

2  
Master Plans

## LAND USE

The Creekside Land Use Plan encompasses 409.5 acres accommodating the development of various residential, commercial and open space uses. The greatest portion of the plan is devoted to residential uses of various housing types and densities in response to the market demand for housing that meets a variety of living styles and income levels.

The four major land use and acreages provided by the Plan are:

- Residential 326.1 acres
- Open Space 51.4 acres
- Commercial 4.7 acres
- Roadways 27.3 acres

A total of 2574 dwelling units are provided for by the Land Use Plan including Single Family Detached (SFD) units, Single Family Attached (SFA) units and Multi-Family (MF) unit housing types. The Land Use Plan is intended to represent the best balance between housing types while providing maximum opportunities for open space and the minimum of impacts in relation to surrounding land uses.

The maximum allowable number of dwelling units shall be as designated for each planning unit on the Land Use Plan (EXHIBIT 2) except as may be modified under the provisions of General Note No. 26 of this document. All areas designated for residential use may be developed at a lower number of dwelling units, without requiring a change in the PRD-Specific Plan zoning. The tabulation on the Land Use Exhibit reflects the average density of each product type. Actual densities in each planning unit may vary above or below the average, based upon the size and shape of the individual planning area.

Planning area boundaries may be modified with the approval of the City Planner. The total number of dwelling units allowed within each Planning Area can exceed that allowed by the Specific Plan up to 25 percent as long as the cumulative total does not exceed 2574 dwelling units from one Planning Area to another, while controlling the total number of units allowed for the project. Such a transfer is contingent upon the master developer providing additional data to verify, prior to tentative map approval, that the infrastructure capacity will be adequate.

On an overall basis the average net residential density for the Single Family Detached category will be 6.2 dwelling units per net residential acre.

Single Family Attached units will be clustered in average net densities of 6.0 to 15.1 units per net residential acre while maximizing open space and access to open space and activity centers.

Multiple-Family units will have a maximum average net density of 28.2 units to the net residential acre while also maximizing open space and access to open space and activity centers.

Two Day Care Centers, one located at Haven Avenue and Riverside Drive, and one located off of Lytle Creek Loop, offer centralized access to Creekside residents and residents from neighboring communities.

Cable T.V. is proposed to serve the Creekside development. A cable T.V. receiving and distribution facility may be located within Creekside in order to provide this service.

A lake of approximately 7.6 acres in size with accompanying open space will be located within the West Village Loop road.

Nine Activity Centers will provide recreational facilities and additional open space.

A Recreational Vehicle parking-storage lot will be located on 2.7 acres at the northwest corner of the East Village. This area can also serve as a maintenance yard for the Homeowners Association. The proposed Recreational Vehicle Storage area will initially be utilized as a temporary Ontario City Fire Department Station to provide an acceptable emergency response time to the Creekside project and adjacent residential areas until a permanent facility can be constructed in the area. During the period of time this site is used by the City for a fire department facility, the site may also be used jointly as a recreational vehicle storage lot. The Edison right-of-way may also serve as an additional open space or as a site for other types of recreational activities operated by the City of Ontario.

A 13.7 acre school site is located within the East Village loop road.

A street sweeper transfer station, built to City Standards, shall be constructed at one or either of the following locations:

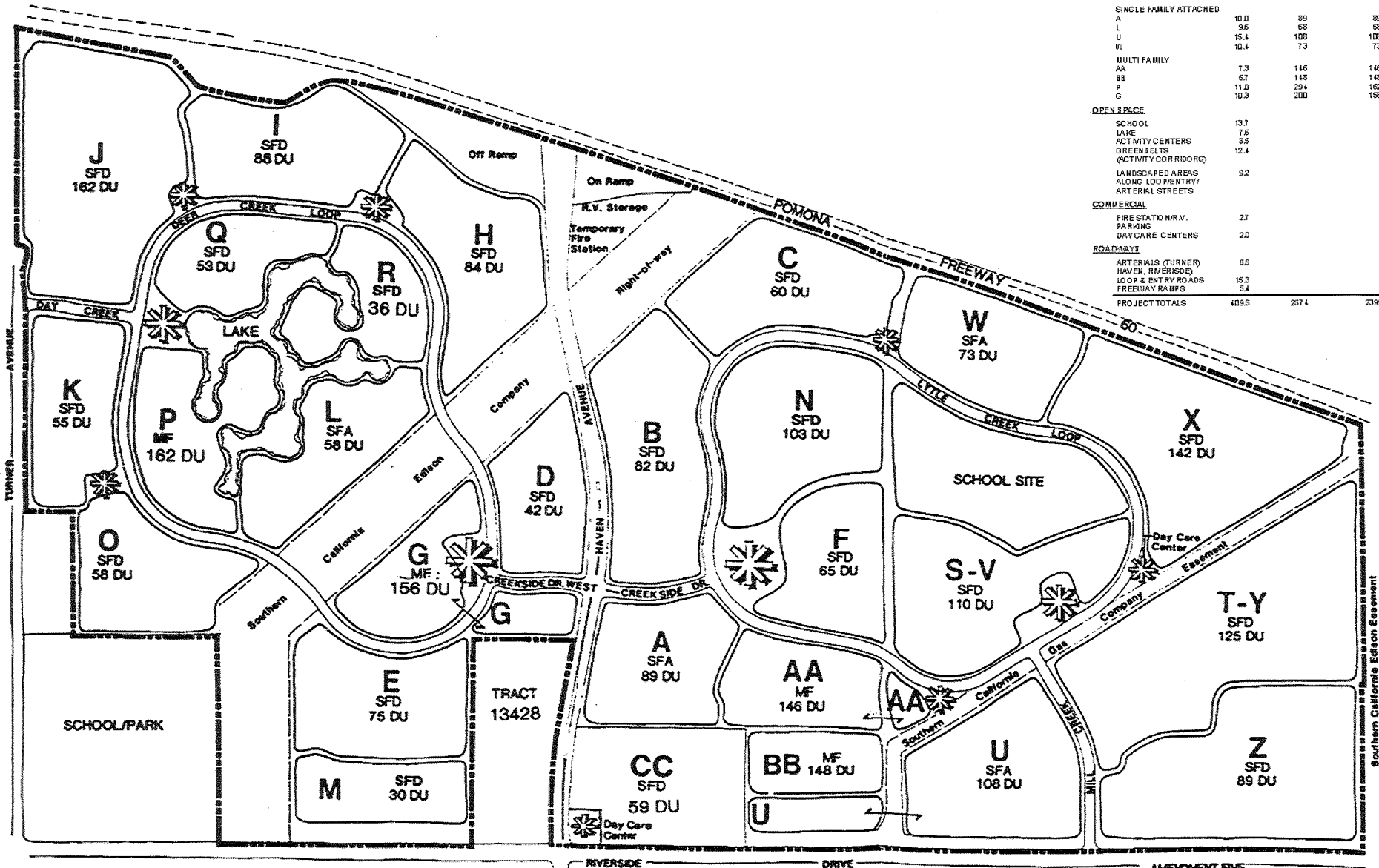
- a. Adjacent to the R.V. Storage Area at the southeast corner of Pomona Freeway and Haven Avenue.
- b. Southeast corner of the Pomona Freeway and Turner Avenue.

# LAND USE

## EXHIBIT 2

### STATISTICAL SUMMARY

LAND USE	NET ACRES	TOTAL NO. OF D.U.'S APPROVED	TOTAL D.U.'S CONSTRUCTED TO DATE	TOTAL D.U.'S PER AMENDMENT #5
<b>RESIDENTIAL</b>				
SINGLE FAMILY DETACHED (SFD)				
F	9.0	65	65	65
H	15.8	84	84	84
I	12.5	86	86	86
J	21.2	162	162	162
N	13.7	103	103	103
S-V	14.0	110	110	110
X	23.2	142	142	142
CC	8.7	59	59	59
SINGLE FAMILY DETACHED (SFD)				
B	13.8	82	82	82
C	12.1	60	60	60
D	7.1	42	42	42
E	12.8	75	75	75
K	8.7	55	55	55
M	6.1	30	30	30
O	9.3	58	58	58
Q	7.5	53	53	53
R	8.0	36	36	36
T-Y	24.4	125	125	125
Z	18.9	89	89	89
SINGLE FAMILY ATTACHED				
A	10.0	89	89	89
L	9.6	86	86	86
U	15.4	108	108	108
W	10.4	73	73	73
MULTI FAMILY				
AA	7.3	146	146	146
BB	6.7	148	148	148
P	11.0	234	153	153
G	10.3	200	196	196
OPEN SPACE				
SCHOOL	13.7			
LAKE	7.6			
ACTIVITY CENTERS	8.5			
GREENBELTS (ACTIVITY CORRIDORS)	12.4			
LANDSCAPED AREAS ALONG LOOP/PENTRIES/ARTERIAL STREETS	9.2			
COMMERCIAL				
FIRE STATION N.R.V. PARKING	2.7			
DAYCARE CENTERS	2.0			
ROADWAYS				
ARTERIALS (TURNER HAVEN, RIVERSIDE)	6.6			
LOOP & ENTRY ROADS	16.3			
FREEWAY RAMPS	5.4			
<b>PROJECT TOTALS</b>	<b>409.5</b>	<b>2674</b>	<b>2399</b>	<b>2548</b>



## LAND USE PHASING

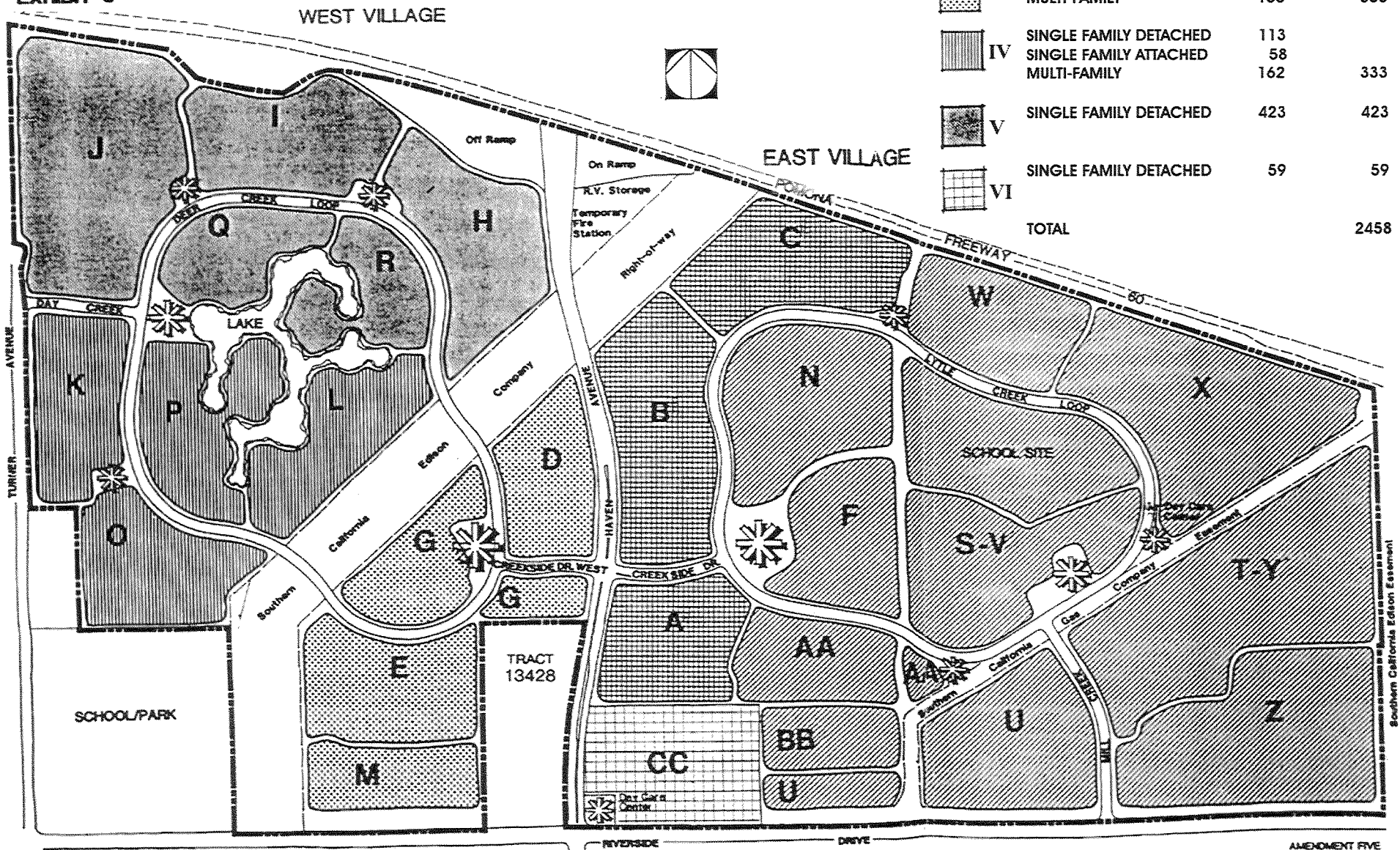
Originally, the project was proposed to be constructed in phases over an approximate eight year period beginning in 1980. To date, the project is built-out with the exception of Planning Area CC, which represents the proposed change from Neighborhood Commercial to single-family residential land uses. Actual build-out of the Creekside PRD indicates a decrease in units from the approved maximum total of 2,574. Including the residential units proposed in Amendment No. Five, total dwelling units constructed within the Creekside PRD will be 2,548.

Landscaping and development of the greenbelt activity corridor (drainage swales) is planned to occur in tandem with the development of adjacent planning units.

# LAND USE PHASING

EXHIBIT 3

PHASE	PRODUCT	MAX. NO OF D.U.'S	PHASE TOTAL
I	SINGLE FAMILY DETACHED	142	231
	SINGLE FAMILY ATTACHED	89	
II	SINGLE FAMILY DETACHED	634	1109
	SINGLE FAMILY ATTACHED	181	
	MULTI-FAMILY I	294	
III	SINGLE FAMILY DETACHED	147	303
	MULTI-FAMILY	156	
IV	SINGLE FAMILY DETACHED	113	333
	SINGLE FAMILY ATTACHED	58	
	MULTI-FAMILY	162	
V	SINGLE FAMILY DETACHED	423	423
VI	SINGLE FAMILY DETACHED	59	59
TOTAL		2458	2458



SCHOOL/PARK

TRACT 13428

RIVERSIDE

DRIVE

## OPEN SPACE-RECREATION

The 51.4 acre open space system will feature a lake of approximately 7.6 acres and a network of activity centers interconnected by a series of landscaped buffers and greenbelt-drainage swales. The lake, activity centers, greenbelts and landscape areas will form the essential visual unifying element of the Creekside development and will serve as the primary recreation amenities for Creekside residents. The lake, activity centers, greenbelt-drainage swales, landscaped areas and Edison right-of-way and other open space amenities are described below: (EXHIBIT 4)

### Master Plan Open Space/Recreation

Lake - An approximate 7.6 acre lake will be provided within the loop road of West Village. A separate section, the Lake Amenity Plan, describes the lake and surrounding area in further detail.

Activity Centers - A hierarchy of activity centers will be provided to serve a diversity of residential recreational needs. As shown on the Open Space-Recreation Plan, three types of activity centers are proposed. Conceptual site plans are shown on EXHIBITS 5, 6, and 7.

Major centers will encompass approximately 1.2 to 2.5 acres and will be located opposite each of the major entryways on the inside of each loop. This location will enable the major centers to serve as theme setting elements while providing convenient access to a cross-section of higher density residential unit types. Major centers (shown as Major A on the exhibit) will include a junior olympic pool, tennis courts, restrooms, play areas, and off-street parking. The major center within the East Village has been constructed.

Secondary activity centers (shown as Major B Center on exhibit) will encompass approximately 2 acres and will include a pool, spa, picnic-barbecue facilities, restrooms, play areas and off-street parking. These will be located with convenient access to the higher density units. Each will be placed opposite each secondary entryway to provide exposure and a them setting element. Easy access via foot or bike will be provided while additionally providing a visibility which will act as a unifying element in the community.

The Major Activity Center B located northwesterly of the lake will be developed as a Beach Club. A sand beach will be located adjacent to the beach

club facilities. A dock and boat rental services may also be provided at this location.

Five minor recreation centers of approximately 1/2 acre each will also be provided and will be located on the outside of the loop road. These may include sports courts, sand volleyball, or other amenities.

One of the minor recreation centers in the West Village may be developed for the exclusive use of adults.

Greenbelts/(Activity Corridors) - The greenbelts will serve a dual function as drainage courses and as a pedestrian-bicycle pathway interconnecting the activity centers. The greenbelts will be located throughout the Creekside community in conjunction with the loop and entry roadways. The greenbelts will provide drainage for the site while incorporating berms, landscaping, pedestrian security lighting, and emergency telephone equipment.

In addition to connecting the interior portion of the community, the greenbelts will provide ready access by foot and bike to exterior portions of the project including the school and park site, a portion of the Edison right-of-way and offsite roadways including Turner Avenue, Haven Avenue, and Riverside Drive.

The greenbelt system will offer nonautomotive travel throughout the Creekside development with the minimal amount of vehicular roadway crossings. Approximately 12 acres will be devoted to greenbelts with eight-foot wide pathways of concrete paving. The greenbelts will range in width from 30 to 70 feet with the widest greenbelt occurring along Riverside Drive.

Landscaped Areas - Landscaped areas are provided along all arterials including the loop roadways and entryways. In addition to setting apart various land uses, the areas supply a unifying theme by establishing an easily identifiable plant material and visually links the Creekside resident to the community. A total of approximately 9.2 acres will be devoted to landscaped areas ranging in width from 5 to 60 feet. Trees used as landscaping along the buffers will identify a hierarchy of streets. In many of the areas, berms will be implemented which will aid in increasing privacy, separating land uses and mitigating noise.

A pedestrian-bike path accompanies the major portion of landscape areas.

#### Other Open Space/Recreation

Lake Trail System - A lake trail system will be provided along portions of the lake adjacent to attached housing as shown on the Open Space-Recreation exhibit. This lake trail system will contain a minimum 18 foot wide band of landscaping with a 8 foot wide pedestrian path, and will accommodate emergency vehicle access.

Edison Right-of-Way - The Edison right-of-way is 330 foot wide strip of land of approximately 30 acres traversing the Creekside development in a northeast-southwest direction. This area is currently being used for several land uses. The area adjacent to the Pomona Freeway is used as a Christmas tree farm. The area between Riverside Drive and Lower Deer Creek Loop is being used by the City of Ontario as a 9-hole golf course. The remaining portion of the SCE Easement between upper and lower Deer Creek Loop is vacant but could be used for agricultural purposes.

The paved pedestrian bicycle path 8 feet in width for pedestrians, joggers and bicyclists, etc.

School - Two schools, one located within the Creekside East Village (approximately 13.7 acres in size) and one located immediately adjacent to the West Village (approximately 15 acres in size), will add open space amenity. In addition to landscape as basketball courts and playground equipment will be provided by these facilities.

Public Park - A 6 acre public park located immediately adjacent to the West Village is planned for development by the City of Ontario. This park will add additional open space amenities for Creekside residents. To the extent of the offsetting park fee credit approved by the City; up to 50 percent, the Master Developer shall provide design services for the existing City parksite located adjacent to the Edison easement and at the northeast of Riverside Drive and Turner Avenue.

Improvements shall include, but not be limited to, landscaping, irrigation, pedestrian/bicycle facilities and trails, restroom/recreation storage building, tennis courts, basketball/multi-purpose courts, children's play area, picnic area and open turf recreation areas. The City of Ontario shall maintain these facilities as part of the public park system per City requirements and an approved Community Park Master Plan.




Open Space within Residential Developments - Additional open space will be provided within Single Family Attached and Multiple Family developments as is normally provided within any attached product. (SFA: minimum 400 square feet per unit and MF: minimum 300 square feet per unit.) This space will be owned in common by project homeowners.




# OPEN SPACE/ RECREATION

EXHIBIT 4


## ACTIVITY CENTERS

-  - MAJOR A  
JR. OLYMPIC POOL, SPA, TENNIS COURT  
CABAÑA, PICNIC / BARBEQUE, PLAY AREAS
-  - MAJOR B  
POOL, SPA, CABAÑA, PICNIC/BARBEQUE,  
PLAY AREAS
-  - MINOR  
PLAY AREAS, PICNIC / BARBEQUE


## GREENBELTS

-  - PEDESTRIAN / BIKE PATHWAYS WITHIN  
ACTIVITY CORRIDORS


## LANDSCAPED AREAS

-  - PEDESTRIAN / BIKE PATHWAYS
- LANDSCAPING

## LAKE TRAILS

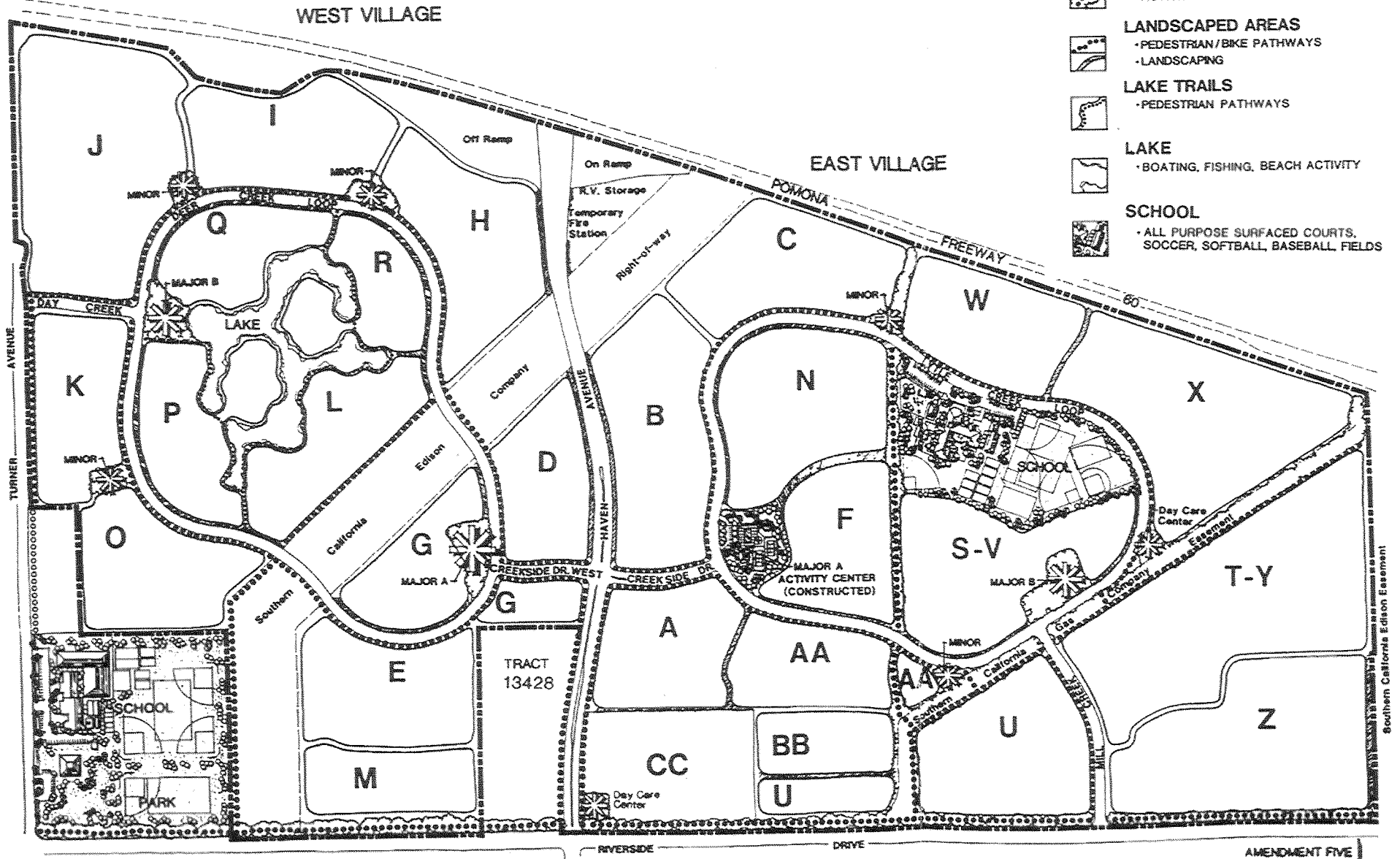
-  - PEDESTRIAN PATHWAYS

## LAKE

-  - BOATING, FISHING, BEACH ACTIVITY

## SCHOOL

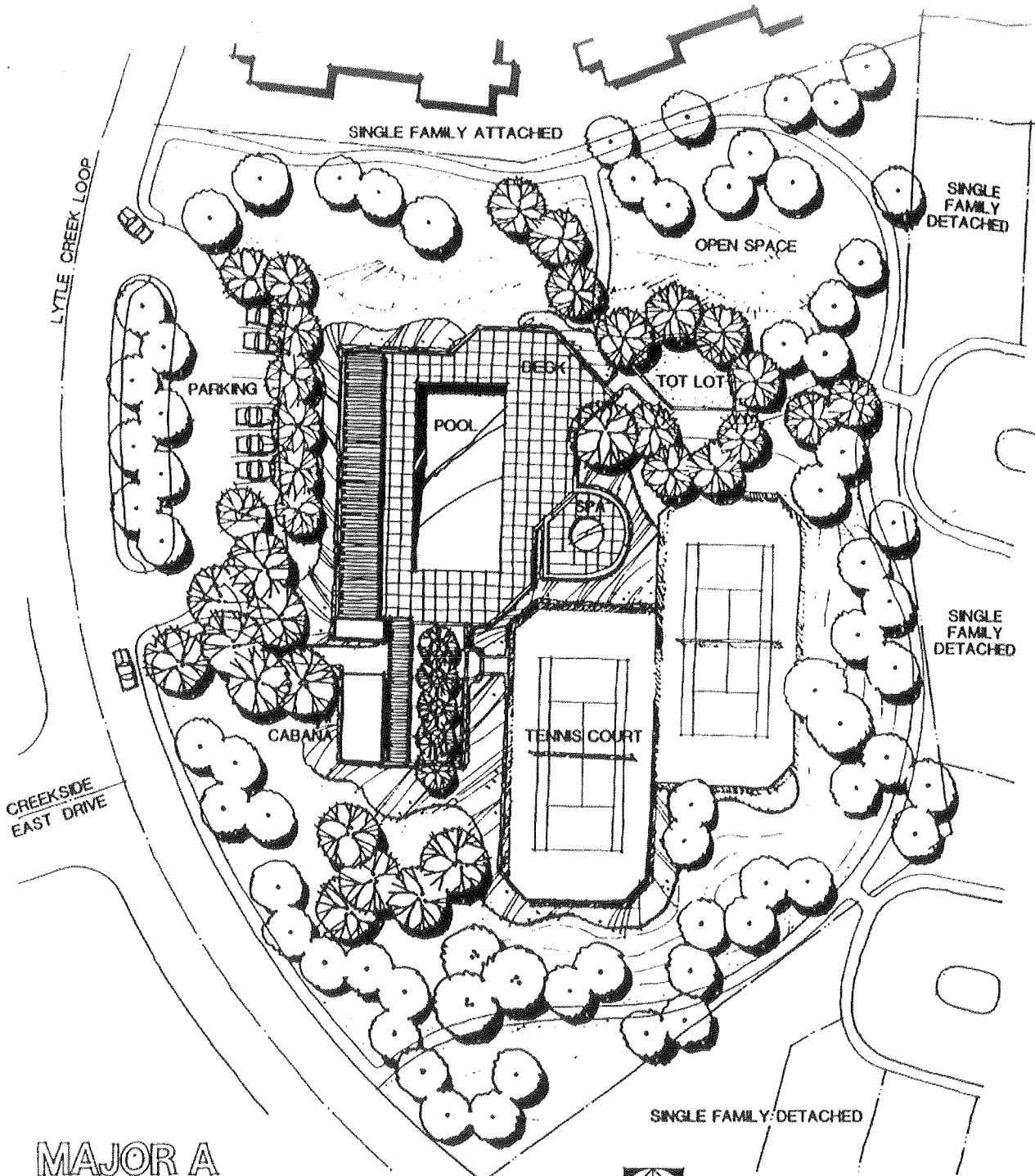
-  - ALL PURPOSE SURFACED COURTS,  
SOCCER, SOFTBALL, BASEBALL FIELDS



AMENDMENT FIVE  
REVISED MAY 1994

# RECREATION CENTERS

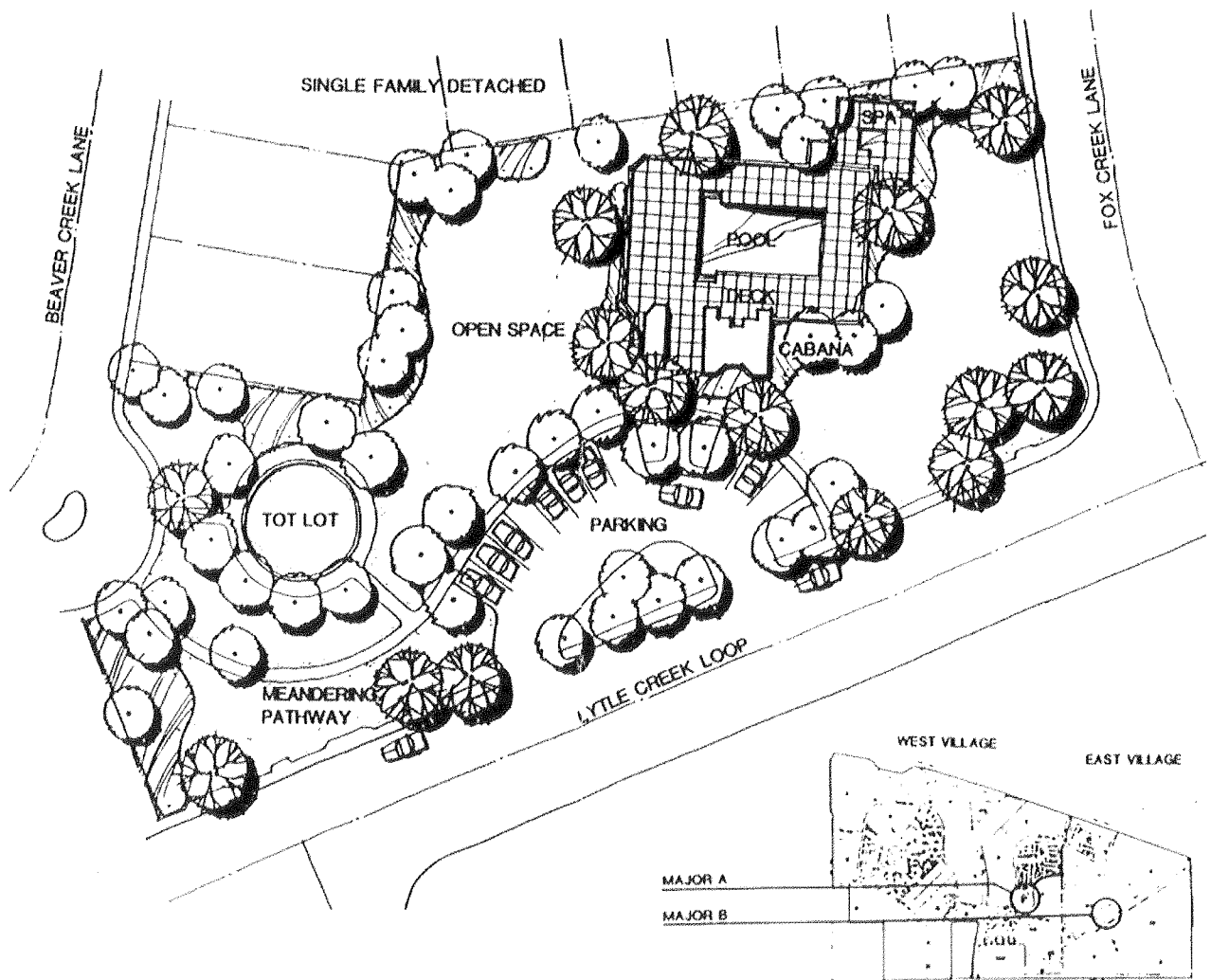
# RECREATION CENTERS



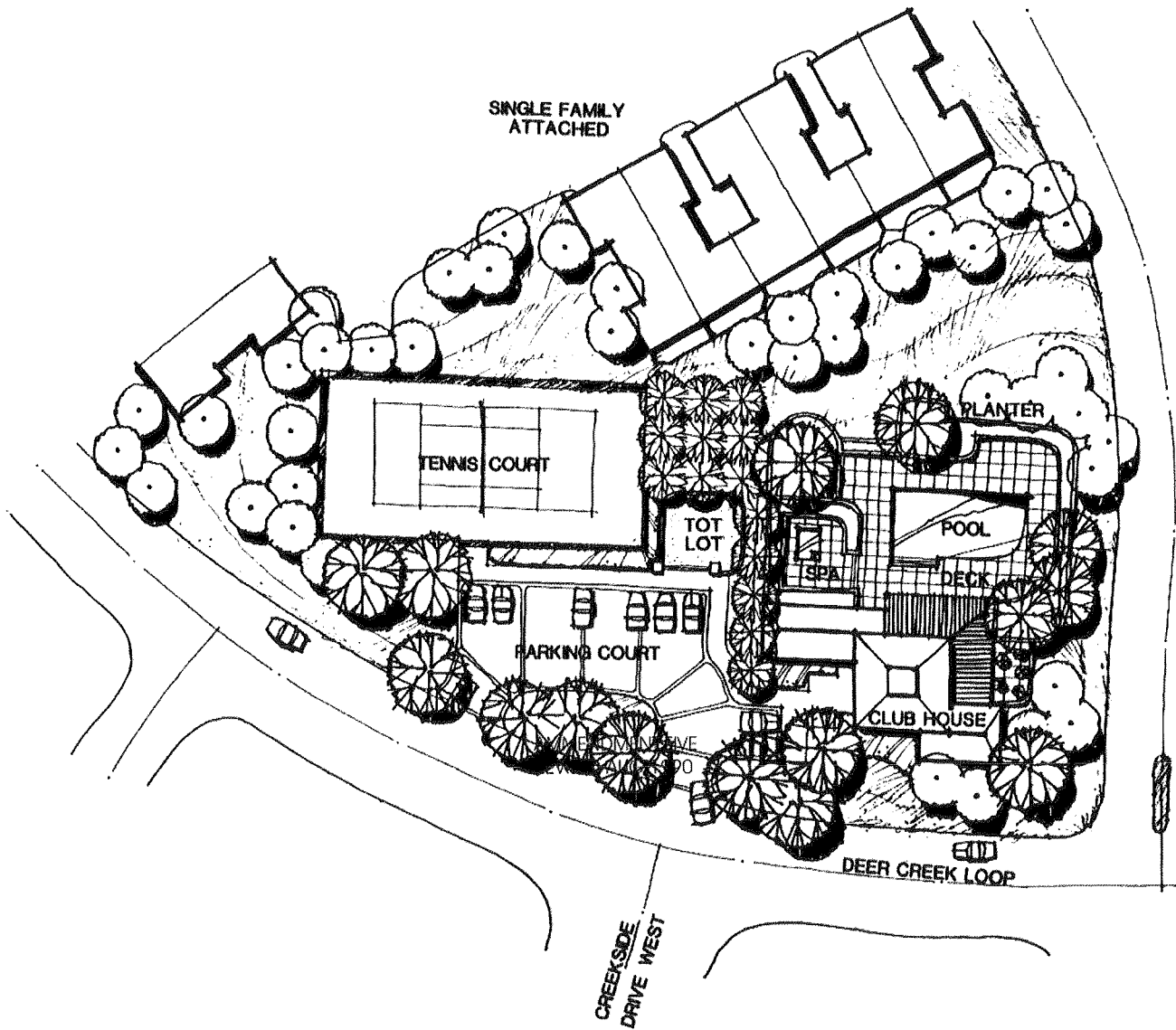
MAJOR A  
RECREATION CENTER



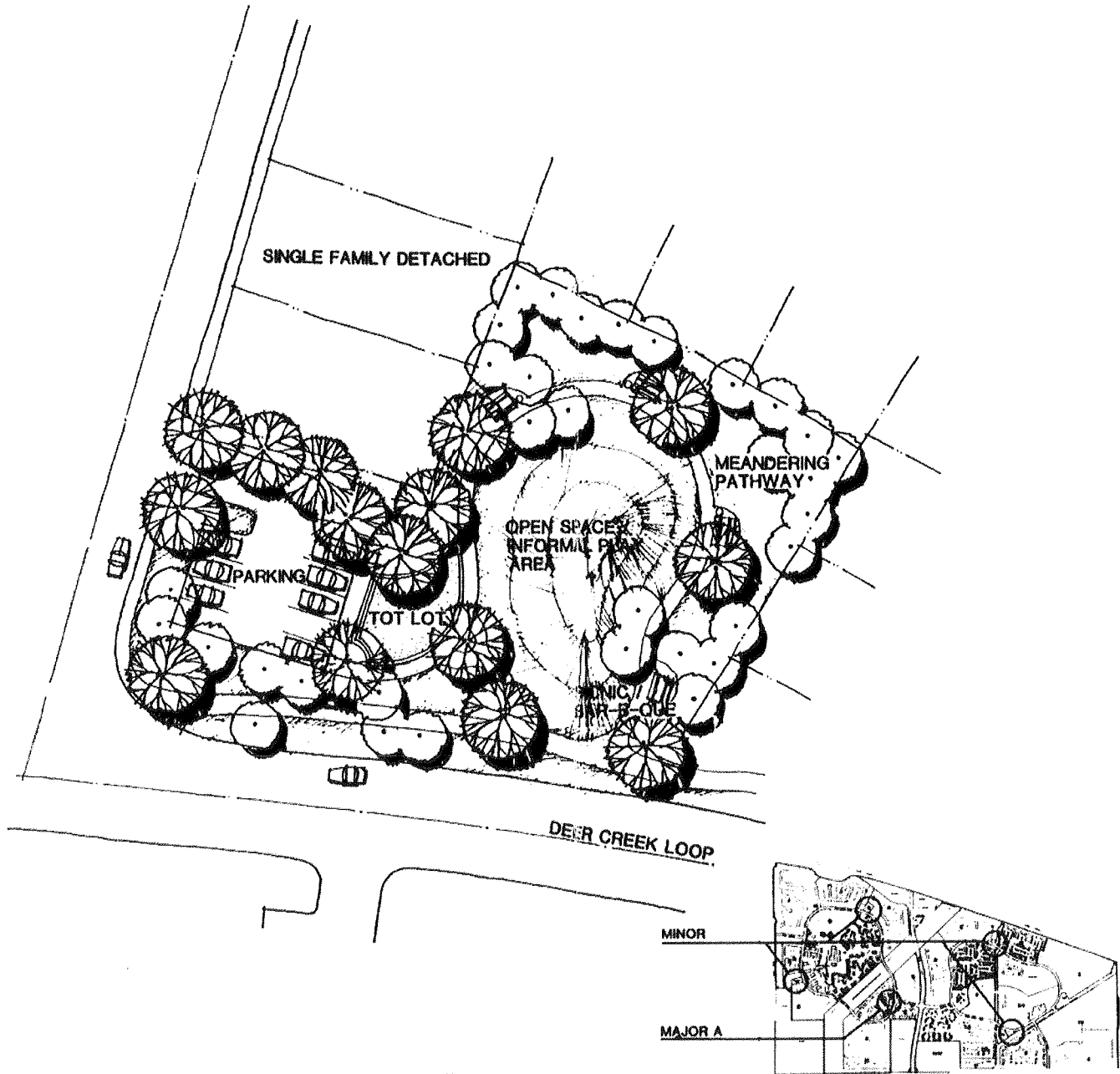
AMENDMENT FIVE  
REVISED AUG 1990



**MAJOR B  
RECREATION CENTER** 



# MAJOR A RECREATION CENTER



# MINOR RECREATION CENTER

TYPICAL-4 LOCATIONS (OTHER ACTIVITIES MAY INCLUDE SPORTS COURTS OR SAND VOLLEYBALL)

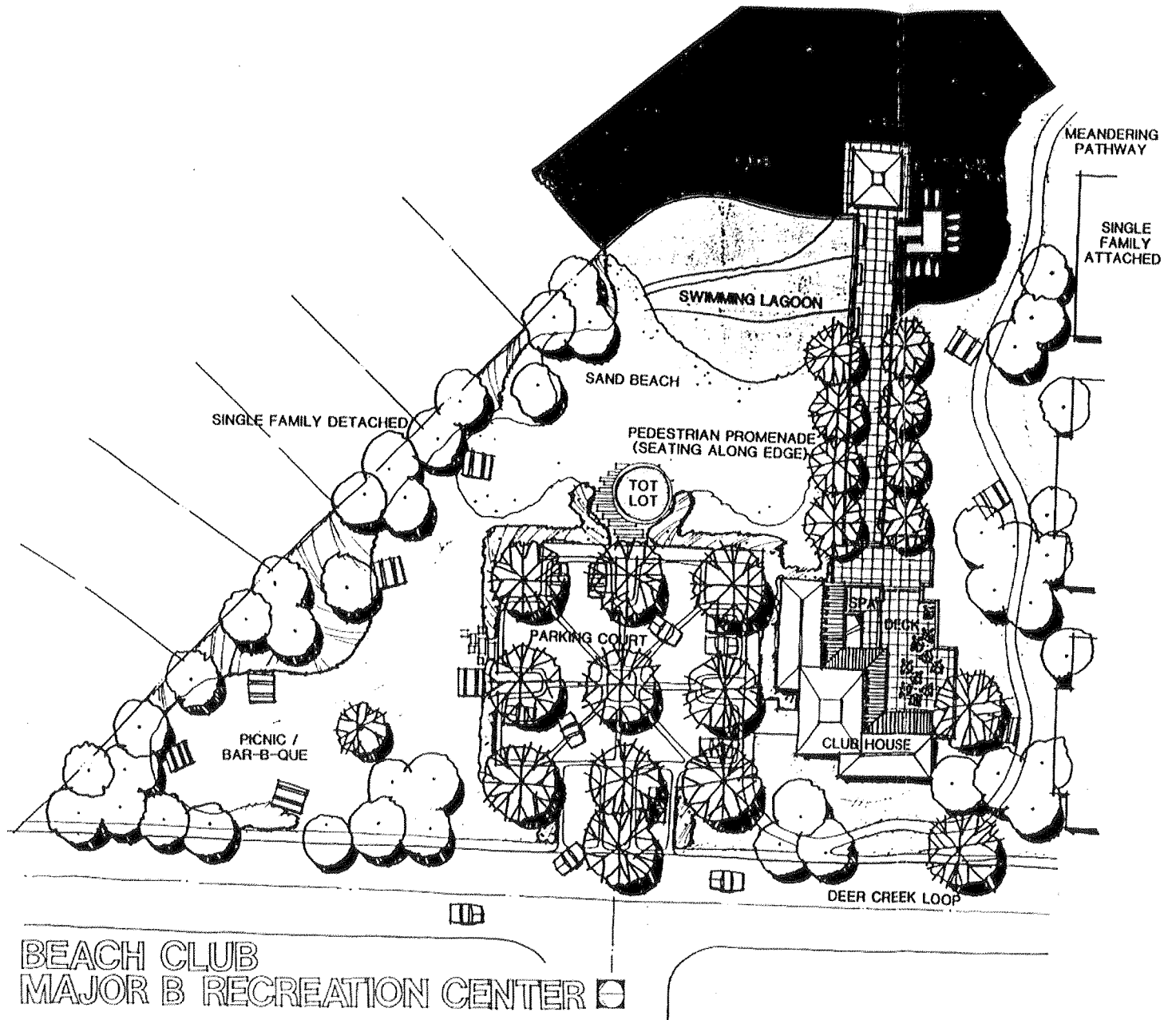
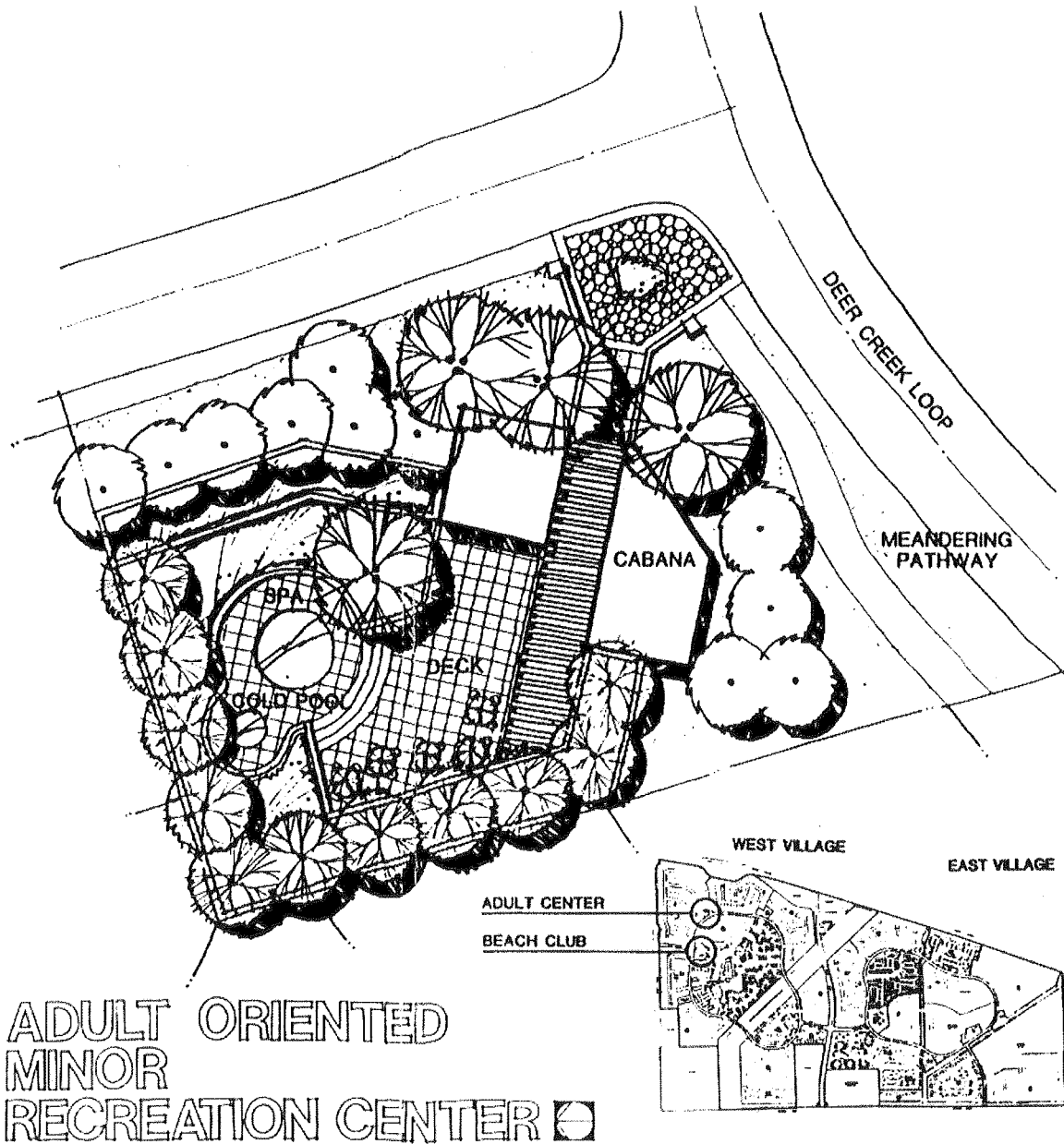


EXHIBIT 7





## VEHICULAR CIRCULATION

The Vehicular Circulation Plan presents the overall concept of traffic movement proposed within the Creekside PRD-Specific Plan. All major components are identified, including major arterials (designated A on the accompanying Exhibit); public residential streets (designated B); private residential streets (designate C); and emergency access points (designated D). The dimensions presented herein are the standards for the Creekside PRD-Specific Plan. Minor realignments in B, C, and D streets to accommodate ultimate subdivision designs can be approved by the Planning Commission without an amendment to the Specific Plan. Inasmuch as the standards for major circulation components have been modified by Amendment No.3 to the Specific Plan, East Village components will be permitted to develop, for consistency, with the standards previously approved by the City Engineer. The accompanying exhibits presented herein, therefore, indicate standards to be implemented in each respective village.

On a conceptual level, the system has been designed in such a way as to optimize the progression from and interaction between higher intensity existing access routes - such as the Pomona Freeway and Riverside Drive - and the circulation and access routes proposed in the PRD. As such, the primary entry to Creekside will be onto Haven Avenue from Riverside Drive. A slight realignment of Haven Avenue, to introduce a gentle curvature of the route as it passes through Creekside, is proposed to enhance and soften the entry statement. Additionally, two secondary entry points are planned, one off Riverside Drive and the second off Turner Avenue. These, too, will be flanked on each side by a landscaped buffer to emphasize the rural theme.

The basic structure of the Plan is oriented around the loop systems. Acting as the spine to the overall Vehicular Circulation Plan, these loops will unify the East and West village components of Creekside PRD. Designed to balance traffic distribution and provide a smooth transition between the public and private streets, the loop system will also emphasize the rural country theme established for the project. Design speed for the loop roads (B-3) will be 35 mph, achieved through a combination of curves and road elevations approved by the City Engineer. Within the project, travelways of public and private streets will maintain a minimum 20 feet

unobstructed clear width for emergency access, unless specifically, designated for emergency access only, in which case a 15 foot minimum clear width applies.

See EXHIBIT 8

# VEHICULAR CIRCULATION

EXHIBIT 8

**MAJOR ARTERIALS**  
 A1.1 A1.2 A1.3 TURNER AVENUE  
 A2.1 A2.2 A2.3 HAVEN AVENUE  
 A3.1 A3.2 RIVERSIDE DRIVE

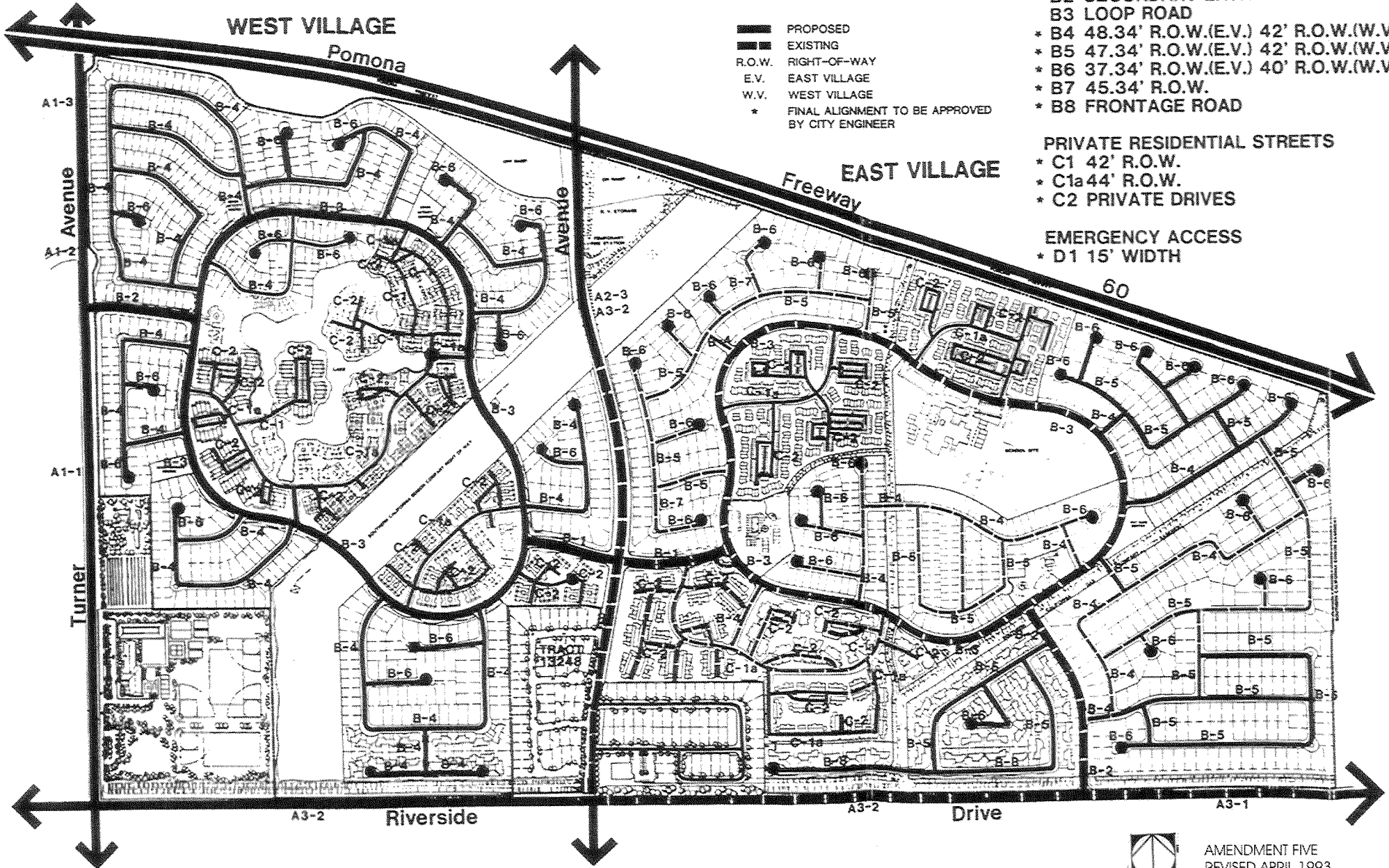
**PUBLIC RESIDENTIAL STREETS**  
 B1 PRIMARY ENTRY  
 B2 SECONDARY ENTRY  
 B3 LOOP ROAD

\* B4 48.34' R.O.W.(E.V.) 42' R.O.W.(W.V.)  
 \* B5 47.34' R.O.W.(E.V.) 42' R.O.W.(W.V.)  
 \* B6 37.34' R.O.W.(E.V.) 40' R.O.W.(W.V.)  
 \* B7 45.34' R.O.W.  
 \* B8 FRONTAGE ROAD

**PRIVATE RESIDENTIAL STREETS**  
 \* C1 42' R.O.W.  
 \* C1a 44' R.O.W.  
 \* C2 PRIVATE DRIVES

**EMERGENCY ACCESS**  
 \* D1 15' WIDTH

— PROPOSED  
 — EXISTING  
 R.O.W. RIGHT-OF-WAY  
 E.V. EAST VILLAGE  
 W.V. WEST VILLAGE  
 \* FINAL ALIGNMENT TO BE APPROVED BY CITY ENGINEER



AMENDMENT FIVE  
 REVISED APRIL 1993

## STREET SECTIONS - MAJOR ARTERIALS

Three major arterials are depicted on the overall Circulation Plan (designated as A), including Turner Avenue, Haven Avenue and Riverside Drive. These routes serve not only to provide major access to the Creekside PRD-Specific Plan, but also to define the periphery and internal "spine" of the project.

### Turner Avenue (A-1)

Turner Avenue, located along the western boundary of Creekside PRD, is proposed to have a varying right-of-way (ROW) of which the paved, curb-to-curb dimension will be 64'. Bordering the single-family detached units only, Turner Avenue from Day Creek south to project boundary will also provide an 8' meandering trail, suitable for bicycle and pedestrian use. A landscaped area and raised berm adjacent to proposed rear yard walls along the PRD site will provide external continuity to the Project's rural character. Northerly of the existing tract entry the ROW will be required to serve only maintenance and utility vehicles. A street sweeper transfer station operated by the City of Ontario is also proposed to be located in this maintenance access area.

See EXHIBIT 9

### Haven Avenue (A-2)

As the major entry and access route, treatment of the Haven Avenue alignment will emphasize the overall country theme of Creekside. Adjacent to the ROW will be raised berms, buffers, landscaping and meandering 8' pedestrian trails to each side. A 14' raised median strip will lend additional landscaping to this central arterial route. Northerly of the project entry Haven Avenue's ROW width will be reduced with the 8' meandering pedestrian trails transitioning into 5' P.C.C. sidewalks (measured from the back of the curb) directly adjacent to the curb. The PRD will maintain accompanying landscape amenities.

See EXHIBIT 10

### Riverside Drive (A-3)

Treatment of the northerly 50' of Riverside Drive alignment is designed to give the project a rich open space character, with ample landscaping and an 8' pedestrian trail system, meandering alongside a landscaped drainage swale.

Currently the underlying property owner is responsible for the maintenance of the landscape swale along Riverside Drive within Planning Area CC. It is the intent of the project applicant to annex TT14805 into the East Creekside Homeowner's Association. Within Tentative Tract 14805 the proposed dwelling units will provide a side elevation

adjacent to Riverside Drive. The minimum distance between these proposed units and the right-of-way of Riverside Drive will be 100 feet. The existing landscaped swale will buffer the proposed units and Riverside Drive. The proposed design concept will lend variety to the external visual appearance of Creekside, while again emphasizing the overall rural theme.

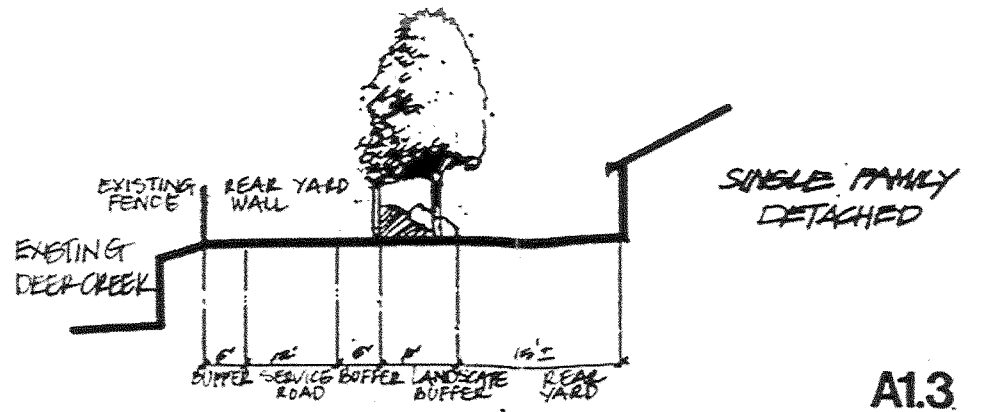
Signalization of the Riverside-Haven intersection and the Creekside Drive (East and West) Haven intersection has occurred.

See EXHIBIT 11

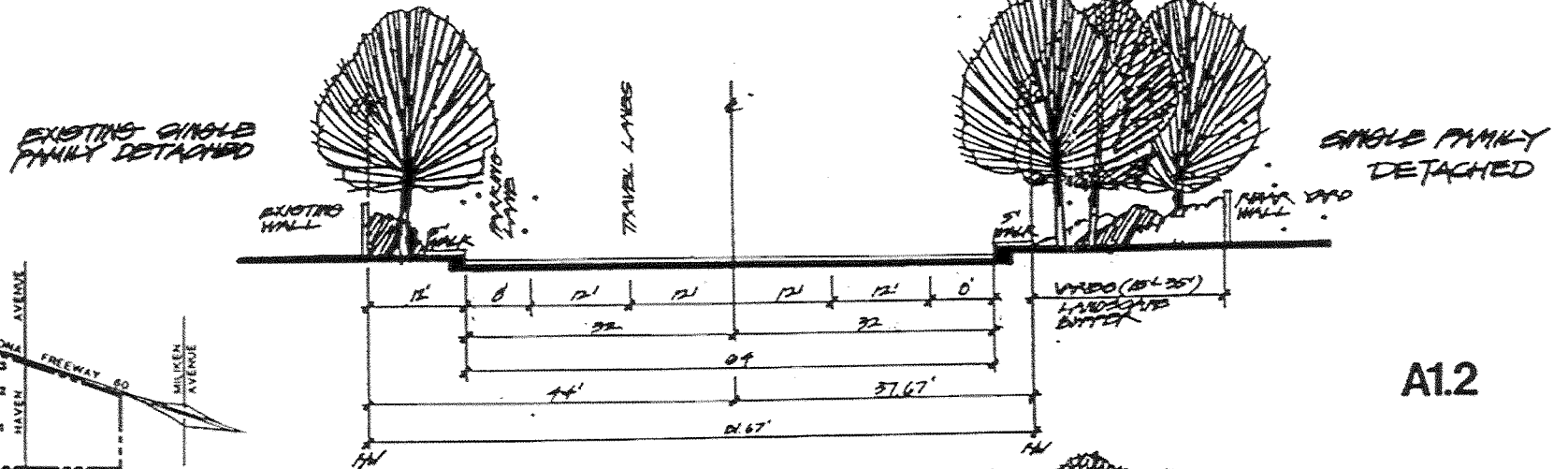
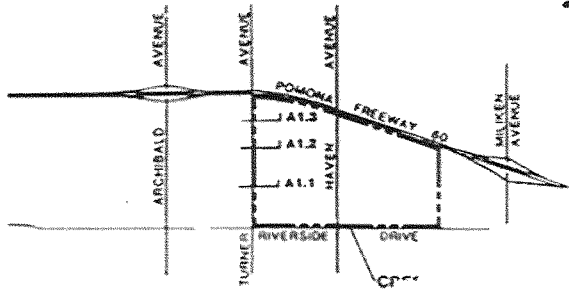
# STREET SECTIONS MAJOR ARTERIAL

## TURNER AVENUE

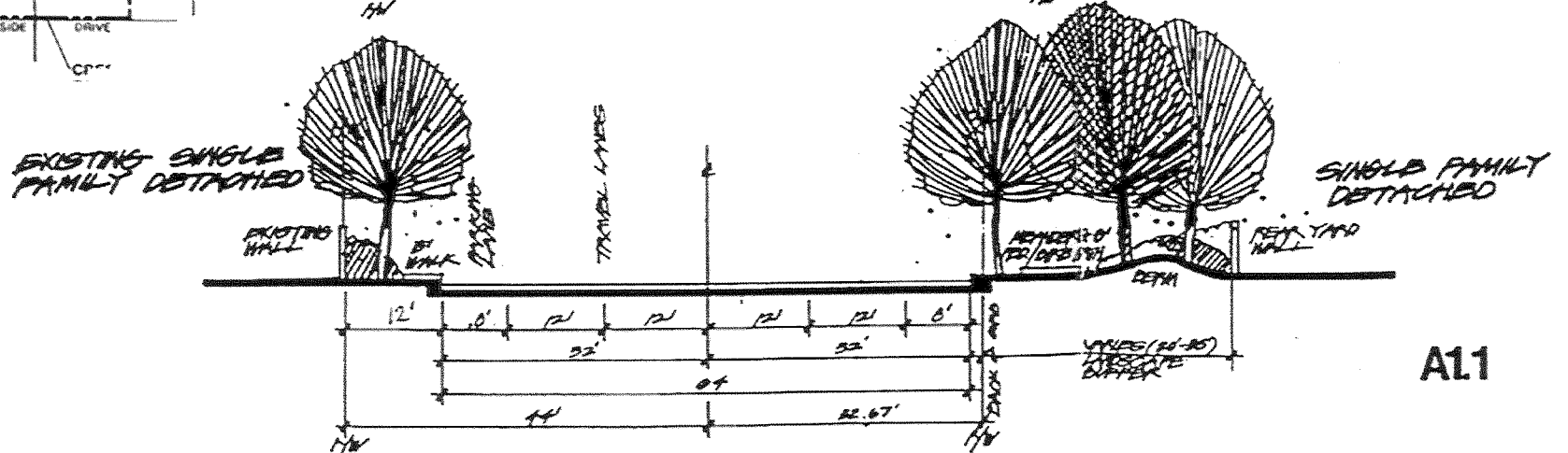
EXHIBIT 9



A1.3



A1.2



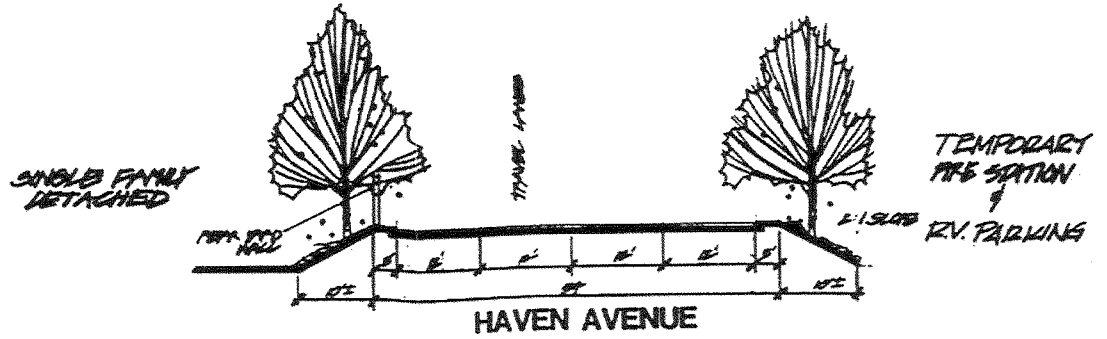
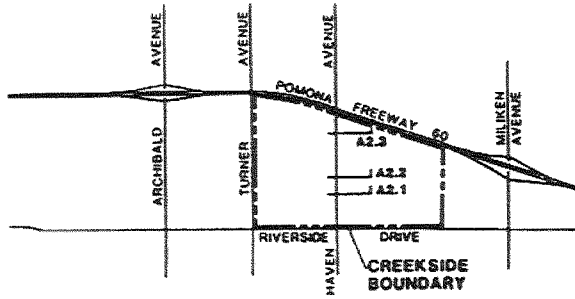
A1.1

AMENDMENT FIVE  
REVISED MAY 1998

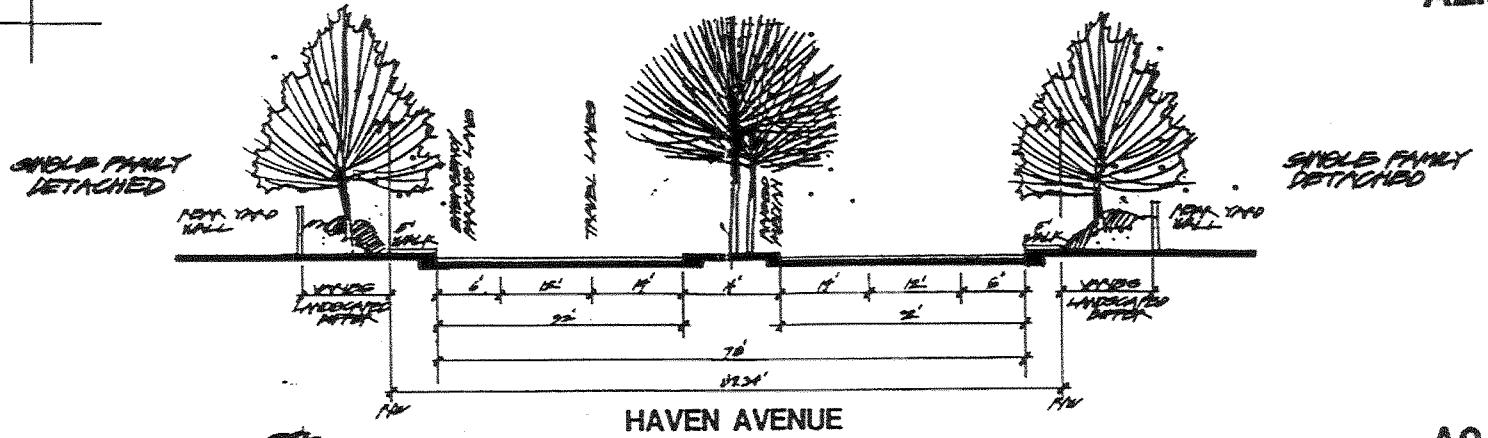
NOTE: STREET WIDTH MEASURED TO FLOWLINE

# HAVEN AVENUE

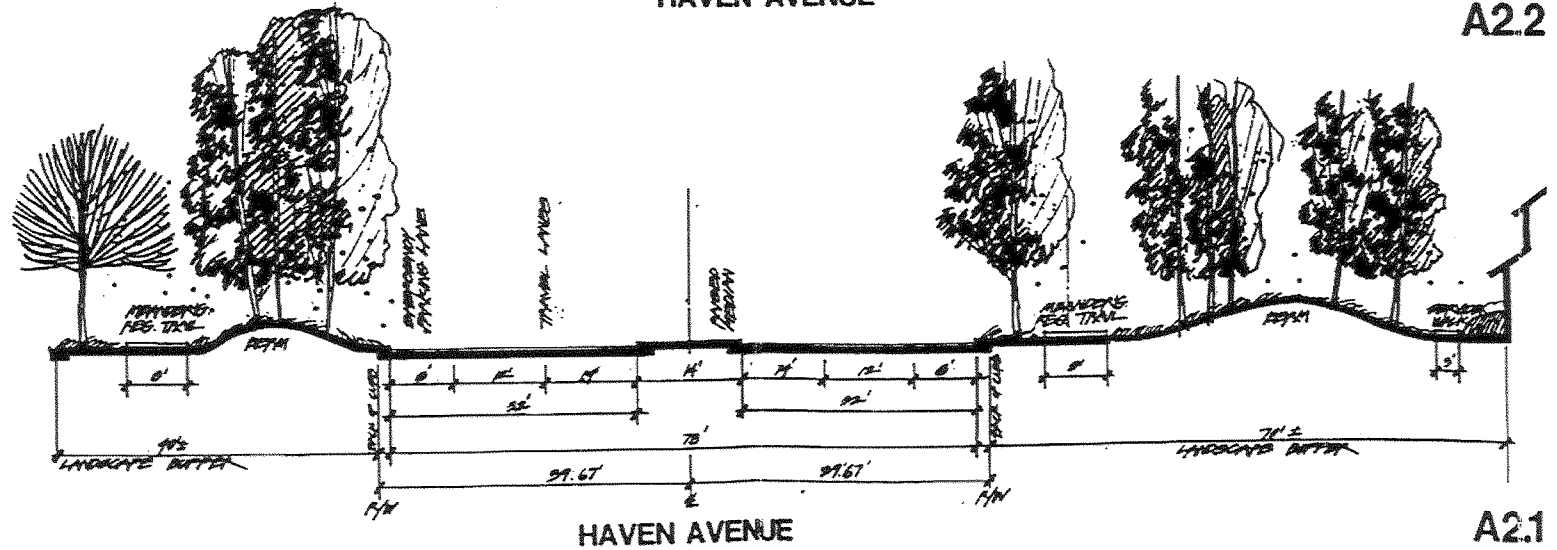
EXHIBIT 10



A2.3



A2.2



A2.1

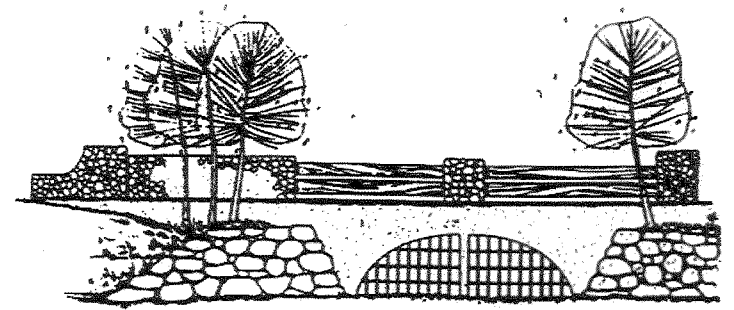
CONCEPT ONLY

NOTE: STREET WIDTHS MEASURED TO FLOWLINE

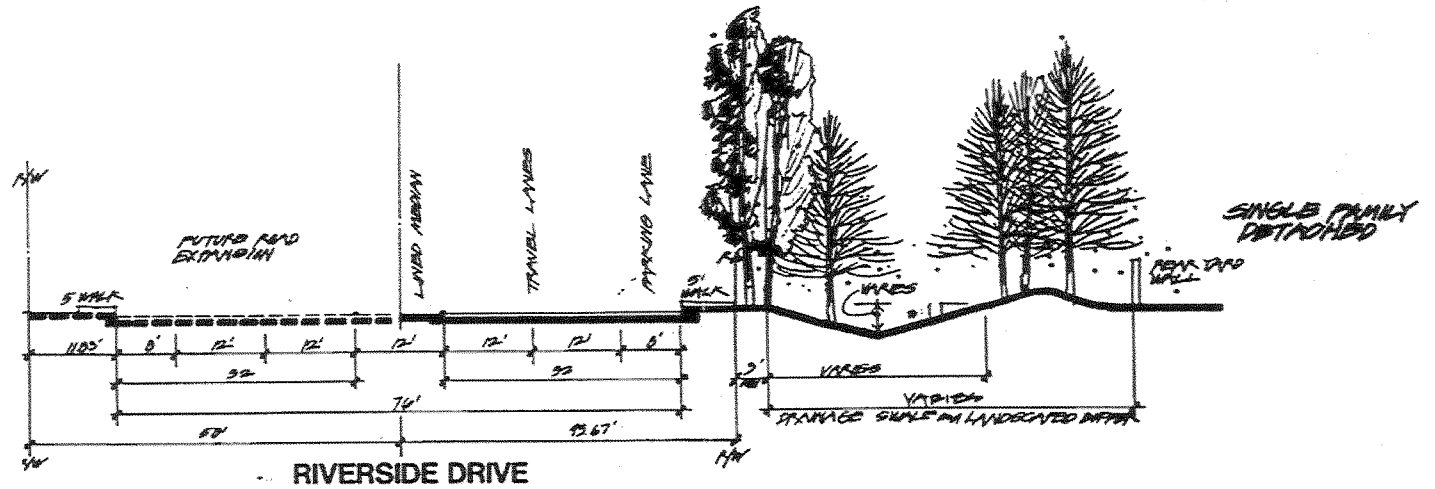
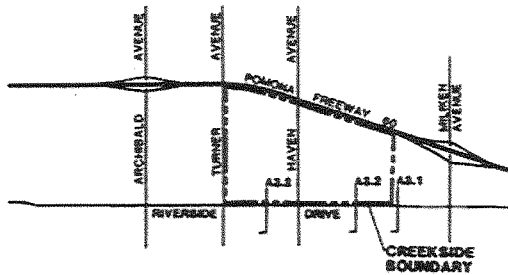
# STREET SECTIONS MAJOR ARTERIAL

## RIVERSIDE DRIVE

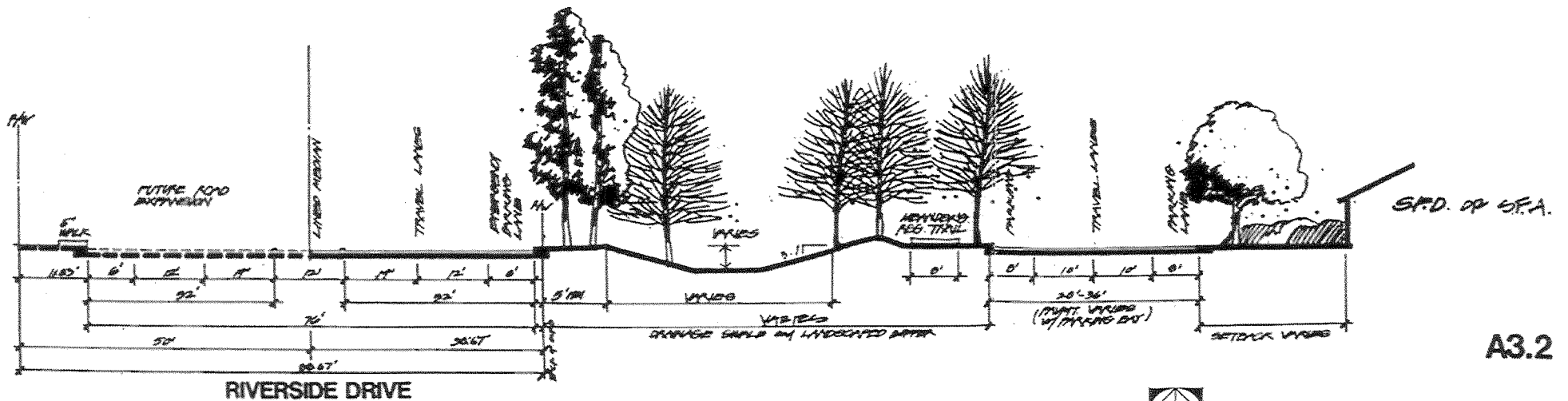
### EXHIBIT 11



CONCEPTUAL ENTRY  
TO COMMERCIAL SITE FROM RIVERSIDE DRIVE



A3.1



A3.2



AMENDMENT FIVE  
REVISED AUG 1990

## STREET SECTIONS - PUBLIC RESIDENTIAL

Public residential routes within the Creekside PRD will play a key role both in the internal circulation system and in serving as a basic entry to the project. Included in this section are the four entry routes, the loop system, and all local public streets. For continuity of the project theme, development of the East Village shall be governed by those circulation standards previously approved. Development of the West Village, however, will conform to those new standards included in this document. Both sets of standards are provided in the enclosed exhibits. All public sidewalks will be constructed of concrete and their dimension shall be referenced from the back of curb.

### Entry Routes (B1 and B2)

Primary entry (B1) to the site will be off of Haven Avenue. The 63' right-of-way (ROW) will include two 12' travel lanes in each direction, with a raised 4' median strip and an 11' turning pocket. In an effort to promote the rural theme, landscaped berms with meandering 8' bicycle-pedestrian trails will be provided adjacent to SFD planning units. Additionally, design of the entry will emphasize the transition from Haven Avenue to the project loop system.

Two secondary points (B2) are also planned, on located off of Riverside Drive and the second off of Turner Avenue. Like the primary entry, these access points will be characterized by ample landscaped buffer areas. The secondary entries will provide a 44' ROW and 46' ROW, East and West Village respectively, and incorporate a 4' sidewalk (measured from back of curb) adjacent to the curb and an 8' meandering pedestrian-bike path, within an easement to the City, along the opposing curb.

Single-family attached and detached dwellings are proposed along these entry routes. Rear and side yard walls will separate the living unit from the entry routes.

Additionally, it is proposed that parking not be permitted along these entry statements.

See EXHIBIT 12

### The Loop Roads (B3)

The loop roads are planned as central elements of the overall Creekside PRD. Not only will they

carry the majority of future traffic, they will also play a key role in establishing a cohesive and unified circulation system within the project. Although the dominant feature of these roads will be their curvilinear design, their aesthetic

treatment is also to be emphasized through the incorporation of landscaped buffers and meandering bicycle-pedestrian pathways and walkways along the entire loop system. The 44' and 46' right-of-way will have a 40' and 44'. East and West Village respectively, curb width including a travel lane and parking lane in each direction. Parking along the loop roads will be limited as determined by the City Engineer during final design. The street may be striped with a painted median island in certain locations to accommodate left turns. Single-family attached units will have both front and rear yard exposure, with rear yard walls separating all single-family detached units from the ROW proper. This street section will incorporate both a 4' sidewalk adjacent to the curb and an 8' meandering bicycle-pedestrian path along the opposite curb. These walks and paths will be maintained by the City.

See EXHIBIT 13

### 48' ROW, 42 ROW (B4)

All B-4 street sections in the project will adhere to the travel lane and parking lane requirements outlined in Exhibit 13. Each side of the alignment will incorporate a 4' walkway (measured from back of curb) with a 6.67' utility/sidewalk easement (for telephone, sewer, water, gas lines, sidewalk, etc.) occurring adjacent to the ROW. B-4 streets will provide access to single family detached units. These ROW widths service from 40 to 120 single-family detached dwelling units.

See EXHIBIT 13

### 46' ROW, 42 ROW (B5)

All 46' ROW and 42' ROW streets (B5), East Village and West Village respectively, will provide a travel lane, a parking lane, and a 4' walkway in each direction. A 6.67' utility-sidewalk easement will flank each side of the ROW; street setbacks will conform to the development standards set forth in Section 3 of this amendment. This ROW width services from 20 to 40 single-family detached dwelling units.

See EXHIBIT 14

### 37' ROW, 38' ROW (B6)

All B6 street sections in the project will adhere to the travel lane and parking lane requirements outlined in Exhibit 14. The B6 street sections are characterized by a 10' travel lane and 8' parking lane in each direction, with a 4' P.C.C. sidewalk to be provided on each side within a pedestrian easement. Also included are 6.67' utility/sidewalk easements on each side of the ROW with street setbacks conforming to the development standards set forth in Section 3. This ROW services from 0 to 20 single-family detached dwelling units on cul-de-sacs which are no longer than 350' measured to the throat of the cul-de-sac.

See EXHIBIT 14

### 45' ROW (B7)

The 45' ROW will provide for two 10' travel lanes, two 8' parking lanes and two 4' P.C.C. sidewalks. A 3' utility/sidewalk easement is located on each side of the ROW. This ROW services 0 to 20 single-family detached dwelling units on through streets within the East Village only.

See EXHIBIT 14

Frontage roads are only proposed along Riverside Drive, within Planning Area U. The frontage roads are designed to provide adequate separation of through traffic from internal traffic and increase the safety of future residents onsite. All frontage roads will offer single 10' travel lanes in each direction with 8' parking lanes on one side. In some locations, parking bays may be constructed, resulting in ROW widths ranging from 28' to 36'. This ROW services single-family detached or attached units on one side of the street.

See EXHIBIT 14

### Street Plan - Cul-de-Sac

Cul-de-Sacs will be provided in a variety of locations throughout the Creekside PRD. With 21' travel lanes and a 10' radius for the landscaped islands, a minimum of 31' effective turning radius will be obtained, ensuring accessibility for service vehicles (such as fire trucks and trash vehicles) in all locations. To further insure accessibility, the landscaped island will have rolled curbs with a 2' wide apron along its edge. Unlike the conventional circular cul-de-sacs, those in Creekside PRD will incorporate a recti-linear

design in order to increase the efficient utilization of the land. This concept also encourages the use of driveways for parking with limited use of on-street parking. Posting of no parking signs

and red painted curbs shall be required in all cul-de-sacs. The maximum length of cul-de-sacs shall be determined by the City Engineer, however, cul-de-sacs utilizing B-6 approach sections shall not exceed 350'.

Pedestrian travel will be by a 4' sidewalk to be provided within a sidewalk easement along the perimeter of all cul-de-sacs; an additional 3' utility easement will be located along each side of the sidewalk easement.

See EASEMENT 15

### Street Plan -Knuckle

In an effort to lend additional variety to the Creekside PRD streetscape semi-cul-de-sacs (i.e., "knuckles") are proposed and will be constructed with landscaped islands in the East and West Villages.

In these areas, through traffic is accommodated by two 11' travel lanes in each direction outside the knuckle; inside the knuckle a single 15' travel lane is provided - this will provide an effective turning radius of 31'.

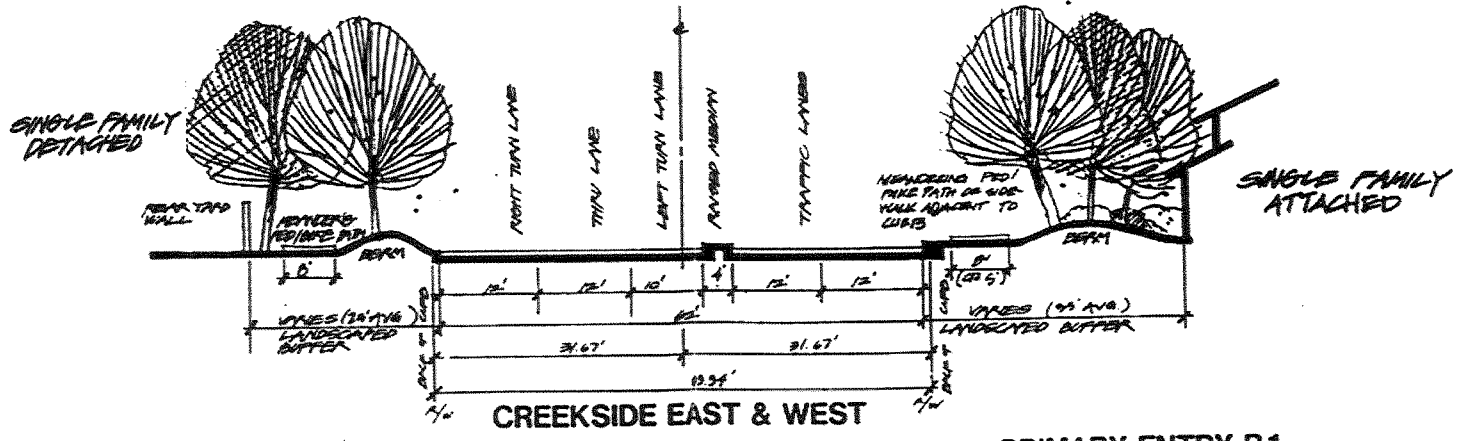
As with the square cul-de-sacs, the knuckle concept will encourage the use by residents of off-street parking (i.e., driveways and garages); posting of no parking signs and red painted curbs shall be required in all knuckles.

Pedestrian travel will be accommodated by 4' sidewalks flanking the knuckle ROW perimeter, and 3' utility easements will be located immediately adjacent to the sidewalk.

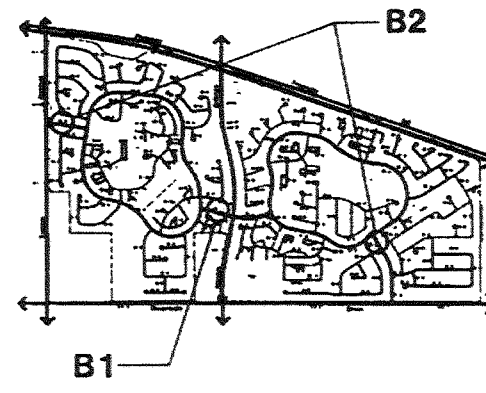
All public street less than 40' wide shall incorporate limited parking signage, as per City of Ontario requirements, to allow for solid waste collection and street sweeping vehicle access.

See EXHIBIT 16

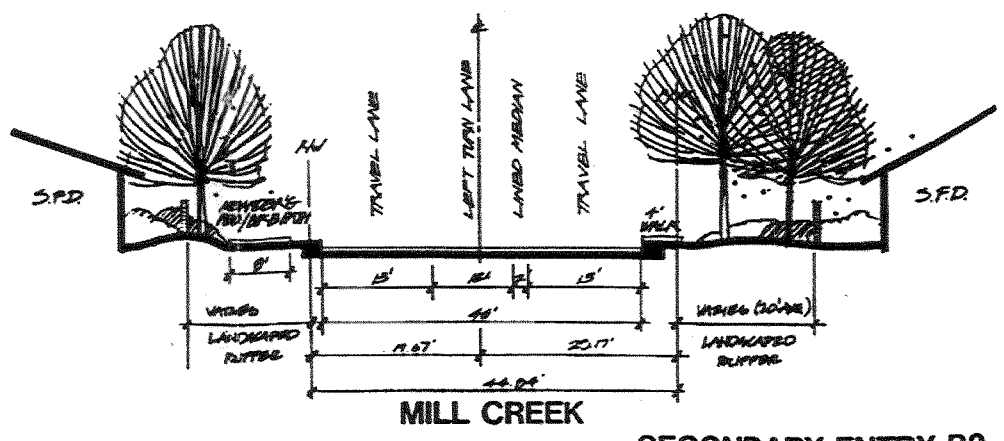




NOTE: STREET WIDTHS MEASURED TO FLOWLINE

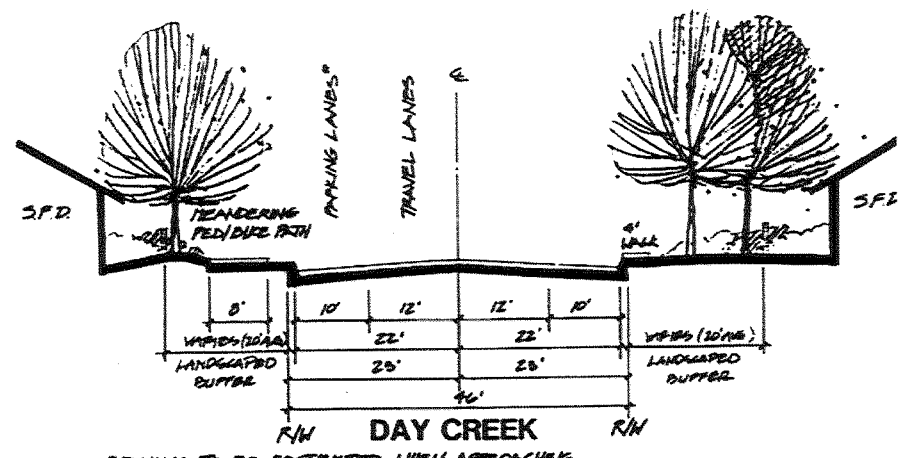


PRIMARY ENTRY B1



MILL CREEK

SECONDARY ENTRY B2  
(EAST VILLAGE ONLY)



DAY CREEK

SECONDARY ENTRY B2  
(WEST VILLAGE ONLY)

STREET SECTIONS  
PUBLIC RESIDENTIAL  
SECONDARY ENTRY  
PRIMARY ENTRY

AMENDMENT FIVE  
REVISED AUG 1990

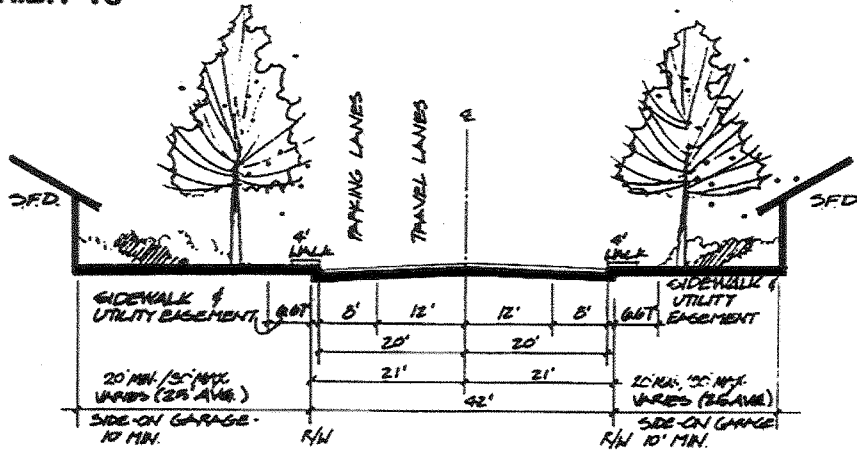
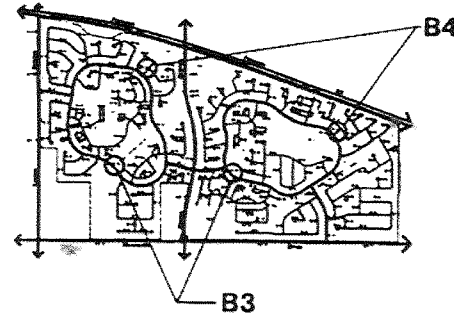
Exhibit 12



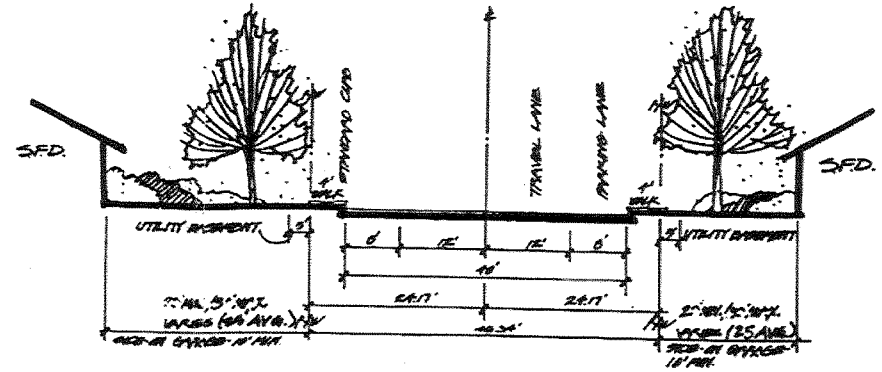
# STREET SECTIONS PUBLIC RESIDENTIAL

48.34' R.O.W.  
42.0' R.O.W.  
LOOP ROAD

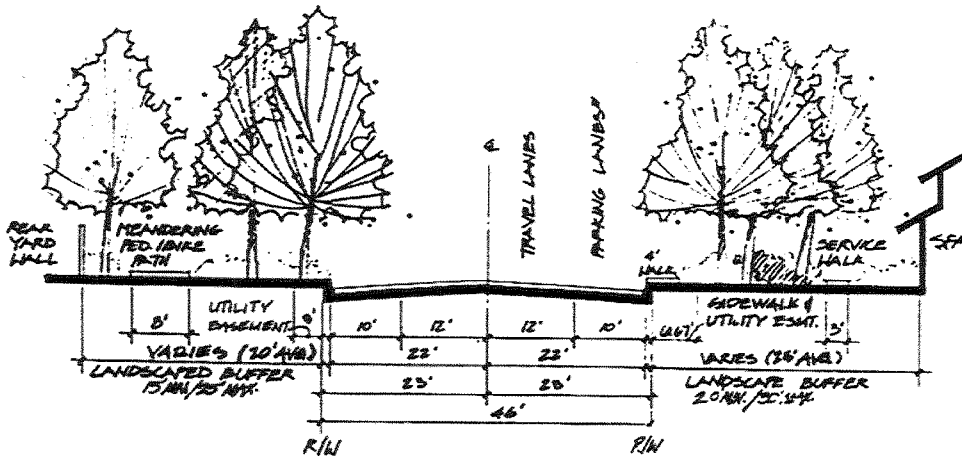
EXHIBIT 13



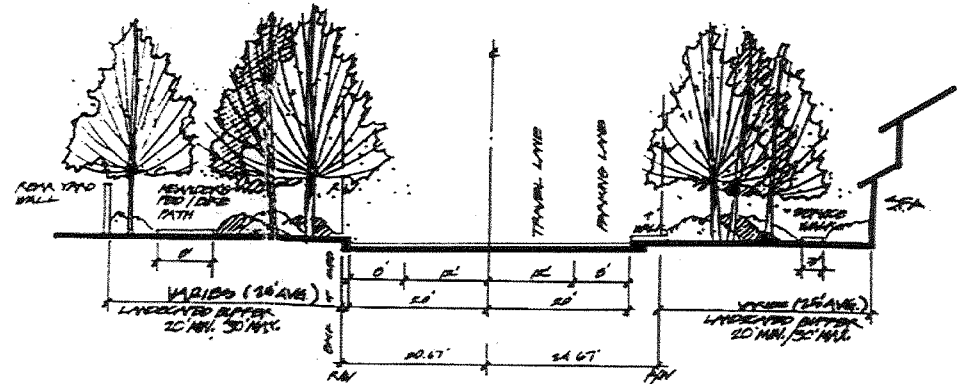
42.0' R.O.W. B4  
(WEST VILLAGE ONLY)



48.34' R.O.W. B4  
(EAST VILLAGE ONLY)

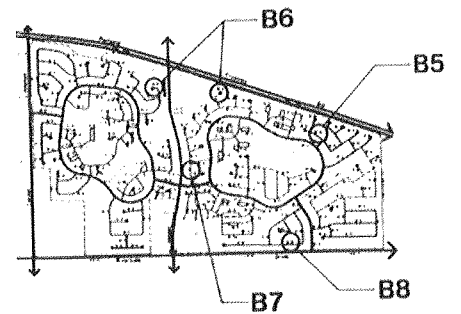
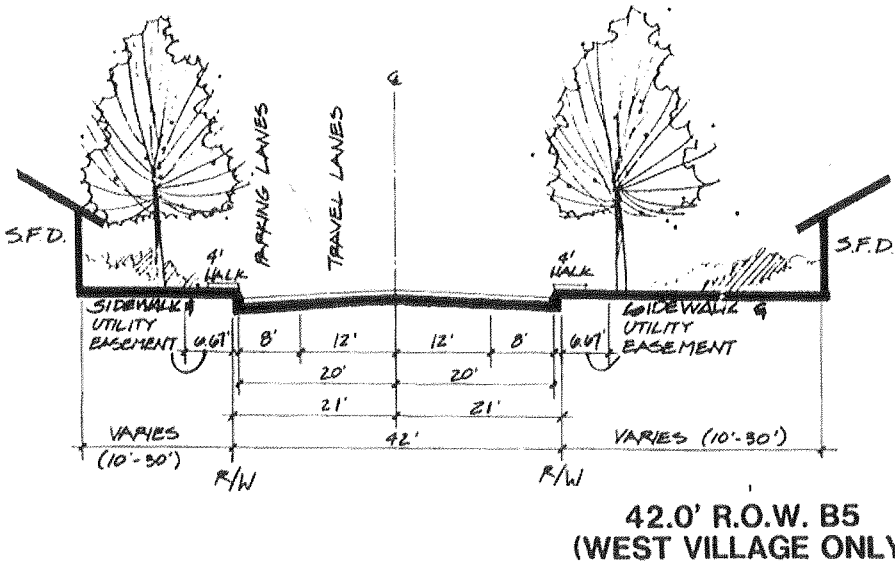
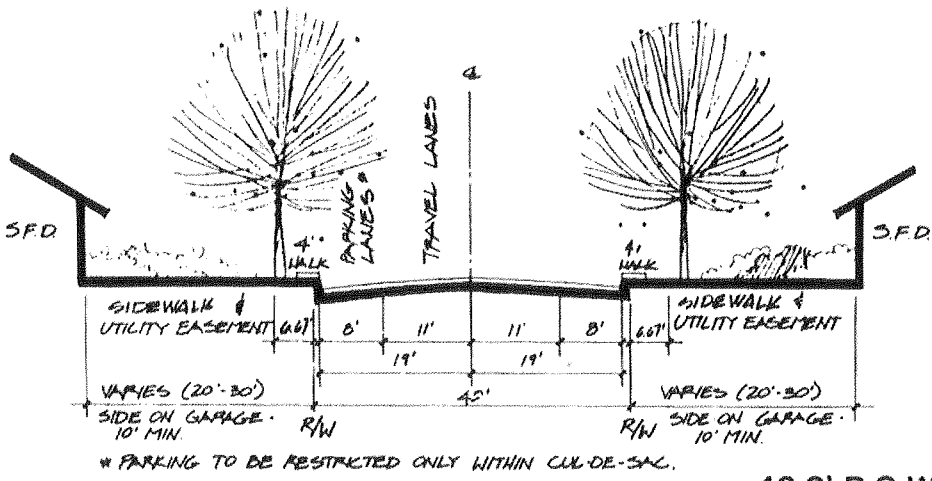
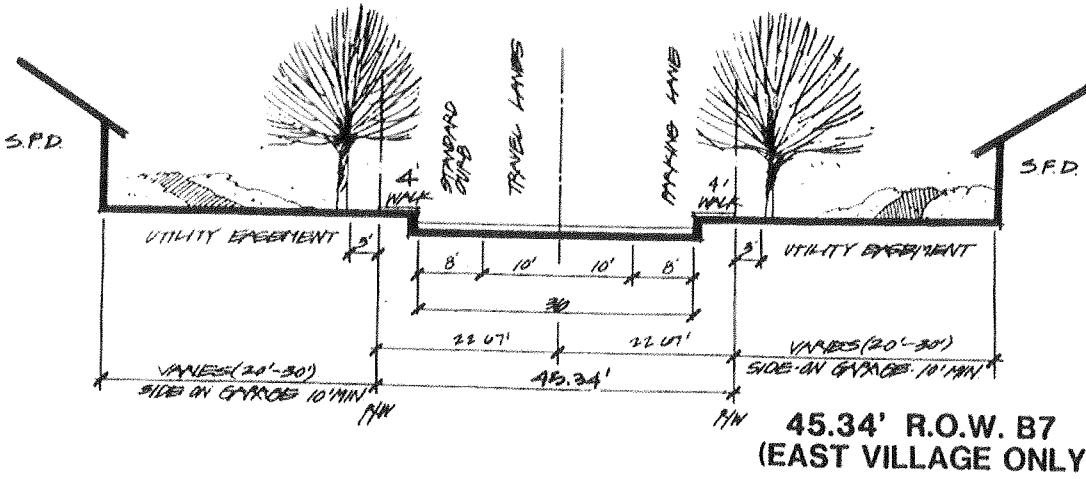


LOOP ROAD B3  
(WEST VILLAGE ONLY)

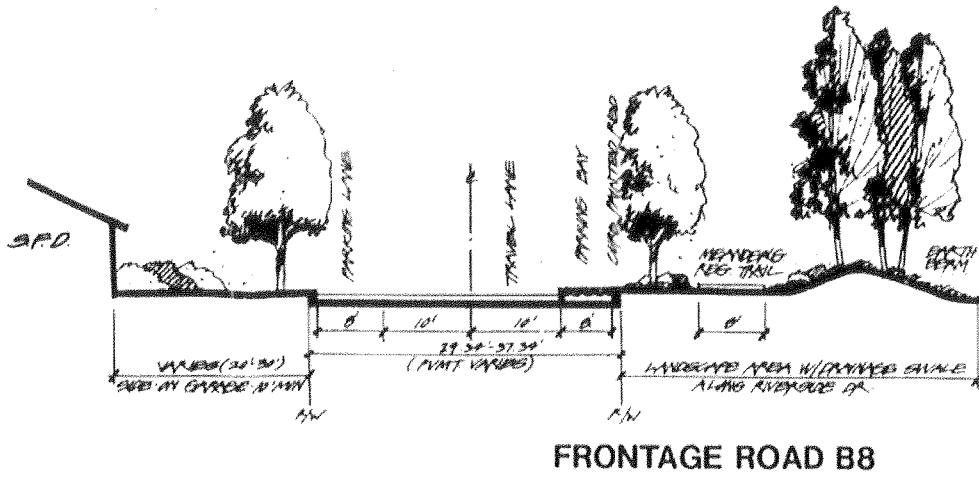


LOOP ROAD B3  
(EAST VILLAGE ONLY)

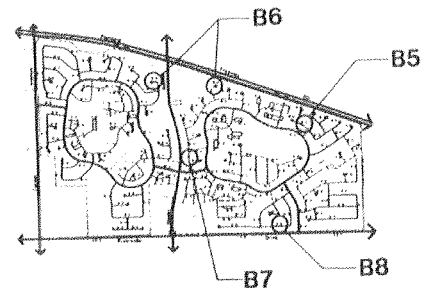
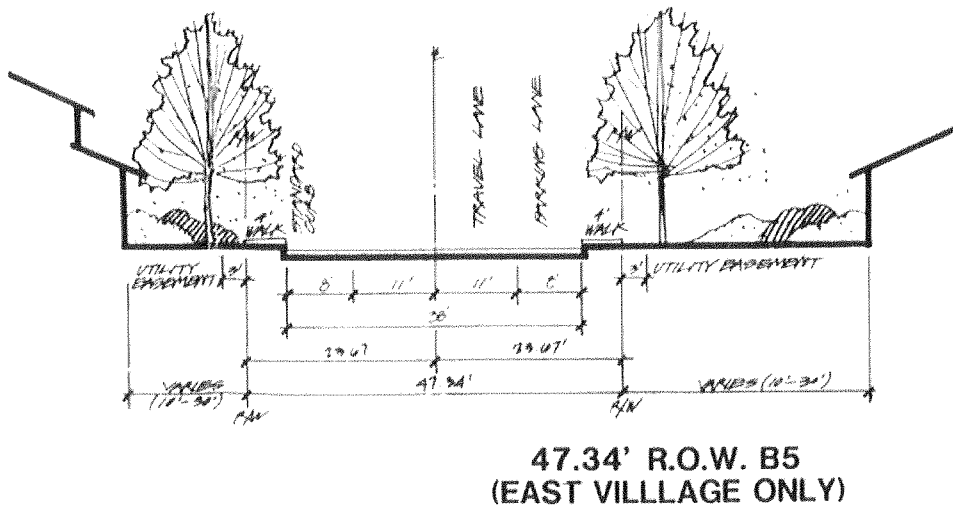
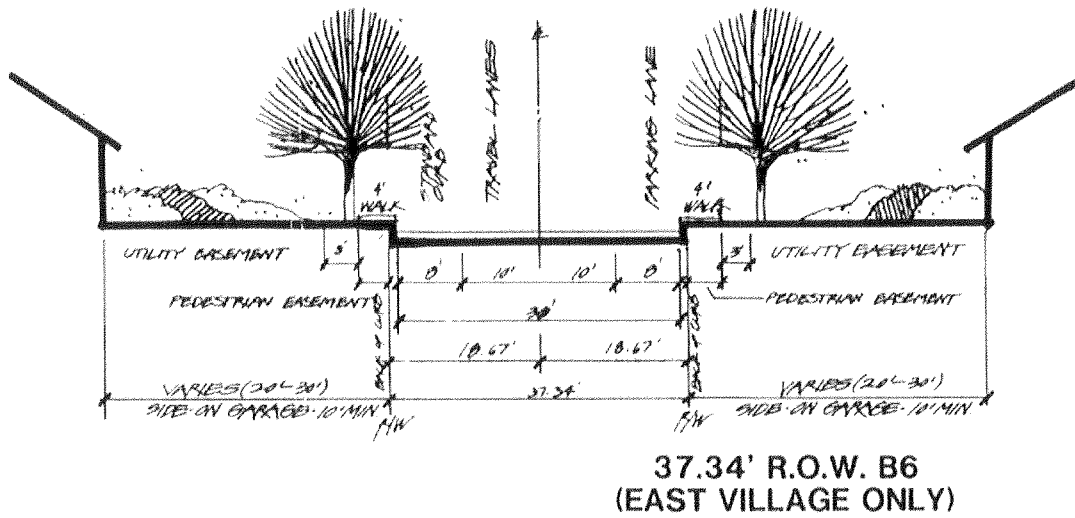
\* PARKING TO BE RESTRICTED WHEN APPROACHING INTERSECTIONS.



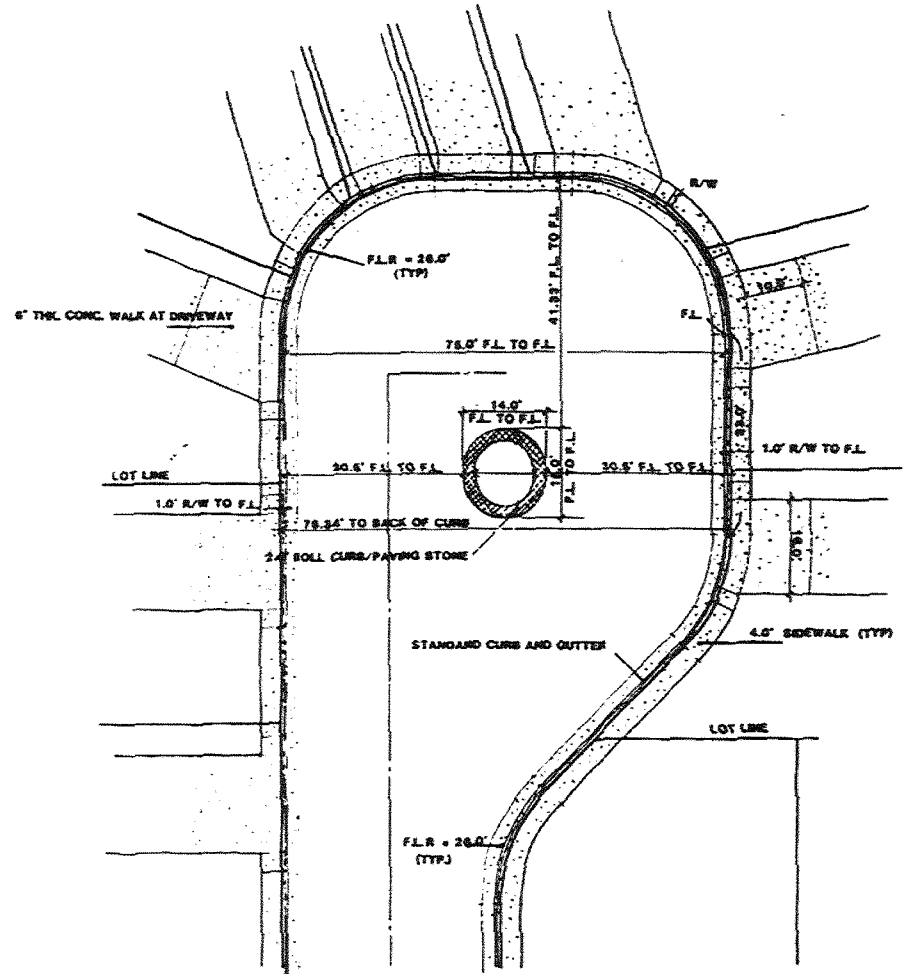
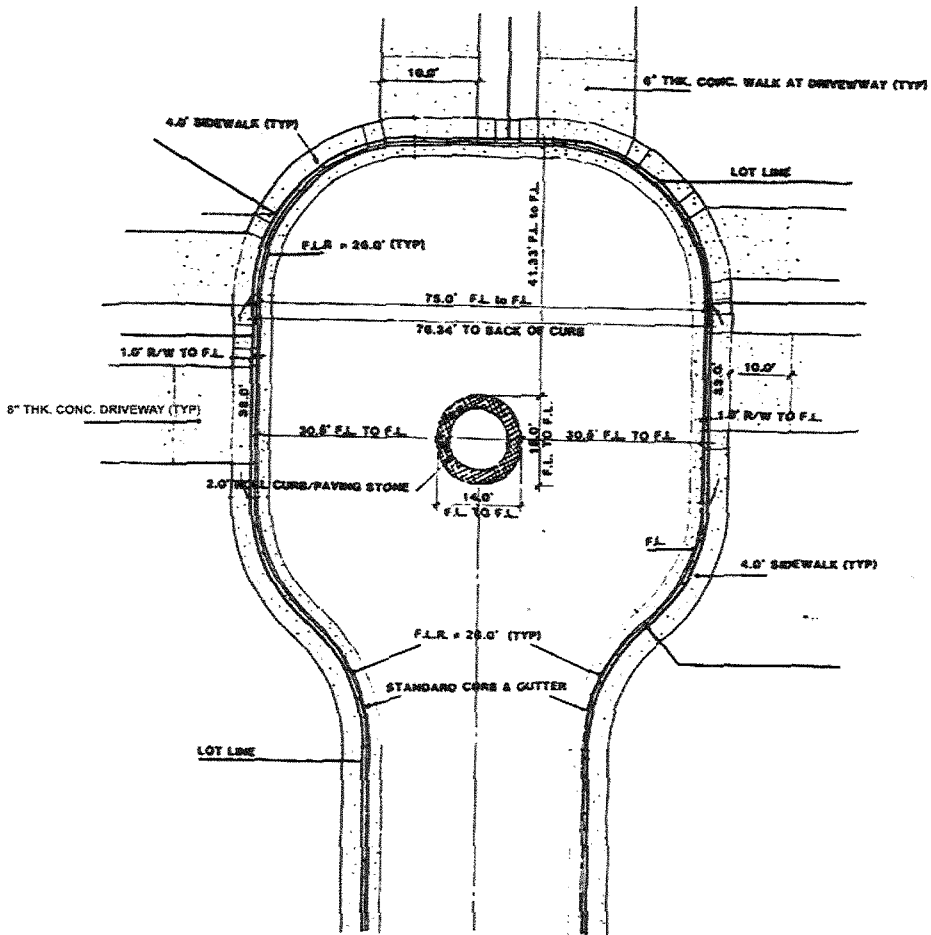
**STREET SECTIONS  
 PUBLIC RESIDENTIAL**  
 Exhibit 14 (Side A)



- FRONTAGE ROAD
- 37.34' R.O.W.
- 40.0' R.O.W.
- 42.0' R.O.W.
- 45.34' R.O.W.
- 47.34' R.O.W.



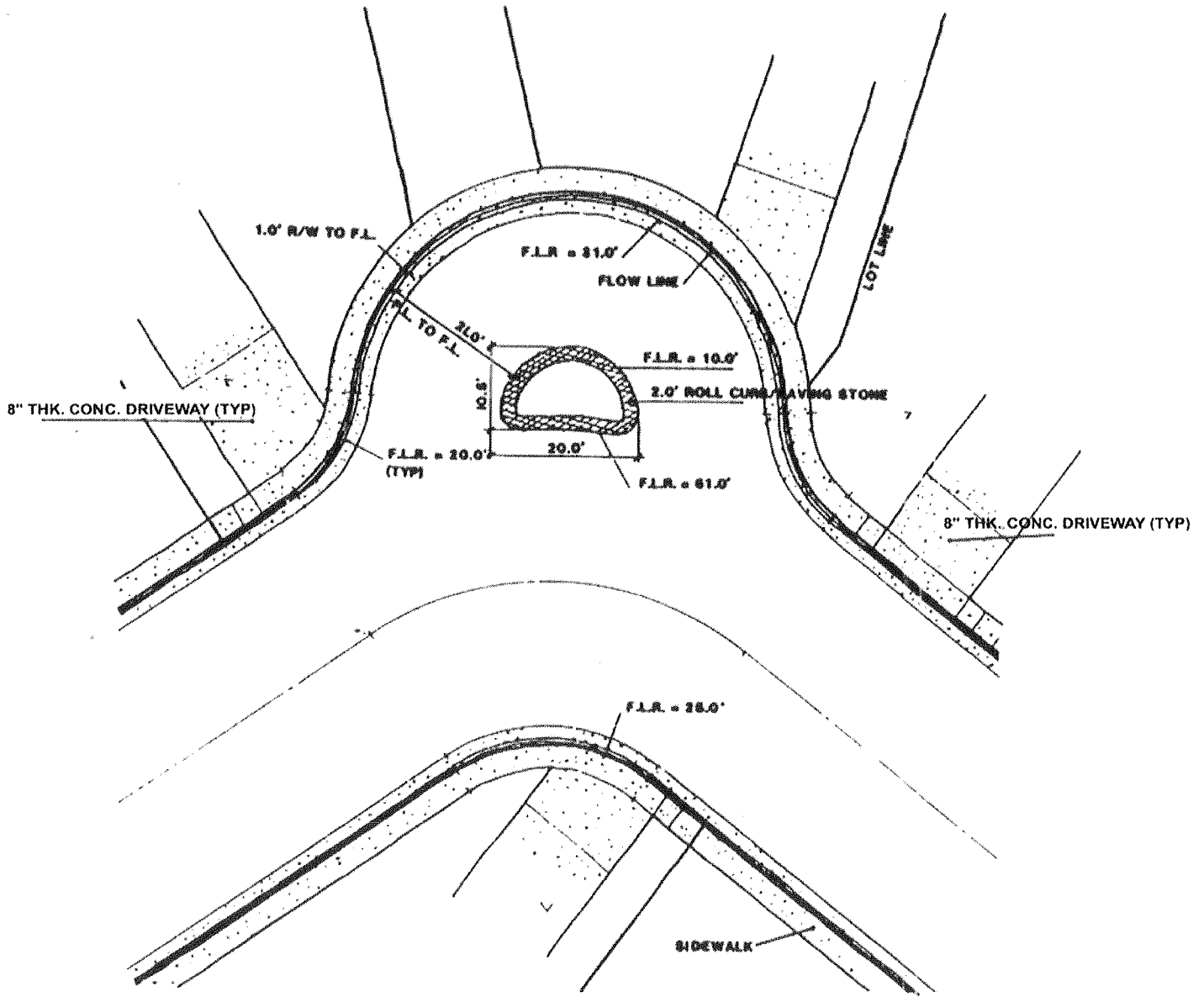
**STREET SECTIONS**  
**PUBLIC RESIDENTIAL**  
Exhibit 14 (Continued)



37

# STREET PLAN CUL-DE-SAC

Exhibit 15



**STREET PLAN KNUCKLE**  
 Exhibit 16

## STREET SECTIONS - PRIVATE RESIDENTIAL

The private residential street system will be constructed in two basic categories: 42' right-of-way (ROW) streets and private drives. Private streets will be signed in accordance with 21107 of the California Vehicle Code to provide for Police Department control. Private streets will be designed in a manner which clearly distinguishes them from public streets. Each is described below as follows:

### 42' ROW (C1)

The 42' ROW streets will provide access to single-family attached units and parking courts throughout the PRD. These routes will contain a single 10' travel lane and 8' parking lane in each direction. Rolled curbs will be utilized to "soften" the transition from street to landscaping to building. Additionally, a 4' walkway will be provided in one direction. Three foot utility easements will flank each side of the road, and setbacks totalling a minimum of 10' from ROW edge to the dwelling units will be provided (5' minimum to carports or garages). The 42' ROW streets will be utilized exclusively by single-family attached housing units. The ROW will serve from 50 to 150 dwelling units. Parallel parking on C1 streets will not be credited toward meeting onsite parking requirements.

See EXHIBIT 17

### 44' ROW (C1a)

The 44' ROW streets will provide access to single family attached units and parking courts throughout the PRD. These routes will contain a single 11' travel lane and 8' parking lane in each direction. In all other respects the design will be identical to the 42' ROW C1 street. This ROW will serve SFA planning units with greater than 150 dwelling units. Parallel parking on C1a streets will not be credited toward meeting onsite parking requirements.

See EXHIBIT 17

### Private Drives (C2)

The private drives will provide direct access to all garage and carport areas. To meet individual requirements within the site, three ROW designs are proposed. The 24' ROW private drive incorporates a single 12' travel lane in each direction; rolled curbs and landscaped buffers

along each side of the drives are incorporated into this design. The 26' ROW also incorporates 12' travel lanes in each direction, with the rolled curb proposed along only one side while a 2' "apron" (for driveway access to carports) defines the adjacent edge. The 28' ROW private drives are proposed where single-family attached travel lanes and 2' carport entry "aprons" are provided in each direction for access to the adjacent carport facilities. For the purpose of readily identifying maintenance responsibilities, all private streets will be identified by a small tag marked "Private" which will be attached to street signs on private streets.

See EXHIBIT 17

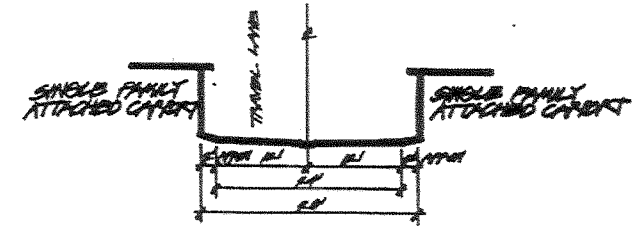
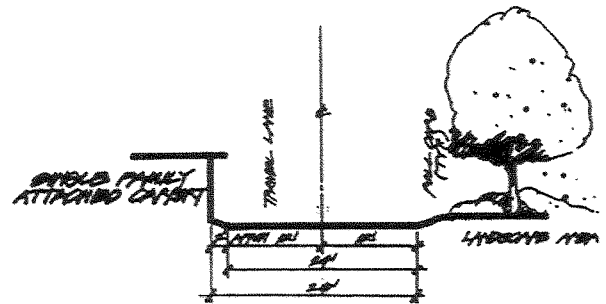
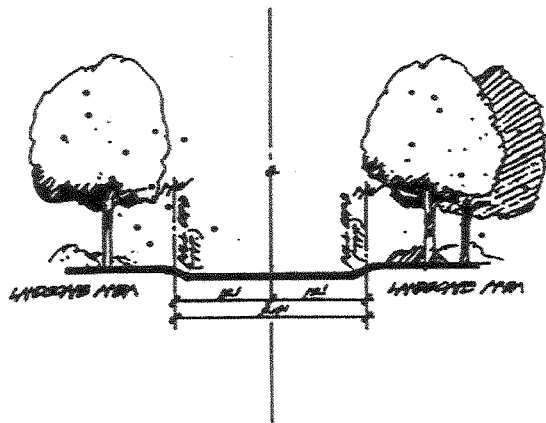
### Emergency Access (D1)

Emergency routes are shown throughout the PRD, all sited in locations to reduce access time and minimize travel on the internal street system. In keeping with appropriate standards, the emergency access routes will be 15' wide in all cases and "paved" with turf block or similar material to provide an aesthetic as well as functional role within the PRD.

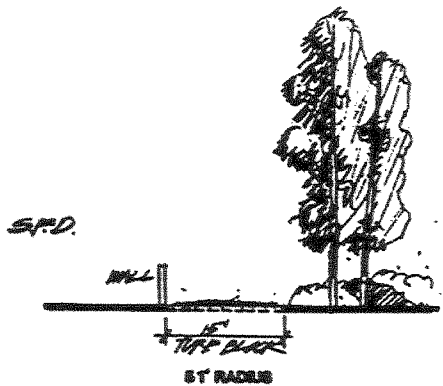
These access routes will have an obstruction-free path with a 51' minimum radius for easy accessibility.

To further insure safety and fire protection, any private drive over 150' length or with obstructed views will be provided with a turnaround or emergency access.

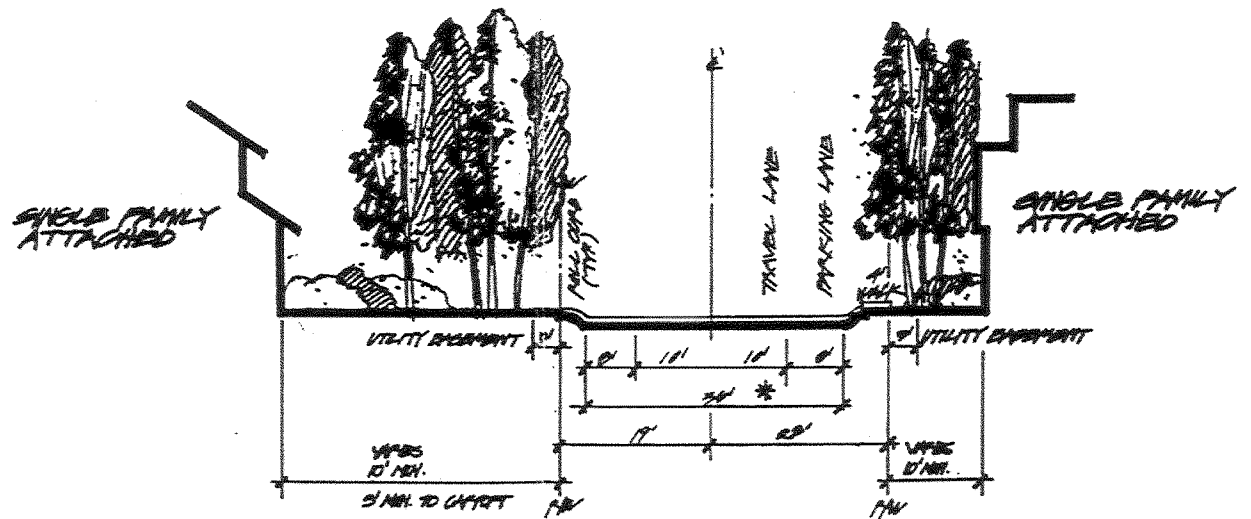
See EXHIBIT 17



Private Drives C2



Emergency Access D1



\* NOTE:  
C1a Pavement  
Width To Be 36'

42' R.O.W. C1  
44' R.O.W. C1a

**STREET SECTIONS  
PRIVATE RESIDENTIAL**

Exhibit 17

**PRIVATE DRIVES  
42' R.O.W.  
EMERGENCY ACCESS**



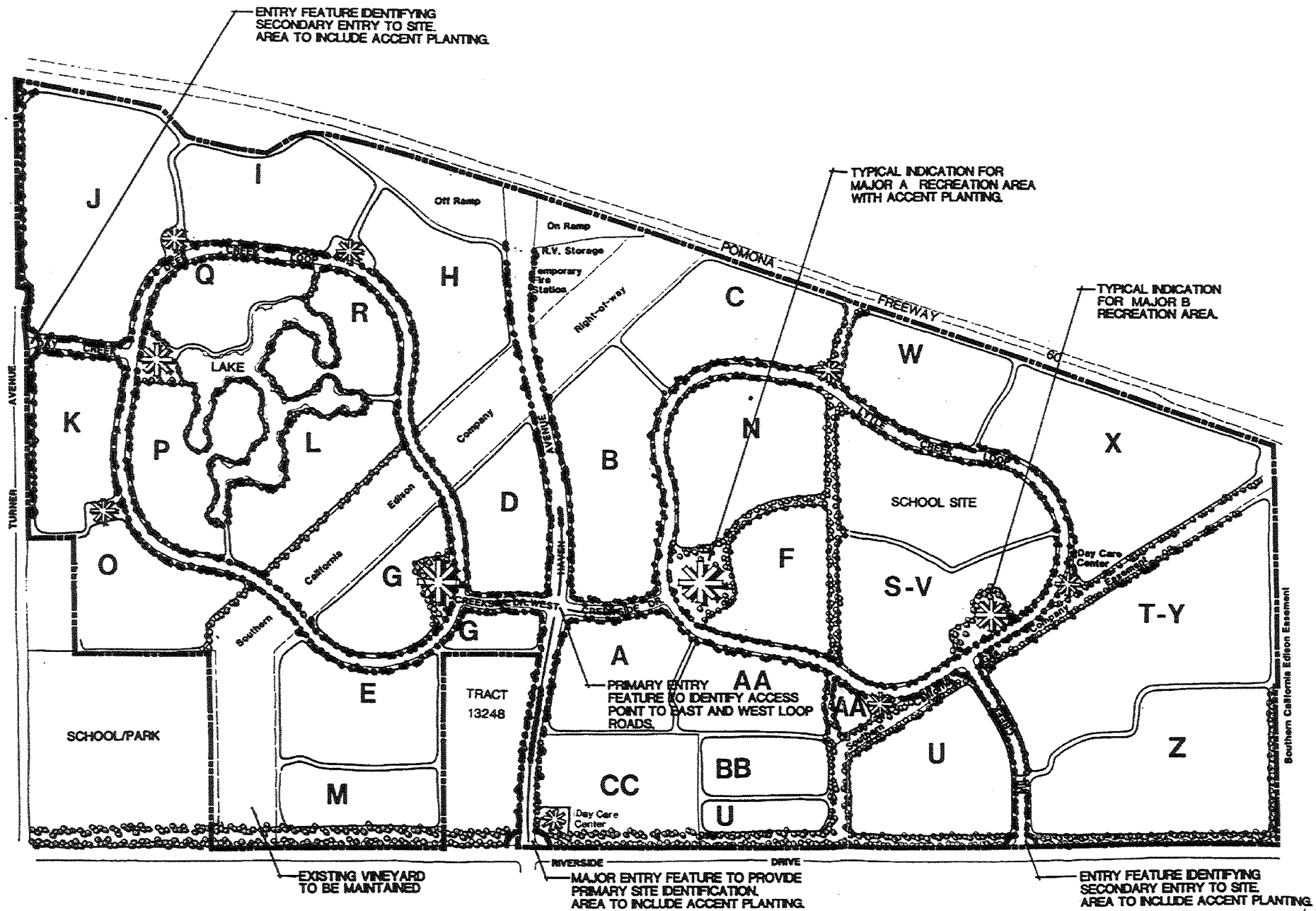
The Conceptual Landscape Plan provides for the primary visual element in establishing the rural theme of the community. The Creekside Village development, as implied in its name, will encompass a residential community interconnected by a landscaped greenbelt-drainage swale network comprised of intermittent creek-like water courses and a lake amidst a landscape of water-oriented vegetation. Eight types of landscape treatment are designated on the Conceptual Landscape Plan including six types of streetscape planting for arterial entry accents, neighborhood entry accents, medians and islands, landscaping for drainage swale areas, and landscaping for lake areas.

The lake plantings and the "stream-like" greenbelt (activity corridor) plantings will provide the primary visual element for reinforcing the rural theme of Creekside.

Each of the entryways on Haven Avenue as well as the Riverside Drive entrance onto Haven Avenue and the Turner Avenue entry will incorporate strong thematic statements thorough the use of accent plantings. Each of these entries will be characterized by the "streamlike" vegetation, boulders, project monumentation and, at the Riverside Drive entry, a "bridge-like" structure traversing the greenbelt-swale. Each activity center will also be emphasized through the use of accent plantings.

Streetscape planting will provide the secondary means of emphasizing the country flavor and will occur along the southerly portion of Haven Avenue on each of the entries and on the loop roads. Such trees as the American Sweet Gum, London Plain Tree and Eucalyptus will be used in these areas. A planting plan for public and private neighborhood streets within the individual planning areas will provide a neighborhood landscaping theme. Low spiny shrubs will be planted adjacent to all solid walls and fences whenever a public area meets a private area. Such shrubs as silverberry; dwarf xylosma, yellow trumpet vine, natel plum, and firethorne will be used for this purpose.

Landscaping will be provided southerly of the Freeway soundwall. Within SFA areas, the developer/builder will install the landscaping and the homeowner sub-association will maintain it. Within SFD areas, the homeowner will be responsible for both installation and maintenance.



CONCEPTUAL LANDSCAPE PLAN  
Exhibit 18

## STREETSCAPE PLANTING

### ARTERIALS (HAVEN, RIVERSIDE, TURNER)

ALNUS RHOMBIFOLIA	WHITE ALDER
PLATANUS ACERIFOLIA	LONDON PLANE TREE
EUCALYPTUS MACULATA	EUCALYPTUS
TURF	
UNMOWN TURF	

<b>MAJOR, PRIMARY, &amp; SECONDARY</b>	<b>ENTRY ACCENTS</b>
POPULUS NIGRA 'ITALICA'	LOMBARDY POPLAR
JUNIPERUS SABINA 'BUFFALO'	JUNIPER
HEMEROCALLIS AURANTIACA	DAYLILY

<b>NEIGHBORHOOD ENTRY ACCENTS</b>	
GLEDITSIA TRIACANTHOS	HONEY LOCUST

<b>MEDIANS</b>	
EUCALYPTUS MACULATA	EUCALYPTUS
PLATANUS ACERIFOLIA	LONDON PLANE TREE
HEMEROCALLIS AURANTIACA	DAYLILY
HEDERA HEDERA HELIX 'HAHN'	HAHN'S IVY
TURF	

<b>COLLECTORS (LOOP ROADS &amp; PROJECT ENTRIES)</b>	
PINUS CANARIENSIS	CANARY ISLAND PINE
PLATANUS ACERIFOLIA	LONDON PLANE TREE
GLEDITSIA TRIACANTHOS	HONEY LOCUST
EUCALYPTUS MACULATA	EUCALYPTUS
LIQUIDAMBAR STYRACIFLUA	SWEET GUM
CLYTOSTOMA CALLISTEGIOIDES	VIOLET TRUMPET VINE
RAPHIOLEPS INDICA 'ENCHANTRESS'	INDIA HAWTHORN
XYLOSMA COMPACTA	XYLOSMA
HEMEROCALLIS AURANTIACA	DAYLILY
ESCALLONIA 'FRACDESI'	FRACDESI ESCALLONIA
HEDERA HELIX 'HAHN'	HAHN'S IVY
TURF	

<b>NEIGHBORHOOD STREETS &amp; ISLANDS (CUL-DE-SACS &amp; KNUCKLES IN EAST VILLAGE ONLY)</b>	
PYRUS CALLERYANA 'BRADFORD'	BRADFORD PEAR
PRUNUS CERASIFERA	
'THUNDERCLOUD'	PURPLE-LEAF PLUM
PLATANUS ACERIFOLIA	LONDON PLANE TREE
LIQUIDAMBAR STYRACIFLUA	SWEET GUM
LAGERSTROEMIA INDICA 'LAVENDER'	GRAPE MYRTLE

## GREENBELTS (ACTIVITY CORRIDORS)

PINUS HALEPENSIS	ALEPPO PINE
ALNUS RHOMBIFOLIA	WHITE ALDER
EUCALYPTUS MACULATA	EUCALYPTUS
AREBUTUS UNEDO 'COMPACTA'	STRAWBERRY TREE
TURF	
UNMOWN TURF	

## LAKE EDGE PLANTING

ULMUS PARVIFOLIA 'DRAKE'	CHINESE ELM
SEQUOIA SEMPERVIRENS	
'SANTA CRUZ'	COAST REDWOOD
PLATANUS ACERIFOLIA	LONDON PLANE TREE
PINUS HALEPENSIS	ALEPPO PINE
EUCALYPTUS NICHOLII	NICHOL'S WILLOW-LEAFED
	PEPPERMINT
CUPRESSOCYPARIS LEYLANDII	LEYLANDII CYPRESS

## LAKE AMENITY PLAN

Overall Lake Concept - A lake approximately 7.6 acres in size will be constructed within the West Village loop road providing recreational and aesthetic amenities to Creekside residents. A beach club facility and a lake oriented pedestrian trail will provide lake access and recreation for all residents. In addition, the lake oriented pedestrian trail will provide lake access and recreation for all residents. In addition, the lake setting will provide adjacent homes with prime water views, enhancing the prestige and environment of this area.

Permitted Lake Uses - The following water oriented activities will be permitted: fishing, boating on craft 10 feet and under (sailboats, paddleboats, row boats, electric boats, etc.), hiking on the trail adjacent to the lake, picnicking and other passive activities in common areas adjacent to the lake.

The following uses will not be permitted: swimming, boating in craft over 10 feet, and motor boating, except as provided above.

Lake Edge Treatment - The exhibits which follow provide details for the lake edge adjacent to single family detached and single family attached units. Three alternative treatments are provided.

SFD Lake Edge - A 10 foot wide privately owned landscape area will be provided along the lake edge adjacent to tall single family detached homes. Homeowners will be responsible for installing landscaping within this area within 90 days of occupancy. Landscaping and other improvements within this area must be approved by the Master Homeowners' Association. A maintenance easement will be retained by the West Village Master Homeowner Association.

A maximum 3 foot high wall will be allowed within a 10 to 15 foot setback from the lake edge. All taller structure must have a 15 foot minimum setback from the lake edge. SFD walls, patios, and buildings facing the lake edge must be approved by the Master Developer. Any encroachment of structures into the 10 foot setback area must be approved by the Master Developer.

See EXHIBIT 19

SFA Lake Edge - An 18 foot wide landscape easement with a pedestrian trail adjacent to single

family attached homes. All single story structures must have a minimum 18 foot setback from the lake edge and all two story structures must have

a minimum 25 foot setback from the lake edge unless specifically approved by the Master Developer. SFA walls, patios, and buildings facing the lake edge must be architecturally pleasing and must be approved by the master developer.

See EXHIBIT 20

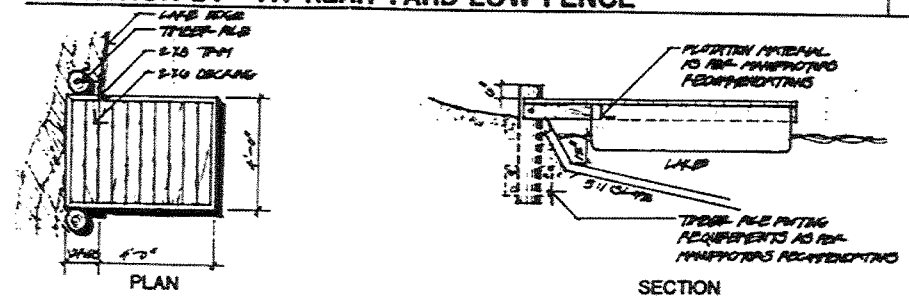
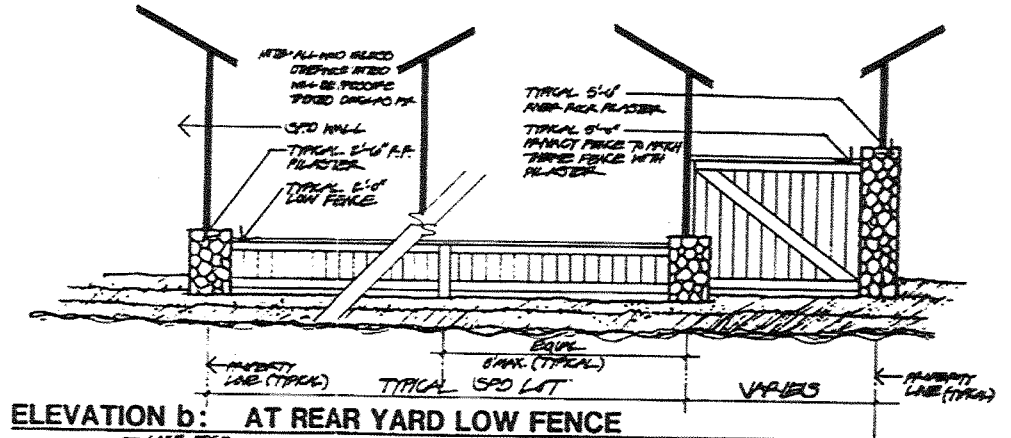
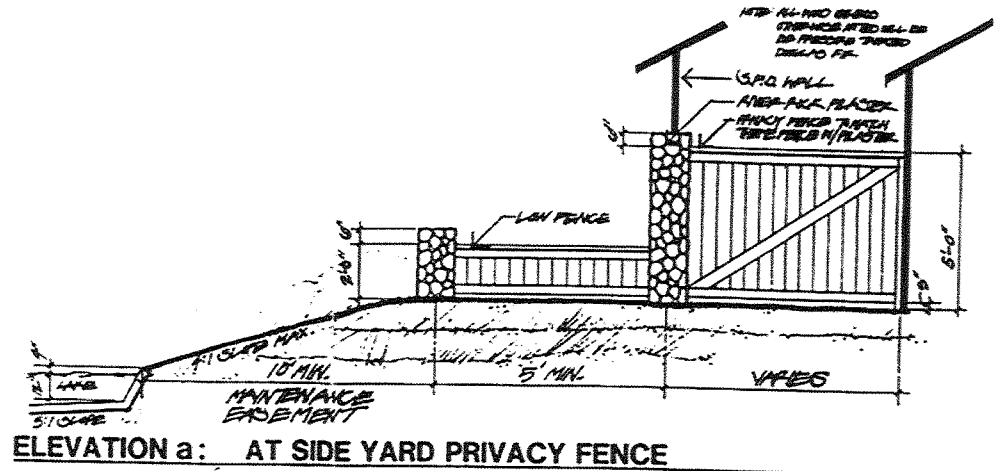
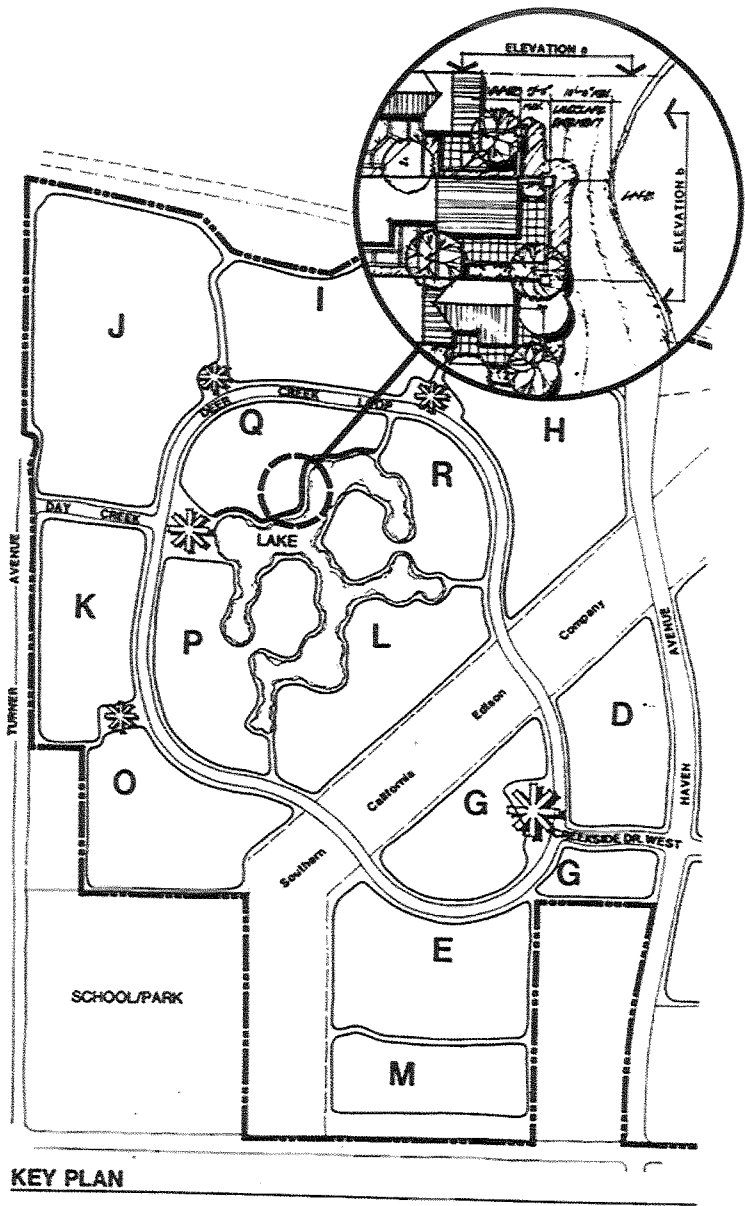
Lake Trail Pedestrian/Bicycle Access - Access adjacent to single family attached homes, will be provided through a 8 foot wide hard surfaced pedestrian/bicycle trail within the 18 foot landscaped "lake-side easement". This access will be important in providing use and enjoyment of the lake for all residents. Several access points to this lake trail will be provided via links from Deer Creek Loop. This trail will be provided with pedestrian lighting and emergency telephone equipment.

Beach Club - The Major B Activity Center located northwesterly of the lake will be developed as a beach club. The architectural theme will reflect a beach club-marine theme. Beach club facilities will include the following: pool, spa, sand beach, restrooms, tot lot, barbecue and offstreet parking.

The following additional facilities may be included: boat dock, boat rental facilities, and sand volleyball.

See EXHIBIT 7

As required for lake security and safety a Homeowners' Association water craft will be maintained at the beach club facility. This craft will be available for use by City police and fire rescue in the case of emergency. The boat shall be properly equipped to meet these special needs. In addition, the pool/lake recreation activity will be supervised in accordance with local requirements. Access to the motorized watercraft will be maintained at all times, and if secured, shall be done so in a manner approved by both the Police and Fire Departments.

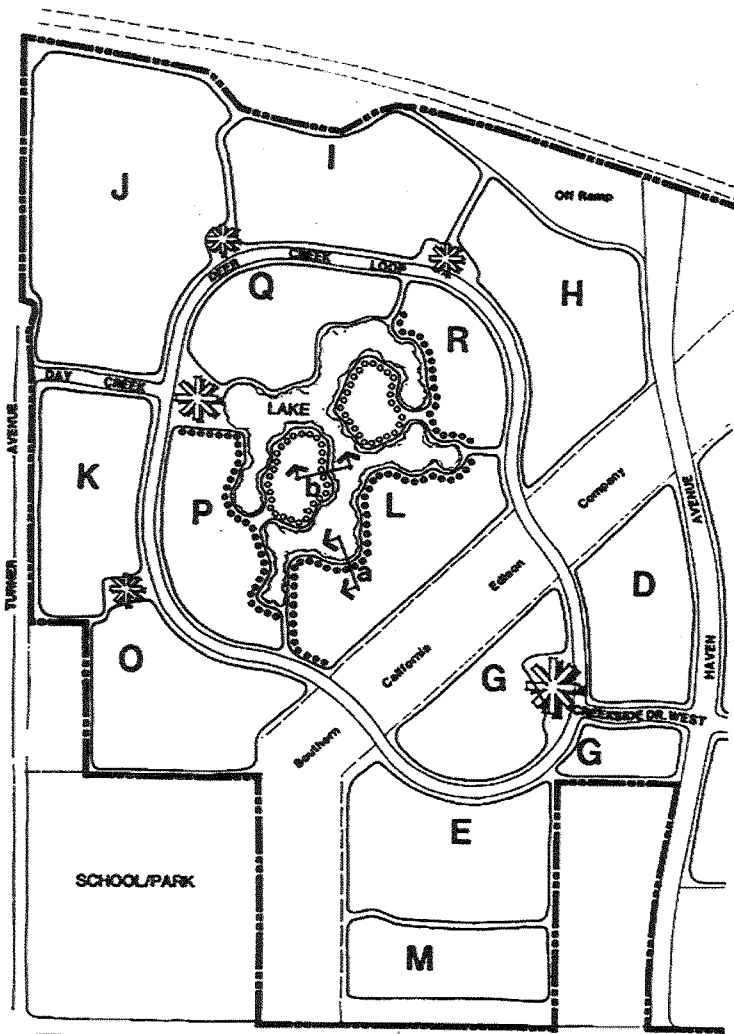


NOTES: 1. ALL FENCING/PREFABRICATED DOCK DETAILS, INCLUDING MATERIALS AND FINISHES WILL BE SUBMITTED AS SHOP DRAWINGS TO THE MASTER BUILDER FOR APPROVAL

2. FOR LAKE EDGE PLANT MATERIAL PALETTE SEE CONCEPTUAL LANDSCAPE PLAN

# LAKE EDGE CONDITIONS

## SINGLE FAMILY DETACHED



**KEY PLAN**

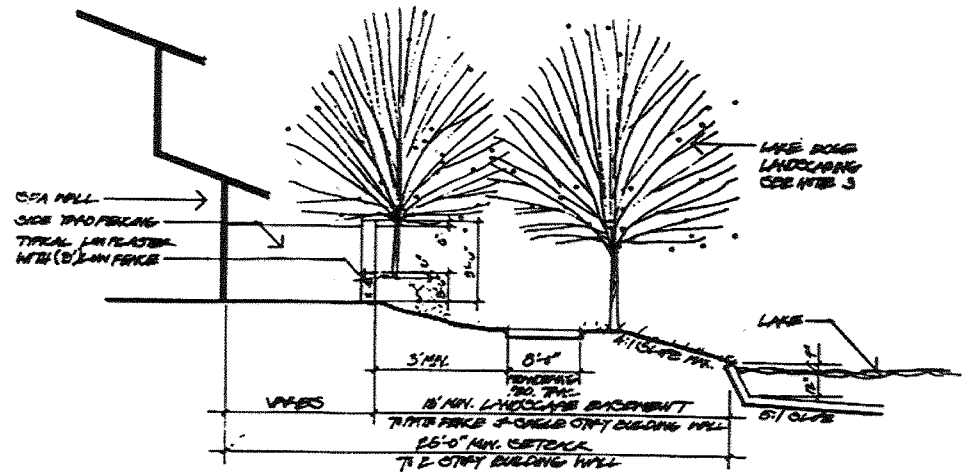
..... FENCING ALONG LANDSCAPE EASEMENT WITH PEDESTRIAN TRAIL

o-o-o-o-o FENCING ALONG LANDSCAPE EASEMENT WITHOUT PEDESTRIAN TRAIL

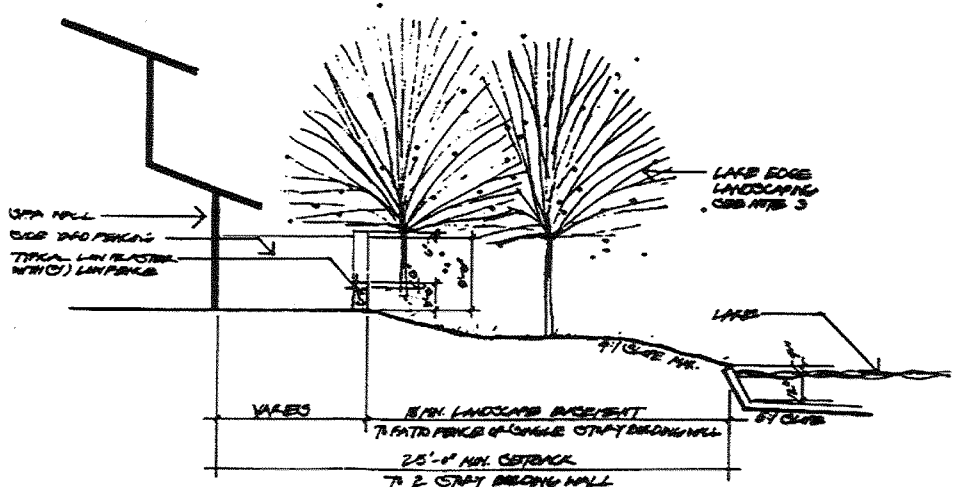
(REFERENCE KEY PLAN THIS SHEET)

**LAKE CONDITONS**  
SINGLE FAMILY ATTACHED

Exhibit 20



**ELEVATION/SECTION a**



**ELEVATION/SECTION b**

NOTES: 1. THE ARCHITECTURAL CHARACTER OF ALL SFA PATIO WALLS AND FENCES WILL REFLECT THE ARCHITECTURE WITHIN THE DEVELOPMENT AREAS

2. ALL FENCING DETAILS, INCLUDING MATERIALS AND FINISHES WILL BE SUBMITTED AS SHOP DRAWINGS TO THE MASTER BUILDER FOR APPROVAL

3. FOR LAKE EDGE PLANT MATERIAL PALETTE SEE CONCEPTUAL LANDSCAPE PLAN

## INFRASTRUCTURE

### Phasing

Phasing of infrastructure streets, utilities, greenbelt/activity corridors and roadways will correspond to the phasing of the residential components of the Creekside development and is subject to approval by the City Engineer. The Infrastructure Phasing Plan designates, in particular, the phasing plans for the roadway system.

See EXHIBIT 21

The following infrastructure facilities will be central to the project development.

### Storm Drain System

The proposed drainage system is a network of open drainage swales and greenbelts traversing and interconnecting individual areas of the Creekside development. The system is designed to carry a 100-year frequency storm flow. Incorporated into the system are sub-drains to carry minor runoff water with surface connections occurring approximately every 300 feet. The Homeowners' Association will maintain the surface of the swales. The sub-drains will be maintained by the city. Siltation and/or locations as shown on Exhibit 22. The West Village lake may also provide an opportunity to peak flow attenuation while acting as a retarding basin.

The swales will be turfed, with gently sloping sides and will carry runoff to the primary drainage swale along the north side of Riverside Drive. Wherever roadways and drainage swales cross, adequate provisions will be made to prevent street flooding.

Runoff from the project will reach Deer Creek by means of an underground pipe system along Riverside Drive from Turner Avenue to Deer Creek. An agreement is currently in place with the County of San Bernardino which addresses master developer participation in construction of ultimate improvements in lower Deer Creek.

See EXHIBIT 22

### Water System

There are existing 10" water mains in Turner Avenue and Riverside Drive, west of Turner. Oversizing of existing distribution facilities or

supplemental pumping stations are anticipated to be necessary for the project. Construction of

offsite mains required to serve the development have been accomplished. When required due to fire flow demands, the project water system will be looped back to Turner Avenue and extended to the north to join the existing system west of the project. The water system, when installed, will meet all City of Ontario standards and will deliver approximately 2000 g.p.m. of fire flow at the eastern most location of the East Village loop road. Reimbursement agreements are proposed for facilities serving areas outside the project area. Unless specific upgrades are accomplished in the water system, no construction of building units will proceed that would exceed the available water for that given area.

See EXHIBIT 23

### Sewer System

To serve the proposed development, a trunk sewer line is proposed in Riverside Drive east of Turner Avenue. A lift station is proposed along Riverside Drive which will lift the sewage into a proposed force main extending westerly to the City's golf course pump station and then to the Chino Basin Municipal Water District (C.B.M.W.D.) Regional Sewage Treatment Plant #1. The specific location of the proposed pump station will be on the northeast corner of Riverside Drive and Turner Avenue.

See EXHIBIT 24

Maintenance Responsibilities

The following matrix has been developed to define the maintenance responsibilities of the City of Ontario and the Master Homeowners' Association/Planned Residential Development (P.R.D.) or subassociations relative to streets backbone infrastructure utilities, sidewalks, pedestrian/bicycle trails, greenbelt activity areas, signage, lighting and landscaping.

MAINTENANCE MATRIX

MAINTENANCE ITEM

Responsibility  
City      P.R.D  
 (or Sub-association)

Landscape Amenities

- |    |  |   |
|----|--|---|
| 1. | Common Areas, Street Buffers, Parkways | X |
| 2. | Greenbelt Activity Areas (Swales)      | X |
| 3. | Recreation Areas, Activity Areas       | X |

Streets

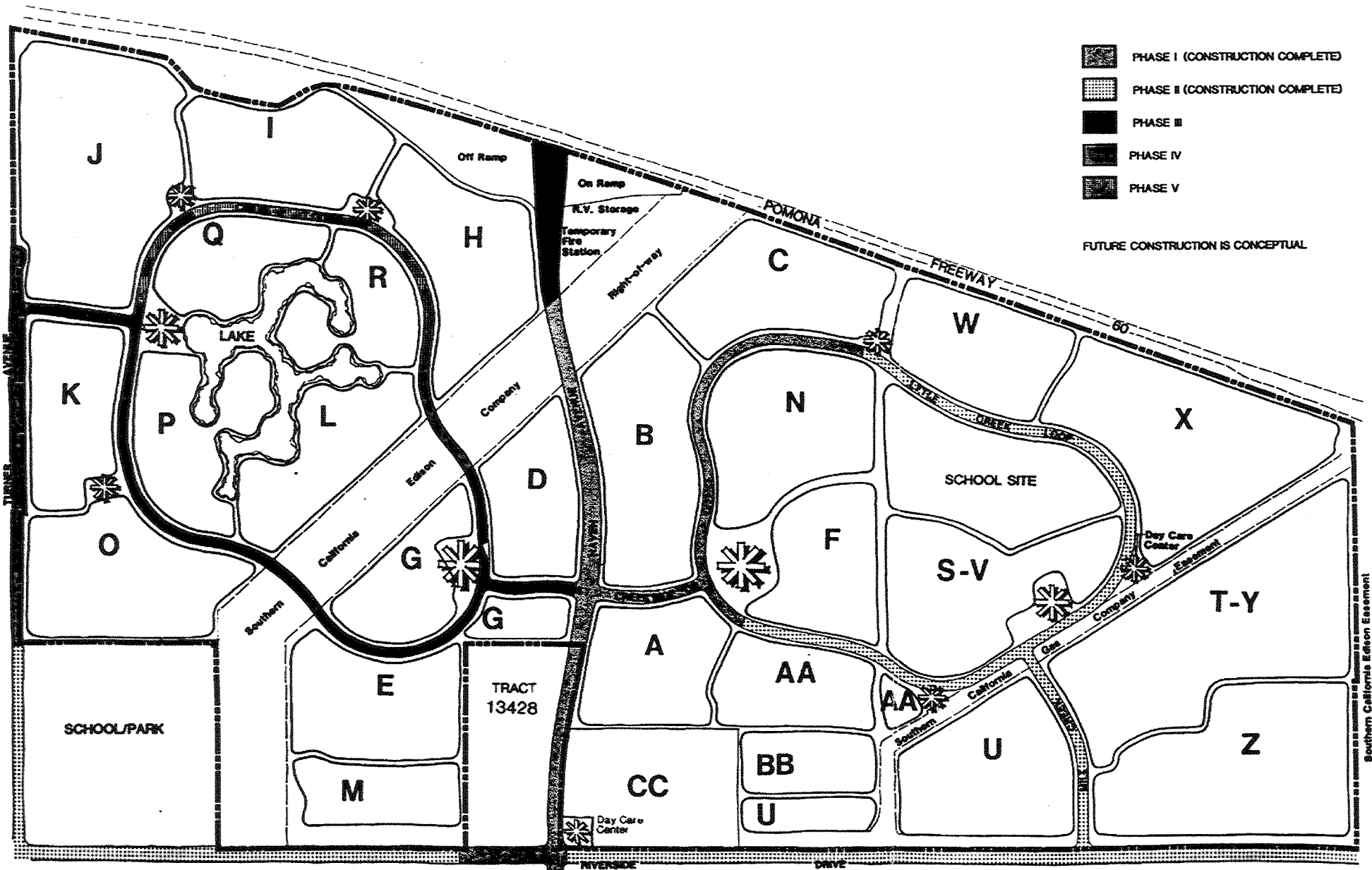
- |    |   |   |
|----|---|---|
| 1. | Public Streets (curbs, gutter, & sidewalks)                                     | X |
| 2. | Private Street (curb, gutter, & sidewalks)                                      | X |
| 3. | Median Islands  | X |
| 4. | Street Lights (City Standard Lighting )   | X |
| 5. | Traffic Control Signs (City Standards)  | X |
| 6. | Traffic Control Signs (P.R.D. non-standard)                                     | X |
| 7. | Sidewalks/Ped-Bicycle Paths within public ROW and dedicated City easements only | X |

- |    |   |   |
|----|---|---|
| 8. | Sidewalks/Ped-Bicycle Paths (all facilities outside public ROW and dedicated City easements, i.e., activity areas, greenbelts, recreation areas). | X |
|----|---|---|

Infrastructure Utilities

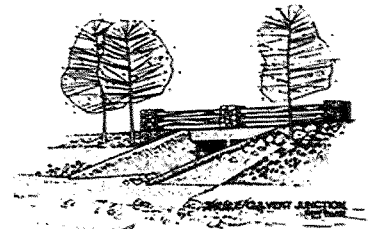
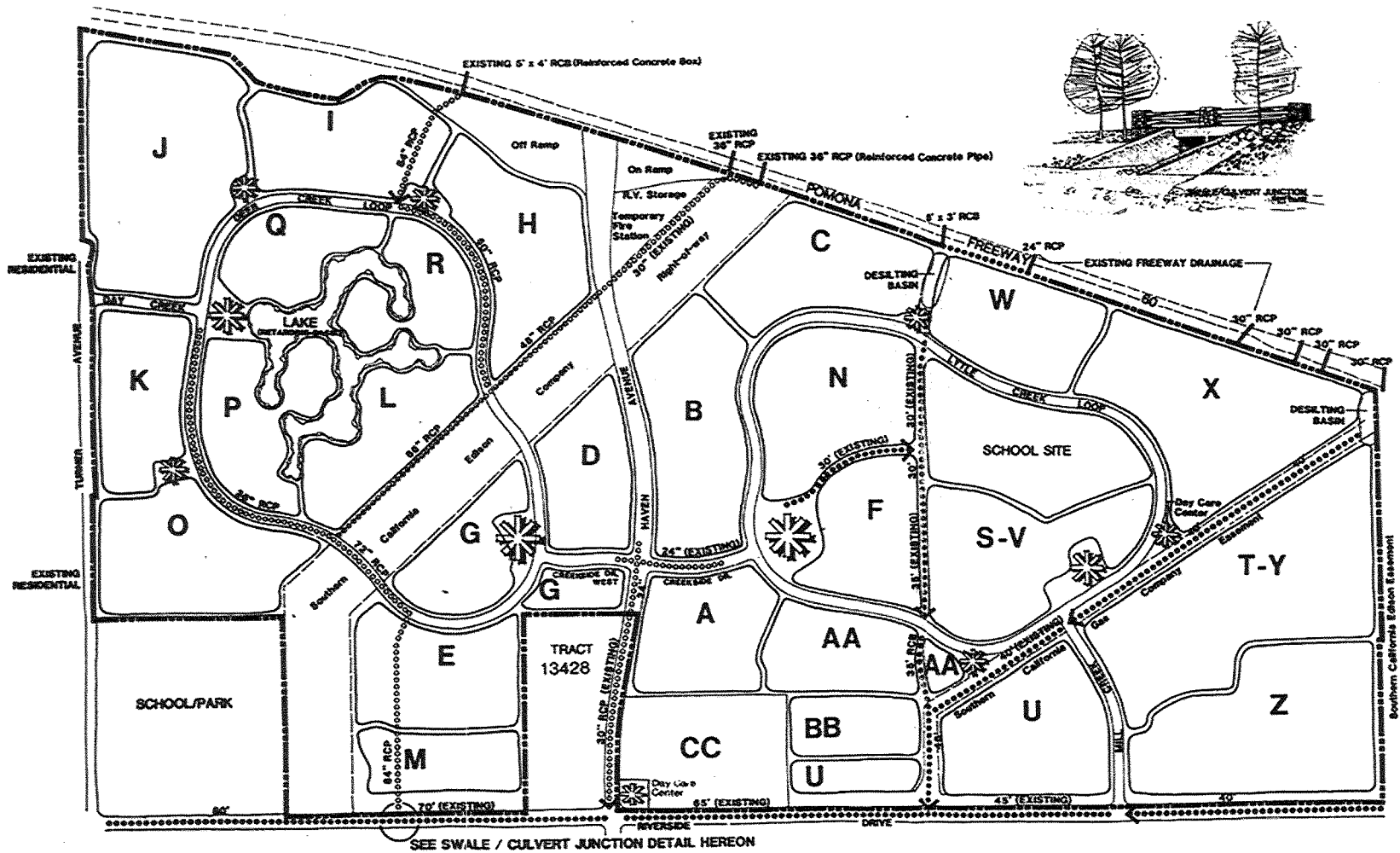
- |    |   |   |
|----|---|---|
| 1. | Storm Drain (mainline S.D. including underchain pipe and inlets in swales)                    | X |
| 2. | Storm Drain (drainage swale surface, area drains)   | X |
| 3. | Sewers (all locations provided construction material conforms to City Standards - 8" minimum) | X |
| 4. | Water (all locations provided construction material conforms to City standards - 6" minimum)  | X |





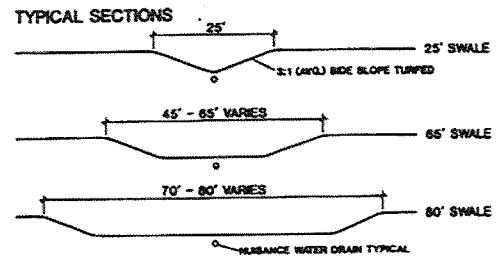
# INFRA-STRUCTURE PHASING STREETS

Exhibit 21

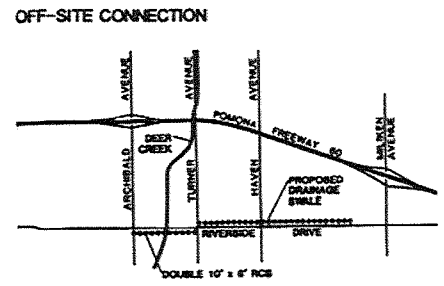


SEE SWALE / CULVERT JUNCTION DETAIL HEREON

- ..... DRAINAGE SWALE
- 24' DRAINAGE SWALE TOP WIDTH
- ..... DRAINAGE PIPE
- 18" PIPE SIZE

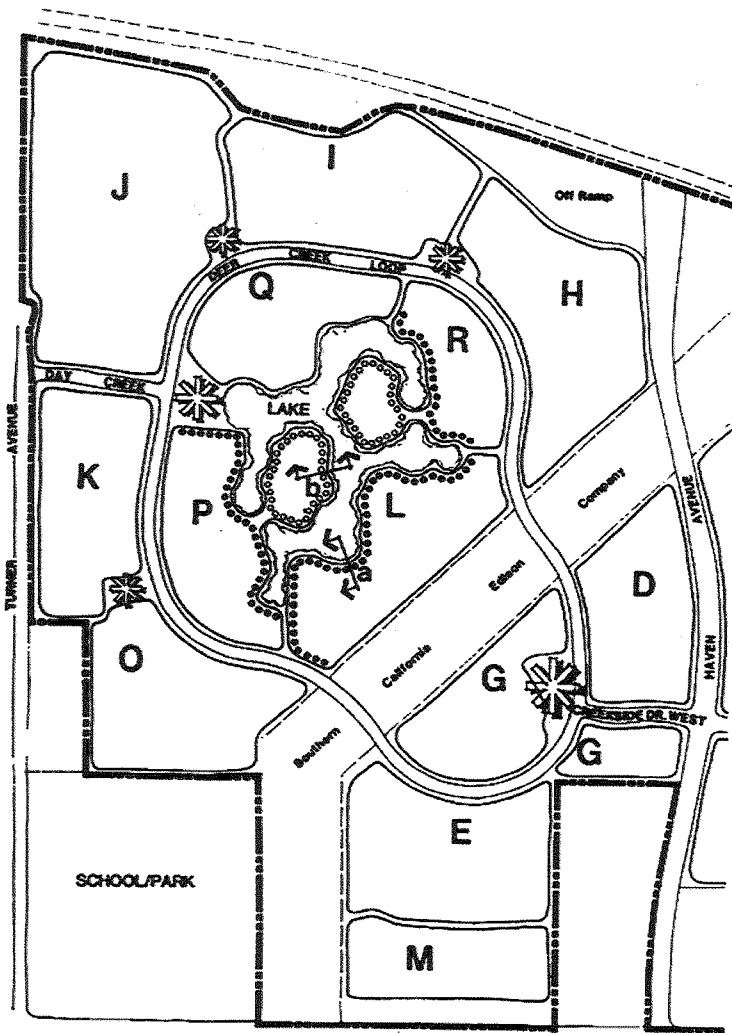


FUTURE CONSTRUCTION IS CONCEPTUAL



# INFRA-STRUCTURE STORM DRAINS

Exhibit 22



**KEY PLAN**

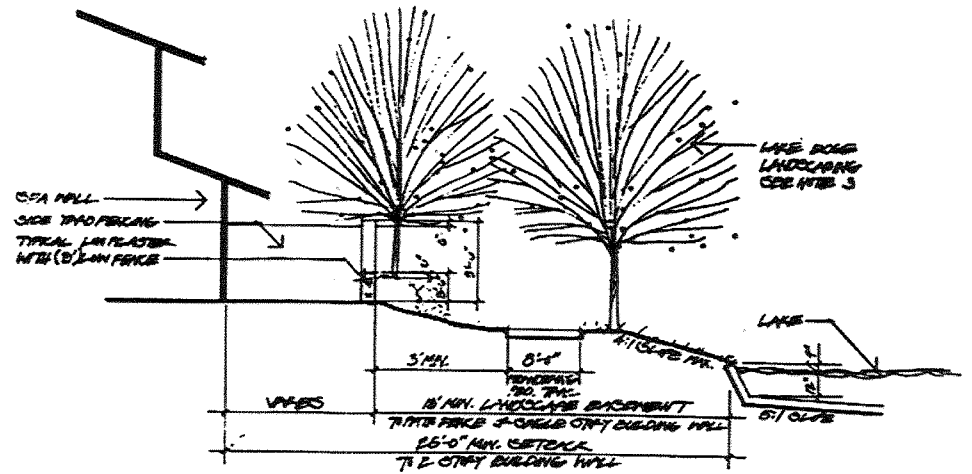
..... FENCING ALONG LANDSCAPE EASEMENT WITH PEDESTRIAN TRAIL

o-o-o-o-o FENCING ALONG LANDSCAPE EASEMENT WITHOUT PEDESTRIAN TRAIL

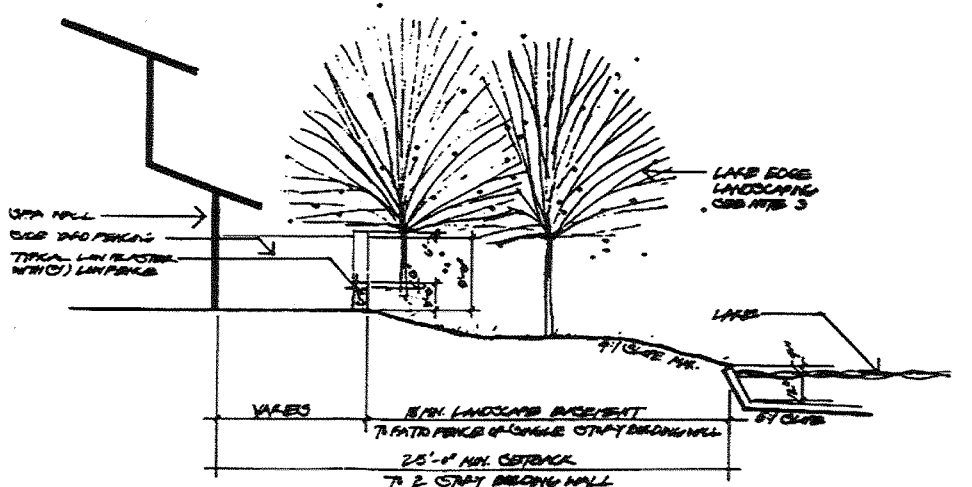
(REFERENCE KEY PLAN THIS SHEET)

**LAKE CONDITONS**  
SINGLE FAMILY ATTACHED

Exhibit 20



**ELEVATION/SECTION a**

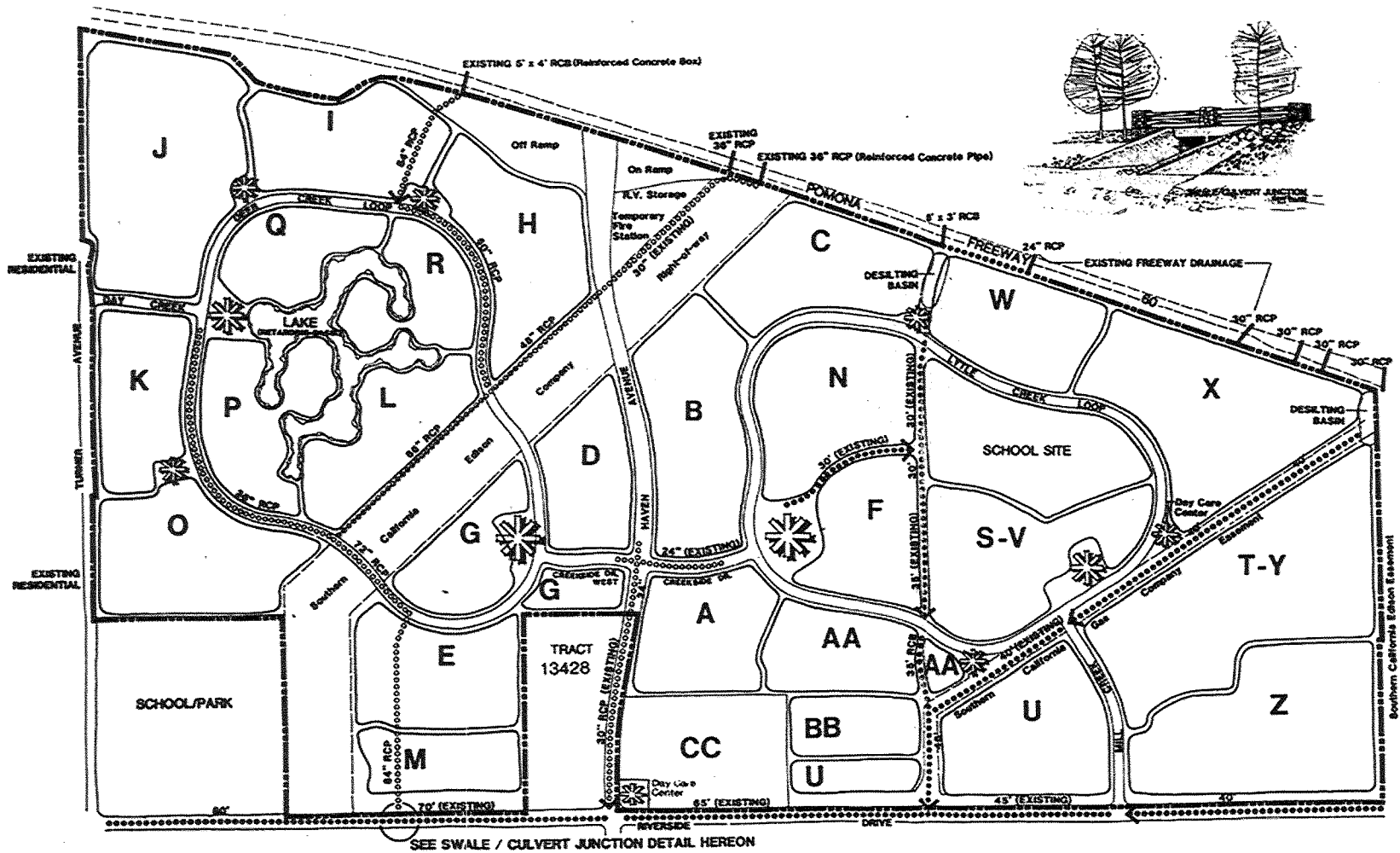


**ELEVATION/SECTION b**

NOTES: 1. THE ARCHITECTURAL CHARACTER OF ALL SFA PATIO WALLS AND FENCES WILL REFLECT THE ARCHITECTURE WITHIN THE DEVELOPMENT AREAS

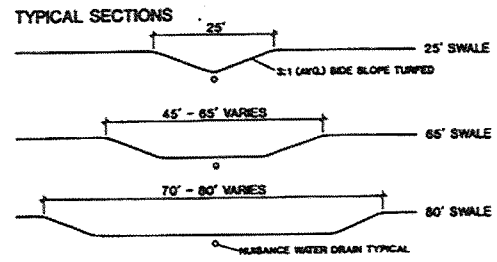
2. ALL FENCING DETAILS, INCLUDING MATERIALS AND FINISHES WILL BE SUBMITTED AS SHOP DRAWINGS TO THE MASTER BUILDER FOR APPROVAL

3. FOR LAKE EDGE PLANT MATERIAL PALETTE SEE CONCEPTUAL LANDSCAPE PLAN

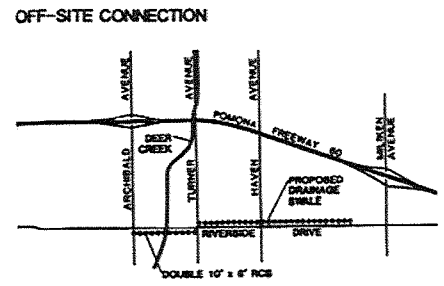


SEE SWALE / CULVERT JUNCTION DETAIL HEREON

- ..... DRAINAGE SWALE
- 24' DRAINAGE SWALE TOP WIDTH
- ..... DRAINAGE PIPE
- 18" PIPE SIZE

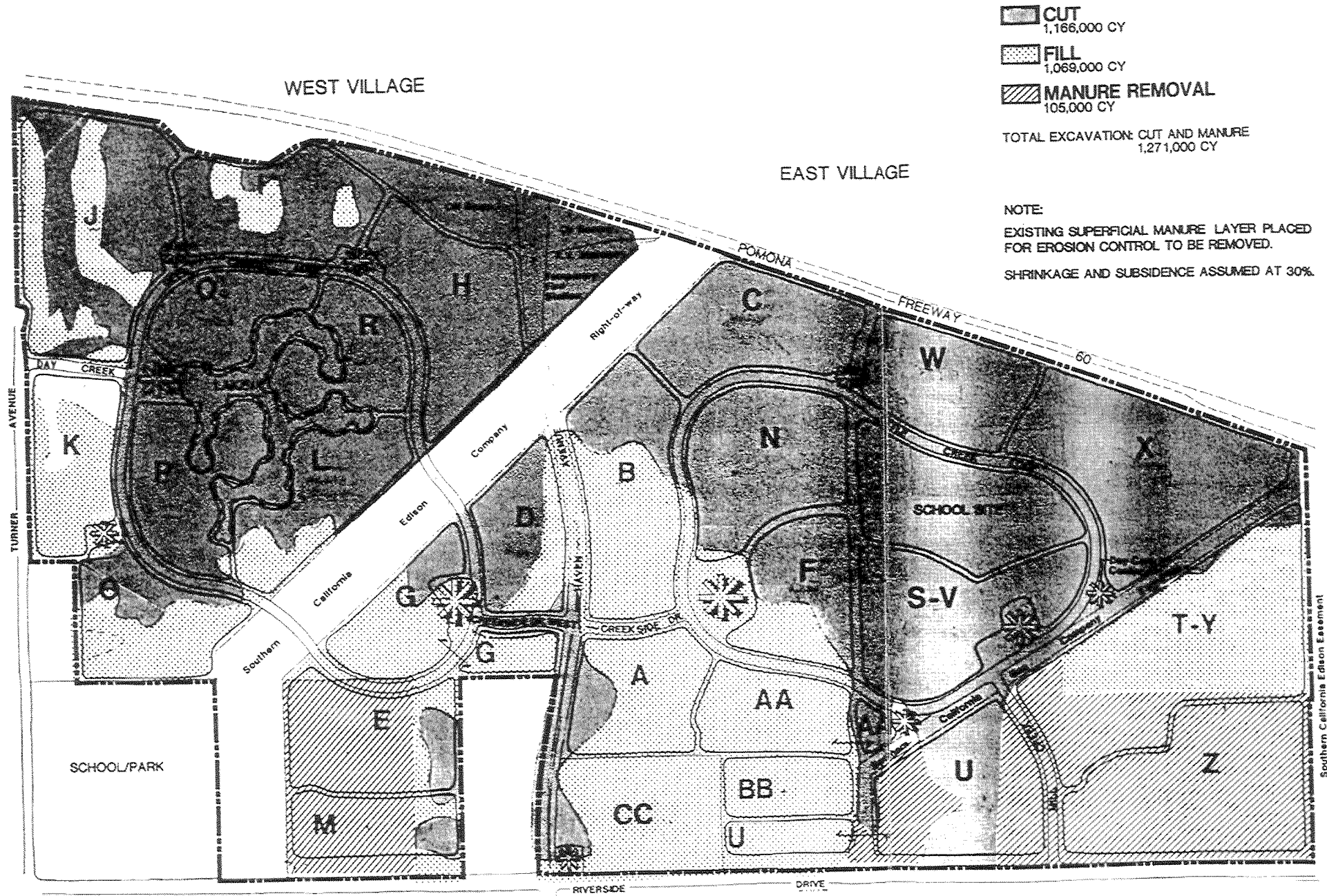


FUTURE CONSTRUCTION IS CONCEPTUAL

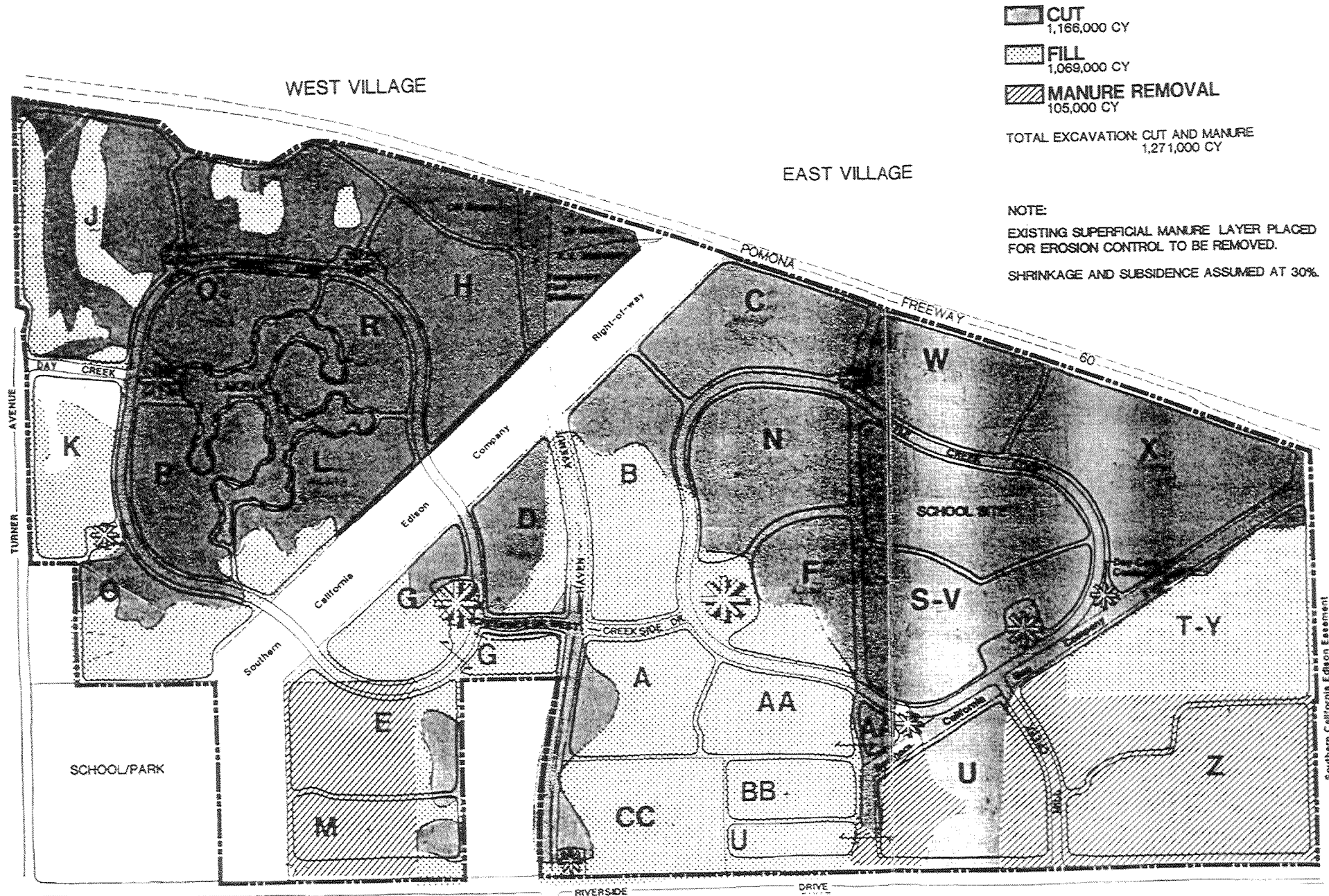


# INFRA-STRUCTURE STORM DRAINS

Exhibit 22

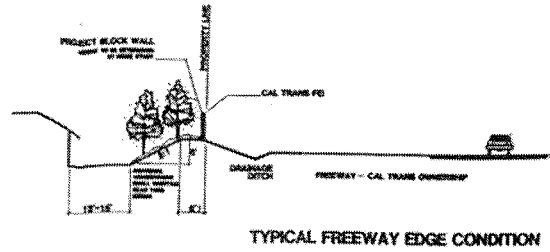


**GRADING**  
Exhibit 25



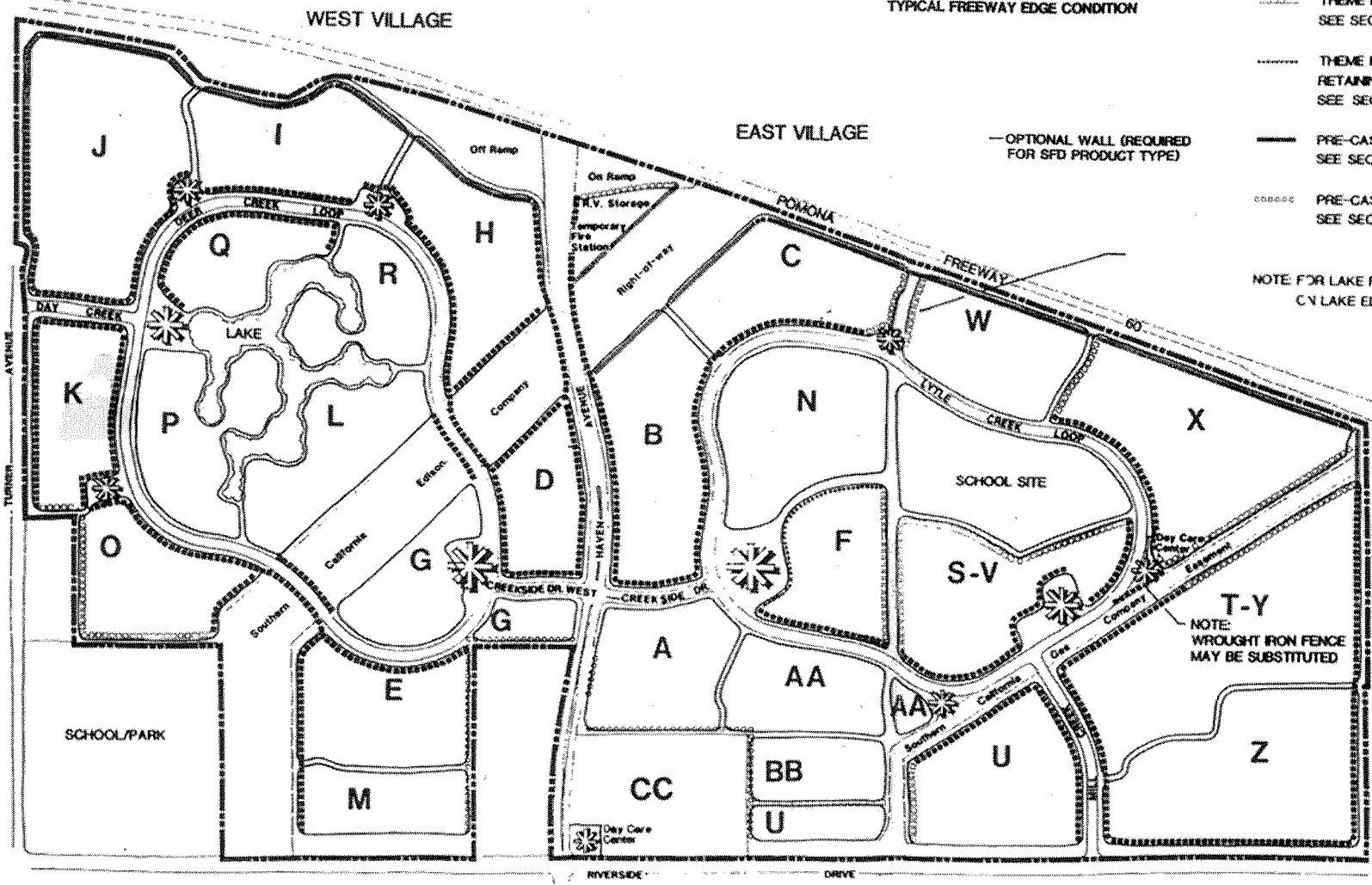
**GRADING**  
 Exhibit 25

Southern California Edison Easement



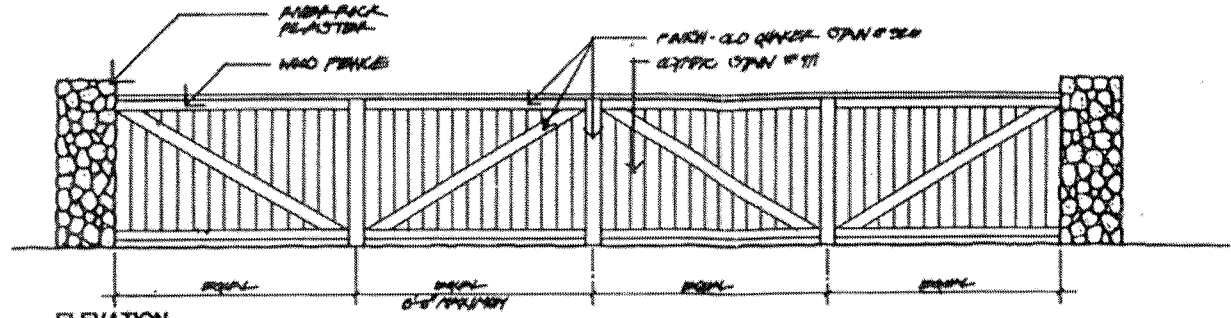
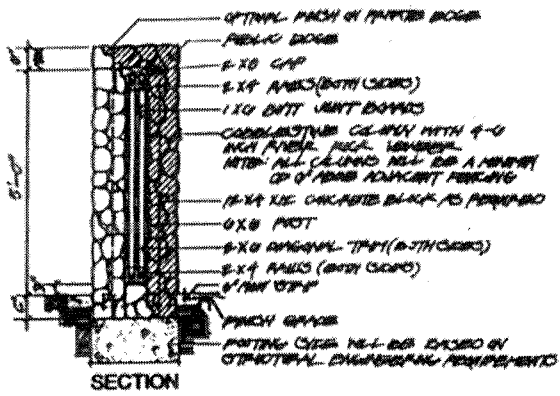
- THEME FENCE WITH PILASTER  
SEE SECTION A, EXHIBIT 27
- THEME FENCE WITH PILASTER WITH RETAINING FOOTING  
SEE SECTION E, EXHIBIT 28
- THEME FENCE WITHOUT PILASTER  
SEE SECTION B, EXHIBIT 27
- THEME FENCE WITHOUT PILASTER WITH RETAINING FOOTING  
SEE SECTION F, EXHIBIT 28
- OPTIONAL WALL (REQUIRED FOR SFD PRODUCT TYPE)
- PRE-CAST PANEL WALL  
SEE SECTION D, EXHIBIT 28
- PRE-CAST PANEL WALL  
SEE SECTION C, EXHIBIT 27

NOTE: FOR LAKE FENCING, SEE SECTIONS C.V. LAKE EDGE CONDITIONS (EXHIBITS 19 & 20).

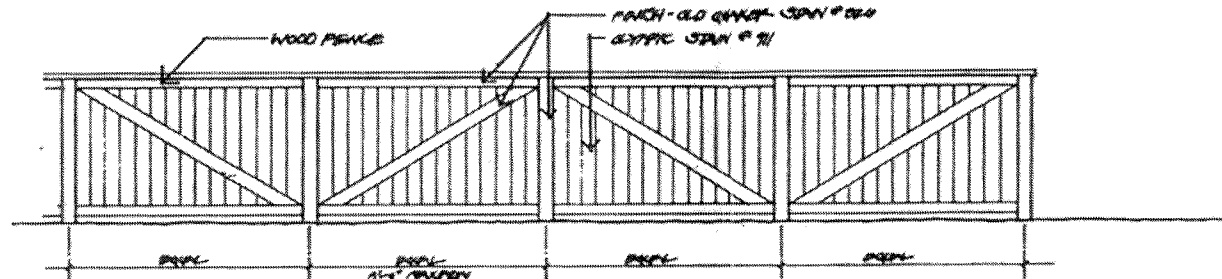
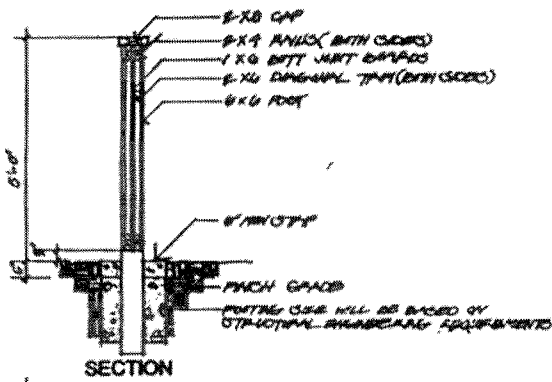


NOTE: WROUGHT IRON FENCE MAY BE SUBSTITUTED

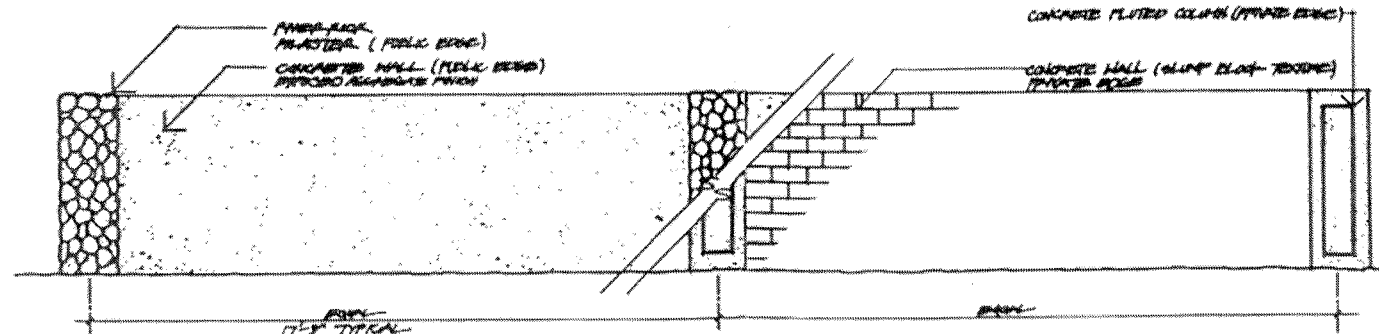
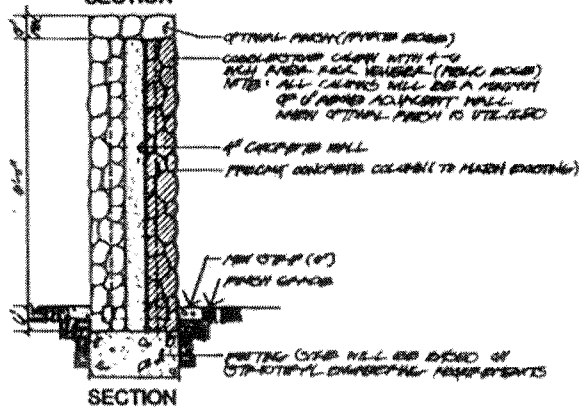
FENCING PLAN  
Exhibit 26



ELEVATION  
A. THEME FENCE WITH PILASTER



ELEVATION  
B. THEME FENCE WITHOUT PILASTER

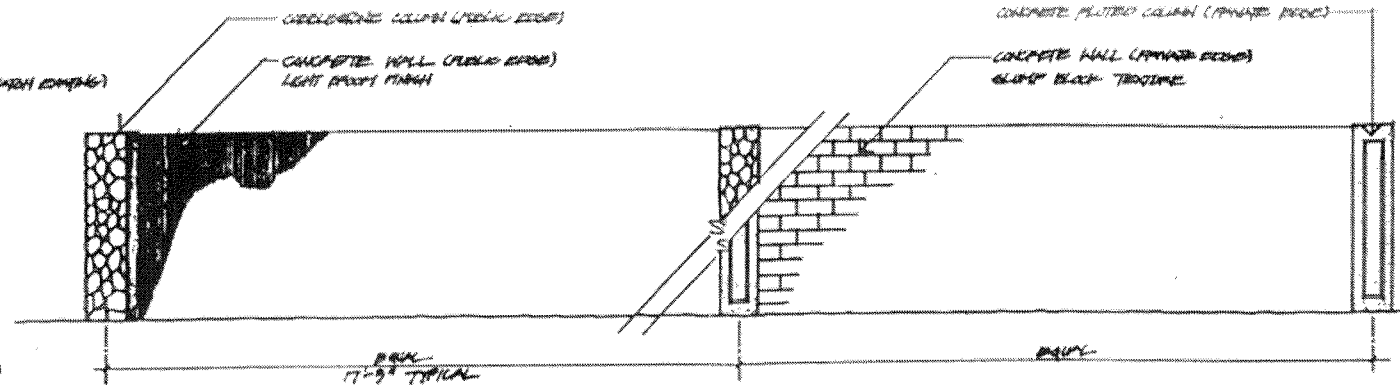
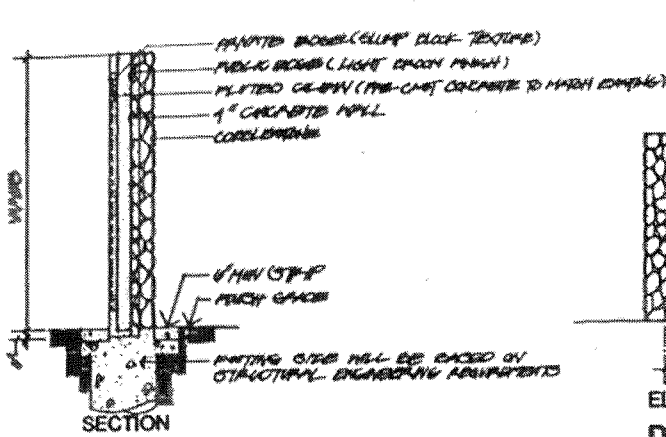


ELEVATION  
C. PRE-CAST PANEL WALL

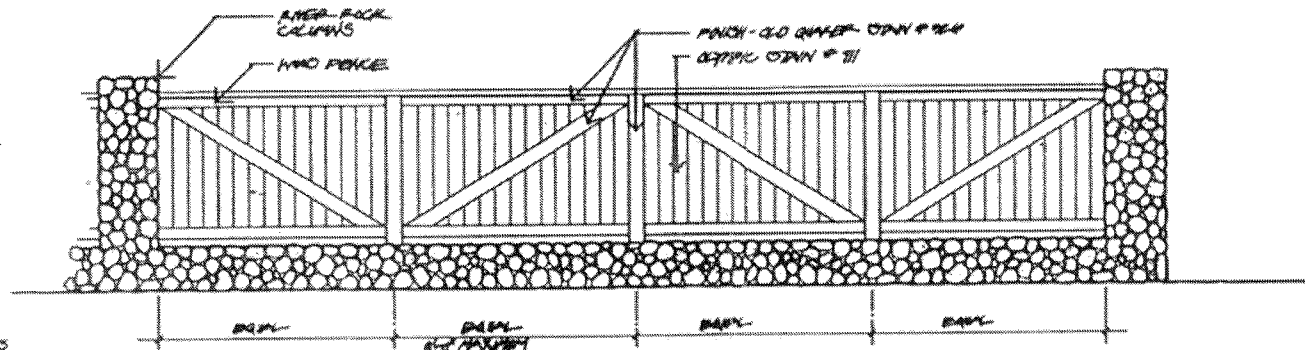
NOTE : 1. ALL FENCES AND WALLS WITH COBBLSTONE PLASTERS TO END WITH A FULL COBBLSTONE PILASTER ALL AROUND

2. ALL FENCE PLOTTING WILL BE SUBMITTED AS SHOP DRAWINGS TO THE MASTER BUILDER FOR APPROVAL.

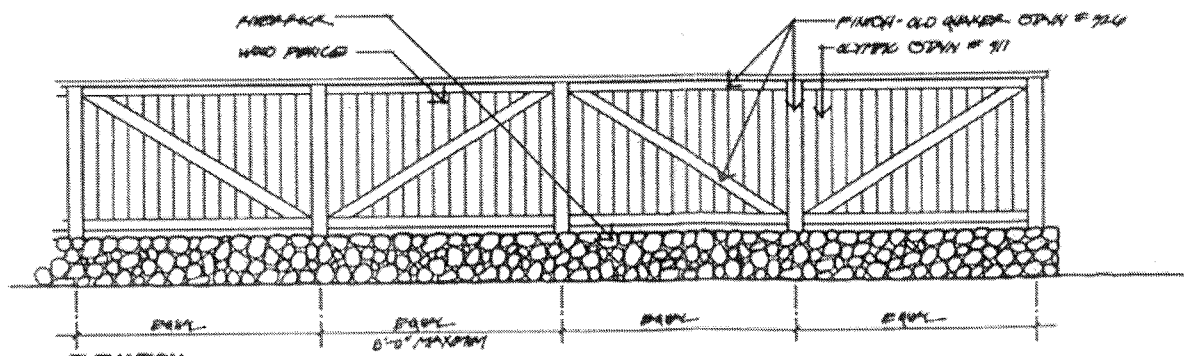




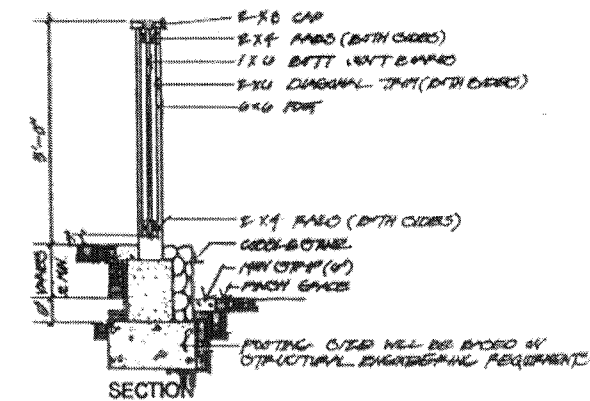
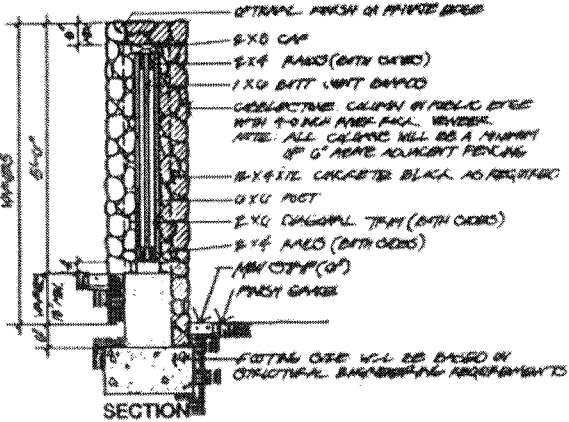
**D. PRE-CAST PANEL WALL**



**E. THEME FENCE WITH PILASTER WITH RETAINING FOOTING**

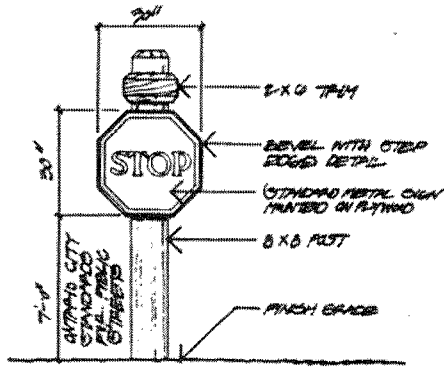


**F. THEME FENCE WITHOUT PILASTER WITH RETAINING FOOTING**

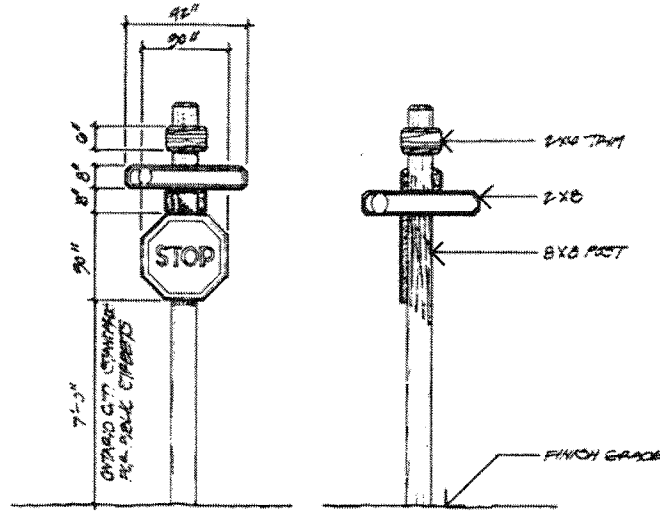


NOTE : 1. ALL FENCES AND WALLS WITH COBBLESTONE PILASTERS TO END WITH A FULL COBBLESTONE PILASTER ALL AROUND

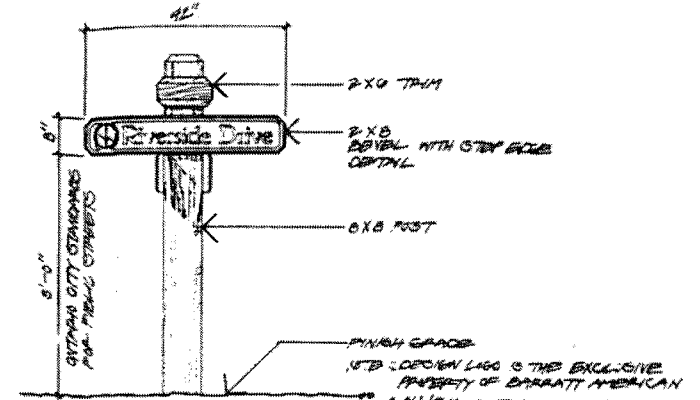
2. ALL FENCE PLOTTING WILL BE SUBMITTED AS SHOP DRAWINGS TO THE MASTER BUILDER FOR APPROVAL



TYPICAL STOP SIGN



TYPICAL STOP SIGN/STREET IDENTIFICATION SIGN



TYPICAL STREET IDENTIFICATION SIGN

### SIGNAGE

DESIGNED BY SERVICE PLUS  
 200 EAST DIXIE AVE. SUITE P  
 GAITHERSBURG, MD 20878-1602

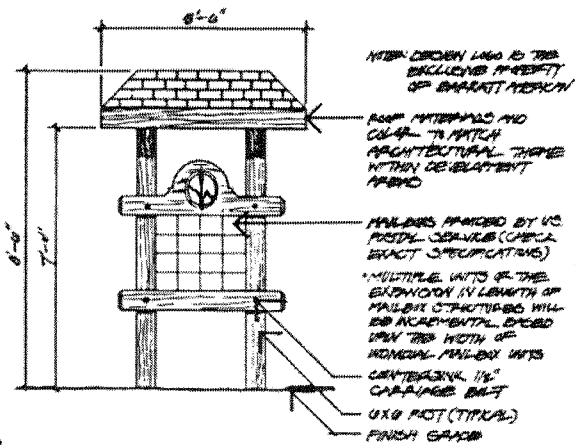
PAVED STREETS WILL BE IDENTIFIED  
 BY A GRAY TRUSS MARKED "PAVED"

NOTE : ALL SIGNAGE, MAILBOX, AND LIGHTING DETAILS,  
 INCLUDING MATERIALS AND FINISHES, WILL BE  
 SUBMITTED AS SHOP DRAWINGS TO THE  
 MASTER BUILDER FOR APPROVAL

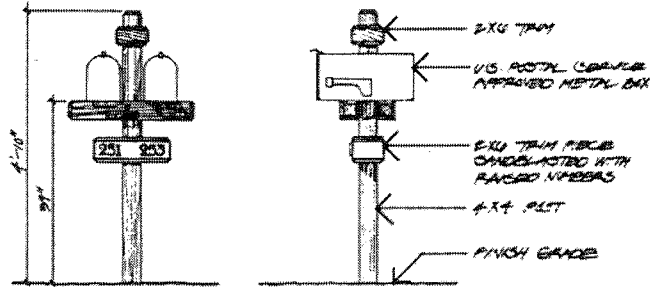
## SIGNAGE / MAILBOX / LIGHTING DETAILS

Exhibit 29

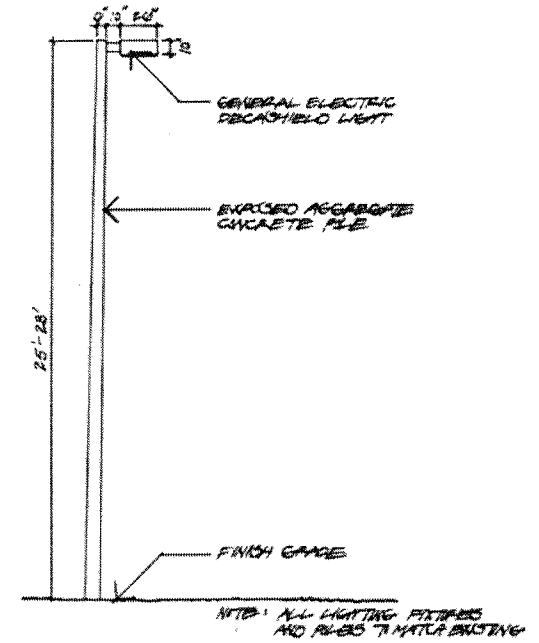
64 - b



TYPICAL SINGLE FAMILY ATTACHED RESIDENCE



TYPICAL SINGLE FAMILY DETACHED RESIDENCE



**LIGHTING**

(LOCATED ON PROJECT ENTRY PAVES, LOOP PAVES, AND ALL PUBLIC (S.P.O. PAVES) WITHIN BOST VILLAGES. LIGHTING WITHIN THE BOST VILLAGES SHALL BE PER CITY STANDARD: AVERAGE PILE PRODUCTS:  
 1-GI-25-PS OR EQUIVALENT  
 1-GI-25-PA OR EQUIVALENT  
 PER CITY STANDARD CITY LIGHTING

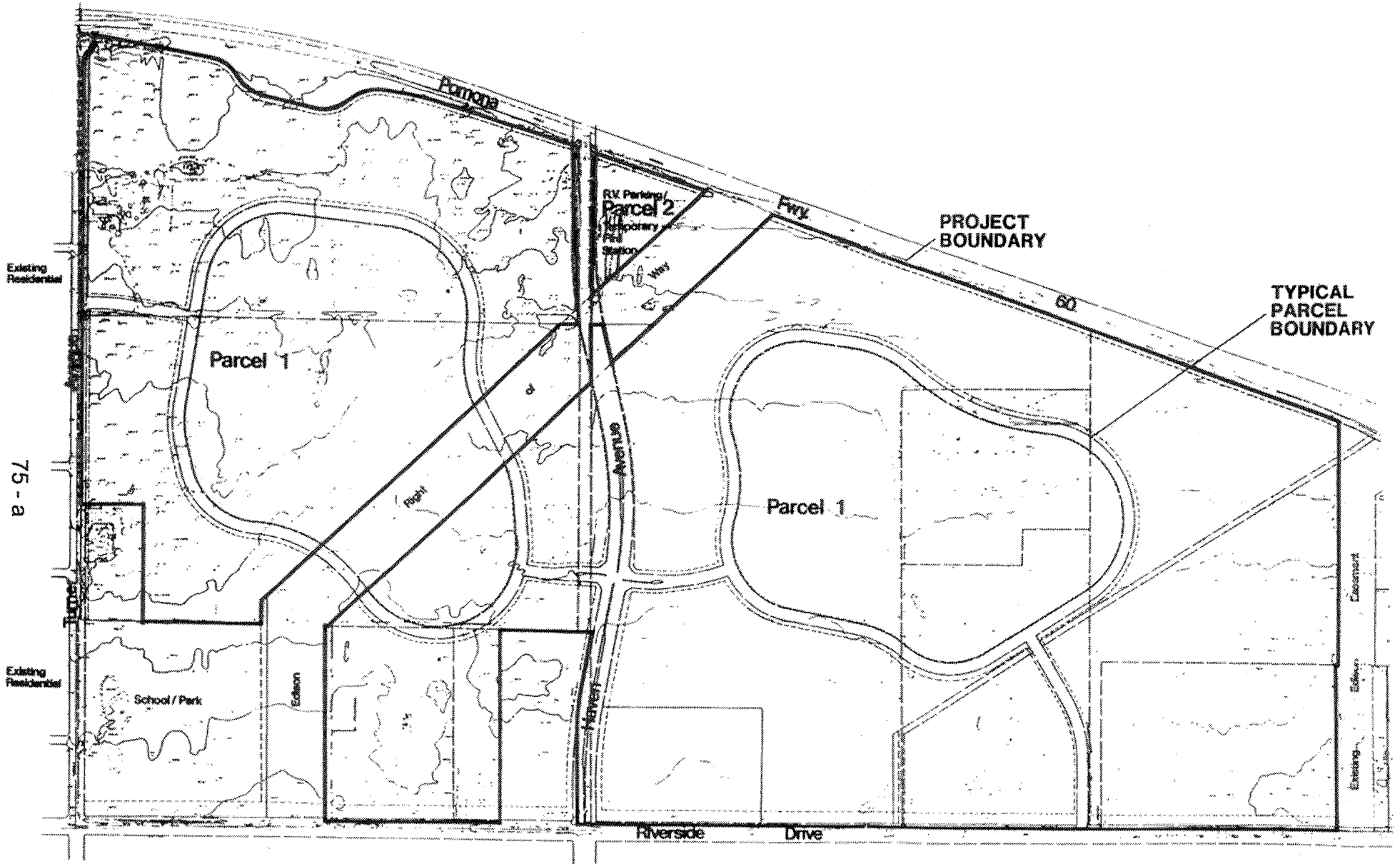
NOTE : ALL SIGNAGE, MAILBOX, AND LIGHTING DETAILS, INCLUDING MATERIALS AND FINISHES, WILL BE SUBMITTED AS SHOP DRAWINGS TO THE MASTER BUILDER FOR APPROVAL

**MAILBOX**

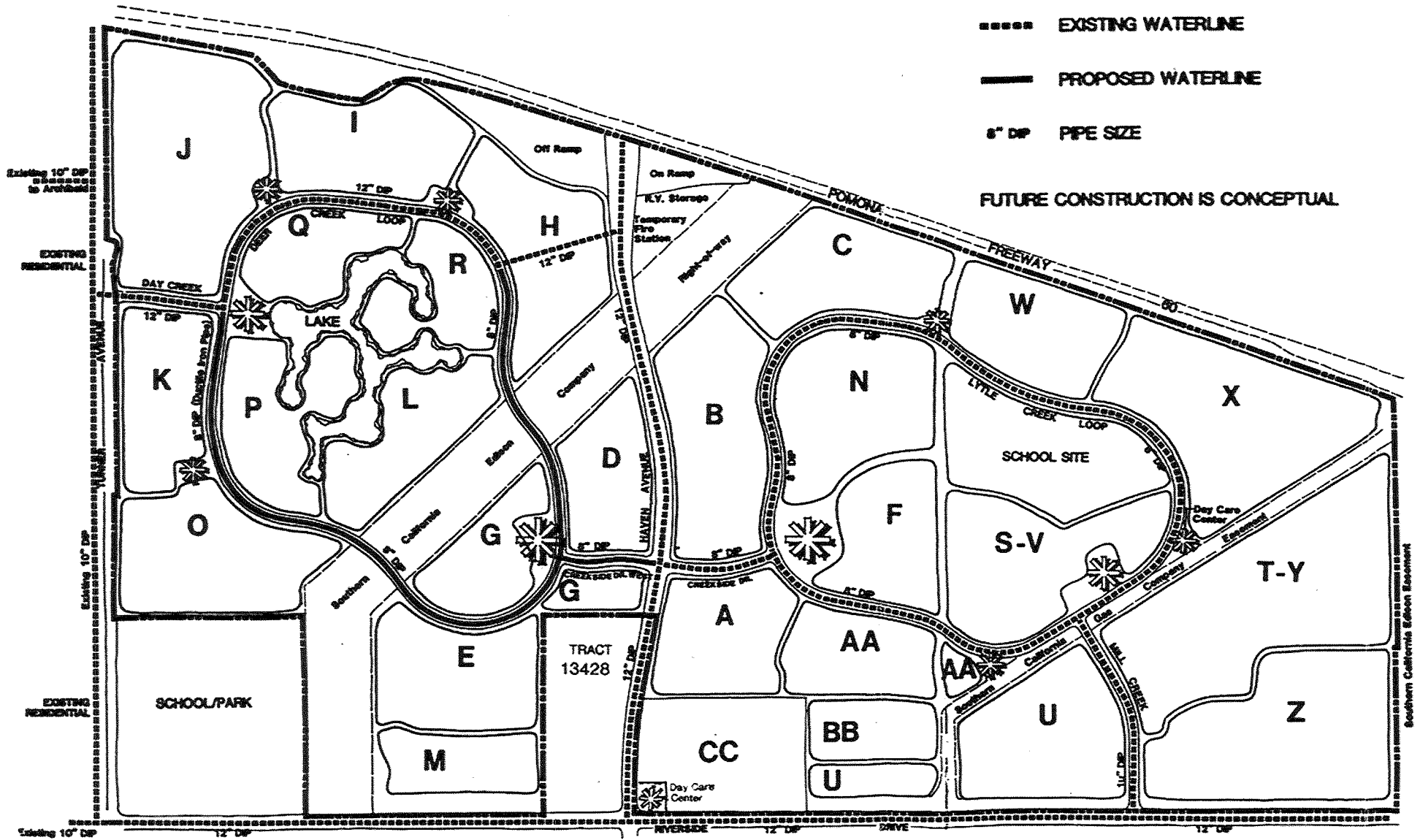
DESIGNED BY DDB CONSULTING  
 150 BOST DIVER ROAD, SUITE 0  
 SANTA ANA, CA 714/896 1663

**SIGNAGE / MAILBOX / LIGHTING DETAILS**

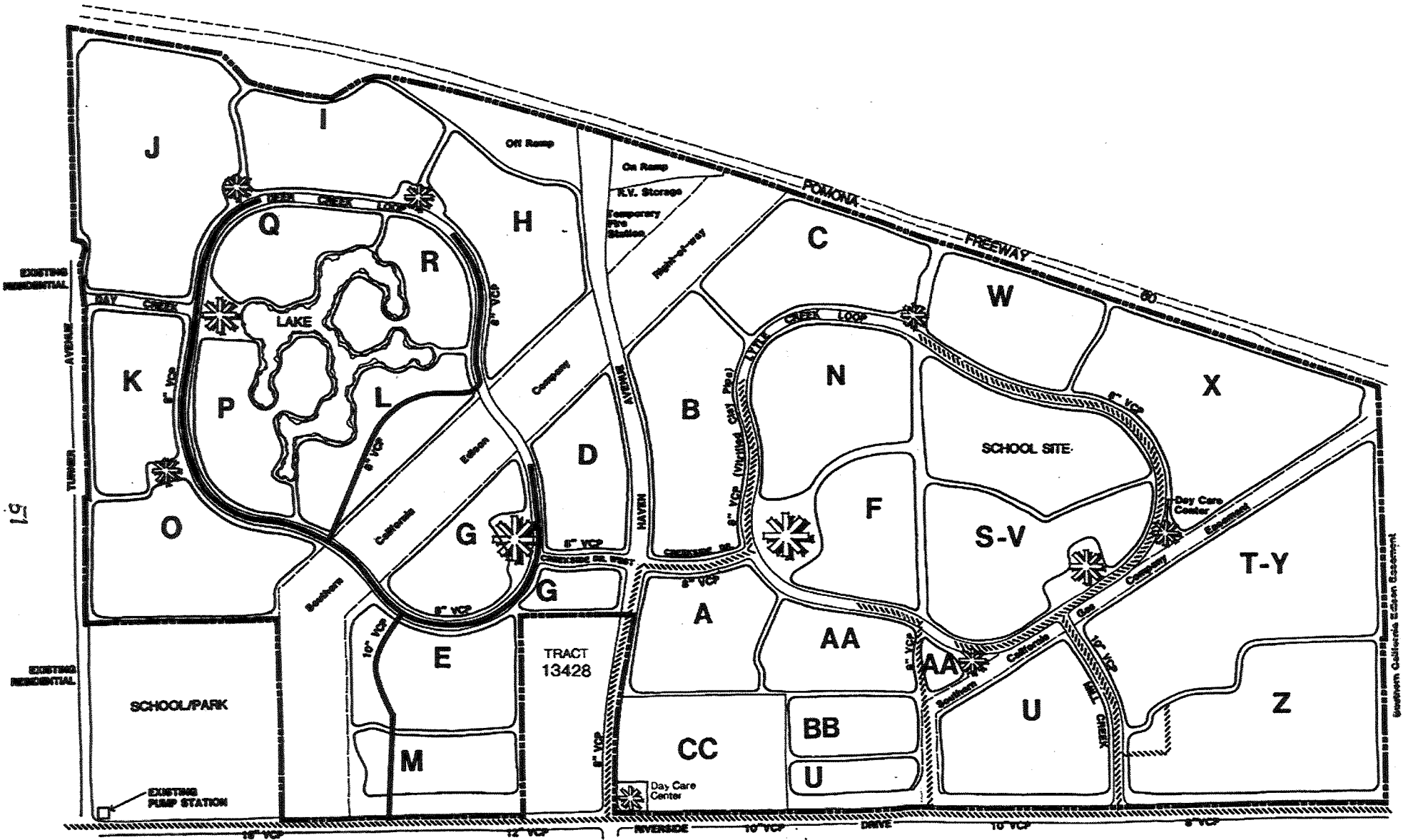
Exhibit 29



**LEGAL DESCRIPTION**  
 Exhibit 31 (Continued on next page)

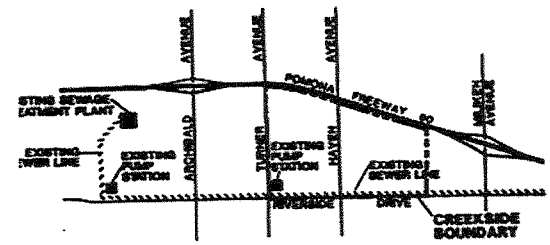


**INFRA-STRUCTURE WATER**  
Exhibit 23



**INFRA-STRUCTURE SEWER**  
Exhibit 24

- PROPOSED SEWER LINE
- - - - - EXISTING SEWER LINE
- 8" VCP PIPE SIZE
- PROPOSED PUMP STATION
- FUTURE CONSTRUCTION IS CONCEPTUAL
- OFF-SITE CONNECTION



## GRADING

The project is essentially flat with an estimated 1 percent crossfall from the Pomona Freeway to Riverside Drive. In an effort to provide added visual appeal and also promote gravity flow in the sewer and stormdrain systems, the grading concept proposes a recontouring of existing terrain to form gently undulated landforms including berms and drainage swales.

Sufficient earth material exists onsite to nearly balance cut and fill activities. The site may require some movement of earth between the East and West Villages. To further ensure this objective, various borrow and stockpile sites will be developed and maintained within the project site throughout the phasing schedule. The proposed lake, when constructed, will provide a source of earth for fill in adjacent development areas. Additionally, grading for the project is predicated on implementing streets and roads at slopes less than 0.5 percent shall require the approval of the city Engineer prior to final design. All grading plans shall be reviewed by the Building Official and City Engineer.

Grading along the northern perimeter of the site (adjacent to the Pomona Freeway) will be accomplished in such a manner as to provide noise attenuation berms along the freeway right-of-way. In addition, grade differentials will be constructed to assist in mitigation of freeway noise.

Dust control measures will be instituted on all onsite grading operations in accordance with adopted city standards and guidelines.

See EXHIBIT 25

WEST VILLAGE

EAST VILLAGE

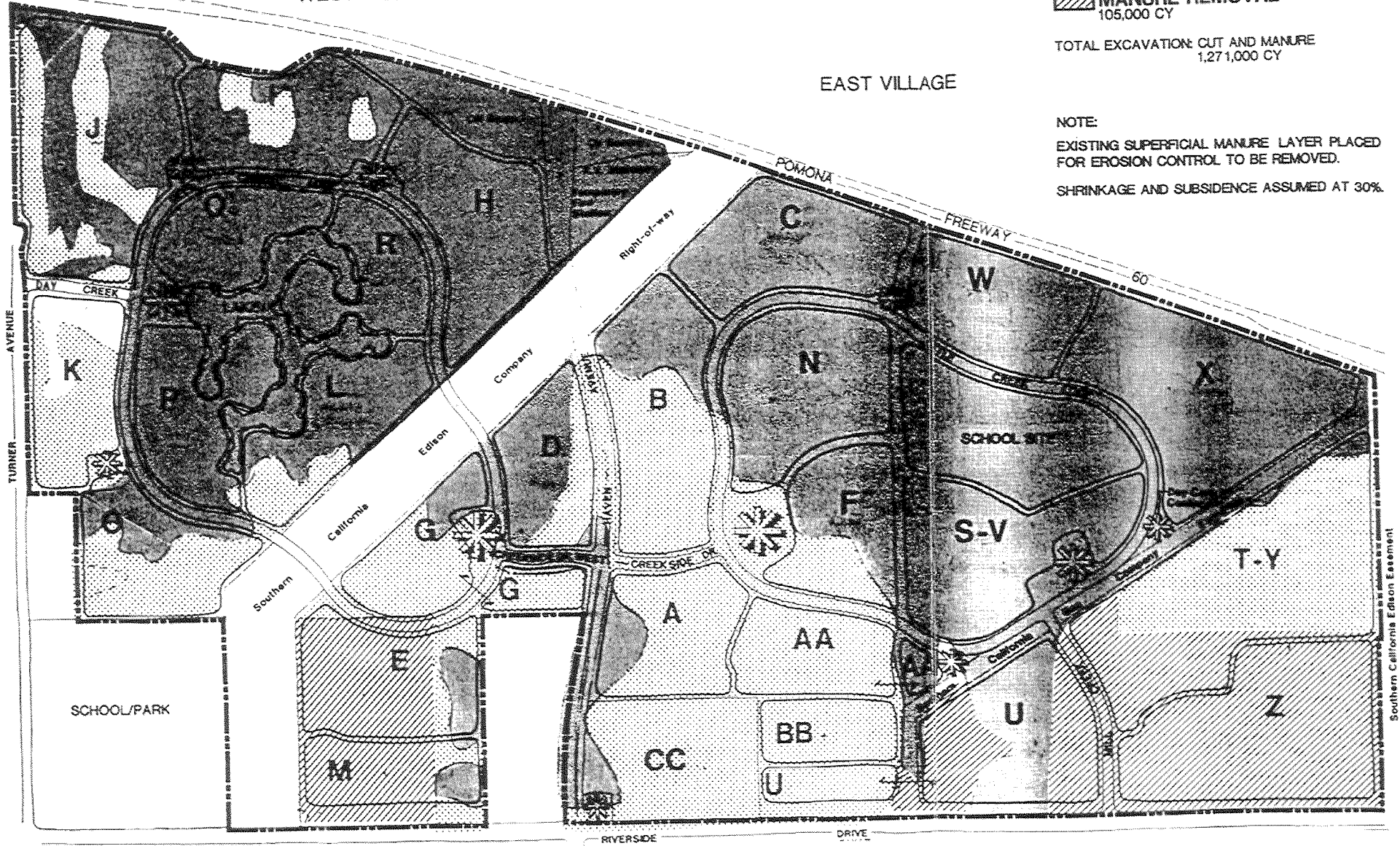
CUT  
1,166,000 CY

FILL  
1,069,000 CY

MANURE REMOVAL  
105,000 CY

TOTAL EXCAVATION: CUT AND MANURE  
1,271,000 CY

NOTE:  
EXISTING SUPERFICIAL MANURE LAYER PLACED  
FOR EROSION CONTROL TO BE REMOVED.  
SHRINKAGE AND SUBSIDENCE ASSUMED AT 30%.



GRADING  
Exhibit 25