

# MEREDITH INTERNATIONAL CENTRE

## Specific Plan

Lead Agency:

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**Introduction**

**Section 1**



## INTRODUCTION

### A. BACKGROUND

The original MEREDITH INTERNATIONAL CENTRE Specific Plan was approved in 1981 by City Council Resolution No. 9447. The original Specific Plan authorized the development of a major, high-intensity mixed-use center on the property, containing 4.15 million square feet of commercial uses and up to 800 residential units. The approved Specific Plan described the planned land use mix as primarily office, hotel, and retail commercial, with some multi-family residential uses, at the following maximum intensity:

|                     |                                     |
|---------------------|-------------------------------------|
| <u>Retail:</u>      | 400,000 square feet                 |
| <u>Office:</u>      | 2,850,000 square feet               |
| <u>Hotel:</u>       | 900,000 square feet (1,200 rooms)   |
| <u>Residential:</u> | 800 units                           |
| <u>TOTAL:</u>       | <i>4,150,000 s.f. and 800 units</i> |

Since the Specific Plan's approval in 1981, the site has remained vacant except for a 2.7-acre commercial site on North Archibald Avenue and an improved segment of Inland Empire Boulevard that traverses the property in an east/west alignment. Because high-intensity uses did not develop on the property in the preceding 34 years and because high intensity uses were not anticipated to be constructed on the property in the foreseeable future, this Specific Plan Amendment (2015 Specific Plan Amendment) was prepared to reduce the planned development intensity and prescribe a land use mix that is more compatible with user and market demand.

### B. REGULATORY AUTHORITY AND SPECIFIC PLAN REQUIREMENTS

This Specific Plan (as amended in 2015) is a regulatory document prepared pursuant to the provisions of California Government Code §§ 65450 through 65457, which grants local government agencies the authority to prepare Specific Plans for the systematic implementation of their General Plan for all or part of the area covered by the General Plan. While the Policy Plan (General Plan) component of The Ontario Plan (hereafter referred to as "Policy Plan") examines the entire City, the MEREDITH INTERNATIONAL CENTER Specific Plan concentrates on the individual development issues of approximately 257.7 acres of land generally located in the north-central portion of the City.

California Government Code §§ 65450 through 65457 establish the authority to adopt a Specific Plan, identify the required contents of a Specific Plan, and mandate consistency with the General Plan. According to California Government Code §65451:

- (a) A Specific Plan shall include text and a diagram which specify all of the following in detail:
  1. The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
  2. The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the

plan and needed to support the land uses described in the plan.

3. Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.
4. A program of implementation measures including regulations, programs, public works projects, and financing measures necessary to carry out items (1), (2) and (3).

(b) The specific plan shall include a statement of the relationship of the specific plan to the general plan.

This Specific Plan includes all of the required elements listed above and establishes the essential link between the policies of the Policy Plan and the MEREDITH INTERNATIONAL CENTRE property. All future development plans and implementing actions within MEREDITH INTERNATIONAL CENTRE are required to be consistent with the regulations set forth in this Specific Plan and with all other applicable City regulations.

### **C. SPECIFIC PLAN CONTENT AND ORGANIZATION**

This MEREDITH INTERNATIONAL CENTRE Specific Plan (as amended in 2015) is a regulatory document that establishes the land use designations, densities, development standards (zoning), and design guidelines for the subject property. In addition, circulation and other infrastructure improvements such as water, wastewater, and flood control/drainage systems are addressed by this Specific Plan to ensure their proper sizing and timely installation.

This Specific Plan is divided into the following sections:

- Section 1 - Introduction
- Section 2 - Development Plan
- Section 3 - Circulation Plan
- Section 4 - Utility Infrastructure Plan
- Section 5 - Development Standards
- Section 6 - Design Guidelines
- Section 7 - Implementation Plan
- Section 8 - General Plan Consistency Statement

Following this introductory section, every Specific Plan section builds from information provided in the proceeding sections. Therefore, is important to read the entirety of this Specific Plan to understand the full scope of directives and objectives for MEREDITH INTERNATIONAL CENTRE.

### **D. LOCATION AND PHYSICAL SETTING**

MEREDITH INTERNATIONAL CENTRE encompasses approximately 257.7 acres of land generally located in the in the north-central portion of the City of Ontario, California within San Bernardino County. As shown on Figure 1-1, *Regional Map*, the property is located approximately ½-mile north of Ontario International Airport, immediately north of Interstate 10 (I-10), and 2.75 miles west of Interstate 15 (I-15). As shown on Figure 1-2, *Vicinity Map*, the property is bounded by East 4th Street on the north, North Vineyard Avenue on the west, and North Archibald Avenue on the east.

MEREDITH INTERNATIONAL CENTRE

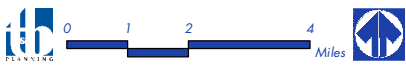
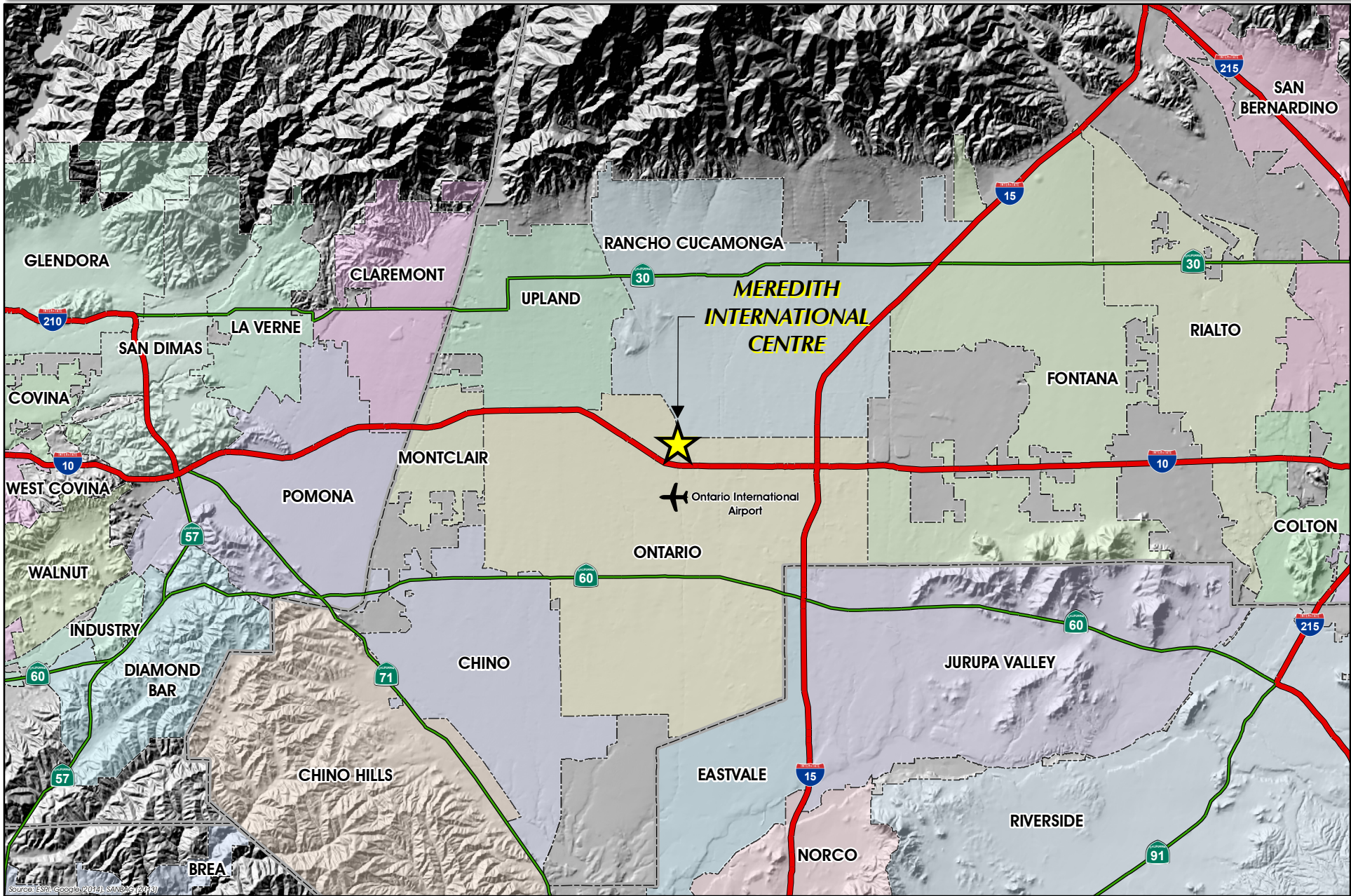
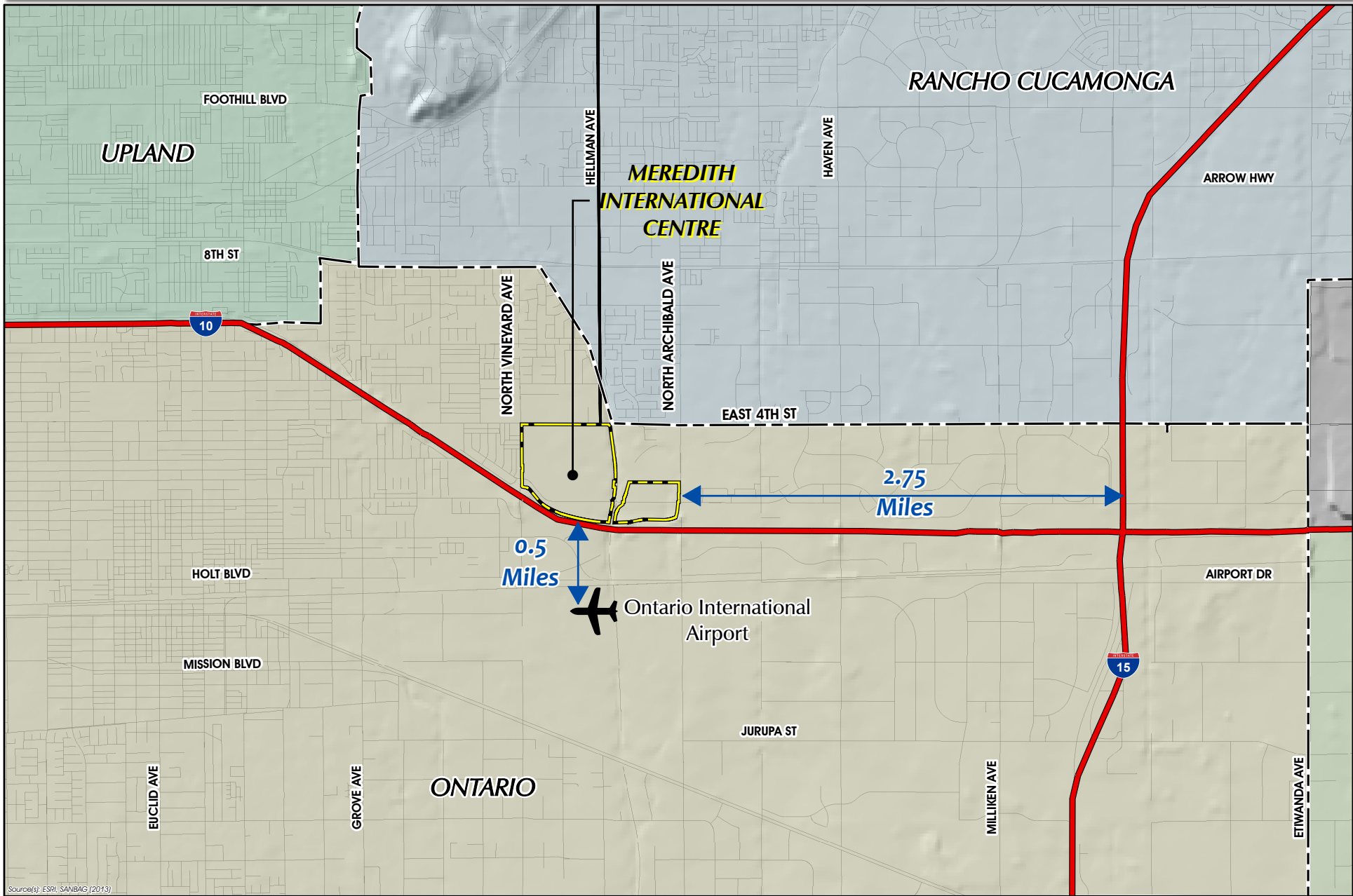


FIGURE 1-1  
REGIONAL MAP

# MEREDITH INTERNATIONAL CENTRE



Source(s): ESRI, SANBAG (2013)

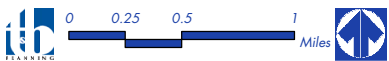


FIGURE 1-2  
VICINITY MAP

The site's adjacency to I-10, and its proximity to I-15 and Ontario International Airport, offers an advantage to the land uses of MEREDITH INTERNATIONAL CENTRE that benefit from proximity to regional transportation corridors. Additionally, in 2014, the Gold Line Foothill Construction Authority was studying the extension of a light rail transit (LRT) line along the east side of the Cucamonga Creek Channel to ultimately connect to Ontario International Airport. Given the myriad of nearby transportation corridors (freeway, airport, and future LRT), this Specific Plan was aptly designed to include land uses for MEREDITH INTERNATIONAL CENTRE that capitalize on opportunities to connect, and provide ease of access, to the major transit corridors of the region.

The Specific Plan property is physically divided into four quadrants by the Cucamonga Creek and Deer Creek Channels that run in a north/south direction and Inland Empire Boulevard that runs in an east/west direction. Figure 1-3, *Aerial Photograph (2015)*, depicts the configuration of the Specific Plan property in context with surrounding properties. As of 2015, the Specific Plan property was vacant except for a 2.7-acre commercial retail development on North Archibald Avenue, a segment of Inland Empire Boulevard, and a 2.0-acre elementary school site on East 4th Street. The school site was not included within the Specific Plan boundaries when the MEREDITH INTERNATIONAL CENTRE Specific Plan was adopted in 1981, but was later added as part of the 2015 Specific Plan Amendment.

As of 2015, land uses surrounding the MEREDITH INTERNATIONAL CENTRE property were mixed and included I-10 to the south, commercial retail, office, and residential uses to the north, commercial retail to the northwest, residential uses to the west, residential and industrial uses to the northeast (north of East 4th

Street, located in the City of Rancho Cucamonga), San Bernardino Flood Control District property and residential uses to the immediate northeast (south of East 4th Street), and a shopping center and the Cucamonga-Guasti Regional Park to the east, across North Archibald Avenue.

## **E. PLANNING GOALS AND OBJECTIVES**

This Specific Plan accomplishes the following objectives:

- Provides economic opportunities and economic growth in the City by authorizing the development and operation of industrial, urban commercial, and urban residential uses on MEREDITH INTERNATIONAL CENTRE property.
- Anticipates market demand by providing for a mixture of industrial, urban commercial, and urban residential land uses that are marketable and financially feasible within the evolving economic profile of the City.
- Provides a mixture of industrial and urban commercial land uses to attract new businesses and jobs to the City.
- Provides for industrial and urban commercial uses adjacent to I-10, in proximity to I-15, and near Ontario International Airport.
- Provides for industrial land uses that accommodate buildings with loading bays that have a short direct access route to I-10 on- and off-ramps.
- Provides for urban commercial and urban residential land uses near the potential extension of the Gold Line LRT.

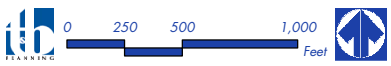


FIGURE 1-3  
AERIAL PHOTOGRAPH (2015)

- Designs a circulation network that provides for the safe and efficient movement of vehicles and pedestrians through and within the Specific Plan area.
- Realigns Inland Empire Boulevard in a northerly direction between the Cucamonga Creek Channel and East Vineyard Avenue to provide adequate intersection spacing with the ultimate configuration of I-10 on- and off-ramps.
- Identifies needed capital improvements for water, sewer, drainage, and road facilities that provide adequate services to the Specific Plan area.
- Sets forth guidelines for architecture, walls, fencing, signage, lighting, and entry treatments that are consistent with the Ontario Development Code and that reflect a positive and well-defined identity for MEREDITH INTERNATIONAL CENTRE.
- Sets forth landscape guidelines that emphasize the use of drought-tolerant and water-efficient plant materials that are consistent with the Ontario Development Code.
- Sets forth guidelines for energy efficiency that promote the conservation of natural resources used in the construction and operation of MEREDITH INTERNATIONAL CENTRE.
- Provides a comprehensive master plan for the property that is feasible to accomplish and results in successful construction and operation of MEREDITH INTERNATIONAL CENTRE.

## **F. APPROVAL PROCESS AND COMPANION ACTIONS**

This Specific Plan is required to be considered by the City of Ontario Planning Commission and City Council, and adopted by ordinance of

the City Council. Any future amendments thereto also require consideration by the Planning Commission and City Council, but may be adopted by resolution of the City Council. More information on implementation procedures is contained in Section 7, *Implementation Plan*, of this document.

A Policy Plan (General Plan) Amendment (GPA) and Zone Change (ZC) were processed concurrently with the 2015 Amendment to this Specific Plan. The GPA and ZC were companion actions to the approval of this Specific Plan (as amended in 2015) that achieved consistency between the General Plan land use designations, this Specific Plan's land use designations, and zoning.

In addition to this Specific Plan and the accompanying GPA and ZC, an Environmental Impact Report (EIR) was certified in compliance with the California Environmental Quality Act (CEQA) to serve as the project-wide environmental document. Together, this Specific Plan and the environmental mitigation measures contained in the accompanying EIR provide a path to develop the property taking into account all applicable goals, objectives, requirements, and environmental regulations.

## **G. RELATIONSHIP TO THE POLICY PLAN COMPONENT OF THE ONTARIO PLAN**

On January 26, 2010, the City of Ontario adopted The Ontario Plan (TOP) which serves as the City's business plan and includes a long term Vision and a principle-based Policy Plan (General Plan). TOP establishes the direction and vision for the City and provides a guidance system that will shape the Ontario community of the future. TOP provides policies to accommodate change over a 30-year period commencing in 2010, the beginning of the planning period.

TOP consists of a six-part Component Framework: 1) Vision, 2) Governance Manual, 3) Policy Plan, 4) City Council Priorities, 5) Implementation, and 6) Tracking and Feedback.

The Policy Plan component of TOP, which acts as the City’s General Plan, designates the MEREDITH INTERNATIONAL CENTRE property for “Mixed Use” and “Industrial (0.55 floor area ratio, FAR)” land uses. The Policy Plan describes Mixed Use as “an intense mixture of uses that, when concentrated, create focal points for community activity and identity and facilitate the use of transit,” and describes Industrial as a “variety of light industrial uses, including warehousing/distribution, assembly, light manufacturing, research and development, storage, repair facilities, and supporting retail and professional office uses.” TOP establishes a development capacity for the Specific Plan area as summarized below:

| TOP Land Use Designation | Gross Acres  | Building Square Footage      | Dwelling Units           |
|--------------------------|--------------|------------------------------|--------------------------|
| Industrial (0.55 FAR)    | 154.6        | 3,703,906                    | --                       |
| Mixed Use <sup>1</sup>   | 103.1        | 3,145,032                    | 1,236                    |
| <b>Total</b>             | <b>257.7</b> | <b>6,848,938<sup>2</sup></b> | <b>1,236<sup>2</sup></b> |

<sup>1</sup> Mixed Use comprises 30.9 acres of residential land uses (40 dwelling units per acre) and 72.2 acres of office/retail land uses (1.0 FAR).

<sup>2</sup> Building square footage and residential density (i.e., dwelling units) do not reflect the Policy Plan maximum, rather they reflect TOP Environmental Impact Report intensity factors.

The MEREDITH INTERNATIONAL CENTRE Specific Plan implements the City’s Policy Plan for the subject property and provides zoning regulations for development of the site by establishing permitted land use, development standards, infrastructure requirements, and

implementation requirements for future development. A comprehensive set of design guidelines and development regulations are included to guide and regulate site planning, landscape, and architectural character within the Specific Plan area to ensure that excellence in project design is achieved during development. The MEREDITH INTERNATIONAL CENTRE Specific Plan also establishes the procedures and requirements to approve new implementing development within the Specific Plan Area.

## H. RELATIONSHIP TO ONTARIO INTERNATIONAL AIRPORT LAND USE COMPATIBILITY PLAN

The MEREDITH INTERNATIONAL CENTRE Specific Plan area is located wholly within the Airport Influence Area (AIA) of the Ontario International Airport, as designated by the Ontario International Airport Land Use Compatibility Plan (ALUCP) and shown in Figure 1-4, *Ontario International Airport Influence Area*. All future development within the Specific Plan area is required to comply with applicable policies and criteria of the Ontario International Airport ALUCP.

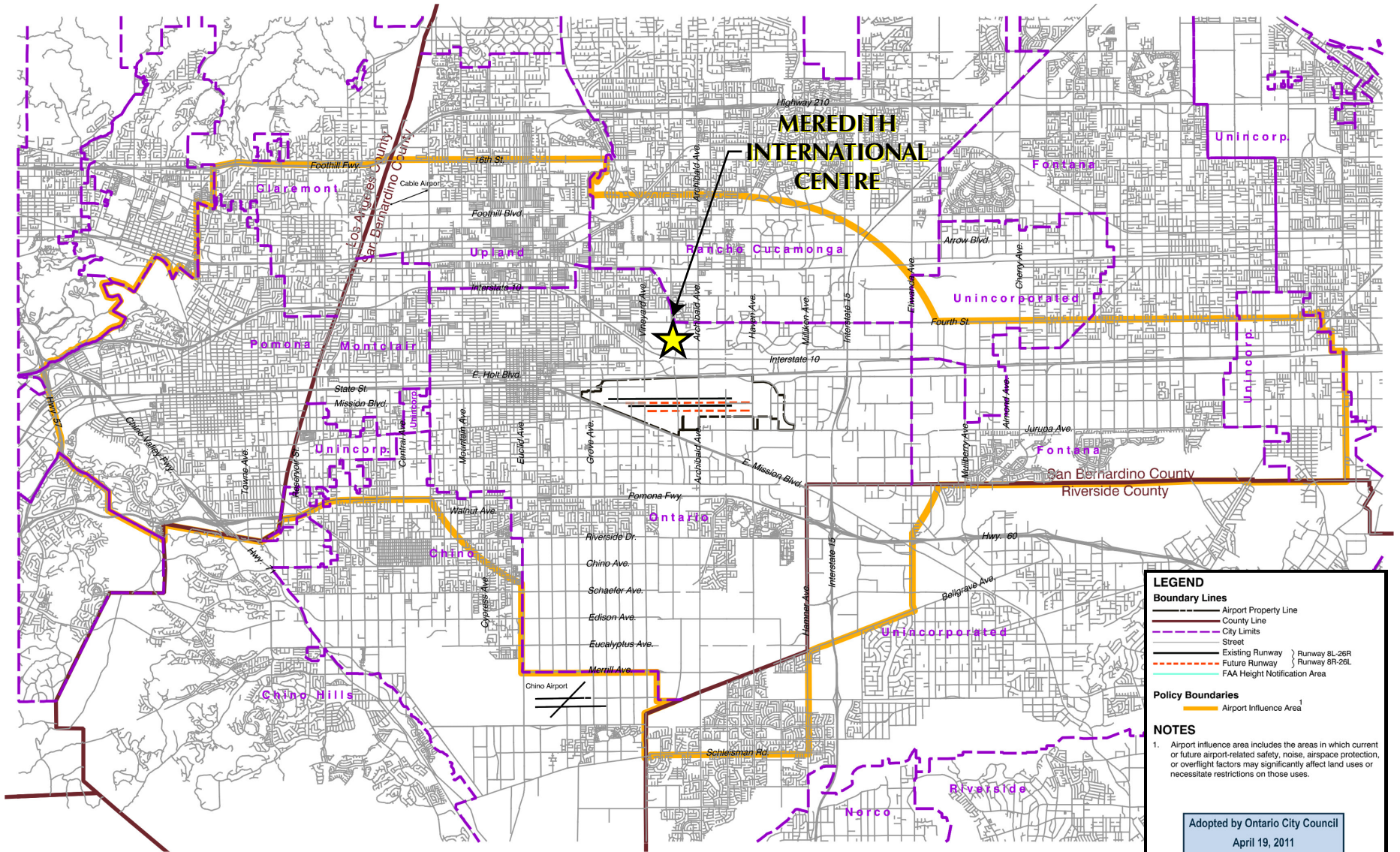
## I. SEVERABILITY CLAUSE

This Specific Plan document enables the City of Ontario to facilitate the processing and approval of implementing permits and approvals necessary for development in MEREDITH INTERNATIONAL CENTRE.

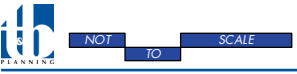
If any regulation, condition, program, or portion of this Specific Plan is held invalid or unenforceable, such portions shall be deemed separate, distinct, and independent provisions, and the invalidity of such portions or provisions shall not affect the validity and enforceability of the remaining provisions contained herein.



# MEREDITH INTERNATIONAL CENTRE



Source: OMT ALLUCP (04-19-2011)



# Development Plan

## Section 2

## DEVELOPMENT PLAN

### A. VISION

The greatest value offered by MEREDITH INTERNATIONAL CENTRE is its location. With views of the San Gabriel Mountains in the distance, the property is located immediately north of Interstate 10, approximately ½-mile north of Ontario International Airport, and 2.75 miles west of Interstate 15. When creating a guiding vision for MEREDITH INTERNATIONAL CENTRE, the property’s setting and proximity to major transportation corridors and facilities were given the highest consideration.

MEREDITH INTERNATIONAL CENTRE establishes a mixture of employment and residential land uses arranged in a sensible and efficient manner that allow ease of access and complement the surrounding community. To that end, MEREDITH INTERNATIONAL CENTRE is designed as a contemporary, horizontally mixed-use center containing industrial, urban commercial, and urban residential land uses that take advantage of the property’s location near regional transportation corridors.

MEREDITH INTERNATIONAL CENTER is a place where businesses can locate and thrive and where multi-family housing opportunities are within walking distance to shopping, goods and services, employment opportunities, and transit. The mixture of uses and their orientation on the property are designed for the benefit of the residents, visitors, and employees of MEREDITH INTERNATIONAL CENTRE, the surrounding community, and the region.



## B. LAND USE PLAN

This Specific Plan specifies three land use districts: Industrial, Urban Commercial, and Urban Residential. For planning purposes, the 257.7-acre MEREDITH INTERNATIONAL CENTRE Specific Plan property is divided into six planning areas.

- Planning Area 1 – Industrial
- Planning Area 1A – Industrial
- Planning Area 2 – Urban Commercial
- Planning Area 3 – Urban Commercial
- Planning Area 4 – Urban Residential
- Planning Area 5 – Urban Commercial

Figure 2-1, *Land Use Plan*, depicts the physical arrangement of land uses on the Specific Plan property. Table 2-1, *Land Use Plan Statistical Summary*, lists each planning area and their respective land use district, acreage, and development intensity. Buildout intensity of this Specific Plan is shown in the table below.

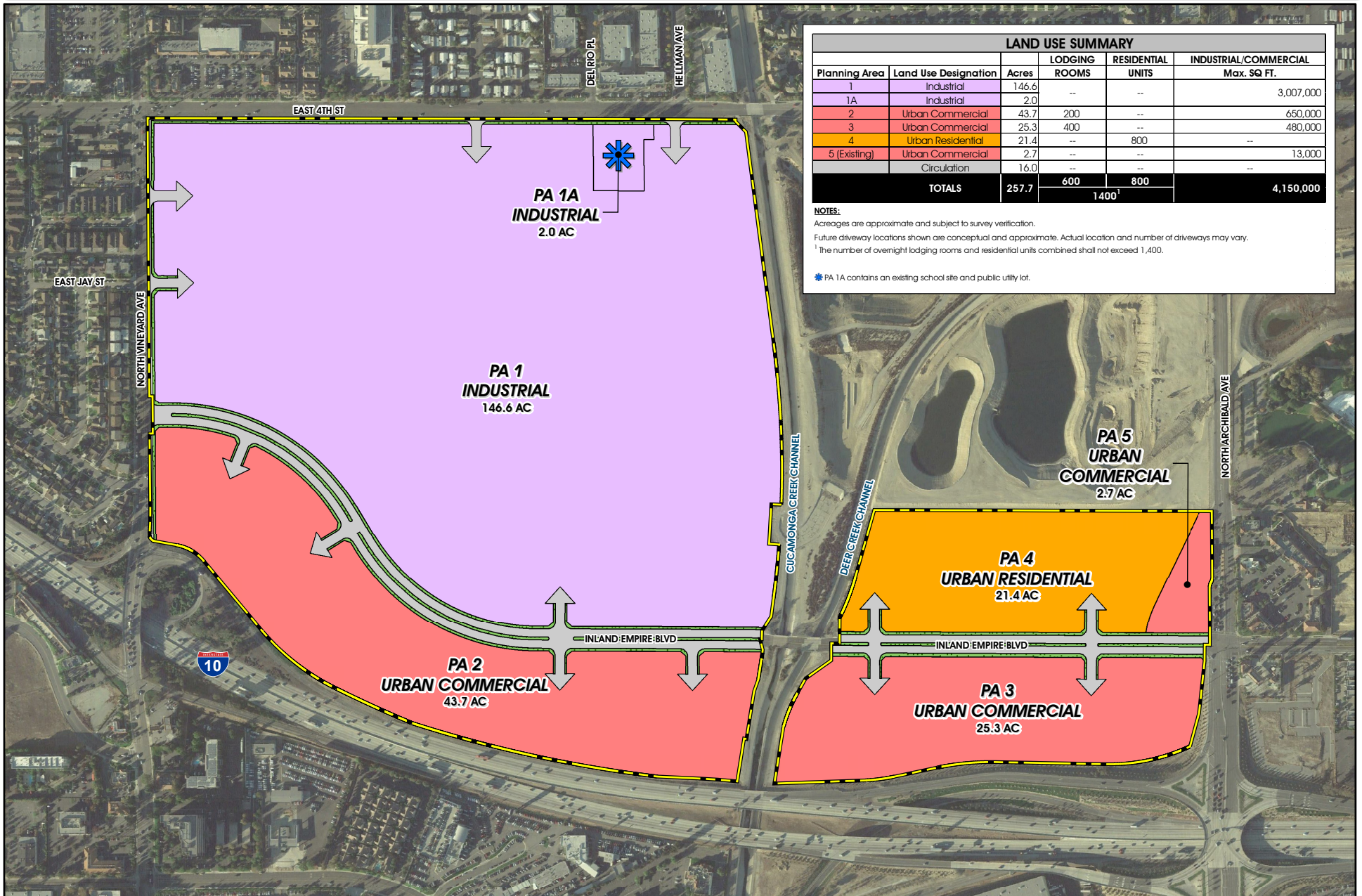
Planning Area boundaries were determined by natural and man-made physical divisions formed by drainage channels, roads, and existing development. The Cucamonga Creek Channel and Deer Creek Channel (not a part of the Specific Plan area) divide the Specific Plan property in a north-south direction. The channels create a physical separation between the Industrial land uses in Planning Area 1 and the Urban Residential land uses in Planning Area 4. The channels also divide the two largest Urban Commercial Planning Areas (Planning Area 2 and Planning Area 3). In a similar manner, the alignment of Inland Empire Boulevard separates the Industrial land uses in Planning Area 1 from the Urban Commercial land uses in Planning Area 2 and separates Planning Area 4 and Planning Area 5 from Planning Area 3.

The boundaries of Planning Area 1A and Planning Area 5 were determined by uses that were present in those areas at the time Specific Plan Amendment No. 1 was prepared (2015). Planning Area 1A (2.0 acres) was developed with an elementary school and public water utility use and Planning Area 5 (2.7 acres) was developed as a small retail commercial shopping center.

| Use Category             | Development Intensity | Planning Area(s) |
|--------------------------|-----------------------|------------------|
| <b>Industrial</b>        | 3,007,000 square feet | 1 and 1A         |
| <b>Urban Commercial</b>  | 1,143,000 square feet | 2, 3, and 5      |
| <b>Urban Residential</b> | 800 units             | 4                |

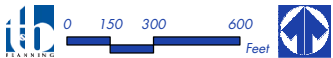
Refer to Figure 2-1, *Land Use Plan*, and Table 2-1, *Land Use Plan Statistical Summary*, for more detailed information.

# MEREDITH INTERNATIONAL CENTRE



| LAND USE SUMMARY |                      |              |               |                   |                                    |
|------------------|----------------------|--------------|---------------|-------------------|------------------------------------|
| Planning Area    | Land Use Designation | Acres        | LODGING ROOMS | RESIDENTIAL UNITS | INDUSTRIAL/COMMERCIAL Max. SQ. FT. |
| 1                | Industrial           | 146.6        | --            | --                | 3,007,000                          |
| 1A               | Industrial           | 2.0          | --            | --                | --                                 |
| 2                | Urban Commercial     | 43.7         | 200           | --                | 650,000                            |
| 3                | Urban Commercial     | 25.3         | 400           | --                | 480,000                            |
| 4                | Urban Residential    | 21.4         | --            | 800               | --                                 |
| 5 (Existing)     | Urban Commercial     | 2.7          | --            | --                | 13,000                             |
|                  | Circulation          | 16.0         | --            | --                | --                                 |
| <b>TOTALS</b>    |                      | <b>257.7</b> | <b>600</b>    | <b>800</b>        | <b>4,150,000</b>                   |

**NOTES:**  
 Acreages are approximate and subject to survey verification.  
 Future driveway locations shown are conceptual and approximate. Actual location and number of driveways may vary.  
<sup>1</sup> The number of overnight lodging rooms and residential units combined shall not exceed 1,400.  
 \* PA 1A contains an existing school site and public utility lot.



**FIGURE 2-1  
 LAND USE PLAN**

*Table 2-1 Land Use Plan Statistical Summary*

| Planning Area | Land Use          | Acreage      | Development Intensity    |                          |                                |
|---------------|-------------------|--------------|--------------------------|--------------------------|--------------------------------|
|               |                   |              | Units                    |                          | Non-Residential Square Footage |
|               |                   |              | Overnight Lodging Room   | Multi-Family Residential |                                |
| 1             | Industrial        | 146.6        | --                       | --                       | 3,007,000                      |
| 1A            | Industrial        | 2.0          | --                       | --                       |                                |
| 2             | Urban Commercial  | 43.7         | 200                      | --                       | 650,000                        |
| 3             | Urban Commercial  | 25.3         | 400                      |                          | 480,000                        |
| 4             | Urban Residential | 21.4         | --                       | 800                      | --                             |
| 5             | Urban Commercial  | 2.7          | --                       | --                       | 13,000 <sup>A</sup>            |
| --            | Circulation       | 16.0         | --                       | --                       | --                             |
| <b>TOTALS</b> |                   | <b>257.7</b> | <b>600</b>               | <b>800</b>               | <b>4,150,000</b>               |
|               |                   |              | <b>1,400<sup>B</sup></b> |                          |                                |

A – Planning Area 5 was fully constructed at the time Specific Plan Amendment No. 1 was prepared. The building square footage of 13,000 SF is approximate.

B – Overnight lodging rooms may be swapped for additional multi-family residences in Planning Area 4 at a ratio of 1:1. Alternatively, multi-family residences may be swapped for additional overnight lodging rooms in Planning Areas 2 and/or 3 at a ratio of 1:1. The number of overnight lodging rooms and residential units combined shall not exceed 1,400.

**B.1. Planning Area 1 - Industrial**

|            |  |
|------------|--|
| VISION     |                           |
| STATISTICS | <p>LAND USE CATEGORY: Industrial</p> <p>ACREAGE: 146.6 acres</p> <p>BUILDING INTENSITY: 3,007,000 s.f.</p> |

Encompassing 146.6 acres in the northwestern quadrant of the Specific Plan area, Planning Area 1 is designated for Industrial land use and is the largest of the planning areas. As shown on Figure 2-2, *Planning Area 1*, this planning area is bordered on the north by East 4th Street, on the south by Inland Empire Boulevard, on the east by the Cucamonga Creek Channel, and on the west by North Vineyard Avenue.

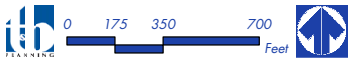
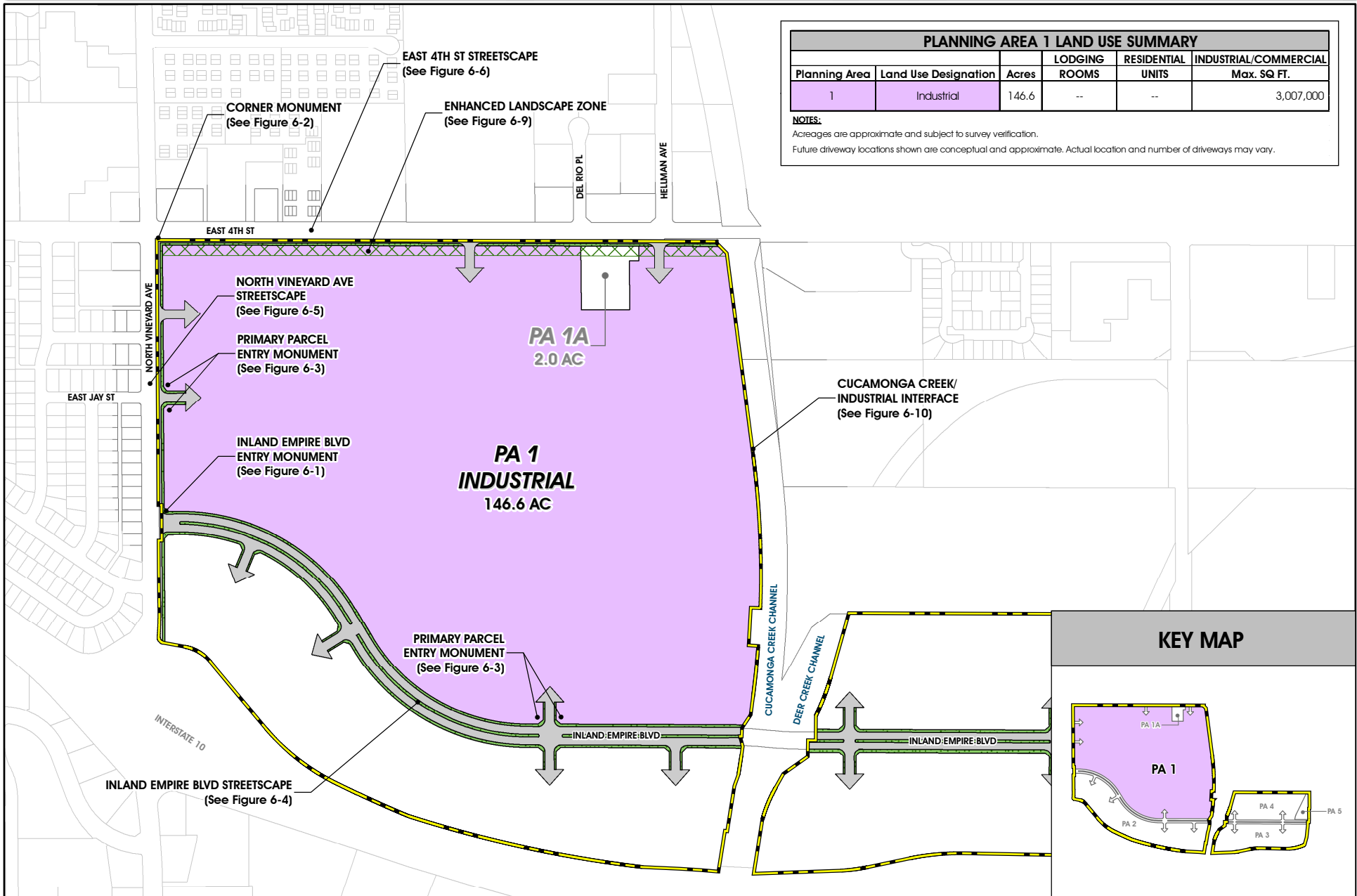
Planning Area 1 is designed to accommodate buildings of various sizes that house users such as general light industrial, manufacturing, and warehouse/distribution operations. These types of uses typically attract both passenger car and truck traffic that benefit from a short, direct route to the regional transportation (freeway) system. To facilitate vehicular access to the Interstate 10/North Vineyard Avenue on- and off-ramps, two local industrial streets, Del Rio Place and Jay Street, are provided within Planning Area 1. Del Rio Place and Jay Street traverse through Planning Area 1 and provide an interior connection between North Vineyard Avenue and Inland Empire Boulevard. Del Rio Place, a north-south oriented

street, provides access from the south via Inland Empire Boulevard. Jay Street, an east-west oriented street, provides access from the west via North Vineyard Avenue. Refer to Section 3, *Circulation Plan*, for more information about circulation to, from, and within Planning Area 1.

North of Planning Area 1, across East 4th Street, is a mixture of commercial and residential land uses. As shown on Figure 2-2, an enhanced landscape zone is provided along the northern boundary of Planning Area 1 as an amenity along East 4th Street. This landscape area provides a transitional area from the buildings in Planning Area 1 to East 4th Street and the land uses north of East 4th Street. The enhanced landscape zone provides a pleasant transition to soften and blend into the adjacent land uses.

The buildings in Planning Area 1 provide attractive building façades facing East 4th Street, with no visible loading bays. Along the western side of Planning Area 1, buildings facing North Vineyard Avenue also offer attractive architectural features with no visible loading bays. The buildings in the western portion of Planning Area 1 are smaller in scale compared to the larger structures elsewhere in Planning Area 1. As discussed in detail in Section 6, *Design Guidelines*, the placement, orientation, design, and architectural features of the building façades visible from surrounding roadways considers and respects the adjacent land uses.

Green spaces that include water quality/detention basins and landscape pockets are deliberately placed in the southern portion of Planning Area 1 along Inland Empire Boulevard. These green spaces serve a practical purpose as part of the development’s storm drain system and also assist in providing a spacious and inviting landscape buffer along the roadway.





**B.2. Planning Area 1A - Industrial**

|                   |   |
|-------------------|---|
| <b>VISION</b>     |              |
| <b>STATISTICS</b> | <p>LAND USE CATEGORY: Industrial</p> <p>ACREAGE: 2.0 acres</p> <p>BUILDING INTENSITY: N/A</p> |

Planning Area 1A is a 2.0-acre area located south of East 4th Street, which is surrounded on its south, east, and west boundaries by Planning Area 1. Refer to Figure 2-3, *Planning Area 1A*. This area was not a part of the Specific Plan previously approved in 1981. Planning Area 1A was added to MEREDITH INTERNATIONAL CENTRE by the 2015 Specific Plan Amendment. At that time, the property contained the 6,767 s.f. Italo M. Bernt Elementary School (located on a 1.9-acre lot) and an adjacent 0.1-acre lot established by Ontario Municipal Utilities Company for use as a water treatment facility. This Specific Plan allows for the continuation of these uses.

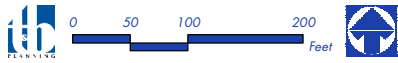
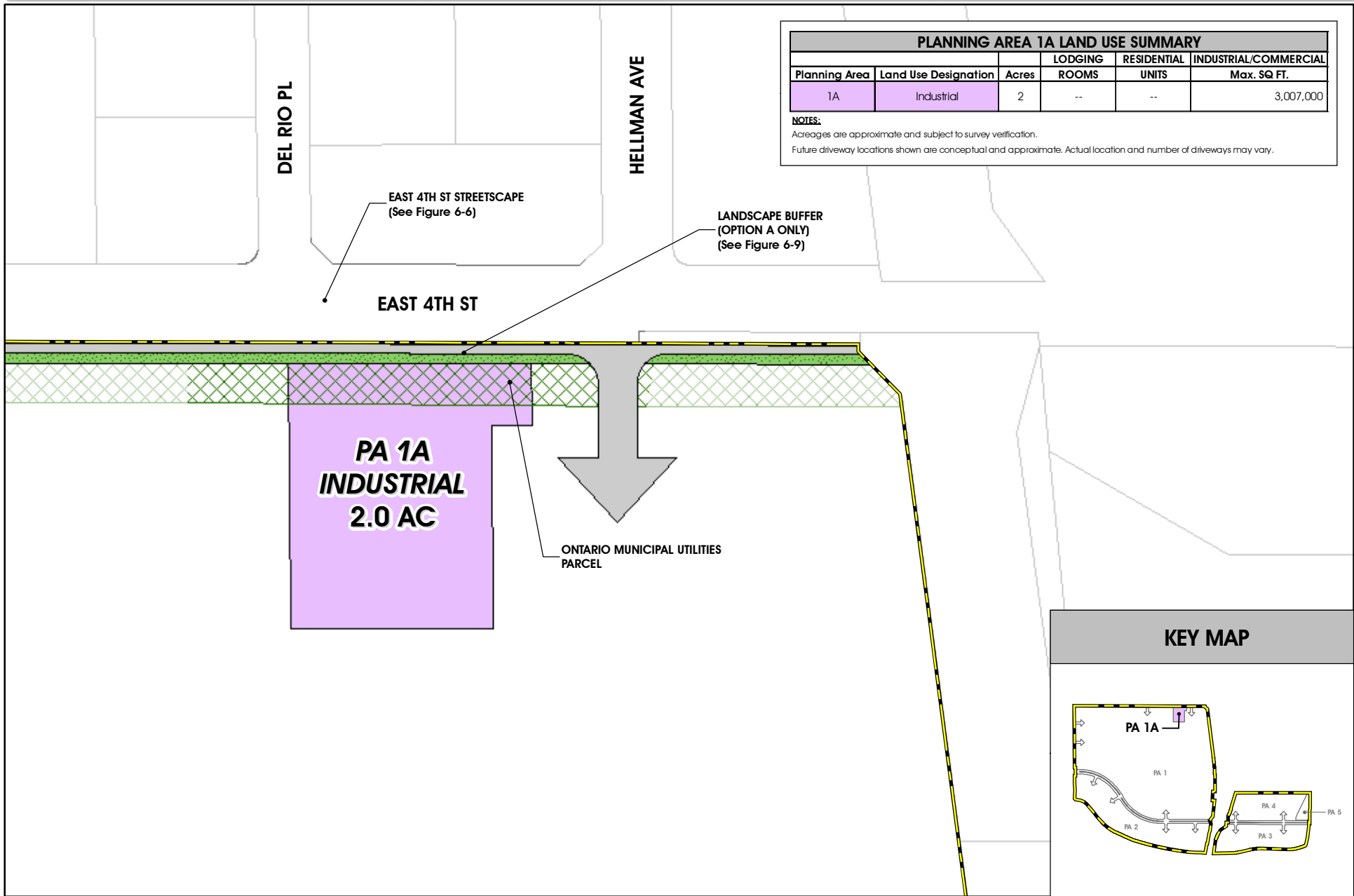
Two build-out options are available to Planning Area 1A. These options are meant to work in collaboration with development of Planning Area 1.

In the event that Planning Area 1A redevelops in conjunction with the development of Planning Area 1 (Option A), no building square footage is allocated specifically to Planning Area 1A. Instead, the maximum building intensity of Planning Area 1 would be as

discussed previously in Subsection 2.B.1, *Planning Area 1 - Industrial*. Under Option A, build-out of Planning Area 1 and Planning Area 1A would occur seamlessly as if they were one planning area. With this coordinated planning, an enhanced landscape zone is designed along the northern boundary of Planning Area 1A extending from Planning Area 1 as an amenity along East 4th Street. Multiple buildings would be accommodated in the combined area of Planning Area 1 and Planning Area 1A, designed to house users such as general light industrial, manufacturing, and warehouse/distribution operations.

In the event that Planning Area 1A does not redevelop (remains a school, Option B), the existing 6,767 s.f. elementary school building located in Planning Area 1A at the time the 2015 Amendment to this Specific Plan was prepared would remain. Under Option B, Planning Area 1A would be buffered from adjacent industrial development in Planning Area 1 by a landscape buffer provided inside the boundary of Planning Area 1. Under Option B, access to Planning Area 1A would be provided via a driveway connecting to East 4th Street. There would be no internal vehicle through-way from Planning Area 1A to the rest of MEREDITH INTERNATIONAL CENTRE.

Both Options A and B accommodate the 0.1-acre Ontario Municipal Utilities Company water treatment facility at Planning Area 1A.



**B.3. Planning Area 2 – Urban Commercial**

**VISION**  
**STATISTICS**



LAND USE CATEGORY: Urban Commercial  
 ACREAGE: 43.7 acres  
 BUILDING INTENSITY: 650,000 s.f.

Area 2. The range of permitted land uses achieves this Specific Plan’s vision and intention to provide a mix of uses that take advantage of proximity to transportation corridors and serve the surrounding community and region. Refer to Section 5, *Development Standards*, for a list of permitted uses and additional information.

Vehicular access into Planning Area 2 is provided via Inland Empire Boulevard. The 4-way intersection at Del Rio Place and Inland Empire Boulevard provides a primary entry point to Planning Area 2 from Inland Empire Boulevard. Direct access into Planning Area 2 from North Vineyard Avenue is not permitted.

Planning Area 2 encompasses 43.7 acres of land located in the southwestern portion of the Specific Plan area. As shown on Figure 2-4, *Planning Area 2*, it is bordered on the north by Inland Empire Boulevard, on the south by Interstate 10, on the west by North Vineyard Avenue, and on the east by the Cucamonga Creek Channel. Having a long, linear configuration adjacent to Interstate 10 with good visibility to passing motorists and easy access to the North Vineyard Avenue on- and off-ramps, Planning Area 2 is designed to accommodate Urban Commercial land uses. In concert with Planning Area 3, the uses in Planning Area 2 are designed to form an attractive visual gateway into MEREDITH INTERNATIONAL CENTRE.

The Urban Commercial designation of Planning Area 2 allows for a range of commercial uses that benefit from the property’s adjacency to Interstate 10 and approximately ½-mile distance to Ontario International Airport. Planning Area 2 is designed as an area offering a variety of market-driven commercial uses to service nearby residents, visitors to the area, and travelers on Interstate 10. Two hundred overnight lodging rooms also are permitted in Planning

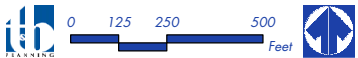
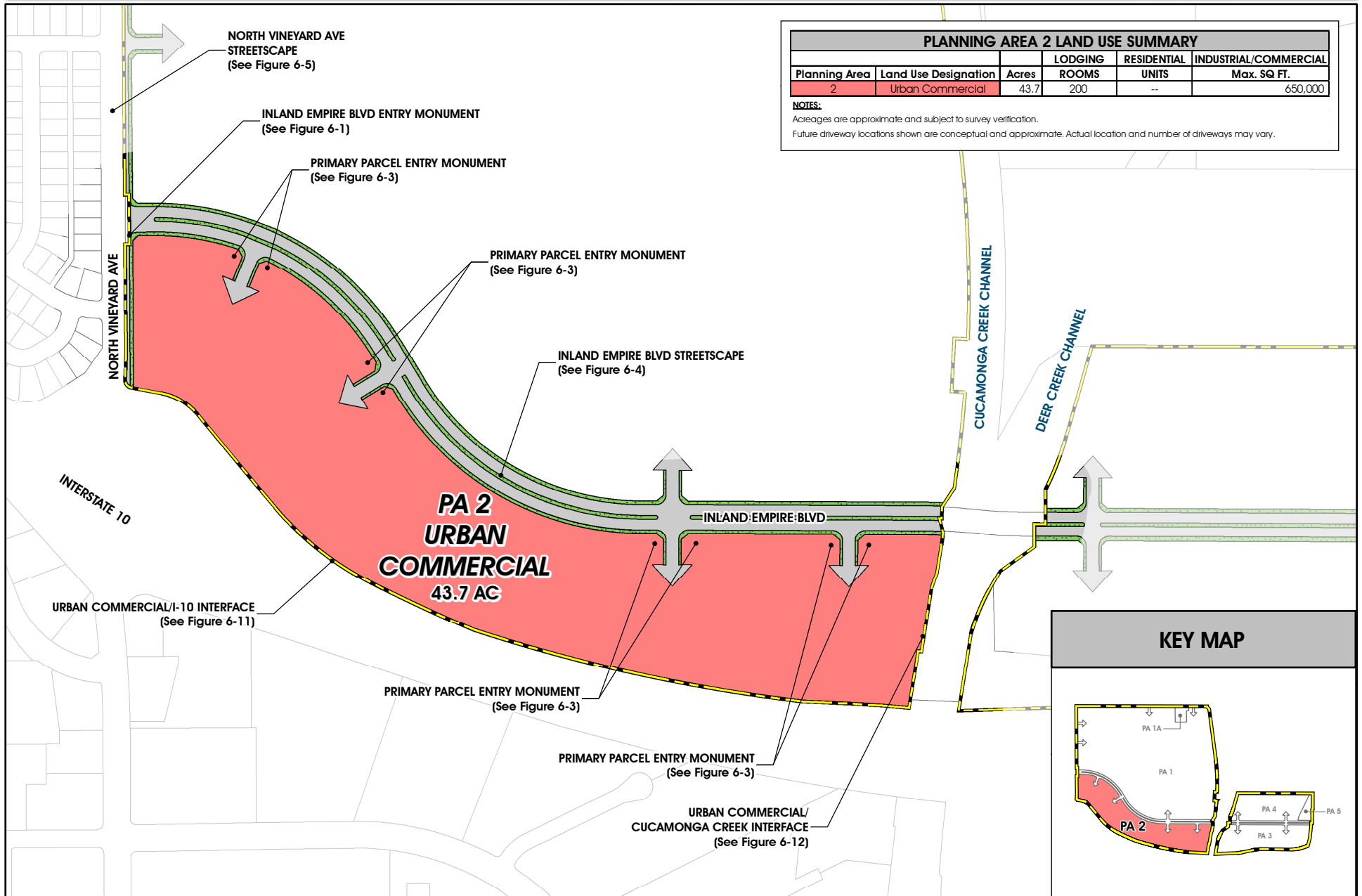



FIGURE 2-4

PLANNING AREA 2

**B.4. Planning Area 3 – Urban Commercial**

|                   |   |
|-------------------|---|
| <b>VISION</b>     |                              |
| <b>STATISTICS</b> | <p>LAND USE CATEGORY: Urban Commercial</p> <p>ACREAGE: 25.3 acres</p> <p>BUILDING INTENSITY: 480,000 s.f.</p> |

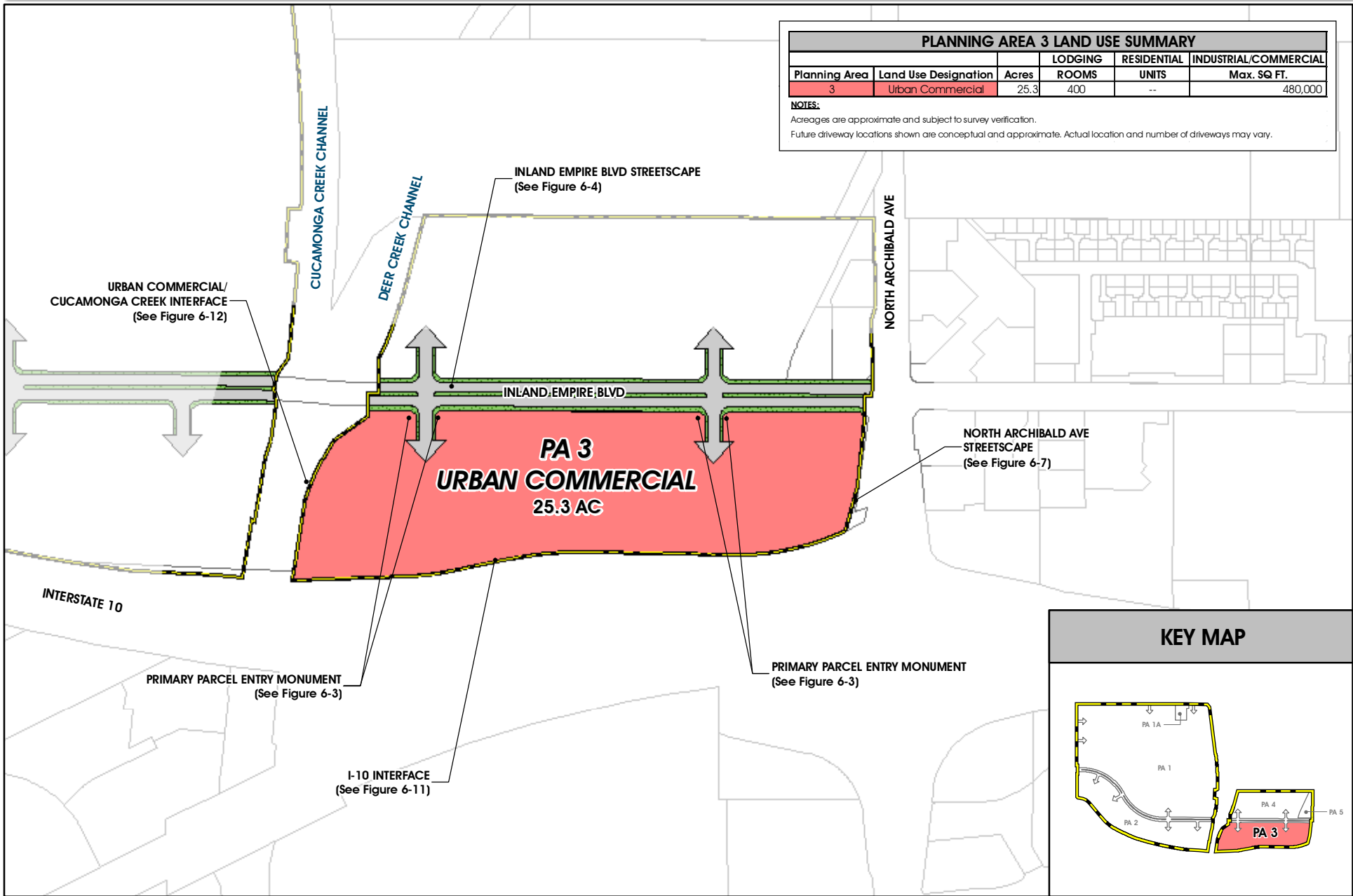
Area 3 is afforded a greater opportunity than Planning Area 2 to offer smaller, pedestrian-oriented retail establishments. Four hundred overnight lodging rooms also are permitted in Planning Area 3 with the intention of serving the surrounding community and region, such as visitors to the nearby Ontario Convention Center and Ontario International Airport. Refer to Section 5, *Development Standards*, for a list of permitted uses and additional information.

Vehicular access into Planning Area 3 is provided via Inland Empire Boulevard. No direct driveway access into the planning area is permitted from North Archibald Avenue.

Planning Area 3 is comprised of 25.3 acres of land located in the southeastern portion of the Specific Plan property. Planning Area 3 is bordered on the north by Inland Empire Boulevard, on the south by Interstate 10, on the west by the Deer Creek Channel, and on the east by North Archibald Avenue. Refer to Figure 2-5, *Planning Area 3*.

Designed to complement the uses of Planning Area 2 also positioned along Interstate 10, Planning Area 3 has good visibility to passing motorists and access to the North Archibald Avenue on- and off-ramps. Planning Area 3 is designed to accommodate Urban Commercial land uses and, in concert with Planning Area 2, form an attractive visual gateway into MEREDITH INTERNATIONAL CENTRE.

Like Planning Area 2, the Urban Commercial designation of Planning Area 3 allows for a range of commercial uses that benefit from adjacency to Interstate 10 and approximately ½-mile distance to the Ontario International Airport. Located closer to MEREDITH INTERNATIONAL CENTRE’s Urban Residential area (Planning Area 4), and potential alignment of the Gold Line LRT corridor, Planning



| PLANNING AREA 3 LAND USE SUMMARY |                      |       |               |                   |                                    |
|----------------------------------|----------------------|-------|---------------|-------------------|------------------------------------|
| Planning Area                    | Land Use Designation | Acres | LODGING ROOMS | RESIDENTIAL UNITS | INDUSTRIAL/COMMERCIAL Max. SQ. FT. |
| 3                                | Urban Commercial     | 25.3  | 400           | --                | 480,000                            |

**NOTES:**  
 Acreages are approximate and subject to survey verification.  
 Future driveway locations shown are conceptual and approximate. Actual location and number of driveways may vary.

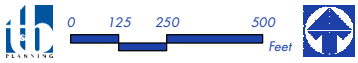



FIGURE 2-5

PLANNING AREA 3

**B.5. Planning Area 4 – Urban Residential**

|                   |   |
|-------------------|---|
| <b>VISION</b>     |    |
| <b>STATISTICS</b> | <p>LAND USE CATEGORY: Urban Residential</p> <p>ACREAGE: 21.4 acres</p> <p>BUILDING INTENSITY: 800 dwelling units</p> <p>MINIMUM DENSITY: 25 dwelling units per acre</p> |

Planning Area 4 is MEREDITH INTERNATIONAL CENTRE’s Urban Residential planning area. Planning Area 4 comprises 21.4 acres of land located in the southeastern portion of the Specific Plan area. As shown on Figure 2-6, *Planning Area 4*, this area is bordered on the north by San Bernardino County Flood Control District (SBFCD) facilities, on the south by Inland Empire Boulevard, on the west by the Deer Creek Channel, and on the east by Planning Area 5.

The Urban Residential designation of Planning Area 4 allows for high-density residential land uses (for-sale or for-rent multi-family residential units) within walking distance to a variety of shopping and employment opportunities, Cucamonga-Guasti Regional Park, and the potential Gold Line LRT corridor. The presence of residential uses contributes to the mixed-use lifestyle of MEREDITH INTERNATIONAL CENTRE and surrounding area.

Planning Area 4 is envisioned to include on-site amenities such as the integration of a private recreation facility, pedestrian connections, and ancillary uses such as carports, garages, and leasing

offices serving the residential development. A landscaped area is designed to occur inside the western boundary of Planning Area 4 to provide a transition treatment between the residential units and the retail commercial uses in adjacent Planning Area 5.

Vehicular access to Planning Area 4 is provided exclusively by driveway connections to Inland Empire Boulevard.

| PLANNING AREA 4 LAND USE SUMMARY |                      |       |               |                   |                                   |
|----------------------------------|----------------------|-------|---------------|-------------------|-----------------------------------|
| Planning Area                    | Land Use Designation | Acres | LODGING ROOMS | RESIDENTIAL UNITS | INDUSTRIAL/COMMERCIAL Max. SQ FT. |
| 4                                | Urban Residential    | 21.4  | --            | 800               | --                                |

**NOTES:**  
 Acreages are approximate and subject to survey verification.  
 Future driveway locations shown are conceptual and approximate. Actual location and number of driveways may vary.

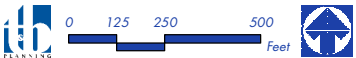
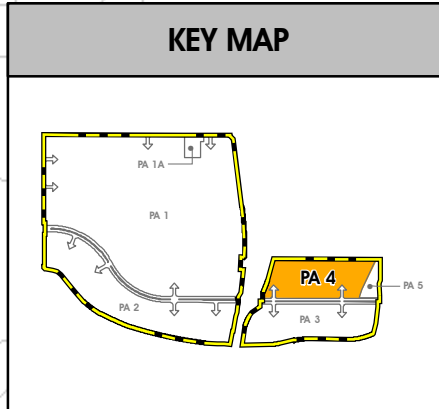
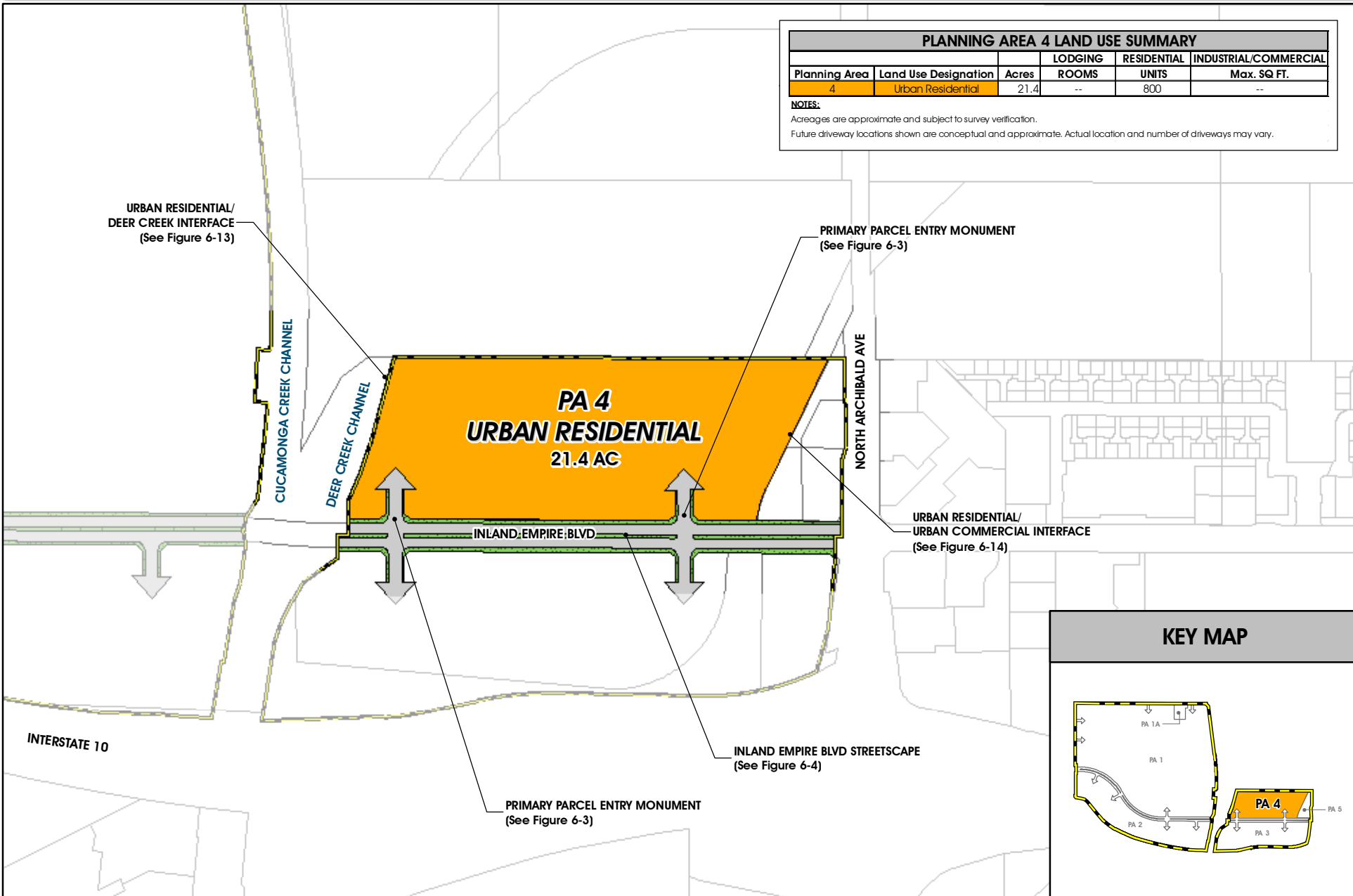



FIGURE 2-6

PLANNING AREA 4

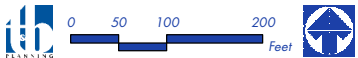
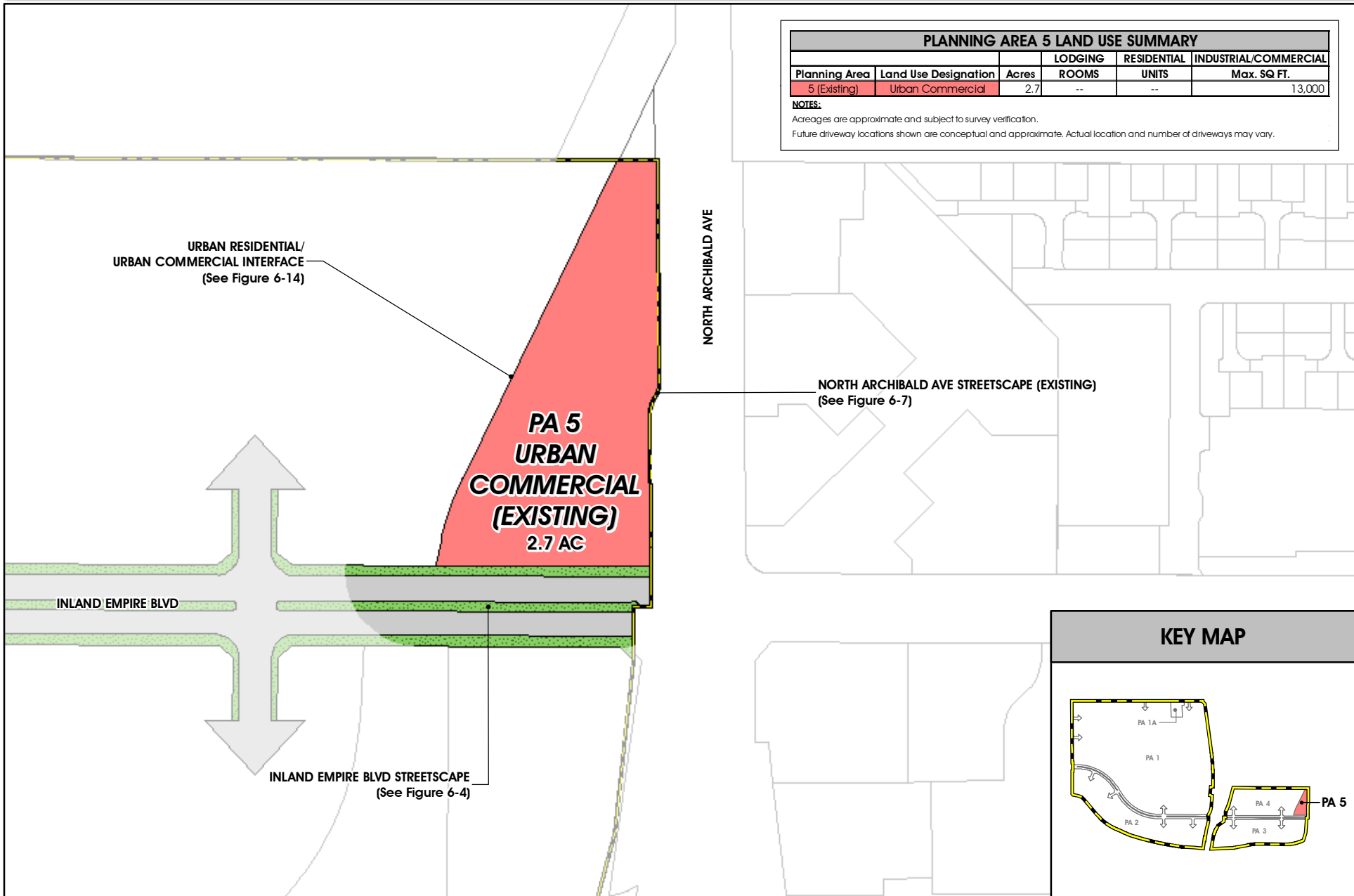


**B.6. Planning Area 5 – Urban Commercial**

|                   |   |
|-------------------|---|
| <b>VISION</b>     |    |
| <b>STATISTICS</b> | <p>LAND USE CATEGORY: Urban Commercial</p> <p>ACREAGE: 2.7 acres</p> <p>BUILDING INTENSITY: 13,000 s.f. (approximately)</p> |

Planning Area 5 is situated on 2.7 acres of land in the eastern portion of the Specific Plan area, as shown on Figure 2-7, *Planning Area 5*. Planning Area 5 is bordered on the north by San Bernardino County Flood Control District (SBFCD) facilities, on the south by Inland Empire Boulevard, on the east by North Archibald Avenue, and on the west by Planning Area 4. Refer to Figure 2-7, *Planning Area 5*.

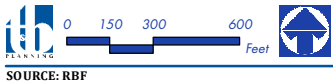
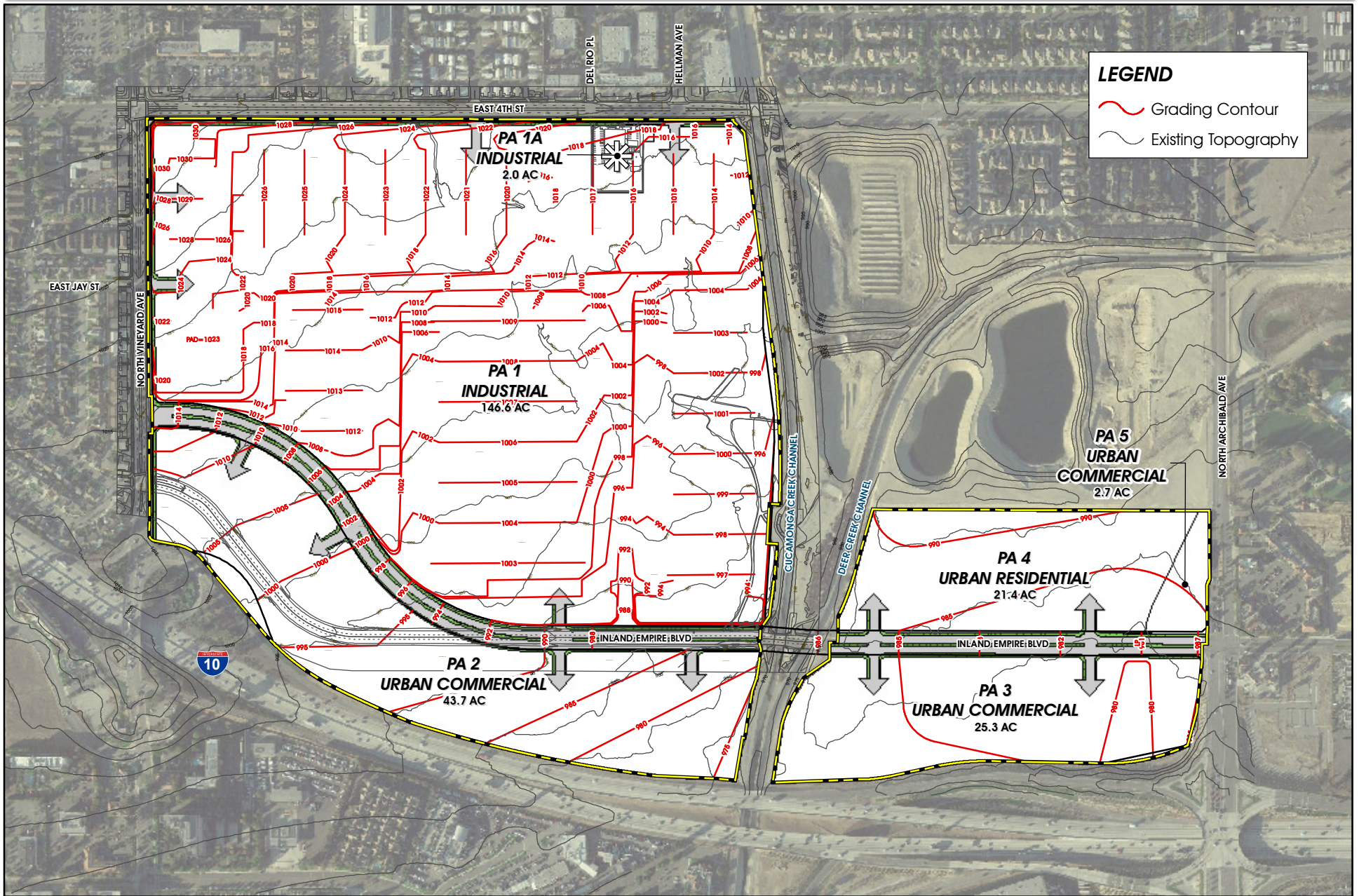
At the time Amendment No. 1 to this Specific Plan was prepared in 2015, Planning Area 5 was fully developed with approximately 13,000 s.f. of Urban Commercial uses; specifically, retail commercial uses and a gas station. This Specific Plan allows for the continuation of these commercial uses and future redevelopment of Planning Area 5 consistent with the standards set forth in Section 5, *Development Standards*.



## C. CONCEPTUAL GRADING PLAN

The natural topography of the MEREDITH INTERNATIONAL CENTRE property is relatively flat. No unusual grading conditions are present and substantial import or export of earth materials is not expected. The primary objectives of the grading plan are to: provide stable development pads for construction; balance the cut and fill grading quantities on-site; and meet City of Ontario building standards and acceptable infrastructure gradient requirements. Approximately 400,000 cubic yards (c.y.) of cut and fill plus additional remedial grading as required by a geotechnical engineer is anticipated. The grading plan is shown in Figure 2-8, *Conceptual Grading Plan*.

# MEREDITH INTERNATIONAL CENTRE



SOURCE: RBF

## D. LANDSCAPING AND GREEN SPACE PLAN

MEREDITH INTERNATIONAL CENTRE’s landscaping and green space plan serves the dual purpose of adding visual appeal while being sensitive to the environment and Southern California climate by using low water use materials. Refer to Section 6, *Design Guidelines*, for detailed information on landscape design.

Landscaping occurs throughout the CENTRE, but most prominently at street corners and along roadways. Street corners include landscaping and identification monuments. Streetscape landscaping presents a combination of evergreen and deciduous trees, low shrubs, and masses of groundcovers to create a visually pleasing experience for pedestrians and passing motorists. Inland Empire Boulevard, North Vineyard Avenue, and East 4th Street also feature a raised, planted median with evenly spaced trees.

An enhanced landscape zone occurs along the CENTRE’s frontage with East 4th Street. This spacious and inviting area features densely planted landscaping and a meandering decomposed granite pathway.

The southern boundary of the MEREDITH INTERNATIONAL CENTRE (Planning Areas 2 and 3) abuts Interstate 10. At this interface, layers of large canopied evergreen shade trees and shrub masses provide a green buffer between Urban Commercial development and the freeway.



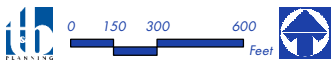
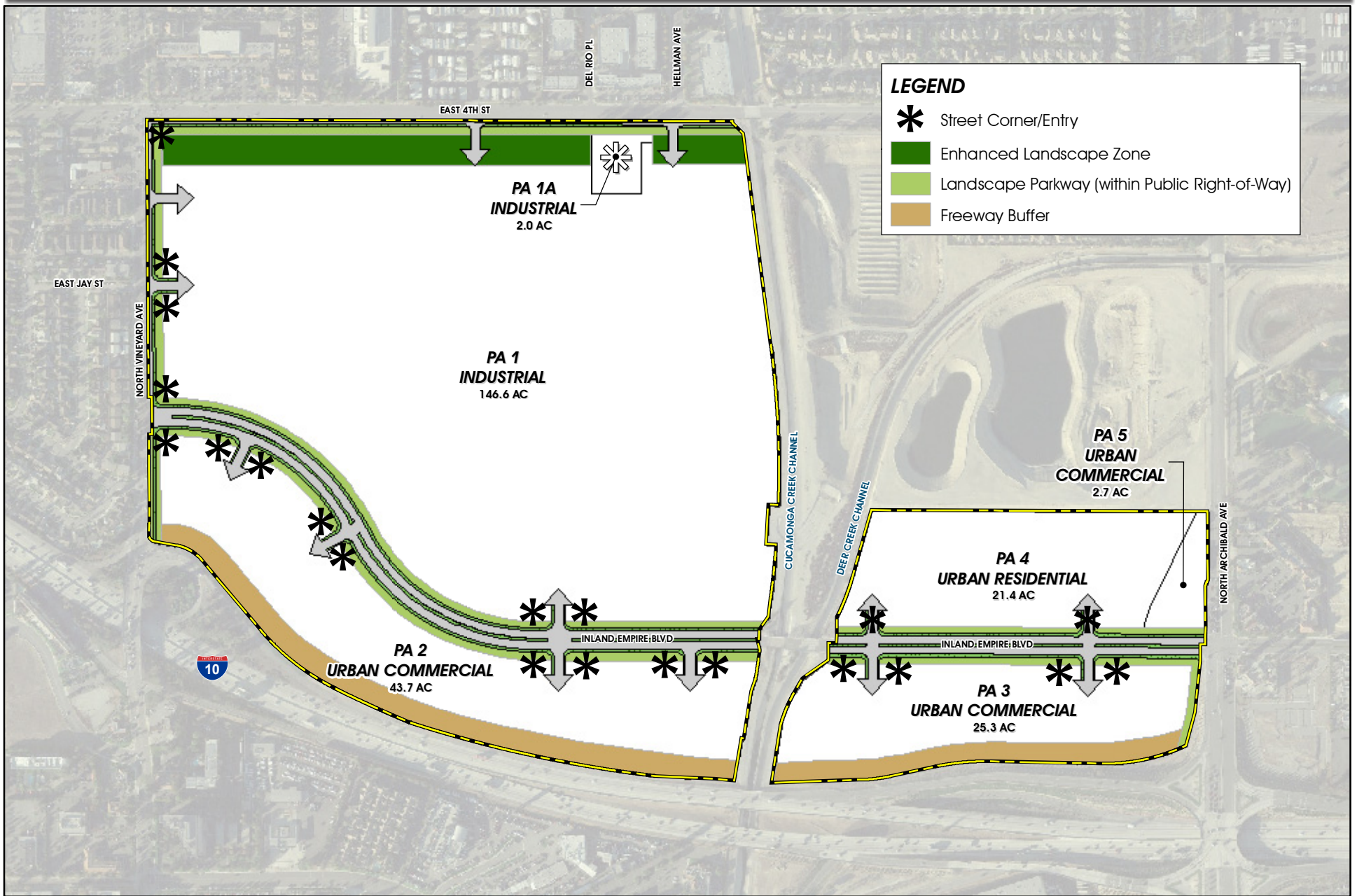


FIGURE 2-9

LANDSCAPING AND GREEN SPACE PLAN

# Circulation Plan

## Section 3

## CIRCULATION PLAN

The MEREDITH INTERNATIONAL CENTRE *Circulation Plan* ensures efficient and safe access for residents, visitors and employees to, from, and through, the Specific Plan’s six planning areas. It also provides an access plan for the transportation of goods to and from the businesses located in the CENTRE.

The main objective of the *Circulation Plan* is to provide direct, safe, and convenient vehicular and non-vehicular access. The design of the *Circulation Plan* creates functional efficiency for drivers, pedestrians, and bicyclists, enhances the aesthetics of the roadway network, and facilitates the movement of employees, visitors, shoppers, customers, and residents. Elements of the Specific Plan’s vehicular and non-vehicular circulation system are discussed below.

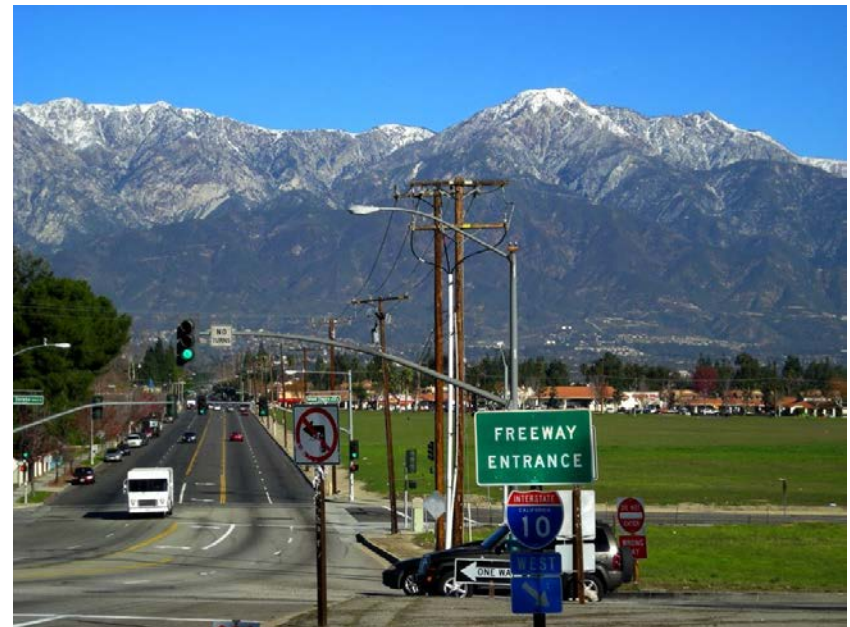
### A. VEHICULAR CIRCULATION PLAN

The circulation system depicted on Figure 3-1, *Conceptual Vehicular Circulation and Mobility Plan*, provides a vehicular traffic design that meets the needs of MEREDITH INTERNATIONAL CENTRE and accommodates through-traffic anticipated by the City of Ontario Policy Plan.

The Specific Plan property is served by a well-developed local roadway system consisting of North Vineyard Avenue to the west, North Archibald Avenue to the east, and East 4th Street to the north. Concurrent with the development of MEREDITH INTERNATIONAL CENTRE, improvements to North Vineyard Avenue and East 4th Street will result in completed street sections that are consistent with the City of Ontario’s *Master Plan of Streets and Highways*. A

description of the roadway improvements designed to occur concurrent with the development of MEREDITH INTERNATIONAL CENTRE are described on the following pages.

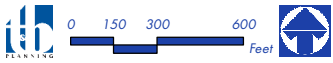
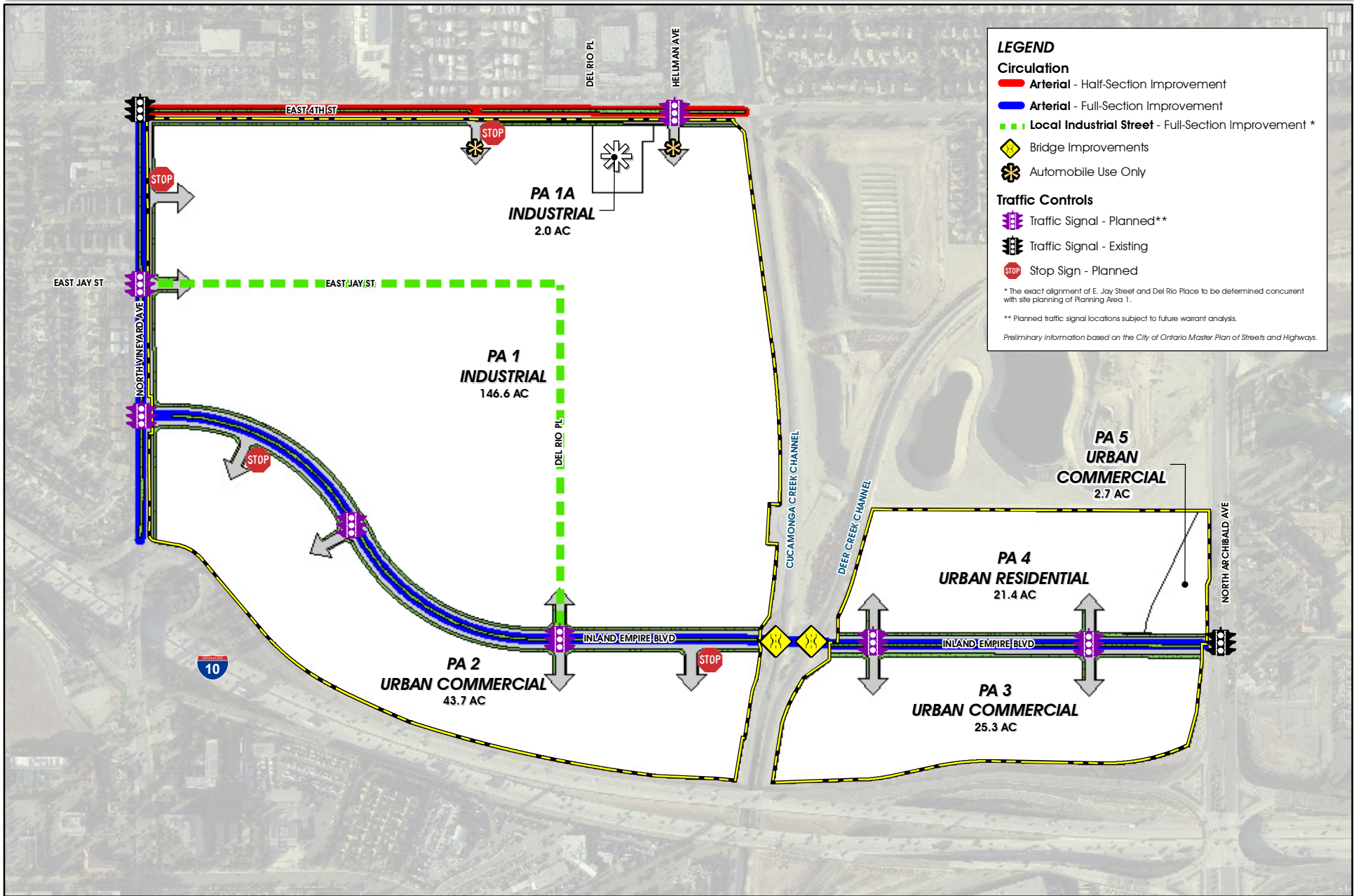
Interstate 10 is located immediately south of the Specific Plan property and forms the southern boundary of Planning Area 2 and Planning Area 3. Interstate 10 is under the authority of CalTrans. Because the freeway falls completely outside of the Specific Plan boundaries, development of MEREDITH INTERNATIONAL CENTRE does not affect CalTrans’ plans for the operation and maintenance of Interstate 10. Drivers traveling between the Specific Plan area and the freeway system have convenient access via the on- and off-ramps at North Vineyard Avenue and North Archibald Avenue.



MEREDITH INTERNATIONAL CENTRE property, 2014, as seen from the North Vineyard Avenue/Interstate 10 interchange. Source: GoogleEarth digitaljoe.



# MEREDITH INTERNATIONAL CENTRE

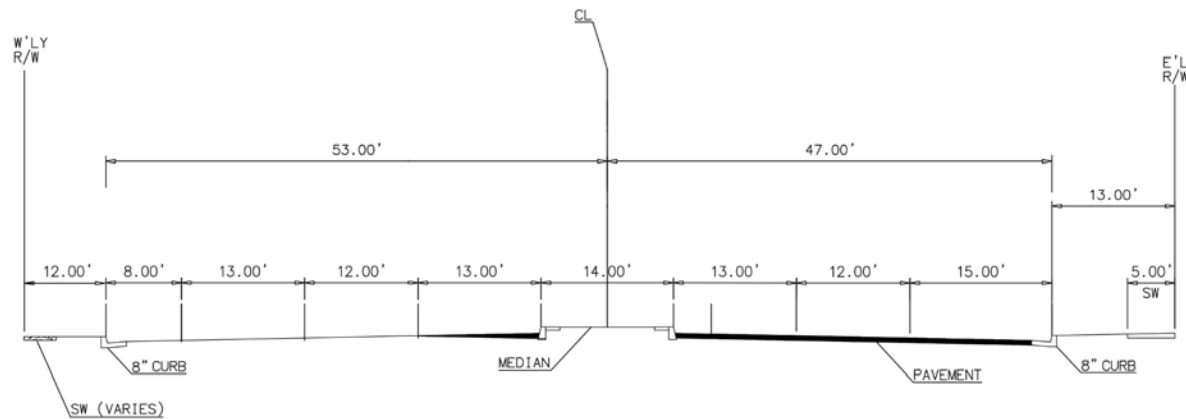


**FIGURE 3-1**  
**CONCEPTUAL VEHICULAR CIRCULATION AND MOBILITY PLAN**

**a. North Vineyard Avenue – Principal Arterial**

North Vineyard Avenue forms the western boundary of Planning Area 1 and Planning Area 2. This roadway provides access to the Specific Plan area from the west via Inland Empire Boulevard and East Jay Street. Driveway access into Planning Area 2 directly from North Vineyard Avenue is not permitted.

The cross-section of North Vineyard Avenue is shown below. As shown, the segment of North Vineyard Avenue abutting the Specific Plan area features a 113-foot wide right-of-way (ROW), including 76 feet of pavement, a 14-foot wide raised median, curb and gutter improvements, and landscaped parkways on both sides of the street. On the east side of the street, the parkway includes an eight-foot (8') wide park strip adjacent to the curb and a five-foot (5') wide sidewalk. The 12-foot wide parkway on the west side of the street was existing as of the date of the writing of this 2015 Specific Plan Amendment and features a curb-adjacent parkway strip of varying width and a sidewalk of varying width.

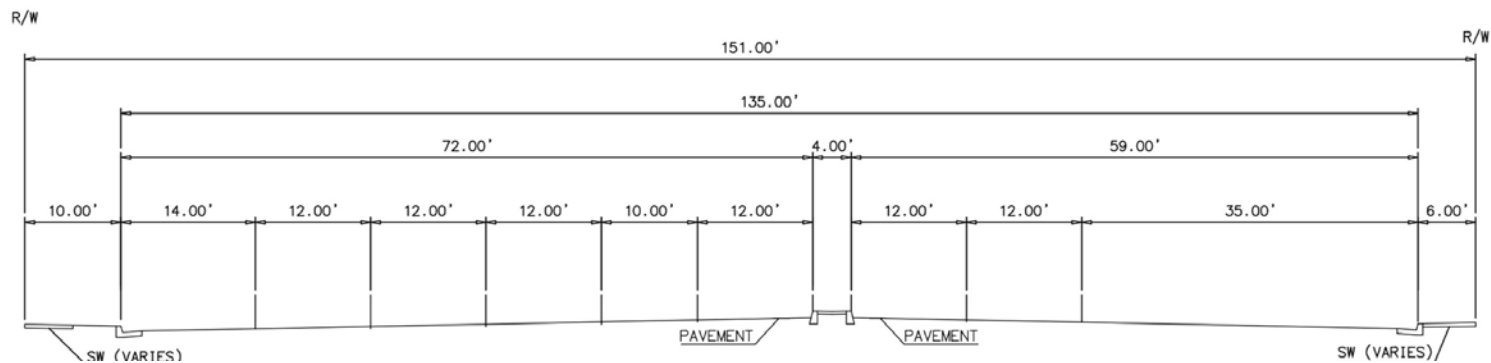


**North Vineyard Avenue Cross-Section**

**b. North Archibald Avenue**

North Archibald Avenue forms the eastern boundary of Planning Area 3 and Planning Area 5 and provides access to the Specific Plan area from the east. Driveway access into Planning Area 3 directly from North Archibald Avenue is not permitted. The two driveways into Planning Area 5 that were in place at the time the 2015 Amendment to this Specific Plan was prepared are expected to remain.

The cross-section of North Archibald Avenue is shown below. As shown, the segment of North Archibald Avenue adjacent to the Specific Plan area features a 151-foot wide ROW. Within the ROW, there is 131 feet of pavement, a four-foot (4') wide raised center median, curb and gutter improvements, and parkways on both sides of the street. On the east side of the street, the parkway includes a six-foot (6') wide sidewalk, while the parkway on the west side of the street features a curb-adjacent parkway of varying width and a sidewalk of varying width. Improvements on North Archibald Avenue that were in place at the time the 2015 Amendment to this Specific Plan was prepared are expected to remain in place. MEREDITH INTERNATIONAL CENTRE adds minor landscape and hardscape treatments along the western side of the right-of-way abutting Planning Area 3.

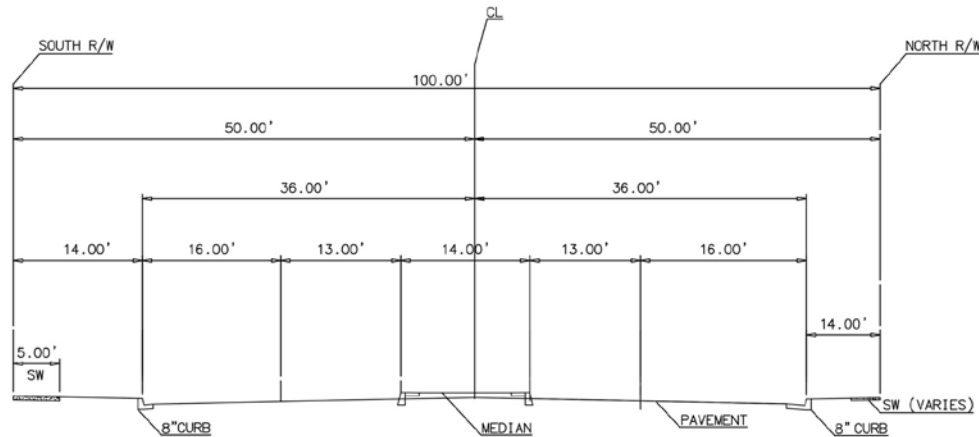


**North Archibald Avenue Cross-Section**

**c. East 4th Street – Principal Arterial**

East 4th Street forms the northern boundary of Planning Area 1 and Planning Area 1A and provides access to MEREDITH INTERNATIONAL CENTRE from the north. Driveways are permitted to access Planning Area 1 and Planning Area 1A from East 4th Street and will primarily be used for passenger vehicles. Driveways accommodating truck traffic will not directly connect to East 4th Street (trucks will access Planning Areas 1 and 1A from driveways along East Jay Street/Del Rio Place).

The cross-section of East 4th Street is shown below. This segment of East 4th Street features 58 feet of pavement, a 14-foot wide raised center median, 14-foot wide landscaped parkways on both sides of the street, and curb and gutter improvements within a 100-foot wide ROW. On the south side of the street, the landscaped parkway includes an eight-foot (8') wide curb adjacent park strip and a five-foot (5') wide sidewalk. The parkway on the north side of the street was existing as of the date of the writing of this 2015 Specific Plan Amendment and features a curb-adjacent park strip of varying width and a sidewalk of varying width.



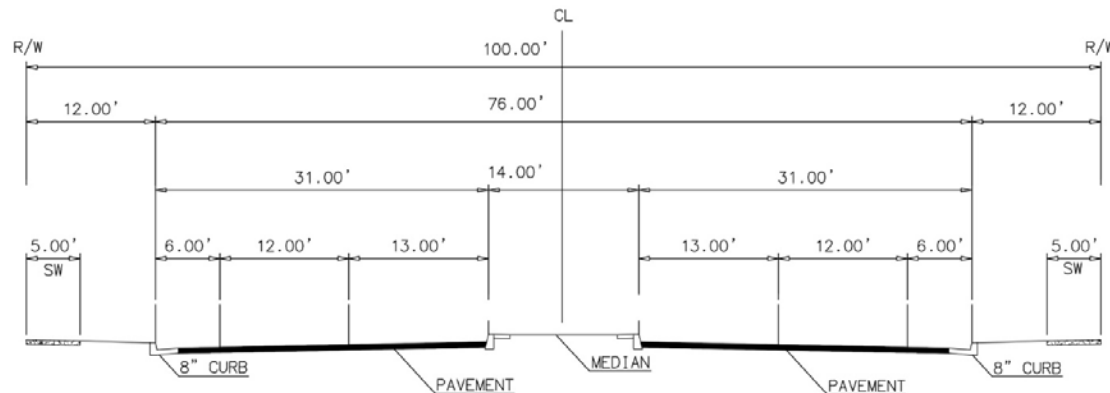
**East 4th Street Cross-Section**

**d. Inland Empire Boulevard – Principal Arterial**

Major east-west circulation within MEREDITH INTERNATIONAL CENTRE is provided via Inland Empire Boulevard, which also serves as a primary connector between North Vineyard Avenue and North Archibald Avenue north of Interstate 10. At the time the 2015 Amendment to this Specific Plan was prepared, Inland Empire Boulevard traversed through the southern portion of the Specific Plan property as a four-lane road segment. As part of MEREDITH INTERNATIONAL CENTRE’s development, the road was planned to be widened and its alignment shifted northerly west of the Cucamonga Creek Channel, to allow for its intersection with North Vineyard Avenue to be adequately spaced from the North Vineyard Avenue/Interstate 10 interchange. A traffic signal is provided at the relocated Inland Empire Avenue/North Vineyard Avenue intersection. The traffic signal located at the Inland Empire Boulevard/North Archibald Avenue intersection remains in place. Vehicular access to Planning Area 1 is provided from Del Rio Place. Vehicular access to Planning Area 2, Planning Area 3, and Planning Area 4 is provided via driveways connecting to Inland Empire Boulevard.

Outside of the Specific Plan boundary, improvements to Inland Empire Boulevard across the Cucamonga Creek and Deer Creek Channels include potential widening of the bridge spans. The bridge span widening may be needed to accommodate road improvements on Inland Empire Boulevard associated with total buildout of MEREDITH INTERNATIONAL CENTRE.

The typical cross-section of Inland Empire Avenue is shown below. As shown, Inland Empire Boulevard features a 100-foot wide ROW that includes 62 feet of pavement, a 14-foot wide raised center median, 12-foot wide parkways on both sides of the street, and curb and gutter improvements. The parkways feature a seven-foot (7’) wide, curb-adjacent landscaped park strip and a five-foot (5’) wide sidewalk. At the bridge spans, Inland Empire Boulevard tapers to a 78-foot ROW width, with 62 feet of pavement, a four-foot (4’) wide painted center median, six-foot (6’) wide sidewalks on both sides of the street, and curb and gutter improvements.

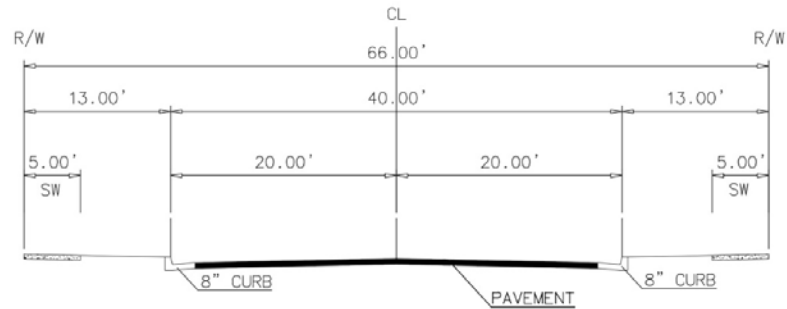


**Inland Empire Blvd Cross-Section**

**e. Del Rio Place and East Jay Street – Industrial Streets**

Two local industrial streets, Del Rio Place and East Jay Street, are provided in Planning Area 1. East Jay Street, an east-west oriented street, provides access from the west via a traffic signal at its intersection with North Vineyard Avenue. Del Rio Place, a north-south oriented street, provides access from the south to Planning Area 1 from Inland Empire Boulevard. A traffic signal is located at the intersection of Inland Empire Boulevard and Del Rio Place. Jay Street connects with Del Rio Place near the eastern portion of Planning Area 1.

As illustrated below, Del Rio Place and East Jay Street feature a 66-foot wide ROWs that include 40 feet of pavement, 13-foot wide parkways on both sides of the street, and curb and gutter improvements. The parkways include an eight-foot (8') wide, curb-adjacent landscaped parkway strip and a five-foot (5') wide sidewalk.



**Del Rio Place and Jay Street Cross-Section**

## **B. NON-VEHICULAR CIRCULATION PLAN**

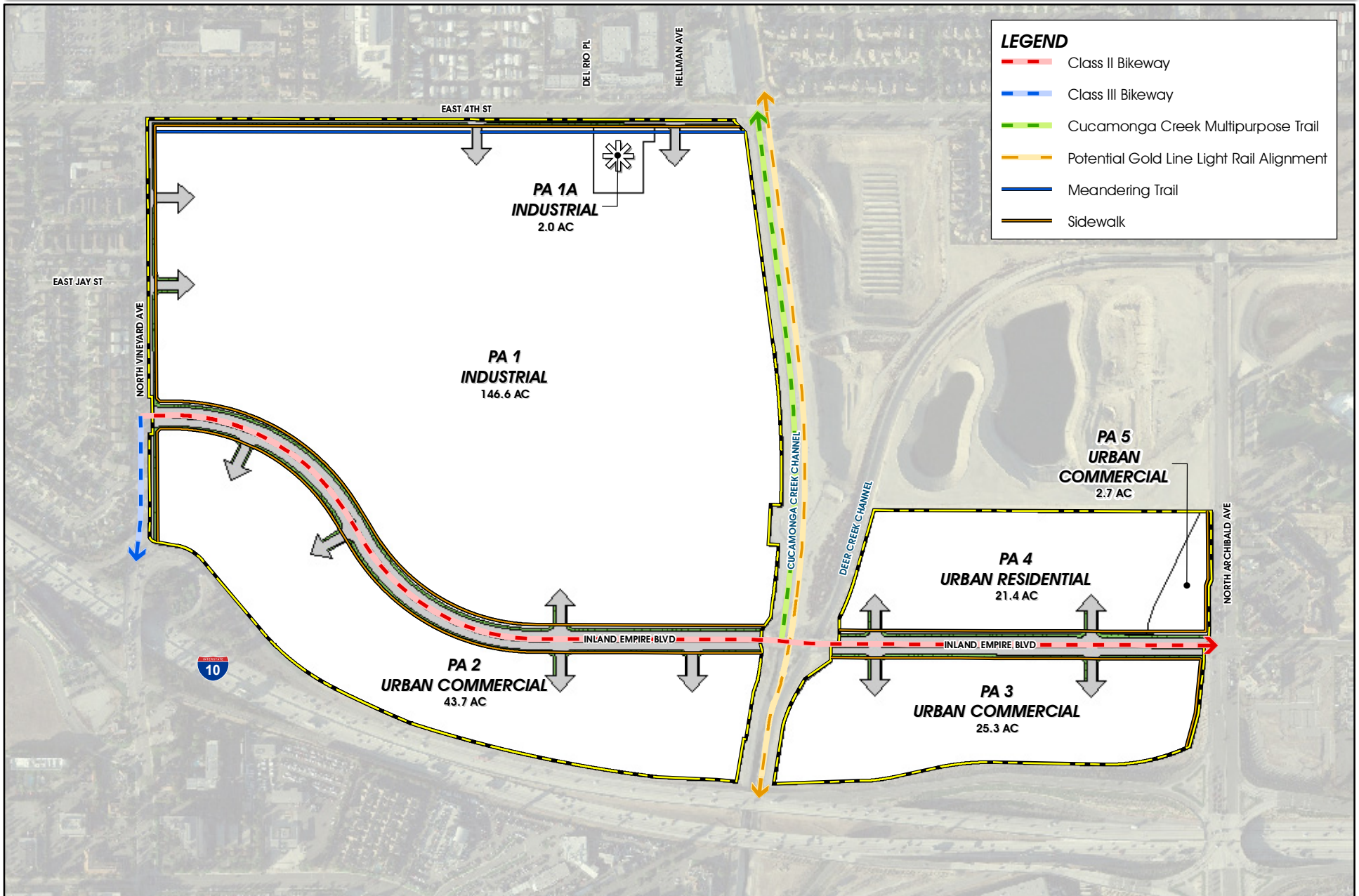
A network of sidewalks, walkways, and bikeways is provided within MEREDITH INTERNATIONAL CENTRE as shown in Figure 3-2, *Conceptual Non-Vehicular Circulation and Mobility Plan*. Given the mixture of land uses and the volume of passenger car and truck traffic circulating on the site, pedestrian and bicycle safety was given careful consideration when preparing the design standards for MEREDITH INTERNATIONAL CENTRE.

The non-vehicular circulation plan promotes pedestrian movement, bicycle use, and encourages the use of available mass transit opportunities. To overcome the challenge of designing a safe non-vehicular mobility plan in an area with high volumes of vehicular traffic, pedestrian and vehicular traffic are separated where it is feasible to provide such separation.

This Specific Plan's *Non-Vehicular Circulation and Mobility Plan* is designed to encourage the use of non-vehicular modes of mobility. Inland Empire Boulevard that traverses the Specific Plan property in an east to west direction is a designated Class II Bikeway Corridor and the Cucamonga Creek Multipurpose Trail is located between Planning Area 1 and Planning Area 4.

The Gold Line Foothill Construction Authority is studying the extension of a light rail transit (LRT) line to Ontario International Airport, which is tentatively envisioned to traverse along the east side of the Cucamonga Creek Channel immediately west of Planning Area 3 and Planning Area 4. This Specific Plan acknowledges the potential off-site LRT alignment and anticipates its use by employees, visitors, and residents of MEREDITH INTERNATIONAL CENTRE.

# MEREDITH INTERNATIONAL CENTRE



**FIGURE 3-2**  
**CONCEPTUAL NON-VEHICULAR CIRCULATION AND MOBILITY PLAN**



# Utility Infrastructure Plan

## Section 4

## UTILITY INFRASTRUCTURE PLAN

Buildout of MEREDITH INTERNATIONAL CENTRE requires the installation of water, sewer, drainage, and other utility infrastructure, as described in this section

### A. DOMESTIC WATER

Ontario Municipal Utilities Company provides domestic water service to the Specific Plan area as part of its service to Zone 1212. As shown on Figure 4-1, *Water Infrastructure Plan*, the MEREDITH INTERNATIONAL CENTRE property is served by water lines installed beneath North Vineyard Avenue, East 4th Street, and Inland Empire Boulevard.

Concurrent with the realignment of Inland Empire Boulevard through the Specific Plan area between the Cucamonga Creek Channel and East Vineyard Avenue, an 18-inch water line is planned to be installed beneath the newly aligned segment of Inland Empire Boulevard. In addition, approximately 400 feet of 6-inch diameter water line installed beneath North Vineyard Avenue at the time the 2015 Specific Plan Amendment was prepared is planned to be upsized to a 12-inch diameter line in conjunction with buildout of the Specific Plan and relocation of the Inland Empire Boulevard/North Vineyard Avenue intersection. New 12-inch diameter water lines are planned beneath the alignments of Del Rio Place and East Jay Street, which provide service to the industrial land uses within Planning Area 1. Due to locally high water pressure, pressure reducers may be required to service buildings in the Specific Plan area.

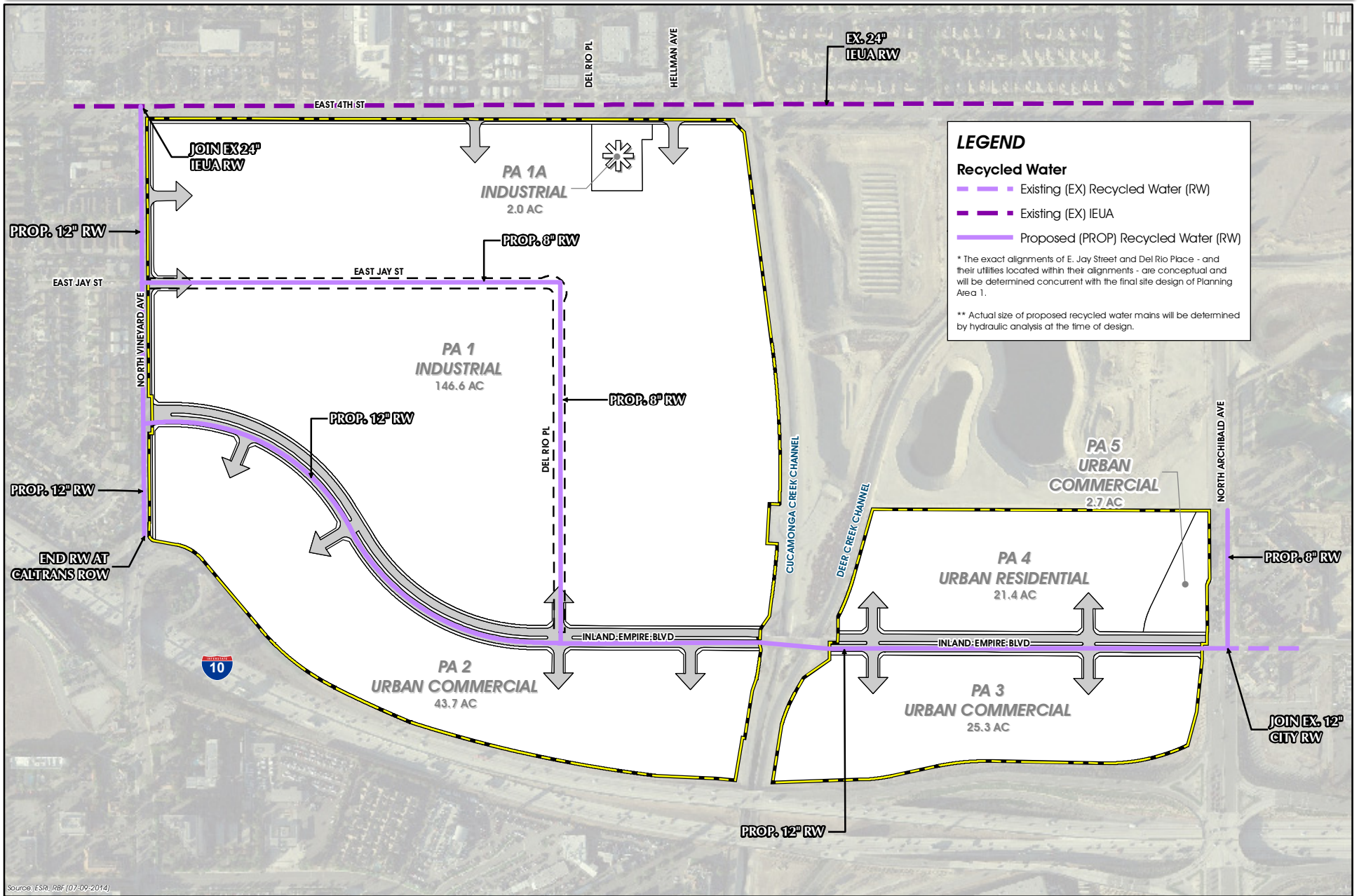
According to a water supply assessment prepared by Ontario Municipal Utilities, the annual domestic water demand of MEREDITH INTERNATIONAL CENTRE is approximately 775 acre-feet.

### B. RECYCLED WATER

Ontario Municipal Utilities supplies recycled water to the Specific Plan area as part of its service to Zone 1270. As shown on Figure 4-2, *Recycled Water Infrastructure Plan*, MEREDITH INTERNATIONAL CENTRE is served by a 24-inch diameter recycled water line installed beneath East 4th Street and a 12-inch diameter line installed beneath Inland Empire Boulevard, east of North Archibald Avenue. An expansion of the recycled water system is planned to occur in association with the development of MEREDITH INTERNATIONAL CENTRE. The conceptual recycled water plan for MEREDITH INTERNATIONAL CENTRE provides for the following improvements: a 12-inch diameter line installed beneath North Vineyard Avenue between East 4th Street and the southwest corner of the Specific Plan area (terminating at the Interstate 10 right-of-way); a 12-inch diameter line installed beneath Inland Empire Boulevard beginning at North Vineyard Avenue and extending east to join the existing recycled water line east of North Archibald Avenue; and an 8-inch diameter line installed beneath North Archibald Avenue beginning at Inland Empire Boulevard and extending approximately 650 feet north. MEREDITH INTERNATIONAL CENTRE also provides 8-inch diameter recycled water lines beneath the alignments of Del Rio Place and East Jay Street to serve industrial land uses within Planning Area 1. Installation of recycled water infrastructure is designed to occur concurrent with the associated roadway improvements. Due to locally high water pressure, pressure reducers may be required to service the Specific Plan area.



# MEREDITH INTERNATIONAL CENTRE



Source: ESRI, RBF (07-09-2014)

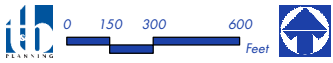


FIGURE 4-2

## RECYCLED WATER INFRASTRUCTURE PLAN

## C. SANITARY SEWER

The Specific Plan area is served by Ontario Municipal Utilities Company, which conveys wastewater to the Inland Empire Utility Agency (IEUA) for transmission to treatment facilities. As shown on Figure 4-3, *Sanitary Sewer Infrastructure Plan*, there is a 12-inch diameter pipe installed beneath North Vineyard Avenue, and an 8-inch diameter pipe installed beneath North Archibald Avenue. In addition, an existing 15-inch diameter pipe traverses Planning Area 3 approximately 300 feet west of North Archibald Avenue; this line conveys sewer flows from the 8-inch line beneath Inland Empire Boulevard south across Interstate 10 to treatment facilities.

IEUA system pipes are installed beneath Inland Empire Boulevard (24-inch diameter), and North Archibald Avenue (24- and 30-inch diameters), as illustrated on Figure 4-2. The IEUA sewer line beneath Inland Empire Boulevard is not available for domestic flows and, therefore, not available to service MEREDITH INTERNATIONAL CENTRE.

The sanitary sewer system for MEREDITH INTERNATIONAL CENTRE is designed to utilize the 15-inch diameter pipe installed within Planning Area 3 as the discharge point for flows produced within the Specific Plan area. To make a connection to this line, a pipe that transitions from an 8-inch to 15-inch diameter is planned beneath Inland Empire Boulevard from just east of North Vineyard Avenue to just west of North Archibald Avenue. Planned 8-inch diameter lines beneath the alignments of East Jay Street and Del Rio Place will convey flows from Planning Area 1 and connect to the planned line beneath Inland Empire Boulevard.

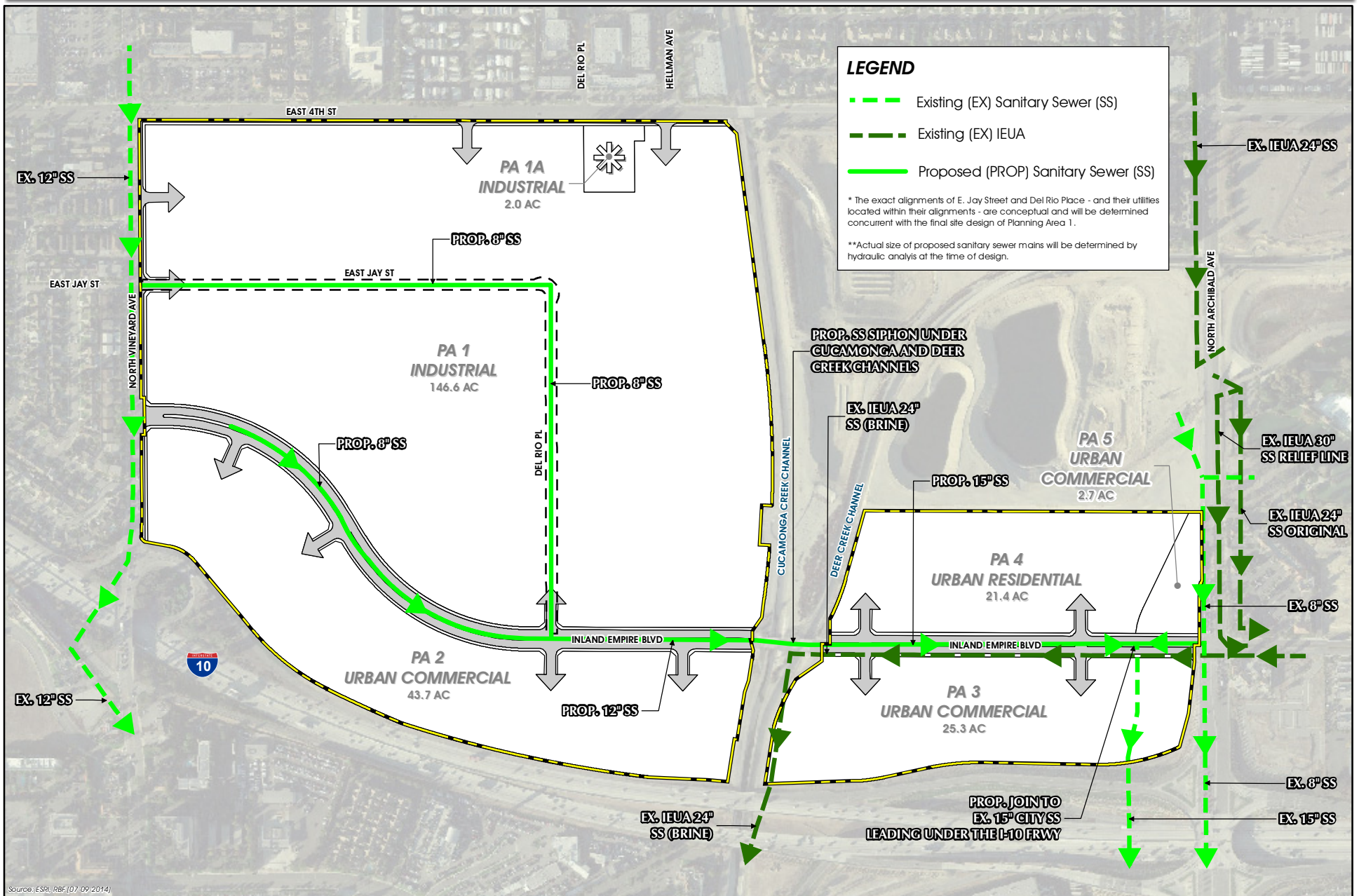
An 8-inch diameter siphon may be required under the Cucamonga Creek Channel and Deer Creek Channel. Additional off-site improvements to the City's sanitary sewer system may be necessary downstream of the MEREDITH INTERNATIONAL CENTRE (to be determined by the City of Ontario as part of the final engineering process for implementing development).

Based on sewer demand rates given in the 2012 City of Ontario Sewer Master Plan, MEREDITH INTERNATIONAL CENTER sanitary sewer flows are estimated at 481,930 gallons per day (gpd).

## D. CONCEPTUAL STORM DRAIN PLAN

The master storm drain plan for MEREDITH INTERNATIONAL CENTRE is shown on Figure 4-4, *Storm Drain Infrastructure Plan*. Planning Area 1 drains in a southeasterly direction and the drainage ultimately flows into the Cucamonga Creek Channel. Basins for water quality and detention purposes are planned within Planning Area 1 to serve functional drainage purposes and may be supplemented by underground storm water chambers or dry wells if at-grade storm water storage space is limited. Flows discharged from Planning Area 1 will be conveyed east via a new storm drain pipe beneath Inland Empire Boulevard and discharged into the Cucamonga Creek Channel (via a new outlet connection).

Planning Area 2 drains easterly to an existing inlet connection to the Cucamonga Creek Channel adjacent to Interstate 10. No additional storm drain backbone infrastructure is needed to serve Planning Area 2.



Source: ESRI, RBF (07-09-2014)

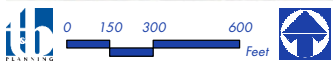
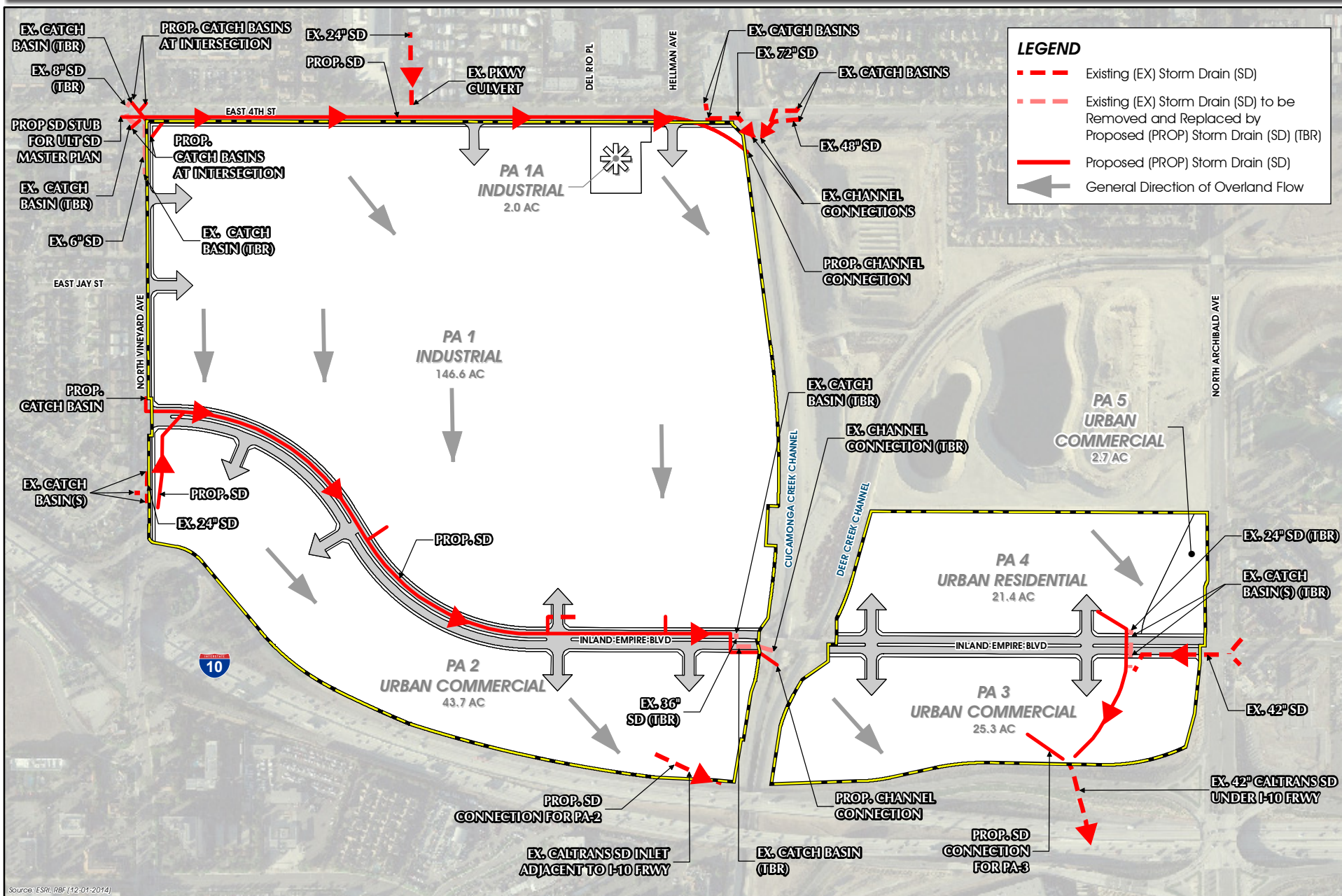


FIGURE 4-3

SANITARY SEWER INFRASTRUCTURE PLAN

# MEREDITH INTERNATIONAL CENTRE



Sources: ESR, RBF (12-01-2014)

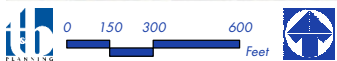


FIGURE 4-4

STORM DRAIN INFRASTRUCTURE PLAN

Planning Areas 3, 4, and 5 drain southerly toward Interstate 10. A new storm drain pipe will convey storm water flows from these areas to an existing culvert that is located south of Planning Area 3 and travels under Interstate 10. A water detention basin is planned in Planning Area 3 and/or Planning Area 4 to detain incremental increases in storm water flows before discharge to the culvert under Interstate 10.

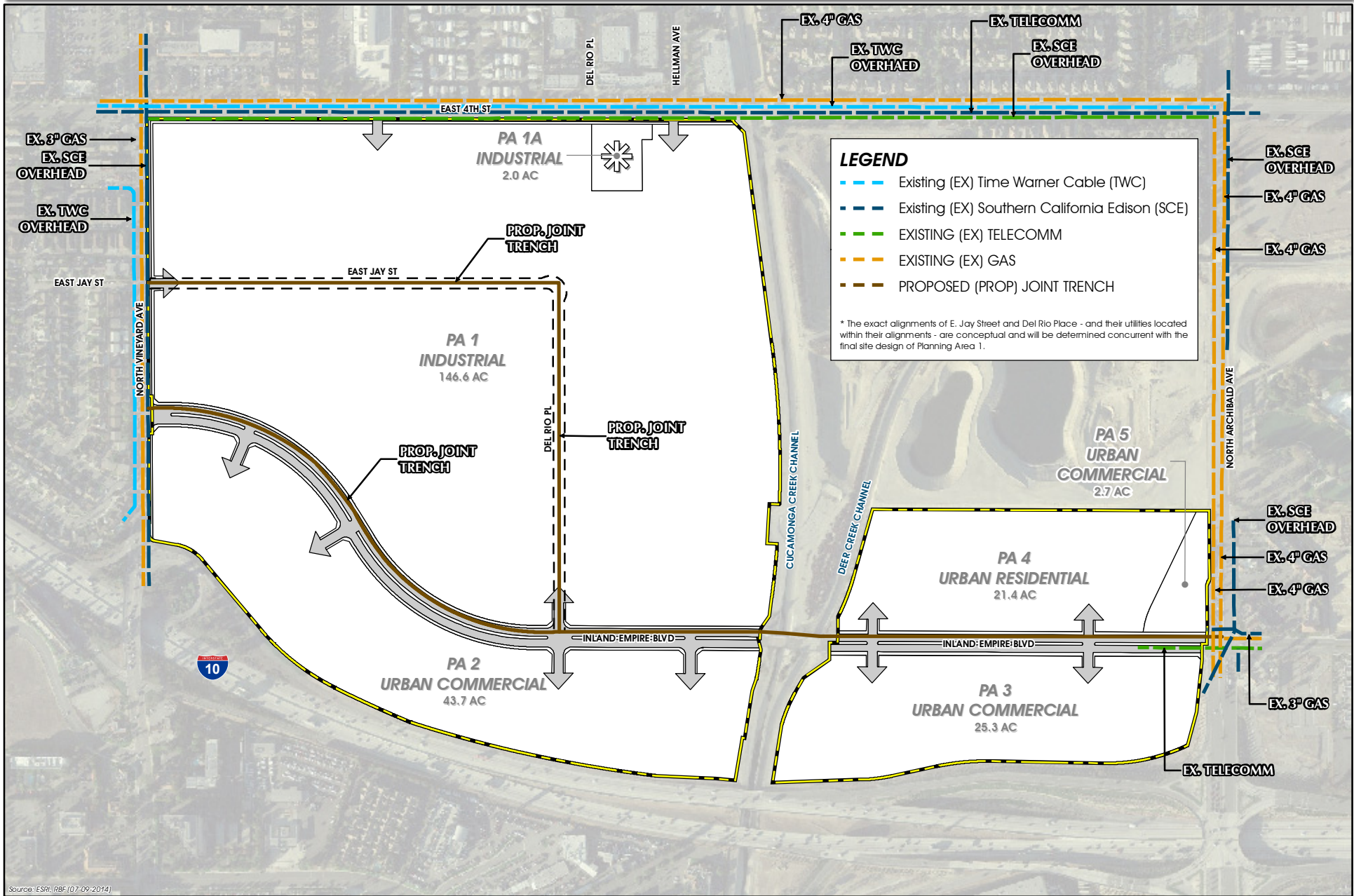
As part of development of MEREDITH INTERNATIONAL CENTRE, new storm drain improvements (e.g., storm drain pipe and catch basins) may be required within East 4th Street to capture storm drain flows that originate north (off-site) of the Specific Plan area. The potential storm drain improvements will convey storm water flows east where they would discharge into the Cucamonga Creek Channel at a new outlet connection in the northeast corner of Planning Area 1. In addition, as part of development of MEREDITH INTERNATIONAL CENTRE, short segments of storm drain and catch basins within Inland Empire Boulevard may be removed.

## **E. DRY UTILITIES**

MEREDITH INTERNATIONAL CENTRE receives electricity service from Southern California Edison (SCE), natural gas service from the Southern California Gas Company, and cable service from Time Warner Cable. As shown on Figure 4-4, *Dry Utility Infrastructure Plan*, electricity, gas, and cable lines are installed in North Vineyard Avenue, East 4th Street, and North Archibald Avenue. New lines are planned to be installed in Inland Empire Boulevard, Jay Street, and Del Rio Place to fully service the Specific Plan area.



# MEREDITH INTERNATIONAL CENTRE



Source: ESRI, RBF (07-09-2014)

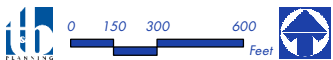


FIGURE 4-5

DRY UTILITY INFRASTRUCTURE PLAN

# Development Standards

## Section 5

## DEVELOPMENT STANDARDS

### A. PURPOSE AND INTENT

This section formally establishes the use restrictions and development standards (zoning) for the MEREDITH INTERNATIONAL CENTRE property. The regulations provided herein work in concert with the architectural and landscape guidelines set forth in Section 6, *Design Guidelines*, of this Specific Plan to achieve the vision of the MEREDITH INTERNATIONAL CENTRE.

### B. DEFINITION OF TERMS

The meaning of words, phrases, titles, and terms shall be the same as provided in the City of Ontario Development Code (hereafter referred to as “Development Code”), unless otherwise specifically defined in this Specific Plan.

### C. APPLICABILITY

The regulations set forth in this section shall apply to all development plans or agreements, tract or parcel maps, site plans, or any other action requiring administrative or discretionary approval within the 257.7-acre MEREDITH INTERNATIONAL CENTRE Specific Plan area. Whenever the development standards contained herein differ from those contained in the Development Code, the provisions of this Specific Plan shall take precedence. Any development standard, condition, or situation not specifically addressed herein shall be subject to the applicable requirements of the Development Code.

### D. PERMITTED, CONDITIONAL, AND ANCILLARY LAND USES

Land within the MEREDITH INTERNATIONAL CENTRE Specific Plan area and structures/facilities thereon may only be developed, divided, and/or used for those activities listed in Table 5-1, *Land Use Matrix*. Table 5-1 lists the permitted, conditional, and ancillary land uses for each land use district established by this Specific Plan (Industrial, Urban Commercial, and Urban Residential). The symbols shown in Table 5-1 have the following meanings:

- “P” means the land use is permitted by right of being in the proper land use district, subject to the development standards applicable to that land use district.
- “C” means the land use is conditionally permitted, subject to the filing of a Conditional Use Permit in accordance with the requirements of the City of Ontario Development Code, and must be approved by the City of Ontario Zoning Administrator or Planning Commission before the use can be established within a particular land use district.
- “A” means the land use is permitted by right of being in the proper land use district, provided the use is ancillary to the primary permitted or conditionally permitted use of the property.
- “-” means the land use is prohibited within the specified land use district.

A use that is not specifically listed in Table 5-1 is deemed a prohibited use unless otherwise allowed pursuant to the applicable interpretations and determinations established by the Development Code.

Table 5-1 Land Use Matrix

| Use Legend: P: Permitted Use C: Conditional Use A: Ancillary Use --: Prohibited                                |                   |                  |                   |   |
|--|-------------------|------------------|-------------------|---|
| Use  | Land Use District |                  |                   | Additional Notes  |
|  | Industrial        | Urban Commercial | Urban Residential |   |
| <b>HIGH-CUBE WAREHOUSE</b>   |                   |                  |                   |   |
| Data processing/storage services   | P                 | --               | --                |   |
| Distribution center  | P                 | --               | --                |   |
| Industrial retail sales (sales of goods and/or product either manufactured, warehoused, or wholesaled on-site) | P                 | --               | --                | Maximum 25% of floor area or 15,000 square feet per building, whichever is less       |
| Motor vehicle storage  | P                 | --               | --                | Indoor storage only   |
| Motor vehicle washing, cleaning, and detailing   | A                 | --               | --                | Associated with routine operation of permitted use only                               |
| Non-store retailers  | P                 | --               | --                | Includes electronic shopping (e-fulfillment/e-commerce) centers and mail-order houses |
| Offices  | A                 | --               | --                |   |
| Warehousing and storage (indoor)   | P                 | --               | --                | Including food cold storage   |
| Warehousing and storage (outdoor)  | P                 | --               | --                |   |
| Wholesale trade  | P                 | --               | --                | Within a wholly enclosed building only  |
| <b>INSTITUTIONAL USES</b>  |                   |                  |                   |   |
| Public Schools - Grades K through 12   | P                 | --               | --                | Planning Area 1A only   |
| Water systems (water wells, water treatment and filtration facilities)   | P                 | --               | --                | Planning Area 1A only   |
| <b>LIGHT INDUSTRIAL</b>  |                   |                  |                   |   |
| Catering establishments  | P                 | --               | --                |   |
| Charitable distribution facilities (e.g. food banks)   | C                 | --               | --                |   |
| Charitable, philanthropic, service and other non-profit organization offices                                   | A                 | --               | --                | Associated with charitable distribution facilities only                               |
| Computer, home electronics and small home appliance repair   | P                 | --               | --                |   |
| Data processing/storage services   | P                 | --               | --                |   |
| Distribution center  | P                 | --               | --                |   |
| Furniture refinishing  | P                 | --               | --                |   |

Table 5-1 Land Use Matrix

| Use Legend: P: Permitted Use C: Conditional Use A: Ancillary Use --: Prohibited                                |                   |                  |                   |   |
|--|-------------------|------------------|-------------------|---|
| Use  | Land Use District |                  |                   | Additional Notes  |
|  | Industrial        | Urban Commercial | Urban Residential |   |
| <b>LIGHT INDUSTRIAL (CONT.)</b>  |                   |                  |                   |   |
| Furniture re-upholstery  | P                 | --               | --                |   |
| General Light Manufacturing  | P                 |                  |                   |   |
| Industrial retail sales (sales of goods and/or product either manufactured, warehoused, or wholesaled on-site) | P                 | --               | --                | Maximum 25% of floor area or 15,000 square feet per building, whichever is less |
| Laundries – commercial   | P                 | --               | --                |   |
| Lawnmower and garden equipment repair  | P                 | --               | --                |   |
| Motor vehicle storage  | P                 | --               | --                |   |
| Motor vehicle washing, cleaning, and detailing   | A                 | --               | --                | Associated with routine operation of permitted use only                         |
| Non-store Retailers  | P                 | --               | --                |   |
| Offices  | P                 | --               | --                |   |
| Printing and related support activities  | P                 | --               | --                |   |
| Public/self-storage  | P                 | --               | --                |   |
| Research and development services/laboratories   | P                 | --               | --                |   |
| Telecommunications Facilities/Cell Tower   | P                 | --               | --                |   |
| Warehousing and storage (indoor)   | P                 | --               | --                | Including food cold storage   |
| Warehousing and storage (outdoor)  | P                 | --               | --                |   |
| Wholesale trade  | P                 | --               | --                | Within a wholly enclosed building only  |
| <b>LODGING</b>   |                   |                  |                   |   |
| Hotels/motels  | --                | P                | --                |   |
| <b>MANUFACTURING</b>   |                   |                  |                   |   |
| Apparel manufacturing  | P                 | --               | --                |   |
| Computer and electronic products manufacturing   | P                 | --               | --                |   |
| Electrical equipment, appliance, and component manufacturing   | P                 | --               | --                |   |
| Fabricated metal product manufacturing   | P                 | --               | --                |   |

Table 5-1 Land Use Matrix

| Use Legend: P: Permitted Use C: Conditional Use A: Ancillary Use --: Prohibited                                |                   |                  |                   |  |
|--|-------------------|------------------|-------------------|--|
| Use  | Land Use District |                  |                   | Additional Notes   |
|  | Industrial        | Urban Commercial | Urban Residential |  |
| <b>MANUFACTURING (CONT.)</b>   |                   |                  |                   |  |
| Furniture and related products manufacturing   | P                 | --               | --                | Includes, but is not limited to, household and institutional furniture, and kitchen cabinet, office and related products manufacturing |
| Industrial retail sales (sales of goods and/or product either manufactured, warehoused, or wholesaled on-site) | P                 | --               | --                | Maximum 25% of floor area or 15,000 square feet per building, whichever is less  |
| Machinery manufacturing  | P                 | --               | --                |  |
| Medical equipment and supplies manufacturing   | P                 | --               | --                |  |
| Offices  | P                 | --               | --                |  |
| Wood product manufacturing   | P                 | --               | --                |  |
| <b>MOTOR VEHICLE SALES</b>   |                   |                  |                   |  |
| Motor vehicle repair and servicing   | --                | A                | --                | Associated with retail sales, leasing, and/or rental lot(s) only   |
| Motor vehicle retail sales, leasing and rental   | --                | P                | --                | Automobiles, light trucks, vans, motorcycles, and recreational vehicles only   |
| Motor vehicle storage  | --                | A                | --                | Indoor/outdoor storage associated with retail sales, leasing, and/or rental lot(s) only  |
| Motor vehicle washing, cleaning, and detailing   | --                | A                | --                | Associated with retail sales, leasing, and/or rental lot(s) only   |
| Offices  | --                | A                | --                |  |
| <b>OFFICE</b>  |                   |                  |                   |  |
| Advertising agencies   | --                | P                | --                |  |
| Animal hospitals/veterinarians   | --                | P                | --                |  |
| Banks, credit unions, and other depository institutions  | --                | P                | --                | With and without drive through facilities  |
| Campaign offices   | --                | P                | --                |  |
| Check cashing  | --                | P                | --                |  |
| Charitable, philanthropic, service and other non-profit organization offices                                   | --                | P                | --                |  |
| Courier and messenger services   | --                | P                | --                |  |

Table 5-1 Land Use Matrix

| Use Legend: P: Permitted Use C: Conditional Use A: Ancillary Use --: Prohibited |                   |                  |                   |  |
|---|-------------------|------------------|-------------------|--|
| Use   | Land Use District |                  |                   | Additional Notes   |
|   | Industrial        | Urban Commercial | Urban Residential |  |
| <b>OFFICE (CONT.)</b>   |                   |                  |                   |  |
| Data processing/storage services  | --                | P                | --                |  |
| Doctor/Optometrlist   | --                | P                | --                |  |
| Government offices  | --                | P                | --                |  |
| Jewelry and watch/clock repair  | --                | P                | --                |  |
| Money transmitting  | --                | P                | --                |  |
| Offices (General)   | --                | P                | --                |  |
| Payday advances   | --                | C                | --                |  |
| Photocopying, printing and duplicating services                                 | --                | P                | --                |  |
| Public utility office   | --                | P                | --                |  |
| Telecommunications Facilities/Cell Tower  | --                | P                | --                |  |
| <b>RESIDENTIAL</b>  |                   |                  |                   |  |
| Home occupations  | --                | --               | P                 | Home occupations limited to one employee. Display and sale of goods is prohibited. |
| Household pets (3 or fewer animals)   | --                | --               | A                 |  |
| Leasing offices   | --                | --               | A                 |  |
| Multiple family dwellings (apartments, condominiums, townhomes)                 | --                | --               | P                 |  |
| Recreational facilities (as part of a residential development)                  | --                | --               | P                 |  |
| Senior citizen housing  | --                | --               | C                 |  |
| <b>SHOPPING CENTER</b>  |                   |                  |                   |  |
| <i>Commercial/Entertainment Services</i>  |                   |                  |                   |  |
| Animal hospitals/veterinarians  | --                | P                | --                |  |
| Automobile fueling stations   | --                | P                | --                | Planning Area 5 only   |
| Banks, credit unions, and other depository institutions                         | --                | P                | --                | With and without drive through facilities  |
| Barber shop and beauty/nail salons  | --                | P                | --                |  |
| Check cashing   | --                | C                | --                |  |
| Computer, home electronics and small home appliance repair                      | --                | P                | --                |  |

Table 5-1 Land Use Matrix

| Use Legend: P: Permitted Use C: Conditional Use A: Ancillary Use --: Prohibited |                   |                  |                   |   |
|---|-------------------|------------------|-------------------|---|
| Use   | Land Use District |                  |                   | Additional Notes                          |
|   | Industrial        | Urban Commercial | Urban Residential |   |
| <b>SHOPPING CENTER (CONT.)</b>  |                   |                  |                   |   |
| Courier and messenger services  | --                | P                | --                |   |
| Dry cleaners  | --                | P                | --                |   |
| Furniture re-upholstery   | --                | P                | --                |   |
| Health club and gymnasium   | --                | P                | --                |   |
| Jewelry and watch/clock repair  | --                | P                | --                |   |
| Laundries – self service  | --                | P                | --                |   |
| Locksmith/key shops   | --                | P                | --                |   |
| Massage establishments and services   | --                | C                | --                |   |
| Money transmitting  | --                | P                | --                |   |
| Movie theaters  | --                | P                | --                |   |
| Payday advances   | --                | C                | --                |   |
| Pet grooming  | --                | P                | --                |   |
| Photocopying, printing and duplicating services                                 | --                | P                | --                |   |
| Photography studios   | --                | P                | --                |   |
| Shoe repair   | --                | P                | --                |   |
| Tailors   | --                | P                | --                |   |
| Telecommunications Facilities/Cell Tower  | --                | P                | --                |   |
| <i>Eating and Drinking Places</i>   |                   |                  |                   |   |
| Alcoholic beverage sales for consumption on the premises                        | --                | C                | --                |   |
| Banquet facilities  | --                | A                | --                |   |
| Bars/cocktail lounges   | --                | C                | --                |   |
| Catering establishments   | --                | P                | --                |   |
| Fast food and specialty food restaurants  | --                | P                | --                | With and without drive through facilities |
| Full service (sit down) restaurants   | --                | P                | --                |   |
| <i>Retail Services</i>  |                   |                  |                   |   |
| Alcoholic beverage sales for consumption off premises                           | --                | C                | --                |   |
| Antique stores  | --                | P                | --                |   |



Table 5-1 Land Use Matrix

| Use Legend: P: Permitted Use C: Conditional Use A: Ancillary Use --: Prohibited |                   |                  |                   |   |
|---|-------------------|------------------|-------------------|---|
| Use   | Land Use District |                  |                   | Additional Notes  |
|   | Industrial        | Urban Commercial | Urban Residential |   |
| <b>SHOPPING CENTER (CONT.)</b>  |                   |                  |                   |   |
| Art galleries and dealers, and art supply stores                                | --                | P                | --                |   |
| Book stores   | --                | P                | --                |   |
| Certified farmers' markets  | --                | P/C              | --                | <u>Indoor:</u> Permitted Use<br><u>Outdoor:</u> Conditional Use                   |
| Cosmetics and beauty supplies stores  | --                | P                | --                |   |
| Convenience stores  | --                | P                | --                |   |
| Clothing stores   | --                | P                | --                |   |
| Electronics and appliance stores  | --                | P                | --                |   |
| Furniture and home furnishing stores  | --                | P                | --                |   |
| Florists  | --                | P                | --                |   |
| Gifts and novelty stores  | --                | P                | --                |   |
| Grocery stores  | --                | P                | --                |   |
| Hobby, toy, and game stores   | --                | P                | --                |   |
| Jewelry stores  | --                | P                | --                |   |
| Luggage and leather goods   | --                | P                | --                |   |
| Music and video stores  | --                | P                | --                |   |
| Office supplies and stationary stores   | --                | P                | --                |   |
| Optical goods   | --                | P                | --                |   |
| Pet and pet supply stores   | --                | P                | --                |   |
| Postal products, supplies, and services   | --                | P                | --                |   |
| Shoe stores   | --                | P                | --                |   |
| Specialty food stores   | --                | P                | --                |   |
| Sporting goods stores   | --                | P                | --                |   |
| Tobacco products stores   | --                | P                | --                | Excludes hookah bars, smoking lounges, and similar facilities                     |
| Thrift and second hand stores   | --                | P                | --                |   |
| <b>TEMPORARY ACTIVITIES</b>   |                   |                  |                   |   |
| Christmas tree sales, sidewalk and parking lot sales, seasonal sales, etc.      | P                 | P                | P                 | Subject to the temporary use requirements of the City of Ontario Development Code |

## E. INDUSTRIAL DEVELOPMENT STANDARDS

The following standards establish the development criteria that shall apply within the Industrial land use district of this Specific Plan (Planning Areas 1 and 1A).

| Legend: ac.: acre ft.: feet s.f.: square feet                |  |
|--|--|
| <b>Site Requirements</b>                                     |  |
| Minimum Lot Size   | 2.0 ac.  |
| Maximum Floor Area Ratio                                     | 0.60 for individual lot<br>0.55 average for Planning Areas 1 and 1A combined |
| Minimum Landscape Coverage <sup>1</sup>                      | 7% for individual lot<br>15% average for Planning Areas 1 and 1A combined    |
| <b>Minimum Setback Requirements</b>                          |  |
| <b>East 4th Street Setback<sup>9</sup></b>                   |  |
| Building <sup>2a</sup>                                       | 75 ft.   |
| Drive Aisle and Passenger Car Parking <sup>2b, 3</sup>       | 20 ft. <sup>4</sup>  |
| <b>North Vineyard Avenue Setback<sup>2b, 9</sup></b>         |  |
| Building   | 75 ft.   |
| Drive Aisle and Passenger Car Parking <sup>3</sup>           | 20 ft. <sup>4</sup>  |
| <b>Inland Empire Boulevard Setback<sup>2c, 9</sup></b>       |  |
| Building   | 35 ft.   |
| Drive Aisle and Passenger Car Parking <sup>3</sup>           | 15 ft. <sup>4</sup>  |
| <b>Other Public Street Setback<sup>2c, 9</sup></b>           |  |
| Building   | 35 ft.   |
| Drive Aisle and Passenger Car Parking <sup>3</sup>           | 15 ft. <sup>5</sup>  |
| <b>Cucamonga Creek Channel Setback<sup>2d</sup></b>          |  |
| Building   | 25 ft.   |
| Drive Aisle and Passenger Car Parking <sup>3</sup>           | 5 ft.  |
| <b>Interior Side Yard Setback</b>                            |  |
| Building   | 0 ft.  |
| Drive Aisle and Passenger Car and Truck Parking <sup>6</sup> | 5 ft.  |

| Legend: ac.: acre ft.: feet s.f.: square feet  |                         |
|--|-------------------------|
| <b>Public School Property Setback<sup>6</sup></b>  |                         |
| Building   | 50 ft.                  |
| Drive Aisle and Parking  | 20 ft.                  |
| Allowable Encroachment into Setback (cornices, eaves, canopies, and similar architectural features) <sup>7</sup> | 5 ft.                   |
| <b>Minimum Building Separation Requirements</b>  |                         |
| <b>Distance Between Structures</b>   |                         |
| Attached structures  | 0 ft.                   |
| Freestanding structures  | 60 ft.                  |
| <b>Drive Aisle and Parking Space Separation</b>  |                         |
| Adjacent to building office element  | 10 ft.                  |
| Adjacent to solid building wall or screen wall/fence   | 5 ft.                   |
| Within screened/enclosed yards   | 0 ft.                   |
| <b>Maximum Building Height Requirements</b>  |                         |
| Building Height  | 70-150 ft. <sup>9</sup> |
| Vertical Architectural Projections (towers, focal elements, cupolas, etc.)                                       | 10 ft.                  |

*Notes:*

1. Landscaping shall include plantings (trees, shrubs, groundcovers, vines) and may include walkways, benches, trellises, thematic fencing, walls, and related amenities.
- 2a. At least 70 feet of setback shall include landscaping, excluding corner lots.
- 2b. At least 20 feet of each setback shall include landscaping.
- 2c. At least 15 feet of each setback shall include landscaping.
- 2d. At least 5 feet of each setback shall include landscaping.
3. Minimum setback does not apply to driveways that are perpendicular to and connect to public streets.
4. The required setback may be reduced up to 5 feet adjacent to bus stops.
5. The required setback may be reduced to 12 feet where the road transitions approaching an intersection or driveway.
6. In the event PA 1A does not redevelop (i.e., remains a school), a minimum 20-foot landscaped buffer shall be provided along the interface between Planning Area 1 and Planning Area 1A.
7. Encroachments into required setbacks shall only be permitted where adequate emergency access can be maintained.
8. As measured from the public right-of-way.
9. Subject to the Ontario International Airport Land Use Compatibility Plan.

### ***Other Industrial Development Standards***

- (1) Driveways connecting Planning Area 1 and Planning Area 1A to East 4th Street shall be used primarily for passenger car traffic only. Durable, legible, weather-proof “No Truck Entry” signs shall be posted at every driveway entrance along East 4th Street. Access for emergency vehicles will be allowed at East 4th Street.
- (2) Loading docks are prohibited on building facades that directly face East 4th Street, or North Vineyard Avenue. Loading docks at side yards are permitted.
- (3) Loading docks and truck parking areas shall be visually screened from East 4th Street, North Vineyard Avenue, and Inland Empire Boulevard by walls, landscaping, and/or other screening features or barriers (such as berms).
- (4) If entry gates are used, they shall be positioned to allow enough distance for the stacking of at least two (2) trucks on the lot to preclude queuing of trucks on public streets.
- (5) The outdoor storage of materials and equipment shall be permitted ancillary to the land uses allowed pursuant to Table 5-1, *Land Use Matrix*. Within outdoor storage areas, materials or equipment shall be stored to a height no greater than fourteen feet (14’). Outdoor loading and storage areas and loading doors shall be screened from public view by decorative concrete or masonry walls with lockable view obstructing gates. Such walls shall be minimum eight feet (8’) in height and shall be of sufficient height to screen all outdoor materials and equipment, tractors and trailers, and loading doors from public view and shall not exceed fourteen feet (14’) in height.
- (6) Ground- and roof-mounted exterior mechanical equipment, heating and ventilating, air conditioning, tanks, and other mechanical devices shall be screened and treated with a neutral color when visible from East 4th Street, North Vineyard Avenue, Inland Empire Boulevard, or residential property.
- (7) Exterior sound amplification devices (e.g., intercom systems, loudspeakers) shall be oriented away from residential properties.
- (8) Exterior lighting fixtures shall be downward directed. Pole-mounted lights shall be shielded with the light source oriented away from public streets and residential properties.
- (9) Primary buildings (not ancillary buildings) shall achieve a minimum “Certified” rating under the United States Green Building Council’s Leadership in Energy & Environmental Design (LEED) program.
- (10) Solar panels shall be installed on primary industrial buildings (not ancillary buildings) to provide electricity to the the building.
- (11) All manufacturing and processing activities shall be conducted within a wholly enclosed building.
- (12) Properties located within the Airport Influence Area (AIA) established by the Ontario International Airport Land Use Compatibility Plan (ALUCP) shall be subject to the requirements and standards of the ALUCP.

## F. URBAN COMMERCIAL DEVELOPMENT STANDARDS

The following standards establish the development criteria that shall apply within the Urban Commercial land use district of this Specific Plan (Planning Areas 2, 3, and 5).

| Legend: ft.: feet s.f.: square feet                   |  |
|---|--|
| <b>Site Requirements</b>                              |  |
| Minimum Lot Size                                      | n/a <sup>1</sup>                             |
| Maximum Floor Area Ratio                              | 1.0  |
| Minimum Landscape Coverage <sup>2</sup>               | 10% for interior lots<br>13% for corner lots |
| <b>Minimum Setback Requirements</b>                   |  |
| <b>North Vineyard Street Setback<sup>3, 6</sup></b>   |  |
| Building  | 20 ft.                                       |
| Drive Aisle and Parking <sup>4</sup>                  | 20 ft.                                       |
| <b>Inland Empire Boulevard Setback<sup>3, 6</sup></b> |  |
| Building  | 20 ft.                                       |
| Drive Aisle and Parking <sup>4</sup>                  | 20 ft.                                       |
| <b>North Archibald Avenue Setback<sup>3, 6</sup></b>  |  |
| Building  | 20 ft.                                       |
| Drive Aisle and Parking <sup>4</sup>                  | 20 ft.                                       |
| <b>Cucamonga Creek Channel Setback<sup>3</sup></b>    |  |
| Building  | 20 ft.                                       |
| Drive Aisle and Parking <sup>4</sup>                  | 20 ft.                                       |
| <b>Interior Side Yard Setback</b>                     |  |
| Building  | 0 ft.  |
| Drive Aisle and Parking <sup>3, 4</sup>               | 5 ft.  |
| <b>Residential Property Setback</b>                   |  |
| Building <sup>3a</sup>                                | 15 ft.                                       |
| Drive Aisle and Parking <sup>3, 4</sup>               | 5 ft.  |

| Legend: ft.: feet s.f.: square feet  |  |
|--|--|
| <b>Freeway Right-of-Way Setback<sup>3, 6</sup></b>   |  |
| Building   | 100 ft. (front of building facing freeway)       |
|  | 20 ft. (side or rear of building facing freeway) |
| Drive Aisle and Parking  | 20 ft.   |
| Allowable Encroachment into Setback (cornices, eaves, canopies, and similar architectural features) <sup>5</sup> | 3 ft.  |
| <b>Minimum Building Separation Requirements</b>  |  |
| <b>Distance Between Structures</b>   |  |
| Attached structures  | 0 ft.  |
| Freestanding structures  |  |
| Front to Front   | 25 ft.   |
| Front to Rear  | 25 ft.   |
| Other  | 15 ft.   |
| <b>Drive Aisle and Parking Space Separation</b>  |  |
| Parking Stall to Building  | 5 ft.  |
| Drive Aisle to Building  | 10 ft. (front)                                   |
|  | 5 ft. (side and rear)                            |
| <b>Maximum Building Height Requirements</b>  |  |
| Building Height  | 70-150 ft. <sup>7</sup>                          |
| Vertical Architectural Projections (towers, focal elements, cupolas, etc.)                                       | 10 ft.   |

*Notes:*

1. Lot size shall be large enough to accommodate the proposed land use and meet all minimum development standards specified within this Specific Plan.
2. Landscaping shall include plantings (trees, shrubs, groundcovers, vines) and may include walkways, benches, trellises, thematic fencing, walls, and related amenities.
3. The entire setback shall include landscaping.
- 3a. At least 5 feet of the setback shall include landscaping.
4. Minimum setback does not apply to driveways that are perpendicular to and connect to public streets.
5. Encroachments into required setbacks shall only be permitted where adequate emergency access can be maintained.
6. As measured from the public right-of-way.
7. Subject to the Ontario International Airport Land Use Compatibility Plan.

### ***Other Urban Commercial Development Standards***

- (1) Loading docks shall be visually screened from Inland Empire Boulevard, North Vineyard Avenue, and North Archibald Avenue by walls, landscaping, and/or other screening features or barriers (such as berms).
- (2) Ground- and roof-mounted exterior mechanical equipment, heating and ventilating, air conditioning, tanks, and other mechanical devices shall be screened and treated with a neutral color when visible from North Vineyard Avenue, Inland Empire Boulevard, or residential property.
- (3) Exterior sound amplification devices (e.g., intercom systems, loudspeakers) shall be oriented away from residential properties.
- (4) Exterior lighting fixtures shall be shielded with the light source oriented away from public streets and freeways and residential properties.
- (5) Curb cuts for direct driveway access to/from retail drive-through lanes from public streets are prohibited. All circulation to/from retail drive-through lanes shall be contained within a larger Urban Commercial project, with points of access to public streets approved by the City.
- (6) Off-street parking shall be provided in accordance with the City of Ontario Development Code.
- (7) Signage shall conform to the design guidelines contained in Section 6 of this Specific Plan and the City of Ontario Development Code.
- (8) Primary buildings (not ancillary buildings) shall achieve a minimum “Certified” rating under the United States Green Building Council’s Leadership in Energy & Environmental Design (LEED) program.
- (9) All business activities shall be conducted within a wholly enclosed building, excepting sale or display or new or used motor vehicles, outdoor cafes and eating areas, and temporary uses and activities pursuant to an approved temporary use permit.
- (10) Properties located within the Airport Influence Area (AIA) established by the Ontario International Airport Land Use Compatibility Plan (ALUCP) shall be subject to the requirements and standards of the ALUCP.

## G. URBAN RESIDENTIAL DEVELOPMENT STANDARDS

The following standards establish the development criteria that shall apply within the Urban Residential land use district of this Specific Plan (Planning Area 4).

| Legend: du/ac: dwelling units per acre ft.: feet s.f.: square feet |   |
|--|---|
| <b>Site Requirements</b>   |   |
| Maximum Lot Coverage   | 75%   |
| Minimum Lot Size   | 20,000 s.f.   |
| Minimum Density  | 25 du/ac  |
| Minimum Lot Dimensions   |   |
| Width  | 100 ft.   |
| Depth  | 200 ft.   |
| Minimum Landscape Coverage <sup>1</sup>                            | 15% for interior lots<br>18% for corner lots                |
| <b>Minimum Setback Requirements</b>                                |   |
| Inland Empire Boulevard Setback <sup>2a, 6</sup>                   |   |
| Building   | 5 ft.   |
| Drive Aisle and Parking Space                                      | 10 ft. (front of building)<br>20 ft. (side of building)     |
| Other Public Street Setback <sup>2a, 6</sup>                       |   |
| Front Yard   | 5 ft. (building)<br>10 ft. (drive aisle and parking space)  |
| Street Side Yard   | 5 ft. (building)<br>10 ft. (drive aisle and parking space)  |
| Private Street Setback <sup>1, 2b</sup>                            |   |
| Front Yard   | 15 ft. (building)<br>10 ft. (drive aisle and parking space) |
| Street Side Yard   | 15 ft. (building)<br>10 ft. (drive aisle and parking space) |
| Interior Side Yard Setback <sup>3</sup>                            |   |
| Building   | 10 ft.  |
| Drive Aisle and Parking Space                                      | 5 ft.   |

| Legend: du/ac: dwelling units per acre ft.: feet s.f.: square feet   |                         |
|--|-------------------------|
| Rear Yard Setback <sup>3</sup>   |                         |
| Building   | 10 ft.                  |
| Drive Aisle and Parking Space  | 5 ft.                   |
| Alley or Common Drive Setback <sup>3</sup>   |                         |
| Allowable Encroachment into Setback (porches, balconies, cornices, eaves, canopies, and similar architectural features) <sup>4</sup> | 3 ft.                   |
| <b>Minimum Building Separation Requirements</b>  |                         |
| Building Separation  |                         |
| Habitable Structures   |                         |
| Front to Front   | 30 ft.                  |
| Front to Rear  | 30 ft.                  |
| Front to Side  | 25 ft.                  |
| Side to Side   | 10 ft.                  |
| Side to Rear   | 20 ft.                  |
| Rear to Rear   | 20 ft.                  |
| Non-Habitable Structures   |                         |
| Drive Aisle and Parking Space Separation   | 10 ft.                  |
| Building to Parking Space  | 3 ft.                   |
| Building to Drive Aisle  | 3 ft.                   |
| Building to Driveway   | 3 ft.                   |
| <b>Maximum Building Height</b>   |                         |
| Building Height  | 70-150 ft. <sup>7</sup> |
| Vertical Architectural Projections (towers, focal elements, cupolas, etc.)   | 10 ft.                  |
| <b>Minimum Open Space Requirements</b>   |                         |
| Private Open Space (per dwelling) <sup>5</sup>   | 60 s.f.                 |
| Common Open Space (per dwelling) <sup>5</sup>  | 100 s.f.                |

*Notes:*

- 1 Landscaping shall include plantings (trees, shrubs, groundcovers, vines) and may include walkways, benches, trellises, thematic fencing, walls, and related amenities.
- 2a. At least 5 feet of the setback shall include landscaping.
- 2b. At least 10 feet of the setback shall include landscaping.
3. Setbacks from private streets shall be measured from the back of sidewalk.
4. Encroachments into required setbacks shall only be permitted where adequate emergency access can be maintained.
5. Refer to "Other Urban Residential Development Standards," on the following page.
6. As measured from the public right-of-way.
7. Subject to the Ontario International Airport Land Use Compatibility Plan.

### ***Other Urban Residential Development Standards***

- (1) Private Open Space
  - (a) Each unit shall have a minimum contiguous area of 60 s.f. of private open space for the exclusive use of a dwelling unit's occupants and guests. The design of private open space areas shall comply with the City of Ontario Development Code.
- (2) Common Open Space
  - (a) A minimum of 250 s.f. of common open space per unit shall be provided for the enjoyment of all residents within a development project. Common open space may consist of active areas – including, but not limited to, recreation buildings, tot lots, and swimming or wading pools – and passive areas incorporating features that enhance the appearance and desirability of a development project – such as greenbelts, pathways, waterscapes, hardscapes, rockscapes, benches, gazebos, raised planters, and other unique features. The design of common open space areas shall comply with the City of Ontario Development Code.
- (3) Off-street parking shall be provided in accordance with the City of Ontario Development Code.
- (4) Signage shall conform to the design guidelines contained in Section 6 of this Specific Plan and the City of Ontario Development Code.
- (5) Primary buildings (not ancillary buildings) shall achieve a minimum “Certified” rating under the United States Green Building Council’s Leadership in Energy & Environmental Design (LEED) program.

- (6) Properties located within the Airport Influence Area (AIA) established by the Ontario International Airport Land Use Compatibility Plan (ALUCP) shall be subject to the requirements and standards of the ALUCP.
- (7) The following disclosure shall be made to all buyers as part of any residential real estate transaction in accordance with the Ontario International Airport ALUCP and California Business and Professions Code §11010 and Civil Code §§ 1102.6, 1103.4, and 1353:

**NOTICE OF AIRPORT IN VICINITY:** This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.

# Design Guidelines

## Section 6



## DESIGN GUIDELINES

### A. PURPOSE AND INTENT

The *Design Guidelines* presented in this Section set expectations for the quality and character of development in MEREDITH INTERNATIONAL CENTRE. By following these *Guidelines*, the Specific Plan area will be a cohesive and attractive mixed-use center that is an asset to the surrounding community and the City of Ontario.

These *Guidelines* provide criteria for architecture, lighting, energy efficiency, signage, and landscape design, while allowing flexibility for architects, landscape architects, developers, builders, and others involved in designing and developing the details of MEREDITH INTERNATIONAL CENTRE's physical features. The CENTRE's visual identity will be expressed primarily through landscape, hardscape, and signage elements. The *Architectural Design Guidelines* contained in this Section are presented in a manner that ensures consistent architectural expression across MEREDITH INTERNATIONAL CENTRE, while allowing for flexibility in modern-day building design. The ultimate application of these *Guidelines* to a specific design feature should consider the associated land use category and the visual prominence of the feature when viewed from public spaces within and around MEREDITH INTERNATIONAL CENTRE.

All photographs, illustrations, and diagrams contained in these *Design Guidelines* serve as visual aids to convey the overall theme. Exact replication of the examples is neither required nor anticipated.

More specifically, the objectives of these Design Guidelines are:

- To provide the City of Ontario with assurance that MEREDITH INTERNATIONAL CENTRE will develop in accordance with the quality and character described within this Specific Plan.
- To provide guidance to developers, builders, engineers, architects, landscape architects, and other professionals to achieve and maintain the desired design quality.
- To provide guidance to City staff, the Planning Commission, and the City Council in their review of design elements associated with implementing development projects in the Specific Plan area.
- To provide guidelines that steer MEREDITH INTERNATIONAL CENTRE to convey a contemporary aesthetic theme and character while allowing flexibility for practical application and creative expression.
- To provide guidelines for energy efficiency that can be implemented in the site planning, design, and construction phases of the Specific Plan to minimize waste deposited at landfills, decrease energy use and fossil fuel consumption, and reduce domestic water consumption.
- To ensure that the Specific Plan implements the intent of the Ontario Policy Plan and Development Code.

These *Design Guidelines* may be subject to modification and contemporary interpretation to allow for responses to unanticipated conditions, including but not limited to changes in the real estate market, needs and desires of tenants, technology advancements, and fluctuations in economic conditions.

The majority of the *Design Guidelines* presented in this Section apply to development within the MEREDITH INTERNATIONAL CENTRE. For this reason, Specific Plan-wide guidelines are presented first, followed by guidelines that are specifically applicable to either Industrial, Urban Commercial, or Urban Residential planning areas.

## B. DESIGN THEME

MEREDITH INTERNATIONAL CENTRE is a contemporary mixed-use center containing Industrial, Urban Commercial, and Urban Residential land uses that takes advantage of the property’s location near regional transportation corridors. MEREDITH INTERNATIONAL CENTRE is a place where businesses can locate, thrive, have access to goods and services, and where multi-family homes in an urban residential area are within walking distance to shopping, employment opportunities, and a variety of transportation modes.

MEREDITH INTERNATIONAL CENTRE features a contemporary design aesthetic, which provides architectural styling with attractive detailing, a light-toned color palette, and timeless features. Signs are modern, lighting is focused and directed, landscaping is colorful and drought-tolerant, and design features are applied that lower energy use demands of building operations.

MEREDITH INTERNATIONAL CENTRE’s design theme complements the City of Ontario’s character and the surrounding region and comports with the City’s Development Code. When viewed from surrounding public streets, MEREDITH INTERNATIONAL CENTRE is perceived as an inviting place to work, shop, live, and invest time and resources.



## C. SPECIFIC PLAN-WIDE DESIGN GUIDELINES

This section sets forth design guidelines that apply to all planning areas within MEREDITH INTERNATIONAL CENTRE. Developers, builders, engineers, architects, landscape architects, and other design professionals should utilize these guidelines in order to maintain design continuity throughout the mixed-use MEREDITH INTERNATIONAL CENTRE.

### C.1. Architecture Design Guidelines

#### a. *Architectural Design Theme*

The architectural style of MEREDITH INTERNATIONAL CENTRE emphasizes building massing over structural articulation. Buildings are characterized by simple and distinct cubic masses with interlocking volumes of wall planes, colors, and materials to create visual appeal. Exterior building colors are light and warm tones with darker or more vibrant accent colors on wall planes at focal points, such as around building entrances and near outdoor gathering spaces. Additionally, architectural designs may mix colors, materials, and textures to articulate façades and create visual appeal.

Design elements shall be compatible in character, massing, and materials in order to promote a clean and contemporary feel. Individual creativity and identity is encouraged, but care must be taken to maintain design integrity and compatibility among all buildings and planning areas to reinforce a unified image within MEREDITH INTERNATIONAL CENTRE. Generally, buildings within the CENTRE shall not be overly “trendy” or strongly historical; however subtle references to the history of the region are acceptable. Architectural styles should complement, not detract

from, the general architectural character found in the City of Ontario and along the Interstate 10 corridor.

#### b. *Building Form*

Building form is one of the primary elements of architecture. Numerous design aspects, including shape, mass (size), scale, proportion, and articulation, are elements of a building’s “form.” Building forms are especially important for building faces that are visible along the following view corridors:

- Building façades in Planning Area 1 and Planning Area 1A that are visible from East 4th Street;
- Building façades in Planning Area 1 and Planning Area 2 that are visible from North Vineyard Avenue;
- Building façades in all planning areas that are visible from Inland Empire Avenue;
- Building façades in Planning Area 3 that are visible from North Archibald Avenue;
- Building façades in Planning Area 2 and Planning Area 3 that face Interstate 10; and

Buildings within MEREDITH INTERNATIONAL CENTRE should comply with the following guidelines to ensure that development is visually appealing and inviting to pedestrians and motorists:

- (1) Simple geometric forms shall constitute the overall building form. Rectangular forms are encouraged to promote balance and visual interest. Arbitrary, complicated building forms shall be avoided.

- (2) Long horizontal wall planes visible from a public street shall include periodic changes in exterior building materials, color, decorative accents, and/or articulated features.
- (3) Modulation and variation of building masses between adjacent buildings visible from public streets is encouraged.
- (4) Pedestrian entrances to buildings should be made obvious through changes in massing, color, and/or building materials.
- (5) Pedestrian and ground-level building entries should be recessed or covered by architectural projections, roofs, or arcades in order to provide shade and visual relief.
- (6) Architectural and trim detailing on building façades shall be clean, simplistic, and not overly complicated.
- (7) Materials applied to any elevations shall turn the corner of the building to a logical termination point in relation to architectural features or massing.

Note that building faces that orient inward to truck courts or service areas and that are not visible from public roads, the Interstate 10 Corridor, or publically accessible viewing areas, are not required to adhere to the preceding building form guidelines.

**c. *Building Materials, Colors, and Textures***

The use of building materials and colors plays a key role in developing a clean, contemporary visual environment; therefore, the selected exterior materials, colors, and textures should complement one another throughout the CENTRE. Slight variations are encouraged within MEREDITH INTERNATIONAL CENTRE to provide visual interest.

- (1) Appropriate primary exterior building materials include stucco, concrete, and similar materials, as well as tilt-up panels. The primary materials shall be accented by secondary materials including but not limited to natural or fabricated stone, wood siding (horizontal or vertical), metal, brick, tile or tile panel systems; glass or glazing units; and glass block.
- (2) Unfinished exterior surfaces are not permitted on any building façade with the exception of natural concrete or masonry.
- (3) The use of metal and/or glass fabrications for storefronts or curtain wall areas are appropriate.
- (4) Trim details may include metal finished in a consistent color, plaster, or concrete elements finished consistently with the building treatment. Use of overly extraneous “themed” detailing, like oversized or excessive foam cornice caps, foam moldings and window detailing is discouraged.
- (5) Material changes shall occur at intersecting planes, preferably at the inside corners of change of wall planes, or where architectural elements intersect.
- (6) Primary exterior building colors shall be light and warm tones. Darker and/or more vibrant accent colors may be provided in focal point areas, such as around building entrances and near outdoor gathering spaces.
- (7) Bright primary colors, garish use of color and arbitrary patterns or stripes that will clash with this color palette are discouraged, except in signage logos.
- (8) Exposed downspouts, service doors and mechanical screen colors shall be the same color as the adjacent wall.

*Building Form*



*Materials, Colors, Textures*



**d. Windows and Doors**

The patterns of openings – windows and doors – shall correspond with the overall rhythm of the building and should be consistent in form, pattern, and color within a planning area. Guidelines for windows and doors within MEREDITH INTERNATIONAL CENTRE are as follows:

- (1) Recessed window and door openings are encouraged to enhance the visual play of light and shadow.
- (2) When possible, the layout of doors and windows on individual building façades should be placed in a repetitive pattern to create continuity.
- (3) Window styles and trims shall be consistent in form and color. Window trims shall be finished in a consistent color on each building.
- (4) Gold or unfinished/untreated metal window or door frames are prohibited. Clear silver anodized frames are allowed.
- (5) Glass shall be clear or colored with subtle reflectiveness. Silver glass is prohibited.
- (6) Pedestrian entrances to buildings shall be clearly defined by features such as overhangs, awnings, and canopies or embellished with decorative framing treatments – including but not limited to accent trim. Dark and confined entries, flush doorways, and tacked-on entry alcoves are discouraged.

*Windows and Doors*



**e. Walls and Fences**

The following guidelines for walls and fencing will ensure that these features will complement the overall MEREDITH INTERNATIONAL CENTRE design theme, attractive from public viewing areas, scaled appropriately, durable, and integrated consistently within MEREDITH INTERNATIONAL CENTRE.

- (1) Freestanding walls and fences should not exceed a height of 14 feet, measured from the base of the wall/fence to the top of wall/fence.
- (2) Landscape hedge walls may be used as an option to minimize the perceived height of walls and fences.
- (3) Walls and fences in public view should be built with attractive, durable materials.
- (4) Chain-link fencing is only allowed in areas not within public view.
- (5) Along street frontages, long expanses of wall surfaces shall be offset and architecturally treated to prevent monotony. Techniques to accomplish this may include, but are not limited to: openings, material changes, pilasters and posts, and staggered sections.
- (6) Wall and fencing materials shall be compatible with other design elements of MEREDITH INTERNATIONAL CENTRE.

**f. Functional Elements**

The design and location of functional elements common to Industrial, Urban Commercial and/or Urban Residential buildings should be carefully considered. Examples of functional elements include loading doors, service docks, ground or wall-mounted equipment, rooftop equipment, and trash enclosures. The design and placement of these elements should minimize their prominence when viewed from public roads, the Interstate 10 Corridor, publically accessible parking lots, and other locations within MEREDITH INTERNATIONAL CENTRE.

**Loading Doors and Service Docks**

- (1) Loading doors, service docks, and equipment areas should be oriented or screened to reduce visibility from public roads and publically accessible locations within MEREDITH INTERNATIONAL CENTRE. Screening may be accomplished with solid walls or fences that are compatible with the architectural expression of the building. Screening may also be accomplished by landscaping.
- (2) No loading or unloading activity is permitted to take place from public streets. Service vehicles shall have clear and convenient access into and within MEREDITH INTERNATIONAL CENTRE and should not disrupt vehicular and pedestrian circulation.

*Walls, Fences, and Functional Elements*



**Ground or Wall-Mounted Equipment**

- (1) Ground-mounted equipment, including but not limited to mechanical equipment, electrical equipment, emergency generators, boilers, storage tanks, risers, electrical conduits, shall be screened when feasible from public viewing areas within Planning Areas 2, 3, 4 and 5 and/or from adjacent public roads. Screening may be accomplished with site walls or landscape elements that are consistent with these Guidelines.
- (2) Electrical equipment rooms shall be located within the building envelope. Pop-outs or shed-like additions are discouraged.
- (3) Wall-mounted items, such as roof ladders or electrical panels, should not be located on the building façade facing adjacent public roads. Wall-mounted items should be screened or incorporated into the architectural elements of the building so as not to be visually apparent from the street or other public areas within MEREDITH INTERNATIONAL CENTRE.

**Rooftop Equipment**

- (1) All rooftop equipment, including but limited to mechanical equipment, electrical equipment, storage tanks, cellular telephone facilities, satellite dishes, skylights, vents, exhaust fans, smoke hatches, and mechanical ducts, shall be screened so as not to be visible from public roads, and publically accessible locations within MEREDITH INTERNATIONAL CENTRE.
- (2) Rooftop screens shall be integrated into the architecture of the main building.
- (3) Wood finished rooftop screens are prohibited.



## **Trash Enclosures**

- (1) All outdoor refuse containers shall be screened within a permanent, durable enclosure and should be oriented so they are not easily visible from public roads, publically accessible parking lots within MEREDITH INTERNATIONAL CENTRE, or other public viewing areas. The enclosure's design shall reflect the architectural style of adjacent buildings and use similar, high-quality materials.
- (2) Refuse collection areas shall be located behind or to the side of buildings, away from the building's main entrance.
- (3) All outdoor trash enclosures shall be constructed with solid roofs to prevent exposure of dumpster contents to rainfall and prevent polluted storm water runoff from these structures.

## C.2. Landscape Design Guidelines

These project-wide *Landscape Design Guidelines* establish landscape principles and standards that apply to all planning areas within MEREDITH INTERNATIONAL CENTRE to ensure that plant materials, entries and monuments, streetscapes and other amenities are compatible with the CENTRE’s design theme and that all land use categories are united under a common design vocabulary. The *Landscape Design Guidelines* also commit MEREDITH INTERNATIONAL CENTRE to an environmentally sensitive design by establishing a water-efficient plant palette and thoughtfully grading green space areas to function as retention/infiltration swales to conserve water resources. These *Landscape Design Guidelines*, when taken with the companion *Architectural Design Guidelines* provided herein, establish an identity for MEREDITH INTERNATIONAL CENTRE that is contemporary, visually appealing, and sensitive to the environment.

Although a great deal of specific design information is presented in these *Landscape Design Guidelines*, these Guidelines are not intended to establish a set of rigid landscaping requirements for MEREDITH INTERNATIONAL CENTRE, and it is recognized that, at times, there will be a need to adapt these Guidelines to meet certain parcel-specific or user-identity requirements. As such, these *Landscape Guidelines* are intended to be flexible, and are subject to modification over time. However, it is critical to MEREDITH INTERNATIONAL CENTRE’s long-term design integrity that any deviations from these Landscape Guidelines are in keeping with the spirit of the core elements of the overall theme described herein to ensure a cohesive and unified landscape concept across MEREDITH INTERNATIONAL CENTRE.

### a. *Plant Palette*

The plant palette for MEREDITH INTERNATIONAL CENTRE includes colorful shrubs and groundcovers, ornamental grasses and succulents, and evergreen and deciduous trees – including flowering varieties – that are commonly used throughout Southern California and complement the CENTRE’s design theme and setting. Many of the plant materials are water-efficient species native to Southern California or naturalized to the arid Southern California climate.



Table 6-1, *Plant Palette*, provides a list of plant materials approved for use in MEREDITH INTERNATIONAL CENTRE. The plants listed in Table 6-1 establish a base palette for MEREDITH INTERNATIONAL CENTRE landscape design. Other similar plant materials may be substituted for species listed in Table 6-1, provided the replacement plants are drought-tolerant and complement the MEREDITH INTERNATIONAL CENTRE design theme.

Table 6-1 Plant Palette

| Botanical Name                               | Common Name             | Application   |
|--|-------------------------|---|
| <b>Trees</b>                                 |                         |   |
| <i>Arbutus 'Marina'</i>                      | Marina Strawberry Tree  | Accent / Background                                   |
| <i>Feijoa sellowiana</i>                     | Feijoa                  | Accent  |
| <i>Geijera parviflora</i>                    | Australian Willow       | Water Quality Basin                                   |
| <i>Koelreuteria paniculata</i>               | Goldenrain              | On site   |
| <i>Lagerstroemia f. 'Natchez'</i>            | Crape Myrtle            | Street Tree / On site / Water Quality Basin           |
| <i>Magnolia grandiflora 'Little Gem'</i>     | Little Gem Magnolia     | Accent  |
| <i>Magnolia spp.</i>                         | Magnolia                | On site   |
| <i>Olea europaea - fruitless</i>             | Fruitless Olive         | Theme   |
| <i>Pinus chinensis</i>                       | Chinese pistache        | On site   |
| <i>Pinus halepensis</i>                      | Aleppo Pine             | Background  |
| <i>Platanus acerifolia</i>                   | London Plane            | Street Tree   |
| <i>Platanus m. 'Alamo'</i>                   | Mexican Sycamore        | Water Quality Basin                                   |
| <i>Platanus racemosa</i>                     | California Sycamore     | Street Tree   |
| <i>Podocarpus gracilor</i>                   | Fern Pine               | On site   |
| <i>Quercus agrifolia</i>                     | Coast Live Oak          | On site / Water Quality Basin                         |
| <i>Quercus virginiana</i>                    | Southern Live Oak       | Street Tree   |
| <i>Schinus Molle</i>                         | California Pepper Tree  | Water Quality Basin                                   |
| <i>Tristania spp.</i>                        | Brisbane Box            | On site   |
| <i>Ulmus parvifolia</i>                      | Chinese Elm             | On site   |
| <b>Shrubs and Groundcovers</b>               |                         |   |
| <i>Aloe arborescens</i>                      | Torch Aloe              | Accent / Protected                                    |
| <i>Aloe petricola</i>                        | Stone Aloe              | Accent / Protected                                    |
| <i>Arbutus u. 'Compacta'</i>                 | Compact Strawberry Tree | Background  |
| <i>Baccharis 'Starn P.P.#11240 Thompson'</i> | Thompson Baccharis      | Midground / Foreground                                |
| <i>Bougainvillea 'La Jolla'</i>              | La Jolla Bougainvillea  | Accent / Protected / Water Quality Basin (slopes)     |
| <i>Callistemon 'Little John'</i>             | Dwarf Callistemon       | Midground / Foreground                                |
| <i>Carex</i>                                 |                         |   |
| <i>Cistus purpureus</i>                      | Purple Rock Rose        | Midground / Water Quality Basin (slopes)              |
| <i>Cistus salviifolius</i>                   | Rock Rose               | Parkway Groundcover                                   |
| <i>Cotoneaster 'Lowfast'</i>                 | Bearberry Cotoneaster   | Parkway Groundcover                                   |
| <i>Cupressus sempervirens</i>                | Italian Cypress         | Accent / Background                                   |
| <i>Dietes bicolor</i>                        | Bicolor Iris            | Midground / Foreground / Water Quality Basin (slopes) |
| <i>Elaeagnus pungens</i>                     | Thorny Olive            | Background / Water Quality Basin (slopes)             |
| <i>Festuca mairei</i>                        | Blue Fescue             | Midground / Foreground                                |
| <i>Furcraea gigantea</i>                     | Mauritius Hemp          | Accent / Protected                                    |
| <i>Heteromeles arbutifolia</i>               | Toyon                   | Water Quality Basin (slopes)                          |
| <i>Leucophyllum f. 'green cloud'</i>         | Texas Ranger            | Background  |
| <i>Muhlenbergia 'regal mist'</i>             | Pink Muhly              | Midground / Foreground                                |
| <i>Myoporum p. 'Putah Creek'</i>             | Putah Creek Myoporum    | Parkway Groundcover                                   |
| <i>Pittosporum tobira</i>                    | Japanese Pittosporum    | Background / Midground                                |
| <i>Rosa californica</i>                      | California Rose         | Water Quality Basin (slopes)                          |
| <i>Rosa spp.</i>                             | Rose                    | Accent / Midground / Foreground                       |
| <i>Rosmarinus var.</i>                       | Rosemary                | Parkway Groundcover                                   |
| <i>Salvia greggii</i>                        | Autumn Sage             | Midground / Foreground                                |
| <i>Senecio mandraliscae</i>                  | Blue Chalk Sticks       | Parkway Groundcover                                   |
| <i>Westringia 'Wynyabbie Gem'</i>            | Coast Rosemary          | Midground   |

## **b. Entries and Monuments**

MEREDITH INTERNATIONAL CENTRE provides a three-tiered hierarchy of entry and corner treatments to identify the CENTRE and distinguish individual planning areas. The entry and corner treatments are designed to provide distinctive visual statements and emphasize the Specific Plan's contemporary aesthetic.

The entry and corner concepts described and illustrated on the following pages have been designed to provide a prominent reminder of the quality and distinctiveness of MEREDITH INTERNATIONAL CENTRE and to complement and reinforce the CENTRE's general architectural and landscape theme. Implemented entry and corner treatments should be flexible to respond to physical contexts and the needs and desires of specific tenants and may differ slightly from the concepts presented herein; however, all entry and corner treatments within MEREDITH INTERNATIONAL CENTRE shall be consistent in theme and character.

### **Inland Empire Boulevard Entry Monuments**

Entry monuments are located at the Inland Empire Boulevard and North Vineyard Avenue intersection (northeast and southeast corners) to announce arrival to MEREDITH INTERNATIONAL CENTRE.

The monuments at Inland Empire Boulevard are the largest of the entry/monument family. The conceptual design for these monuments include a low, curved wall with entry signage (approximately 3.5 feet tall and 32 feet wide) framed by battered stone pedestals (approximately 4 feet tall and 4-feet wide, measured at cap). The pedestals feature precast concrete caps. Landscaping at the Inland Empire Boulevard entries includes low foreground plants

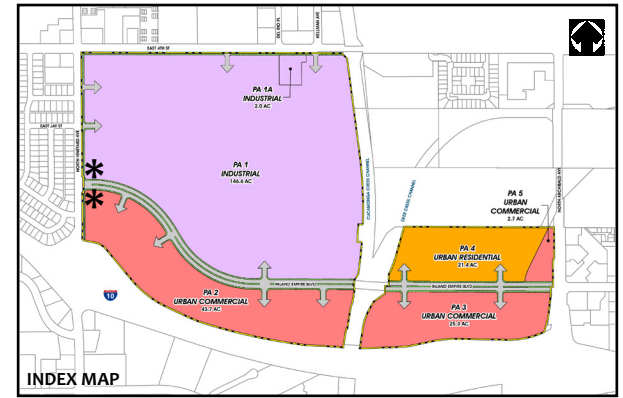
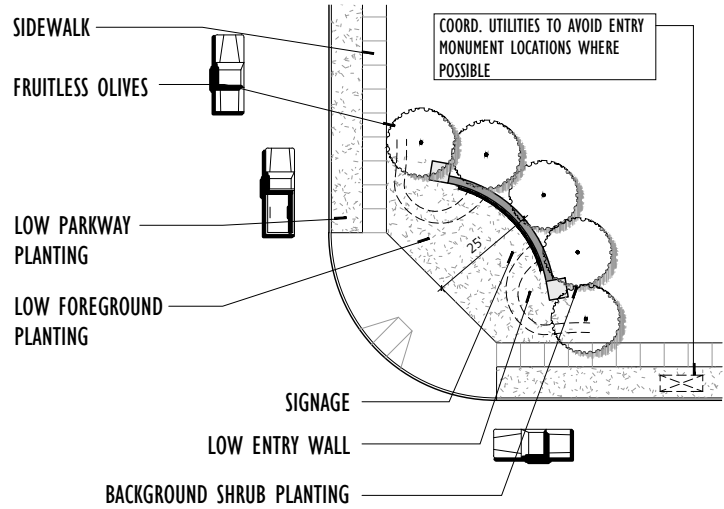
with medium shrubs and densely planted olive trees behind the monument wall. Figure 6-1, *Inland Empire Boulevard Entry*, conceptually illustrates these monuments.

### **Corner Monuments**

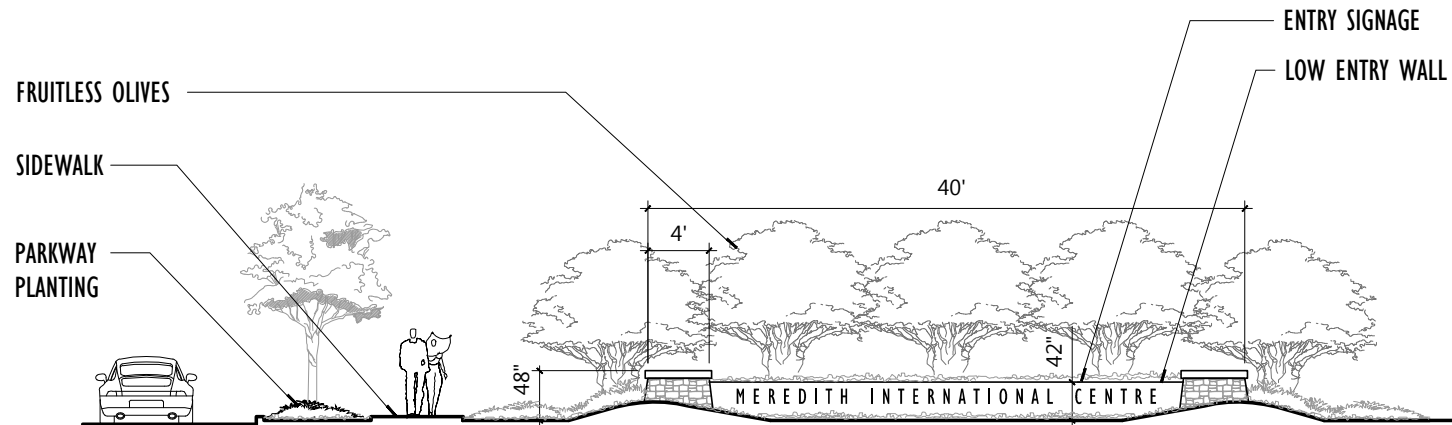
MEREDITH INTERNATIONAL CENTRE provides a monument at the southeast corner of the East 4th Street and North Vineyard Avenue intersection (Planning Area 1) to subtly promote the CENTRE's design theme at a key focal point. This Corner Monument features two battered stone pedestals with precast concrete caps. The stone pedestals will be approximately 3.5 feet tall and 4 feet wide (measured at the cap). Landscaping at the Corner Monument includes low foreground plantings (groundcovers), colorful, tiered foreground plantings (low shrubs, accent grasses, and succulents), background hedges, and a small cluster of olive trees. Figure 6-2, *Corner Monument*, conceptually illustrates the Corner Monument.

### **Primary Parcel Entry Monuments**

Primary Parcel Entry Monuments may be provided at primary driveways to Planning Areas 1, 2, 3, and 4. Most Primary Parcel Entry Monuments are planned along Inland Empire Boulevard. Primary Parcel Entry Monuments are not required on driveways fronting East Jay Street, Del Rio Place, or East 4th Street. These monuments feature paired battered stone pedestals with concrete caps (approximately 3.5 feet tall and 4-feet wide – measured at cap), foreground plantings (groundcovers), background shrubs and hedges, and a small cluster of olive trees. As an option, low monument signs may be incorporated into Primary Parcel Entry Monuments as conceptually illustrated on Figure 6-3, *Primary Parcel Entry Monument*.



**PLAN VIEW**



**ELEVATION VIEW**

Source: SJA (01-19-2015)

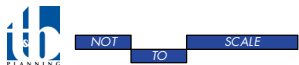
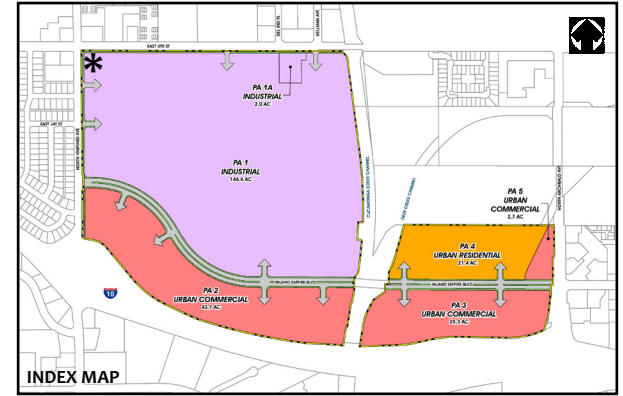
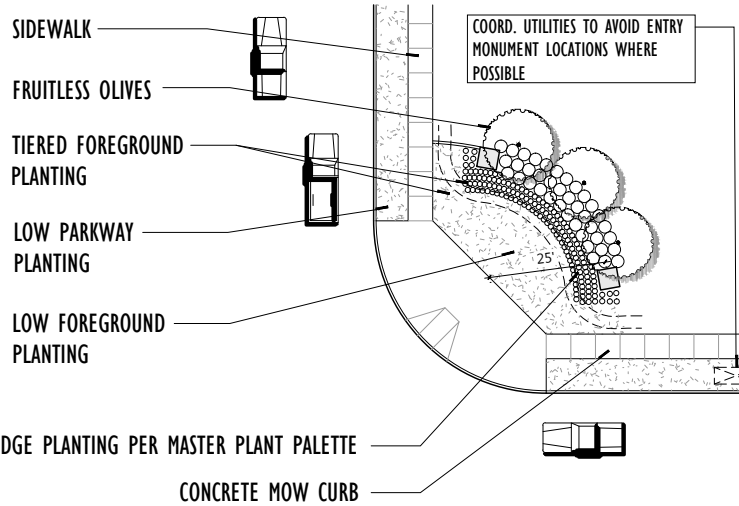
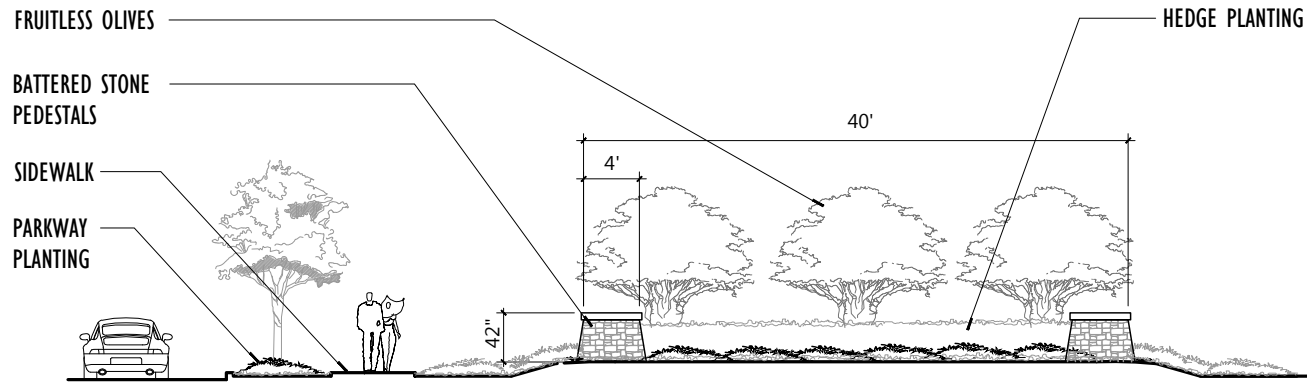


FIGURE 6-1

INLAND EMPIRE BOULEVARD ENTRY MONUMENT

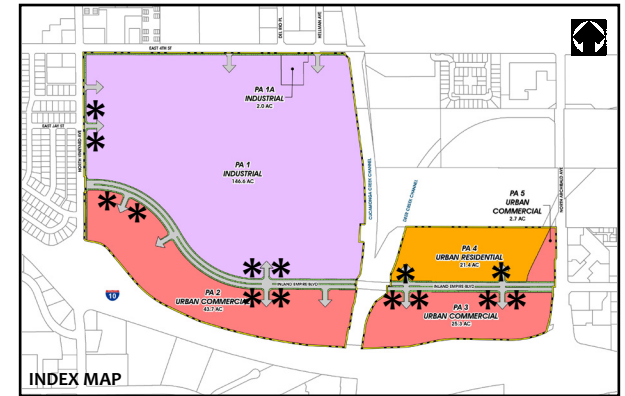
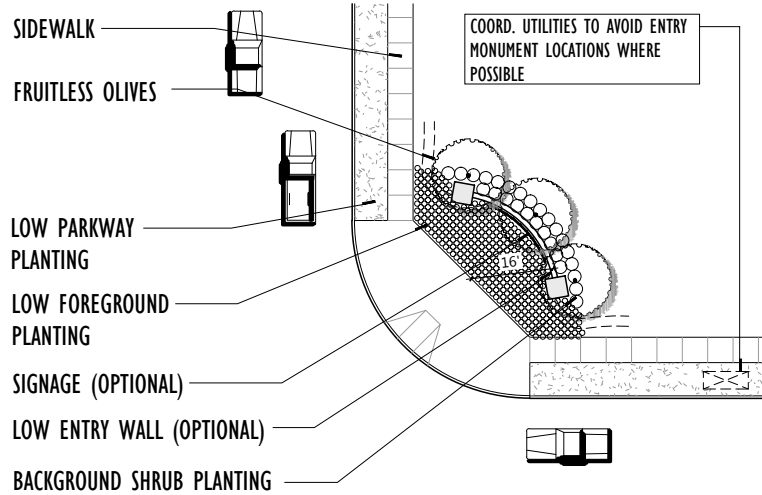


**PLAN VIEW**

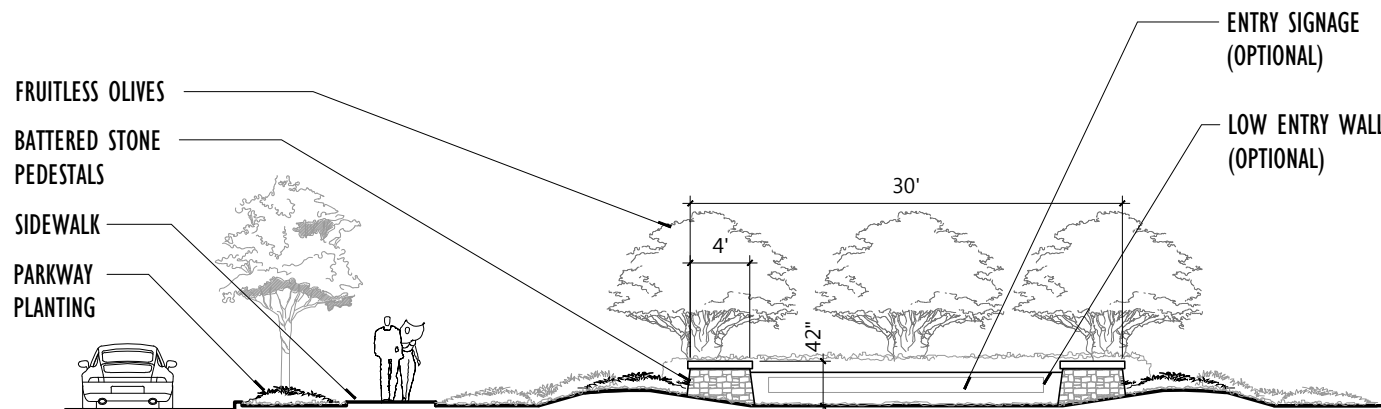


**ELEVATION VIEW**

Source: SJA (01-19-2015)



**PLAN VIEW**



**ELEVATION VIEW**

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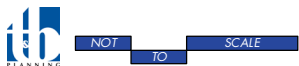


FIGURE 6-3

PRIMARY PARCEL ENTRY MONUMENT

### **c. Streetscapes**

Streetscape landscaping plays an important role in creating a sense of place. In addition, streetscapes serve functional purposes, including screening undesirable views from public view. Within MEREDITH INTERNATIONAL CENTRE, streetscapes are planted with a combination of evergreen and deciduous trees, low shrubs, and masses of groundcovers to create a visually pleasing experience for pedestrians and passing motorists. The landscaping plant palette for streetscapes should link the roadways to the rest of MEREDITH INTERNATIONAL CENTRE and should reflect the CENTRE's landscape design theme.

The conceptual streetscape landscape treatments within MEREDITH INTERNATIONAL CENTRE are presented on the following pages.

#### **Inland Empire Boulevard**

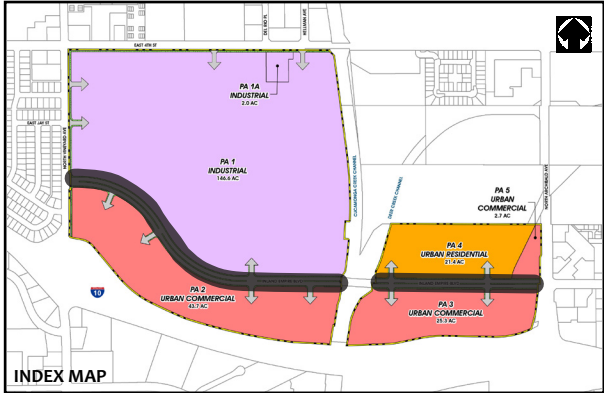
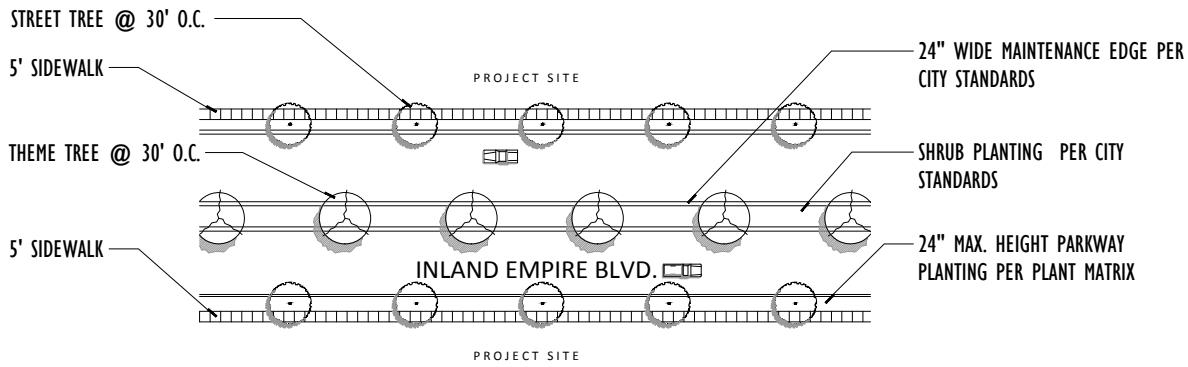
The typical Inland Empire section features two landscaped components: a 14-foot-wide raised median and 12-foot-wide parkways on both sides of the street. The raised median is planted with thematic accent trees at regular intervals – to reinforce the theme established at entries and monuments – and colorful groundcovers and succulents. Parkway include a curb-adjacent park strip planted with deciduous and/or evergreen trees, such as California Sycamore and/or Southern Live Oak, and low flowering groundcovers and succulents, as well as a sidewalk. Evergreen trees are planted outside of the right-of-way to provide pedestrians using the sidewalk with additional opportunities for shade and to screen undesirable views. The typical Inland Empire Boulevard section is illustrated on Figure 6-4, *Inland Empire Boulevard Streetscape*.

The bridge segment of Inland Empire Boulevard (crossing the Cucamonga Creek and Deer Creek Channels) do not feature a landscaped median or a landscaped park strip; however, this segment does include a 6-foot-wide, curb-adjacent sidewalk.

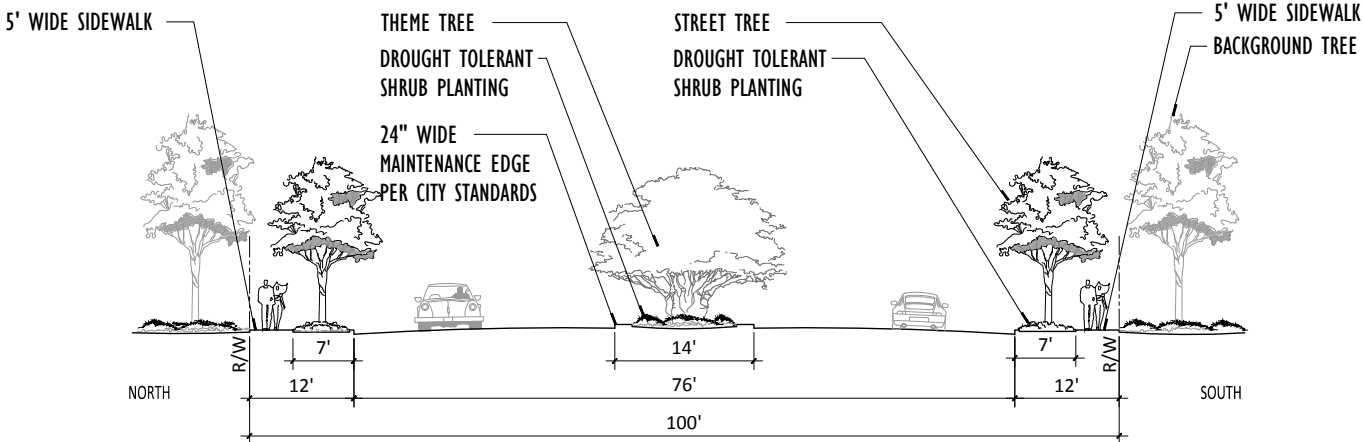
#### **North Vineyard Avenue**

The segment of North Vineyard Avenue abutting the MEREDITH INTERNATIONAL CENTRE features two landscaped components: 1) a 14-foot-wide raised center median and 2) parkways on both sides of the street. The raised center median features deciduous trees selected to match the existing landscape theme along North Vineyard Avenue. The parkway on the east side of the street provides evergreen and coniferous trees planted at regular intervals in the curb-adjacent park strip; tree species will be selected to match the existing landscape theme along North Vineyard Avenue. In addition, the park strip features drought tolerant shrub plantings (maximum height of 24 inches). Evergreen trees are planted outside of the right-of-way to provide pedestrians using the sidewalk with additional opportunities for shade and to screen undesirable views. The 12-foot wide parkway on the west side of the street was existing as of the date of the writing of this Specific Plan (2015) and will be maintained in its existing condition. The typical North Vineyard Avenue section is illustrated on Figure 6-5, *North Vineyard Avenue Streetscape*.



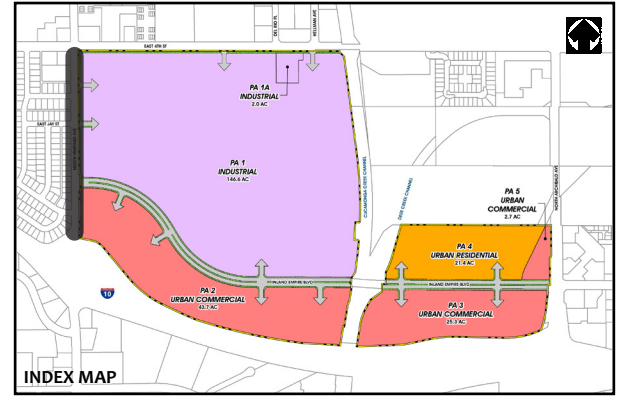
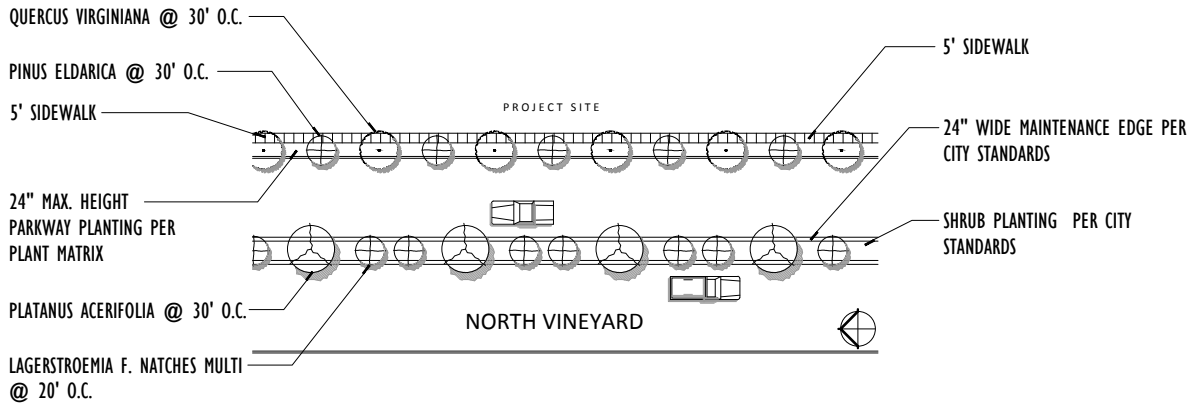


**PLAN VIEW**

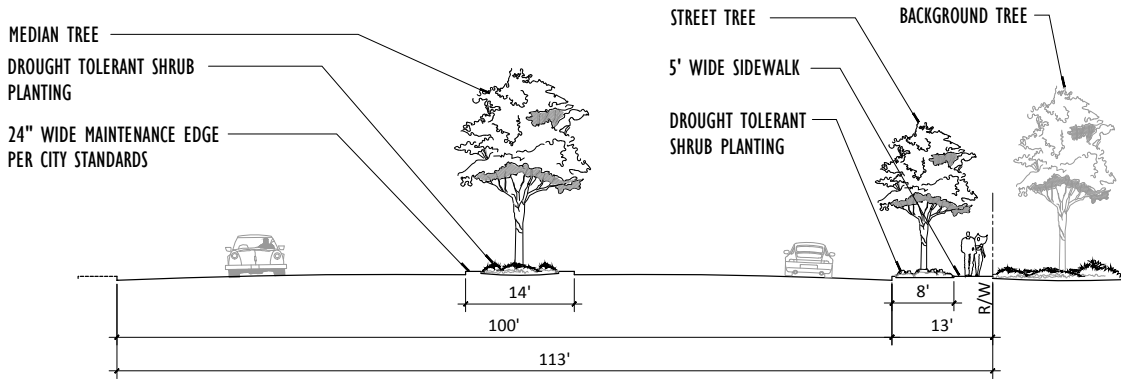


**ELEVATION VIEW**

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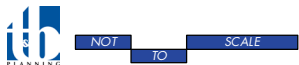


PLAN VIEW



ELEVATION VIEW

Source: SJA (01-19-2015)



### **East 4th Street**

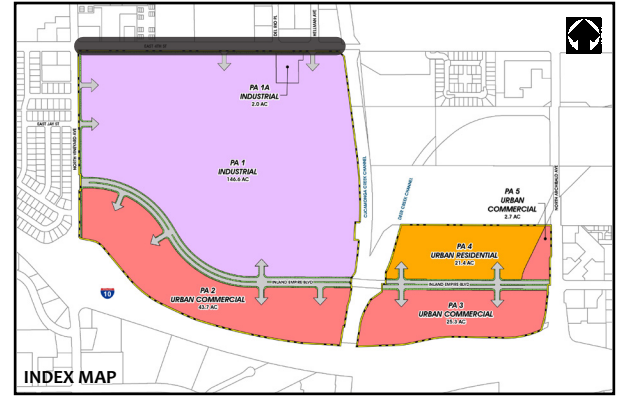
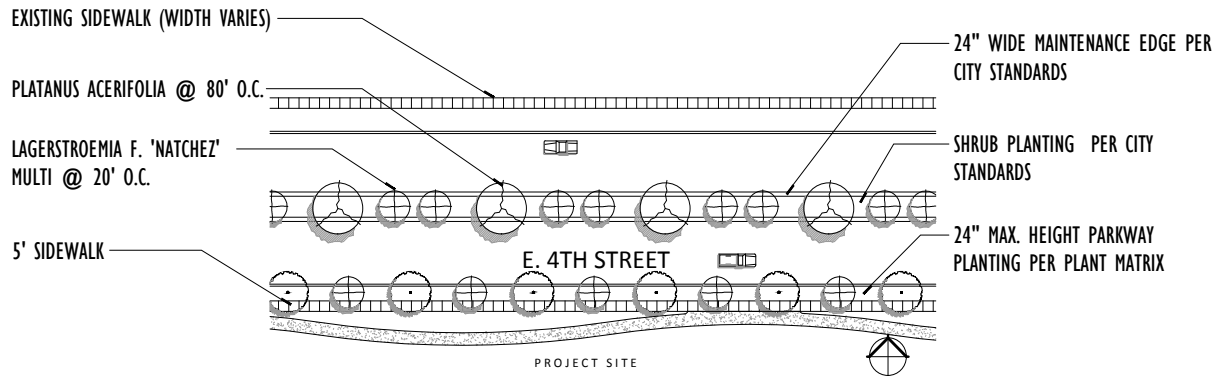
The segment of East 4th Street abutting the MEREDITH INTERNATIONAL CENTRE features two landscaped components: 1) a 14-foot-wide raised center median and 2) 14-foot wide parkways on both sides of the street. The raised median features deciduous trees and low shrubs (maximum 24-inch height) selected to match the existing landscape theme along East 4th Street. The parkway on the south side of the street includes street trees planted at regular intervals and low, colorful, drought tolerant shrub plantings in the curb-adjacent park strip. Plant species will be selected to match the existing landscape theme along East 4th Street and to create a gradual visual transition to the enhanced landscape zone provided along East 4th Street (refer to Figure 6-9, *Industrial/East 4th Street Interface*). The parkway on the north side of the street was existing as of the date of the writing of this Specific Plan (2015) and will be maintained in its existing condition. Figure 6-6, *East 4th Street Streetscape*, illustrates the typical streetscape for East 4th Street.

### **North Archibald Avenue**

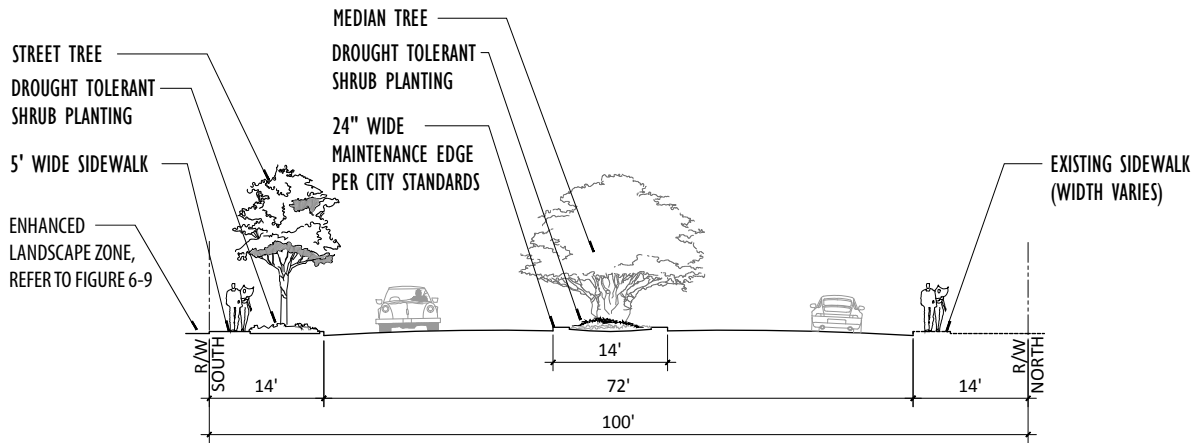
Improvements to North Archibald Avenue that were in place at the time this Specific Plan was prepared (2015) are expected to remain in place, including streetscape improvements. Figure 6-7, *North Archibald Avenue Streetscape*, depicts the existing streetscape improvements along the segment of North Archibald Avenue that abuts the MEREDITH INTERNATIONAL CENTRE. As shown, the parkways on the western side of the street feature a curb adjacent park strip planted with trees and low, colorful shrubs and groundcovers.

### **East Jay Street and Del Rio Place**

East Jay Street and Del Rio Place provide direct access to individual parcels in Planning Area 1 and, as such, the streetscapes for these streets are designed to facilitate operations of Industrial land uses as well as provide opportunities for screening. As illustrated on Figure 6-8, *East Jay Street and Del Rio Place Streetscape*, East Jay Street and Del Rio Place provide 13-foot-wide parkways on both sides of the street. The parkways feature 8-foot-wide curb adjacent park strips (planted with deciduous and/or evergreen trees and low flowering groundcovers and succulents), as well as 5-foot-wide sidewalks abutting the limits of the right-of-way. Landscaped hedges and/or fencing may be provided adjacent to parking, loading, and/or storage areas for additional screening.

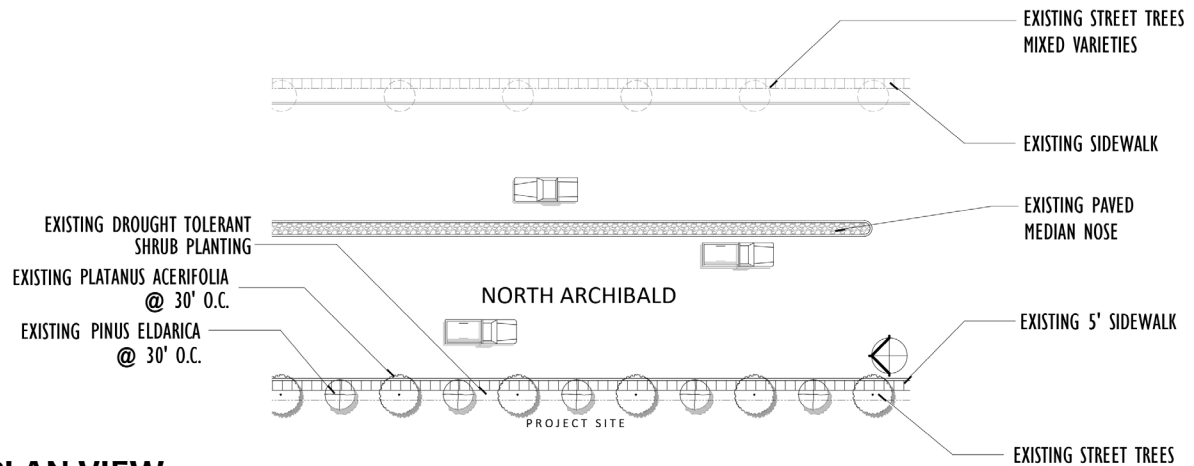
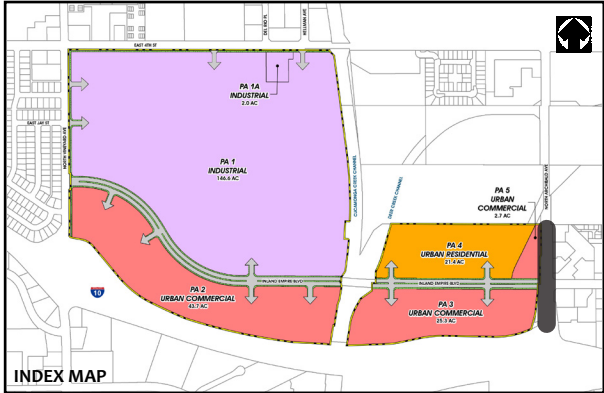


PLAN VIEW

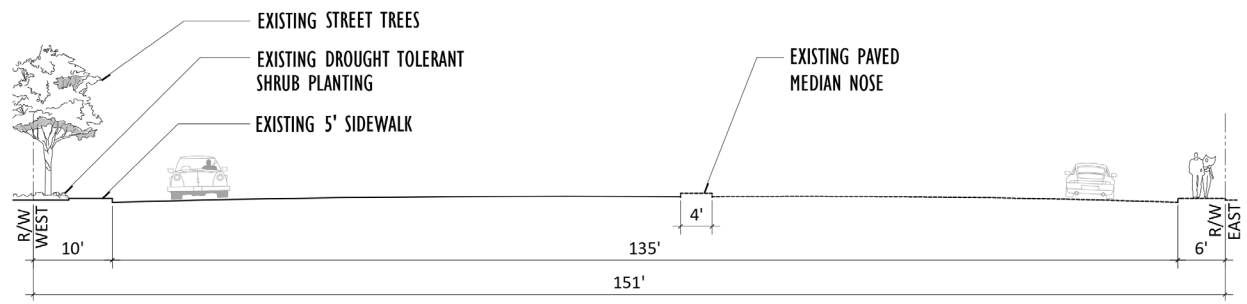


ELEVATION VIEW

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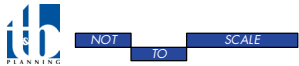


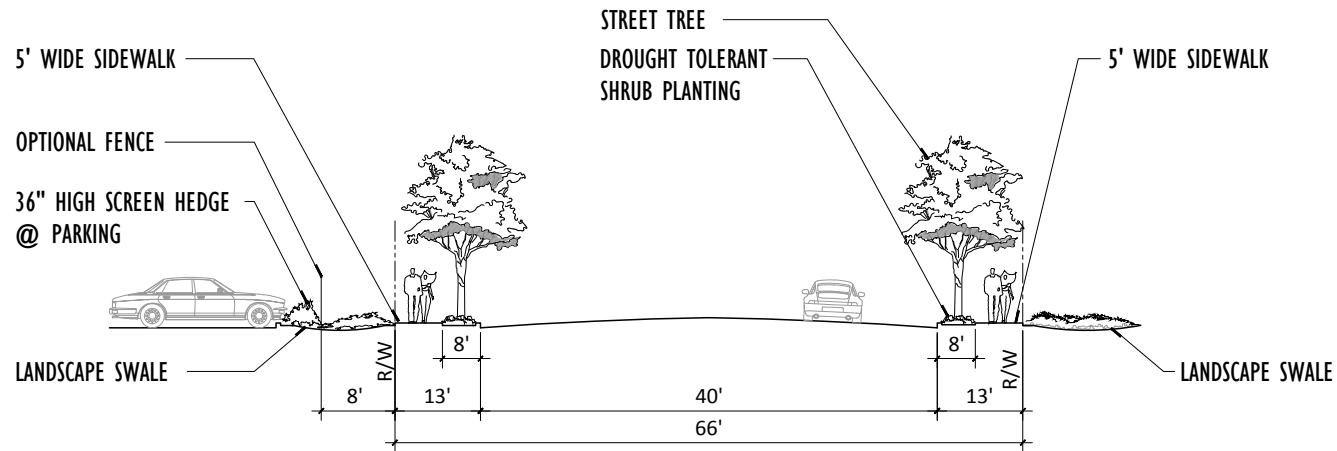
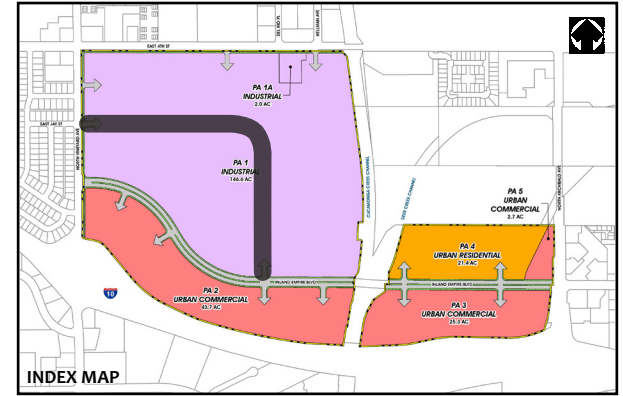
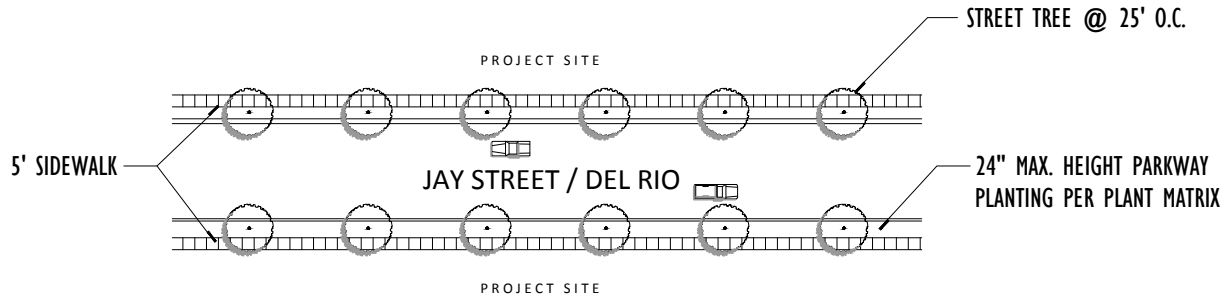
PLAN VIEW



ELEVATION VIEW

Source: SJA (01-19-2015)





Source: SJA (01-19-2015)

**d. Irrigation**

The following general irrigation concepts shall be considered in the design and installation of irrigation systems within MEREDITH INTERNATIONAL CENTRE:

- (1) All landscaped areas shall be equipped with permanent, automatic, underground irrigation system. Drip systems are encouraged in all areas needing irrigation. Irrigation systems must conform to all City of Ontario requirements.
- (2) Irrigation systems should be designed to apply water slowly, allowing plants to be deep soaked and to reduce run-off.
- (3) “Pop-up” type sprinkler heads may be used adjacent to all walks, drives, curbs (car overhangs), parking areas and public right-of-way but must be designed to prevent all run-off and overspray
- (4) The design of irrigation systems, particularly the location of controller boxes, valves, and other above-ground equipment (e.g., backflow prevention devices), shall be incorporated into the overall landscaping design. Where aboveground equipment is provided, it shall be screened or otherwise removed from public view, to the extent possible.
- (5) The irrigation system shall be programmed to operate between 8:00pm and 9:00am.

### **C.3. Outdoor Lighting Guidelines**

Outdoor lighting of MEREDITH INTERNATIONAL CENTRE is an important architectural element that creates visual appeal, facilitates safe pedestrian and vehicular circulation, and adds to security. These Outdoor Lighting Guidelines are categorized into two (2) elements: Public Lighting and Parcel Lighting. “Public Lighting” refers to lighting positioned along the perimeter of MEREDITH INTERNATIONAL CENTRE and along internal public streets. “Parcel Lighting” refers to illumination of internal areas for purposes of safety, security, and nighttime ambience.

The following guidelines provide effective and visually appealing outdoor lighting that serve a variety of spaces such as public streets, parking lots, loading docks, walkways, courtyards/patios, building entries, rooftops, and building-mounted façade lights.

#### ***a. General Lighting Guidelines***

- (1) All outdoor lighting, including spotlights, floodlights, electrical reflectors, and other means of illumination for signs, structures, landscaping, parking, loading, unloading, and similar areas, shall feature cutoff devices as appropriate and be focused, directed, and arranged to minimize glare and “spill over” to public streets or adjoining property.
- (2) Low intensity, energy-conserving night lighting is preferred.
- (3) All lighting fixtures shall be from the same - or complementary - family of fixtures with respect to design, materials, fixture color, and light color.

*Outdoor Lighting*





- (4) Lights shall be unbreakable plastic, recessed, or otherwise designed to reduce the problems associated with damage and replacement of fixtures.
- (5) Neon and similar types of lighting are prohibited in all areas within MEREDITH INTERNATIONAL CENTRE. The only exception is within Urban Commercial planning areas (Planning Areas 2, 3 and 5) where such lighting may be located in storefront windows.
- (6) All electrical meter pedestals and light switch/control equipment shall be located with minimum public visibility if possible or shall be screened with appropriate plant materials.

**b. Public Lighting**

- (1) Lighting within the public right-of-way shall adhere to applicable City of Ontario requirements.

**c. Parcel Lighting**

- (1) Parking lots, loading dock areas, pedestrian walkways, building entrances, and public sidewalks shall be illuminated to the level necessary for building operation and security reasons. Dimmers and motion detectors are permitted.
- (2) Along pedestrian movement corridors, the use of low mounted fixtures (ground or bollard height), which reinforce the pedestrian scale, are encouraged.
- (3) Building-mounted lights generally shall be for architectural accent purposes. Building-mounted lights are permissible for general illumination on building façades that are not fronting public roads, so long as the light is directed downward and

concentrated so “spill over” to other properties does not occur.

- (4) Exterior lights should be used to accent entrances, plazas, activity areas, and special features.
- (5) Trees and other landscape features may be illuminated by concealed uplight fixtures. The angle of the uplight should be no more than 25 degrees.

#### **C.4. Signage Guidelines**

Within a large, mixed-use center like MEREDITH INTERNATIONAL CENTRE, signage serves a variety of purposes:

- To identify MEREDITH INTERNATIONAL CENTRE with elements that convey a distinct character;
- To ensure the efficient circulation of vehicle traffic within the site;
- To clearly identify vehicular entry points and to direct vehicles to designated parking areas; and
- To enhance the pedestrian experience through the design of way finding components: directories, directional signage and destination identifiers.

As such, clear, concise and easy-to-understand signage that is also visually appealing is vitally important for positive worker, resident, and visitor experiences at MEREDITH INTERNATIONAL CENTRE. General design requirements for signage within MEREDITH INTERNATIONAL CENTRE are as follows:

- (1) Signage shall be compatible with and complementary to the building's exterior materials, colors and finishes.
- (2) The dimensions and shape of free-standing signs and sign panels or elements mounted on building façades or marquees shall be scaled proportionately to the architecture.
- (3) All signs shall be contained within the parcel to which applicable and shall be so oriented as to preclude hazardous obstructions to person and/or vision of pedestrians and/or vehicle operators.
- (4) Tenant identification signage shall be in keeping with the character established for MEREDITH INTERNATIONAL CENTRE with variations allowed to accommodate individual tenant identities/corporate branding standards.
- (5) All signs are expected to be of the highest quality to pass eye-level examination and scrutiny, and shall comply with the following fabrication specifications:
  - (a) Signs shall be constructed to eliminate burrs, dents, cutting edges and sharp corners;
  - (b) Welds on exposed surfaces be imperceptible in the finished work;
  - (c) Surfaces which are intended to be flat shall be without dents, bulges, oil canning, gaps or other physical deformities;
  - (d) All fasteners shall be concealed;
  - (e) Access panels shall be tight-fitting, light-proof and flush with adjacent surfaces;
  - (f) Manufacturers' recommended fabrication procedures regarding expansion/contraction, fastening and restraining of acrylic plastic shall be followed; and
  - (g) Painted, polished and plated surfaces shall be unblemished in the finished work.
- (6) Prohibited sign components include the following:
  - (a) Letters with exposed fastening and unfinished edges (unless architecturally consistent);
  - (b) Paper, cardboard, Styrofoam or untreated cloth;

Signage



Signage



- (c) Visible moving parts or simulated moving parts by means of fluttering, rotation, or reflecting devices; and
  - (d) Flashing, flickering, blinking, rotating, moving lights, or any other illuminating device that changes light intensity or color, except for parts designed to give public service information such as time, date, temperature, or similar information.
- (7) Illuminated signs shall comply with the following standards:
- (a) All sign elements must be internally and/or externally illuminated;
  - (b) Primary sign, secondary sign (if applicable) and canopy signs shall remain illuminated during business hours after sundown and shall be controlled by a time clock; and
  - (c) All conductors, transformers, cabinets, housing and other equipment shall be concealed and/or incorporated into storefront and/or sign components.
- (8) Signs shall be constructed so as to not have exposed wiring, raceways, ballasts, conduit, transformers, or the like.
- (9) Direction signs shall be located at any vehicular or pedestrian decision point within MEREDITH INTERNATIONAL CENTRE.
- (10) Vehicular direction signs shall comply with the following standards:
- (a) Vehicular direction signs shall clearly direct to destination anchors within MEREDITH INTERNATIONAL CENTRE, on-site parking areas, and/or freeways;
  - (b) Vehicular direction signs shall be consistent in size, shape and design throughout MEREDITH INTERNATIONAL CENTRE;
  - (c) Vehicular signs should have no more than three messages per sign;
  - (d) Typography on vehicular direction signs should be legible and have enough contrast to be read from an appropriate windshield viewing distance; and
  - (e) Vehicular direction signs shall incorporate reflective vinyl copy for night-time illumination.
- (11) All direction signs and general information signs (e.g., restrooms, telephones, fire extinguishers, elevators, escalators, stairs) throughout MEREDITH INTERNATIONAL CENTRE shall incorporate the appropriate identity symbol as established by the Society of Environmental Graphic Design (SEGD) and comply with all state, local and federal regulations.

## **D. INDUSTRIAL DESIGN GUIDELINES**

This section sets forth design guidelines that address considerations unique to the Industrial land use category within MEREDITH INTERNATIONAL CENTRE. These design guidelines shall be implemented by development within Planning Area 1 and shall supplement the *Common Design Guidelines* previously provided in Subsection 6.C.

### **D.1. Architecture Design Guidelines**

#### **a. *Site Planning Guidelines***

Buildings within Industrial areas (Planning Area 1) should be oriented as follows.

- (1) Pedestrian access to buildings shall be designed so that the access is visible from surrounding roadways, sidewalks, and parking areas.
- (2) Elements such as trash enclosure areas, loading bay doors, and service docks shall be oriented or screened to minimize their visibility from East 4th Street, North Vineyard Avenue, and Inland Empire Boulevard
- (3) Site design shall specifically address the needs of pick-up, delivery, and service vehicles related to Industrial land uses.
  - (a) Interior driveways and drive aisles shall be designed to provide adequate stacking and prevent queuing of vehicles on public streets.
  - (b) Service entrances shall be sited such that they do not interfere with owner/tenant/customer access.

- (c) Loading areas shall be designed to provide for backing and maneuvering on-site and not from a public street.
- (d) Appropriate on-site service vehicle parking/turnouts shall be provided in an efficient, non-obtrusive location appropriate to the scale and needs of the development.
- (e) Loading vehicles, when parked, shall not impede normal traffic flow.

#### **b. *Building Form***

The following building form guidelines provide for Industrial development within MEREDITH INTERNATIONAL CENTRE that that complements planned development within the CENTRE.

- (1) Architectural design of Industrial buildings may be utilitarian in form; however, consideration should be given to blending the quality of development and façade elevation into the context of MEREDITH INTERNATIONAL CENTRE design theme.
- (2) Textured forms, reveals, or scoring on concrete tilt-up panels is recommended for visual relief; however, smooth panels with color variation may be used in lieu of textured finishes.

#### **c. *Roof Forms***

Roofs shall be designed for functionality and to complement the overall architectural design of the building. Although primary roof forms are expected to be horizontal and flat, architectural projections are permitted and encouraged.

- (1) Primary roof forms should be flat or gently sloping. The ridge line elevation of the primary roof form should not exceed the parapet wall.

- (2) Arched, gable, hip and shed roof forms shall not be used as a primary roof form, but may be used as a secondary/accent roof form.
- (3) All rooftop mounted equipment shall be screened from public view using materials complementary to those used on the main structure (see Subsection 6.C.1.f).

## **D.2. Landscape Design Guidelines**

### ***a. Land Use Transitions***

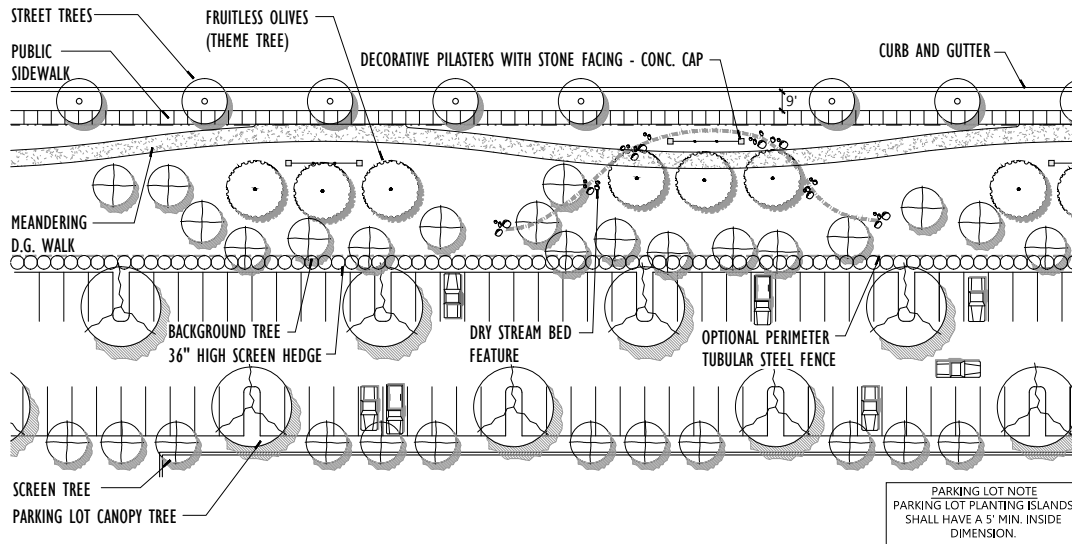
MEREDITH INTERNATIONAL CENTRE utilizes landscape treatments to provide visual and physical buffers between on-site Industrial land uses and various abutting, on- and off-site land uses. Conceptual interface conditions are presented below.

#### **Industrial/East 4th Street Interface**

An enhanced landscaped zone is provided between Industrial land uses in Planning Area 1 and East 4th Street. This enhanced landscaped zone effectively serves as a parkway and is intended to promote pedestrian activity within the public right-of-way. The conceptual enhanced landscape zone may feature a meandering sidewalk, and features such as a dry streambed and flowering vines trained to a cable fence (which features decorative stone pilasters). Landscaping within the enhanced landscape zone may include clusters of theme (fruitless olives), evergreen and coniferous trees, and densely planted colorful shrubs. This conceptual interface condition is illustrated on Figure 6-9, *Industrial/East 4th Street Interface*.

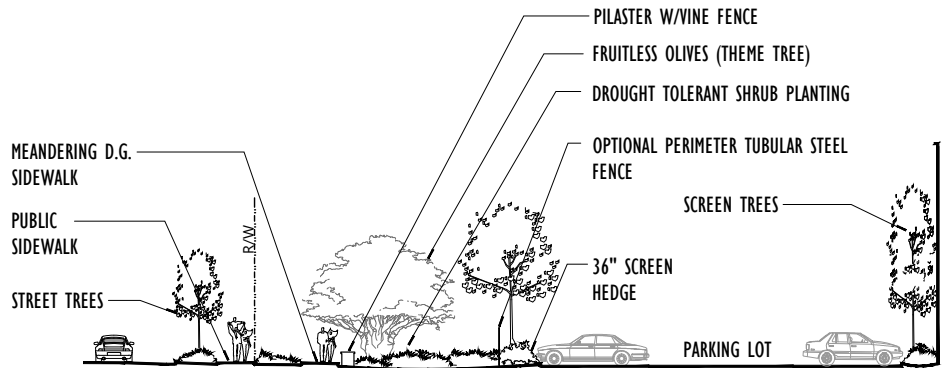
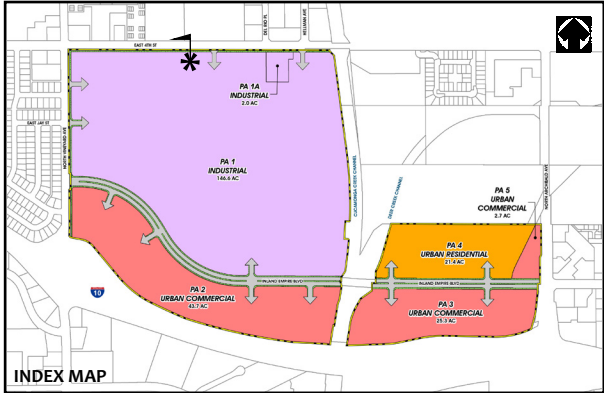
#### **Industrial/Cucamonga Creek Interface**

The Cucamonga Creek abuts the eastern boundary of Planning Area 1. MEREDITH INTERNATIONAL CENTRE provides a fence to physically separate development from the Creek. Shade and screen trees, shrubs, and groundcovers may be planted along the fence to soften the appearance of the fence from within MEREDITH INTERNATIONAL CENTRE and to screen on-site buildings from off-site public viewing areas. Alternatively, a screen wall over 12 feet in height (featuring an articulated exterior wall face) with no landscaping is allowed along this interface. See Figure 6-10, *Industrial/Cucamonga Creek Interface*.



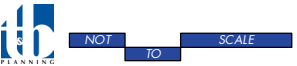
PARKING LOT NOTE  
PARKING LOT PLANTING ISLANDS  
SHALL HAVE A 5' MIN. INSIDE  
DIMENSION.

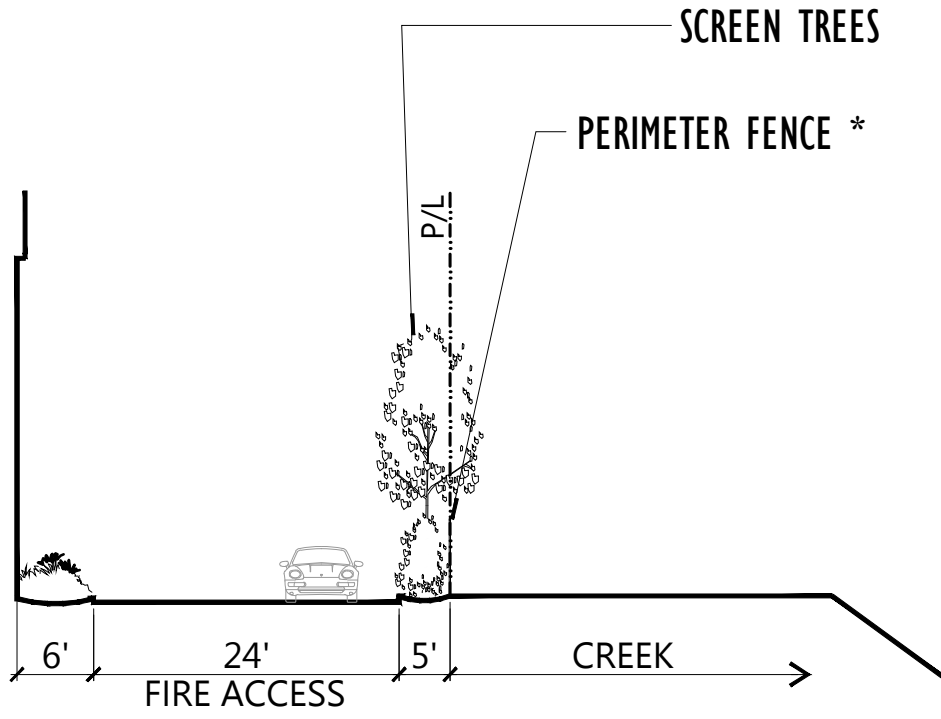
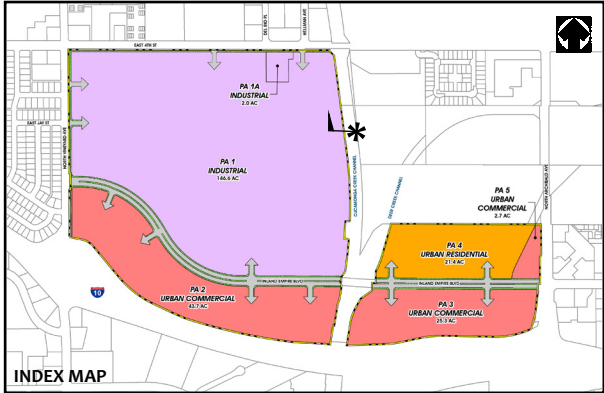
**PLAN VIEW**



**ELEVATION VIEW**

Source: SJA (01-19-2015)





**PA 1/CUCAMONGA CREEK**

\* Alternatively, a screen wall over 12 feet in height with no landscaping is allowed along this interface.

Source: SJA (01-19-2015)



## **E. URBAN COMMERCIAL DESIGN GUIDELINES**

This section sets forth design guidelines that address considerations unique to the Urban Commercial land use category within MEREDITH INTERNATIONAL CENTRE. These design guidelines shall be implemented by development within Planning Areas 2, 3, and 5, and shall supplement the *Common Design Guidelines* previously provided in Subsection 6.C.

### **E.1. Architecture Design Guidelines**

#### **a. *Site Planning Guidelines***

The design of building sites, public spaces, and the pedestrian environmental plays an important role in creating an innovative, exciting Urban Commercial center with a welcoming, positive atmosphere. Building site design influences not only how users experience the center, but also how motorists on nearby roadways view the development.

Additionally, the southern portion of Planning Areas 2 and 3 are visible to motorists on Interstate 10 and special care is required to ensure buildings are visually appealing when viewed from the freeway.

The following site planning design guidelines promote aesthetic quality, efficient use of land, and public safety.

- (1) Gathering places for people should be furnished with appropriate site amenities such as benches, low walls, and shade (trees and/or shade structures).
- (2) Primary building entrances should face off-street parking areas.

- (3) Site design shall specifically address the needs of pick-up, delivery, and service vehicles related to Urban Commercial land uses.
  - (a) Service entrances shall be sited such that they do not interfere with owner/tenant/customer access.
  - (b) Loading areas shall be designed to provide for backing and maneuvering on-site and not from a public street.
  - (c) Appropriate on-site service vehicle parking/turnouts shall be provided in an efficient, non-obtrusive location appropriate to the scale and needs of the development.
  - (d) Loading vehicles, when parked, shall not impede normal traffic flow.
- (4) When possible, individual parcels should make internal connections to adjoining Urban Commercial parcels to encourage walking instead of driving to the same destination.
- (5) Well-defined pedestrian connections shall be provided from parking areas to building entrances.
- (6) Clearly delineated pedestrian paths should be provided from perimeter sidewalks to implementing development.
- (7) Bicycle parking areas shall be located near the building's main entrance.

#### **b. *Building Form***

The following building form guidelines provide for Urban Commercial development within MEREDITH INTERNATIONAL CENTRE that is visually appealing and welcoming.

- (1) Building massing shall consist of a mix of building heights to provide visual interest.

- (2) Building form should be simple and maximize the play of light on mass and void to provide strong contrasts. Blank walls shall be carefully balanced between glass and wall areas.
- (3) Building faces that orient toward North Vineyard Avenue, North Archibald Avenue, Inland Empire Boulevard, and Interstate 10 shall consider their appearance in relation to the public streets and freeway corridor. Free-standing architectural treatment may be used in front of the building walls to create layering.
- (4) Lower building masses, signage, doors, light fixtures, and/or landscape planters should be provided adjacent to pedestrian entries – particularly entries that face North Vineyard Avenue, North Archibald Avenue, Inland Empire Boulevard, and Interstate 10 – and walkways in order to create human-scaled development.

**c. Roof Forms**

Roofs shall be designed for functionality and to complement the overall architectural design of the building. Although primary roof forms are expected to be horizontal and flat, architectural projections are permitted and encouraged.

- (1) Buildings shall incorporate vertical plane breaks, changes in building/ridge height, or other accent roof forms to create visual interest.
- (2) Primary roof forms should be flat or gently sloping. The ridge line elevation of the primary roof form should not exceed the parapet wall.

- (3) Arched, gable, hip and shed roof forms shall not be used as a primary roof form, but may be used as a secondary/accent roof form.
- (4) When used, parapet walls shall incorporate side/rear elevation returns to preclude an unfinished, “false front” appearance.
- (5) Exposed downspouts and overflow holes on building front façades are prohibited.
- (6) All rooftop mounted equipment shall be screened from public view using materials complementary to those used on the main structure (see Subsection 6.C.1.f, *Functional Elements*).

**d. Functional Elements**

**Loading Doors and Service Docks**

- (1) Service and delivery/loading areas shall be separated from visitor, customer, and resident parking areas and pedestrian circulation areas (walkways, pathways, etc.).

**E.2. Landscape Design Guidelines**

**a. Land Use Transitions**

MEREDITH INTERNATIONAL CENTRE utilizes landscape treatments to provide visual and physical buffers between on-site Urban Commercial land uses and various abutting, off-site land uses. The conceptual interface conditions are described below and illustrated on the following pages.

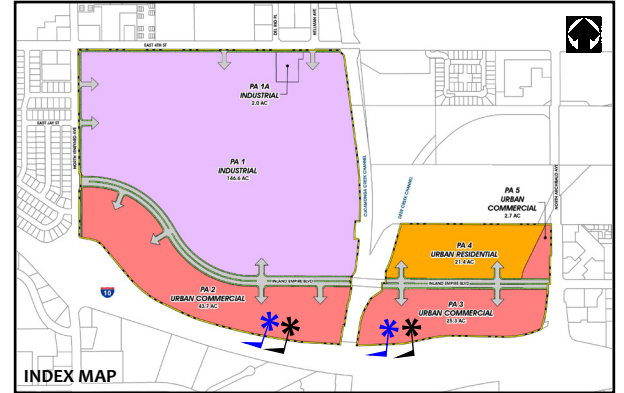
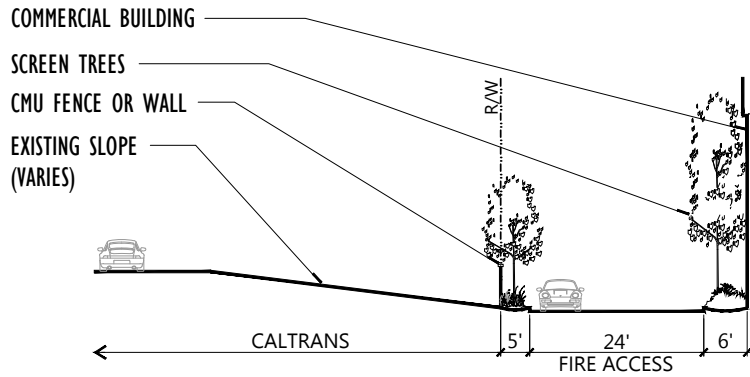
### **Urban Commercial/Interstate 10 Interface**

The southern boundary of MEREDITH INTERNATIONAL CENTRE (Planning Areas 2 and 3) abuts Interstate 10. At this interface, layers of shade trees and shrub masses provide a green buffer between Urban Commercial development and the freeway, and a solid masonry wall or fence separation at MEREDITH INTERNATIONAL CENTRE.

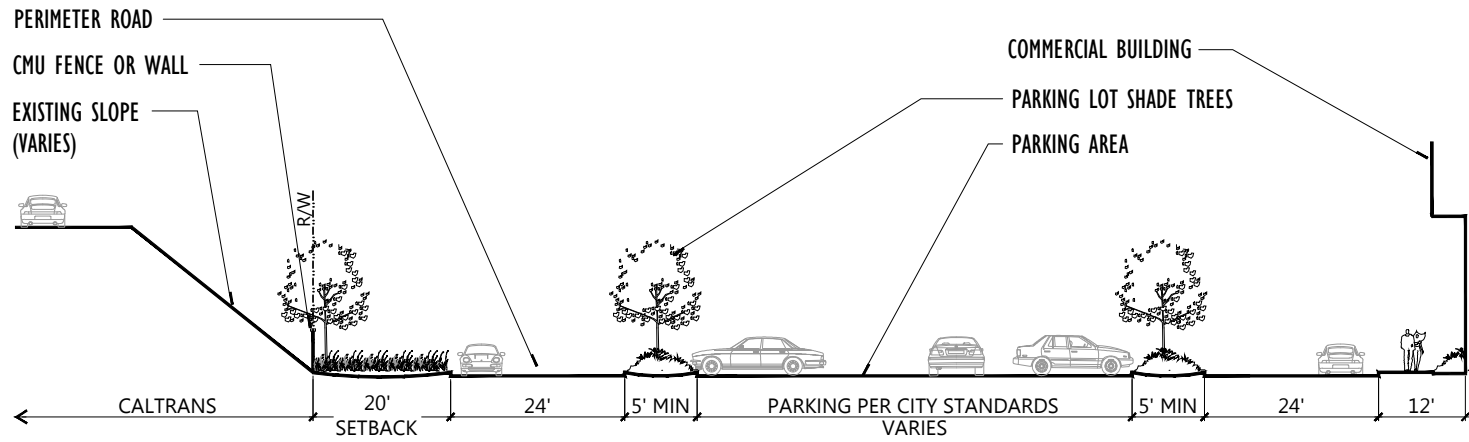
Figure 6-11, *Urban Commercial/Interstate 10 Interface*, illustrates two conceptual design options at the Urban Commercial/Interstate 10 interface: one option where a front building elevation and parking lot are oriented toward the freeway and another option where a side or rear elevation and drive aisle are oriented toward the freeway. Figure 6-11 also illustrates the varying topographic conditions along this interface; along some portions of the interface, Interstate 10 is elevated above MEREDITH INTERNATIONAL CENTRE and along other portions the CENTRE is generally level with the freeway.

### **Urban Commercial/Cucamonga Creek Interface**

The Cucamonga Creek abuts the eastern boundary of Planning Area 2 and the western boundary of Planning Area 3. To protect the character and integrity of this open space area, MEREDITH INTERNATIONAL CENTRE provides a solid masonry wall or fence along this interface to physically separate development from the Creek. Large canopied shade trees and masses and shrubs and groundcovers are planted along the masonry wall or fence to soften the appearance of the wall from within MEREDITH INTERNATIONAL CENTRE. Figure 6-12, *Urban Commercial/Cucamonga Creek Interface*, presents a conceptual illustration of this interface.

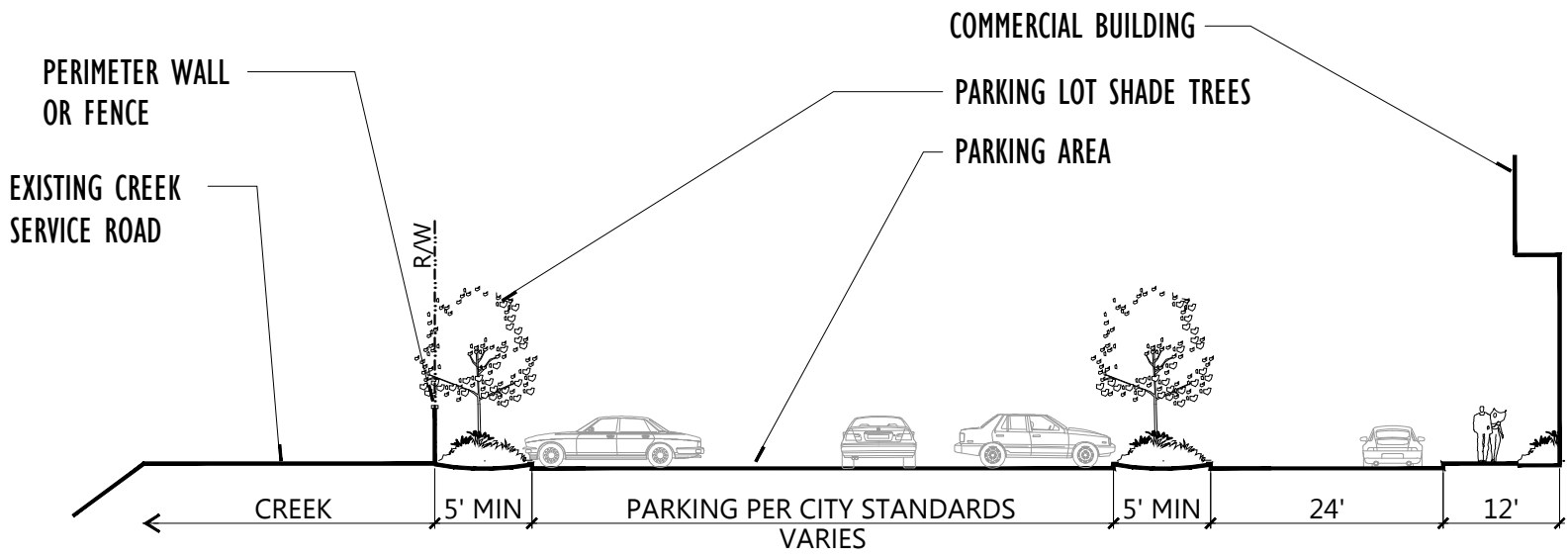
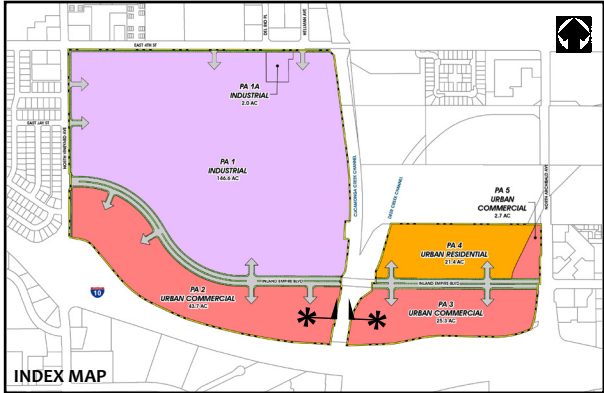


**PA 2 & 3/I-10 INTERFACE**



**PA 2 & 3/I-10 INTERFACE**

Source: SJA (01-19-2015)



**PA 2 & 3/CUCAMONGA CREEK**

Source: SJA (01-19-2015)

## **F. URBAN RESIDENTIAL DESIGN GUIDELINES**

This section sets forth design guidelines that address considerations unique to the Urban Residential land use category within MEREDITH INTERNATIONAL CENTRE. These design guidelines shall be implemented by development within Planning Area 4 and shall supplement the *Common Design Guidelines* previously provided in Subsection 6.C.

### **F.1. Architecture Design Guidelines**

#### **a. *Site Planning Guidelines***

- (1) Site planning should emphasize pedestrian access and connections to public sidewalks, bikeways, the trail adjacent to Cucamonga Creek, and the potential Gold Line LRT.
- (2) Transitional spaces between common areas and private areas such as entry courtyards, private patios, low walls, and porches are encouraged.
- (3) The location of doors and windows should consider indoor/outdoor relationships to create intimate and secure spaces.
- (4) Private outdoor spaces such as balconies and patios should be oriented away from noise-generating uses (e.g., adjacent public streets and Planning Area 5) when possible.

#### **b. *Building Form***

The following building form guidelines provide for Urban Residential development within MEREDITH INTERNATIONAL CENTRE that is visually appealing, street friendly, and pedestrian scale.

- (1) The scale of buildings shall be broken down through the use of varied building massing and forms on a single structure.
- (2) Building details should be proportional to the overall massing.
- (3) Corner buildings should provide massing and details that reflect the visual prominence of this location at a pedestrian scale.
- (4) Vertical and horizontal plane breaks are strongly encouraged on all four sides of buildings as a functional element or detail enhancement. Offset forms are appropriate for changes in materials and/or colors. Blank or unarticulated (uninterrupted) walls are discouraged in areas visible from the public street and/or common areas.
- (5) Building elevations should consider sun orientation by including shaded and sheltered areas.
- (6) Balconies are recommended to break up large wall planes, add human scale to buildings, and provide outdoor living opportunities. Balcony composition should create visual interest and should reinforce the architectural style of the building. Balconies should be designed to screen stored items.

#### **c. *Windows and Doors***

Supplemental window and door guidelines for Urban Residential development within MEREDITH INTERNATIONAL CENTRE are provided below:

- (1) Doors should be protected by porch elements or recessed entries.

- (2) Window details such as shutters, trim surrounds, window boxes and window recesses are encouraged in keeping with the architectural style.

#### **d. Roof Forms**

Roofs shall be designed for functionality and to complement the overall architectural design of the building. The following roof design guidelines shall apply to Urban Residential development within MEREDITH INTERNATIONAL CENTRE.

- (1) Buildings shall incorporate vertical plane breaks, changes in building/ridge height, direction of gables, or other accent roof forms to create visual interest.
- (2) Appropriate forms include traditional hip and gable designs but also include curved roofs, flat roofs with parapet walls, and half gable roofs.
- (3) Exposed downspouts and overflow holes on building front façades shall be discrete and carefully located.
- (4) Rooftop mounted equipment is prohibited unless screened from view at surrounding streets.

## **F.2. Landscape Design Guidelines**

### **a. Land Use Transitions**

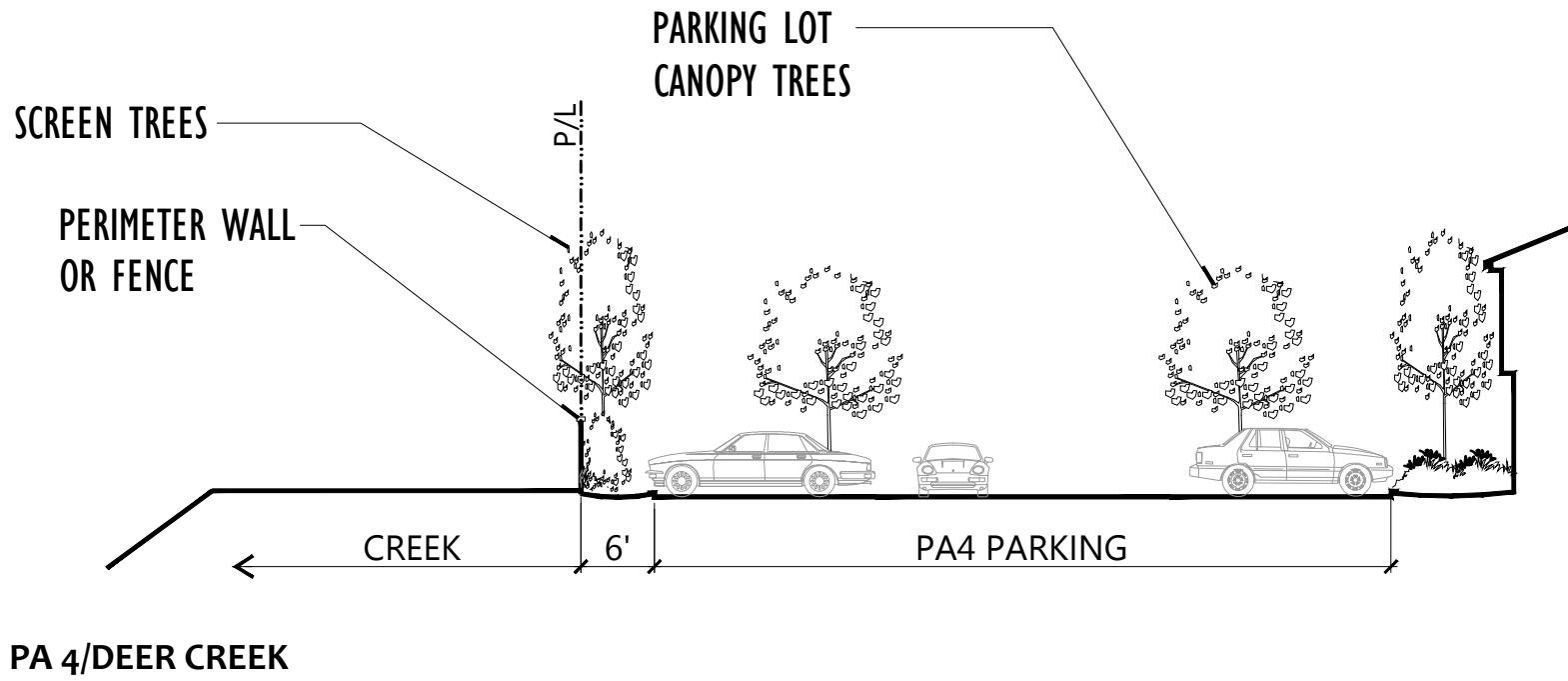
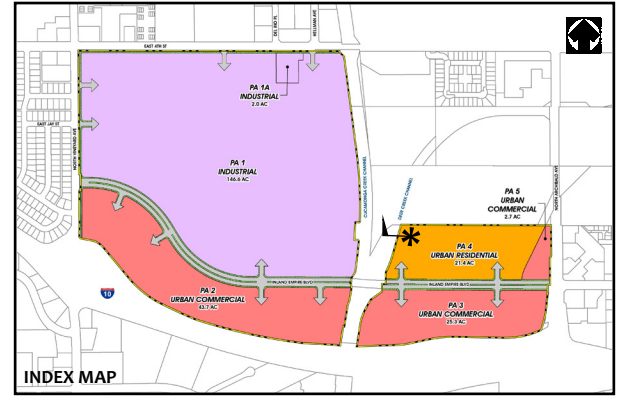
MEREDITH INTERNATIONAL CENTRE utilizes landscape treatments to provide visual and physical buffers between on-site Urban Residential land uses and various abutting, on- and off-site land uses. The conceptual interface conditions are described below and illustrated on the following pages.

### **Urban Residential/Deer Creek Interface**

The western boundary of Planning Area 4 abuts Deer Creek. MEREDITH INTERNATIONAL CENTRE provides a solid masonry wall or fence along this interface to physically separate development from the Creek. Large canopied shade trees and masses and shrubs and groundcovers are planted along the masonry wall or fence to soften the appearance of the wall or fence from residential areas within Planning Area 4. Figure 6-13, *Urban Residential/Deer Creek Interface*, provides a conceptual illustration of this interface.

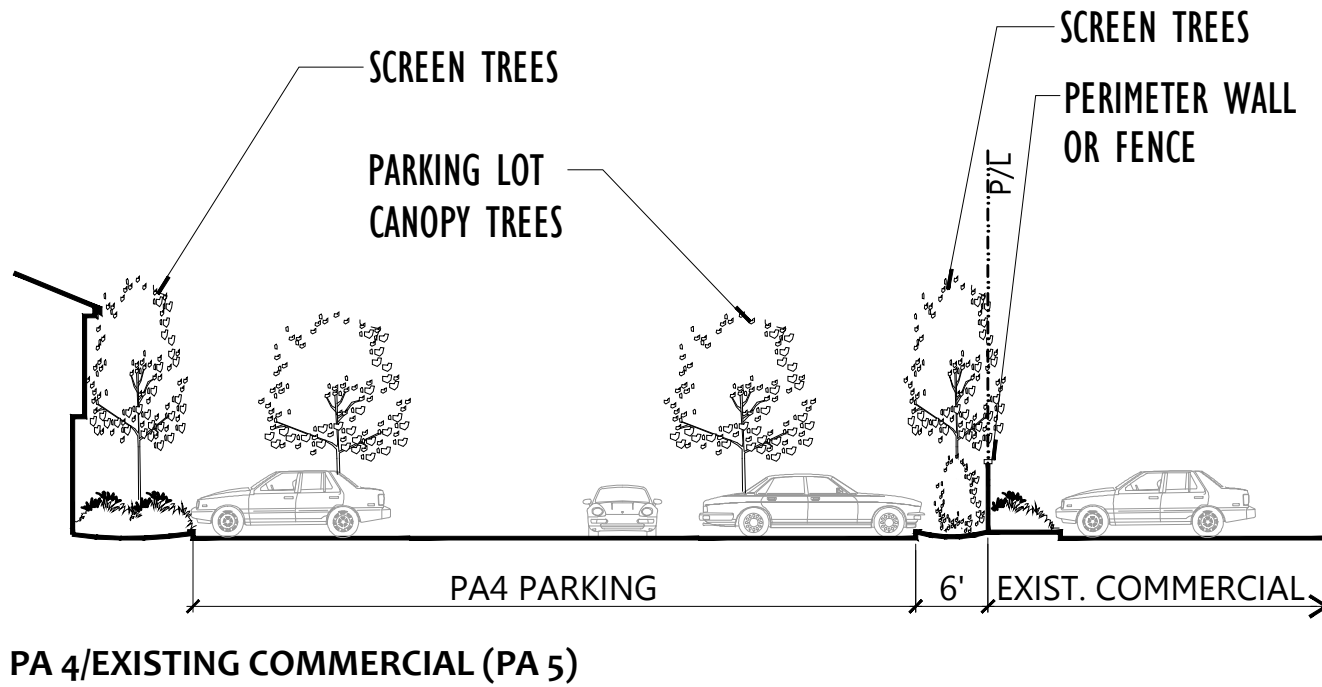
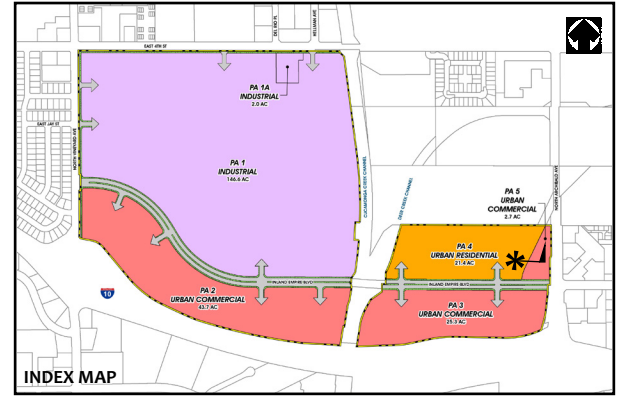
### **Urban Residential/Urban Commercial Interface**

As depicted in Figure 6-14, *Urban Residential/Urban Commercial Interface*, MEREDITH INTERNATIONAL CENTRE provides a solid masonry wall or fence and several layers of landscaping (primarily evergreen and coniferous screen trees) to create a physical barrier between planned residential land uses in Planning Area 4 and existing Urban Commercial land uses in Planning Area 5 and to reduce the visibility of Urban Commercial buildings. The height of the masonry wall or fence shall be adequate to separate residents in Planning Area 4 from routine operations in Planning Area 5. Landscaping, including shrubs/hedges and vines shall be planted on both sides of the masonry wall or fence to soften the appearance.



Source: SJA (01-19-2015)





Source: SJA (01-19-2015)

# Implementation Plan

## Section 7

## IMPLEMENTATION PLAN

Approval of the MEREDITH INTERNATIONAL CENTRE Specific Plan Amendment (2015 Specific Plan Amendment) indicates acceptance by the City of Ontario City Council of a general framework for the development of the MEREDITH INTERNATIONAL CENTRE property. Part of that framework establishes specific development standards that constitute the zoning regulations for the Specific Pan. The provisions contained herein are intended to regulate development within the Specific Plan area.

Development within the MEREDITH INTERNATIONAL CENTRE Specific Plan boundary shall be implemented through the City approval of tentative and final parcel maps and through the Development Plan Review process as established in the City of Ontario Development Code. The implementation process described herein provides the mechanisms for review and approval of development projects within MEREDITH INTERNATIONAL CENTRE.

### A. SEVERABILITY

If any portion of this Specific Plan document is declared to be invalid or ineffective in whole or in part, such decision shall not affect the validity of the remaining portions thereof. The legislative body hereby declares that they would have enacted these regulations and each portion thereof irrespective of the fact that any one or more portions be declared invalid or ineffective.

### B. MINOR MODIFICATIONS TO THE SPECIFIC PLAN

The City recognizes that modifications to the text and exhibits of this document may be needed over time. Upon direction by the City of Ontario Planning Department, certain modifications to text, exhibits, and/or development standards and design guidelines may not require a formal Specific Plan Amendment (i.e. through public hearing). The following minor modifications to this document do not require formal Specific Plan Amendment and are subject to review and approval by the Director of City Planning. The Director of City Planning shall have the discretion to defer to any such request for modification to the Planning Commission or City Council.

- Expansions or reductions of the net acreage covered by a given Planning Area.
- A decrease in development intensity/density (non-residential square footage, lodging rooms, and/or residential units).
- Modification of design criteria such as architectural details, landscape treatments, fencing, lighting, and entry treatments.
- Changes to the Phasing Plan, provided infrastructure is available to serve the phase as determined by the City Engineer.
- Implementation of alternative landscape materials, wall materials, wall alignment, entry monument design, and streetscape design that are generally consistent with the conceptual design guidelines contained within this Specific Plan.

- Modifications to Architectural Design Guidelines, such as variation of materials within a particular architectural style and variations in materials and colors.
- Final infrastructure facility sizing and precise location of dry utilities, water, sewer, and storm drainage improvements when directed by the City Engineer.
- Roadway alignment when the changes are warranted.
- Revisions to exhibits which do not substantially change the intent of the Specific Plan.
- Modification and deletions to the list of permitted and conditional uses.
- Specific modifications of a similar nature to those listed above which are deemed minor by the City Planning Director, which are in keeping with the intent of this Specific Plan and which are in conformance with the City of Ontario General Plan.

### **C. FORMAL AMENDMENTS TO THE SPECIFIC PLAN**

All modifications to this document which do not meet the criteria of a Substantial Conformance as defined in this Section shall be deemed to require a Formal Specific Plan Amendment. This document was prepared pursuant to California Code §65450, et. seq. Amendments shall be processed in accordance with the applicable requirements of the law, which include §65450, et. seq. of the California Government Code.

Formal Specific Plan Amendments shall be subject to the review and approval of the City Council. The Planning Commission should first hear and consider all applications for formal Specific Plan Amendments and provide a recommendation to the City Council. As required by the California Government Code, all government

agencies significantly affected by the proposed Amendment shall be notified of the proposed action prior to the approval. In addition, and as required by the California Environmental Quality Act (CEQA), formal Specific Plan Amendments shall be appropriately reviewed in accordance with the State CEQA Guidelines, with the City of Ontario serving as CEQA Lead Agency.

Any formal Specific Plan Amendment initiated by an applicant require preliminary review by the City Planning Director, filing of an official application and required materials supporting the amendment, submittal of a fee deposit, Planning Commission review and recommendations, and City Council review and final decision.

Amendments also may be initiated by the City Council or Planning Commission by majority vote. All Planning Commission-requested amendments shall be submitted and considered by the City Council and accepted for processing by a majority vote. City staff may initiate an amendment by submitting the requested amendment to the Planning Commission for a vote. Only amendments accepted by a majority vote of the Planning Commission shall be submitted to the City Council for consideration; however, the Applicant shall have the right of appeal to City Council if the Planning Commission fails to approve.

In considering approval or disapproval of formal Specific Plan Amendments, the City Council shall find that the request is:

- Consistent with the Policy Plan (General Plan) in effect at the time of consideration.
- Compatible with surrounding land uses with respect to use, development standards, density, or issues of health, public safety, and general welfare.

- Consistent with the overall design character and general structure of MEREDITH INTERNATIONAL CENTRE as set forth in the Specific Plan Design Guidelines in Section 6 of this document.

## **D. SUBDIVISION MAPS**

Approval of subdivision (parcel) maps may occur concurrently or subsequent to the adoption of the Specific Plan. All tentative and final subdivision maps shall be reviewed and approved pursuant to applicable provisions of the City of Ontario Subdivision Ordinance and consistent with the applicable provisions of the Land Use, Infrastructure, Development Regulations, and Design Guidelines adopted as part of the Specific Plan. As part of the City of Ontario subdivision map review process, the City shall determine if reciprocal access is needed between adjacent parcels.

## **E. DEVELOPMENT PLAN REVIEW**

All development within the MEREDITH INTERNATIONAL CENTRE property shall be subject to the Development Plan Review Process established in Article 8 of the City of Ontario Development Code. Adoption of this document by the City includes adoption of the design guidelines contained in Section 6, which shall provide for the design of development projects. Where the design guidelines are silent, the applicable design guidelines contained within the City's Development Code shall apply. The design guidelines are intended to be flexible in nature while establishing basic evaluation criteria for the review by the City of development projects during design review.

## **F. CONDITIONAL USE PERMITS**

Uses specified as conditionally permitted uses within Section 5, "Development Regulations," of this Specific Plan shall be reviewed

and approved by the City pursuant to the requirements of the City's Development Code, Article 9, "Conditional Use Permits."

## **G. DEVELOPMENT AGREEMENTS**

Approval of a statutory development agreement, authorized pursuant to California Government Code Sections 65864 et seq., may be approved as it is applicable to the entire MEREDITH INTERNATIONAL CENTRE Specific Plan are to a single planning area or collection of planning areas therein. The development agreement may include, but not be limited to, methods for financing, acquisition, and construction of infrastructure, and the provision of economic opportunities for the City of Ontario and employment and/or housing opportunities for community residents.

## **H. MAXIMUM VEHICLE TRIP CAP & SPECIFIC PLAN LAND USE TRIP BUDGET**

MEREDITH INTERNATIONAL CENTRE has a calculated maximum vehicle generation trip cap for Planning Areas 1, 1A, 2, 3, and 4 of 42,057 daily passenger car equivalent (PCE) trips, calculated using the trip rates shown on Table 7-1, *Trip Generation Rates* (using the Institute of Transportation Engineers (ITE) rates, 9<sup>th</sup> Edition) and the PCE conversion rates shown on Table 7-2, *Passenger Car Equivalent Conversion Rates*. A trip cap is not applied to Planning Area 5, because that planning area was built out and existing at the time this document was prepared.

**Table 7-1 Trip Generation Rates**

| Project Description                          | Daily | AM Peak Hour |      |       | PM Peak Hour |      |       |
|--|-------|--------------|------|-------|--------------|------|-------|
|  |       | Enter        | Exit | Total | Enter        | Exit | Total |
| <b>Trip Generation Factors:</b>              |       |              |      |       |              |      |       |
| ▪ 110: General Light Industrial (TE/1000 SF) | 6.97  | 0.81         | 0.11 | 0.92  | 0.12         | 0.85 | 0.97  |
| ▪ 140: Manufacturing (TE/1000 SF)            | 3.82  | 0.57         | 0.16 | 0.73  | 0.26         | 0.47 | 0.73  |
| ▪ 152: High-Cube Warehouse (TE/1000 SF)      | 1.68  | 0.08         | 0.03 | 0.11  | 0.04         | 0.08 | 0.12  |
| ▪ 220: Apartments (TE/DU)                    | 6.65  | 0.10         | 0.41 | 0.51  | 0.40         | 0.22 | 0.62  |
| ▪ 310: Hotel (TE/Room)                       | 8.17  | 0.31         | 0.22 | 0.53  | 0.31         | 0.29 | 0.60  |
| ▪ 710: General Office                        | 11.03 | 1.37         | 0.19 | 1.56  | 0.25         | 1.24 | 1.49  |
| ▪ 820: Shopping Center (TE/1000 SF)          | 42.70 | 0.60         | 0.36 | 0.96  | 1.78         | 1.93 | 3.71  |

**Table 7-2 Passenger Car Equivalent Conversion Rates**

| Project Description  | Daily       | AM Peak Hour |             |             | PM Peak Hour |             |             |
|--|-------------|--------------|-------------|-------------|--------------|-------------|-------------|
|  |             | Enter        | Exit        | Total       | Enter        | Exit        | Total       |
| <b>Trip Generation Factors:</b>                                  |             |              |             |             |              |             |             |
| ▪ <b>110: General Light Industrial - Total (TE/1000 SF)</b>      | <b>6.97</b> | <b>0.81</b>  | <b>0.11</b> | <b>0.92</b> | <b>0.12</b>  | <b>0.85</b> | <b>0.97</b> |
| □ Passenger Cars – 78.6% of Daily (TE/1000 SF)                   | 5.48        | 0.49         | 0.07        | 0.56        | 0.09         | 0.66        | 0.75        |
| □ 2-Axle Trucks – 8.0% Daily/32.70% Peak Hour (TE/1000 SF)       | 0.56        | 0.11         | 0.01        | 0.12        | 0.01         | 0.06        | 0.07        |
| □ 3-Axle Trucks- 3.9% Daily/17.90% Peak Hour (TE/1000 SF)        | 0.27        | 0.05         | 0.01        | 0.06        | 0.00         | 0.04        | 0.04        |
| □ 4+ Axle Trucks – 9.5% Daily/49.40% Peak Hour (TE/1000 SF)      | 0.66        | 0.16         | 0.02        | 0.18        | 0.01         | 0.10        | 0.11        |
| ▪ <b>140: Manufacturing – Total (TE/1000 SF)</b>                 | <b>3.82</b> | <b>0.57</b>  | <b>0.16</b> | <b>0.73</b> | <b>0.26</b>  | <b>0.47</b> | <b>0.73</b> |
| □ Passenger Cars – 61.2% of Daily (TE/1000 SF)                   | 2.34        | 0.31         | 0.09        | 0.40        | 0.17         | 0.29        | 0.46        |
| □ 2-Axle Trucks – 6.1% Daily/11.00 % Peak Hour (TE/1000 SF)      | 0.23        | 0.03         | 0.01        | 0.04        | 0.01         | 0.02        | 0.03        |
| □ 3-Axle Trucks- 12.7% Daily/36.00% Peak Hour (TE/1000 SF)       | 0.49        | 0.09         | 0.03        | 0.12        | 0.04         | 0.06        | 0.10        |
| □ 4+ Axle Trucks – 19.9% Daily/53.00% Peak Hour (TE/1000 SF)     | 0.76        | 0.13         | 0.04        | 0.17        | 0.05         | 0.09        | 0.14        |
| ▪ <b>152: High-Cube Warehouse - Total (TE/1000 SF)</b>           | <b>1.68</b> | <b>0.08</b>  | <b>0.03</b> | <b>0.11</b> | <b>0.04</b>  | <b>0.08</b> | <b>0.12</b> |
| □ Passenger Cars – 79.57% of Daily (TE/1000 SF)                  | 1.34        | 0.05         | 0.02        | 0.07        | 0.02         | 0.05        | 0.07        |
| □ 2-Axle Trucks – 3.46% Daily/16.95% Peak Hour (TE/1000 SF)      | 0.06        | 0.01         | 0.00        | 0.01        | 0.00         | 0.01        | 0.01        |
| □ 3-Axle Trucks- 4.64% Daily/22.71% Peak Hour (TE/1000 SF)       | 0.08        | 0.01         | 0.00        | 0.01        | 0.00         | 0.01        | 0.01        |
| □ 4+ Axle Trucks – 12.33% of Daily/60.34% Peak Hour (TE/1000 SF) | 0.20        | 0.01         | 0.01        | 0.02        | 0.01         | 0.02        | 0.03        |

The list below presents a trip budget for the land uses assumed to be developed in MEREDITH INTERNATIONAL CENTRE. During every Development Plan Review and occupancy permit application review process, the City of Ontario shall review trip generation rates for proposed uses. The total number of PCE average daily trips (ADT) and peak hour trips for any given planning area shall not exceed the values shown on Table 7-3, *Maximum Trip Cap by Planning Area*.

**Table 7-3 Maximum Trip Cap by Planning Area**

| Planning Area    | ADT           | AM PEAK      |            | PM PEAK      |              |
|------------------|---------------|--------------|------------|--------------|--------------|
|                  |               | IN           | OUT        | IN           | OUT          |
| 1 & 1A           | 10,710        | 1,041        | 221        | 204          | 1,047        |
| 2                | 17,262        | 501          | 193        | 524          | 733          |
| 3                | 10,135        | 342          | 156        | 325          | 432          |
| 4                | 5,320         | 80           | 328        | 320          | 176          |
| Internal Capture | -1,370        | -42          | -18        | -42          | -59          |
| <b>TOTAL</b>     | <b>42,057</b> | <b>1,922</b> | <b>880</b> | <b>1,331</b> | <b>2,329</b> |

<sup>1</sup> Passenger car equivalent (PCE) calculated using conversion rates presented in Table 7-1.

The non-residential building square footages, number of overnight lodging rooms, and number of residential units allocated to Planning Areas 1, 1A, 2, 3, and 4 by this Specific Plan (refer to Table 2-1) may be adjusted by the Planning Director provided that the maximum values shown on Table 7-3 are not exceeded. Trip allocations can be exchanged among planning areas, upon mutual written agreement of the planning area owners.

The following procedures shall be followed in association with the review and processing of applications for Site Development Plans and occupancy permits.

- (1) The City of Ontario Planning Department shall maintain a current Trip Budget Allocation form depicting the maximum trip caps for each planning area and the Specific Plan overall. The form shall specify the assumed square footages/number of overnight lodging rooms/number of residential for the land uses allocated by this Specific Plan to each planning area (see Table 2-1). In addition, the form shall specify the associated number of calculated daily vehicle trips (refer to the Traffic Impact Analysis report prepared by LLG for the MEREDITH INTERNATIONAL CENTRE Specific Plan).
- (2) During the Planning Department's review of a Development Plan Review or occupancy permit application, trip generation shall be calculated for the proposed use(s) by a licensed traffic engineer using the trip rates shown in Table 7-1, *Trip Generation Rates*, based on land use type and using ITE rates, 9<sup>th</sup> Edition and the PCE conversion factors listed in Table 7-2, *Passenger Car Equivalent Conversion Rates*. The City of Ontario shall update the Trip Budget Allocation form to if the maximum trip cap for the planning area will be maintained or exceeded.
- (3) If the trip cap for a planning area will not be exceeded, the use may occur. If the trip cap for a planning area will be exceeded, either:
  - (a) The use shall not be permitted;
  - (b) Trips shall be transferred from another planning area within MEREDITH INTERNATIONAL CENTRE, upon the

written approval of both planning area owners and upon the completion of a traffic analysis by a licensed traffic engineer showing that the transfer of trips from one planning area to another would not cause impacts to the transportation system beyond those disclosed in the MEREDITH INTERNATIONAL CENTER Environmental Impact Report;

- (c) A traffic analysis shall be performed by a licensed traffic engineer demonstrating that the increased number of trips for the planning area and the Specific Plan overall will not cause impacts to the transportation system beyond those disclosed in the MEREDITH INTERNATIONAL CENTER Environmental Impact Report; or
- (d) A traffic demand management program (TDM) may be introduced to allow a trip cap increase up to 20% (across the entire Specific Plan area or an individual planning area) if equal offset is demonstrated through implementation of the TDM.

## **I. APPEALS**

Appeals from any determination of the City Planning Director, Zoning Administrator or the Planning Commission, may be made by the applicant or any other aggrieved party by filing an application on forms provided by the City of Ontario and accompanied by the appropriate filing fee, where applicable, within ten (10) days following the final date of action for which an appeal is made. Appeals shall be processed consistent with the provisions of Article 5, "Appeals" of the City of Ontario Development Code.

## **J. INFRASTRUCTURE AND OTHER PUBLIC IMPROVEMENTS**

Each planning area shall be responsible for roadway improvements fronting its individual parcel. In addition, each planning area shall pay its fair share of major infrastructure costs for the Specific Plan area. The major infrastructure costs may be offset by public assistance such as a Community Facility District (CFD). CFDs are established pursuant to the Mello-Roos Community Facilities District Act of 1982, or other special district, to provide funding for the construction of a variety of public facilities and the provision of public services. City Council approval is a prerequisite for the implementation of any and all special district-financing mechanisms. In addition, Development Impact Fee (DIF) credits may be applied for infrastructure completed by developer.

## **K. MAINTENANCE**

The public and private improvements constructed within MEREDITH INTERNATIONAL CENTRE shall be maintained through a combination of public and private entities as described in Table 7-3, *Maintenance Responsibilities*.

Maintenance will be funded by a combination of private and public funds. For common areas located within a planning area, the CC&Rs shall govern the maintenance responsibilities. For areas outside the planning area boundaries, municipal maintenance districts may fund the maintenance of these areas.



Table 7-3 Maintenance Responsibilities

| Facility  | City of Ontario /<br>Community Facilities<br>District | Private Maintenance<br>Association / Homeowners'<br>Association | Public Utility |
|---|---|---|----------------|
| <i>Master Plan of Streets and Highways</i> roadways<br>(North Vineyard Avenue, East 4th Street, North Archibald Avenue,<br>and Inland Empire Boulevard) |   |   |                |
| ➤ Curb-to-curb improvements   | ✓   |   |                |
| ➤ Landscaping within public right-of-way, including medians<br>and parkways   |   | ✓   |                |
| ➤ Sidewalks   | ✓   |   |                |
| Interior Public Streets<br>(East Jay Street and Del Rio Place)  |   |   |                |
| ➤ Curb-to-curb improvements   | ✓   |   |                |
| ➤ Landscaping within public right-of-way, including<br>parkways   |   | ✓   |                |
| ➤ Sidewalks   | ✓   |   |                |
| Private Streets/Alleys/Drive Aisles   |   | ✓   |                |
| Off-Street Parking Areas  |   | ✓   |                |
| Traffic Signals   | ✓   |   |                |
| Traffic Control Signs - in the public right-of-way  | ✓   |   |                |
| Street Lights - in the public right-of-way  | ✓   |   |                |
| Street Lights - not in the public right-of-way  |   | ✓   |                |
| Traffic Control Signs - not in the public right-of-way  |   | ✓   |                |
| Common Open Space   |   | ✓   |                |
| East 4th Street Enhanced Landscape Zone   |   | ✓   |                |
| Corner and Entry Monuments  |   | ✓   |                |
| Walls and Fences  |   | ✓   |                |
| Private Recreation Areas  |   | ✓   |                |
| Storm Water Drainage/Water Quality Facilities - in the public<br>right-of-way   | ✓   |   |                |
| Storm Water Drainage/Water Quality Facilities - not in the public<br>right-of-way   |   | ✓   |                |
| Water Infrastructure - in the public right-of-way   | ✓   |   |                |
| Sanitary Sewer Infrastructure - in the public right-of-way  | ✓   |   |                |
| Dry Utilities (electricity, natural gas, communications systems)  |   |   | ✓              |

## **L. CONCEPTUAL PHASING PLAN**

The development of MEREDITH INTERNATIONAL CENTRE is expected to occur in phases in response to market demands and according to a logical and orderly extension of roadways, public utilities, and infrastructure. Infrastructure to support build out of the Specific Plan shall be phased to support the development within each planning area and in accordance with the development agreement for the 2015 Specific Plan Amendment. A detailed description of the dry utilities, domestic water services, sanitary sewer services, recycled water services, and storm drain facilities, is described in Section 4 of this document.

# **Policy Plan Consistency**

## **Section 8**

## POLICY PLAN CONSISTENCY

California Government Code (Title 7, Division 1, Chapter 3, Article 8, §§ 65450 through 65457) allows local governments to adopt and administer specific plans as tools to implement their general plan; however, specific plans must demonstrate consistency with the goals and policies set forth in the local general plan. This section provides a summary discussion to demonstrate that MEREDITH INTERNATIONAL CENTRE is consistent with, and results in the implementation of, applicable primary goals and policies of the Policy Plan (General Plan) component of *The Ontario Plan*.

| Ontario Plan Policy  | Specific Plan Consistency  |
|--|--|
| <b>Land Use Element</b>  |  |
| <b>Goal LU1:</b> <i>A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.</i> |  |
| <p><u>LU1-1: Strategic Growth.</u> We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.</p>             | <p>The Policy Plan designates the MEREDITH INTERNATIONAL CENTRE property as “Mixed Use” which is described as “an intense mixture of uses, that, when concentrated, create focal points for community activity and identity and facilitate the use of transit” and “Industrial” which is described as a “variety of light industrial uses, including warehousing/distribution, assembly, light manufacturing, research and development, storage, repair facilities, and supporting retail and professional office uses.” The Specific Plan includes a horizontal mixture of Industrial, Urban Commercial, and Urban Residential uses on an under-utilized property surrounded by developed, urban land uses and in close proximity to regional transportation corridors (Interstate 10 and Interstate 15). In addition, the MEREDITH INTERNATIONAL property is located approximately ½-mile north of Ontario International Airport and adjacent to the potential Gold Line LRT corridor. MEREDITH INTERNATIONAL CENTRE will utilize and upgrade, as needed, existing public roadway and utility infrastructure facilities. Accordingly, the MEREDITH INTERNATIONAL CENTRE would assist the City in concentrating growth in strategic locations that create a local identity, maximize available infrastructure, and foster the development of transit.</p> |
| <p><u>LU1-2 Sustainable Community Strategy.</u> We integrate state, regional and local Sustainable Community/Smart Growth principles into the development and entitlement process.</p>                                     | <p>MEREDITH INTERNATIONAL CENTRE features numerous sustainable features. The land use design for MEREDITH INTERNATIONAL CENTRE provides businesses and multi-family residences within walking distance to shopping, goods and services, employment opportunities, and transit. MEREDITH INTERNATIONAL CENTRE also accommodates a Class II Bikeway Corridor along Inland Empire Boulevard in accordance with the Policy Plan Mobility Element, and provides sidewalks and pathways adjacent to roadways to promote pedestrian activity. The Specific Plan area is located adjacent to the potential Gold Line LRT corridor, which would provide a non-vehicular transit option for residents and employees traveling to and from the Specific Plan area. The Industrial structures in Planning Area 1 incorporate solar panels to provide electricity to the industrial buildings, and all primary structures in the Specific Plan area will be designed to achieve the a minimum “Certified” rating under the United States Green Building Council’s Leadership in Energy &amp; Environmental Design (LEED) program. The plant palette for MEREDITH INTERNATIONAL CENTRE is comprised of</p>   |

| Ontario Plan Policy  | Specific Plan Consistency  |
|--|--|
|  | water-efficient species native to Southern California or naturalized to the arid Southern California climate, and the use of turf will be minimized throughout the Specific Plan area. Additionally, green space areas will be thoughtfully graded to provide increased retention/infiltration of storm water runoff. As such, MEREDITH INTERNATIONAL CENTRE is consistent with, and results in the implementation of, this policy.  |
| <p><u>LU1-3 Adequate Capacity.</u> We require adequate infrastructure and services for all development.</p>  | <p>MEREDITH INTERNATIONAL CENTRE provides adequate roadway and utility infrastructure improvements to meet the demands of the Specific Plan while maintaining adequate service levels for existing, surrounding development. Refer to the Environmental Impact Report (EIR) prepared for the MEREDITH INTERNATIONAL CENTRE Specific Plan for a detailed analysis of the adequacy of the Specific Plan's infrastructure improvements.</p>   |
| <p><u>LU1-4 Mobility.</u> We require development and urban design, where appropriate, that reduces reliance on the automobile and capitalizes on multi-modal transportation opportunities.</p>   | <p>MEREDITH INTERNATIONAL CENTRE offers unique opportunities for public transit and non-vehicular circulation. The property is located adjacent to the potential Gold Line LRT corridor. Also, the eastern boundary of the Specific Plan area, North Archibald Avenue, is identified as a bus rapid transit corridor by the Policy Plan. Additionally, MEREDITH INTERNATIONAL CENTRE provides a Class II Bikeway along Inland Empire Boulevard, as well as sidewalks and pathways to promote non-vehicular transportation.</p> |
| <p><u>LU1-6 Complete Community.</u> We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario.</p> | <p>MEREDITH INTERNATIONAL CENTRE provides a mixture of employment, service, and residential land uses arranged in a sensible and efficient manner that allow ease of access and complement the surrounding community. As such, MEREDITH INTERNATIONAL CENTRE is consistent with, and results in the implementation of, this policy.</p>  |
| <p><u>LU1-7 Revenues and Costs.</u> We require future amendments to our Land Use Plan to be accompanied by analyses of fiscal impacts.</p>   | <p>A fiscal impact analysis was prepared in support of the MEREDITH INTERNATIONAL CENTRE. Refer to the MEREDITH INTERNATIONAL CENTRE Specific Plan Amendment EIR for a detailed analysis of potential fiscal effects associated with implementation of the Specific Plan.</p>  |

| Ontario Plan Policy  | Specific Plan Consistency   |
|--|---|
| <b><i>Goal LU2: Compatibility between wide ranges of uses.</i></b>   |   |
| <u>LU2-2 Buffers.</u> We require new uses to provide mitigation or buffers between existing uses where potential adverse impacts could occur.        | The MEREDITH INTERNATIONAL CENTRE provides landscaping along East 4th Street and North Vineyard Avenue to provide a visual buffer between planned on-site Industrial and Urban Commercial land uses and existing off-site residential land uses. Furthermore, MEREDITH INTERNATIONAL CENTRE provides a buffer along the Specific Plan property's interface with Interstate 10 to minimize adverse aesthetic and noise effects of the freeway. As such, MEREDITH INTERNATIONAL CENTRE is consistent with, and results in the implementation of, this policy.   |
| <u>LU2-6 Infrastructure Compatibility.</u> We require infrastructure to be aesthetically pleasing and in context with the community character.       | Perimeter and interior streets will be landscaped with a combination of evergreen and deciduous trees – including flowering varieties – shrubs and groundcovers in an aesthetically pleasing manner to establish the MEREDITH INTERNATIONAL CENTRE design theme and to complement existing surrounding development. Additionally, the Specific Plan will locate utility connections, utility cabinets, etc. in areas not visible from publically accessible areas where feasible, or, in instances where utility connections or utility cabinets must be placed in areas that are visible from publically accessible areas, the Specific Plan will provide screening and/or landscaping to minimize views of the infrastructure improvements. |
| <b><i>Goal LU4: Development that provides short-term value only when the opportunity to achieve our Vision can be preserved.</i></b>                 |   |
| <u>LU4-3 Infrastructure Timing.</u> We require that the necessary infrastructure and services be in place prior to or concurrently with development. | MEREDITH INTERNATIONAL CENTRE will improve roadways, public utilities, and infrastructure in a logical sequence in accordance with future development of the Specific Plan and as required by the City of Ontario and applicable service providers. Improvements will be provided as necessary to serve the MEREDITH INTERNATIONAL CENTRE property and ensure that adequate vehicular access, facilities, and infrastructure are available as needed for public health and safety. MEREDITH INTERNATIONAL CENTRE is consistent with Policy LU4-3.   |
| <b><i>Goal LU5: Integrated airport systems and facilities that minimize negative impacts to the community and maximize economic benefits.</i></b>    |   |
| <u>LU5-5 Airport Compatibility Planning for ONT.</u> We create and maintain the Airport Land Use Compatibility Plan for ONT.                         | The land uses provided by the MEREDITH INTERNATIONAL CENTRE Specific Plan are consistent with the Airport Land Use Compatibility Plan (ALUCP) for Ontario International Airport. Furthermore, future development on the Specific Plan property would be required to comply with the development standards and design guidelines established in this Specific Plan, as well as the applicable requirements of the City of Ontario Development Code, which would preclude any potential inconsistencies with  |
| <u>LU5-7 ALUCP Consistency with Land Use Regulations.</u> We comply with state law that requires general plans,                                      |   |

| Ontario Plan Policy  | Specific Plan Consistency   |
|--|---|
| specific plans and all new development be consistent with the policies and criteria set forth within an Airport Land Use Compatibility Plan for any public use airport.  | the Ontario International Airport ALUCP. Therefore, MEREDITH INTERNATIONAL CENTRE is consistent with the Policy Plan policy.  |
| <b>Community Design Element</b>  |   |
| <i><b>Goal CD1:</b> A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.</i> |   |
| <u>CD1-2 Growth Areas.</u> We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.   | This Specific Plan includes detailed architectural and landscape design guidelines (refer to Section 6) that address all aspects of land development, including site design, architectural design, landscape materials, monuments/entries, signage and lighting to ensure future development within the Specific Plan is aesthetically pleasing, cohesive, and distinctive, yet also complements the existing surrounding development. Accordingly, MEREDITH INTERNATIONAL CENTRE is consistent with this policy.   |
| <u>CD1-3 Neighborhood Improvement.</u> We require viable existing residential and non-residential neighborhoods to be preserved, protected and enhanced in accordance with our land use policies.                | MEREDITH INTERNATIONAL CENTRE is designed to protect the integrity of nearby, existing residential land uses. Landscape buffers provided along North Vineyard Avenue and East 4th Street screen off-site residential areas from planned on-site Industrial and Urban Commercial land uses, and provide additional physical separation between these uses. Additionally, buildings would be sited to minimize adverse effects to nearby residential uses. For example, loading docks would be completely screened by a combination of walls and landscaping and exterior lighting fixtures would be focused on the Specific Plan property and focused/shielded to prevent light trespass on adjacent properties. The design features provided by MEREDITH INTERNATIONAL CENTRE to prevent “edge effects” with surrounding land uses are fully listed in Section 5, <i>Development Standards</i> , and Section 6, <i>Design Guidelines</i> , of this Specific Plan. Accordingly, MEREDITH INTERNATIONAL CENTRE will preserve and protect surrounding residential neighborhoods. |



| Ontario Plan Policy   | Specific Plan Consistency   |
|---|---|
| <p><u>CD1-4 Transportation Corridors.</u> We will enhance our major transportation corridors within the City through landscape, hardscape, signage and lighting.</p>  | <p>MEREDITH INTERNATIONAL CENTRE will install landscaping – including evergreen and deciduous trees, low shrubs, and groundcovers – along perimeter and interior streets concurrent with development. As part of improvements planned by this Specific Plan, MEREDITH INTERNATIONAL CENTRE will provide an expansive landscape buffer along the southern edge of East 4th Street. This landscape buffer includes a meandering decomposed granite trail, landscaping, and thematic architectural features (e.g., rail fencing trained with vines, a dry creek bed) and functions as a linear parkway. MEREDITH INTERNATIONAL CENTRE also provides a series of monument and entry treatments, which incorporate architectural features (e.g., battered stone pillars, monument signs) and landscaping to welcome employees, residents, and visitors and establish the Specific Plan’s design theme. MEREDITH INTERNATIONAL CENTRE is consistent with, and results in the implementation of, Policy CD1-4.</p> |
| <p><u>CD1-5 View Corridors.</u> We require all major north-south streets be designed and redeveloped to feature views of the San Gabriel Mountains, which are part of the City’s visual identity and a key to geographic orientation. Such views should be free of visual clutter, including billboards and may be enhanced by framing with trees.</p>  | <p>MEREDITH INTERNATIONAL CENTRE does not include any design components that would detract from views of the San Gabriel Mountains from major north-south street corridors. As part of build-out of this Specific Plan, landscaping – including trees – would be planted along major north-south streets (North Vineyard Avenue and North Archibald Avenue) in conjunction with improvements to these roadways. As such, MEREDITH INTERNATIONAL CENTRE is consistent with, and results in the implementation of, this policy.</p>   |
| <p><b><i>Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.</i></b></p>  |   |
| <p><u>CD2-1 Quality Architecture.</u> We encourage all development projects to convey visual interest and character through:</p> <ul style="list-style-type: none"> <li>➤ building volume, massing, and height to provide appropriate scale and proportion;</li> <li>➤ a true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and</li> <li>➤ exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.</li> </ul> | <p>Future development of the MEREDITH INTERNATIONAL CENTRE will be guided by the Specific Plan’s <i>Design Guidelines</i>, which include comprehensive architectural criteria that provide for the development of an attractive, contemporary mixed-use center. The <i>Design Guidelines</i> specifically address architectural style, building form (shape, mass, scale, proportion, articulation), and building materials, colors, and textures to ensure that development is visually appealing and inviting to pedestrians and motorists. MEREDITH INTERNATIONAL CENTRE’s design theme complements the City of Ontario’s character and would not conflict with Policy CD2-1.</p>  |

| Ontario Plan Policy  | Specific Plan Consistency  |
|--|--|
| <p><u>CD2-2 Neighborhood Design.</u> We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:</p> <ul style="list-style-type: none"> <li>➤ a pattern of smaller, walkable blocks that promote access, activity and safety;</li> <li>➤ variable setbacks and parcel sizes to accommodate a diversity of housing types;</li> <li>➤ traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;</li> <li>➤ floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and</li> <li>➤ landscaped parkways, with sidewalks separated from the curb.</li> </ul> | <p>The design of residential component of the MEREDITH INTERNATIONAL CENTRE will include common open space areas to promote social interaction and will emphasize pedestrian access and connections to public sidewalks, bikeways, and the potential Gold Line LRT. Residential buildings will locate windows maximize view opportunities, while also providing intimate and secure living spaces. Landscaping, including trees, shrubs, and groundcovers, will be planted along all exterior and interior streets. MEREDITH INTERNATIONAL CENTRE is consistent with Policy CD2-2.</p>   |
| <p><u>CD2-3 Commercial Centers.</u> We desire commercial centers to be distinctive, pedestrian friendly, functional and vibrant with a range of businesses, places to gather, and connectivity to the neighborhoods they serve.</p>  | <p>The Urban Commercial component of the MEREDITH INTERNATIONAL CENTRE places an emphasis on aesthetic quality and efficient use of land to create a welcoming, positive atmosphere. The MEREDITH INTERNATIONAL CENTRE <i>Design Guidelines</i> encourage future development to provide gathering places furnished with site amenities (benches, low walls, landscaping, shade structures) and well-defined pathways and connections to encourage pedestrian and bicycle activity. The MEREDITH INTERNATIONAL CENTRE <i>Design Guidelines</i> also establish guidelines to ensure the functionality of Urban Commercial buildings (needs of pick-up, delivery, and service vehicles) while precluding potential conflicts with automobile traffic and pedestrians. Accordingly, MEREDITH INTERNATIONAL CENTRE is consistent with Policy CD2-3.</p> |

| Ontario Plan Policy  | Specific Plan Consistency   |
|--|---|
| <p><u>CD2-4 Mixed Use, Urban Office and Transit Serving Areas.</u> We require mixed use, urban office and transit serving areas to be designed and developed as pedestrian oriented “villages” that promote a vibrant, comfortable and functional environment.</p>   | <p>The MEREDITH INTERNATIONAL CENTRE Specific Plan provides for the development of an innovative mixed-use center that features Industrial, Urban Commercial, and Urban Residential land uses that complement existing surrounding land uses and provide employment, service, and shopping opportunities in close walking distance residential land uses and transit opportunities. Therefore, MEREDITH INTERNATIONAL CENTRE implements this Policy Plan policy.</p>  |
| <p><u>CD2-5 Streetscapes.</u> We design new and, when necessary, retrofit existing streets to improve walkability, bicycling and transit integration, strengthen connectivity, and enhance community identity through improvements to the public right of way such as sidewalks, street trees, parkways, curbs, street lighting and street furniture.</p>  | <p>As part of implementation of the MEREDITH INTERNATIONAL CENTRE, existing public streets (East 4th Street, North Vineyard Avenue, North Archibald Avenue, Inland Empire Boulevard) will be retrofitted, as necessary, with new travel lanes, medians, bicycle lanes, and sidewalks to ensure safe vehicular and non-vehicular transportation. In addition, MEREDITH INTERNATIONAL CENTRE will provide landscaping (tree, groundcovers, etc.) along all exterior street frontages and along interior streets to establish the Specific Plan’s design theme and create a welcoming visual environment for employees, residents, and guests. MEREDITH INTERNATIONAL CENTRE is consistent with, and results in the implementation of, Policy CD2-5.</p>   |
| <p><u>CD2-7 Sustainability.</u> We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.</p> | <p>By nature of its location near existing residential land uses and regional transportation corridors and its planned distribution of employment, service, retail, and residential land uses to serve the surrounding community and region, the MEREDITH INTERNATIONAL CENTRE is consistent with sustainable, “smart growth” principles. Furthermore, Industrial structures in Planning Area 1 incorporate solar panels to provide electricity to the industrial buildings, and all primary structures in the Specific Plan area will be designed to achieve the a minimum “Certified” rating under the United States Green Building Council’s Leadership in Energy &amp; Environmental Design (LEED) program to minimize energy demand. Also, the plant palette for MEREDITH INTERNATIONAL CENTRE is comprised of drought-tolerant plant species native to Southern California or naturalized to the arid Southern California climate, and the use of turf will be minimized throughout the Specific Plan area, to minimize water use. MEREDITH INTERNATIONAL CENTRE is consistent with Policy CD2-7.</p> |

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| <p><u>CD2-8 Safe Design.</u> We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting</p> | <p>The MEREDITH INTERNATIONAL CENTRE requires all sidewalks, pathways, parking lots, building entrances, and other facilities that may be used by pedestrians to be visible to passersby and well illuminated. MEREDITH INTERNATIONAL CENTRE would be consistent with this policy.</p>   |
| <p><u>CD2-9 Landscape Design.</u> We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.</p>   | <p>The landscape concept for MEREDITH INTERNATIONAL CENTRE incorporates the use of attractive, durable landscaping materials, and an irrigation system designed to keep plant materials in good health while conserving water. Landscaping will be provided throughout MEREDITH INTERNATIONAL CENTRE, including along roadways, at monuments/entries, within common open space areas, and adjacent to buildings. MEREDITH INTERNATIONAL CENTRE results in the implementation of Policy CD2-9.</p>  |
| <p><u>CD2-10 Surface Parking Areas.</u> We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.</p>              | <p>Parking lots within the MEREDITH INTERNATIONAL CENTRE will be designed and constructed in accordance with the requirements of the City's Development Code, and will include landscaping, lighting, and well-defined drive aisles and parking spaces. MEREDITH INTERNATIONAL CENTRE is consistent with this policy.</p>  |
| <p><u>CD2-11 Entry Statements.</u> We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.</p>  | <p>MEREDITH INTERNATIONAL CENTRE provides a three-tiered hierarchy of entry monuments and corner treatments to identify the CENTRE and distinguish individual planning areas, in conformance with the intent of Policy CD2-11. The entry monuments and corner treatments, which are illustrated in Section 6, <i>Design Guidelines</i>, incorporate landscaping and architectural features (e.g., monument walls, battered stone pillars) to provide attractive and distinctive visual statements.</p>   |
| <p><u>CD2-12 Site and Building Signage.</u> We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.</p>   | <p>The <i>Design Guidelines</i> for MEREDITH INTERNATIONAL CENTRE (refer to Section 6 of this Specific Plan) include comprehensive signage guidelines to ensure that future development will construct clear, concise, easy-to-read signs that reflect and complement the Specific Plan's design theme, provide for safe and efficient circulation of vehicle traffic, and facilitate pedestrian travel. Signs will be of high-quality and the use of distracting sign elements, such as flashing lights or moving parts, is prohibited. MEREDITH INTERNATIONAL CENTRE is consistent with Policy CD2-12.</p> |

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| <p><b>Goal CD3:</b> <i>Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.</i></p> |  |
| <p><u>CD3-1 Design.</u> We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.</p>   | <p>MEREDITH INTERNATIONAL CENTRE provides a coordinated, interconnected circulation network for vehicles, bicycles, and pedestrians. All perimeter and interior streets, including sidewalks within parkways, will be improved as illustrated in Section 3, <i>Circulation Plan</i>, and Section 6, <i>Design Guidelines</i>, of this Specific Plan and in accordance with City standards. Future development also will be required to comply with City standards related to the location of landscape plantings to ensure that adequate sight lines are provided for motorists and pedestrians. MEREDITH INTERNATIONAL CENTRE is consistent with this policy.</p> |
| <p><u>CD3-2 Connectivity between Streets, Sidewalks, Walkways and Plazas.</u> We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.</p>  | <p>The MEREDITH INTERNATIONAL CENTRE <i>Design Guidelines</i> establish site planning and landscaping measures to provide efficient, well-defined pedestrian connections that follow a cohesive design theme. As such, the MEREDITH INTERNATIONAL CENTRE Specific Plan is consistent with Policy CD3-2.</p>  |
| <p><u>CD3-6 Landscaping.</u> We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.</p>  | <p>MEREDITH INTERNATIONAL CENTRE utilizes landscaping to establish an attractive, cohesive design theme, as a focal point at entrance monuments and corner treatments, to buffer on-site land from off-site land uses and transportation facilities (i.e., Interstate 10), and to screen objectionable views from public views. Specific landscape design concepts for MEREDITH INTERNATIONAL CENTRE are described and illustrated in Section 6, <i>Design Guidelines</i>, of this Specific Plan. MEREDITH INTERNATIONAL CENTRE is consistent with Policy CD3-6.</p>   |
| <p><b>Goal CD5:</b> <i>A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.</i></p>   |  |
| <p><u>CD5-1 Maintenance of Buildings and Property.</u> We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.</p>  | <p>This Specific Plan defines the entities responsible for maintenance of publically and privately-owned improvements within the MEREDITH INTERNATIONAL CENTRE, including roadways and utility infrastructure. Compliance with the maintenance responsibility matrix established in Section 7, <i>Implementation</i>, will ensure that all improvements within the Specific Plan area are properly and consistently maintained.</p>  |
| <p><u>CD5-2 Maintenance of Infrastructure.</u> We require the continual maintenance of infrastructure.</p>  | <p>MEREDITH INTERNATIONAL CENTRE is consistent with Policy Plan Policies CD5-1 and CD5-2.</p>  |

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| <b>Mobility Element</b>  |   |
| <b>Goal M1:</b> <i>A system of roadways that meets the mobility needs of a dynamic and prosperous Ontario.</i>   |   |
| <p><b>M1-1 Roadway Design and Maintenance.</b> We require our roadways to:</p> <ul style="list-style-type: none"> <li>➤ Comply with federal, state and local design and safety standards.</li> <li>➤ Meet the needs of multiple transportation modes and users.</li> <li>➤ Handle the capacity envisioned in the Functional Roadway Classification Plan.</li> <li>➤ Maintain a peak hour Level of Service (LOS) E or better at all intersections.</li> <li>➤ Be compatible with the streetscape and surrounding land uses.</li> <li>➤ Be maintained in accordance with best practices and our Right-of-Way Management Plan.</li> </ul> | <p>MEREDITH INTERNATIONAL CENTRE will improve all existing perimeter streets and new internal streets in accordance with the City's <i>Master Plan of Streets and Highways</i> and City design standards, and would comply with the San Bernardino County Municipal Separate Storm Sewer System (MS4) Permit and Water Quality Management Plan. As described in Section 3, <i>Circulation Plan</i>, of this Specific Plan, MEREDITH INTERNATIONAL CENTRE includes roadway, bikeway, and sidewalk/pathway improvements to facilitate efficient vehicular and non-vehicular transportation through and around the Specific Plan area. The analysis included in the MEREDITH INTERNATIONAL CENTRE Specific Plan Amendment EIR demonstrates that the Specific Plan's roadway network is designed to operate at acceptable levels of service upon full build-out of the Specific Plan area. All public roadways will be maintained in accordance with City requirements. Based on the foregoing, MEREDITH INTERNATIONAL CENTRE is consistent with this policy.</p> |
| <p><b>M1-2 Mitigation of Impacts.</b> We require development to mitigate its traffic impacts.</p>  | <p>The Specific Plan will not preclude the implementation of applicable mitigation measures identified in the MEREDITH INTERNATIONAL CENTRE Specific Plan Amendment EIR.</p>  |

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| <p><b>Goal M2:</b> <i>A system of trails and corridors that facilitate and encourage bicycling and walking.</i></p>   |   |
| <p><u>M2-1 Bikeway Plan.</u> We maintain our Multipurpose Trails &amp; Bikeway Corridor Plan to create a comprehensive system of on- and off-street bikeways that connect residential areas, businesses, schools, parks, and other key destination points.</p> <p><u>M2-2 Bicycle System.</u> We provide off-street multipurpose trails and Class II bikeways as our primary paths of travel and use the Class III for connectivity in constrained circumstances.</p> | <p>MEREDITH INTERNATIONAL CENTRE includes the construction of a Class II Bikeway along Inland Empire Boulevard, in conformance with the City's <i>Multipurpose Trails &amp; Bikeway Corridor Plan</i>. Also, MEREDITH INTERNATIONAL CENTRE Specific Plan would not preclude implementation of the City's planned Cucamonga Creek Multipurpose Trail. Accordingly, the MEREDITH INTERNATIONAL CENTRE results in the implementation of Policy M2-1.</p>   |
| <p><u>M2-3 Pedestrian Walkways.</u> We require walkways that promote safe and convenient travel between residential areas, businesses, schools, parks, recreation areas, and other key destination points.</p>  | <p>MEREDITH INTERNATIONAL CENTRE provides sidewalks along all perimeter and internal public streets to facilitate safe and convenient pedestrian travel to the Specific Plan area and between the planned Industrial, Urban Commercial, and Urban Residential land uses. Additionally, a meandering, decomposed granite pathway will be provided along East 4th Street. All sidewalks and pathways will be constructed in accordance with City standards, and landscaping will be spaced to provide motorists and pedestrians with adequate sight lines to promote safe travel. MEREDITH INTERNATIONAL CENTRE is consistent with this policy.</p> |
| <p><b>Housing Element</b></p>   |   |
| <p><b>Goal H2:</b> <i>Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.</i></p>   |   |
| <p><u>H2-1 Corridor Housing.</u> We revitalize transportation corridors by encouraging the production of higher density residential and mixed-uses that are architecturally, functionally, and aesthetically suited to corridors.</p>   | <p>The MEREDITH INTERNATIONAL CENTRE property is located in in close proximity to regional transportation corridors (Interstate 10 and Interstate 15) and adjacent to the potential Gold Line LRT corridor. MEREDITH INTERNATIONAL CENTRE provides high-density, multi-family housing (minimum 25 dwelling units per acre), as well as a mixture of employment, service, and residential land uses, arranged in a sensible and efficient manner that allow ease of access and complement the surrounding community. MEREDITH INTERNATIONAL CENTRE is consistent with this policy.</p>   |

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| <p><u>H2-3 Ontario Airport Metro Center.</u> We foster vibrant, urban, intense and highly amenitized community in the Ontario Airport Metro Center Area through a mix of residential, entertainment, retail and office-oriented uses.</p> | <p>The MEREDITH INTERNATIONAL CENTRE Specific Plan provides for the development of an innovative mixed-use center that features Industrial, Urban Commercial, and Urban Residential land uses that complement existing surrounding land uses and provide employment, service, and shopping opportunities in close walking distance residential land uses and transit opportunities. MEREDITH INTERNATIONAL CENTRE implements this Policy Plan policy.</p>  |
| <p><u>H2-5 Housing Design.</u> We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.</p>                             | <p>Future development of the MEREDITH INTERNATIONAL CENTRE will be guided by the Specific Plan’s Design Guidelines, which include comprehensive architectural criteria that provide for the development of an attractive, contemporary mixed-use center. The Design Guidelines specifically address architectural style, building form (shape, mass, scale, proportion, articulation), and building materials, colors, and textures to ensure that development is visually appealing and inviting to pedestrians and motorists. Additionally, the Specific Plan incorporates numerous design features maximize the environmental sustainability of future development. MEREDITH INTERNATIONAL CENTRE is consistent with Policy H2-5.</p> |
| <p><b>Environmental Resources Element</b></p>   |  |
| <p><b><i>Goal ER1: A reliable and cost effective system that permits the City to manage its diverse water resources and needs.</i></b></p>  |  |
| <p><u>ER1-3 Conservation.</u> We require conservation strategies that reduce water usage.</p>   | <p>MEREDITH INTERNATIONAL CENTRE incorporates a drought-tolerant plant palette and water-efficient irrigation system design to minimize the water demands of planned development. In addition, future construction will be required to comply with the water-efficiency mandates of the Building Code (Title 24), including the provision of water-efficient fixtures. Accordingly, the MEREDITH INTERNATIONAL Specific Plan incorporates water conservation strategies and is consistent with Policy ER1-3.</p>   |



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| <p><u>ER1-5 Groundwater Management.</u> We protect groundwater quality by incorporating strategies that prevent pollution, require remediation where necessary, capture and treat urban run-off, and recharge the aquifer.</p> <p><u>ER1-6 Urban Run-off Quantity.</u> We encourage the use of low impact development strategies to intercept run-off, slow the discharge rate, increase infiltration and ultimately reduce discharge volumes to traditional storm drain systems.</p> <p><u>ER1-7 Urban Run-off Quality.</u> We require the control and management of urban run-off, consistent with Regional Water Quality Control Board regulations.</p> | <p>As discussed in detail in the MEREDITH INTERNATIONAL CENTRE Specific Plan Amendment EIR, implementation of the Specific Plan will not result in new, substantial sources of urban runoff, and also will not violate applicable storm water quality requirements. Also, MEREDITH INTERNATIONAL CENTRE includes permeable surfaces (e.g., landscaped areas, water quality/infiltration basins) that would allow surface water to percolate and contribute to recharge of the aquifer. No component of MEREDITH INTERNATIONAL CENTRE will adversely affect the performance of the groundwater recharge basin located east of the Specific Plan area (east of the Cucamonga Creek Channel). MEREDITH INTERNATIONAL CENTRE is consistent with Policy Plan Policies ER1-5, ER1-6 and ER1-7.</p> |
| <p><u>ER1-8 Wastewater Management.</u> We require the management of wastewater discharge and collection consistent with waste discharge requirements adopted by the Regional Water Quality Control Board.</p>  | <p>All sewer conveyance infrastructure needed to serve MEREDITH INTERNATIONAL CENTRE will be constructed in accordance with City design standards. Accordingly, MEREDITH INTERNATIONAL CENTRE will not violate any applicable waste discharge requirements and is consistent with Policy ER1-8/</p>  |
| <p><i>Goal ER3: Cost-effective and reliable energy system sustained through a combination of low impact building, site and neighborhood energy conservation and diverse sources of energy generation that collectively helps to minimize the region's carbon footprint.</i></p>  |  |
| <p><u>ER3-6 Generation – Renewable Sources.</u> We promote the use of renewable energy sources to serve public and private sector development.</p>   | <p>As required by Section 5, <i>Development Standards</i>, of this Specific Plan, primary Industrial structures in Planning Area 1 will incorporate solar panels to produce electricity for the industrial buildings. Accordingly, MEREDITH INTERNATIONAL CENTRE would be consistent with the City's goal of providing additional sources of renewable energy.</p>   |

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| <b>Goal ER4:</b> <i>Improved indoor and outdoor air quality and reduced locally generated pollutant emissions.</i>   |  |
| <p><u>ER4-1 Land Use.</u> We reduce GHG and other local pollutant emissions through compact, mixed use, and transit-oriented development and development that improves the regional jobs-housing balance</p>   | <p>The MEREDITH INTERNATIONAL CENTRE is a compact, mixed-use development that provides employment, service, retail, and residential land uses in close proximity to a major transportation corridor (Interstate 10) and potential transit corridor (Gold Line LRT). By virtue of its location near existing residential land uses and major transportation and transit corridors, MEREDITH INTERNATIONAL CENTRE promotes non-vehicular transportation and has the potential to reduce vehicle miles traveled, which would reduce tailpipe emissions – a major source of greenhouse gases (GHGs). As such, the MEREDITH INTERNATIONAL CENTRE would not prevent the City from achieving this Policy Plan goal.</p> |
| <p><u>ER4-3 Greenhouse Gases (GHG) Emissions Reductions.</u> We will reduce GHG emissions in accordance with regional, state and federal regulations.</p>  | <p>As described in the MEREDITH INTERNATIONAL CENTRE Specific Plan Amendment EIR, the Specific Plan would be consistent with applicable regional, state, and federal regulations related to the reduction of GHG emissions and would not obstruct implementation of any GHG reduction plans/programs. MEREDITH INTERNATIONAL CENTRE is consistent with Policy ER4-3.</p>   |
| <p><u>ER4-8 Tree Planting.</u> We protect healthy trees within the City and plant new trees to increase carbon sequestration and help the regional/local air quality.</p>  | <p>As part of development of the MEREDITH INTERNATIONAL CENTRE, a substantial number of trees would be planted within the Specific Plan area. Accordingly, MEREDITH INTERNATIONAL CENTRE will result in the implementation of this policy.</p>   |
| <b>Parks &amp; Recreation Element</b>  |  |
| <b>Goal PR1:</b> <i>A system of safe and accessible parks that meets the needs of the community.</i>   |  |
| <p><u>PR1-5 Acreage Standard.</u> We strive to provide 5 acres of parkland (public and private) per 1,000 residents.</p> <p><u>PR1-6 Private Parks.</u> We expect development to provide a minimum of 2 acres of developed private park space per 1,000 residents.</p> | <p>The residential component of MEREDITH INTERNATIONAL CENTRE will provide common open space for the benefit of residents. The common open space will feature active and passive recreational amenities. In addition, future residential development will be required to comply with State (Quimby Act) and City requirements for the provision of park land. Accordingly, implementation of MEREDITH INTERNATIONAL CENTRE will not preclude the City from satisfying Policies PR1-5 and PR1-6.</p>  |

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| <b>Community Economics Element</b>   |  |
| <b><i>Goal CE1: A complete community that provides for all incomes and stages of life.</i></b>   |  |
| <p><u>CE1-1 Jobs-Housing Balance.</u> We pursue improvement to the Inland Empire’s balance between jobs and housing by promoting job growth that reduces the regional economy’s reliance on out-commuting.</p> | <p>MEREDITH INTERNATIONAL CENTRE would provide over 220 acres of new employment-generating land uses (up to 4,150,000 square feet of building area), in close proximity to existing and planned residential land uses (MEREDITH INTERNATIONAL CENTRE allows up to 800 residential units). The new jobs opportunities provided within the MEREDITH INTERNATIONAL CENTRE Specific Plan area will assist the City’s efforts to promote job growth and improve the balance between jobs and housing within the City limits. MEREDITH INTERNATIONAL CENTRE is consistent with Policy CE1-1.</p> |
| <p><u>CE1-7 Retail Goods and Services.</u> We seek to ensure a mix of retail businesses that provide the full continuum of goods and services for the community.</p>   | <p>The Urban Residential land uses within MEREDITH INTERNATIONAL CENTRE allow a diverse mix of service and retail businesses, including over 25 types of commercial services (including eating and drinking establishments) and over 25 types of retail services that will serve a wide variety of needs for the community. MEREDITH INTERNATIONAL CENTRE is consistent with, and results in the implementation of, Policy CE1-7.</p>  |

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| <p><b>Goal CE2:</b> <i>A City of distinctive neighborhoods, districts, and corridors, where people choose to be.</i></p>  |   |
| <p><b>CE2-1 Development Projects.</b> We require new development and redevelopment to create unique, high-quality places that add value to the community.</p>   | <p>The MEREDITH INTERNATIONAL CENTRE Specific Plan provides for the development of the Specific Plan property as a high-quality, contemporary, horizontally mixed-use center containing industrial, urban commercial, and urban residential land uses. This Specific Plan’s <i>Design Guidelines</i> (refer to Section 6) establish criteria for architecture, lighting, signage, and landscape design promote development of an attractive mixed-use center with timeless design features that is perceived as an inviting place to work, shop, live, and invest time and resources. This Specific Plan is consistent with Policy CE2-1.</p> |
| <p><b>CE2-5 Private Maintenance.</b> We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.</p> <p><b>CE2-6 Public Maintenance.</b> We require the establishment and operation of maintenance districts or other vehicles to fund the long-term operation and maintenance of the public realm whether on private land, in rights-of-way, or on publicly-owned property.</p> | <p>This Specific Plan defines the entities responsible for maintenance of publically and privately-owned improvements within the MEREDITH INTERNATIONAL CENTRE, including roadways and utility infrastructure. Compliance with the maintenance responsibility matrix established in Section 7, <i>Implementation</i>, will ensure that all improvements within the Specific Plan area are properly and consistently maintained. MEREDITH INTERNATIONAL CENTRE is consistent with Policy Plan Policies CE2-5 and CE2-6.</p>  |
| <p><b>Goal CE3:</b> <i>Decision-making deliberations that incorporate the full short-term and long-term economic and fiscal implications of proposed City Council actions.</i></p>  |   |
| <p><b>CE3-2 General Plan Amendments.</b> We require those proposing General Plan amendments to disclose reasonably foreseeable impacts through a fiscal analysis.</p>   | <p>A fiscal impact analysis was prepared in support of the MEREDITH INTERNATIONAL CENTRE. Refer to the MEREDITH INTERNATIONAL CENTRE Specific Plan Amendment EIR for a detailed analysis of potential reasonably foreseeable fiscal effects associated with implementation of this Specific Plan.</p>   |