

## 5.0 COMPONENT PLANS

---

### 5.1 CIRCULATION AND TRANSPORTATION PLAN

#### A. VEHICULAR CIRCULATION

Primary access to the Shea Business Center will be via a northerly extension of Wineville Avenue from Airport Drive. A grade separation will be constructed along Wineville as it crosses the Southern Pacific rail line adjacent to Airport Drive. The project sponsor owns the property through which Wineville Avenue will cross.

Airport Drive is a major east-west route with direct access to the major north-south roadway network and indirect access to both the Ontario Freeway (I-15) and the San Bernardino Freeway (I-10). Adjacent major north-south roadways include Etiwanda Avenue to the east of the site and Milliken Avenue to the west.

Access through the site will be provided by Wineville Avenue and Shea Center Drive as shown in Exhibit 8. Emergency access to the site will be along the connecting loop from Airport Drive and via an emergency access easement through the easternmost Shea Center parcels to connect with Shea Center Drive. The exact eastern terminus of Shea Center Drive and the precise alignment of the emergency access easement will be determined at the time of site plan approval.

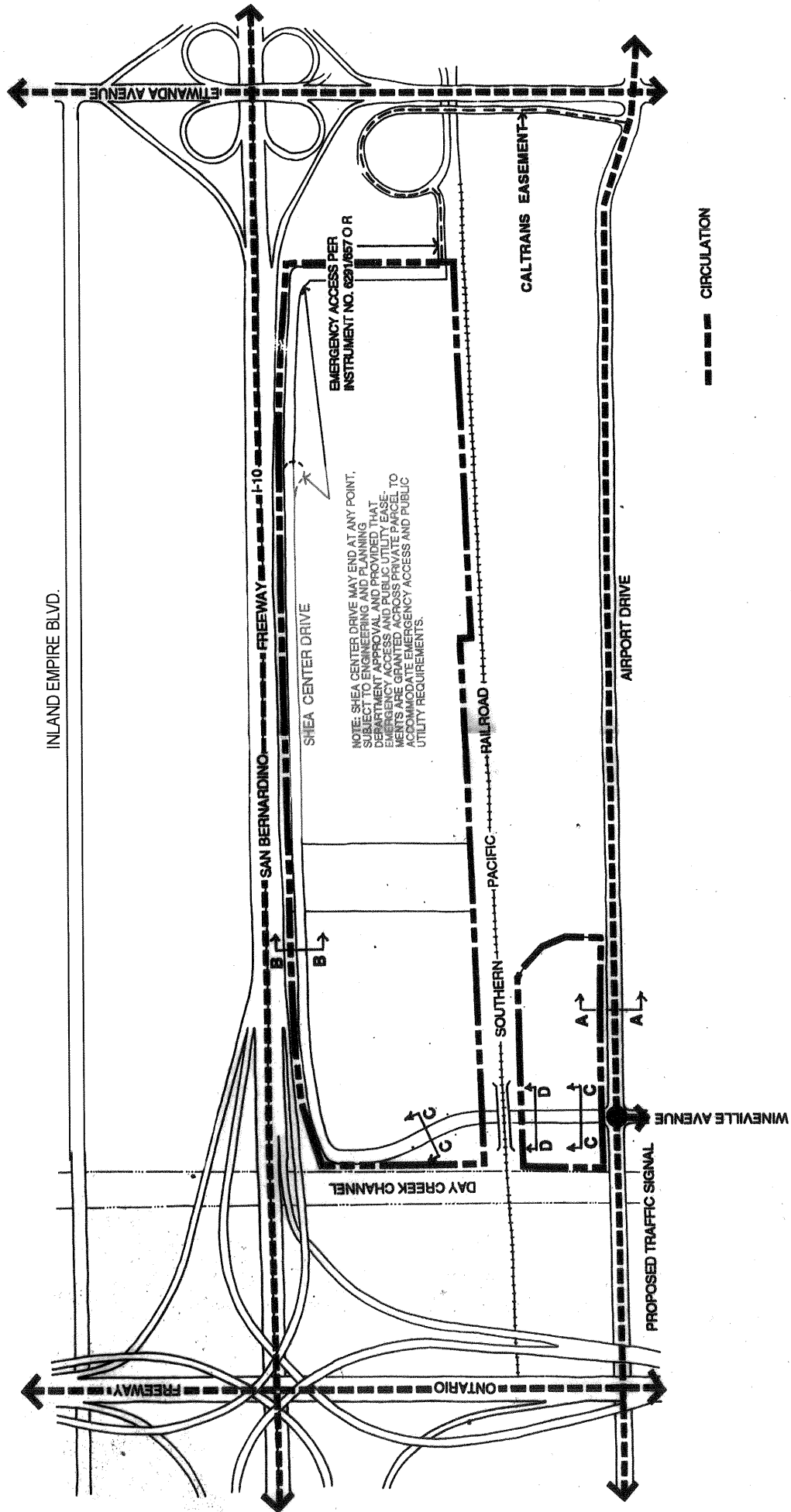
The master developer will be responsible for their fair share of required improvements at the intersection of Airport Drive and Etiwanda Avenue. The fair share will be based on the ratio of project-generated increase in traffic versus total increase in traffic using this intersection which will be created by future development of surrounding sites. The required improvements and the master developer's fair share will be determined at the parcel map approval stage.

A traffic study (Traffic Impact Analysis Report for the Shea Center Ontario, O'Rourke Engineering) has been prepared to define street geometric requirements and to identify the project proponent's responsibility for construction of the recommended circulation improvements.

The total overall development of the Shea Business Center will be limited by the total peak hour trips as stated in Table 5 of the Environmental Impact Report. The project sponsor will be responsible for supplying the City of Ontario with the number of trips generated from each site as this information develops. The trip generation rates will be from the most current edition of the ITE Trip Generation Manual or other publications as approved by the City Engineer. The supplied information will be used to monitor the traffic generated by this development.

In order to assist in keeping the overall project under the ceiling established for total peak hour trips, each individual user generating one hundred or more employees is required to prepare and implement a Transportation Demand Management for their individual development. The Property Owner's Association will promote the utilization of Transportation Demand Management measures for the entire project site. Additionally, the master developer and/or individual users shall comply with those requirements of the San Bernardino County Congestion Management Plan which are applicable to future site development applications, upon adoption by SANBAG.

Exhibit 8



Shea Center Drive, as proposed will have an east-west alignment. Public roadways will be developed to City standards. Building access will be primarily through the use of private drives or individual driveways linking truck and material storage, loading docks, employee and patron parking.

All street signage, traffic control signalization, etc. shall conform to the Manual on Uniform Traffic Control Devices, published by the U.S. Department of Transportation, Federal Highway Administration, and the City of Ontario standards.

Airport Drive ("A-A" on Exhibit 8) will be constructed as a four-lane roadway. The cross section for this street is shown on Exhibit 9.

Shea Center Drive will be constructed as a two-lane roadway. The public portion of Shea Center Drive may end at any point, subject to Engineering and Planning Department approval and provided that easements are granted across the easterly private parcel to accommodate emergency access and public utility requirements. Exhibit 9 provides the street section of Shea Center Drive ("B-B"). Lane configurations will be designed as approved by the City Engineer. See Appendix C, The Traffic Analysis, for additional information.

Wineville Avenue ("C-C" and "D-D" on Exhibit 8) will be constructed as a four-lane roadway, which will be divided as it passes under the railroad. Both conditions are depicted in Exhibit 10.

The project sponsor and/or successor shall be responsible for construction of all required circulation infrastructure improvements including railroad grade separation and Shea Center Drive from the property's south property line to Airport Drive.

The access points to serve the individual sites shall be determined at the site plan stage. Special attention shall be given to signs distance requirements and the impact the driveway will have on the flow of traffic on public streets, especially where centerline radii of less than 1,100 feet have been applied.

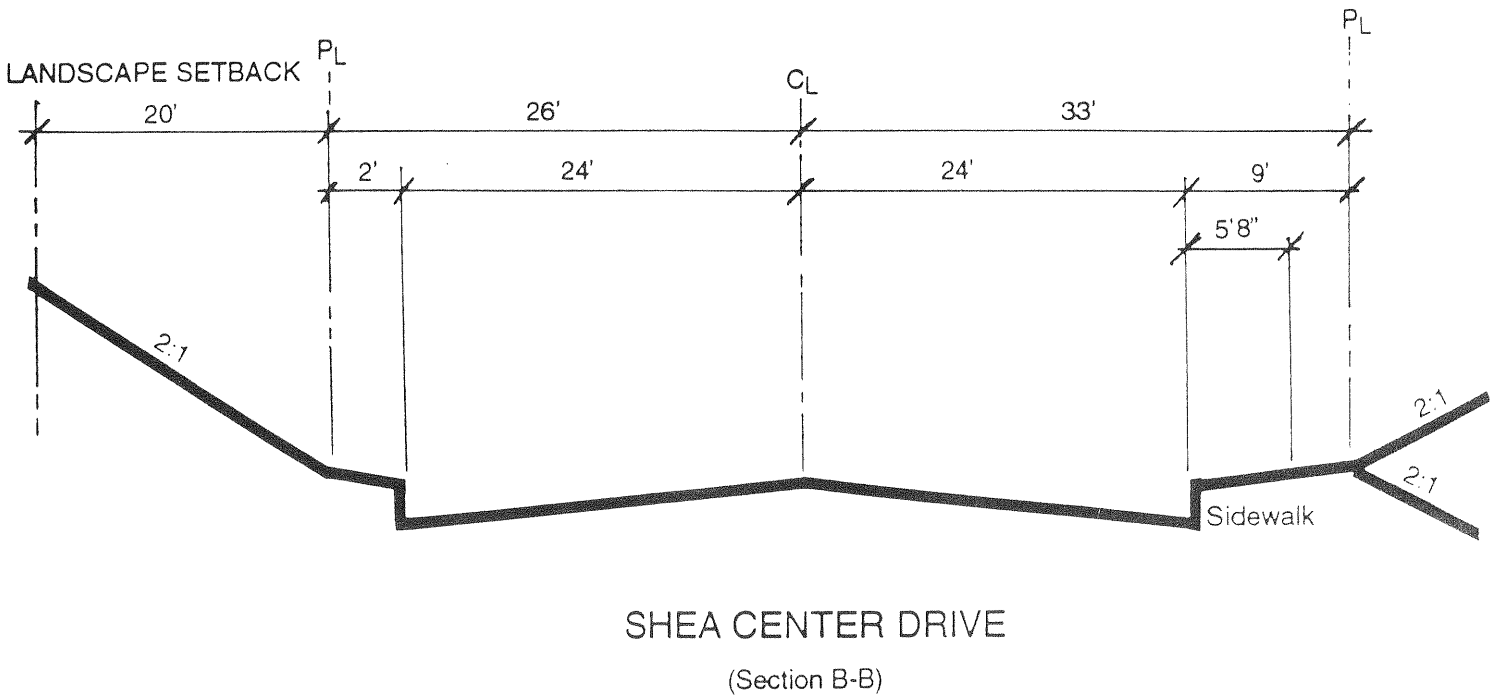
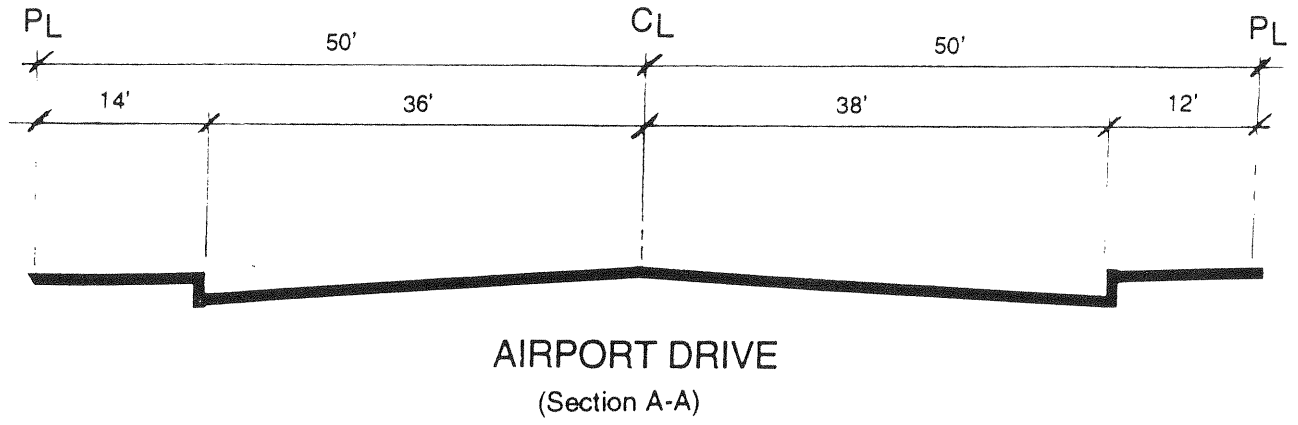
#### **B. PEDESTRIAN CIRCULATION AND PUBLIC TRANSIT**

If any bus stops are constructed within the project area, safe pedestrian walkways between commercial buildings and public transit areas shall be installed onsite by the project sponsor or successor. Sidewalks will be provided on the south side only of Shea Center Drive.

#### **C. RAILROAD ACCESS**

Since buildings within the Shea Business Center located adjacent to the Southern Pacific Railroad may be rail served, they shall be designed for such service. Approvals from the Railroad will be necessary for the Wineville Avenue grade separation and all rail spur connections to the main line. A plan of railroad access will be prepared concurrent with site plan review to illustrate how rail spurs from the Southern Pacific main line will serve uses within the project. All costs and fees for the construction of rail service to the Specific Plan area will be borne by the project sponsor or individual users within the specific plan area. This shall include necessary crossing signals and any additional costs required to provide rail facilities for the Specific Plan area.

# STREET SECTIONS



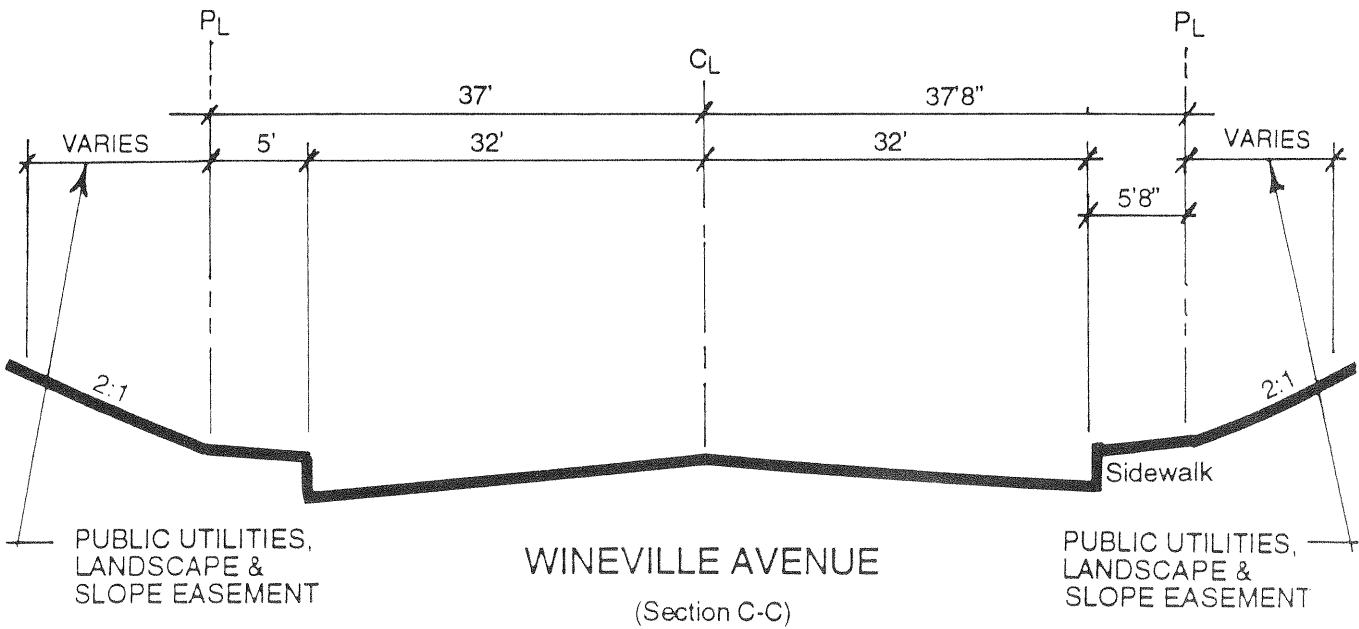
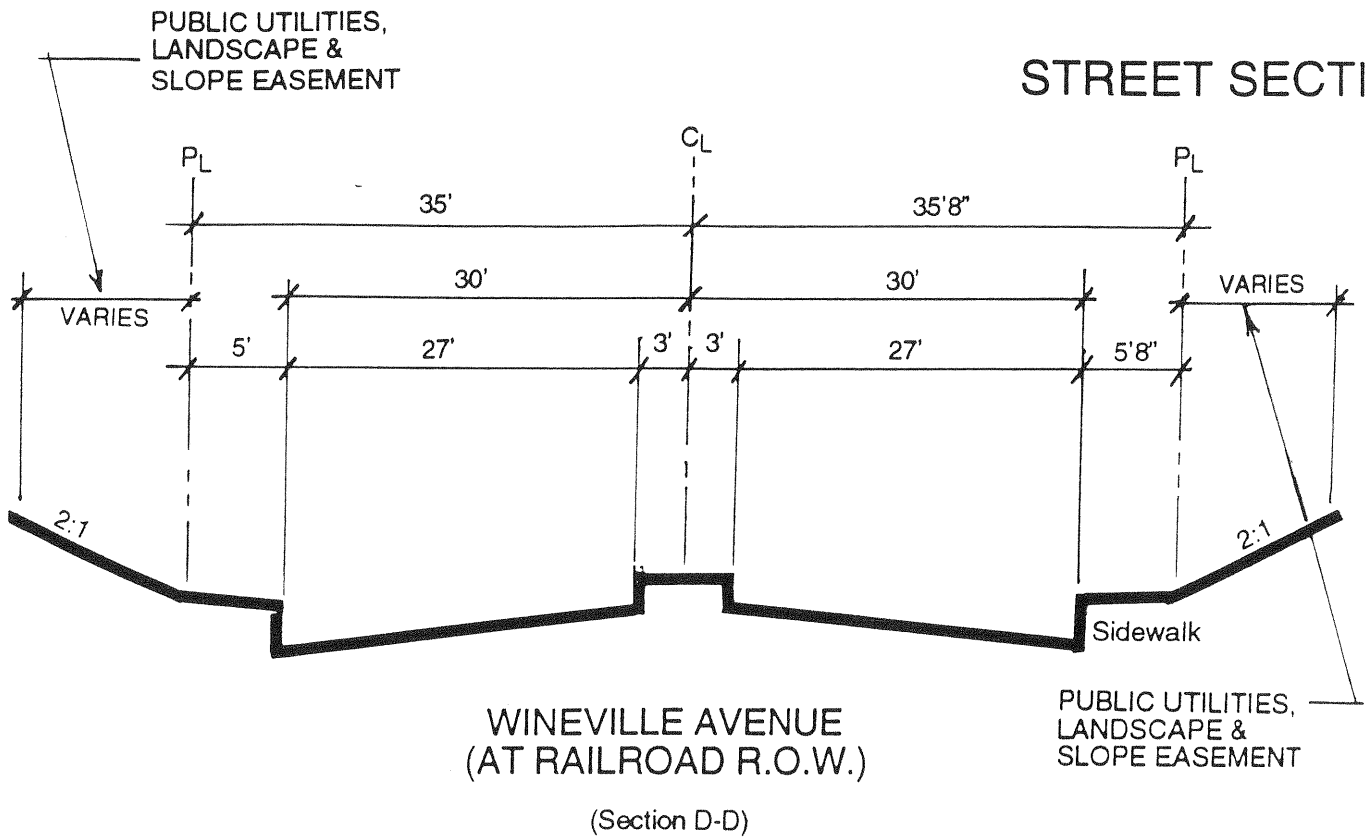
## SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

Yvonne Neal Associates  
Williamson & Schmid


EXHIBIT 9

# STREET SECTIONS



## SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

Yvonne Neal Associates  
Williamson & Schmid

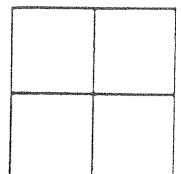


EXHIBIT 10

## 5.2 INFRASTRUCTURE PLAN

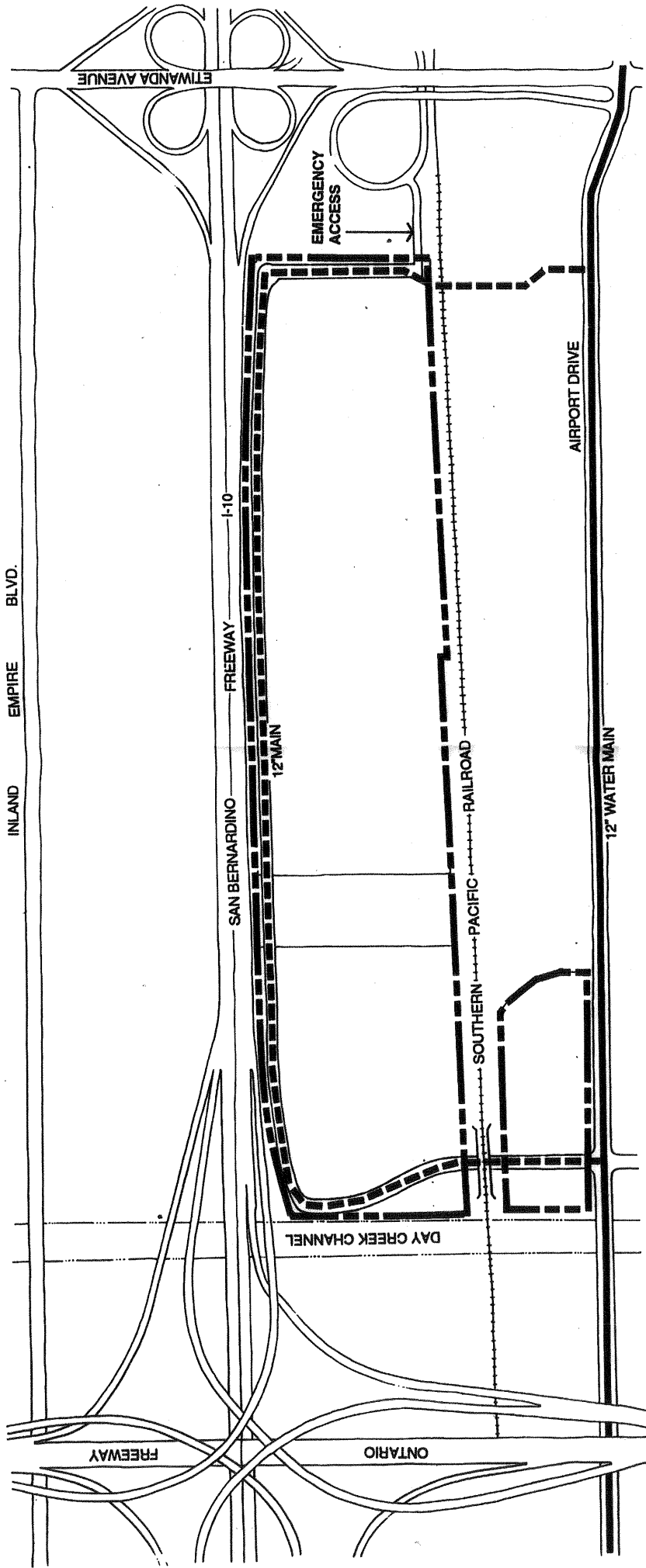
### A. WATER FACILITIES

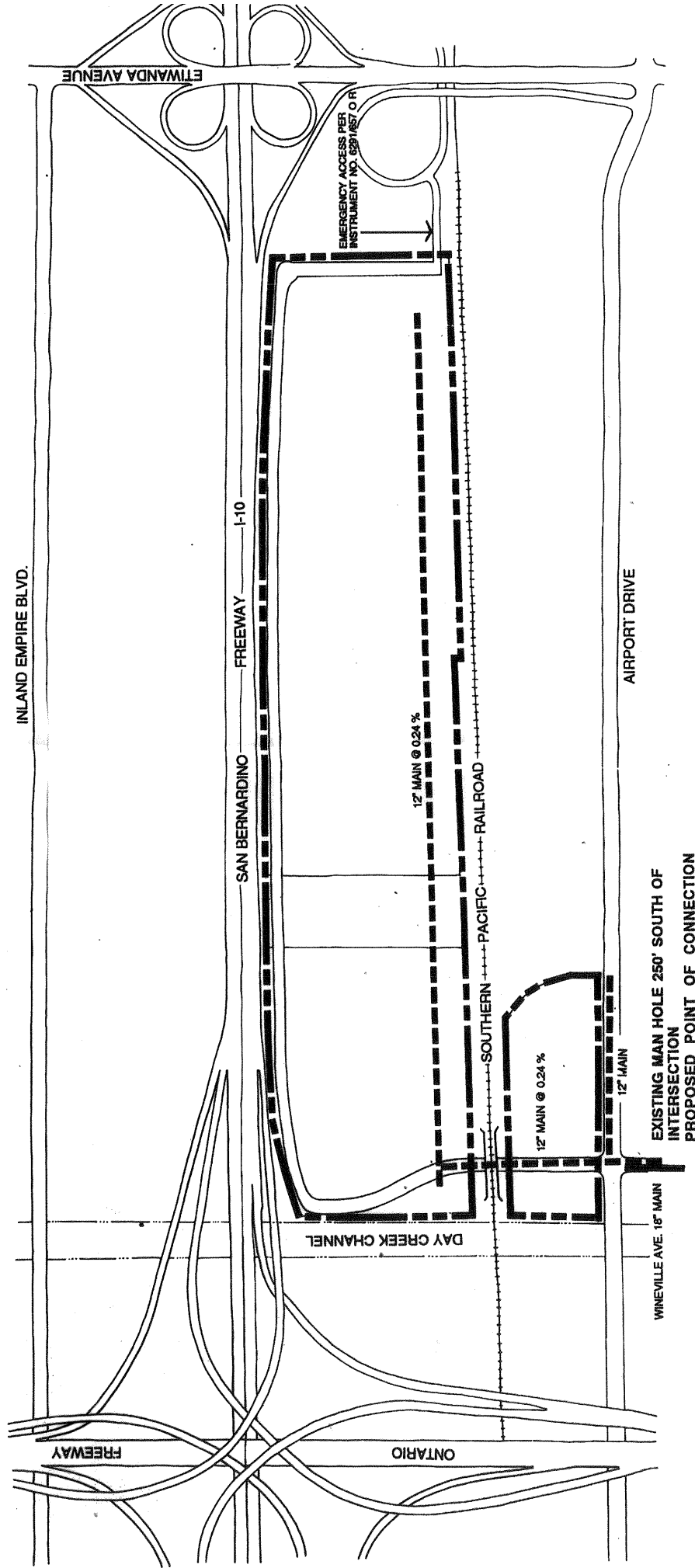
A City of Ontario 12-inch water line is located in the Airport Drive right-of-way. Service to the project site is proposed via two connections to the line in Airport Drive, and looping through the site (see Exhibit 11). One connection will be made at the intersection of Airport Drive and Wineville Avenue. The proposed 12-inch line will be located within the public right-of-way, running north and then east to the easterly terminus of Shea Center Drive. The new line will then run southerly across the Southern Pacific Railroad right-of-way adjacent to the ultimate Etiwanda Creek right-of-way to Airport Drive. At this point, a second connection will be made to the existing water line in Airport Drive to complete the loop. Prior to connection, an easement permitting access parallel to Etiwanda Avenue southerly and underneath the railroad to Airport Drive will be obtained from the Railroad by the project sponsor. A right-of-way for Shea Center Drive will be obtained prior to any connections. An easement has been granted from Union Carbide Industrial Gases, Inc. to Shea Center Ontario at the east end of the project site, between the railroad track and Airport Drive, which shall be dedicated to the City of Ontario for a water line easement. This easement was executed in December of 1990 and recorded with the County of San Bernardino (record number 91-022633). All connections to existing and future public water lines will require the approval of the City of Ontario City Engineer. The construction of the proposed public loop water system will be commenced and completed during the development of Phase I of the project.

Connection to the loop system serving individual uses within the Shea Business Center will be made via private water lines on private property and minimum 8-inch public lines within easements to be granted to the City of Ontario, as approved by the City Engineer.

### B. SEWER FACILITIES

An 18-inch sewer exists in Wineville Avenue with the terminal upstream manhole located at the intersection of Airport Drive. Service to the project site will be provided by connecting to the line in Wineville Avenue, at the existing manhole located approximately 250' south of the Airport Drive intersection in at Wineville as approved by the City Engineer. A 12-inch sewer will be constructed within the Wineville Avenue right-of-way northerly along the Southern Pacific Railroad underpass and into the project site. The existing sewer located in Airport Drive west of the site will be extended along the Airport Drive frontage of the project. A 12-inch public sewer located north of the railroad tracks will be run to the east end of the property (see Exhibit 12). Public utility easements to accommodate this sewer line shall be granted, as necessary. A portion of this 12-inch line may need to be private depending on parcelization of the property and accessibility to the facility. Determination of exact location, slope, and public/private disposition of the sewer system shall be reviewed and approved by the City Engineer prior to construction.







### C. FLOOD CONTROL AND DRAINAGE FACILITIES

Existing drainage across the site is from north to south, and then easterly to Etiwanda Creek and westerly to Day Creek. A preliminary hydrology study was used to formulate the location and sizing of the proposed drainage improvements to be constructed for this Specific Plan, as illustrated in Exhibit 13.

Improvements to Day Creek Channel from Airport Drive south to the Riverside Basin (Phase IIB, Day Creek Water Conservation and Flood Control Project) are complete. With the completion of the Phase IIB and IIIA improvements, the site has been eliminated from the 100-year floodplain of Day Creek.

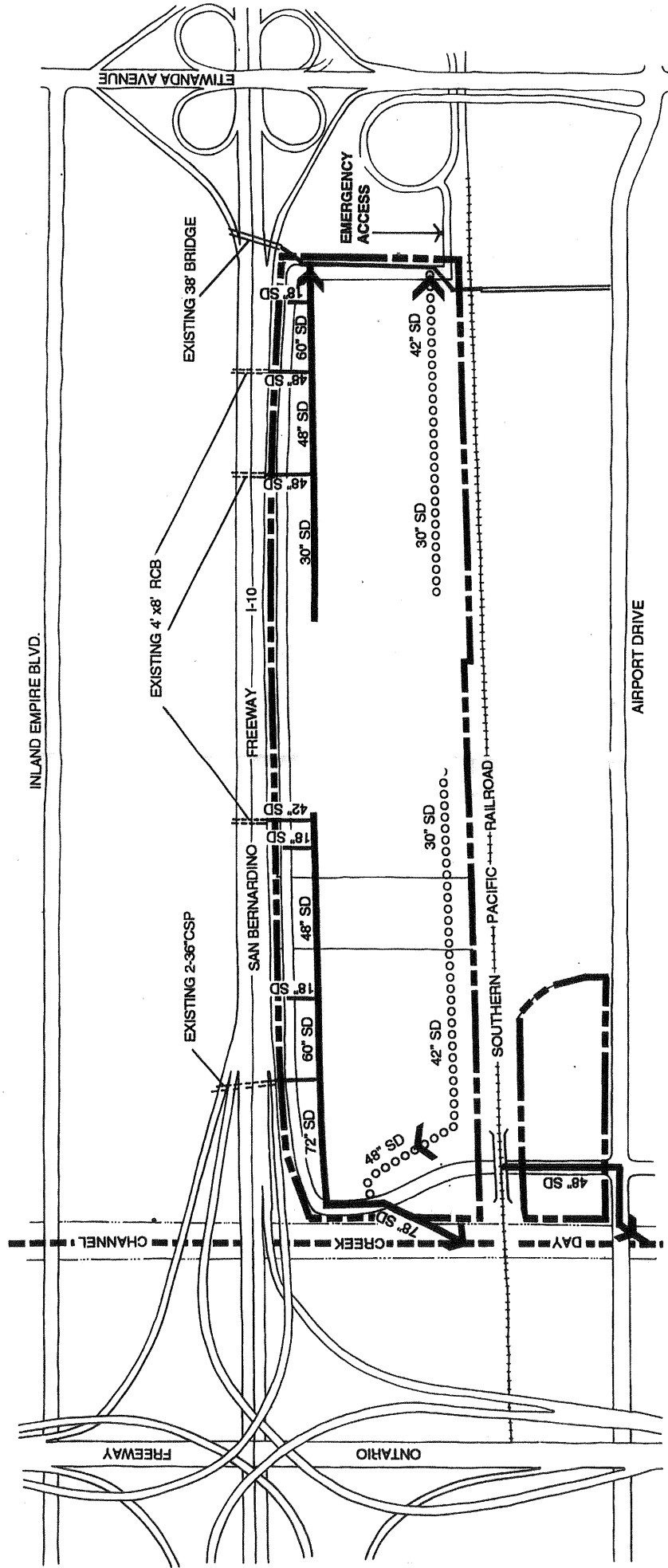
There are no improvements scheduled for Lower Etiwanda Creek at this time. There are, however, several studies currently underway for the San Sevaine/Etiwanda Creek Drainage Plan (Williamson and Schmid) which will provide flood protection along lower Etiwanda Creek. The adopted Day, Etiwanda, and San Sevaine Creeks System Drainage Plan (March 1983) proposes the diversion of upper Etiwanda Creek flows to the San Sevaine Channel at Foothill Boulevard. This would eliminate the majority of the flow in Etiwanda Creek through the site. The timing of the diversion, however, is dependent on San Sevaine Channel improvements from Foothill Boulevard to its outlet at the Santa Ana River. Several options are available which would allow the diversion of upper Etiwanda Creek flows without increasing the flooding in the San Sevaine Channel. San Bernardino County Flood Control District; Riverside County Flood Control and Water Conservation District; the cities of Ontario, Rancho Cucamonga, and Fontana; and area property owners, including Shea Business Properties, have been working together to develop an interim solution to the existing flooding problems in Etiwanda Creek and San Sevaine Channel.

With the diversion of Etiwanda Creek to San Sevaine Channel and the improvement of Lower Etiwanda Creek for the ultimate 100-year discharge, the 100-year floodplain from Etiwanda Creek through the site would be eliminated. Until that time, however, there are several options available to protect the site including, but not limited to, the following, all of which require City review and approval:

1. An engineered channel could be constructed through the site which would collect and contain the interim 100-year discharge at the freeway. On-site detention may be needed to mitigate the increase in flows due to site development.
2. The ultimate Etiwanda Creek Channel could be constructed through the site with the additional interim flows contained within a reduced floodplain. On-site detention may be needed to mitigate the increase in flows due to site development.

Option 1 would require the construction of an oversized channel through the site which would be spending money on interim facilities. This option could also increase the discharge downstream by channelizing the flow in a more efficient manner.

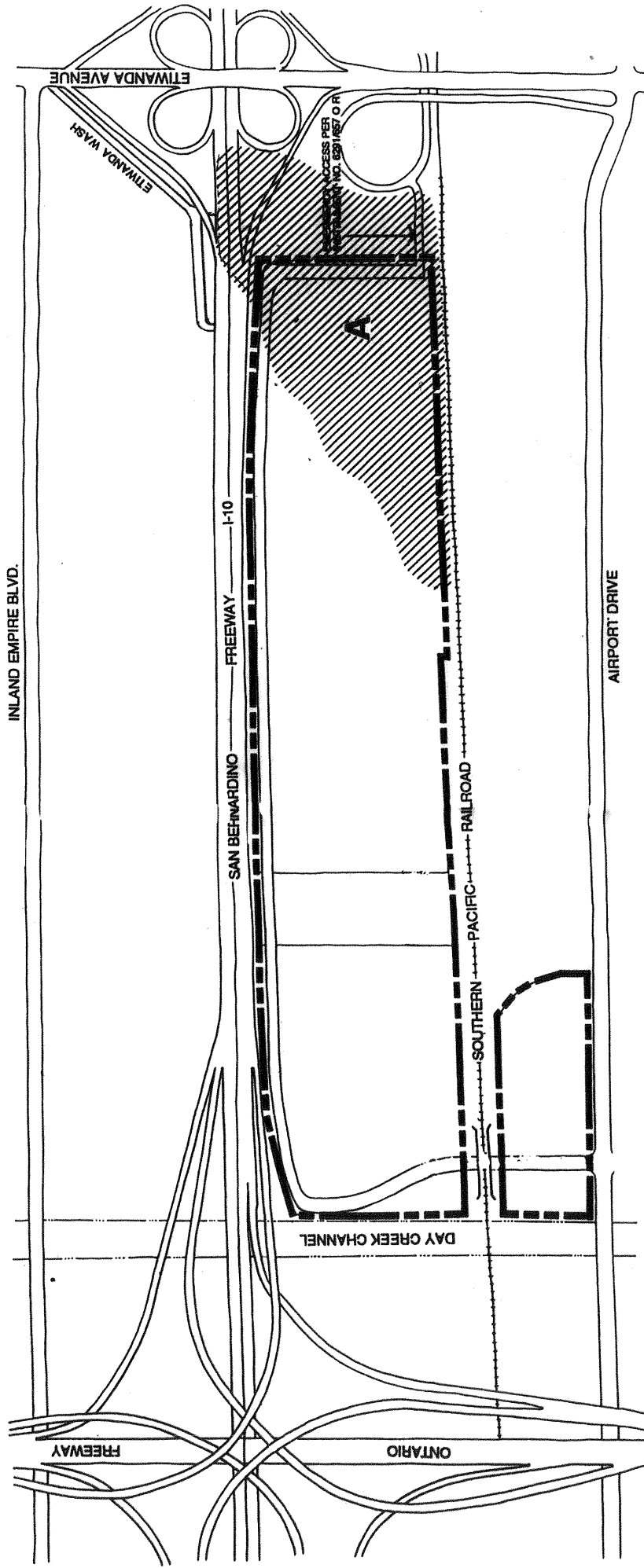
# Exhibit 13



NOTE: Exact location and size of storm drains will be determined by detailed study. Pipe size shown is preliminary.

## DRAINAGE PLAN

Exhibit 14



LEGEND  
100 YEAR FLOOD PLAIN

EXISTING PLUS BARRIER  
HYDROLOGY

Option 2 is beneficial in that it provides the ultimate channel section without wasting money and it reduces the existing floodplain. Option 2 does not eliminate the floodplain, however, and as was the case with Option 1, the discharge could be increased downstream due to the increased channel efficiency.

The floodplain of Etiwanda Creek only affects the easterly third of the project site (see Exhibit 14). Since the upstream channel improvements are still being studied, several things could change (affecting the interim drainage plan) by the time the interior portions of the site are ready for development. For this reason, it is proposed that any improvements to Etiwanda Creek through the site be designed and implemented along with the development of the associated portions of the project.

Onsite storm drain improvements will be designed to collect the ultimate discharge from the site, as well as discharge from the existing culverts under the freeway, and outlet flows to the Day or Etiwanda Creek Channels. Onsite facilities will also collect interim flows, or a portion thereof, from the existing culverts. Future improvements north of the freeway have been proposed to intercept the ultimate 100-year runoff and discharge to either the Day or Etiwanda Creeks north of the freeway or the existing 8' x 4' RCB culverts under the freeway. The flow through the culverts will be significantly reduced by the construction of proposed upstream improvements.

The proposed drainage for the project will direct flow away from the onsite structures to streets and storm drain facilities. As shown in Exhibit 13, storm drains under I-10 will be extended southerly, connecting to drains which will be constructed south of Shea Center Drive, directing flows to outlets approved by the City Engineer. Storm drain connections to Day Creek Channel will be made at the connections provided at the time channel improvements were constructed with the approval of the San Bernardino County Flood Control District.

The proposed public storm drain facilities shall be contained in public right-in-way, the most desirable of which is public street right-of-way. If drains accepting public water must be outside dedicated public street right-of-way, drainage easements dedicated to the City of Ontario along with acceptable access for maintenance of said facilities shall be provided.

### **5.3 PUBLIC UTILITIES PLAN**

#### **A. SOLID WASTE DISPOSAL**

Solid waste collection service will be provided by the City of Ontario. The type, size quantity, and location of all solid waste receptacles shall be as approved by the City of Ontario Solid Waste Superintendent. All refuse enclosures shall be constructed to City of Ontario standards. Refuse compaction shall comply with City-adopted programs to implement AB 939. Provisions for separation and storage of recyclable solid waste will comply with the City's adopted program for implementation of AB 939.

#### **B. NATURAL GAS**

The Southern California Gas Company will provide natural gas service to the area. Existing improvements in the vicinity include a regulatory station just easterly of the Ontario Freeway and a medium distribution main extending westerly of the Freeway from this station. A 10-inch high pressure main is located in Airport Drive southerly of the project. Onsite gas facilities, including connections to existing offsite lines, will be in accordance with Southern California Gas Company requirements.

All natural gas lines within the site will be placed underground within dedicated public streets, dedicated public utility easements along streets, or in dedicated easements as approved by the City Engineer and the Southern California Gas Company.

**C. ELECTRICITY**

Southern California Edison Company (SCE) will provide electrical service to the project site. All electrical service facilities developed onsite will comply with the requirements of the Southern California Edison Company. New electrical service lines constructed for the Shea Business Center will be placed underground within dedicated public streets, dedicated public utility easements along streets, or in dedicated easements as approved by the City Engineer and Southern California Edison.

**D. TELEPHONE**

Telephone service to the project site will be provided by General Telephone (GTE). All telephone connections will be improved in accordance with the requirements of GTE. New lines constructed for the Shea Business Center will be placed underground within dedicated public streets, dedicated public utility easements along streets, or in dedicated easements as approved by the City Engineer and GTE.

**5.4 COMMUNITY SERVICES AND FACILITIES PLAN**

**A. FIRE PROTECTION**

Fire protection to the project site will be provided by the City of Ontario. Primary service to the Shea Business Center will be from Station No. 7, which is located on the south side of Jurupa Street, east of the Ontario Freeway (I-15).

**B. POLICE PROTECTION**

All development within this project site will comply with the security requirements of Ontario Security Code, OMC 4-11.01. Police protection to the project site will be provided by the City of Ontario through their existing police services. These services are being expanded on an ongoing basis to serve expanding commercial and industrial uses in the eastern portion of the City. The Applicant and future users of the site will mitigate impacts by the participation in an impact fee program upon adoption of said program by the City of Ontario.

**5.5 IMAGE ENHANCEMENT PLAN**

**A. ARCHITECTURAL DESIGN GUIDELINES**

**1. PURPOSE AND INTENT**

The following architectural guidelines have been developed to unify the project with contemporary design goals in order to promote an aesthetic, park-like work setting. The purpose of the architectural design guidelines is to ensure quality development that reinforces continuity within the Specific Plan area.

Recognizing the City's of Ontario's desire for architectural and landscape enhanced image along Freeway (I-10), future Development Plans will include architectural and landscape treatments which reflect this enhanced image. The following design guidelines are tools which can be used to accomplish this objective.

In addition, Section 4.4, Table 1 of this Specific Plan contains particular setback requirements for those parcels which abut the San Bernardino (I-10) Freeway. Also, Section 4.6 of this document restates the language, affecting both building orientation and landscape treatments, pertaining to development adjacent to major transportation corridors, including the I-10 Freeway, as adopted by the City of Ontario Planning Commission.

## 2. ARCHITECTURAL TREATMENT

The architectural treatment of buildings provides the means of unifying and identifying the character of Shea Business Center. The following guidelines shall be observed:

- a. Architecture shall be contemporary in character.
- b. Building masses shall rely on simple formal arrangements with an emphasis on strong geometry. It is the goal to locate larger building masses toward the eastern end of the project site.
- c. Building color palette shall consist of light colors from white to warm, cool grey and light beige with limited use of accent colors of darker value.
- d. Materials shall be used which enhance the contemporary image of the architecture. The following building materials are permitted.
  - (1) Glass/curtain wall. Glazing types shall be clear, blue, aqua and green. Bronze or warm tinted glazing is not recommended.
  - (2) Brick (limited to groundplane use at site and building entries; i.e. paving).
  - (3) Tile.
  - (4) Composite prefinished metal panels.
  - (5) "Tilt-up", "poured-in-place", or "precast concrete" with painted or "natural" finish (sand blasted or textured), and uniform in color.
  - (6) Concrete block – textured, split-face or sand blasted.
  - (7) Glass block.
  - (8) Glass fiber, reinforced concrete.
  - (9) Plaster/stucco.
  - (10) Quarry stone (granite, marble, etc.).
  - (11) Enhanced architectural features such as trellises, columns, and other elements may be used to provide massing relief.

Note: Metal buildings are not permitted in the Shea Business Center.

### 3. ARCHITECTURAL DESIGN CONSIDERATIONS

Design consideration should be given to:

- a. Views from within the buildings and from off-site.
- b. Solar orientation and governing climatic conditions.
- c. Orientation toward major streets and freeways.
- d. Vehicular and pedestrian circulation patterns.
- e. The facilities functional organization.

Special design consideration should be given to the following specific architectural conditions:

- a. The use of reoccurring elements which create a cohesive visual and spatial expression shall be employed at areas requiring special consideration (i.e. entries).
- b. Articulation of the building surfaces shall be achieved through the use of openings and recesses creating shadow patterns and depth providing visual interest to the overall building form.
- c. Building entrances shall be well articulated to project a formal entry statement through the use of enhanced materials, building forms and landscaping.
- d. Apparent building mass of larger buildings shall be reduced visually through sensitive articulation and surface treatment of exterior walls. Long, inarticulate walls should be minimized. Ideas such as staggering and layering of vertical articulation can reduce visual impact and should be applied.

### 4. EQUIPMENT AND SERVICE SCREENING

Rooftop equipment screening shall be considered as part of building architecture. While roofs are to be generally flat with adequate minimum slope for adequate drainage, all rooftop equipment shall be concealed from view by parapets and/or other screening devices, so as not to be visible from surrounding streets or the San Bernardino Freeway on a horizontal sight line. Aesthetic consideration shall be given to the design of all such screening devices so as to achieve integration with the overall architectural design of the building.

All service/loading areas shall be screened from public view. Screening shall be achieved by using screen walls, ornamental landscaping and/or portions of the building.

Site line analysis shall be submitted to the City of Ontario with all development applications.

5. PERFORMANCE STANDARDS AND CONSIDERATIONS

All buildings within Shea Center will meet state energy efficiency and conservation guidelines for commercial construction. Use of both passive and active devices and techniques to improve building energy performance, while creating architectural variety and visual detail, are encouraged.

a. Use of systems which shift utility demand (i.e., gas and electricity) to off-peak hours are encouraged.

b. Roof-top solar collectors can be installed if visually hidden by parapets or other architectural details.

c. Buildings should be designed and oriented on the site to take advantage of solar access.

d. Sound generation allowed on a site shall not exceed the allowable dba of the Noise Control Ordinance of the City of Ontario.

e. Operable windows in office and lobby areas shall be included wherever possible.

6. ART IN PUBLIC PLACES

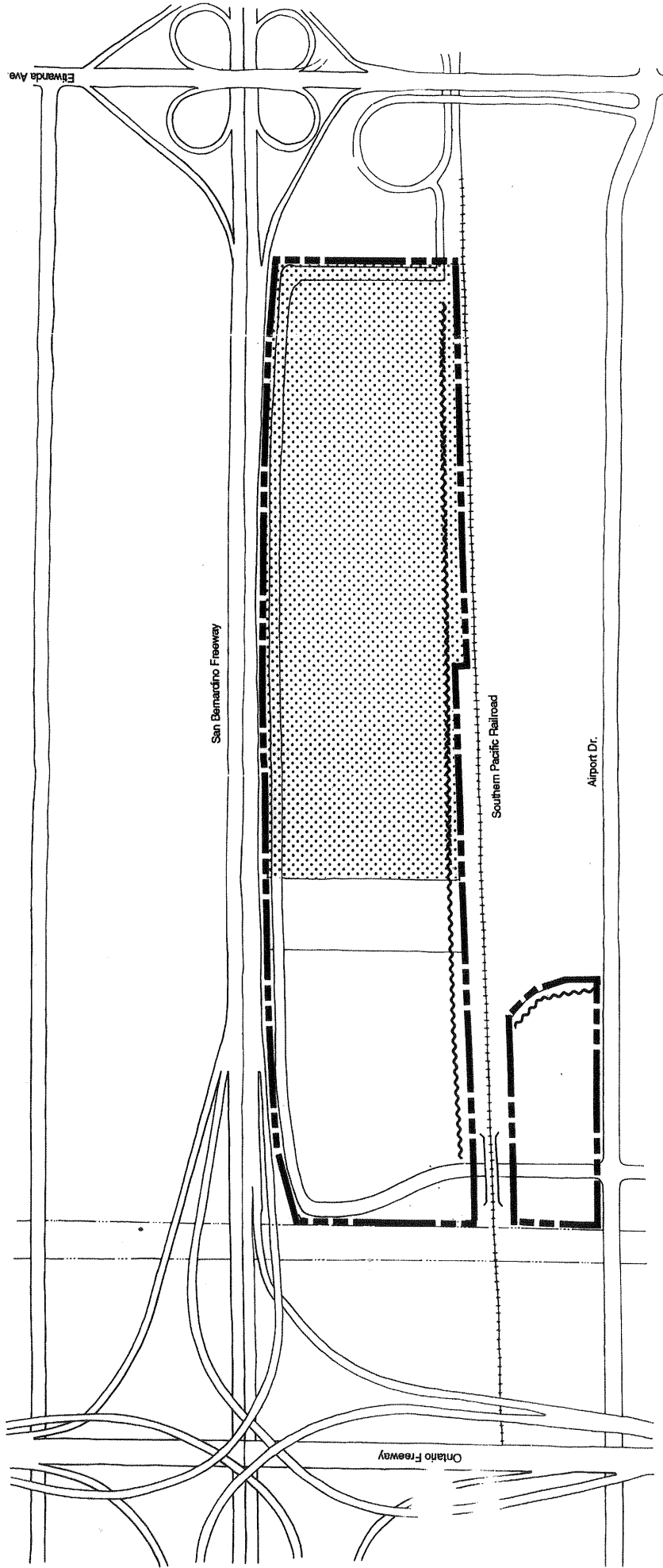
A public art piece will be constructed within the project site. The design, location and timing of such art will be mutually agreed upon by the master developer and the City of Ontario Development Director. Cost for such a piece will not exceed \$15,000.

7. CONCEPTUAL SITE EXHIBITS

The following Concept Plan (Exhibit 15) and site plan exhibits (16 through 21) illustrate the range of conditions within the Shea Business Center. These exhibits are purely conceptual and are presented according to building type.



Exhibit 15



AREA WHERE LOADING AREAS FACING THE FREEWAY ARE PERMITTED IF PROPERLY SCREENED FROM THE FREEWAY AND/OR SHEA CENTER DRIVE, AS PER SECTION 4.6

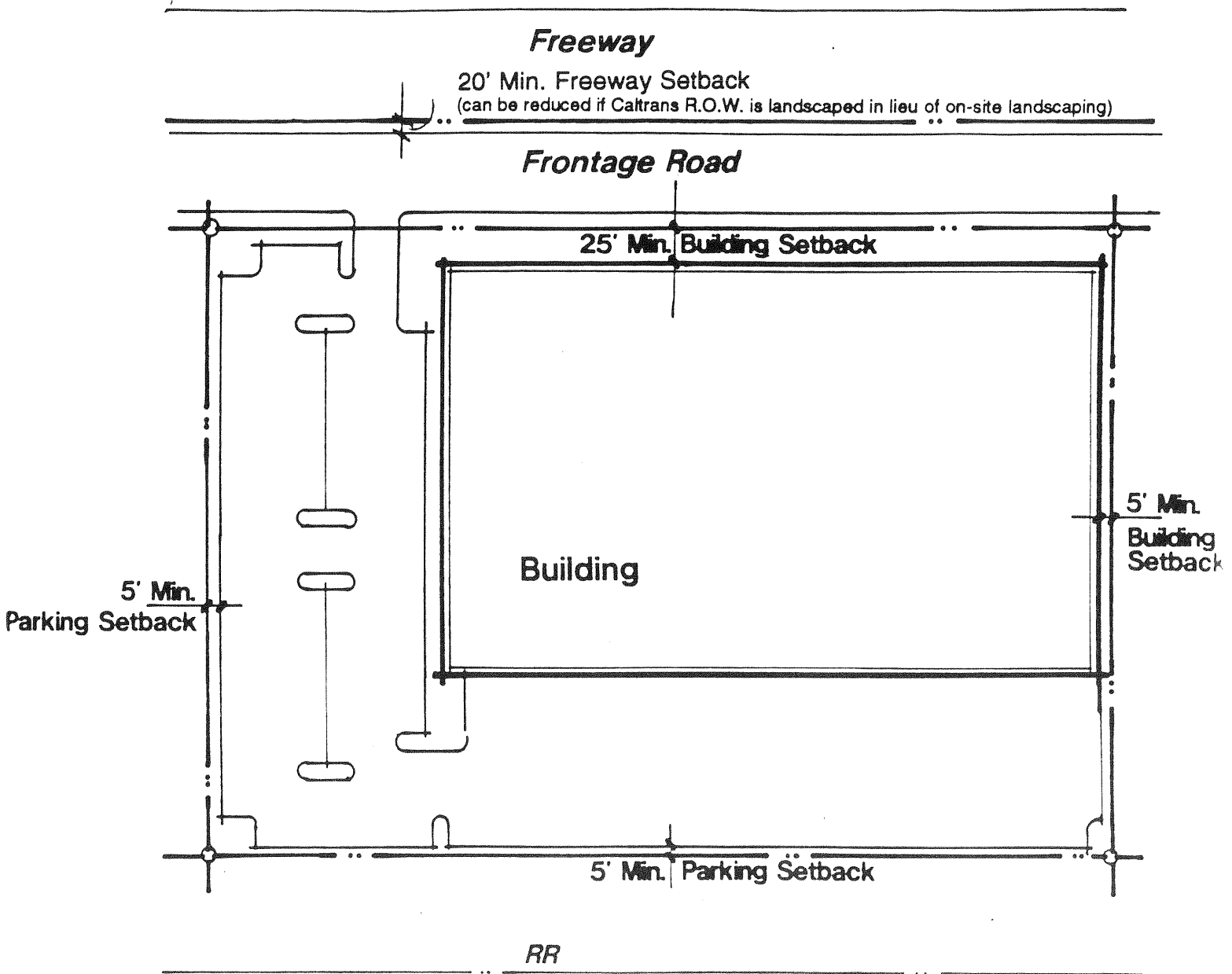


POTENTIAL RAIL ACCESS SITES



CONCEPT PLAN

# SHEA CENTER DRIVE/I-10 FREEWAY CONCEPTUAL SITE PLAN



Note: All setbacks measured from Property Line

## SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

Hill Pinckert Architects

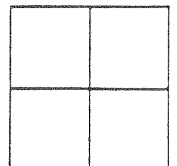
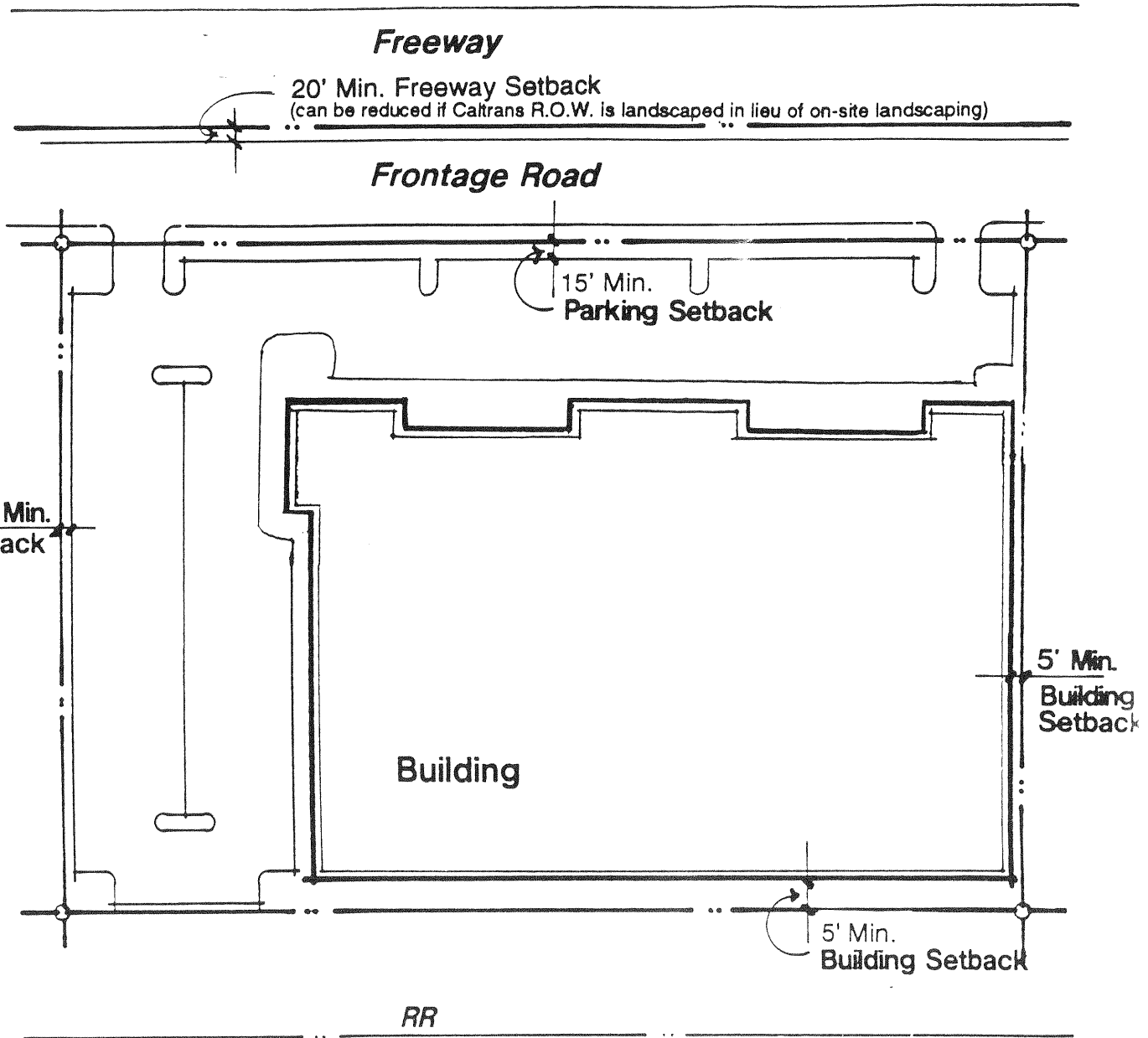


EXHIBIT 16

# SHEA CENTER DRIVE/I-10 FREEWAY CONCEPTUAL SITE PLAN



Note: All setbacks measured from Property Line

## SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

Hill Pinckert Architects

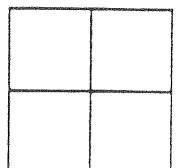
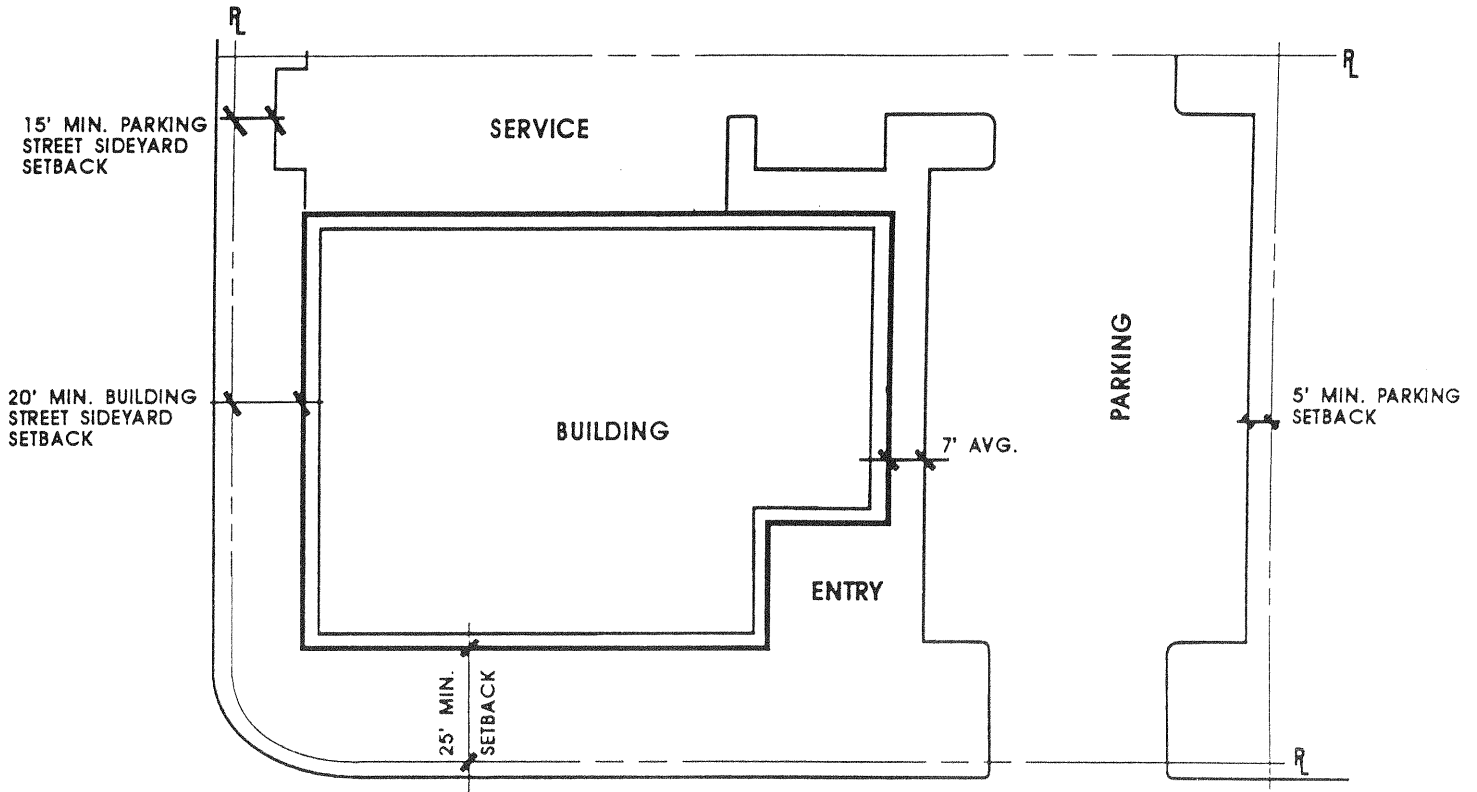


EXHIBIT 17

# CORNER PARCEL STREET SIDEYARD

## CONCEPTUAL SITE PLAN



NOTE: ALL SETBACKS MEASURED FROM PROPERTY LINE.

### SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

PLANNING NETWORK  
GILBERT AJA ASSOCIATE ARCHITECTS  
LIFESCAPES, INCORPORATED

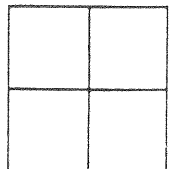
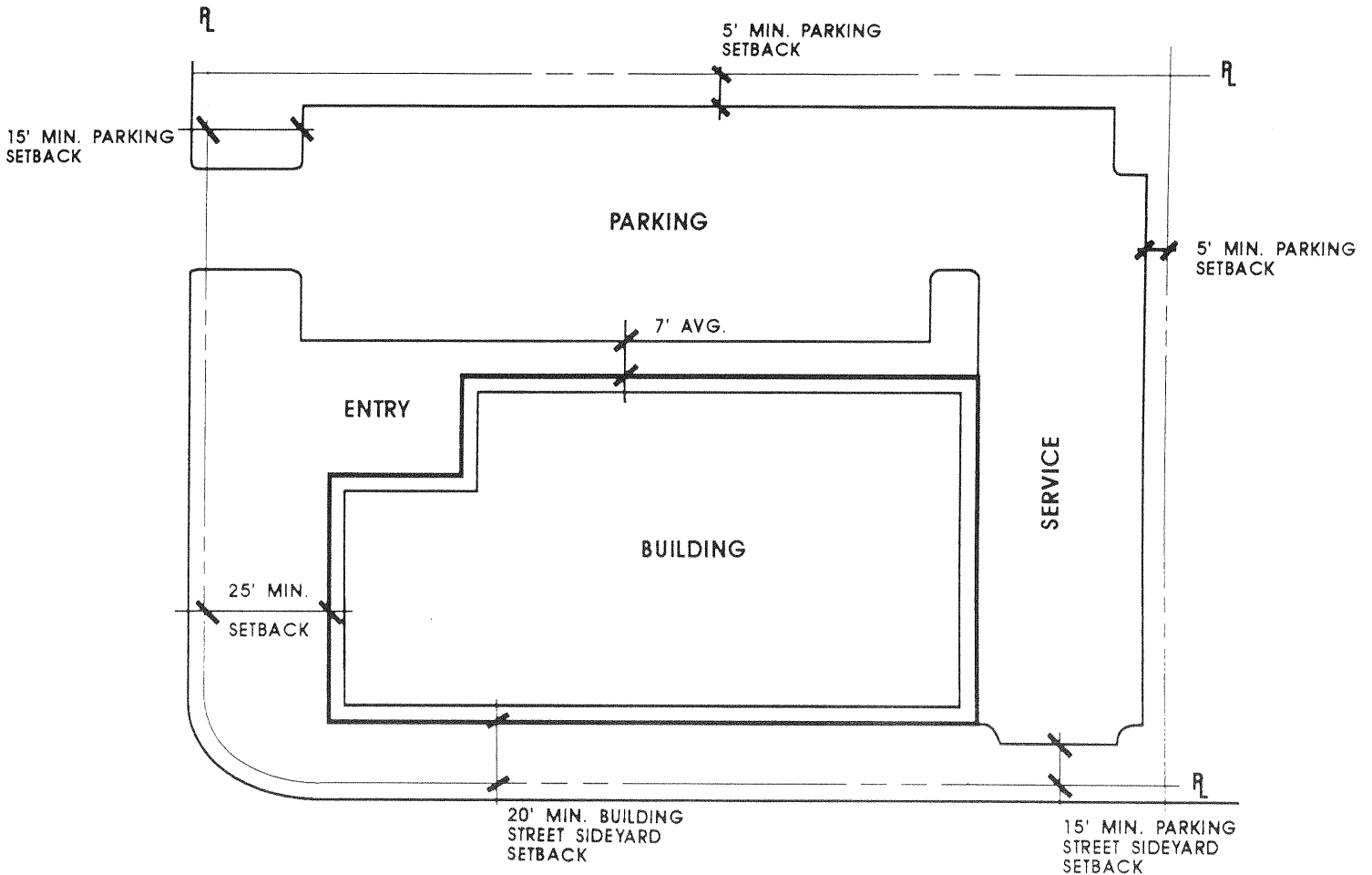


EXHIBIT 18

# CORNER PARCEL STREET SIDEYARD

## CONCEPTUAL SITE PLAN



NOTE: ALL SETBACKS MEASURED FROM PROPERTY LINE.

### SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

PLANNING NETWORK  
GILBERT AJA ASSOCIATE ARCHITECTS  
LIFESCAPES, INCORPORATED

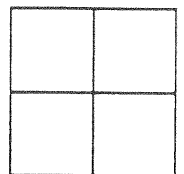
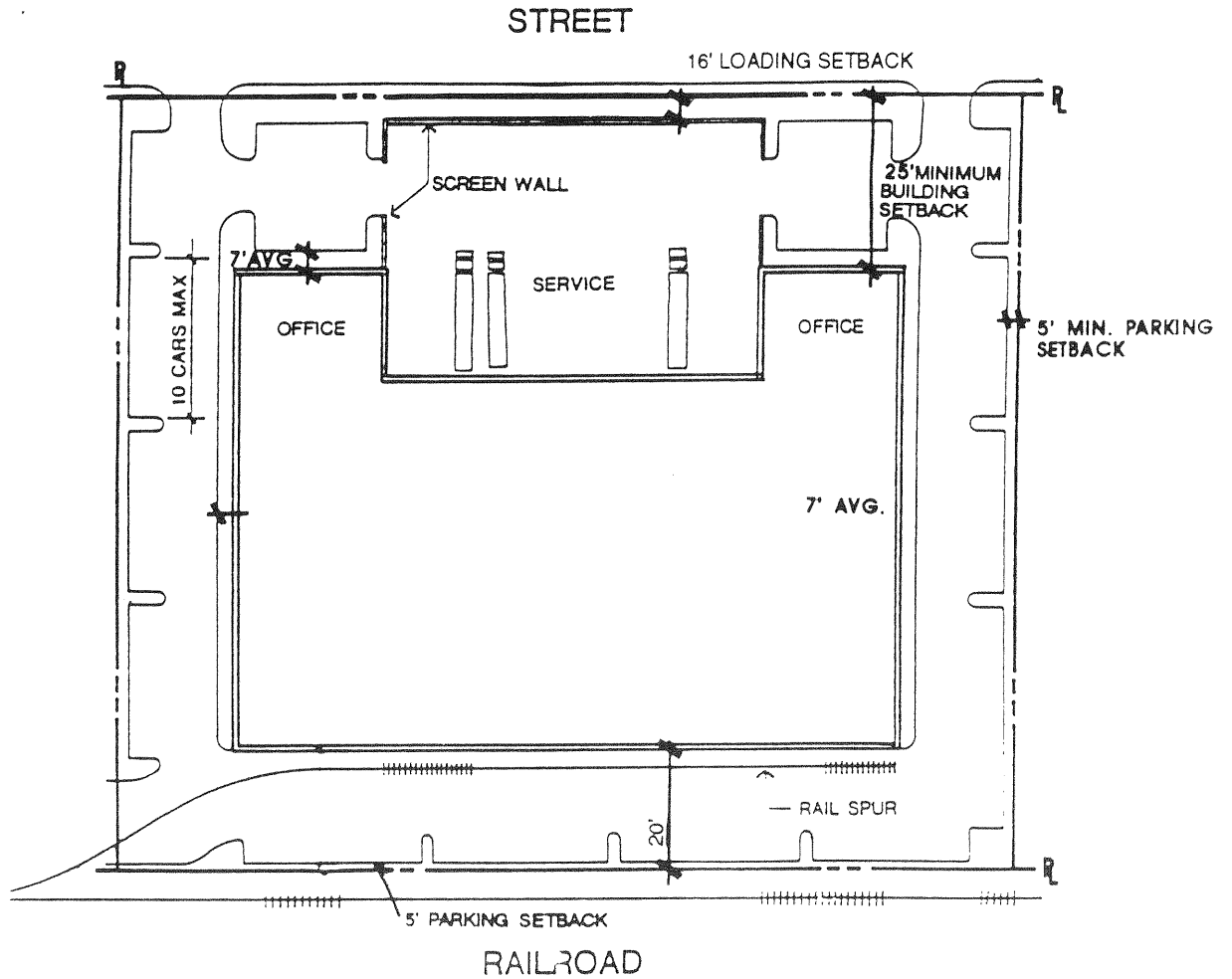


EXHIBIT 19

# RAIL INDUSTRIAL CONCEPTUAL SITE PLAN



NOTE: ALL SETBACKS MEASURED FROM PROPERTY LINE.

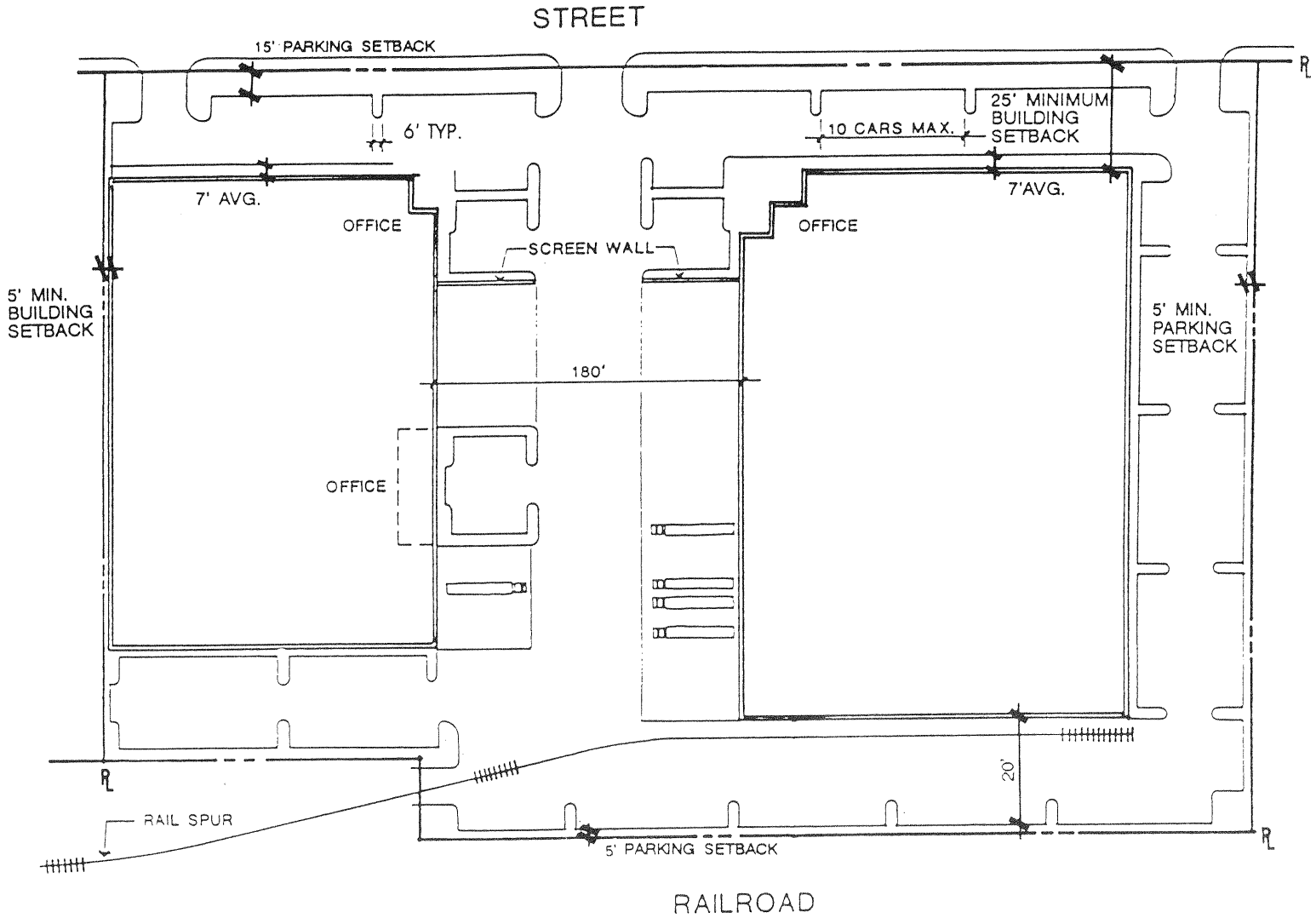
## SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES



EXHIBIT 20

# RAIL INDUSTRIAL CONCEPTUAL SITE PLAN



NOTE: ALL SETBACKS MEASURED FROM PROPERTY LINE.

## SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES



EXHIBIT 21

## **B. STREETScape/LANDSCAPE CONCEPT**

The master landscape concept is developed to provide an integrated, attractive and functional theme for the Shea Business Center through the use of high quality planting, entry monumentation and earth berming.

The concept calls for a structured arrangement of planting in clusters and masses that flow randomly through the site to create a park-like setting that will carry over into the individual parcels.

The master landscape concept will set the character and provide the structure for which individual site developers will follow so that the theme will be continued throughout the entire project. The continuity of the landscape concept to the individual parcels will help to create a "park-like" setting throughout the project.

The master plan recognizes and responds to various concerns specific to the development and to several regional concerns as well. Refer to Exhibit 22, Conceptual Landscape Plan.

### **1. REGIONAL CONCERNS**

- a. To select plants and arrange them in a manner that will be least affected by the severe wind conditions that exist in Ontario.
- b. To provide safety and enhance traffic movement through proper plant selection, alignment and set back.
- c. To further enhance the region's business park development by creating a quality environment in a park-like setting.
- d. Anticipating escalating water costs in the Southern California region over the foreseeable future, the following considerations in landscape planning at the project are of importance.
  - (1) The proposed plant materials, native and/or adaptive, have drought-tolerant qualities, as well as tolerance to withstand micro/macro climatic conditions, i.e., heat, frost and high velocity winds. The term "drought-tolerant" should not be interpreted to mean that irrigation is unnecessary. Once established, selected plants can be maintained on minimum water requirements.
  - (2) Due to high percolation rates of existing soils, a drip irrigation system should be used wherever possible to maximize results from applied water and required fertilizers.

### **2. DEVELOPMENT CONCERNS**

- a. To create distinctive entry statements through planting and monumentation designed to provide identity and to direct traffic into the project. The emphasis of the entry statements will be to orient travelers coming from all available directions.
- b. To create a landscape planting scheme along the San Bernardino Freeway (I-10) that distinguishes the business park from the adjacent developments.



- c. To provide a functional and attractive streetscape along Shea Center Drive that will allow for and enhance the entry development of the individual parcels while creating a continuous theme through the entire project.
- d. To utilize earth mounding, dense planting and screen walls to aid in the screening of areas such as loading zones, truck parking areas and railroad zones from sight lines along Shea Center Drive and the San Bernardino Freeway.

A more specific discussion of the various landscape elements follows.

### 3. LANDSCAPE CONCEPT MASTER PLAN

#### a. Entries

An important element of the landscape concept plan is the special treatment given to project entries. These areas serve to demarcate the boundaries of the business center as well as set the tone for the overall quality and structure located within the project. Refer to Exhibits 24 and 25. Signs and landscaped entry themes should be uplighted.

The primary entry to the project will be the west entrance that must cross below the Southern Pacific Railroad overpass as shown in Exhibits 24 and 25. This entry at Wineville and Airport Drive will utilize informal clusters of eucalyptus trees and palms to identify the entry and to reflect the plantings along the freeway side of the project. The understory to the street trees and eucalyptus clusters will consist of a hierarchy of small, medium and large shrubs, and accent trees arranged in free-flowing masses with each providing a different color, texture or structure. A median will extend out from the center line of the overpass so that additional plantings could be placed to further accentuate the entry.

#### b. Streetscape

Continuity of the canopy and vertical accent tree clusters developed at the project entry will occur on Shea Center Drive, the main street that travels through the project along the I-10 Freeway, and Airport Drive. The clusters will be located in structured pattern and will accentuate entries into individual sites while providing a pleasing look for vehicular traffic. Refer to Exhibit 22 and 22a. The understory planting along Shea Center Drive will serve to screen views from the street to such areas as loading zones, parking lots and railroad zones. Earth mounding will also help to screen these areas and serve to break up the flat terrain. Refer to Exhibit 22.

#### c. Freeway Edge

The location of this project adjacent to the San Bernardino Freeway allows for a superb opportunity for overall project identity. Because most users will be industrial in nature, sight lines and selective screening become very important and may be maintained by the use of visual buffer panels alternating with planted areas. To create an identity along the freeway, yet provide continuity with the "I-10 Corridor", trees located along the freeway must complement plantings along the I-10 and eucalyptus "clustering" shall be incorporated into the freeway landscaping. Refer to Exhibits 22 and 26.

At this time, a 20-foot landscaped setback area is proposed within the Shea Business Center adjacent to the freeway. In the event that less than 20 feet of

landscaping is provided within the Shea Business Center, the remaining area (to total 20 feet) must be provided within the freeway right of way. To coordinate landscape appearance and maintenance programs between Shea Business Center property and freeway property, a Cooperative Agreement may be executed between the developer, the City of Ontario and the State of California Department of Transportation (Caltrans) to achieve a mutually acceptable landscape program which has a unified visual effect from the freeway. Refer to Exhibit 26.

4. INDIVIDUAL SITE LANDSCAPING

a. On-Site Landscaping

Landscape criteria have also been established for landscaping of on-site areas. This landscaping will be the responsibility of individual parcel owners and will be reviewed and approved as part of the development site plan approval process.

These landscaped areas will include building and parking setbacks, parking areas, buffers, and areas directly adjacent to buildings. The landscape for these areas will provide a mixture of trees, shrubs, vines, groundcover, and turf as appropriate. A recommended plant palette has been provided that offers a variety of plant material that do well in this climate. However, since the water requirements of plant materials may vary extensively in particular planting areas, attention should also be given to installing irrigation systems that can regulate water requirements as needed.

The quantity and actual placement of trees, shrubs, groundcover, and turf shall be adequate to screen and soften buildings and their associated loading and parking areas from adjacent public streets. Such landscaping shall be designed with consideration given to parcel size and the intended building use.

b. Parking Lots

The landscaped edge along Shea Center Drive will serve to screen parking lot areas. Where parking is anticipated, heavier understory planting and earth mounding will protect vehicular traffic from viewing large expanses of parking lots. Development standards for individual parcel builders will be utilized to break up large parking areas with planting breaks or medians. Five percent of the parking lot area shall be landscaped.

Table 3

**RECOMMENDED PLANT PALETTE**

PLANTING PALETTE	Evergreen	Deciduous	Height	Southwest Exposure	Drought-Tolerant	Growth Rate	SEASONAL COLOR	COMMENTS
ARBUTUS UNEDO Strawberry Tree	•		35'	•	•	S	spring white	bears fruit
CINNAMOMUM CAMPHORA Camphor Tree*	•		40'			S	spring flowers	surface roots
ERIOBOTRYA JAPONICA Japanese Loquat	•		30'	•	•	S		bears fruit
GEIJERA PARVIFLORA Australian Willow*	•		30'	•	•	M	spring creamy white	
JACARANDA ACUTIFOLIA Jacaranda		•	30'	•	L	M	spring summer lavender-blue	
KOELREUTERIA BIPINNATA Chinese Flame Tree		•	25'	•	•	M	summer fall yellow	wind, smog tolerant
LAGERSTROENIA INDICA Crape Myrtle		•	20'	•	•	S	spring summer white, pink	

PLANTING PALETTE	Evergreen	Deciduous	Height	Southwest Exposure	Drought-Tolerant	Growth Rate	SEASONAL COLOR	COMMENTS
PHOENIX DACTYLIFERA Date Palm*	•		80'	•	•	S		hardy to 20 degrees
RHUS LANCEA African Sumac	•		25'	•	•	M		
<b>UPRIGHT TREES:</b>								
BRACHYCHITON POPULNEUS Bottle Tree*	•		40'	•	•	M	spring white	
EUCALYPTUS SPECIES Various Species	•		50'	•		F		wind tolerant some litter
PINUS HALEPENSIS Aleppo Pine	•		40'	•	•	F		wind tolerant
PINUS PINEA Stone Pine	•		50'	•	•	M		wind tolerant
<b>GROUNDCOVERS:</b>								
APTENIA CORDIFOLIA Harts and Flowers		•	2"	•	•	M	spring purple	
GAZANIA Gazania	•		10"	•	M	F	spring to winter yellow-red	

PLANTING PALETTE	Evergreen	Deciduous	Height	Southwest Exposure	Drought-Tolerant	Growth Rate	SEASONAL COLOR	COMMENTS
LONICERA JAPONICA HALLIANA Japanese Honeysuckle	•		18"	•	M	F	spring summer white-yellow	
Tall Fescue	•		6"	•	•			
TRACHELOSPERMUM JASMINOIDES Star Jasmine	•		20"	•		M	summer white	
VERBENA PERUVIANA Verbena		•	2"	•	•	F	varieties	
<b>LOW SHRUBS:</b>								
HEMEROCALLIS Day Lily	•		8'	•	-	M	summer fall yellow/orange	needs shade
JUNIPERUS CHINENSIS "PROCUMBENS NANA" Blue Rug Juniper	•		5'	•	•	S		
LANTANA MONTEVIDENSIS Lantana	•		6'	•	•	F	spring Posy lilac	
MORAEA IRIDIOIDES African Iris	•		4'	•	•	M	spring to fall white	

PLANTING PALETTE	Evergreen	Deciduous	Height	Southwest Exposure	Drought-Tolerant	Growth Rate	SEASONAL COLOR	COMMENTS
PITTIOSPORUM TOBIRA "WHEELERS DWARF" Wheeler's Pittosporum	•		15'	•	•	F	spring white	
<b>MEDIUM SHRUBS:</b>								
CARRISA GRANDIFLORA Natal Plum	•		7'	•	M	F	spring to winter red	wind, smog tolerant good hedge
OSMANTHUS HETEROPHYLLUS Holly-Leaf Osmanthus	•		8'	•	•	S	fall winter white	
RAPHIOLEPIS INDIICA India Hawthorn	•		5'	•	M	S	fall to spring pink	smog tolerant
XYLOSMA CONGESTUM "COMPACTA" Shiny Xylosma	•		9'	•	M	F		
<b>HIGH SHRUBS:</b>								
CASSIA ARTEMISIOIDES Feathery Cassia	•		10'	•	•	M	winter/summer grey-yellow	
FEIJOWA SELLOWIANA Pineapple Guava	•		12'	•	•	M		

<b>PLANTING PALETTE</b>	Evergreen	Deciduous	Height	Southwest Exposure	Drought-Tolerant	Growth Rate	SEASONAL COLOR	COMMENTS
LIGUSTRUM LUCIDUM Glossy Privet	•		8'	•	•	M		drops fruit
NERIUM OLEANDER Dwarf Varieties	•		12'	•	•	F	spring to fall white, pink	wind, smog tolerant good hedge
PHOTINIA FRASERI Photinia	•		10'	•	•	M	spring white	
VIBURNUM JAPONICUM Japanese Viburnum	•		15'	•	L	M	spring white	needs some shade
XYLOSMA CONGESTUM Shiny Xylosma	•		9'	•	M	F		
<b>VINES:</b>								
CLEMATIS ARMANDII Evergreen Clematis	•		20'	•	- -	F	spring white	shade tolerant needs support
CLYTOSTOMA CALLISTEGIOIDES Violet Trumpet Vine	•		20'	•	- -	F	spring to fall violet	shade tolerant needs support
GELSEMIUM SEMPERVIRENS Carolina Jessamine	•		20'	•	•	M	winter to spring white	

PLANTING PALETTE	Evergreen	Deciduous	Height	Southwest Exposure	Drought-Tolerant	Growth Rate	SEASONAL COLOR	COMMENTS
TRACHELOSPERMUM JASMINOIDES Star Jasmine	•		20'		•	M	summer white	

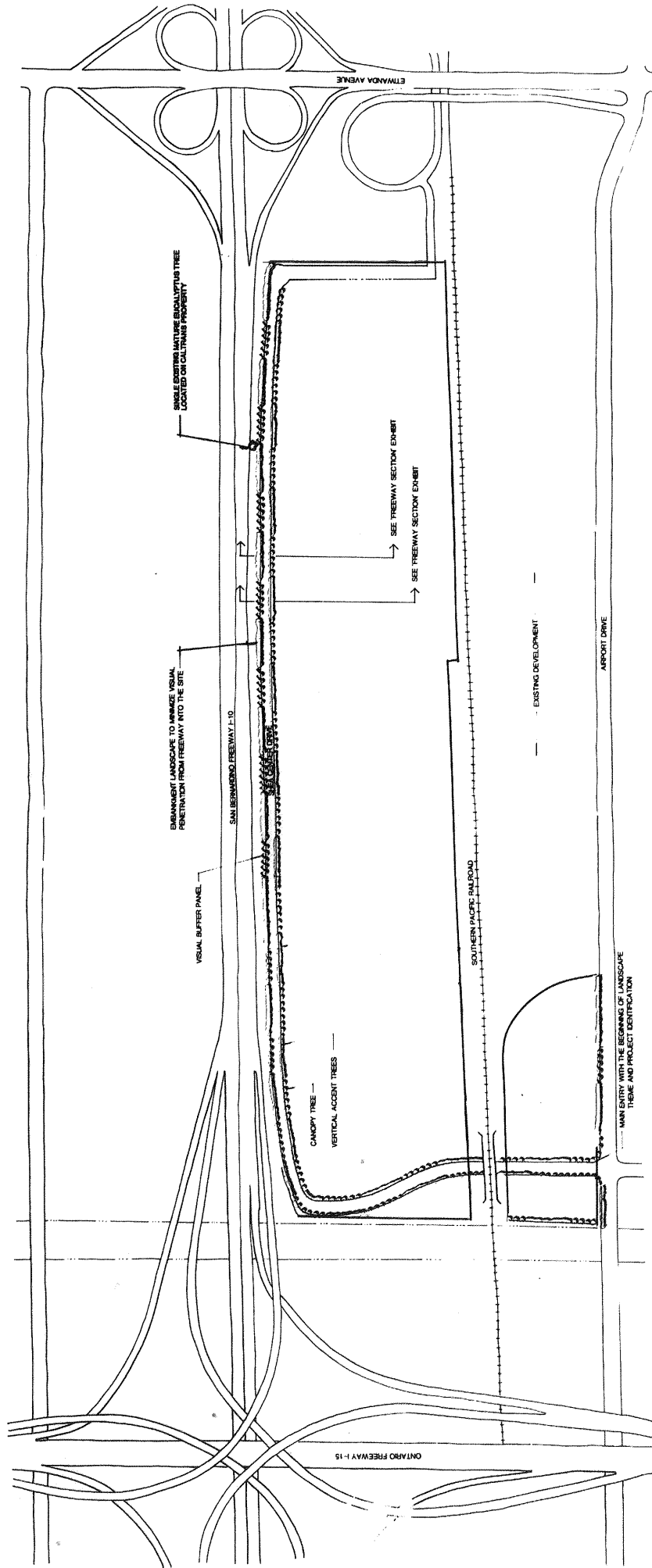
• Trees included in the "I-10 Corridor Beautification Guidelines," City of Ontario, August 5, 1992.

**GENERAL NOTES:**

- Spacing shall be as indicated by the landscaping plans prepared by the landscape architect.
- Shrubs located at entries and major intersections shall have a minimum size of five (5) gallons, with minor exceptions.



# Exhibit 22



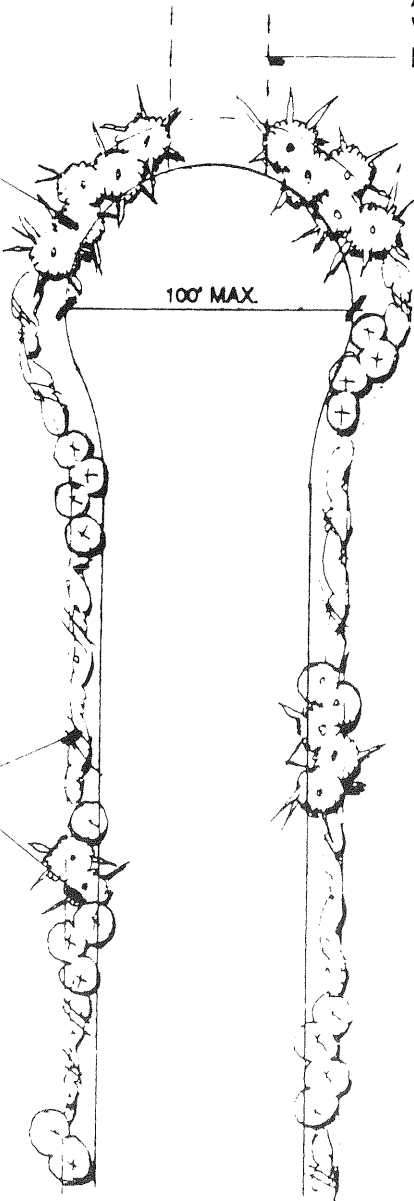
# LANDSCAPE PLAN

# SHEA CENTER DRIVE CUL DE SAC

Clusters of Vertical Accent Trees  
to Create a Visual Terminus

Connection to Emergency  
Access Road Shall be Made  
Via an Access Easement on  
Private Parcel

Groupings of Canopy Trees and  
Vertical Accent Trees with Hedge Screening



## SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

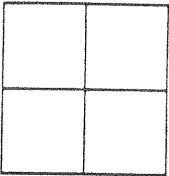
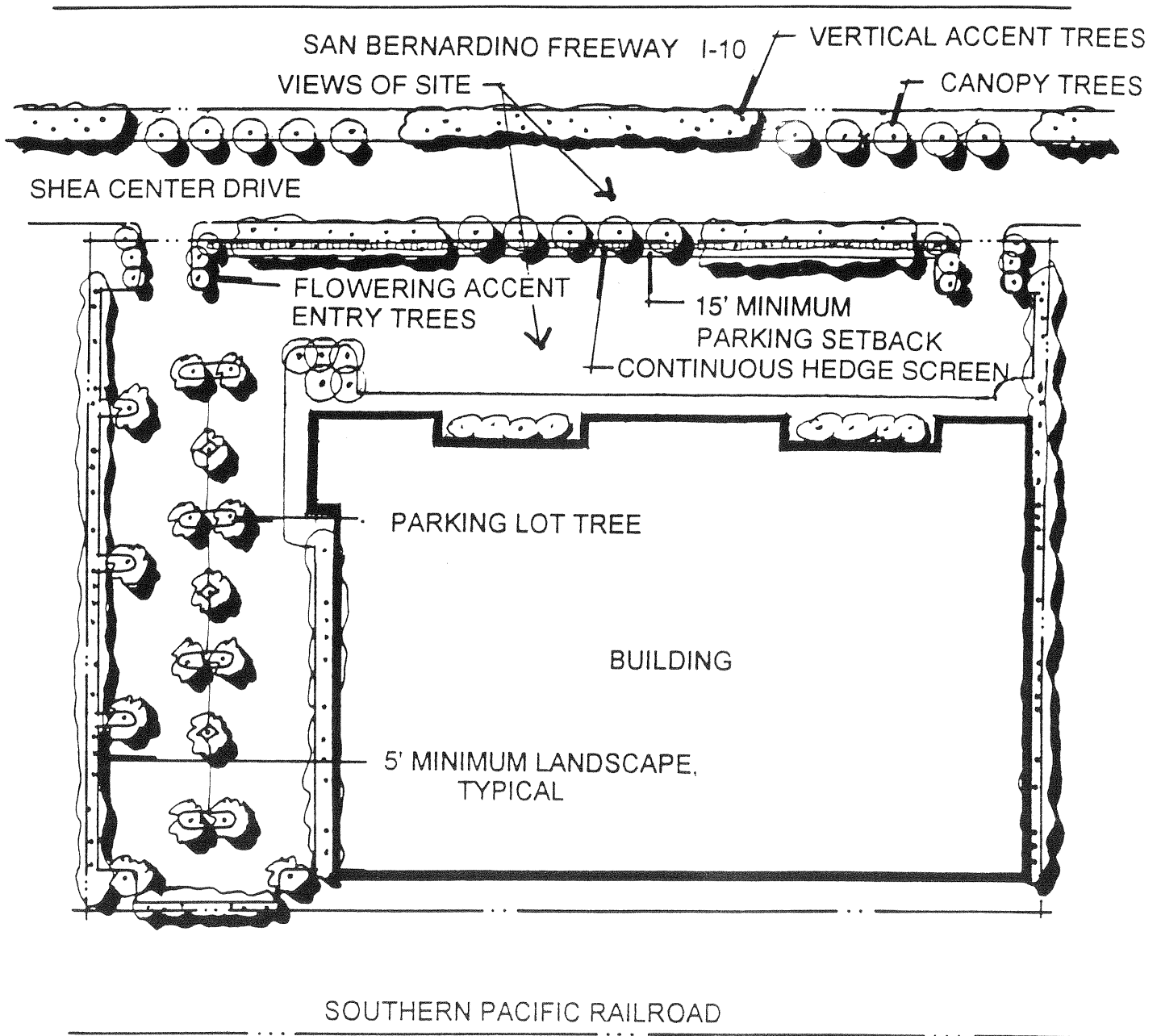


EXHIBIT 22a

# TYPICAL PARCEL DEVELOPMENT



## SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

Hill Pinckert Architects  
Wilson Associates

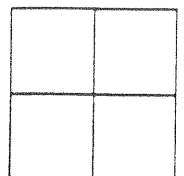
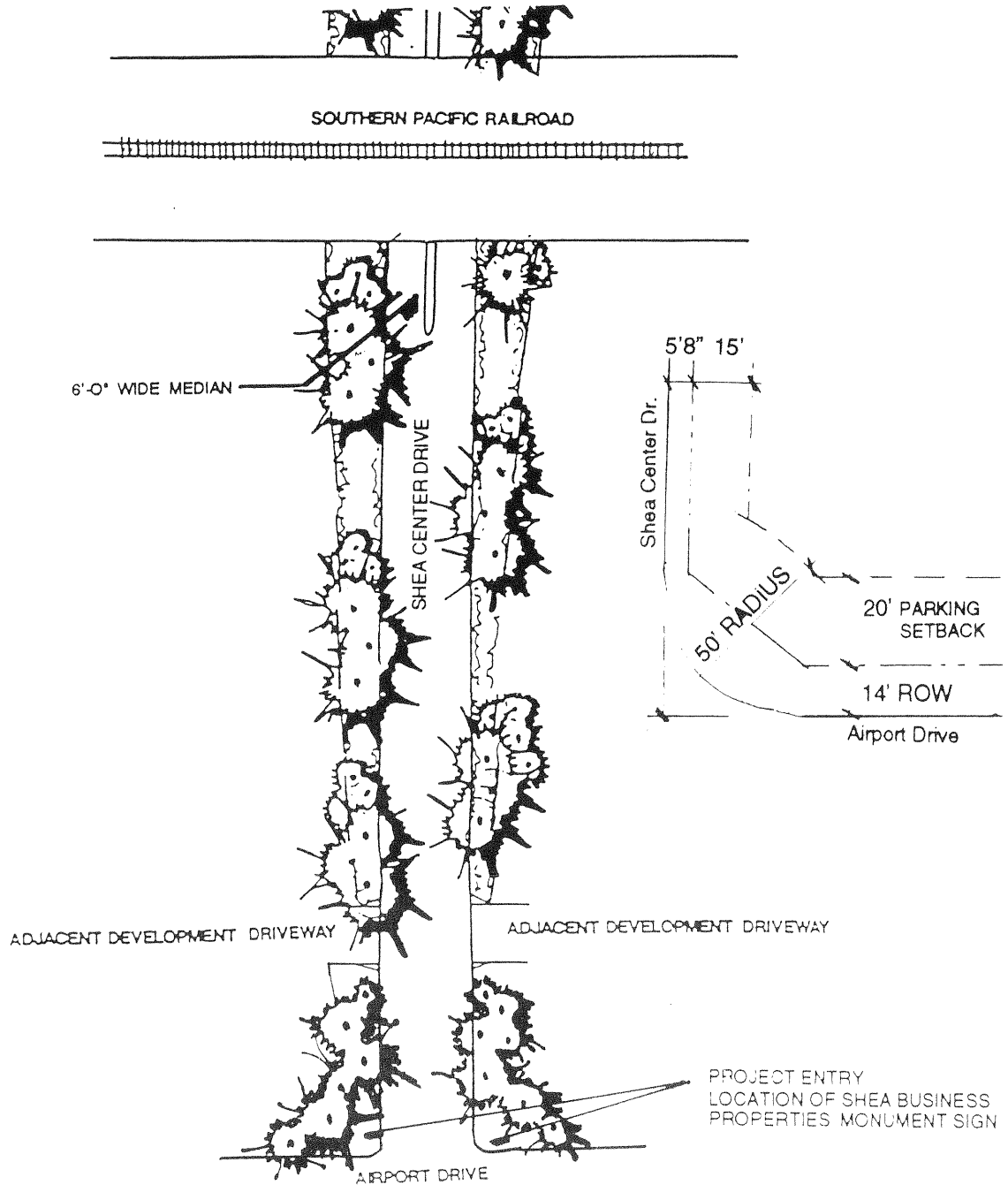


EXHIBIT 23

# MAJOR ENTRY



## SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

PLANNING NETWORK  
GILBERT AJA ASSOCIATE ARCHITECTS  
LIFESCAPES, INCORPORATED  
Yvonne Neal Associates

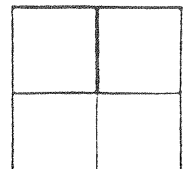
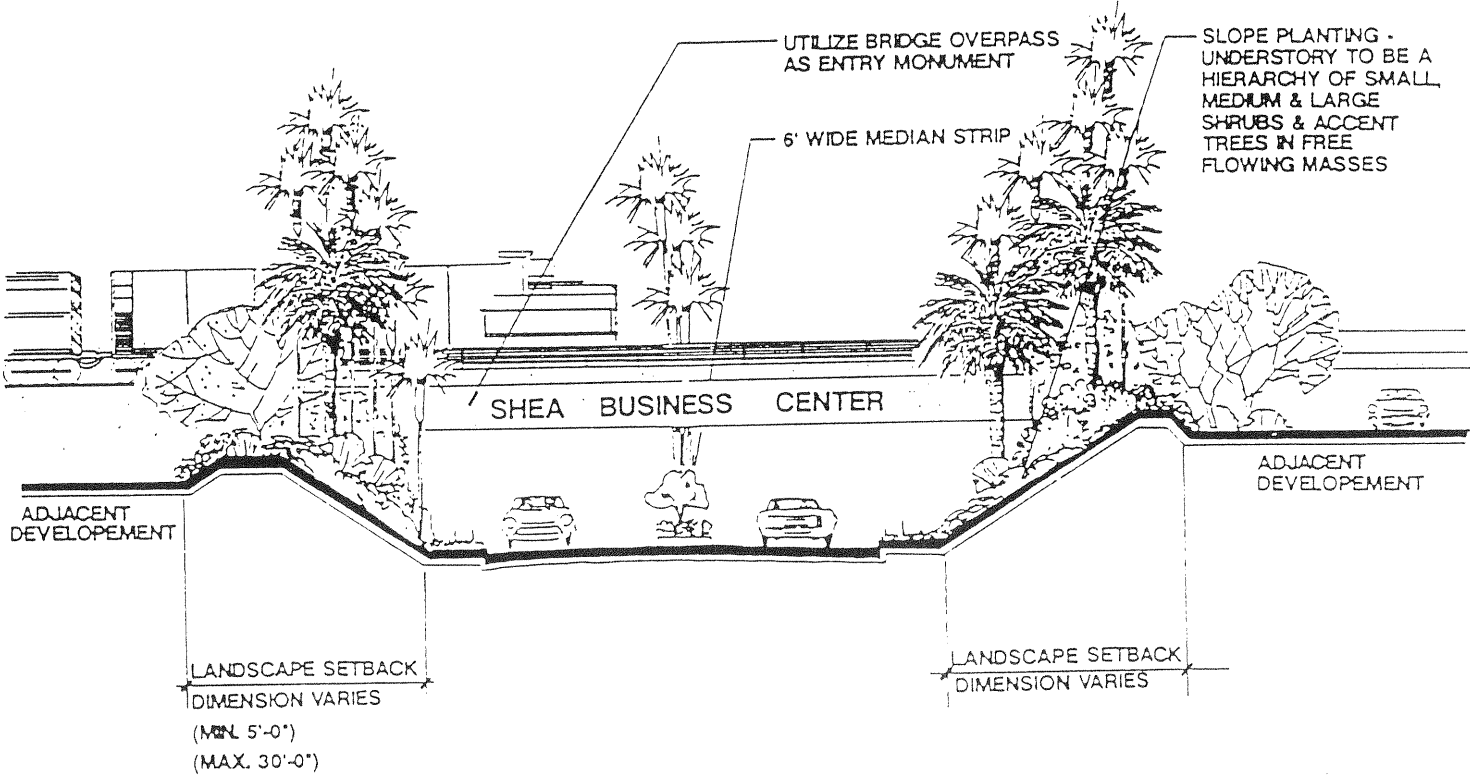


EXHIBIT 24

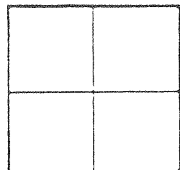
# MAJOR ENTRY

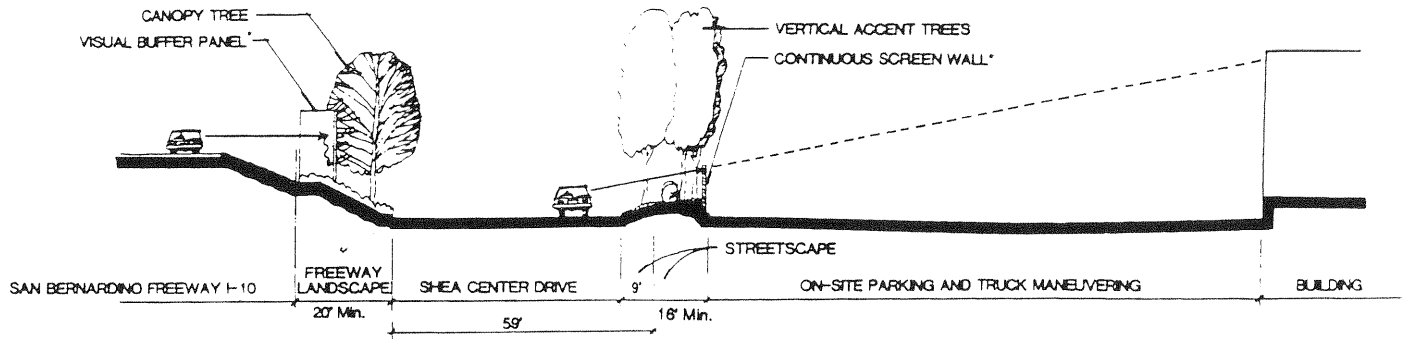


## SHEA BUSINESS CENTER SPECIFIC PLAN

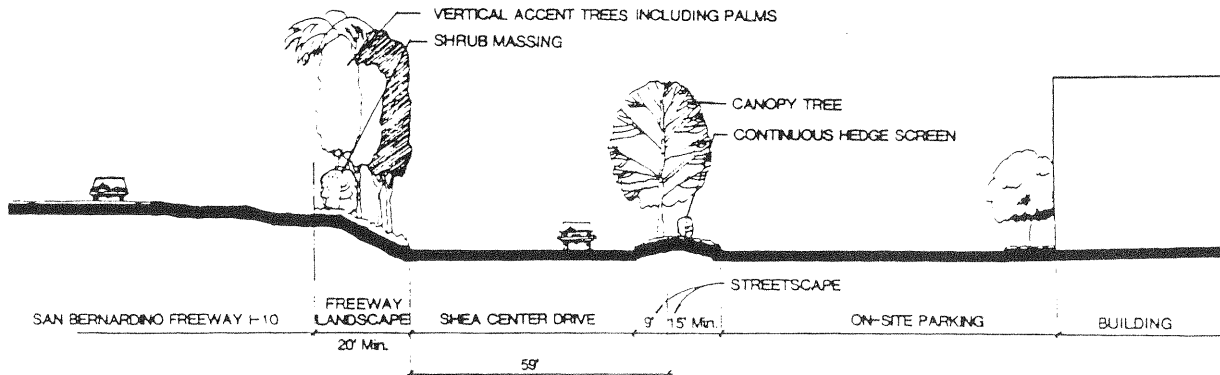
SHEA BUSINESS PROPERTIES

PLANNING NETWORK  
 GILBERT AJA ASSOCIATE ARCHITECTS  
 LIFESCAPES, INCORPORATED





## LOADING AREA ADJACENT TO PUBLIC STREET



## PARKING ADJACENT TO PUBLIC STREET

### SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

Wilson Associates  
**Yvonne m. Neal**  
 ASSOCIATES


EXHIBIT 26

## C. SIGN PROGRAM

The Shea Business Center signing program comprises an important element of the project's overall identity. The following guidelines and requirements are intended to produce consistency in sign design to reinforce the collective image of Shea Business Center as a quality business location.

All signs shall be designed and constructed in conformance with these guidelines and requirements, and the specifications issued by the Shea Business Center. A sign permit from the City of Ontario will also be required.

### 1. DEFINITION OF SIGN TYPES AND RELATED TERMS

#### a. Entry Statements/Master Identification

Signs, graphics, and landscape treatments at perimeter access points to the project defining the entries to the development.

#### b. Freestanding Identification

Signs along streets and roadways which identify facilities, businesses, tenants and addresses.

#### c. Building Identification

Signs mounted on the face(s) of buildings which identify the building or the major building tenant.

#### d. Complex Identification

Signs which are freestanding and identify a multi-building development.

#### e. Tenant Identification

Signs mounted on the face(s) of buildings or which are freestanding and identify a single tenant within the building.

#### f. Directional and Regulatory Signs

Signs within the development and within individual projects which control and direct the circulation of vehicles and pedestrians.

#### g. Temporary Signs

Any sign, barrier, pennant, valance, or advertising display used for marketing purposes for a short period of time.

#### h. Sign Area

The area of a sign having an integral part of a building, wall, awing, canopy, marquee, or other part of a structure as its background shall be the area enclosed within the shortest line drawn to include all letters,

designs, tubing, direct illumination sources, or other components of the sign, including all intervening spaces. The area of all other signs shall be the largest cross-sectional area measured to a line encompassing all portions of the sign, including the background and tubing, but excluding the supporting posts or poles without attached lighting. In determining the area of a sign having more than one face, only the area of one face shall be counted.

i. Halo Lit Letters (Individual)

Opaque, fabricated metal letter form with internal luminous tubing, mounted a few inches off face of building. Illumination falls only on building surface immediately adjacent to letter, creating halo effect.

j. Interior Illuminated Letters (Individual)

Fabricated letter form with internal luminous tubing and translucent acrylic face.

k. Post and Panel Sign

A post supports each end of a sign panel.

l. Flag Sign

Sign panel projects horizontally from a single pole.

m. Flush-Left Layout

Typography begins at left margin and any additional lines of typography are also flush with first line at left margin.

n. Centered Layout

Each line of typography is centered horizontally within the sign panel.

o. Integral Graphic Bond

Constant horizontal band or fascia area of an architectural complex where graphics must be placed.

## 2. GENERAL REQUIREMENTS FOR ALL SIGNS

- a. All owner/tenant identification signs shall conform to the guidelines of the "Specific Signage Requirements Summary" at the end of this section.
- b. A sign program shall be submitted in conjunction with the submittal of a site plan and/or architectural plans.
- c. All signing shall be of materials compatible with exterior building colors, materials and finishes, and be of a high quality of fabrication.



- d. No signing will be permitted which does not directly relate to the primary service or function of the given owner/tenant activity.
  - e. All owner(s)/tenant(s) shall be responsible for the proper maintenance of their signs.
  - f. On notice by the City of Ontario or the Shea Business Center approving agent, an owner/tenant may be required to restore or repair any sign which is not properly maintained.
  - g. Signs are to be free of all labels and fabricator's advertising, except for those required by code.
  - h. All electrical service to any sign shall be fully concealed, and shall be on the owner/tenant's meter.
  - i. All signs will conform to appropriate building and electrical codes and bear the U.L. label if illuminated. The owner/tenant and contractor shall be responsible for obtaining any and all permits required.
  - j. Owners/tenants will be responsible for the design, fabrication, and installation of individual owner/tenant signs.
  - k. Each individual owner/tenant shall submit three (3) sets of professionally executed sign drawings for approval by approving agent and the City of Ontario. These drawings shall be of a scale of 1" = 1' or larger, showing sign locations, size, layout, design, colors, letter styles, and materials.
  - l. All permits and fee requirements for signs shall be obtained from the City of Ontario and paid for by the owner/tenant prior to installation.
  - m. No sign shall have visible moving parts or simulate movement by means of fluttering, rotating, or reflecting devices.
  - n. No sign shall have flashing, blinking or moving lights, or any other illuminating device which has changing light intensity, brightness or color.
  - o. No can signs will be allowed.
3. GENERAL REQUIREMENTS FOR GROUND-MOUNTED SITE IDENTIFICATION SIGNS
- a. No more than one site identification sign may be permitted per street entry, with a maximum of two such signs per street. These signs may be single or double faced.
  - b. All double faced site identification signs located within the parking setback area must be aligned perpendicular to the street. Single faced signs may be aligned parallel to the street or at a 45° angle.
  - c. Site identification signs and on-building signs (see subsection 3) may be located in the same street frontage only if the monument signs contain the name of the project or complex and not the name of any tenant.
  - d. Sign face copy is limited to a single name off the project, complex, building, or tenant occupying the site.

- e. All freestanding signs shall be integrated with adjacent landscaping and grading.
- f. Design for the signs will consist of a standard base common to the Shea Business Center and a customized cap to accommodate the message content. The caps may be constructed of a variety of materials, provided, however, that such materials are compatible with adjacent architecture. Sign materials may include concrete, stucco, brushed or polished metal, anodized aluminum, ceramic tile, granite, wood, fiberglass, or equivalent materials.
- g. Graphics on the sign will consist of the tenant name logo, and/or address.

4. GENERAL REQUIREMENTS FOR ON-BUILDING TENANT IDENTIFICATION

On building identification signs include parapet (top-of-building) signs, eyebrow (top of first floor) signs, multi-tenant entry door signs, and street address signs.

- a. All building-mounted signs shall be constructed so as not to have exposed writing, raceways, ballasts, conduit and transformers.
- b. Allowable materials for on-building signs include acrylic, bronze, brass, stainless steel, painted aluminum, and fiberglass. High density foam will require special approval. Applied vinyl or silkscreen lettering is acceptable only on or adjacent to entry doors and rear service doors.
- c. Parapet Level Signs
  - (1) On-building tenant identification signs located at the parapet level will serve to identify the entire building, and are therefore limited to one primary tenant name only. Two or more different names are not allowed at the parapet level.
  - (2) A maximum of one parapet level sign is allowed per building elevation, and a maximum of two such signs will be permitted per building. Parapet signs may not be located on adjacent sides of the same building corner.
  - (3) Buildings one to three (1-3) stories in height may not have parapet and eyebrow signs on the same building elevation. Buildings four (4) stories or more may have a maximum of one (1) parapet and two (2) eyebrow signs on the same elevation.
- d. Eyebrow Level Signs
  - (1) On-building identification signs for tenants other than a single primary tenant must be located at eyebrow level.
  - (2) A maximum of two (2) eyebrow signs are allowed per building elevation.

(3) Buildings one to three (1–3) stories in height may not have parapet and eyebrow signs on the same building elevation. Buildings four (4) stories or more may have a maximum of one (1) parapet and two (2) eyebrow signs on the same elevation.

e. Multi-Tenant Entry Door Signs

Tenant identification for multi-tenant facilities with individual entrances is limited to graphics located directly above or adjacent to the entry door of each tenant. The color of the foreground and background for each such sign shall be identical within an individual building, as shall be lettering style.

f. Retail Signage

Retail tenants (such as shops, restaurants, and banks) may be allowed additional flexibility in the number, color, letter style, and lighting of on-building tenant identification graphics, subject to specific approval, provided that these signs are compatible with the overall planned sign program.

g. Building Address Signs

(1) Building address numbers shall be displayed on every building as required for clear identification of the building.

(2) The numerals shall face the street, access road, or approach walkway as necessary. The address numbers shall be non-illuminated, and shall be of a material and form consistent with other building identification signs used on the same building.

5. SHEA BUSINESS CENTER IDENTIFICATION SIGNS

Shea Business Center Identification signs will consist of a pylon sign visible from the San Bernardino Freeway and a monument sign on the corner of Airport Drive and Wineville Avenue (see Exhibits 24 and 27a). In addition, signage may be allowed as an integral part of the railroad undercrossing. Design and materials for a sign on the railroad bridge must be reviewed and approved by the City prior to construction of the undercrossing (Exhibit 25).

Pylon signage visible from the freeway was selected because of its stability in the windy conditions common to the area. Signage detail will include colored banding and channeled lettering. Regulations for the Freeway Identification Sign include: an overall height not to exceed 35 feet, a vertical dimension of the sign face not to exceed 25 feet, and a horizontal dimension of the sign face not to exceed 25 feet. In the case of either the pylon freeway sign or monument entry signs, the display surface shall not exceed 300 square feet.

6. SINGLE OWNER/TENANT IDENTIFICATION SIGN REQUIREMENTS

a. Street Identification

Within each property, there are a number of requirements for signs. The primary need is owner/tenant or facility identification.

- (1) In the case of a single owner/tenant building, the sign will be a freestanding monolith placed adjacent to the main entry to the property.
- (2) Typography may vary according to the user's identity.
- (3) Single owner/tenant identification signs shall be indirectly illuminated.

b. Building-Mounted Identification

- (1) Single owner/tenant facilities may also have the owner/tenant's business name and/or logo mounted on the face of the building in an architecturally appropriate position.
- (2) These graphics shall be aluminum or metal plate elements, individually mounted.
- (3) Individual letters or logos may have interior illumination; metal "can" signs will not be allowed. All conduits, raceways and wiring shall be subsurface; no clips or support brackets may be visible from the front elevation.
- (4) The scale and proportion of graphics shall be in consonance with building architecture.
- (5) All design and layouts shall be reviewed and approved by Shea Business Center approving agent through site plan review prior to obtaining a sign permit from the City.

7. MULTIPLE TENANT OFFICE, INDUSTRIAL, OR COMMERCIAL BUILDING OR MULTIPLE BUILDING COMPLEX SIGNAGE

For each multiple tenant building or multiple building complex, a customized signage program will be implemented to identify the individual businesses at their respective entries. Such sign programs shall be reviewed and approved by the Shea Business Center approving agent and the City of Ontario. Sign programs will be reviewed and approved by Shea Business Center's approving agent prior to submittal to the City for approval.

These sign programs will be based on the architectural style and detailing of the building (or building complex), and will include form, size, and finish of the elements and their relationship to entries, fenestration, structural members, and materials.

Directional and regulatory signs within a property will conform to the standard sign system for the entire Shea Business Center. These will be post and panel or flag signs. Directional signs will be of a modular nature to allow for additions or deletions.

8. SERVICE STATION SIGN REQUIREMENTS

a. All Service Stations

Promotional graphics are not permitted.

Glass areas on store fronts should remain free of graphics. Only the hours of operation and other pertinent information are allowed. The amount and size of copy should be unobtrusive and constructed of white die-cut vinyl letters with a 4-inch maximum height.

b. Self Service Only Stations

Self service only stations are allowed two canopy-mounted signs. Price signs should be either column-mounted (on the canopy support) or pump-mounted. Promotional graphics are not permitted.

c. Full Service and Combination Full/Self Service Stations

Price signs should either be mounted on the column of the canopy or pump-mounted, as should "Full/Self Service" signs.

Outside displays of merchandise are discouraged (i.e., racks of automobile tires). When such displays are used, however, they must be kept within the canopy area.

9. DIRECTIONAL AND REGULATORY SIGNS

Directional signs provide functional directions, such as "shipping and receiving." Regulatory signs control vehicular movement, such as "handicapped parking only." These signs will be post and panel, and flag-type signs, constructed of aluminum, fiberglass, or equivalent materials. Size, design, layout, and color shall conform to project standard (to be submitted with overall sign design). Copy will be as succinct as needed to convey the message. Signs will be located as utility and safety dictate, with placement approved by Shea Business Center's approving agent. There shall be no more than two directional/regulatory signs per driveway.

(a) Traffic Control Signage

All traffic signage shall conform to the Manual on Uniform Traffic Control Devices, published by the U.S. Department of Transportation, Federal Highway Administration, and the City of Ontario standards.

## 10. TEMPORARY SIGNS

### a. Free-Standing Signs

The developer of each facility/building within the Shea Business Center may display one temporary free-standing sign whose purpose is to disseminate information pertinent to a site and its stage of development. The sign is to be designed to conform to the Shea Business Center standards and submitted for approval to Shea Business Center's approving agent prior to its installation on the site. No temporary promotional signs will be allowed on the premises of the adjoining public street rights-of-way.

A sign may be constructed on a site any time after the site has been placed in escrow. Information can be added, or the sign can be exchanged for another, to indicate the advent of construction, or to recruit employees, or to identify the leasing agent. However, each revision or sign replacement must conform to the guideline's criteria. A sign that is to be replaced with another must be removed before the other sign can be installed. Temporary signs must be removed from the site when the initial leasing program is 95 percent complete.

- (1) Form – Free-standing monolith with panels which meet the grade.
- (2) Scale – Rectangular ratio of height to width shall be 2:1 or width to height shall be 2:1. Total area not to exceed 98 square feet.
- (3) Materials – Designed to last the length of its intended use without significant fading, peeling, blistering, warping, cracking, rotting or delamination. Shea Business Center reserves the right without liability to cause removal of any sign deemed to be in violation of the provision by virtue of deterioration or damage.
- (4) Duration – Temporary signs shall remain in place for no more than twelve (12) months. This period may be extended upon approval by Shea Business Center's approving agent and the City of Ontario.
- (5) Security Deposit – A security deposit fee, pursuant to adopted fee ordinance, shall be posted with the City of Ontario to guarantee removal of the temporary sign(s). Failure to remove sign(s) after specified duration will result in forfeiture of security deposit.

b. Wall Signs

One temporary sign announcing the opening of a new business is permitted subject to the following requirements:

- (1) The total area of all such signs or advertising devices shall not exceed the area of permanent signs for the use permitted by these sign criteria.

No such device shall be located in a manner not permitted for permanent signs.

- (2) The temporary signs may remain in place for a period not to exceed thirty (30) days after the date of installation of the sign, or until a permanent sign is installed, whichever occurs first.
- (3) Prior to submittal of an application to the City of Ontario for installation of the temporary sign, the proponent shall obtain approval from Shea Center Ontario's approving agent.

**MASTER SIGN PLAN**

**INDUSTRIAL**

CATEGORY	COMPLEX IDENTIFICATION	TENANT IDENTIFICATION
Sign Type Number of Signs	Monument Sign 1 per complex per street frontage	Wall Sign 1 per occupant
Placement and Location	Perpendicular or 45° angle to street, minimum 20 feet from driveway, minimum 15 feet from curb	At primary entrance
Sign Area Letter Size Sign Height	60 Square Feet Maximum 24 inches 54" maximum height to length ratio not to exceed 1:3	40 Square Feet Maximum 24 inches maximum height n/a
Forms and Materials	Must relate to architectural style	Individual letter; metal, fiberglass, and acrylic plexiglass
Message and Layout	May be two-sided; name of owner/tenant or building and street address, centered layout or flush left	Tenant or business name and/or logo
Color	Must relate to architectural design	Must obtain association approval
Illumination	Ground lit	Interior illuminated

**NOTE: A sign program is required for all multi-building complexes.**



# INDUSTRIAL COMPLEX IDENTIFICATION SIGN

## FREESTANDING IDENTIFICATION SIGN



NOT TO EXCEED  
60 SQUARE FEET  
MAXIMUM

24" MAXIMUM  
LETTER SIZE

54" MAXIMUM  
HEIGHT TO LENGTH  
RATIO NOT TO  
EXCEED 1:3

## SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

PLANNING NETWORK  
GILBERT AJA ASSOCIATE ARCHITECTS  
LIFESCAPES, INCORPORATED

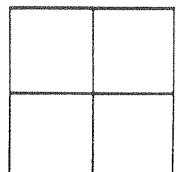
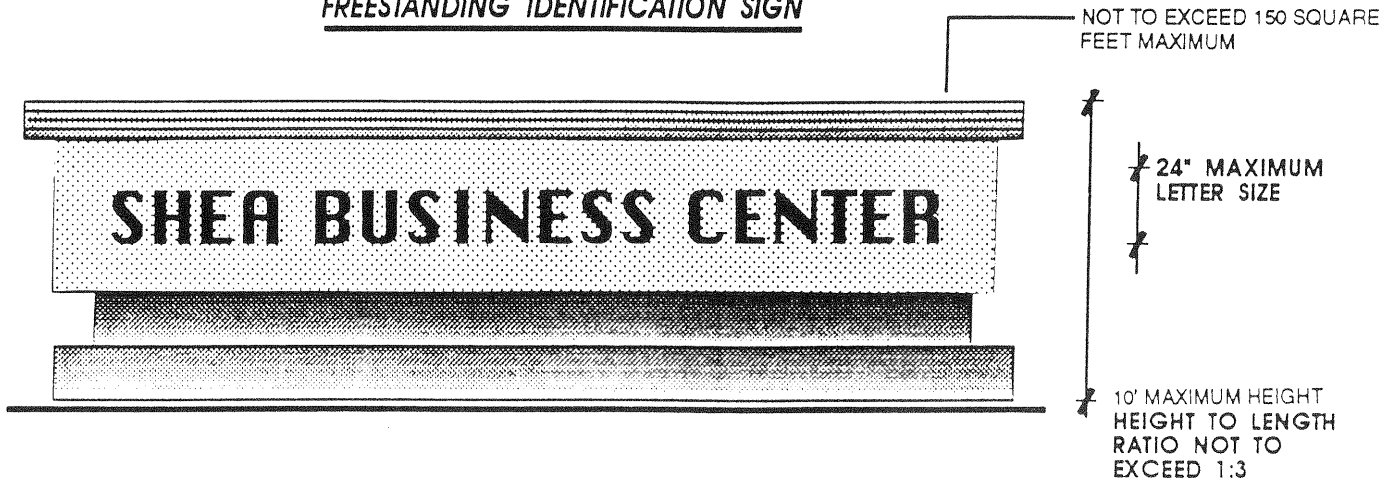


EXHIBIT 27

# SHEA BUSINESS PROPERTIES IDENTIFICATION SIGN

## FREESTANDING IDENTIFICATION SIGN



## SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

Yvonne Neal Associates

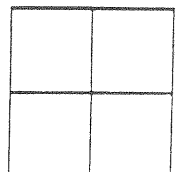
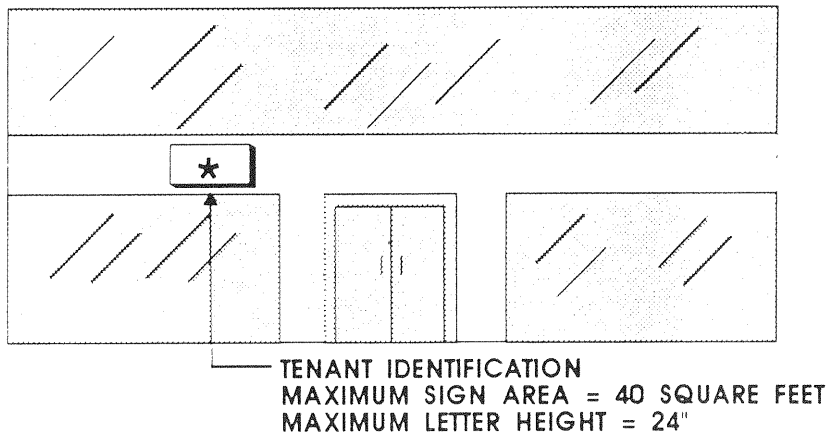


EXHIBIT 27a

# INDUSTRIAL TENANT IDENTIFICATION SIGN

## TENANT IDENTIFICATION SIGN



NOTE: TENANT SIGN MAY BE LOCATED ON AWNINGS, CANOPIES OR SIMILIAR TREATMENT.

MORE THAN ONE TENANT MAY DISPLAY COPY ON SIGN.

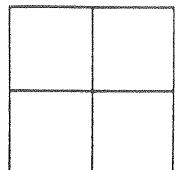
ONE (1) TENANT IDENTIFICATION SIGN PER TENANT.

\*NOT INTENDED TO IMPLY GEOMETRIC SHAPE OR SPECIFIC LOCATION.

## SHEA BUSINESS CENTER SPECIFIC PLAN

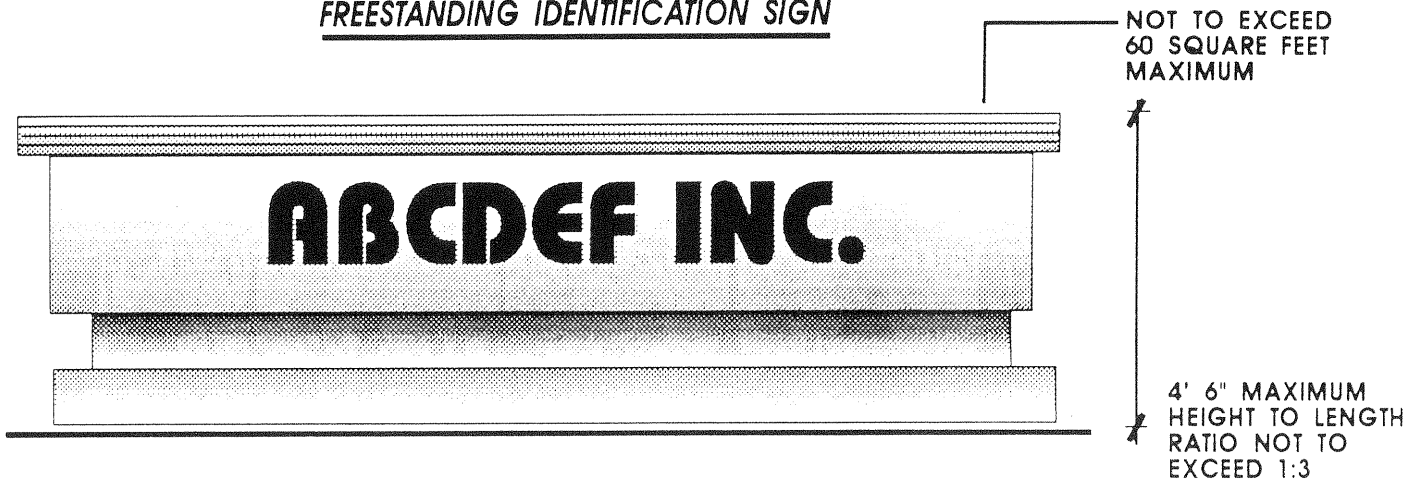
SHEA BUSINESS PROPERTIES

PLANNING NETWORK  
GILBERT AJA ASSOCIATE ARCHITECTS  
LIFESCAPES, INCORPORATED



# BUSINESS PARK COMPLEX IDENTIFICATION SIGN

## FREESTANDING IDENTIFICATION SIGN



## SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

PLANNING NETWORK  
GILBERT AJA ASSOCIATE ARCHITECTS  
LIFESCAPES, INCORPORATED

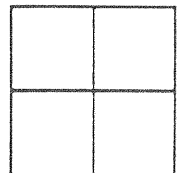
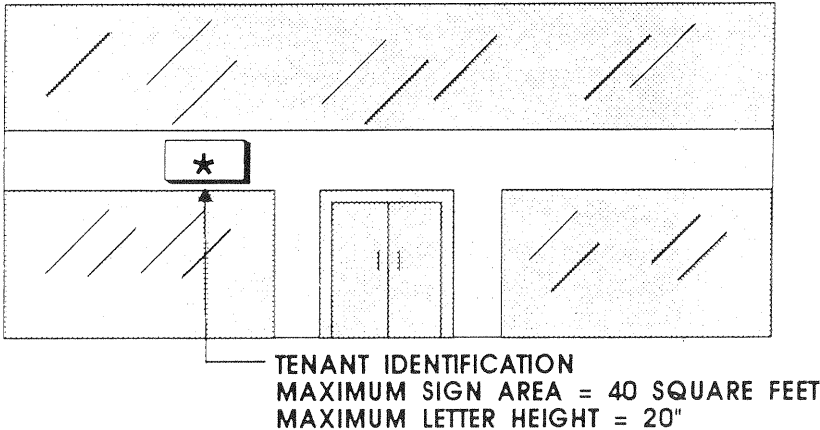


EXHIBIT 29

# BUSINESS PARK TENANT IDENTIFICATION SIGN

## TENANT IDENTIFICATION SIGN



NOTE: TENANT SIGN MAY BE LOCATED ON AWNINGS, CANOPIES OR SIMILIAR TREATMENT.

MORE THAN ONE TENANT MAY DISPLAY COPY ON SIGN.

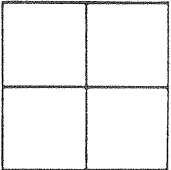
ONE (1) TENANT IDENTIFICATION SIGN PER TENANT.

\*NOT INTENDED TO IMPLY GEOMETRIC SHAPE OR SPECIFIC LOCATION.

## SHEA BUSINESS CENTER SPECIFIC PLAN

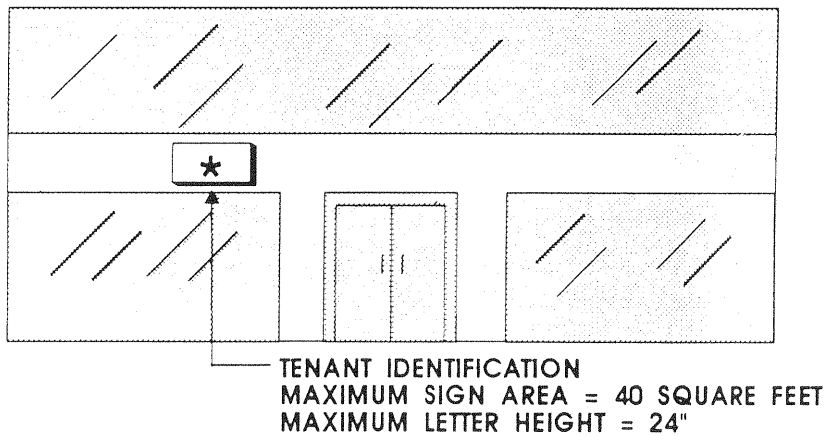
SHEA BUSINESS PROPERTIES

PLANNING NETWORK  
GILBERT AJA ASSOCIATE ARCHITECTS  
LIFESCAPES, INCORPORATED



# SHOWROOM RETAIL TENANT IDENTIFICATION SIGN

## TENANT IDENTIFICATION SIGN



NOTE: TENANT SIGN MAY BE LOCATED ON AWNINGS, CANOPIES OR SIMILIAR TREATMENT.

MORE THAN ONE TENANT MAY DISPLAY COPY ON SIGN.

ONE (1) TENANT IDENTIFICATION SIGN PER TENANT.

\*NOT INTENDED TO IMPLY GEOMETRIC SHAPE OR SPECIFIC LOCATION.

## SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

PLANNING NETWORK  
GILBERT AJA ASSOCIATE ARCHITECTS  
LIFESCAPES, INCORPORATED

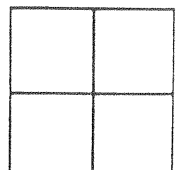


EXHIBIT 31

**MASTER SIGN PLAN  
SHOWROOM RETAIL**

---

CATEGORY	TENANT IDENTIFICATION
Sign Type	Wall Sign
Number of Signs	1 per street or freeway frontage
Placement and Location	1 per street or freeway frontage
Sign Area	40 Square Feet Maximum
Letter Size	24 inches maximum height
Sign Height	n/a
Forms and Materials	Individual letter; metal, fiberglass, and acrylic plexiglass
Message and Layout	Tenant or business name and/or logo
Color	Must obtain association approval
Illumination	Interior illuminated

**NOTE: A sign program is required for all multi-building complexes**

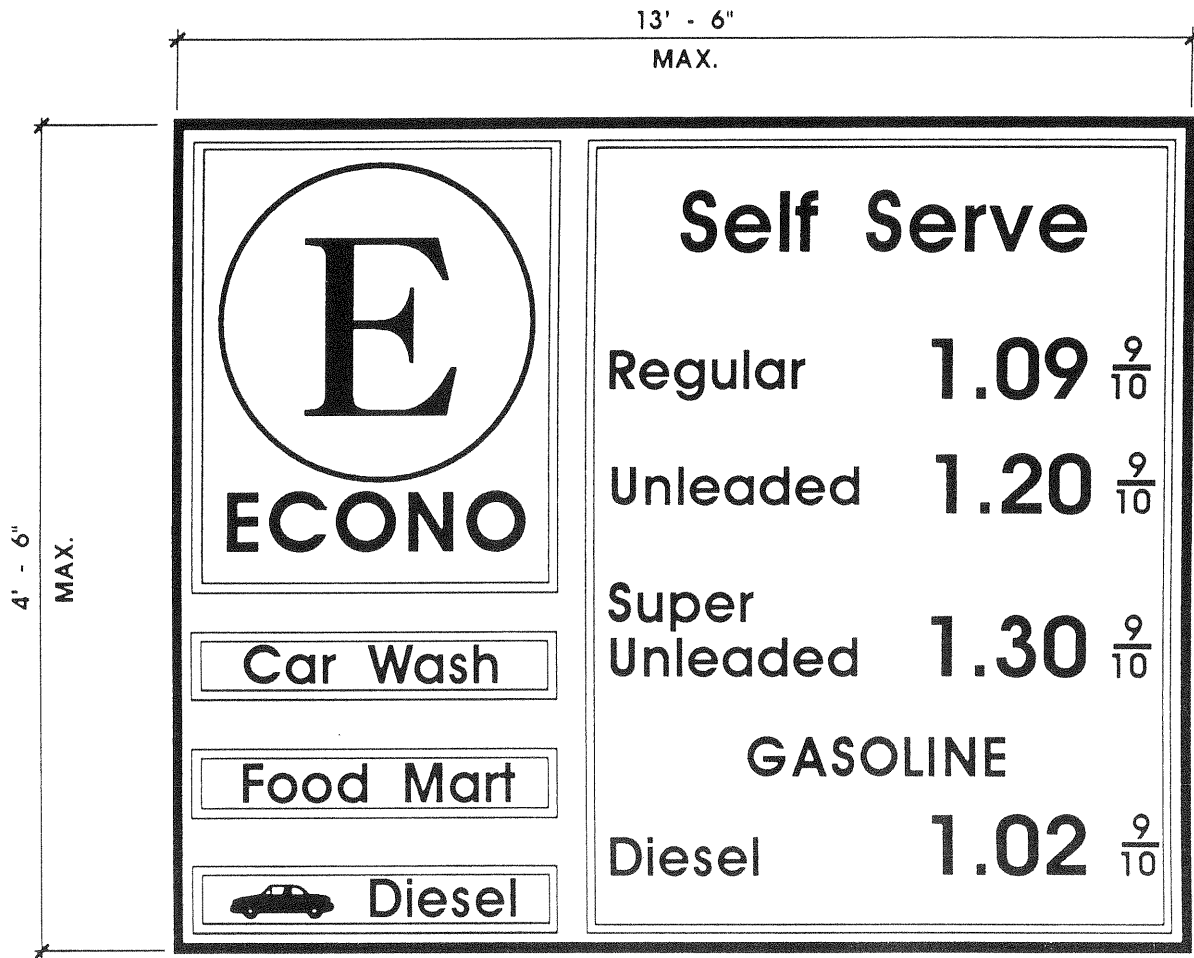
**MASTER SIGN PLAN  
SERVICE STATION**

CATEGORY	TENANT IDENTIFICATION
Sign Type	Freestanding
Number of Signs	1 (with possibility for 2 if station fronts 2 streets)
Placement and Location	Adjacent to entry; Perpendicular to street
Sign Area	60 square feet
Letter Size	n/a
Sign Height	54 inches maximum height to length ratio not to exceed 1:3
Forms and Materials	Base: brick, concrete, tile, metal, or stone Cap: translucent face, vacuum form plastic, acrylic or fiberglass
Message and Layout	Company logo only; two-sided
Color	Must obtain association approval
Illumination	Interior illuminated

**NOTE: A sign program is required for all multi-building complexes**



# SERVICE STATION TENANT IDENTIFICATION SIGN



NOTE: MAXIMUM SIGN AREA SHALL NOT EXCEED 60 SQUARE FEET, NOTWITHSTANDING MAXIMUM SIGN DIMENSIONS INDICATED

## SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

PLANNING NETWORK  
GILBERT AJA ASSOCIATE ARCHITECTS  
LIFESCAPES, INCORPORATED

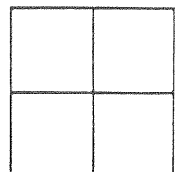


EXHIBIT 32

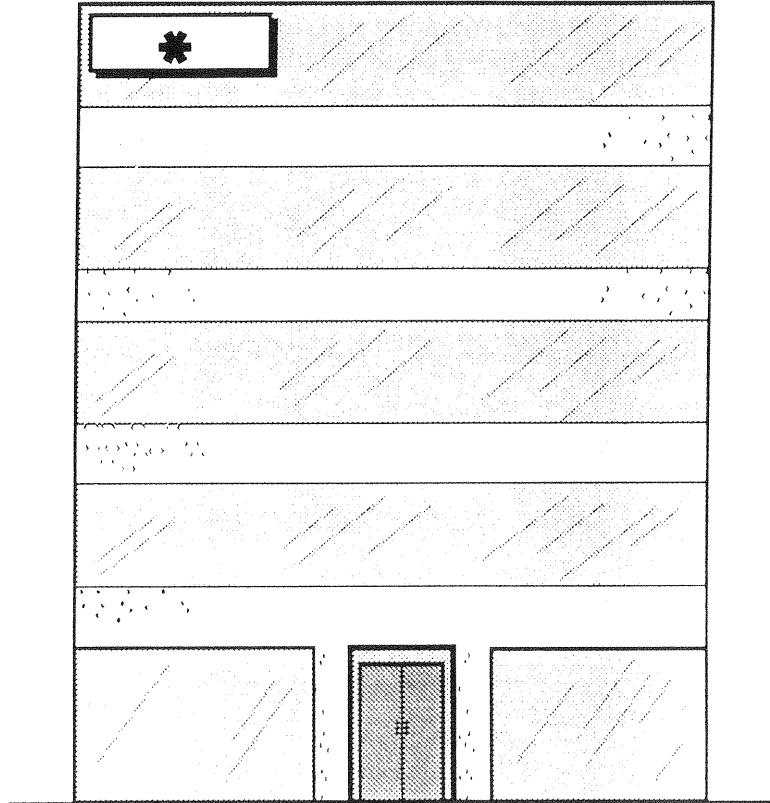
---

**MULTI-STORY  
SIGN CRITERIA**

---

NUMBER OF STORIES	SIGN AREA	MAXIMUM HEIGHT OR SIGN OR LETTERS
3	200 Square Feet	20 Inches
4	225 Square Feet	25 Inches
5	250 Square Feet	30 Inches
6	275 Square Feet	40 Inches
7	300 Square Feet	45 Inches
8 - 10	350 Square Feet	50 Inches

# MULTI-STORY SIGN CRITERIA



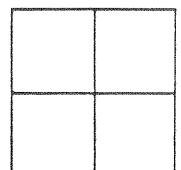
BUILDING HEIGHT STORIES	SIGN SIZE TOTAL SQUARE FEET	MAXIMUM SIZE HEIGHT
3	200	20"
4	225	25"
5	250	30"
6	275	40"
7	300	45"
8-10	350	50"

\* NOTE: THIS DIAGRAM IS INTENDED TO DESCRIBE THE AREA AND HEIGHT CONSTRAINT, BUT NOT INTENDED TO DEFINE THE GEOMETRIC SHAPE OR SPECIFIC LOCATION.

## SHEA BUSINESS CENTER SPECIFIC PLAN

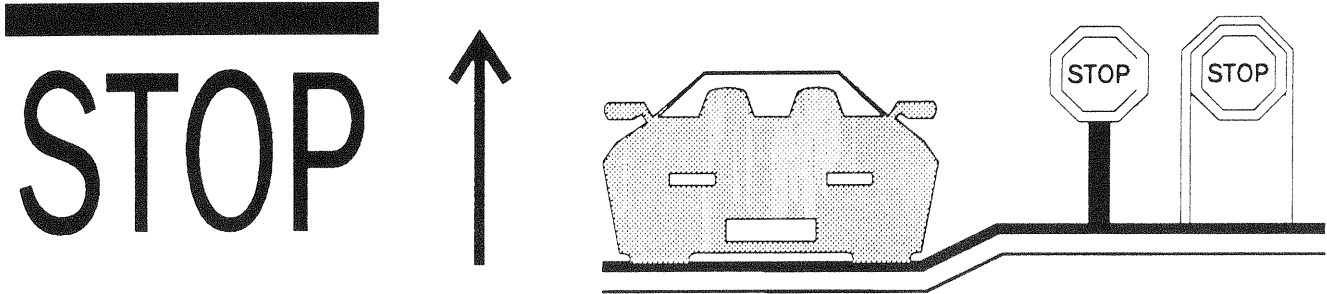
SHEA BUSINESS PROPERTIES

PLANNING NETWORK  
GILBERT AJA ASSOCIATE ARCHITECTS  
LIFESCAPES, INCORPORATED

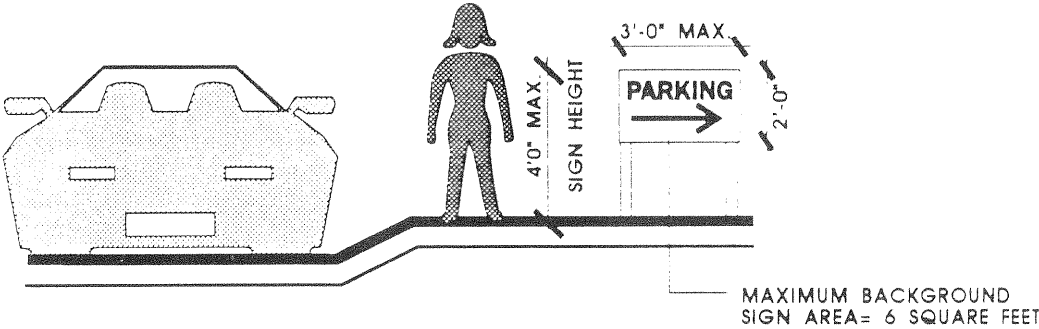


PAVEMENT GRAPHICS

STOP SIGNS



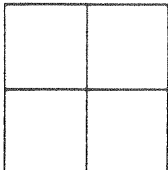
DIRECTIONAL INFORMATION



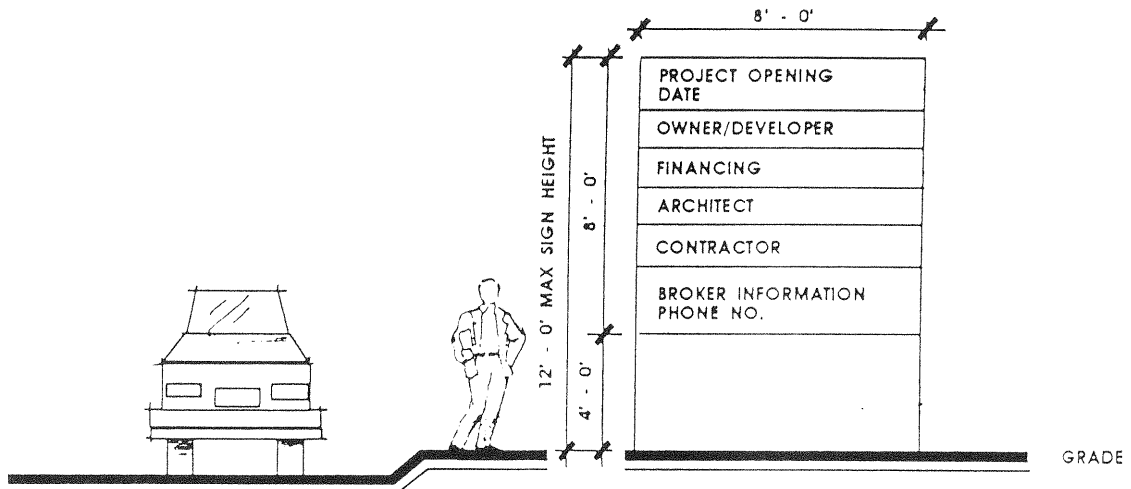
SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

PLANNING NETWORK  
GILBERT AJA ASSOCIATE ARCHITECTS  
LIFESCAPES, INCORPORATED



# TEMPORARY SIGNS



## SHEA BUSINESS CENTER SPECIFIC PLAN

SHEA BUSINESS PROPERTIES

PLANNING NETWORK  
GILBERT AJA ASSOCIATE ARCHITECTS  
LIFESCAPES, INCORPORATED

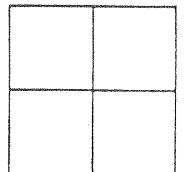


EXHIBIT 35

## 5.6 GRADING CONCEPTS

Because the Shea Business Center site is essentially flat, grading operations will be minimal. Conceptual grading of the project site has been designed so that the necessary cut and fill earthwork will balance on a project-wide basis.

Grading will occur throughout the site on a limited scale. The general intent of the grading program is to provide suitable building pad areas and to direct runoff away from buildings and into drainage facilities located within Shea Center Drive. No slopes shall be steeper than 2:1. All slopes will be planted with appropriate groundcover for erosion control.

The major area of landform modification is the proposed underpass under the Southern Pacific rail line along Wineville Avenue. The result will be slopes up to 30 feet in height. Although these slopes will generally not be visible from adjacent uses, these slopes will form the "window" to Shea Business Center along the approach from Airport Drive.

## 5.7 MAINTENANCE PLAN

Maintenance responsibilities within the Shea Business Center will be divided between the City of Ontario, special districts, a property owners' association, and individual property owners. Covenants, Conditions and Restrictions (CC&R's) have been recorded for the plan area in order to guarantee the maintenance of facilities and landscaping (see Appendix G B).

### A. STREETS

Each of the streets identified as public within this plan, including the Southern Pacific Railroad underpass, will be dedicated to, and maintained by, the City in accordance with established City policies. Private drives will be maintained by individual property owners or the property owners' association. The specific method of maintaining private access facilities will be determined at the time of site plan review.

### B. LANDSCAPE AND LIGHTING FACILITIES

Property Owners' Association will be responsible for maintaining parkway landscaping, including landscaping easements adjacent thereto. Individual project identity signs and monuments will be maintained by the adjacent property owner. Individual property owners will also be responsible for maintaining the onsite landscaping.

### C. DRAINAGE FACILITIES

Permanent drainage improvements within the Shea Business Center Specific Plan constructed within the public rights-of-way, will be dedicated to, and maintained by either the City of Ontario or the San Bernardino County Flood Control district, as appropriate.

Where it is necessary to construct drainage improvements outside of public rights-of-way, drainage easements will be dedicated to the City of Ontario or County Flood Control District, as appropriate. Upon dedication, the City or District will assume responsibility for maintenance; surface improvements within drainage easements will not be transferred. Open concrete channels shall be discouraged within streetscapes. The design of all drainage facilities will be as approved by the City Engineer.

Drainage facilities on private property in the absence of an easement will be considered to be private drains. Maintenance of such private drains will be the responsibility of the land

owner or the association charged with the general maintenance of the landscaping and other common improvements of the area in question.

**D. WATER AND SEWER**

Water and sewer facilities within the Shea Business Center Specific Plan constructed within public rights-of-way will be dedicated to, and maintained by the City of Ontario or Chino Basin Municipal Water District. Water facilities on private property in the absence of an easement will be considered to be private. Maintenance of such private lines will be the responsibility of the land owner or the association charged with the general maintenance of the landscaping and other common improvements of the area in question.

Where it is necessary to construct water or sewer improvements outside of public rights-of-way, easements will be dedicated to the City of Ontario and/or Chino Basin Municipal Water District, as appropriate. Upon dedication, the City or District will assume responsibility for maintenance of the underground facilities only; maintenance responsibilities for surface improvements within easement areas, other than those facilities for the easement was specifically granted, will not be transferred.

**E. OTHER FACILITIES**

The Southern California Gas Company will maintain natural gas lines within the project site. Southern California Edison will maintain on-site electrical facilities. General Telephone will maintain telephone facilities.

**Table 4  
SUMMARY OF MAINTENANCE RESPONSIBILITIES**

	Property Owners Association	Maintenance District	Individual Property Owner	City of Ontario, SBCFC, CBMWD	Edison Co.	Gas Company	General Telephone
Landscape Easements	•						
Landscape Parkway – Public Streets	•						
Landscape Parkway – Private Streets			•				
Landscape Medians	•						
Landscape Entrances & Identity Statements	•						
Individual Signage			•				
Street Lighting		•					
Public Streets (including Sidewalks)				•			
Public Storm Drains				•			
Water/Sewer Facilities within Public Row and/or Public Utility Easements				•			
Regional Sewer Lines				•			
Freeway Buffer Planting	•						
Electricity					•		
Gas						•	
Telephone							•