new regional facilities is also the responsibility of CBMWD.

The CBMWD operates two systems. A nonreclaimable wastewater line is used to export industrial and other nonreclaimable wastes from the basin. These wastes are transported via underground pipelines to the Los Angeles County Sanitation District for treatment at Harbor Island and ocean disposal. Reclaimable wastewater is piped to CBMWD Regional Plant #1, which is located between Vineyard and Archibald Avenues in the south central portion of Ontario. Reclaimed water is used to irrigate the adjacent municipal golf course, and is also used to meet CBMWD obligations to deliver Santa Ana River water to Orange County.

Chino Basin Municipal Water District has a ten-year capital improvement program to increase capacities of the various treatment plants in the area. There is no apparent capacity problem in the near future. The City is presently updating its sewer system master plan. This update is anticipated to be completed by late 1992. There are no known deficiencies in the Grove Avenue Specific Plan area of the City's sewer system at this time. The Master Plan study will provide an updated evaluation of sewer capacity. As development occurs on Grove Avenue, individual property owners will extend sewers onto their site and extend master plan sewers within future roadways.

The Concept Sewer Plan for the Grove Avenue Specific Plan area is shown in **Exhibit 14**. An eight-inch sewer line runs in Grove Avenue north of Francis Street and a ten-inch sewer line runs in Grove Avenue south of Francis Street to Philadelphia Street. Additional existing and proposed sewer lines are required as shown on the Concept Sewer Plan.

Project sewage demand for the build-out of the Grove Avenue Specific Plan area is as follows:

**TABLE 4  
PROJECTED SEWER DEMAND**

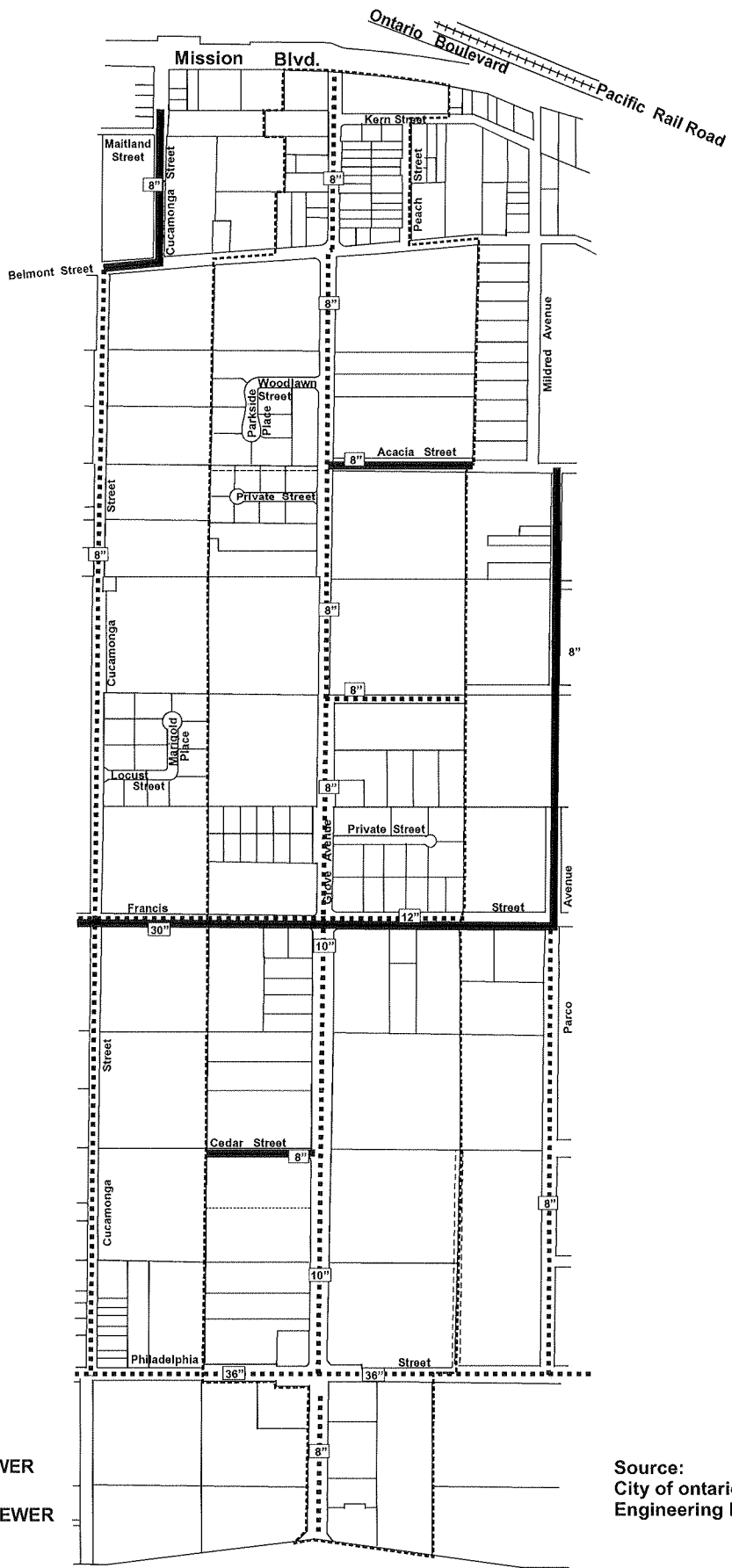
<b>Land Use</b>	<b>Usage Factors</b>	<b>Square Feet</b>	<b>Total Demand (gallons)</b>
Business Park	100 gal./day/1,000 sq. ft.	1,952,403	195,240
Office/Commercial	100 gal./day/1,000 sq. ft.	1,060,207	106,020
<b>Total:</b>		<b>3,012,610</b>	<b>301,260</b>



Source: Ontario General Plan Environmental Impact Report, October 1990.

### **Storm Drain Facilities**

The Grove Avenue Specific Plan area is within the West Cucamonga Creek Drainage area. West Cucamonga Creek is a San Bernardino County Flood Control facility and is built to carry the 100-year design flow. West Cucamonga Creek is located approximately one-half mile east of Grove Avenue.

The Concept Storm Drain Plan is shown on **Exhibit 15**. As part of the Grove Avenue assessment district, storm drain facilities were installed in Francis Street and Grove Avenue north of Francis; however, this system was primarily designed to handle the storm water from the area west of Grove Avenue and north of Francis Street. The following storm drain improvements are needed to serve the Specific Plan area and surrounding areas:



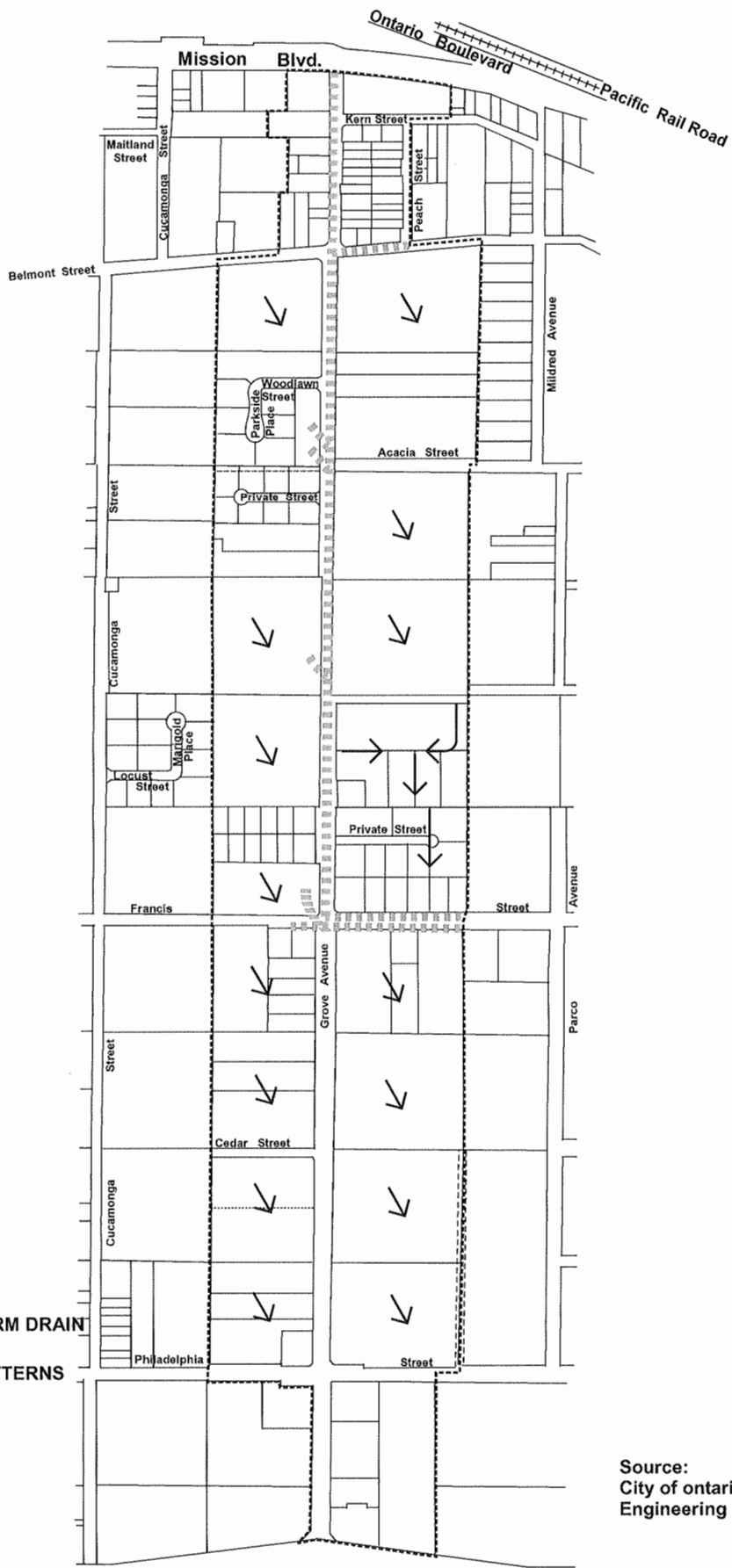
-  EXISTING SEWER
-  PROPOSED SEWER

Source:  
City of Ontario  
Engineering Department

**Concept Sewer Plan**

**Exhibit 14**





Source:  
City of Ontario  
Engineering Department

**Concept Storm Drain Plan**

**Exhibit 15**



1. A 66-inch reinforced concrete pipe system from West Cucamonga Creek, westerly in Acacia to approximately 300 feet west of Mildred Avenue;
2. A 54-inch reinforced concrete pipe system, beginning at West Cucamonga Creek, extending west in Locust Street to 300 feet west of Parco Avenue; and
3. A 72-inch reinforced concrete pipe system in Philadelphia, beginning at West Cucamonga Creek, going west all the way to Grove Avenue.

As precise plans for development along Grove Avenue are submitted to the City, complete hydrology studies and storm drain designs will be required.

### **Solid Waste Disposal**

The City of Ontario provides solid waste disposal service throughout the City, including the project site. Four and one-half refuse trucks currently service the City's commercial areas. Three refuse trucks currently serve the industrial areas. Solid waste collected within the project site will be transported to San Bernardino County's Milliken Landfill. The Solid Waste Superintendent will approve the type, size, quantity, and location of all solid waste receptacles and recyclable facilities. All refuse and recyclable facilities enclosures shall be constructed to City specifications.

### **Natural Gas**

The Southern California Gas Company provides natural gas service to the City of Ontario. There is presently a two- to four-inch, high pressure gas main within the Grove Avenue right-of-way. Service will be in accordance with the Company's policies and extension of service rules on file with the California Public Utilities Commission.

### **Electricity**

Electricity to the Grove Avenue Specific Plan is provided by the Southern California Edison Company. Electric power lines along Grove Avenue are being undergrounded as part of the Grove Avenue Improvement Plan.

### **Telephone**

Telephone service to the Specific Plan area is provided by the General Telephone Company (GTE). There are existing facilities on Grove Avenue that will serve the site. All telephone

lines within the site will be placed underground within dedicated public streets, dedicated public utility easements along streets, or in dedicated easements within private streets, as approved by the City engineer and GTE. General Telephone will maintain telephone facilities.

## **4. DESIGN GUIDELINES**

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### **SITE PLANNING GUIDELINES**

In addition to the specific plan development standard contained in Section 5, the Specific Plan contains detailed design guidelines. These guidelines specify criteria for such items as landscaping, signage, site planning, architectural treatment, etc. It is the purpose of these guidelines to be used to implement the goals and objectives of the Specific Plan. Other purposes of the design guidelines are:

- To provide development standards to ensure that the Specific Plan area will develop in accordance with the quality and character proposed herein.
- To provide guidance to property owners, developers, builders, engineers, architects, landscape architects, and other professionals in order to maintain the desired design quality.
- To provide guidance to City staff, the Planning Commission, and the City Council in the review of future development projects in the Specific Plan area.

The design guidelines are intended to be flexible so that over time, they can respond to unanticipated conditions, such as changes in economic conditions, community desires, and the marketplace. Deviation from the guidelines listed below may be allowed if project proponents can demonstrate that it would result in a superior design.

The design concept for Grove Avenue is intended to provide a consistent, high quality image and cohesive integration of all parcels within the Grove Avenue Specific Plan area. The hierarchy of design elements within the Specific Plan area, as well as an orderly transition of land use intensity, serve to reinforce the overall desired image for the Grove Avenue Corridor. The essential site planning elements include building location and orientation, access, circulation, parking, service areas, grading and drainage. Each element is discussed in this section of the Specific Plan and identified with a numerical reference.

#### **Building Location and Orientation**

The most visible and continuous expression of image within a development is the character of its streetscape. The location and orientation of each building along the street is important due to its direct visual impact.



Building setbacks have been established to reinforce visual continuity, reduce massing, and provide landscape opportunities between buildings, roads, parking and adjacent parcels.

**Grove Avenue** -- The streetscape zones for Grove Avenue require a minimum building setback of forty feet for the entire length of the Specific Plan Area from the Pomona Freeway (State Route 60) to Mission Boulevard. Included within the setbacks between the 60 Freeway and Belmont is a landscape zone averaging twenty-five (25) feet with a minimum of twenty (20) feet, as measured from the property line. North of Belmont, the landscape zone is reduced to an average of twenty (20) feet, with a minimum of fifteen (15) feet.

**Secondary Arterials** -- A secondary streetscape zone is required along perpendicular streets in order to establish an arrival sequence which reinforces the streetscape zones and appearance of Grove Avenue. This secondary streetscape zone requires a minimum building setback of thirty (30) feet. Within the building setback is a minimum landscape zone of fifteen (15) feet with an average of twenty (20) feet.

Buildings should be oriented to promote visibility and should be suitable to site conditions, as well as be compatible with the overall design concept of the Specific Plan and surrounding development. ~~The building setback from a rear or side yard is a minimum of ten (10) feet.~~

The following guidelines apply to all development proposals within the Specific Plan area:

1. Buildings shall be arranged to provide safe and efficient access and circulation for both vehicles and pedestrians.
2. Buildings shall be oriented to provide views into the site and to promote the development's visibility, while maintaining compatibility with adjacent buildings and setbacks.
3. Building placement shall properly address unique site conditions.
4. Long, straight, continuous stretches of one structural or planning element, such as building facade or parking, shall be avoided.
5. Shapes and forms of buildings and other structural elements shall be varied and articulated to create visual interest.
6. Massing and scale of buildings along street, landscape zones, and between buildings shall be reduced in order to reduce physical and visual impact.
7. Buildings shall be oriented so as to obscure service areas, loading facilities, and other unsightly ancillary activities from view of nearby roadways.

8. All ancillary structures such as trash enclosures and storage facilities shall be located away from view of nearby roadways and screened to reduce visual impact.
9. Open plazas and courtyards shall be encouraged.

### **Access and Circulation**

The character of the streetscape along Grove Avenue represents the most visible and continuous expression of image. The design treatment for each project should reinforce the overall image for Grove Avenue. This is especially important where the continuity of the streetscape is disrupted by a significant break, as with a project entry, which should reinforce the corridor image, maintain a sense of continuity, and establish an arrival sequence.

10. Access points along Grove Avenue shall be limited to minimize vehicular conflicts and congestion.
  - Access shall be taken from adjacent and perpendicular streets where possible.
  - Shared access drives shall be incorporated where possible to reduce curb cuts, promote more efficient traffic movement, and enhance Grove Avenue's image.
  - To limit access points along Grove Avenue, the following minimum distances shall be followed: (a) two hundred (200) feet between curb returns and a collector street and (b) three hundred (300) feet between driveways.
11. The parcel entry areas shall provide the visitor with an overview of the parcel and a well articulated entry sequence from the street to the building. Hardscape and landscape treatments shall be used to create a sense of arrival through color, texture, signage and change of scale.
12. Pedestrian access from adjacent streets, parcels, and parking areas shall be provided to all building entries and integrated into the overall site design.
13. Vehicular circulation shall be safe, direct, and efficient. Vehicular and pedestrian traffic shall be kept separate in order to minimize potential conflicts. This can be accomplished by using special paving, landscaping and lighting.
14. Provisions for bus shelters, bus turnouts, and bus stops shall be included in each application for a development project, as appropriate.
15. Bicycle lockers, bicycle wheel locks, or bicycle storage areas shall be provided, as appropriate.

## **Parking**

Project design should reduce the physical and visual impacts of parking areas, minimize conflicts with pedestrian activities, and provide adequate and convenient vehicular access. The design of parking areas should follow the guidelines listed below:

16. Long, unbroken masses of parking shall be avoided.
17. Parked vehicles shall not overhang sidewalks.
18. Landscape and berming shall be used to screen parking areas from view of nearby roadways.
19. Parking spaces shall be provided in convenient locations for handicap, carpool, motorcycle and bicycle parking.
20. Dead end parking aisles shall be avoided.
21. No on-street parking shall be permitted. A sufficient amount of on-site parking shall be provided in order to accommodate all vehicles associated with the use of each site.
22. Parking areas shall not be located within any required landscape zones.
23. Parking areas shall include landscaping to provide shade and soften the appearance (see Landscape Guidelines). Spaces appropriate for planting are to be provided within parking areas.
24. Structured parking shall be screened by landscaping and/or architectural design and conform to the same setback and height requirements as other buildings (see Development Standards).

## **Service Areas**

Service, storage, maintenance, loading and trash collection areas must be located out of or screened from view of streets, public areas and buildings on adjacent sites (see Landscape Guidelines).

25. Service elements such as loading doors shall be integrated with the facade in an appropriate and organized manner.
26. All service, storage, maintenance, loading and trash collection areas shall adhere to building and parking setback requirements.

27. No on-street vehicle loading or unloading shall be permitted. Provisions shall be made on-site for any loading activities. Parallel parking spaces for delivery trucks should be provided along service drives or in specially designated courts or loading docks.
28. All materials, supplies and equipment, including trucks or other vehicles stored on-site must be enclosed in a building or a screened area.

### **Grading and Drainage**

All grading is to be completed and all drainage facilities are to be constructed to City of Ontario standards.

29. Grading and drainage shall be designed so that surface drainage and soil erosion do not flow onto adjacent parcels.
30. All parking areas shall be designed to drain to a single swale if feasible; however, center swale drainage patterns are permitted.
31. Naturalized swales shall be used where feasible throughout landscape areas.
32. Open concrete drainage channels shall be prohibited within streetscape areas.
33. Concrete swales shall be minimized and shall be screened from view or incorporated into landscape design with a shrub screen, dry creekbed, etc.
34. On-site grading shall be designed to accommodate and be compatible with surrounding streetscape grades, adjacent pads and open space.

### **Utilities**

All utilities are to be installed in accordance with City of Ontario requirements, and the following guidelines:

35. Utilities systems, including water, electricity, gas, sewer and storm drains shall be installed underground.
36. Any above-ground equipment (such as transformers and controllers) shall be screened from off-site views.
37. Utility plans shall be submitted along with site plans.

## **LANDSCAPING GUIDELINES**

The landscape element, as an integral element in achieving the intended image, identifies the primary landscaping theme that will visually enhance the character of the Grove Avenue Specific Plan area. The landscape plan establishes a framework for consistency between individual parcels throughout the entire Specific Plan area. This section also provides guidelines for the implementation of the Landscape Concept Plan. Elements of this plan include streetscape zones, buffer and rear-yard treatments, corner statements, as well as plant palettes for each of these areas and properties within the Specific Plan area. The Landscape Concept Plan identifies the primary landscaping elements that will visually emphasize the character of the Grove Avenue Specific Plan area. The Landscape Concept Plan for the Grove Avenue Specific Plan area is shown in **Exhibit 16**.

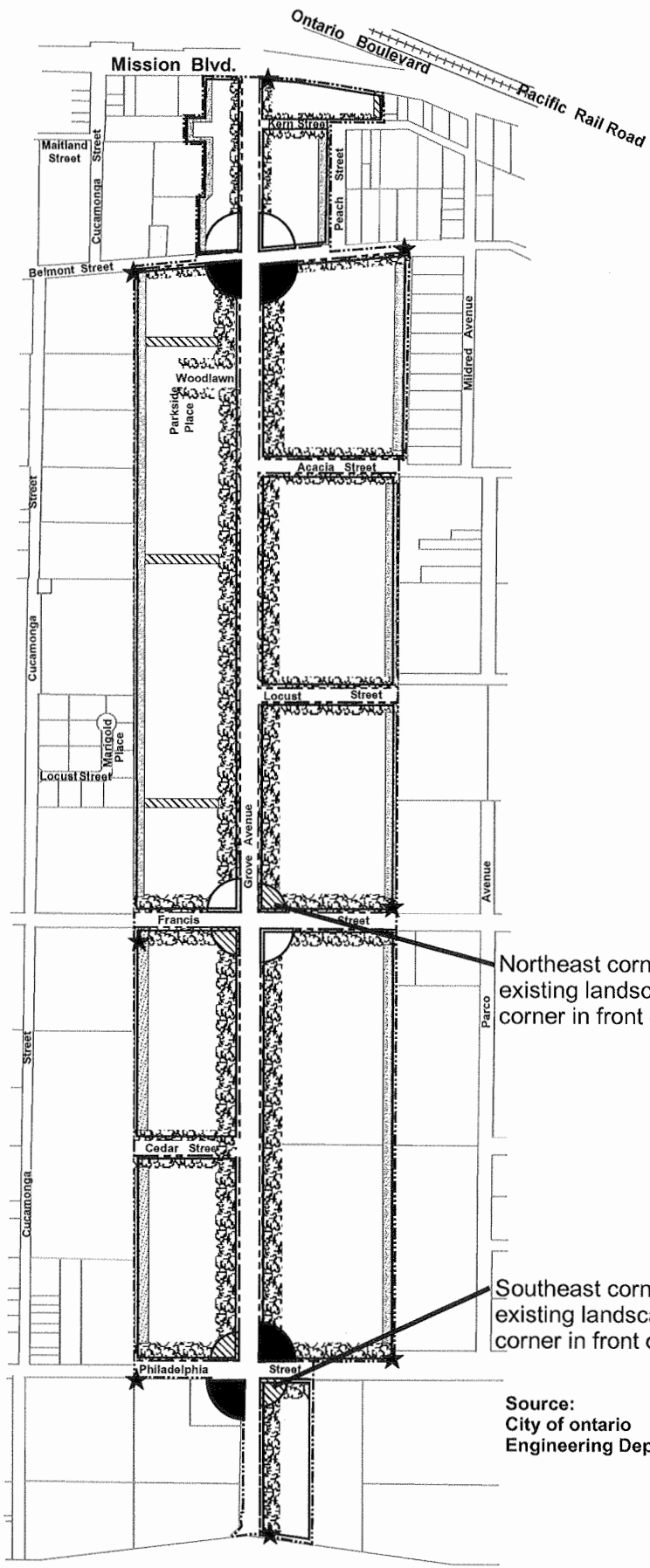
The elements of the Grove Avenue landscape concept are provided below, including major, minor and accent corner treatments at intersections, major and minor streetscape treatment, buffer areas between properties and in rear-yards. The landscape concept encourages the use of hardscape such as walls, planters special pavings, along with berms. These are to be balanced with a variety of plants materials in creating a cohesive and visual interesting landscape theme for the Specific Plan area.

### **Landscape Design Goals and Objectives**

- To enhance the visual character of the Grove Avenue Corridor.
- To reduce emphasis on vehicular impact by careful placement of parking lots, and screening of same from view.
- To emphasize pedestrian access and circulation, especially between and around buildings.
- To use repetition of various landscape treatments throughout the Grove Avenue Corridor to tie buildings together.
- To use drought tolerant, hydrozone and/or xeriscape techniques and materials, where feasible.

### **Existing Streetscapes**

There are a number of existing businesses along Grove Avenue. These will be encouraged, but not required, to implement the Specific Plan landscape guidelines. However, as redevelopment opportunities arise, approvals may be conditioned upon implementation of these landscape guidelines, where feasible.



Northeast corner to replicate existing landscape at southwest corner in front of Farmer Boys.

Southeast corner to replicate existing landscape at northwest corner in front of Arco.

Source:  
City of Ontario  
Engineering Department

LEGEND	
	MAJOR STREET LANDSCAPE TREATMENT
	MINOR STREET LANDSCAPE
	SIDE YARD BUFFER AREA
	REAR YARD BARRIER
	MAJOR CORNER TREATMENT
	MINOR CORNER TREATMENT
	ACCENT CORNER TREATMENT
	ENTRY MONUMENTS

Pomona (60) Freeway



In addition, an extensive landscape program has been adopted for Mission Boulevard which creates a sequence of landscape experience to distinctly identify the regional importance of this transportation corridor within the City of Ontario. The Landscape Concept Plan for Grove Avenue defers to the Mission Boulevard Plan where they intersect. Landscape/streetscape improvements are to transition between the landscape improvements along Mission Boulevard and Grove Avenue.

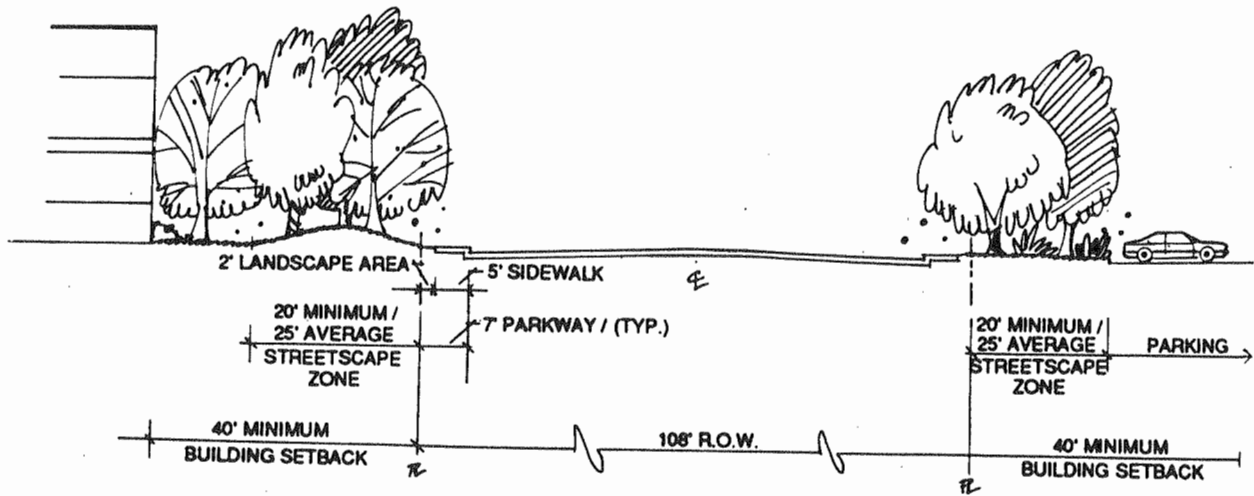
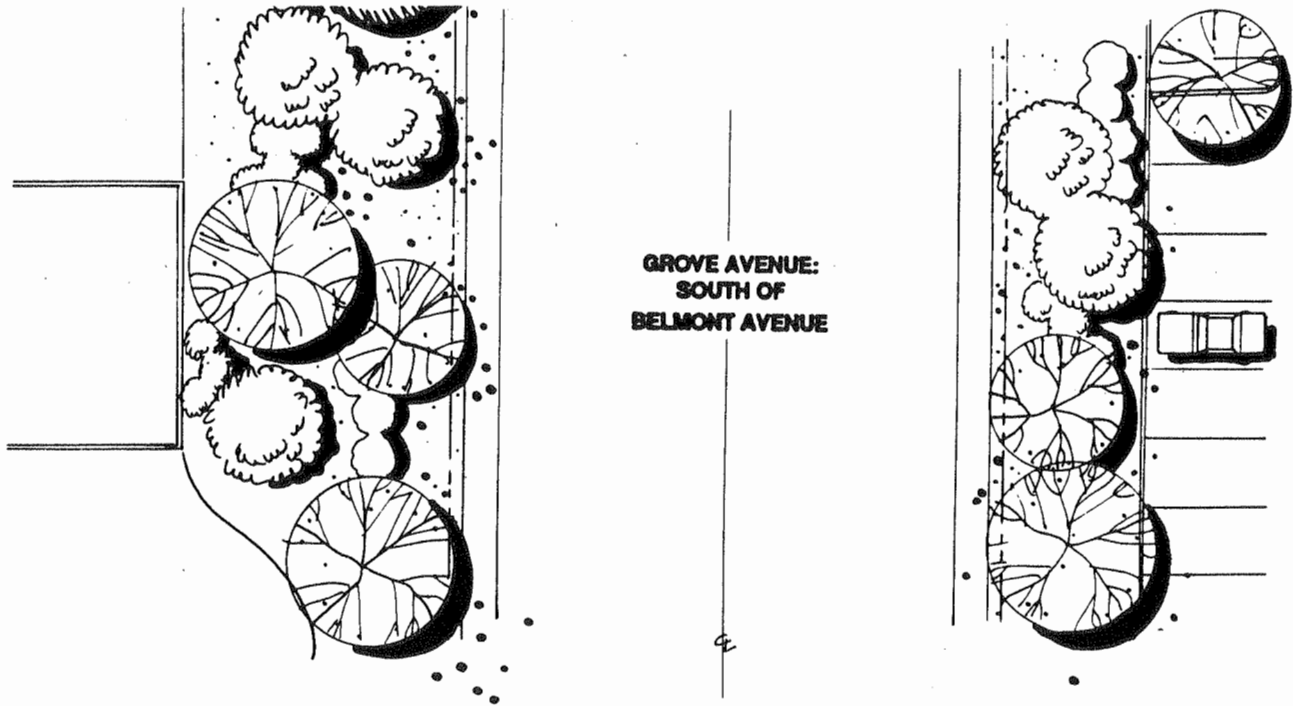
### **Major and Minor Streetscapes**

The landscape setback areas for the major streetscape areas which include Grove Avenue south of Belmont Street, Francis Street and Philadelphia Street shall be a minimum of 20 feet with a 25-foot average. This will be measured from the edge of the rights-of-way which includes a 5-foot sidewalk and 2-foot planted parkway (**Exhibit 17**). The Landscape Concept Plan for Grove Avenue should be followed in the design of landscaping at the on-ramps/off-ramps of the Pomona (State Route 60) Freeway, and will provide a harmonious transition to the Union Pacific Railroad Underpass project.

Along the minor streetscape areas which include Grove Avenue north of Belmont Street and all other side streets that intersect with Grove Avenue within the Specific Plan area, the landscape setback areas shall be a minimum of 15 feet with an average of 20 feet. Again the streetscape areas will be measured from the edge of the roadway rights-of-way (**Exhibit 18**).

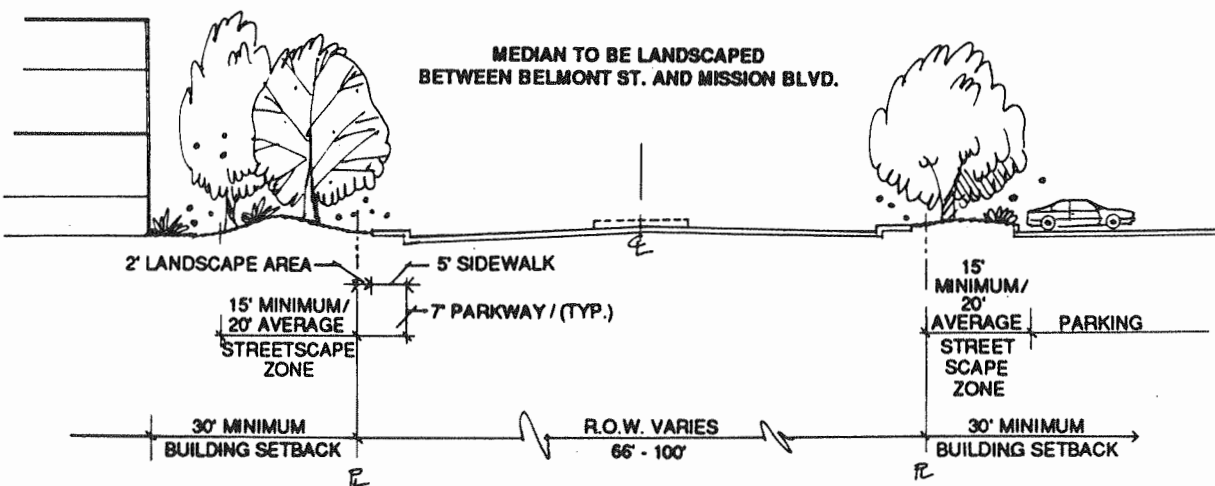
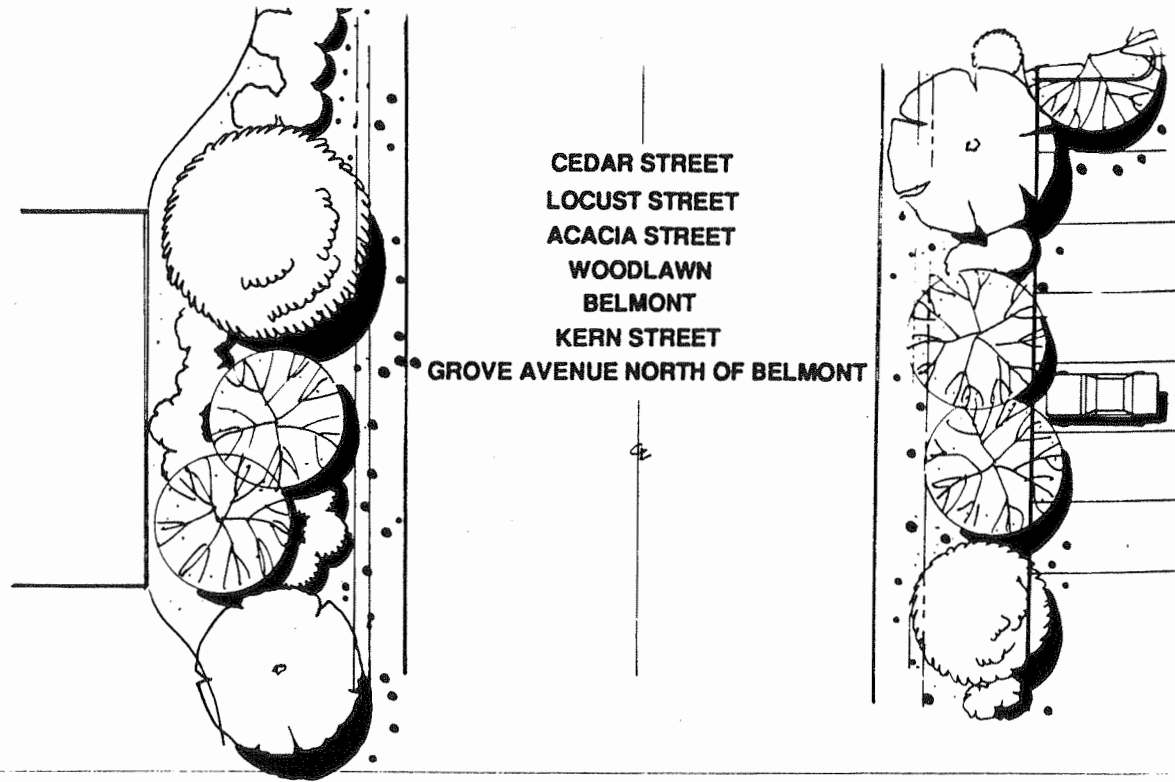
The landscape theme is an informal streetscape design. This will entail using broad expanses of several different ground covers and turf, clusters of shrubs meandering through the landscape setback areas and a mix of deciduous and evergreen trees, of varying height, form, texture and color.

The design treatment will provide visual continuity and interest throughout the Grove Avenue Specific Plan area, while providing visual links with the existing landscape treatments. The mixture of plants will be clustered and will vary as the plantings meander through the streetscape areas. This will create a cohesive and aesthetic image for the Specific Plan area, framing views of the buildings while screening unwanted elements. The use of groundcovers in-lieu of or in addition to turf will vary the texture of the ground plane as well as emphasizing the lower maintenance, water conserving plant palette. To attain the desired planting effect, trees should be planted based on an average of 35 feet on-center. Since the trees will be informally clustered, this average will determine the minimum number of trees that are to be planted per street frontage. This is an average and tree spacing may vary depending on the species selected.



Source: City of Ontario  
Public Facilities  
Development





Source: City of Ontario  
Public Facilities  
Development

## Major and Minor Streetscape - Plant Palette

### Trees:

Evergreen -- 60%;

- Cinnamomum camphora -- Camphor Tree; (Major streetscape)
- Giejera parviflora -- Australian Willow; (Minor streetscape)
- Chamaerops Humilis -- Mediterranean Fan Palm

Deciduous -- 30%;

- Eucalyptus ssp. - No Common Name
- Platanous acerifolia "yarwoodii" -- California Sycamore
- Ulmus parifolia - Evergreen Elm

Accent -- 10%;

- Cotinus coggyria -- Smoke Tree;
- Jacaranda mimosifolia -- Jacaranda;
- Liquidambar styraciflua -- Sweet Gum
- Pyrus kawakamii -- Evergreen Pear

*The designated street trees are:*

*Giejera parviflora along Grove Avenue north of Belmont Street;*

*Cinnamomum camphora along Grove Avenue south of Belmont Street; and*

*Ulmus parvifolia and Liquidambar styraciflua along the cross streets.*

*(Additional tree species as listed shall be incorporated into the informal and natural plantings.)*

### Shrubs:

- Abelia grandiflora -- Glossy Abelia
- Phormium tenax -- New Zealand Flax
- Photinia fraseri -- No Common Name
- Pittosporum tobira -- Tobira
- Raphiolepis indica -- India Hawthorn

### Groundcovers:

- Acacia redolens 'prostrata' -- No Common Name
- Gazania 'Matsua' -- No Common Name
- Hypericum calycinum -- Creeping St. Johnswort
- Myoporum parvifolium -- No Common Name
- Osteospermum fruticosum -- African Daisy

- Tall Fescue (Marathon) -- (No more than 30% of total area)
- Trachelospermum jasminoides -- Star Jasmine

### **Rear Yard and Side Yard Buffer Areas**

Landscaped buffer areas are to be provided between properties within the Specific Plan area as well as between on-site rear yards and off-site properties. Landscaped buffer areas shall not extend into the streetscape zones. Rear yard buffer areas shall be a minimum of 5 feet with a 10-foot building setback. Side yard landscaped buffer areas shall be a minimum of 5 feet with a 10-foot building setback. Side yard buffer areas shall extend from the rear property line to the edge of the streetscape zone.

Side yard buffer areas will be planted with tall shrubs planted and pruned to form hedge or shrubs or vines may be combined with walls and/or berms (**Exhibit 19**). Rear-yard buffer areas will use vines combined with walls. Each segment (from property line to property line in the rear or property line to streetscape zone on the side) should be planted with one plant species. These buffer areas are narrow spaces, so more than one species is inappropriate.

This treatment will be used to screen views between separate parcels as well as eliminate views between the site and existing off-site uses. The shrubs are to be installed in rows to form a hedge. Vines planted in conjunction with walls should be planted to provide complete coverage of the wall. Spacing will vary depending on the species that are selected.

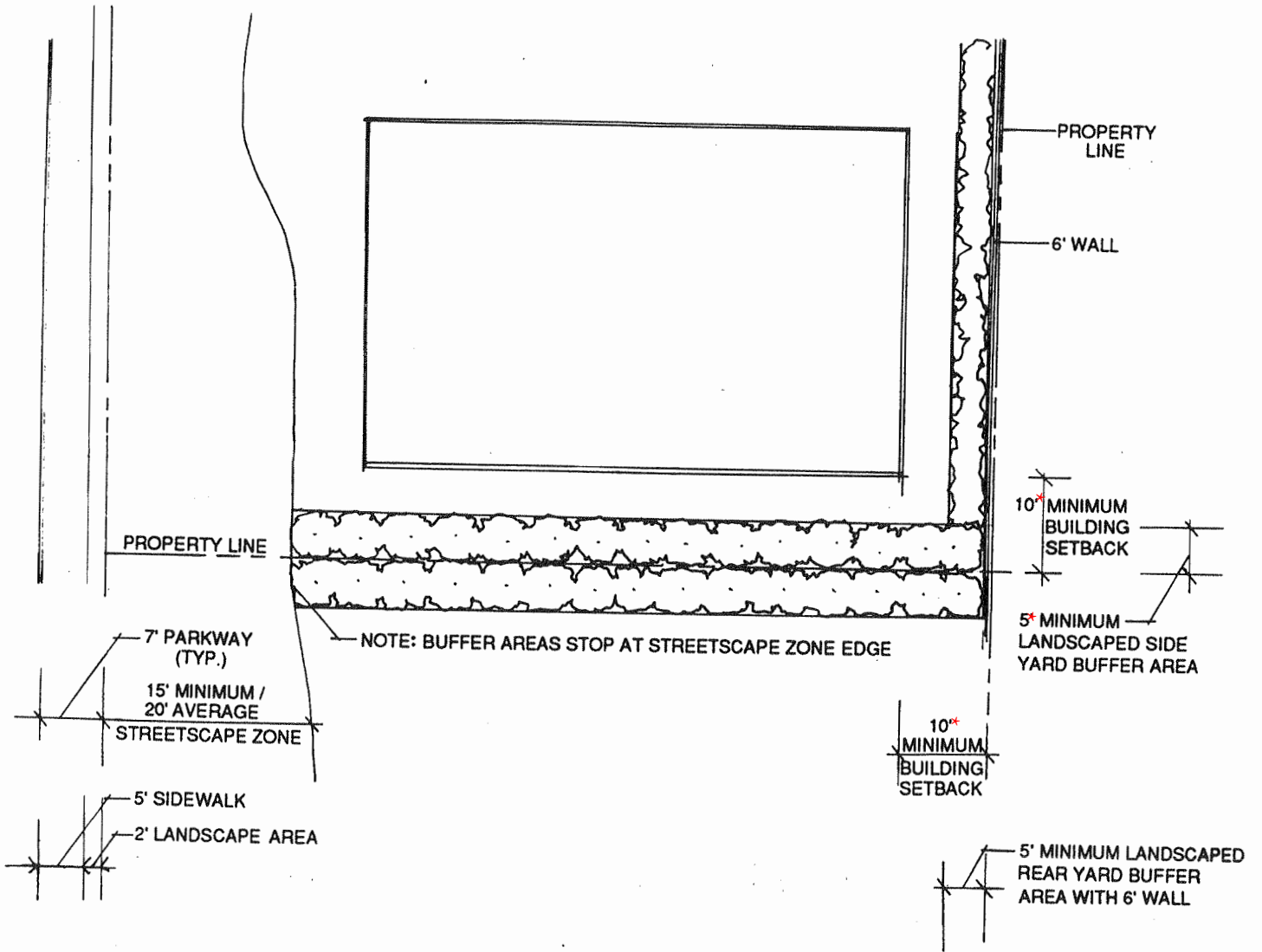
### **Side yard and Rear yard Buffer Area - Plant Palette**

#### **Trees:**

- Eucalyptus polyanthemos -- Silver Dollar Gum
- Liquidambar styraciflua -- Sweet Gum
- Pittosporum crassifolium -- No Common Name
- Podocarpis gracillor -- Fern Pine
- Pyrus kawakamii -- Evergreen Pear

#### **Shrubs:**

- Ligustrum japonica -- Japanese Privet
- Nerium oleander 'Petite Pink' -- Dwarf Oleander
- Photinia fraseri -- No Common Name
- Pittosporum tobira -- Tobira
- Prunus caroliniana -- Carolina Laurel Cherry



\*0-foot building setback allowed for interior or rear property lines adjacent to a building or within a fully screened loading and storage yard area

Source: City of Ontario  
Public Facilities  
Development

- *Prunus ilicifolia* -- Hollyleaf Cherry
- *Prunus lyonii* -- Catalina Cherry

#### Vines:

- *Bougainvillea* -- No Common Name
- *Ficus pumila* -- Creeping Fig
- *Gelsemium sempervirens* -- Carolina Jessamine
- *Parthenocissus tricuspidata* -- Boston Ivy
- *Wisteria* Spp. -- No Common Name

#### Groundcovers:

- *Acacia reddens* 'prostrata' -- No Common Name
- *Gazania* 'Matsua' -- No Common Name
- *Hemerocallis* -- Day Lily
- *Hypericum calycinum* -- Creeping St. Johnswort
- *Rosmarinus officianalis* -- Rosemary
- Tall Fescue -- Turf

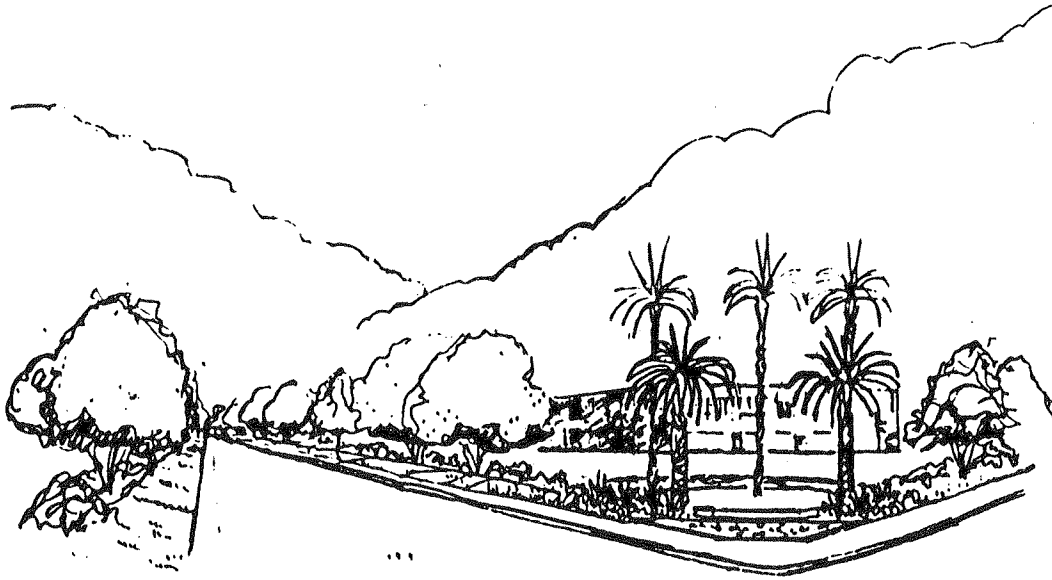
#### Corner Treatment

Within the Grove Avenue Specific Plan area, there are a series of intersections where there are opportunities for major, minor and accent corner treatments (**Exhibits 20, 21 and 22**). The major corner treatment areas are sited along Grove Avenue on the southwestern and southeastern corners at Belmont Street and on the northeastern and southwestern corners of Philadelphia Street. The minor corner treatment areas are sited along Grove Avenue at the northwestern and southeastern corners at Francis Street and at the northeast and northwest corners of Belmont Street. Accent corners are sited along Grove Avenue on the southwest and northeast corners at Francis Street and on the northwest and southeast at Philadelphia Street.

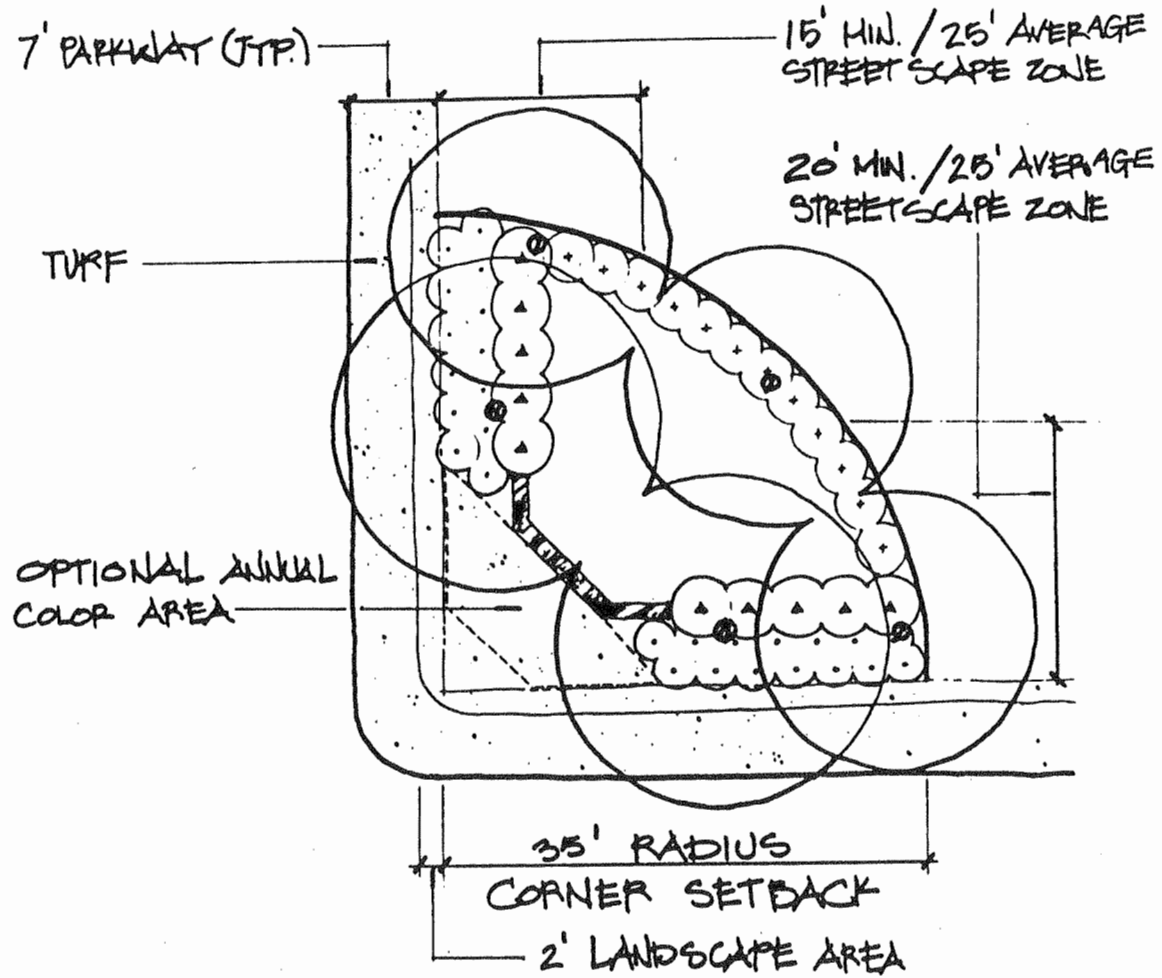
The setbacks at the corners are as follows:

- \* Major Corner Setback - 35-foot Radius;
- \* Minor Corner Setback - 25-foot Radius; and
- \* Accent Corner Setback - 15-foot Radius, or less if constrained by an existing development area.

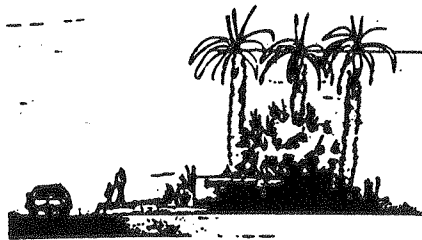
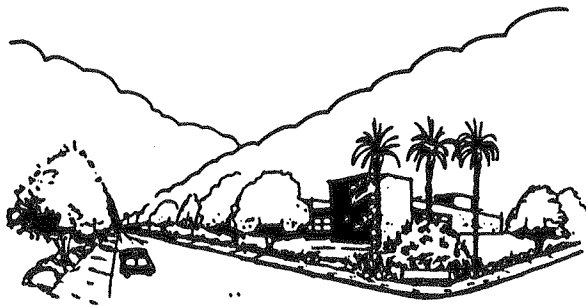
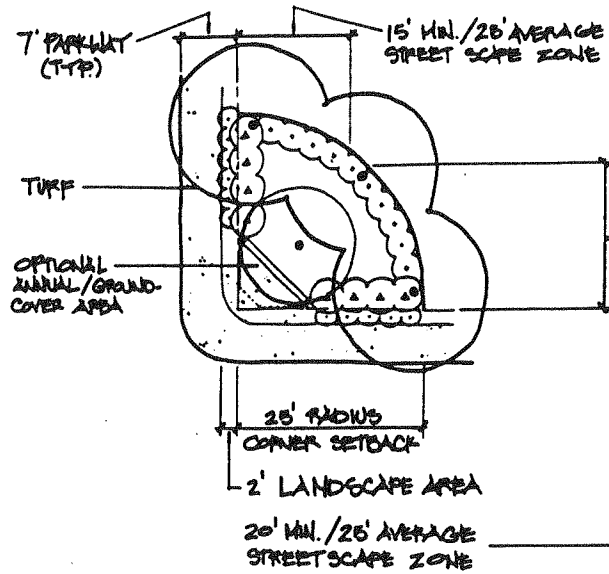
All three corner treatment configurations are to be formal plantings. The general design then consists of a background planting of shrubs with long, attenuated, strappy leaves, a half circle of palms, and the foreground is planted with a flowering evergreen groundcover. The textures and forms of the plants provide the basis for the planting design. Visual focal points are created that denote special corners while complimenting the streetscape zones. The strappy form of the



Source: City of Ontario  
Public Facilities  
Development



Source: City of Ontario  
Public Facilities  
Development



Source: City of Ontario  
Public Facilities  
Development



shrubs provides a backdrop for the palms while repeating the shape of the palm crown at the street level. The groundcover provides a soft mounding carpet of green in the foreground. In addition, the shrubs and groundcover are to be used informally within the streetscape zone, providing visual continuity along Grove Avenue.

### **Major Corner Treatment Area - Plant Palette**

#### **Trees:**

- Phoenix canariensis - Canary Island Palm, 15' brown trunk height (bth)
- Phoenix dactylifera - Date Palm, 25' bth

#### **Shrubs:**

- Hemerocallis 'Yellow' -- Yellow Day Lily
- Ligustrum japonica -- Japanese Privet
- Phormium tenax - New Zealand Flax

#### **Groundcovers:**

- Agapanthus africanus -- Lily of the Nile
- Annual color/turf (optional)
- Gazania 'Mitsuwa Yellow' - No Common Name
- Trachelospermum jasminoides - Star Jasmine

### **Minor Corner Treatment Area - Plant Palette**

#### **Trees:**

- Chamerops humilis - Mediterranean Fan Palm, 36" box (multi-trunk)
- Phoenix dactylifera - Date Palm, 20' bth (25' bth on Belmont Street only)

#### **Shrubs:**

- Hemerocallis 'Yellow' - Yellow Day Lily
- Phormium Tenax - New Zealand Flax

#### **Groundcovers:**

- Gazania 'Mitsuwa Yellow' - No Common Name
- Tall Fescue - Turf

## **Parcel Entry Areas**

The design of parcel entry areas should consist of flowering accent trees and shrubs with distinct forms that complement and stand apart from the informal streetscape. These areas will be somewhat formal, but are to provide continuity between the streetscapes and the properties.

## **Landscaping Standards for Grove Avenue Development**

Landscaping is required on-site for individual parcels and for parking areas. These landscaped areas will include building and parking setbacks, parking areas, buffers, and planting areas directly adjacent to buildings. The quantity and actual placement of trees, shrubs, groundcover and turf shall be adequate to screen and soften buildings and their associated loading and parking areas from adjacent public streets. Such landscaping shall be designed with consideration given to parcel size, the intended building use and water conservation. The following standards shall apply to all development within the Specific Plan area:

- There shall be a minimum of 15% of an individual parcel area landscaped with trees, shrubs, and groundcovers. Off-site public rights-of-way parkway, provided that they are contiguous with landscape setbacks, cannot be counted as a part of the overall required landscape area, but may be included in the overall design.
- For all landscaped areas, plus any off-site required improvements, there shall be a permanent automatic irrigation system installed. Temporary irrigation systems may be used for temporary landscaping on any undeveloped areas of the project, subject to review and approval of the Public Facilities Department.
- The minimum sizes for trees shall be 15 gallons and the minimum sizes for shrubs shall be 5 gallons. Smaller container-size plant material shall be approved by the City of Ontario. In order to conserve water, xeriscape landscaping is encouraged, using hardscape features and drought tolerant plants.
- All landscaping shall be properly maintained so that it thrives and matures over the years. Any landscaping materials that die due to natural or unnatural causes shall be replaced within thirty days of a written notice, from the City of Ontario to the property owner and/or leases.
- Trees shall be double-staked and the staking shall be maintained until the trees are strong enough to withstand the normal area winds.
- Annual color plantings shall be replaced immediately upon decline.

- All required landscaping shall be maintained free of debris, weed-free and in a healthy condition. This includes pruning, mowing of lawns, weeding, removal of litter, fertilizing, replacement of plant materials where necessary, and the regular watering of all plant material.
- At least one tree per 10 parking stalls shall be provided. All trees installed to meet this requirement shall be 24" box minimum size in the landscaped planter beds at the end of the parking bays and 15 gallon minimum size between the end areas. A landscaped planter bed area shall be provided every 10 parking spaces.
- Maintenance of the streetscape and buffer zones shall be the responsibility of the developer.
- All landscape planter beds located within the interior parking areas shall not be less than five feet in width and bordered by a concrete curb no more than six inches in height adjacent to the parking surface.
- All loading areas shall be within the rear and side yards and shall be continuously screened from off-site views with walls, fencing, or landscaping or a combination thereof. Shrub materials used exclusively for screening shall be a minimum of five gallons in size. Trees used for screening purposes shall be minimum of 24" box.
- Automatic irrigation systems shall be designed to provide adequate coverage with minimum runoff; drip and bubbler systems shall be used when possible. Water efficient landscape areas must include both proper irrigation design as well as proper plant material selection in order to provide healthy, vigorous plant growth.
- All landscaping shall comply with the City of Ontario Landscape Standards.

### **Onsite Landscaping**

The installation and maintenance of the on-site landscaping for individual building sites will be the responsibility of each individual parcel owner subject to approval of a landscape plan by the City of Ontario as part of the development site plan review process.

While the overall uniformity and compatibility of each project with the adjacent streetscapes within the Grove Avenue Specific Plan area is encouraged, allowance for individual expression by the property owners has been considered. Careful attention should be paid to similar materials in contiguous landscape areas such as adjoining parcels or streetscape zones, in order to create an effective transition. The following plant palette is designed to assist landscape architects, contractors, and property owners in choosing plant materials for the individual parcels.

A variety of plant types from the following list are drought tolerant plant materials that require small amounts of water to survive in the Ontario area used within the interior of each individual parcel or project, excluding streetscape areas, buffer areas and parcel entry areas. It must be noted that there are many additional plants, not shown on this list that may qualify as drought tolerant and/or security plant material, with approval by the City of Ontario. It will be the responsibility of the Developer/Landscaped Architect to provide the City with plans depicting "harmonious transition" techniques when working drawings are submitted.

The term "drought tolerant" does not refer to survival without water. A vast majority of drought tolerant plants must have supplemental irrigation beyond annual rainfall to survive and thus water efficient irrigation systems must be provided. Turf areas (high water use) shall always be separate from shrub and groundcover areas (lower water use). All plants requiring similar water quantities should be grouped together in "hydro zones" to promote efficiency of water usage.

### **Suggested Plant List For Grove Avenue Specific Plan Area Property Owners**

#### **Trees:**

Chamaerops Lumilis - Mediterranean Fan Palm  
Cinnamomum camphora - Camphor Tree  
Eucalyptus ssp. - No Common Name  
Geigera parviflora - Australian Willow  
Jacaranda acutifolia - Jacaranda  
Lagerstroemia indica - Crape Myrtle  
Ligustrum lucidum - Glossy Privet  
Liquidambar styraciflua - American Sweet Gum  
Pinus ssp. - Pine  
Pistacia chinensis - Chinese Pistacio (male only)  
Platanus acerifolia - London Plane Tree  
Prunus blireiana - Flowering Plum  
Pyrus Kawakamii - Evergreen Pear  
Ulmus parvifolia (Brea or Drake) - Evergreen Elm

#### **Shrubs:**

Abelia grandiflora - Glossy Abelia  
Agapanthus africanus - Lily of the Nile  
Buxus microphylla japonica - Japanese Boxwood  
Callistemon - Bottlebrush  
Cotoneaster - No Common Name  
Elaeagnus pungens - California Silver Berry

Escallonia - No Common Name  
Hakea suaveolens - Sweet Hakea  
Hypericum calycinum - Aaron's Beard  
Juniperus - Juniper  
Nerium oldeander 'Petite Pink' - Dwarf Oleander  
Phormium tenax - New Zealand Flax  
Photinia fraseri - No Common Name  
Pittosporum tobira - Tobira  
Potentilla fruticosa - Cinquefoil  
Raphiolepis indica - India Hawthorn

**Groundcovers:**

Acacia redolens "prostrata" - No Common Name  
Baccharis pilularis "Twin Peaks" - Coyote Brush  
Cotoneaster microphylla - Rockspray Cotoneaster  
Gazania 'Matsua' - No Common Name  
Hemerocallis - Day Lily  
Myoporum parvifolium "Putan Creek" - No Common Name  
Rosmarinus officinalis - Rosemary  
Tall Fescue (30% maximum of total landscape area)  
Trachelospermum jasminoides - Star Jasmine

**Vines:**

Anemopagma chamberlaynii - Yellow Trumpet Vine  
Bougainvillea - No Common Name  
Ficus pumila - Creeping Fig  
Gelsemium sermpervirens-Carolina Jessamine  
Macfadyena unguis-cati - Cat's Claw  
Tecomaria capensis - Cape Honeysuckle  
Wisteria Spp. - No Common Name

**SIGNAGE GUIDELINES**

**General Criteria**

The provisions of Article 26 of the City of Ontario Municipal Code shall apply, except as amended by these guidelines. The purpose of these sign standards are to provide the means for adequate identification of the Specific Plan area, as well as the buildings and businesses within the Grove Avenue Corridor by regulating and controlling the design, location, and maintenance

of all signage within the Grove Avenue Specific Plan area. The sign standards establish specific standards for all exterior signing to ensure consistency and harmony with the architectural quality of the Grove Avenue Specific Plan area and the City of Ontario Sign Ordinance.

The following general regulations shall apply to the signage of all development within the Grove Avenue Specific Plan area:

1. No sign shall be permitted that does not pertain directly to an approved business conducted on the premises.
2. All signs, except as provided herein, shall be permanent in nature and shall be consistent with and reflect the architectural design of the building with which they are associated, and shall incorporate unifying features such as materials and/or colors.
3. All light sources, either internal or external, provided to illuminate signage shall be placed or directed away from public streets, sidewalk or adjacent premises to not cause glare or reflection that may constitute a traffic hazard or nuisance.
4. All signage shall be designed free of bracing, angle-iron, guy wires, cables or similar nuisance.
5. The exposed backs of all signs visible to the public shall be suitably covered, finished and properly maintained.
6. The following signs shall be prohibited in the Grove Avenue Specific Plan area:
  - Window signs
  - Inflatable signs, balloons, animals or symbols
  - Roof-top signs
  - Portable signs
  - Rotating, revolving or flashing signs
  - Signs advertising or displaying any unlawful act, business or purpose
  - Signs painted directly on the exterior of buildings
  - Signs which create a safety hazard by obstructing views of traffic (pedestrian or vehicular) and traffic control devices (public rights-of-way) emergency access or access to or from building, fire escapes, doors and windows

- Any strings or pennants, banners or streamers, clusters of flags, strings or twirlers or propellers, flares, balloons and similar attention-getting devices used for advertising, including noise-emitting devices, with the exception of the following: (a) national, state, local government, institutional or corporate flags, properly displayed, and (b) holiday decoration, in season, used for an aggregate period of sixty days in any one calendar year.
7. **Compliance Required:** No person shall place, erect, re-erect, enlarge, alter, move, improve, remove, repair, convert or equip any sign or sign structure or cause or permit the same to be done contrary to or in violation of the provisions of these sign standards. Unapproved sign(s) must be brought into conformance at the expense of the persons responsible for the installation of said sign(s).
  8. **Maintenance:** All signs shall be maintained in a neat and good physical condition. Prompt correction of any deficiencies shall be made by the entity responsible for the maintenance of said sign(s).
  9. **Lighting:** The quality of sign lighting shall relate to the character that is intended for the area. No sign illumination should cause a glare or illuminate adjacent sites. Signs may be illuminated by continuous and uniform interval lighting or by external lighting sources. All sign illumination shall follow these guidelines:
    - All external light sources shall be adequately shielded to guide or direct the light toward the sign face and prevent glare or illumination of adjacent properties and structures.
    - Light fixtures shall be an integral part of the design and color scheme of the sign and shall have a compatible design, color and light hardware.
    - Creative uses of internal lighting are encouraged when the color and intensity of the light is well blended into the sign design.
    - Artistic applications of lighting in signs are acceptable when used for uses conducted after dark, such as restaurants.
    - All sign illumination levels shall comply with any and all applicable requirements and policies of the City of Ontario.
  10. **Planned Sign Program:** The establishment of a planned sign program in accordance with this section shall be an alternative to the standard sign regulations contained in these signage guidelines.

A. Establishment

Signs within such a planned sign program may be permitted even if they do not conform to all of these requirements. Such exceptions, however, may only be granted by the Development Advisory Board if it is found that variation from the guidelines is necessary in order to achieve aesthetic compatibility of the signs within a project area, and to allow design creativity. The use of planned sign programs may allow some flexibility in number and size and in the placement of signs. However, the total aggregate area of the signs permitted by the sign program shall not be greater than the total aggregate area of all signs otherwise permitted by these guidelines.

B. Eligibility

Any buildings, business park, shopping center, or other contiguous group of businesses is eligible for consideration of a planned sign program.

C. Minimum Project Size

No minimum frontage or site size requirements.

D. Design Elements

Signs within the sign program shall have one or more common design elements, such as color, materials, illumination, sign type, sign shape, letter size, and letter type.

E. Building Context

The sign program shall specify signs in harmony with the materials, colors, architecture, and other design features of the buildings they identify.

F. Review

Planned Sign programs shall be reviewed by the Development Advisory Board. Signs within a previously-approved planned sign program shall be reviewed by staff.

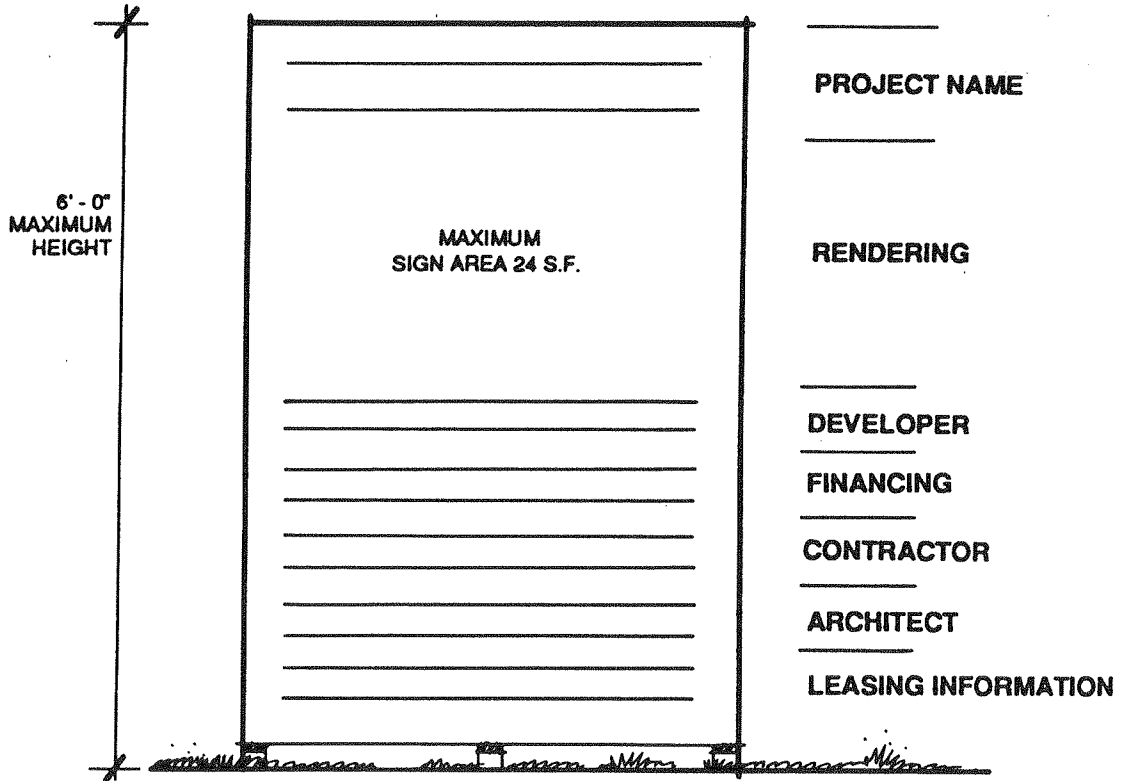


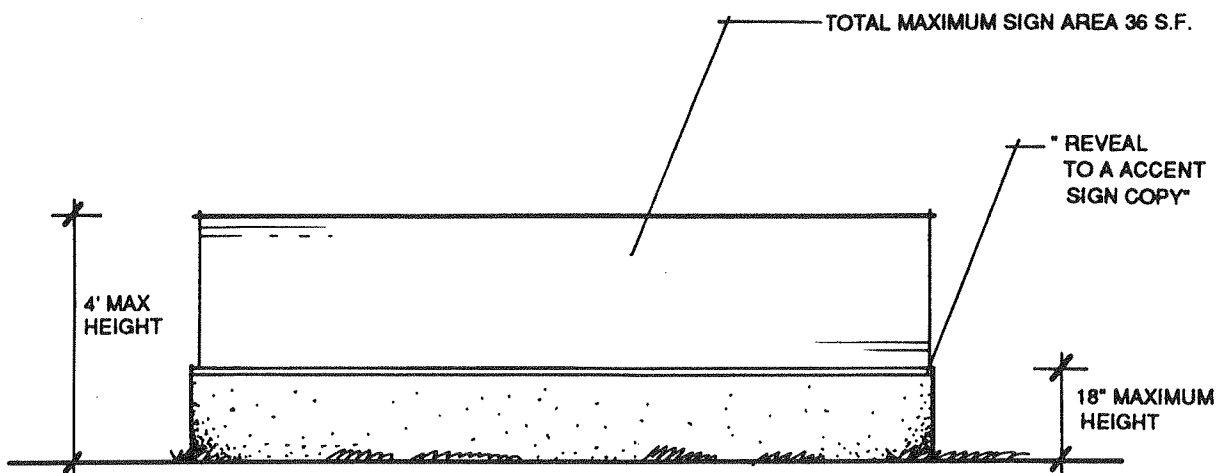
## Temporary Signs (Exhibit 23)

11. Temporary Ground Signs: All temporary ground signs shall be set back a minimum of five feet from the property line.
  - All temporary ground signs shall be non-illuminated and fabricated of sturdy, quality materials capable of lasting the length of their intended use.
  - All temporary ground signs shall remain in place for no more than twelve months or when the initial leasing program is ninety-five percent complete, whichever comes first. This period may be extended for no more than twelve months at a time, subject to approval of the City Planner.
12. Temporary Construction Signs: The purpose of this sign is to identify future building construction. This sign may contain a rendering of the future facility. A construction sign shall only identify the project, the leasing agent, developer, financing institution, general contractor and architect.
  - One construction sign per building shall be permitted.
  - The maximum area of a construction sign shall not exceed twenty-four square feet, and six feet in height as measured down the highest adjacent grade.

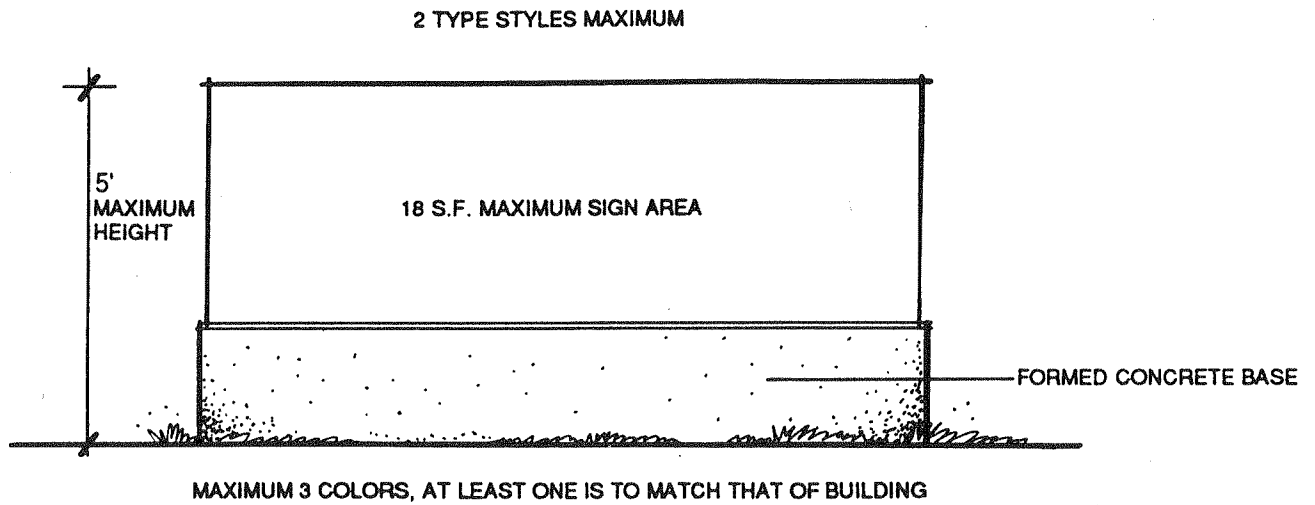
## Permanent Signs

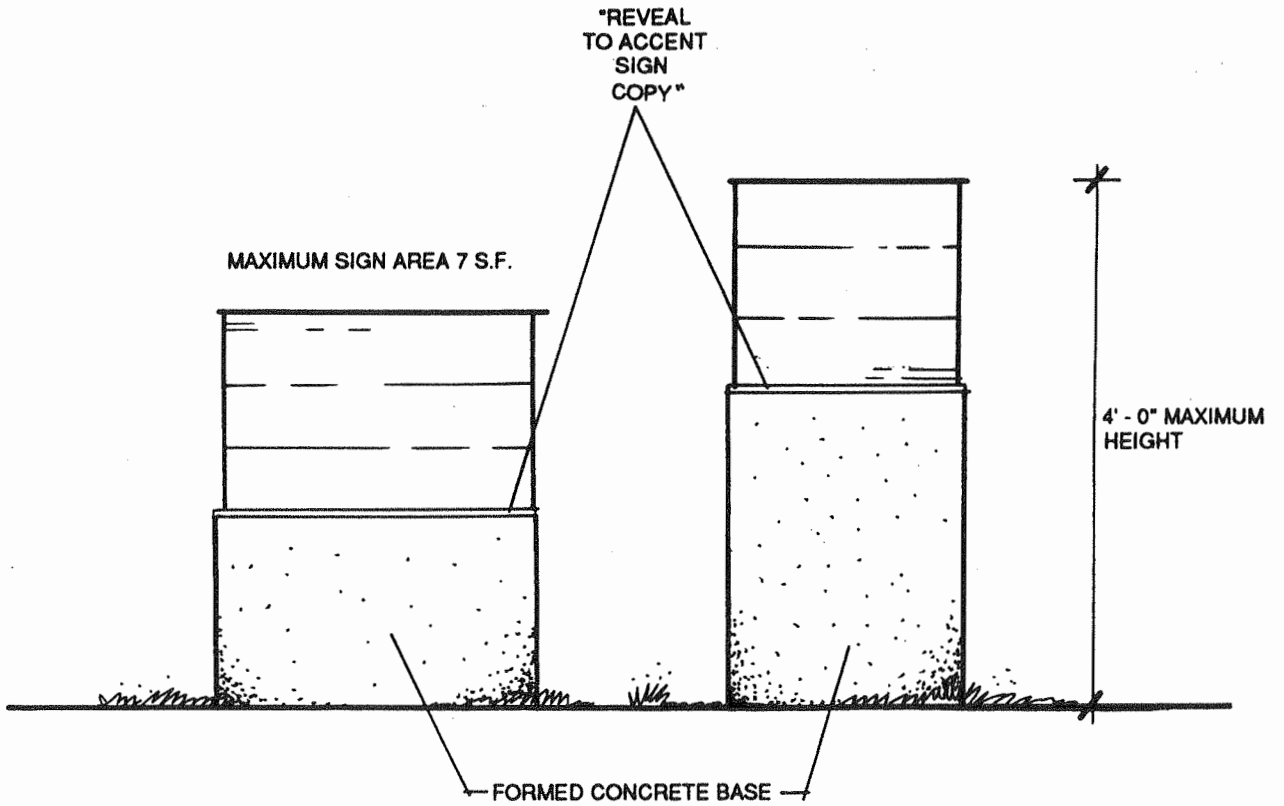
13. Entry Monuments: For the purpose of identification of the Grove Avenue Specific Plan area, one monument sign shall be located at key entry points. Two permanent entry monuments shall be erected at each of the major entry points, including the north side of the interchange of Grove Avenue with the Pomona Freeway and the south side of Grove Avenue at Mission Boulevard. One entry monument shall be erected at secondary entry points at project edges with major cross streets including the southwest and northeast ends of Philadelphia, Francis, and Belmont Streets. (Exhibit 24).
  - All monument signs shall be of sturdy, high quality materials including a base which consists of formed concrete with a smooth finish. Base height shall not exceed eighteen inches and shall be in proportion with the total dimensions of the monument.
  - The sign panel shall display name and logo of Grove Avenue. A horizontal reveal between base and panel shall be used to accent the copy and logo.

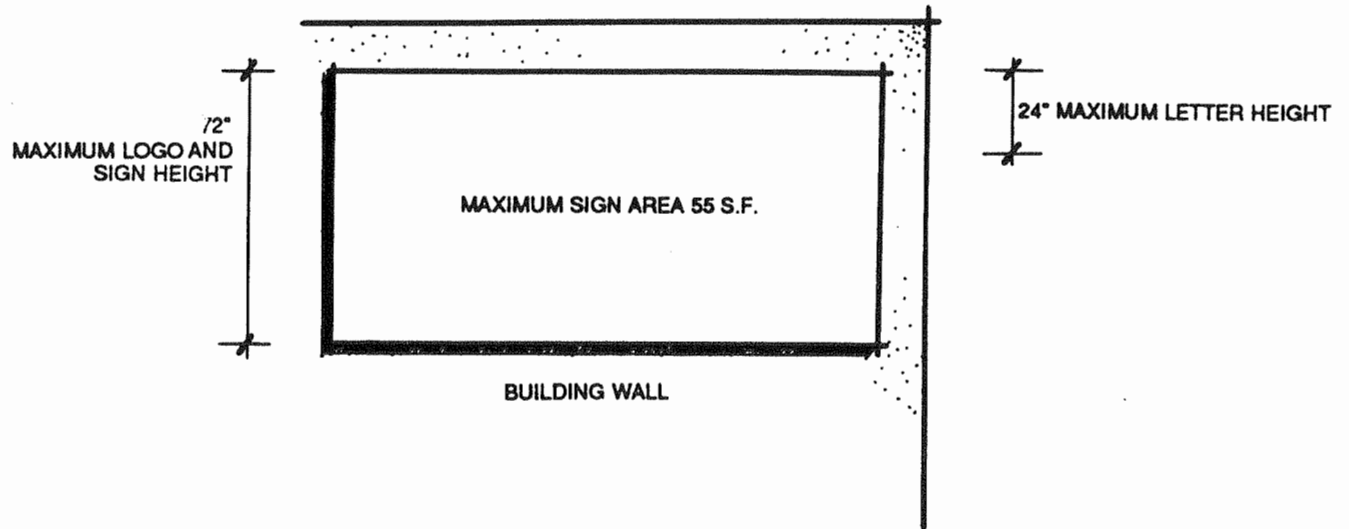




- Total sign height shall not exceed four feet and maximum area will be thirty-six square feet.
  - All entry monument signs shall be illuminated by concealed ground level flood lighting.
  - Monuments will be set back a minimum of ten feet from the individual project property line, so as not to create a hazard for either pedestrian or vehicular traffic. The sign copy shall be limited to identification of "Grove Avenue".
14. Permanent Freestanding Identification (Monument) Signs: The purpose of this sign, which shall be double faced, is to identify project entries and individual freestanding uses such as restaurants and the automotive service stations. Each parcel may have one such sign (**Exhibit 25**).
- Construction shall be similar to that of the entry monuments with a formed concrete base with smooth finish. The base shall not exceed twelve inches from highest adjacent grade and shall be proportional with the total sign area.
  - Sign panels shall be limited to two letter styles and three colors at least one of which is to match the architecture of the building that it is identifying. A horizontal reveal between base and panel shall be provided to accent the copy.
  - The maximum sign area shall not exceed eighteen square feet per side and five feet in height from the highest adjacent grade.
  - All signs shall be internally illuminated.
15. Permanent Vehicular Directional Signs: One sign or more, as needed, shall be allowed at each driveway into a parking lot (**Exhibit 26**).
- The maximum sign area shall not exceed four square feet per side and four feet from the highest adjacent grade.
  - All directional signs shall be non-illuminated.
16. Permanent Building Mounted Signs: All signs and logos attached to the building shall be individual, surface mounted letters. No exposed raceways or cabinet signs shall be allowed. The area of wall signs with individuals letters or a logo shall be measured by a rectangle around the outside of the lettering and/or the pictorial symbol (**Exhibit 27**).







- All wall mounted signs shall be constructed of sturdy, quality materials. The height of such signs shall not exceed the height of the building roof line or architecturally integrated parapet wall.
  - If illuminated, only internal or halo illumination shall be permitted.
  - The allowable sign area shall not exceed one square foot per lineal foot of the building elevation/tenant space where the sign is located. A maximum of fifty-five square feet per sign shall be permitted. For establishments with an entrance on the side wall, the same standards shall apply. Signage shall only be allowed on two elevations.
  - Letter height shall not exceed twenty-four inches or be smaller than a minimum size of twelve inches, except that if only the first letter of a word is capitalized, that capital letter may be up to thirty-six inches in height. Official corporate logos may also exceed twenty-four inches in height, up to a maximum height of thirty-six inches.
  - All wall mounted signs shall be constructed of sturdy, quality materials. The height of such signs shall not exceed the height of the building roof line or architecturally integrated parapet wall.
17. **Retail Commercial Signs:** A detailed, comprehensive sign plan shall be submitted with all retail commercial site plans. The sign plan shall describe the allowable sizes, colors, materials, and lettering styles for all of the permitted signs within the retail commercial center.
- In multi-tenant commercial developments, major tenants shall be allowed a single wall or fascia sign. The signs shall be located on the building face above entry doors and canopies.
  - Small shop tenants shall be allowed a single wall or fascia sign shop identification sign to be placed under a canopy or over the shop door.
  - Company logos, colors and type styles shall be permitted as part of a commercial sign.
  - All signs shall be composed of individual, internally-illuminated letters. Illuminated background signs shall be permitted.
  - Sign panels shall be limited to two letter styles and three colors at least one of which will match the architecture to which it is associated



- The maximum sign area shall be in proportion with the lease size, building wall or fascia.
- All signs shall be internally illuminated.

## **ARCHITECTURAL DESIGN GUIDELINES**

### **Intent**

It is the intent of these architectural design guidelines to regulate development within the Grove Avenue Specific Plan area to achieve an image of a high quality office and commercial business corridor. As an integral element of the overall design concept, architecture within the Grove Avenue Specific Plan will establish a strong identify and constant pattern or unified image, without restricting innovation or creativity.

The following design guidelines are intended to identify an architectural vocabulary which allows sufficient variety for individual expression yet maintains compatibility for each individual site development. The guidelines are to be followed in order to ensure that an overall design integrity within the Grove Avenue Specific Plan area will be achieved.

### **Design Elements**

#### Building Form and Massing

1. Buildings should be composed of clean and simple geometric forms with coordinated massing to produce a unified expression of composition and scale.
2. Architectural interest should be created through modulation and fragmentation of large expanses of building surfaces and mass, as well as arrangement of doors and windows. Use of different building forms and components are required to add a sense of depth through shade and shadow, as well as to articulate individual building functions.
3. Building mass should be reduced vertically or set back at successive levels through use of arcades, colonnades, covered walks, or verandas. Different roof levels should be tied together by common elements of roof angle, materials, and form. Individual building functions should be expressed by separate forms; building components should be grouped around walks, entry plazas, and courtyards.
4. Any complicated or arbitrarily decorative forms shall be prohibited.

## Facade Treatment

The facade is the architectural element which communicates the purpose, structure, and scale of a building. It creates a harmonious composition of functional requirements through structural articulation, proportion, materials, and color to reflect a coordinated design which reinforces the high quality architectural character desired for Grove Avenue.

5. Facades should incorporate a clear, efficient contemporary style which is distinctive yet compatible with the function of the building, its elements, and surrounding development.
6. Large expanses of uninterrupted walls or blank surfaces shall be avoided or otherwise oriented away from highly visible portions of the site or screened and softened by landscape treatments.
7. Complicated, inconsistent, or arbitrarily decorative details and forms, which are in sharp contrast, are prohibited.
8. Highly contrasting graphic patterns on facades are prohibited.
9. Complicated, inconsistent, or arbitrarily decorative forms, as well as flush doorways and tacked on entry alcoves and other components, are prohibited.

## Fenestration

Facades and their arrangement of doors and windows (fenestration) should define function and structure, as well as be consistent in form, pattern, and color.

10. Glazed surfaces should be balanced with surface walls and delineated by structure or finished mullions.
11. Functional glass and patterns in doors and windows should be used.
12. Recessed windows and doors should be included as part of the design in addition to protective shading and decorative framing of the openings, including moldings, tile, painted borders, mullions. Awnings should be used to amplify the window and door openings by providing contrast and reinforcing their architectural significance. Awnings shall be made of canvas in a solid accent color. Metal awnings are prohibited.
13. Uninterrupted horizontal glazing and glass cube forms, as well as arbitrary, decorative glass patterns, will not be permitted.
14. The use of highly reflective, sloping or louvered glass, as well as unfinished metal frame windows and doors is inappropriate.

### Structural Relationships

The base of the building is generally understood to be the first or combined first and second floors and is the critical element in giving the building a positive relation to the street. It contains building entries, lobbies, and areas of higher activity.

15. Distinctive architectural expression of the building's vertical support and structural base should be made an element of the building design by using techniques such as the expression of a cornice line, proper structural scale, changes in materials or surface texture, geometry, proportions of openings, setbacks, and recesses that create shadows.
16. The structural base of a building should be built of permanent masonry materials such as stone, block, stucco, brick, tile or integrally colored concrete. Neither metal nor porcelain panels are permitted as predominant materials.
17. Sun shielding should be accomplished architecturally either by recessing or use of awnings.
18. Buildings entrances should be clearly defined and visibly inviting. Entries should be integrated within the overall building form to add articulation (such as recessed or protective doorways). Entries should also be highlighted with textured hardscape and coordinated landscaping.

### Roof Forms and Materials

19. Roof lines should be of generally horizontal lines used to articulate and reduce building mass. Alternate roof planes such as gable, hop and shed, or harmonious combinations of each can be used as accents or focal elements.
20. Gambrel, mansard, and other "period" roof styles, as well as other arbitrarily decorative treatments are inappropriate.
21. Visible roof materials should be of either concrete tile or metal which complement building color and/or treatments. Parapets are required for flat roofs to screen roofing materials and mechanical equipment from view.
22. Gutters and downspouts shall be integrated within the architectural design of buildings to be visually unobtrusive and painted to match adjacent roof or wall material. All flashing, sheet metal, vent stacks, pipes, and other minor roof projections shall be painted to match the roof.

23. Skylights are to be restricted to flat roof or screened locations and are not to be visible from surrounding areas. Their form and color shall be consistent with the building design.

#### Texture, Color and Materials

24. Architectural textures, materials, and colors that do not starkly contrast or conflict with surrounding developments are encouraged. Materials should be smooth, clear, and of a high quality to reinforce the desired image for Grove Avenue.
25. Ground level windows shall be transparent. Mirrored glass is prohibited at the base of the building and use of glazing at the ground level (except at entries) is prohibited.
26. One dominant material and color should be selected for the building exterior. Roof, trim, and accent materials should complement the dominant materials and color.
27. Highly reflective glass is not permitted, as is wood trim, siding and shingles.
28. Predominant colors of exterior building materials should be neutral, warm earth tones, or pastels of a very light value. Soft, subtle and warm colors should dominate as the primary building color and complimentary colors should be used for trim and accent features.
29. Service doors should be the same color as the adjacent walls.
30. Contrasting graphic patterns or accents applied to wall surfaces are not permitted.

#### Parking Structures

Parking structures are to be designed to the highest standards of quality, and should comply with the other architectural guidelines in this section.

31. Sloped ramps are to be located within the structure and away from any street elevations.
32. Ground floor areas adjacent to primary pedestrian paths should be occupied with retail or service uses. Where this occurs, there should be special architectural treatments of the commercial frontage.
33. Pedestrian entries to parking garages should be at ground level. Upper level connections between parking structures and the buildings they serve are discouraged.
34. Colorful canopies, awnings, and porte cocheres should be used to mark both vehicular and pedestrian entries to the garage.

35. All lighting within and on the roof of the structure should be shielded so that the light sources are not visible from adjacent property or public rights-of-way.

#### Ancillary Buildings

36. Ancillary buildings should relate in style and materials to the main buildings with which they are associated. They should be built of permanent materials.
37. Ancillary buildings, such as storage buildings, telephone booths and gate houses, should conform to all setback guidelines.

#### Walls, Fences, and Outdoor Storage Areas

In some areas, screening, buffering, and delineation within an individual project will be necessary to separate different uses, define spaces, and conceal unattractive views.

38. Walls, fences, and outdoor storage areas are to be designed to complement the building architecture and to be an integral part of the architectural and landscape design of the site.
39. Walls are to be built of materials, finishes, and colors that are compatible with adjacent buildings and are to be finished with graffiti resistant paint or otherwise be graffiti-proofed or maintained graffiti free.
40. Outdoor storage areas are to be enclosed or effectively screened from public view by the use of walls, fencing, landscaping, berming, or a combination thereof.
41. No walls, fences, or outdoor storage areas are permitted within the streetscape, building, or setback areas with the exception that walls may be permitted between streetscape areas and the front of a building if they are limited to three feet in height.
42. Fences are to be used only in areas which are not visible from surrounding streets and they are to be screened with dense landscaping which will be of equal height at maturity.

## 5. GENERAL DEVELOPMENT STANDARDS

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### SPECIFIC PLAN DEVELOPMENT REGULATIONS

#### Introduction

This section of the Grove Avenue Specific Plan establishes the zoning and the land use development standards for each of the land use categories within the Grove Avenue Specific Plan (Exhibit 8, Land Use Plan). Whenever the regulations contained herein conflict with the City of Ontario Zoning Ordinance, the regulations contained herein shall take precedence. In the absence of specific provisions within these site development standards, the requirements of the City's Zoning Ordinance in effect at the time this Specific Plan is adopted will prevail. Existing development within the Grove Avenue Specific Plan area shall be required to adhere to the general development standards of the Grove Avenue Specific Plan at such time as a change in use or additional permit approval is sought from the City of Ontario.

The zoning categories permitted in the Grove Avenue Specific Plan area are:

BP	.....	Business Park
<del>AA</del>	<del>.....</del>	<del>Airport Approach Overlay</del>
C	.....	Commercial
O/C	.....	Office/Commercial

Permitted and conditionally permitted land uses are discussed individually for each land use District. All land uses permitted, conditionally permitted, or prohibited, shall adhere to the City of Ontario Zoning Ordinance, unless otherwise provided herein.

## LAND USE DISTRICT REGULATIONS

### Business Park (BP) District Regulations

**Purpose:** This land use designation is primarily intended for airport-related warehousing, light industrial, research and development, and office-based firms seeking an attractive and pleasant working environment, and public facilities and utilities. The design and development standards for the Business Park District have been established to promote a professional image.

**Permitted Uses:** The uses permitted in the Business Park (BP) District shall include those uses listed below when developed in compliance with the purpose and intent of this Specific Plan. All business shall be conducted entirely within an enclosed building. **Allowable land uses may be non-permitted or restricted within ONT Safety and Noise Impact Zones refer to the ONT ALUCP for land use restrictions.**

1. Light Industrial Uses, including:

A. Manufacturing. Those manufacturing uses which are compatible with commercial, office, and light industrial uses are permitted and on-site warehousing and distribution is allowed if such activities are ancillary to the manufacturing use. This zone is intended to provide and encourage uses which do not need extensive utilities or heavy truck usage.

- Aircraft and aircraft accessories and parts manufacture
- Cabinet shops and furniture manufacture
- Ceramic products manufacture using only previously pulverized clay and kilns fired by electricity or gas
- Custom manufacturing and assembly
- Light Manufacturing of drafting equipment, optical goods, photographic processing, precision instruments, scientific/medical/dental instruments, toys and novelties, watches/clocks/gauges/thermometers
- Manufacture of business machines, including electronic data processing accounting machines, calculators, typewriters, communications and testing equipment and related equipment
- Manufacture and assembly of electrical supplies, such as coils, condensers, crystal holders, solid state circuitry, lamps, switches and wire and cable assembly
- Manufacture and maintenance of electric and neon signs, commercial advertising structures, light sheet metal products, including heating and ventilating ducts and equipment and similar uses

- B. Assembly. Those uses which involve assembly are permitted; on-site warehousing and distribution of the assembled goods is allowed if such activities are ancillary to the assembly use. Assembly uses include:
- Assembly of electrical equipment, such as radios, stereo, and television receivers, phonographs, other types of sound equipment, and motion picture cameras and projectors
  - Assembly of small electric appliances, such as lighting fixtures, irons, fans, toasters and electric toys
2. Research/development laboratories for aerodynamics, computer/business machines, electronics, laboratories, commercial testing, research, experimental, or other including pilot plants, and product development
3. Administrative and professional offices with ancillary commercial uses. "Ancillary" shall be defined as any commercial use which does not exceed 20% of the gross floor area of a building and which use is designated to be primarily for the employees, visitors, and/or clients of the offices. All ancillary commercial areas need to be specifically designated and additional parking provided. An increase in this maximum allowable building square footage devoted to ancillary commercial uses shall only be permitted if a Conditional Use Permit has been obtained. Administrative and professional offices with the following on-site ancillary commercial and administrative and professional service uses are permitted:
- A. Ancillary commercial
- Deli/sandwich shop
- B. Administrative and Professional Services
- Automobile rental agency
  - Building maintenance services
  - Business support services
  - Communication services
  - Medical and health care services
  - Personal services
  - Printing/photography/blueprint services
  - Professional services



4. Public Facilities and Utilities

- Governmental offices and public facilities
- Postal facilities
- Transit services

Uses Subject to a Conditional Use Permit: It is recognized that certain uses, while having certain characteristics that are similar to those of the "Permitted Uses" listed above, have the potential to impact surrounding properties, and therefore require additional approval and consideration. Such uses are listed below and may be permitted in the Business Park District, subject to the approval of a Conditional Use Permit.

1. Light Industrial uses, including automotive and truck repair - minor (under 5,000 lbs. gross vehicle weight. All repair activities shall be indoors).
2. Commercial Uses, including:
  - Administrative and professional offices with greater than 20% of a buildings gross floor area devoted to ancillary commercial uses as defined herein
  - Day care facilities
  - Dry cleaning shop
  - Durable goods sales, wholesale and retail
  - Eating and drinking establishments
  - Electrical equipment sales and repair shops
  - Entertainment and recreation
  - Financial institutions (without drive-in teller facilities)
  - Floral shop
  - Health clubs and spas
  - Laundries, commercial
  - Office supply store
  - Pharmacy
  - Photography equipment/supply store
  - Retail sales of goods produced or warehoused on-site
  - Shoe shine/repair store

- Stationery store
  - Tailor shop
  - Travel agency
  - Vocational and trade schools
3. Caretaker's residence, where 24-hour surveillance is necessary (maximum 1,000 square feet).

Prohibited Uses: The following uses are prohibited in the Business Park District:

- Adult businesses
- Agricultural uses
- Amusement enterprises, including fairs, merry-go-rounds, ferris wheels, and similar uses operated on a temporary basis
- Automobile dismantling, and junk, rag, metal salvage
- Animal slaughtering
- Bone, coal, and wood distillation
- Cement, lime, gypsum, and plaster of paris manufacture, except that the manufacture of by-products shall be permitted
- Cocktail lounges
- Equestrian services and supply establishments
- Explosive manufacture and storage
- Hazardous materials production and storage
- Heavy manufacturing
- Outdoor storage (excluding screened outdoor storage areas that are associated with a permitted and/or conditionally permitted use)
- Residential uses

Other Uses: All other uses not listed as Permitted or Conditional Uses are prohibited, unless a finding is made by the Planning Commission that the use is similar to, and no more objectionable than, a use that is permitted or conditionally permitted in the Business Park District.

**Table 5  
BUSINESS PARK DISTRICT REQUIRED SETBACKS**

	<b>Parking</b>	<b>Building</b>
Grove Avenue	20 feet minimum, 25 feet average	40 feet <sup>2</sup>
Local Street	20 feet minimum, <del>25 feet average</del>	<del>30</del> 20 feet <sup>2</sup>
Interior Property Line	5 feet <sup>1</sup>	10 feet <sup>1 &amp; 2</sup>
Rear Property Line	5 feet <sup>1</sup>	10 feet <sup>1 &amp; 2</sup>

Setbacks: All setbacks are measured from the property lines.

Building Height: The maximum building height of all buildings shall be 60 feet. Refer to the **ONT ALUCP for maximum allowable building height, building heights may be more restrictive than 60 feet.**

Lot Size: The minimum lot size shall be one acre.

Floor Area Ratio: The maximum floor area ratio shall be 0.40 **FAR. An increase in the FAR may be permitted if the traffic generating characteristics of a specific project, as based on the report from a traffic engineer, do not exceed the number of trips that the EIR90-2 assumed would occur for the site.**

Notes:

1. 0-foot building setback allowed for interior or rear property lines adjacent to a building or within a fully screened loading and storage yard area.
2. Encroachments into required Setback Areas - Cornices, eaves, canopies, decorative wall elements, and similar architectural features may extend into a required street or interior property line setback area a maximum of 50 percent of the required setback, not to exceed 4 FT.

## Airport Approach (AA) Overlay Zone Regulations

**Purpose:** The purpose of the Airport Approach Overlay Zone is to minimize the number of people exposed to aircraft crash hazards by establishing land use restrictions that permit less intensive land uses than those allowed in the Business Park (BP) District.

**Permitted Uses:** The uses permitted in the Airport Approach (AA) Overlay Zone shall include those uses listed below when developed in compliance with the purpose and intent of this Specific Plan, the Airport Environs Element of the General Plan, and Part 77 of the Federal Aviation Regulations, Objects Affecting Navigable Airspace. Those uses that are permitted in this Airport Approach (AA) Overlay Zone shall differ according to whether the site is located within the Federal Aviation Administration (FAA) Clear Zone or Approach Safety Zone of the Ontario International Airport (as shown on **Exhibit 9, Air Safety Zones**). All of the permitted uses are allowable, subject to recordation of a noise and aviation easement in favor of the Ontario International Airport and a determination that the proposed uses do not:

- Use flammables
  - Attract birds
  - Contain overhead power lines
  - Cause electrical interference
  - Emit smoke
  - Use high intensity lighting
  - Contain a high concentration of people (greater than 25 persons/acre at any time)
1. Clear Zone of the Ontario International Airport (as shown on **Exhibit 9, Air Safety Zones**). The following uses are permitted as long as there are no permanent structures and such areas are to be minimally used by people (no more than 10 persons/acre at any one time):
    - A. Automobile rental agency
    - B. Auto storage for rental agencies
    - C. Parks and recreational open space
  2. Approach Safety Zone of the Ontario International Airport (as shown on **Exhibit 9, Air Safety Zones**). The following uses are permitted:
    - A. Warehousing
    - B. Light industrial uses, including:

1. Manufacturing. Those manufacturing uses which are compatible with commercial, office, and light industrial uses (*manufacturing uses that need extensive utilities or rely on use of heavy trucks shall not be permitted*)
  - Aircraft and aircraft accessories and parts manufacture
  - Cabinet shops and furniture manufacture
  - Custom manufacturing and assembly
  - Light manufacturing and assembly of drafting equipment, optical goods, photographic processing precision instruments, scientific/medical/dental instruments, toys and novelties, and watches/clocks/gauges/thermometers
  - Manufacture of business machines, including electronic data processing accounting machines, calculators, typewriters, communications and testing equipment and related equipment
  - Manufacture and assembly of electrical supplies, such as coils, condensers, crystal holders, solid state circuitry, lamps, switches and wire and cable assembly
  - Manufacture and maintenance of electric and neon signs, commercial advertising structures, light sheet metal products, including heating and ventilating ducts and equipment and similar uses
2. Assembly. Those uses which involve assembly are permitted; on-site warehousing and distribution of the assembled goods is allowed if such activities are ancillary to the assembly use. Assembly uses include:
  - Assembly of electrical equipment, such as radios, stereo, and television receivers, phonographs, other types of sound equipment, and motion picture cameras and projectors
  - Assembly of small electric appliances, such as lighting fixtures, irons, fans, toasters and electric toys
- C. Research/development laboratories for aerodynamics, computer/business machines, electronics, commercial testing, research, experimental, or other including pilots plants, and product development

Uses Subject to a Conditional Use Permit: It is recognized that certain uses, while having certain characteristics that are similar to those of the "Permitted Uses" listed above, have the potential to impact surrounding properties, and therefore require additional approval and consideration.

Such uses are listed below and may be permitted in the Airport Approach Overlay Zone, subject to the approval of a Conditional Use Permit:

1. Light Industrial uses, including automotive and truck repair - minor (under 5,000 lbs. gross vehicle weight. All repair activities shall be indoors)
2. Commercial uses, including automobile rental agencies and retail sales of goods produced or warehoused on-site
3. Caretaker's residence, where 24-hour surveillance is necessary (maximum 1,000 square feet)

Prohibited Uses: The following uses are prohibited in the Airport Approach Overlay Zone:

- Adult businesses
- Any use that would use flammables, attract birds, contain overhead powerlines, cause electrical interference, emit smoke, use high intensity lighting, or have greater than 25 persons/acre at any time
- Amusement enterprises, including fairs, merry-go-rounds, ferris wheels, and other similar uses operated on a temporary basis
- Automobile dismantling, and junk, rag, metal salvage
- Animal slaughtering
- Bone, coal, and wood distillation
- Cement, lime, gypsum, and plaster of paris manufacture
- Ceramic products manufacture
- Cocktail lounges
- Equestrian services and supply establishments
- Explosive manufacture and storage
- Hazardous materials production and storage
- Heavy manufacturing
- Hotel/motel
- Outdoor storage (excluding screened outdoor storage areas that are associated with a permitted and/or conditionally permitted use)
- Residential uses
- Restaurants/bars

- Schools
- Service stations

Other Uses: All other uses not listed as Permitted or Conditional Uses are prohibited, unless a finding is made by the Planning Commission that the use is similar to, and no more objectionable than, a use that is permitted or conditionally permitted in the Airport Approach Overlay Zone.

**Table 6  
AIRPORT APPROACH OVERLAY ZONE REQUIRED SETBACKS**

	<b>Parking</b>	<b>Building</b>
Grove Avenue	20 feet minimum, 25 feet average	40 feet
Local Street	20 feet minimum, 25 feet average	30 feet
Interior property line	5 feet	10 feet
Rear property line	5 feet	10 feet

Setbacks: All setbacks are measured from the property lines, unless otherwise noted.

Building Height: The maximum building height of all buildings shall be one story and shall not encroach into the limits of Part 77 of the Federal Aviation Regulations, Objects Affecting Navigable Airspace.

Lot Size: The minimum lot size shall be one acre.

Floor Area Ratio: The maximum floor area ratio shall be 0.25.

Density: The maximum density shall be 25 persons per acre at any time, except for those sites within the FAA Clear Zone of the Ontario International Airport, which shall be limited to no more than 10 persons/acre at any time.

## Office/Commercial (O/C) District Regulations

**Purpose:** This land use designation is intended for office development with a limited amount of support commercial uses/services. Office development will be encouraged to include corporate and general offices. Commercial services such as eating establishments, blueprinting and copying and other services which are required to support a major business center will be encouraged within the Office/Commercial District.

**Permitted Uses:** Uses permitted in the Office/Commercial (O/C) District shall include those uses listed below when developed in compliance with the purpose and intent of this Specific Plan. **Allowable land uses may be non-permitted or restricted within ONT Safety and Noise Impact Zones refer to the ONT ALUCP for land use restrictions.**

Administrative and professional offices with ancillary commercial uses. "Ancillary" shall be defined as any commercial use which does not exceed 20% of the gross floor area of building and which use is designated to be primarily for the employees, visitors, and/or clients of the offices. All ancillary commercial areas need to be specifically designated and additional parking provided. An increase in this maximum allowable building square footage devoted to ancillary commercial uses shall only be permitted if a Conditional Use Permit has been obtained. Administrative and professional offices with the following onsite ancillary commercial and administrative and professional service uses are permitted:

A. Ancillary Commercial

- Convenience sales and services
- Day care facilities
- Deli/sandwich shop
- Medical and health care facilities

B. Administrative and Professional Services

- Automotive rental agencies
- Business supply retail and services
- Business support services
- Financial institutions (without drive-in teller facilities)
- Personal services

1. Public Facilities and Utilities



Uses Subject to a Conditional Use Permit: It is recognized that certain uses, while having certain characteristics that are similar to those of the "Permitted Uses" listed above, have the potential to impact surrounding properties, and therefore, require additional approval and consideration. Such uses are listed below and may be permitted in the O/C District, subject to the approval of a Conditional Use Permit:

1. Commercial Uses, including:

- Administrative and professional offices with greater than 20% of a buildings gross floor area devoted to the ancillary commercial uses as defined herein
- Conference/convention facilities
- Eating and drinking establishments
- Entertainment and recreation uses (including, theaters, skating rinks, game arcades, bowling alleys and all other similar uses.)
- Fast food sales
- Food and beverage sales
- Health clubs and spas
- Vocational and trade schools

Prohibited Uses: The following uses are prohibited in the Office/Commercial District:

- Adult businesses
- Agricultural uses
- Amusement enterprises, such as fairs, merry-go-rounds, ferris wheels, and similar uses operated on a temporary basis
- Automobile dismantling, and junk, rag, metal salvage
- Cocktail lounges
- Contractors storage yard
- Outdoor storage (excluding screened outdoor storage areas that are associated with a permitted and/or conditionally permitted uses)
- Recreational vehicle parks
- Residential uses
- Second hand stores and thrift shops
- Tattoo parlors

- Wholesale business establishments
- Sandblasting establishments

Other Uses: All other uses not listed as Permitted or Conditional Use are prohibited unless a finding is made by the Planning Commission that the use is similar to, and not more objectionable than, a use that is permitted or conditionally permitted in the Office/Commercial District.

**Table 7  
OFFICE/COMMERCIAL DISTRICT REQUIRED SETBACKS**

	<u>Parking</u>	<u>Building</u>
Grove Avenue	20 feet minimum, 25 feet average	40 feet
Local Street	20 feet minimum, 25 feet average	30 feet
Interior property line	5 feet	10 feet
Rear property line	5 feet	10 feet

Setbacks: All setbacks are measured from the property lines.

Building Height: The maximum building height of all buildings shall be 60 feet. **Refer to the ONT ALUCP for maximum allowable building height, building heights may be more restrictive than 60 feet.**

Lot Size: The minimum lot size shall be one acre.

Floor Area Ratio: The maximum floor area ratio shall be 0.30.

## Commercial (C) District Regulations

**Purpose:** This land use designation is intended for commercial sales and retail facilities which support business operations within the Grove Avenue Corridor and/or serve the needs of airport-bound visitors. Those commercial uses which can take advantage of high traffic volumes along Grove Avenue will be permitted in the Commercial District.

**Permitted Uses:** The uses permitted in the Commercial (C) District shall include those uses listed below when developed in compliance with the purpose and intent of this Specific Plan. **Allowable land uses may be non-permitted or restricted within ONT Safety and Noise Impact Zones refer to the ONT ALUCP for land use restrictions.**

1. Commercial Uses, including:

- Apparel shops, shoe stores and shoe repair
- Art and artists supply stores
- Bakery
- Barber shops and beauty shops
- Blueprint and photocopy services; printing, publishing, lithography and engraving
- Camera and photography shops and studios, photographic developing and printing
- Catering services
- Cleaning establishments and laundries
- Department stores and variety stores
- Drugstores and pharmacies
- Electrical equipment sales and repair shops, electronic parts and equipment sales
- Employment agencies
- Financial institutions
- Florists
- Food stores, supermarkets, delicatessens, and specialty food stores, candy stores and confectioneries
- Furniture stores
- Hardware stores
- Health studios and reducing salons
- Household appliance sales and repair shops
- Interior decorating shops

- Laboratories
- Locksmiths
- Mail order and catalog sales offices
- Medical, dental and optical offices, clinics and laboratories, optician and optometric stores
- Messenger services
- Musical instrument repair shops, music stores
- Office and business machine stores, typewriter sales and service establishments
- Picture framing shops
- Realtors and real estate offices
- Deli/Sandwich shops
- Scientific instrument shops
- Secretarial services
- Shoe stores
- Specialty stores, including antique shops, art and artists supply stores, bicycle shops, bookstores, china and glassware shops, gift shops and greeting card shops, hobby shops, jewelry stores, pet and bird stores, stamp and coin shops, stationery stores, and toy stores
- Sporting goods stores
- Travel agencies and bureaus
- Watch and clock repair shops
- Restaurants (with no Drive-Thru)

Uses Subject to a Conditional Use Permit: It is recognized that certain uses, while having certain characteristics that are similar to those of the "Permitted Uses" above, have the potential to impact surrounding properties, and therefore require additional approval and consideration. Such uses are listed below and may be permitted in the Commercial District, subject to the approval of a Conditional Use Permit.

- Animal hospitals
- Arts and crafts schools and colleges
- Auction rooms
- Automotive rental agencies
- Business colleges and professional schools

- Financial institutions (without drive-in teller facilities)
- Glass replacement and repair shops
- Music and dance studios
- Radio and television broadcasting studios
- Restaurants with Drive-Thru
- Service stations with mini-markets
- Tailor schools
- Trade schools
- Veterinarians' offices and small animal hospitals
- Car Washes—Full Service and Self Service (excludes facilities ancillary to fueling stations)<sup>1</sup>

Prohibited Uses: The following uses are prohibited in the Commercial District.

- Adult businesses
- Agricultural uses
- Amusement enterprises, such as fairs, merry-go-rounds, ferris wheels, and similar uses operated on a temporary basis
- Automobile dismantling, and junk, rag, metal salvage
- Automobile service centers
- Cocktail lounges
- Contractors storage yard
- Outdoor storage (excluding outdoor storage areas that are associated with permitted and/or conditionally permitted uses)
- Recreational vehicle parks
- Residential uses
- Second hand stores and thrift shops
- Tattoo parlors
- Wholesale business establishments
- Sandblasting establishments

Other Uses: All other uses not listed as Permitted or Conditional Use are prohibited unless a finding is made by the Planning Commission that the use is similar to, and not more objectionable than that is permitted or conditionally permitted in the Commercial District.

<sup>1</sup> Car Washes—Full Service and Self Service are not permitted on the Commercial zoned corners at Grove Avenue and Philadelphia and Grove Avenue and Francis Street.

**Table 8  
COMMERCIAL DISTRICT REQUIRED SETBACKS**

	<b>Parking</b>	<b>Building</b>
Grove Avenue	20 feet minimum, 25 feet average	40 feet
Local Street	20 feet minimum, 25 feet average	30 feet
Interior property line	5 feet	10 feet
Rear property line	5 feet	10 feet

Setbacks: All setbacks are measured from the property lines.

Building Height: The maximum building height of all buildings shall be 35 feet. Refer to the **ONT ALUCP for maximum allowable building height, building heights may be more restrictive than 35 feet.**

Lot Size: The minimum lot size shall be 1/2 acre.

Floor Area Ratio: The maximum floor area ratio shall be 0.30.

## **GENERAL DEVELOPMENT STANDARDS**

### **Parking**

Development of individual parcels shall meet the purposes and general provisions of the City of Ontario Parking Standards.

### **Loading Area Requirements**

Loading areas shall be designed to provide for backing and maneuvering on-site and not from public streets. Loading areas shall not be visible from streets or perimeter passenger circulation drives and shall be screened from adjacent parcels. Loading doors shall not be permitted to directly face Grove Avenue.

### **Outdoor Storage Requirements**

No outdoor storage shall be permitted, unless it is ancillary to a permitted or conditionally permitted use and it is screened from public view (Refer to the Architectural Design Guidelines). Trash container areas shall be screened and unobtrusive. These trash container areas shall be constructed to the City of Ontario standards. The quantity and location shall be determined by the Solid Waste Superintendent.

### **Roofing and Rooftop Equipment Standards**

Rooftop equipment shall not be visible from adjacent streets. Mechanical equipment, including ducts and pipes, shall be contained within rooftop penthouses, and/or behind the parapet wall, unless another method, such as opaque screening, is approved by the City of Ontario Planning Department as being capable of completely concealing all rooftop equipment.

## **6. SPECIFIC PLAN ADMINISTRATION**

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### **GENERAL REGULATIONS**

#### **1. Authority**

The Grove Avenue Specific Plan is established through the authority granted to the City of Ontario by the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457 (Specific Plans).

#### **2. Relationship**

The Specific Plan provides policies, standards, and regulations guiding the development of particular parcels and the overall corridor. Any area not addressed in the Specific Plan is governed by the Ontario Municipal Code. No provision of this Specific Plan is intended to repeal, abrogate, annul, impair, or interfere with any existing ordinance, resolution or policy, except as specifically repealed herein.

#### **3. Zoning Ordinance**

Upon adoption of the Grove Avenue Specific Plan, all land use designations, development regulations, and standards set forth, to the extent described herein, replace those contained in the applicable sections of Ontario Municipal Code.

#### **4. Conflicts With Other Regulations**

When the provisions of this Specific Plan impose more restrictive regulations than required by any other ordinance, resolution, or policy, the provisions of this Specific Plan shall govern.

This Specific Plan is regulatory in nature, and serves as zoning law for the properties and planning areas involved. Development plans, planning area plans, tract and parcel maps are to be in conformance with this Specific Plan and the City of Ontario General Plan.

#### **5. Existing Agreements**

This Specific Plan is not intended to interfere with or abrogate any easements, covenants, or other existing agreements which are more restrictive than the provisions contained herein.



6. **Interpretations**

The City Planner shall be responsible for interpreting the provisions of this Specific Plan. All such interpretations shall be in writing and shall be permanently maintained. Any person aggrieved by such interpretation may appeal the decision to the Planning Commission.

7. **Ambiguities**

Unless otherwise provided, any ambiguity concerning the content or application of the Grove Avenue Specific Plan shall be resolved by the City Planner.

8. **Enforcement**

The City Planner or designee shall enforce the provisions of this Specific Plan.

9. **Violation**

It is unlawful for any person to erect, construct, enlarge, alter, repair, move, use, occupy, or maintain any building, structure, equipment, or portion thereof within the Specific Plan area or cause the same to be done contrary to, or in violation of, any provision of this Specific Plan.

No person shall violate any provision or fail to comply with any requirement of this Specific Plan. Any person violating any provision or failing to comply with any requirement of this Specific Plan is guilty of a misdemeanor.

10. **Conformance with the Grove Avenue Specific Plan**

All existing development within the Grove Avenue Specific Plan shall be required to adhere to the general development standards and design guidelines contained herein at such time as a change in use or additional permit approval (e.g., signage, land use, landscaping) is sought from the City of Ontario.

11. **Nonconforming Uses**

A nonconforming use is a use of a structure or a site which was lawfully established and which was maintained prior to the adoption of this Specific Plan. The provisions of Article 32, Nonconforming Uses and Structures, of the Ontario Municipal Code shall apply to all nonconforming uses and/or developments within the Specific Plan area, with the following exception:

The reconstruction, modification, addition, or placement of any building or structure, the use or development of which is nonconforming, shall be permitted if a determination has been made by the Planning Commission that reasonable conditions have been attached to the proposal which conditions would bring the proposal into substantial compliance with the Grove Avenue Specific Plan.

12. **Nuisance**

Any use of property, building or structure hereafter erected, built, maintained or structurally altered contrary to the provisions of this Specific Plan, shall be considered a public nuisance.

13. **Severability**

In the event that any section, subsection, condition or term of this Specific Plan is declared illegal or unenforceable by any court of competent jurisdiction, the other sections, subsections, conditions, and terms shall remain in force and effect to the full extent permitted by law.

14. **Property Maintenance**

Property maintenance shall be required for all real property, as follows:

1. Landscape and Parking Areas: Landscape and parking areas shall be maintained in the following manner:
  - Parking areas shall be routinely swept and kept free from accumulations of trash, debris, bottles, rubbish, and other discards.
  - Parking area paint striping, including handicapped stalls, parking signs, painted fire curbs and signs, handicapped signs, shall be continually maintained.
  - A program of continual landscape maintenance shall be employed, insuring periodic watering, fertilizing and replacement of dead plant materials. All landscaping shall be maintained in accordance with approved plans.
  - All loading and unloading areas, truck parking stalls, trash enclosures, trash compaction and other storage areas shall be kept free from accumulation of trash, debris, bottles, rubbish and other discards.

- Except for undeveloped properties, no portion of any site shall be unfinished. All areas except for buildings, parking, and walkways shall be landscaped and permanently maintained.
- No vegetation, wall, fence, sign or structure shall be unsightly, by reason of graffiti, disrepair, neglect, or damage.
- Undeveloped property shall be kept free from weeds, debris and rubbish. Vacant property shall be posted to prevent trespassing, sale of vehicles, and any other code violations.
- Trash cans and trash dumpsters shall be kept in appropriate enclosures and trash storage areas.

2. Building Exteriors and Roofs

- No portion of any building or structure shall remain unfinished.
- All buildings and structures shall be maintained in accordance with the elevations, colors, and materials on file with Planning Department.
- All exteriors of buildings shall be continually maintained, painted, or repaired.
- All mechanical equipment, including vents, pipes, air conditioners and similar structures shall be screened from view. Equipment screening shall be periodically maintained, painted, or repaired.
- All roofs shall be continually maintained or replaced to ensure weatherproofing.

3. Signs

- All sign and sign structures shall be continually maintained in good physical condition per the signage guidelines of this Specific Plan. Maintenance may include painting, repair, and replacement of sign faces.
- All damaged signs and sign structures shall be immediately repaired.

## **DEVELOPMENT REVIEW AND APPROVAL PROCESS**

All projects within the Grove Avenue Specific Plan area (including, but not limited to, planning area plans, subdivisions, site plans, and conditional use permits) shall be subject to the development plan and design concept review process of the City, as modified herein. All projects shall be in compliance with the applicable standards, criteria, and regulations of this Specific Plan.

Each project within the Grove Avenue Specific Plan area shall be subject to environmental review per the California Environmental Quality Act (CEQA). A notice of intent shall be filed with each application and an initial study shall be prepared, if required by CEQA. At a minimum, all applicable mitigation measures from EIR 90-2 prepared for the Grove Avenue Specific Plan are to be made conditions of project approval.

### **1. Planning Area Plan Requirements**

Parcels of ten acres or more shall be required to prepare a Planning Area Plan (PAP), subject to Planning Commission review and approval prior to, or concurrent with, subdivision approval. At a minimum, the PAP shall address land use, circulation, public facilities, and infrastructure, in conformance with the Grove Avenue Specific Plan, as well as other issues identified through the site plan or subdivision review process. Any site plan or subdivision for property within an approved PAP shall be consistent with it.

The PAP shall cover the entire site, and shall be in conformance with the design guidelines and general development standards of the Specific Plan, except that variation from the rear and side yard setbacks may be permitted by the Planning Commission through its approval of a PAP. Exceptions may also be permitted in the establishment of administrative and professional offices with ancillary commercial uses, such that the ancillary commercial use or uses may be established on-site in a building other than that of the offices. Such variations or exceptions may be granted if it can be demonstrated that the entire site is planned as an integrated unit and that flexibility in the prescribed Specific Plan requirements is needed in order to guarantee development of a well-designed, visually pleasing project consistent with the goal of upgrading the appearance of the Grove Avenue Corridor.

### **2. Subdivision Requirements**

Any application to subdivide property within the Specific Plan area shall not be accepted as complete without the filing of a site plan and any other required applications, such as a planning area plan (if the parcel is ten acres or larger), and/or a conditional use permit (if the proposed uses would require one).

3. Site Plan and/or Conditional Use Permit Requirements

All applications for site plans and/or conditional use permits are to include the following materials in addition to the standard application package:

- a. Fiscal Impact Analysis, as determined by City guidelines
- b. Transportation Demand Management (TDM) Plan, as required by EIR #90-2; and
- c. Traffic Impact Analysis of Consistency with County-wide Congestion Management Program, as determined by City guidelines.

APPENDIX A

MITIGATION MONITORING PROGRAM

FOR

ENVIRONMENTAL IMPACT REPORT NO. 90-2

(File No. EIR 90-2)

RESOLUTION NO. 93-49

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, ADOPTING A MITIGATION MONITORING PROGRAM FOR ENVIRONMENTAL IMPACT REPORT NO. 90-2 PREPARED FOR THE GROVE AVENUE SPECIFIC PLAN AND RELATED ZONE CHANGE COVERING AN APPROXIMATELY 250 ACRE SITE LOCATED ALONG BOTH SIDES OF GROVE AVENUE BOUNDED BY MISSION BOULEVARD ON THE NORTH AND THE POMONA FREEWAY (STATE ROUTE 60) ON THE SOUTH (FILE NO'S. 4388-SP AND 4515-Z)**

**WHEREAS**, the Grove Avenue Specific Plan and related Zone Change, File No's. 4388-SP and 4515-Z, are subject to requirements of the California Environmental Quality Act, Public Resources Code, Section 21,000 et. seq.; and

**WHEREAS**, Section 21081.6 of the Public Resources Code requires that the City of Ontario adopt a reporting or monitoring program for any changes to a project, which have been a condition of project approval by the City in order to mitigate or avoid significant effects on the environment and ensure compliance during project implementation; and

**WHEREAS**, Environmental Impact Report No. 90-2 identifies significant effects on the environment, which require mitigation and thus, monitoring of their implementation; and

**WHEREAS**, a system of monitoring the implementation of all mitigation measures contained in Environmental Impact Report No. 90-2 is set forth in a mitigation monitoring program, which is attached to this resolution and made a part hereof.

**NOW, THEREFORE, BE IT RESOLVED**, that the mitigation monitoring program for this project is approved.

I hereby certify that the above resolution was duly passed and approved by the City Council of the City of Ontario at a regular meeting held on the 18th day of May, 1993.



*DeLoree G. Artubian*  
City Clerk of the City of Ontario

## **MITIGATION MONITORING PROGRAM**

In order to ensure that the mitigation measures contained in the EIR are properly implemented, a monitoring program has been devised. The timing, process, and responsibility for monitoring the implementation of each of the mitigation measures are described on the following pages. The City of Ontario Development Agency (which consists of the City's Planning, Building, and Engineering Departments and Public Facilities Development) will have primary responsibility for monitoring and reporting the implementation of most of the mitigation measures. The mitigation measures have been identified by impact category and numbered for ease of reference.



Mitigation Measures	Monitoring Responsibility	Mitigation Process	Monitoring Milestone	Verification by City
<p><b>LAND USE</b></p> <p>No mitigation required.</p>	-----	-----	-----	-----
<p><b>VISUAL CHARACTER</b></p> <p>No mitigation required.</p>	-----	-----	-----	-----
<p><b>TRAFFIC AND CIRCULATION</b></p> <p>1. <u>Grove Avenue at Mission Boulevard:</u> Year 2010 lane configuration expected for this intersection include three through-lanes in all directions, dual left-turn lanes for east/west directions, and exclusive right-turn lanes and right-turn overlaps for all directions except northbound. Mitigation would require removal of an existing building on the southeast corner to provide a northbound right-turn lane (LOS F, delay of 62.3 seconds/vehicle), or north/south dual left-turn lane (LOS E, delay of 46.1 seconds/vehicle). Addition of north/south dual left-turn lanes would fully mitigate the project impacts at this intersection. No other feasible mitigation measures were identified.</p>	City of Ontario Engineering Dept.	Include in the City's Capital Improvement Program.	Constructed as funding is available.	

Mitigation Measures	Monitoring Responsibility	Mitigation Process	Monitoring Milestone	Verification by City
<b>TRAFFIC AND CIRCULATION (CONTINUED)</b>				
<p>2. <u>Grove Avenue at Francis Street:</u> Addition of northbound and southbound right-turn lanes would yield a 2010 LOS E, and delay of 53.7 seconds. These improvements mitigate all but two seconds of the increase of average vehicle delay.</p>	<p>City of Ontario Engineering Dept.</p>	<p>Include in the City's Capital Improvement Program.</p>	<p>Constructed as funding is available.</p>	
<p>3. <u>Grove Avenue at Philadelphia Street:</u> Addition of east/west dual left-turn lanes would yield a 2010 LOS F, and delay of 65.8 seconds/vehicle. These improvements would fully mitigate the project impacts at this intersection.</p>	<p>City of Ontario Engineering Dept.</p>	<p>Include in the City's Capital Improvement Program.</p>	<p>Constructed as funding is available.</p>	
<p>4. Transportation demand management (TDM) measures will be required as part of the approval of all future projects in the Grove Avenue Corridor Specific Plan area in order to reduce the number of vehicle trips on the local street network.</p>	<p>City of Ontario Development Agency</p>	<p>Submittal of TDM plans.</p>	<p>Submitted in conjunction with site plans.</p>	
<p>5. <u>State Route 60 - Eastern Ramps at Grove Avenue:</u> Addition of an eastbound left/right-turn lane and a second southbound left-turn lane would yield a 2010 LOS D, and delay of 39.8 seconds/vehicle.</p>	<p>City of Ontario Engineering Dept.</p>	<p>Include in the City's Capital Improvement Program.</p>	<p>Constructed as funding is available.</p>	

Mitigation Measures	Monitoring Responsibility	Mitigation Process	Monitoring Milestone	Verification by City
<b>AIR QUALITY</b>				
<p>6. Encourage the use of alternate transportation modes by promoting public transit usage and providing secure bicycle facilities. Develop and implement a transportation management program (TDM required per the Grove Avenue Corridor Specific Plan).</p>	<p>City of Ontario Development Agency</p>	<p>Submittal of TDM plan by applicant.</p>	<p>Prior to site plan approval.</p>	
<p>7. Provide mass transit accommodations; such as bus turnout lanes, park and ride areas, and bus shelters (mass transit accommodations required per the Grove Avenue Corridor Specific Plan).</p>	<p>City of Ontario Development Agency</p>	<p>Site plan review.</p>	<p>Prior to site plan approval.</p>	
<p>8. Provide sufficient service establishments within the office areas, such as restaurants, copy centers, etc., to minimize the number and length of trips to obtain these common services. (Encouraged and permitted per the Grove Avenue Corridor Specific Plan).</p>	<p>City of Ontario Development Agency</p>	<p>Site plan approval.</p>	<p>Prior to site plan approval.</p>	

Mitigation Measures	Monitoring Responsibility	Mitigation Process	Monitoring Milestone	Verification by City
<b>AIR QUALITY (CONTINUED)</b>				
9. Individual development projects are to be required to join the Airport Communities TMA administered by the Inland Empire Economic Council.	City of Ontario Development Agency	Development review.	Proof of membership prior to issuance of a certificate of occupancy.	
10. Encourage formation of van-pools with company vehicles or subsidy and encourage public transit with free transit passes. Work with the OMNITRANS to expand scheduled bus service and implement long-term public transportation projects. Evaluate the potential for subscription bus service for areas where there are large concentrations of employees.	City of Ontario Development Agency	Include as part of TDM plans.	Submitted in conjunction with site plans.	
11. Provide energy conserving street lighting.	City of Ontario Development Agency	Site plan review.	Prior to site plan approval.	
12. Synchronize traffic signals, where feasible.	City of Ontario Engineering Dept.	Ongoing review.	As warranted and feasible.	
13. Provide landscaping with drought tolerant plant species to shade buildings during summer (per the Grove Avenue Corridor Specific Plan).	City of Ontario Public Facilities Development	Site plan review.	Prior to site plan approval.	

Mitigation Measures	Monitoring Responsibility	Mitigation Process	Monitoring Milestone	Verification by City
<b>AIR QUALITY (CONTINUED)</b>				
<p>14. Vehicle Trips should be further reduced through implementation of travel demand management (as required per the Grove Avenue Corridor Specific Plan) using the following methods:</p> <ul style="list-style-type: none"> <li>• Establish a program of alternative work schedules.</li> <li>• Establish a telecommuting program.</li> <li>• Schedule goods movements for off-peak traffic hours.</li> <li>• Contribute to local shuttle and regional transit systems.</li> <li>• Provide dedicated turn lanes as appropriate.</li> <li>• Limit on-street parking.</li> </ul>	City of Ontario Development Agency	Submittal of TDM plan by applicant.	Submitted in conjunction with site plans.	
<p>15. Individual development projects shall be reviewed for Congestion Management Program consistency (per the Grove Avenue Corridor Specific Plan).</p>	City of Ontario Development Agency	Environmental review.	Prior to project approval.	

Mitigation Measures	Monitoring Responsibility	Mitigation Process	Monitoring Milestone	Verification by City
<p><b>GEOLOGY/SEISMICITY</b></p> <p>No mitigation required.</p>	-----	-----	-----	
<p><b>HYDROLOGY/DRAINAGE</b></p>				
<p>16. As precise plans for the development are submitted to the City, complete hydrology studies and storm drain designs shall be required.</p>	City of Ontario Engineering Dept.	Développement review.	Prior to project approval.	
<p>17. Storm drain facilities constructed in the Grove Avenue Corridor Specific Plan area shall be consistent with and meet the requirements of the City of Ontario's Storm Drain Master Plans.</p>	City of Ontario Engineering Dept.	Development review.	Prior to project approval.	
<p>18. If grading operations occur during the rainy season, October through April, appropriate erosion control measures should be maintained as directed by the City. Vegetation removal should be kept to the minimum necessary for construction. All finished grading should provide drainage away from building foundations and all permanent structures.</p>	City of Ontario Engineering Dept.	Grading plan review.	Prior to issuance of grading permits and during site inspections.	

Mitigation Measures	Monitoring Responsibility	Mitigation Process	Monitoring Milestone	Verification by City
<b>NOISE</b>  No mitigation required.	-----	-----	-----	
<b>UTILITIES AND COMMUNITY FACILITIES</b>  No mitigation required.	-----	-----	-----	
<b>FISCAL ANALYSIS</b>  No mitigation required.	-----	-----	-----	



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Chairman and Members of the Planning Commission

**FROM:** Cathy Wahlstrom, Planning Director *RE FOR CW*

**DATE:** May 26, 2020

**SUBJECT:** MONTHLY PLANNING DEPARTMENT ACTIVITY REPORT; MONTH OF APRIL 2020

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Attached, you will find the Planning Department Monthly Activity Report for the month of April 2020. The report describes all new applications received by the Planning Department and actions taken on applications during the month. Please contact me if you have any questions regarding this information.

The attached reports, along with reports from past months, may also be viewed on the City's web site. New applications may be viewed at <http://www.ontarioca.gov/planning/reports/monthly-activity-reports-applications>, and actions taken on applications may be viewed at <http://www.ontarioca.gov/planning/reports/monthly-activity-reports-actions>.



City of Ontario Planning Department  
**Monthly Activity Report—Actions**  
Month of April 2020

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**DEVELOPMENT ADVISORY BOARD MEETING**  
April 6, 2020

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*Meeting Cancelled*

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**ZONING ADMINISTRATOR MEETING**  
April 6, 2020

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**ENVIRONMENTAL ASSESSMENT AND CONDITIONAL USE PERMIT REVIEW FOR FILE NO.**

**PCUP19-014:** A Conditional Use Permit to establish a second floor 504-square foot Accessory Dwelling Unit (ADU) addition to an existing detached garage on 0.206-acre of land located at 926 North San Antonio Avenue, within the LDR-5 (Low Density Residential - 2.1 to 5.0 DU/Acre) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15303 (Class 3, New Construction or Conversion of Small Structures) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1048-032-02) **submitted by Gabriela Cibrian.**

**Action: Approved, subject to conditions.**

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**CITY COUNCIL/HOUSING AUTHORITY MEETING**  
April 7, 2020

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*No Planning Department items are on the agenda*

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**DEVELOPMENT ADVISORY BOARD MEETING**  
April 20, 2020

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**ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV19-014:**

A Development Plan to construct a gasoline fueling station (Mobil) and 3,500 square foot convenience store on 0.97-acre of land located at the southwest corner of Philadelphia Street and Archibald Avenue, within the Support Commercial land use district of the Archibald Center Specific Plan. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence

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Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1083-011-20). **Submitted by Atabak Youssefzadeh.**

**Action: Approved, subject to conditions.**

**ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV19-062:**

A Development Plan to construct one industrial building totaling 61,130 square feet on 3.12 acres of land located at the southeast corner of East Jurupa Street and South Hudson Avenue, within the Light Industrial land use district of the California Commerce Center Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA18-002, a Specific Plan for which the Final Supplement EIR - California Commerce Center (SCH No. 2006061102) was adopted by the Ontario City Council on June 2, 2007, in conjunction with File No. PSPA06-006. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APNs: 0238-121-30 and 0238-121-31). **Submitted by Newcastle Partners, Inc.**

**Action: Approved, subject to conditions.**

**ENVIRONMENTAL ASSESSMENT, TENTATIVE PARCEL MAP, AND DEVELOPMENT PLAN REVIEW FOR FILE NOS. PMTT19-013 (PM 20157) AND PDEV19-050:**

A Tentative Parcel Map (File No. PMTT19-013) to subdivide 5 acres of land into a single parcel, in conjunction with a Development Plan (File No. PDEV19-050) to construct a 104,993 square-foot industrial building, located at the northwest corner of Sunkist Street and Campus Avenue, at 617 East Sunkist Street, within the IL (Light Industrial) zoning district. The environmental impacts of this project were previously analyzed with The Ontario Plan Environmental Impact Report (SCH# 2008101140), certified by the City Council on January 27, 2010, in conjunction with File No. PGPA06-001. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1049-232-21) **submitted by Herdman Architecture and Design. Planning Commission action is required.**

**Action: Recommended Planning Commission approval, subject to conditions.**

**ENVIRONMENTAL ASSESSMENT, TENTATIVE TRACT MAP AND DEVELOPMENT PLAN REVIEW FOR FILE NOS. PMTT19-016 (TT 20308) AND PDEV19-054:**

A Tentative Tract Map (File No. PMTT19-016) to subdivide 3.02 acres of land into one numbered lot and two lettered lots for condominium purposes in conjunction with a Development Plan (File No. PDEV19-054) to construct 72 multi-family residential units (Townhomes). The project is located at the southwest corner of Via Alba and Via Villagio, within the Residential land use district of the Piemonte Overlay-Ontario Center Specific Plan. The environmental impacts of this project were previously

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reviewed in conjunction with File No. PSPA16-003, for which a Mitigated Negative Declaration was adopted by the City Council on May 16, 2017. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 0210-204-40) **submitted by The New Home Company Southern California LLC. Planning Commission action is required.**

**Action: Recommended Planning Commission approval, subject to conditions.**

**ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV19-040:**

A Development Plan to construct one industrial building totaling 211,358 square feet on 9.34 acres of land located at 1610 and 1612 South Cucamonga Avenue, within the IG (General Industrial) zoning district. The environmental impacts of this project were previously analyzed with The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140) certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1050-201-02) **submitted by Alere Property Group LLC. Planning Commission action is required.**

**Action: Recommended Planning Commission approval, subject to conditions.**

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**ZONING ADMINISTRATOR MEETING**  
**April 20, 2020**

*Meeting Cancelled*

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**CITY COUNCIL/HOUSING AUTHORITY MEETING**  
**April 21, 2020**

*No Planning Department items are on the agenda*

**PLANNING/HISTORIC PRESERVATION COMMISSION MEETING**  
**April 28, 2020**

**ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV19-040:**

A Development Plan to construct one industrial building totaling 211,358 square feet on 9.34 acres of land located at 1610 and 1612 South Cucamonga Avenue, within the IG (General Industrial) zoning district. The environmental impacts of this project were previously analyzed with The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140) certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1050-201-02) **submitted by Alere Property Group LLC.**

**Action: Approved, subject to conditions.**

**TWENTIETH ANNUAL MODEL COLONY AWARDS FILE NO. PHP20-004:** A request for the Historic Preservation Commission to accept the nominations for the Twentieth Annual Model Colony Awards; **submitted by City of Ontario. City Council presentation of Awards.**

**Action: Approved the 2020 “Model Colony” Award nominations.**

**ENVIRONMENTAL ASSESSMENT, TENTATIVE PARCEL MAP, AND DEVELOPMENT PLAN REVIEW FOR FILE NOS. PMTT19-013 (PM 20157) AND PDEV19-050:**

A Tentative Parcel Map (File No. PMTT19-013) to subdivide 5 acres of land into a single parcel, in conjunction with a Development Plan (File No. PDEV19-050) to construct a 104,993 square-foot industrial building, located at the northwest corner of Sunkist Street and Campus Avenue, at 617 East Sunkist Street, within the IL (Light Industrial) zoning district. The environmental impacts of this project were previously analyzed with The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140) certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1049-232-21) **submitted by Herdman Architecture and Design.**

**Action: Approved, subject to conditions.**

**ENVIRONMENTAL ASSESSMENT AND SPECIFIC PLAN AMENDMENT REVIEW FOR FILE NO.**

**PSPA19-009:** An Amendment to the Piemonte Overlay of the Ontario Center Specific Plan, to modify the Minimum Parking Requirements (Section 3.3.5.1) to allow tandem parking up to a maximum of 50% of the required parking. The Ontario Center Specific Plan-Piemonte Overlay encompasses 84.43 acres of land, generally located north of Concours Street, south of Fourth

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Street, west of Via Alba, and east of Haven Avenue. The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA16-003, for which a Mitigated Negative Declaration was adopted by the City Council on May 16, 2017. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 0210-204-26, 0210-204-37, 0210-204-40, 0210-531-15, 0210-531-16) **submitted by LCD Residential at Ontario, LLC. City Council action is required.**

**Action: Recommended City Council approval.**

**ENVIRONMENTAL ASSESSMENT, TENTATIVE TRACT MAP AND DEVELOPMENT PLAN REVIEW**

**FOR FILE NOS. PMTT19-016 (TT 20308) AND PDEV19-054:** A Tentative Tract Map (File No. PMTT19-016) to subdivide 3.02 acres of land into one numbered lot and two lettered lots for condominium purposes in conjunction with a Development Plan (File No. PDEV19-054) to construct 72 multi-family residential units (Townhomes). The project is located at the southwest corner of Via Alba and Via Villagio, within the Residential land use district of the Ontario Center Specific Plan-Piemonte Overlay. The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA16-003, for which a Mitigated Negative Declaration was adopted by the City Council on May 16, 2017. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0210-204-40) **submitted by The New Home Company Southern California LLC.**

**Action: Approved, subject to conditions.**

**ENVIRONMENTAL ASSESSMENT AND SPECIFIC PLAN AMENDMENT REVIEW FOR FILE NO.**

**PSPA19-004:** An Amendment to the 95.35-acre Toyota Ontario Business Park Specific Plan, revising the current land use district covering Planning Area 1, from Office/Research and Development (“Office/R&D”) to Industrial Mixed Use, allowing for warehouse, distribution, and manufacturing land uses on the site in conjunction with the currently allowed Office/R&D land uses. Additionally, the Amendment will update the Specific Plan’s landscape palette to conform to current California friendly landscape practices. The Specific Plan area is generally located south of Jurupa Street, east of Milliken Avenue, north of Francis Street, and west of the I-15 freeway. The environmental impacts of this project were analyzed in an Addendum to The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140), certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario

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International Airport Land Use Compatibility Plan; (APN: 0238-121-75) **submitted by MIG. This item was continued from the March 26, 2020 Planning Commission meeting. City Council action is required.**

**Action: Recommended City Council approval.**

**ENVIRONMENTAL ASSESSMENT AND REVIEW FOR FILE NO. PSPA19-007:** An Amendment to the Parkside Specific Plan (File No. PSPA19-007) to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). The environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. This application is consistent with the previously adopted EIR and introduces no new significant environmental impacts. All previously adopted mitigation measures shall be a condition of project approval and are incorporated herein by reference. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). (APN(s): 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, and 0218-221-10); **submitted by SC Ontario Development Company, LLC. City Council action is required.**

**Action: Continued to the May 26, 2020, Planning Commission meeting.**

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## Monthly Activity Report—New Applications

Month of April 2020

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### **PADV20-005:**

**Submitted by MLC Holdings**

An Administrative Housing Element Available Land Inventory Amendment request to reduce the required housing from 140 to 134 units on 9.62 acres of land located at 2862 South Campus Avenue, within the MDR-18 (Medium Density Residential - 11.1 to 18.0 DUs/Acre) zoning district (APNs: 1051-531-05 and 1051-531-06). Related Files: PDEV20-003 and PMTT20-002. **Staff action is required.**

### **PCUP20-008:**

**Submitted by Amer Sabbah**

A Conditional Use Permit to establish alcoholic sales for off-premises consumption, limited to beer and wine (Type 20 ABC License), in conjunction with a proposed 3,454 square foot convenience store and fueling station on 0.97-acre of land located at 1224 and 1264 East Holt Boulevard, within the BP (Business Park) zoning district (APN: 0110-131-01, 0110-131-19). Related File: PDEV20-010. **Planning Commission action is required.**

### **PCUP20-009:**

**Submitted by Toscana Square, LLC c/o Orbis Real Estate Partners**

A Conditional Use Permit to establish alcoholic beverage sales for off-premises consumption, limited to beer and wine (Type 20 ABC License), in conjunction with a proposed 3,062 square foot convenience store on 1.4 acres of land located at the northwest corner of Milliken Avenue and Riverside Drive, within the Commercial land use district of the Tuscana Village Specific Plan (APN: 1083-361-01). Related File: PDEV20-012. **Planning Commission action is required.**

### **PDEV20-009:**

**Submitted by Kathy Huynh**

A Development Plan to construct a mixed-use project consisting of 57 multiple-family dwellings and 2,744 square feet of retail space on approximately 0.81-acre of land located at 549 West Holt Boulevard, within the MU-1 (Downtown Mixed Use) zoning district (APN: 1049-021-09). Related Files: PUD-20-001 and PPRE19-002. **Planning Commission action is required.**

### **PDEV20-010:**

**Submitted by Amer Sabbah**

A Development Plan to construct one 3,454 square foot commercial building, 959 square foot carwash, and fueling station with a 3,572 square foot canopy on 0.97-acre of land located at 1224 and 1264 East Holt Boulevard, within the BP (Business Park) zoning district (APNs: 0110-131-01 and 0110-131-19). Related File: PCUP20-008. **Planning Commission action is required.**

### **PDEV20-011:**

**Submitted by Glen H Rev Tr 5/13/08 Amnd Weissman**

A Development Plan to construct a 69-unit, four story apartment complex over one-story podium parking structure, on 2.74 acres of land located at 1411 and 1429 North Grove Avenue, within the HDR-45 (High Density Residential – 25.1 to 45.0 DUs/Acre) zoning district (APNs: 1047-433-15 and 1047-433-16). **Planning Commission action is required.**

## Monthly Activity Report—New Applications

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**PDEV20-012: Submitted by Toscana Square, LLC, c/o Orbis Real Estate Partners**

A Development Plan to construct a 3,062 square foot convenience store, a 991 square foot drive-thru car wash, and a fueling station on 1.4 acres of land located at the northwest corner of Milliken Avenue and Riverside Drive, within the Commercial land use district of the Tuscana Village Specific Plan (APN: 1083-361-01). Related File: PCUP20-009. **Planning Commission action is required.**

**PSGN20-035: Submitted by Architectural Design and Sign Inc**

A Sign Plan for the installation of one new primary illuminated wall sign for STATER BROS MARKETS and three new illuminated descriptor wall signs (to read: “Healthy Foods,” “Fresh Local,” and “Full Service”), located at 3460 East Ontario Ranch Road, within the Retail land use district of The Avenue Specific Plan (APN: 0218-402-43). **Staff action is required.**

**PSGN20-036: Submitted by Trulite Signs Inc.**

A Sign Plan for the installation of a wall-mounted illuminated sign for METRO BY T-MOBILE, located at 326 East Holt Blvd, Suite C, within the MU-1 (Downtown Mixed Use) and EA (Euclid Avenue Overlay) zoning districts (APN: 1049-065-11). **Staff action is required.**

**PSGN20-037: Submitted by Layton Construction Company**

A Sign Plan for the installation of two directional signs, one clearance bar, two menu screens, one canopy with order screen, one reface of an existing monument, one blade sign, and three wall-mounted signs for STARBUCKS, located at 3490 East Ontario Ranch Road (New Haven Marketplace), within the Retail land use district of The Avenue Specific Plan (APN: 0218-402-43). **Staff action is required.**

**PSGN20-038: Submitted by Alarmax**

A Sign Plan for the installation of a wall-mounted sign for ALARMAX, located at 1630 South Grove Avenue, Unit A, within the Business Park land use district of the Grove Avenue Specific Plan (APN: 1050-171-32). **Staff action is required.**

**PSGN20-039: Submitted by Bender Sign Company**

\*\*\* Replaced by File No. PSGN20-041 \*\*\*

**PSGN20-040: Submitted by Trulite Signs Inc.**

A Sign Plan for the installation of two illuminated wall signs and two monument replacement signs for METRO BY T MOBILE, located at 1363 East Fourth Street, Suite B, within the CN (Neighborhood Commercial) zoning district (APN: 0108-381-30). **Staff action is required.**



## Monthly Activity Report—New Applications

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### PSGN20-041:

Submitted by Bender Sign Company

A Sign Plan for the installation of one illuminated wall mounted sign for FULL STEAM STAFFING, located at 2121 South Haven Avenue, within the Haven Gateway Centre Specific Plan (APN: 0211-301-04). **Staff action is required.**

### PSGN20-042:

Submitted by Trulite Signs Inc.

A Sign Plan for the installation of one illuminated wall sign and two monument replacement signs for METRO BY T MOBILE, located at 1128 West Mission Boulevard, Suite K, within the CN (Neighborhood Commercial) zoning district (APN: 1011-181-11). **Staff action is required.**

### PSGN20-043:

Submitted by Coast Sign

A Sign Plan of the installation of a wall sign for BBVA (inside of Superior Market), located at 815 West Holt Boulevard, within the CC (Community Commercial) zoning district (APN: 1011-141-40). **Staff action is required.**

### PSGP20-004:

Submitted by Unicare Community Health Center

A Sign Program for a multiple-tenant office/retail complex at 437 North Euclid Avenue and 123 West C Street, within the MU-1 (Downtown Mixed Use) zoning district (APNs: 1048-354-01, 1048-354-02 and 1048-354-03). **Staff action is required.**

### PSPA20-004:

Submitted by City of Ontario

An Amendment to the Grove Avenue Specific Plan to establish consistency with the Ontario International Airport Land Use Compatibility Plan and to change the land use designation on 11.1 acres of land located at 1935 South Grove Avenue, from Office/Commercial to Business Park, to establish consistency with The Ontario Plan Policy Plan Land Use Plan (Policy Plan Exhibit LU-01) (APN: 113-451-14). **City Council action is required.**

### PUD-20-001:

Submitted by Kathy Huynh

A Planned Unit Development to establish development standards and design guidelines on 0.81-acre of land located at 549 West Holt Boulevard, within the MU-1 (Downtown Mixed Use) zoning district (APN: 1049-021-09). Related Files: PUD20-001 and PPRE19-002. **City Council action is required.**

### PVER20-019:

Submitted by AEI Consultants

A Zoning Letter for property located within PA4 (Urban Residential land use district) of the Meredith International Center Specific Plan at 2615 East Date Palm Paseo (APNs: 0110-311-61 and 0110-311-62). **Staff action is required.**

## Monthly Activity Report—New Applications

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**PVER20-020:** Submitted by 200 N Euclid Ave, LLC

An Administrative Use Permit to establish live entertainment and alcoholic beverage sales for on-premises consumption, including beer, wine and distilled spirits (Type 47 ABC license) in conjunction with a proposed 8,460 square foot specialty restaurant and bar, with a 4,200 square foot outdoor patio, on 0.154-acre of land located at 200 North Euclid Avenue, within the MU-1 (Downtown Mixed Use) and EA (Euclid Avenue Overlay) zoning districts (APNs: 1048-552-19, 1048-552-22, and portion of 1048-552-21). **Staff action is required.**

**PVER20-021:** Submitted by Nancy's Insurance

A Zoning Verification for 3175 East Sedona Court, Bldg E-11 (APN: 0210-571-05). **Staff action is required.**

**PVER20-022:** Submitted by The Casitas

A Zoning Verification for 1900 South Campus Avenue (APN: 1050-421-03). **Staff action is required.**

**PVER20-023:** Submitted by Armada Analytics

A Zoning Verification for 1624 East G Street (APNs: 0110-144-01 through 0110-144-59 and 0110-144-67 through 0110-144-69). **Staff action is required.**

**PVER20-024:** Submitted by First American CDS

A Zoning Verification for property located at 1600 South Milliken Avenue, within the IG (General Industrial) zoning district (APN: 0211-231-05). **Staff action is required.**