



5.1 COMMUNITY DESIGN CONCEPT

The Borba Village Design Guidelines have been prepared to ensure that the traditional neighborhood character envisioned for the planned residential community is implemented. The Specific Plan Design Guidelines are intended to provide criteria for the design of various land uses and facilities within Borba Village in order to further the goal of achieving a traditional residential

community adjacent to a complementary commercial center. The design guidelines address the goals of creating a livable, cohesive community enabling and promoting neighborhood interaction.

The sketches and graphic representations contained herein are for conceptual purposes only and are to be used as general visual aids in understanding the basic intent of the guidelines. They are not meant to depict any actual lot or building design. In an effort to encourage creativity and innovation, the guidelines express “intent” rather than “absolute”, thereby allowing certain flexibility in fulfilling the intended design goals and objectives.

The Community Design Concept is directed towards the creation of a traditional community which is internally integrated and which relates positively to the existing surrounding community. The key elements of the community development plan for Borba Village include:

- Pedestrian accessibility and mobility throughout the entire residential community.
- Strong sense of community.
- Strong visual and physical connection of land use components with open space amenities.
- Unifying elements which define the various land use components as one cohesive community.

The community design guidelines focus on the integration of these elements into the Borba Village community. Together, the appropriate treatment of these elements will establish a distinctive and cohesive image consistent throughout the planned community.

5.2 STREETSAPES

5.2.1 Euclid Avenue

Euclid Avenue is a major north/south arterial in the City of Ontario bordering the project site on the east. Euclid Avenue traditionally has been developed with a mix of residential and commercial land uses adjacent to the street and, in many instances, uses adjacent to Euclid Avenue relate to the street with residential units fronting on the street. Within Borba Village, single family residential, multifamily residential and commercial service uses will abut Euclid Avenue. The relationship of each land use to Euclid Avenue will necessitate variations in the streetscape along this community edge while maintaining a uniform image for Euclid Avenue.

Residential

The streetscape for Euclid Avenue adjacent to the single family residential component of Borba Village continues the heavily landscaped treatment existing along Euclid Avenue and maintains the open and street friendly relationship of residential uses to Euclid Avenue which exists elsewhere on the street. Streetscape treatment for Euclid Avenue includes a 10 foot landscaped parkway incorporating street trees and other elements compatible with existing planting and a 5 foot wide public sidewalk within the westerly right of way of Euclid Avenue. A raised, landscaped buffer will be provided with a public access point provided from Euclid Avenue to the residential street. The Euclid Avenue streetscape adjacent to single family residential uses is illustrated on Exhibit 16.

Commercial

Commercial frontage along Euclid Avenue will include a streetscape continuing the use of landscape materials compatible with that existing along Euclid Avenue. A 5 foot public sidewalk will be provided separated from the street by a 10 foot wide landscaped parkway. In order to minimize the visual impact of parking within the commercial area adjacent to Euclid Avenue an additional 20 foot landscaped setback will be provided between the public right of way and parking areas. The Euclid Avenue streetscape adjacent to commercial uses is illustrated in Exhibit 17.

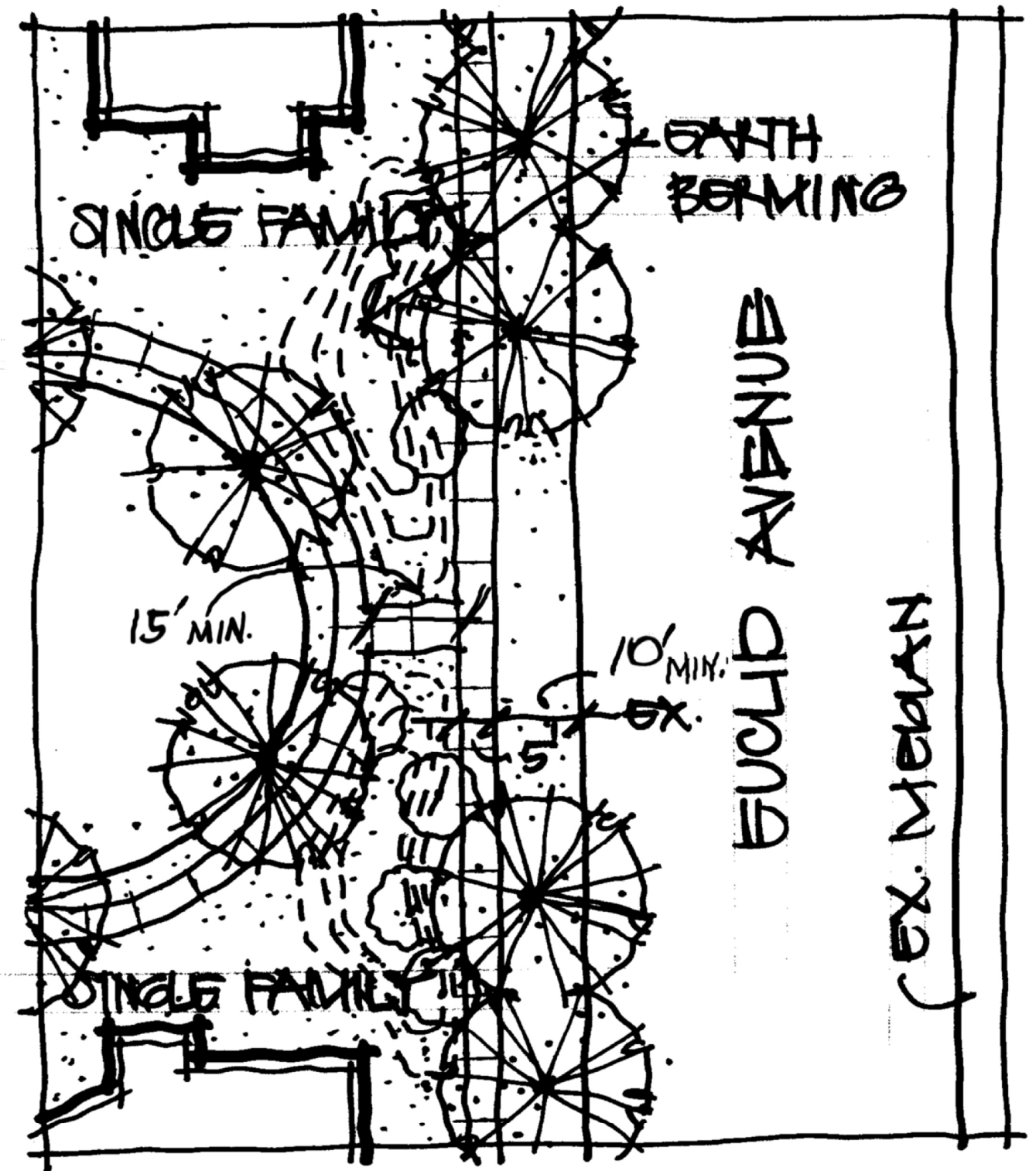
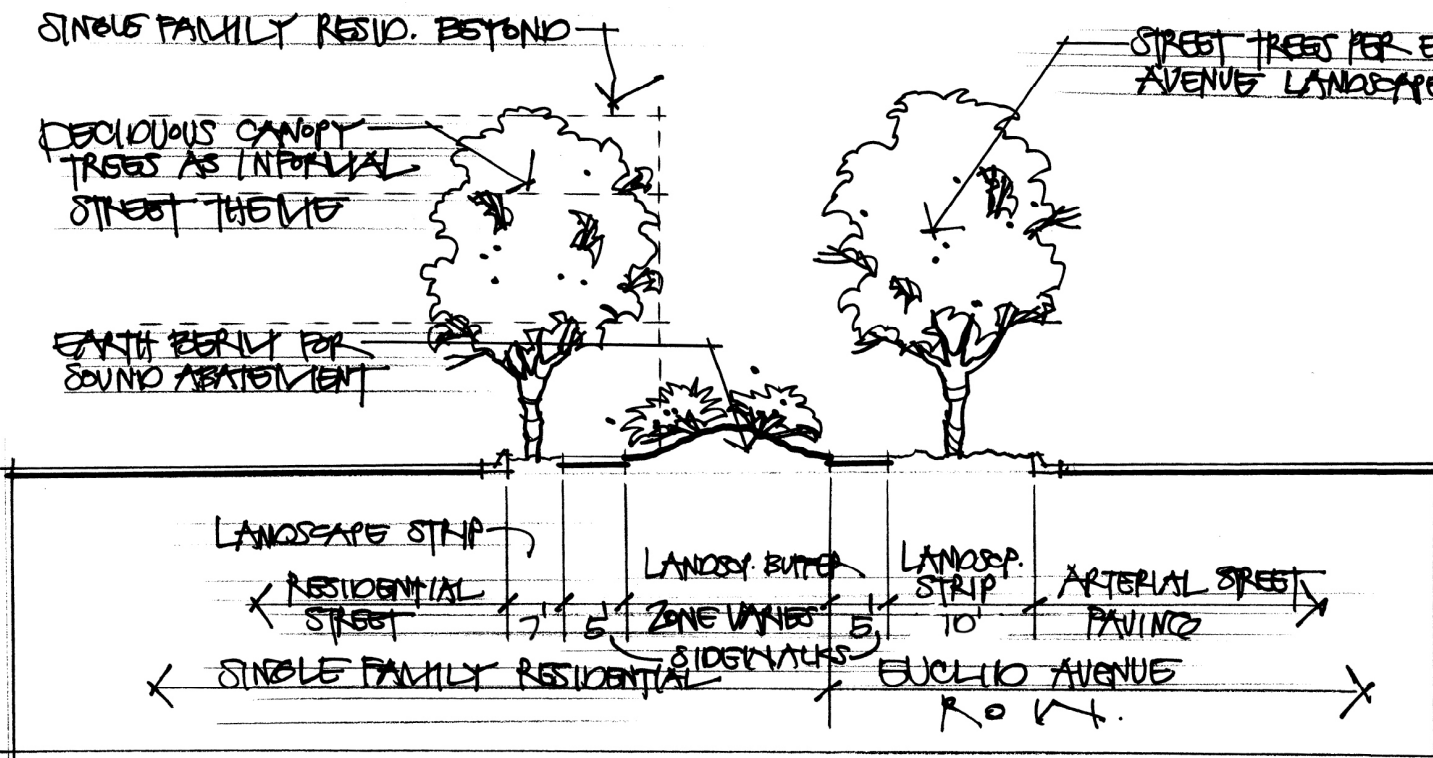
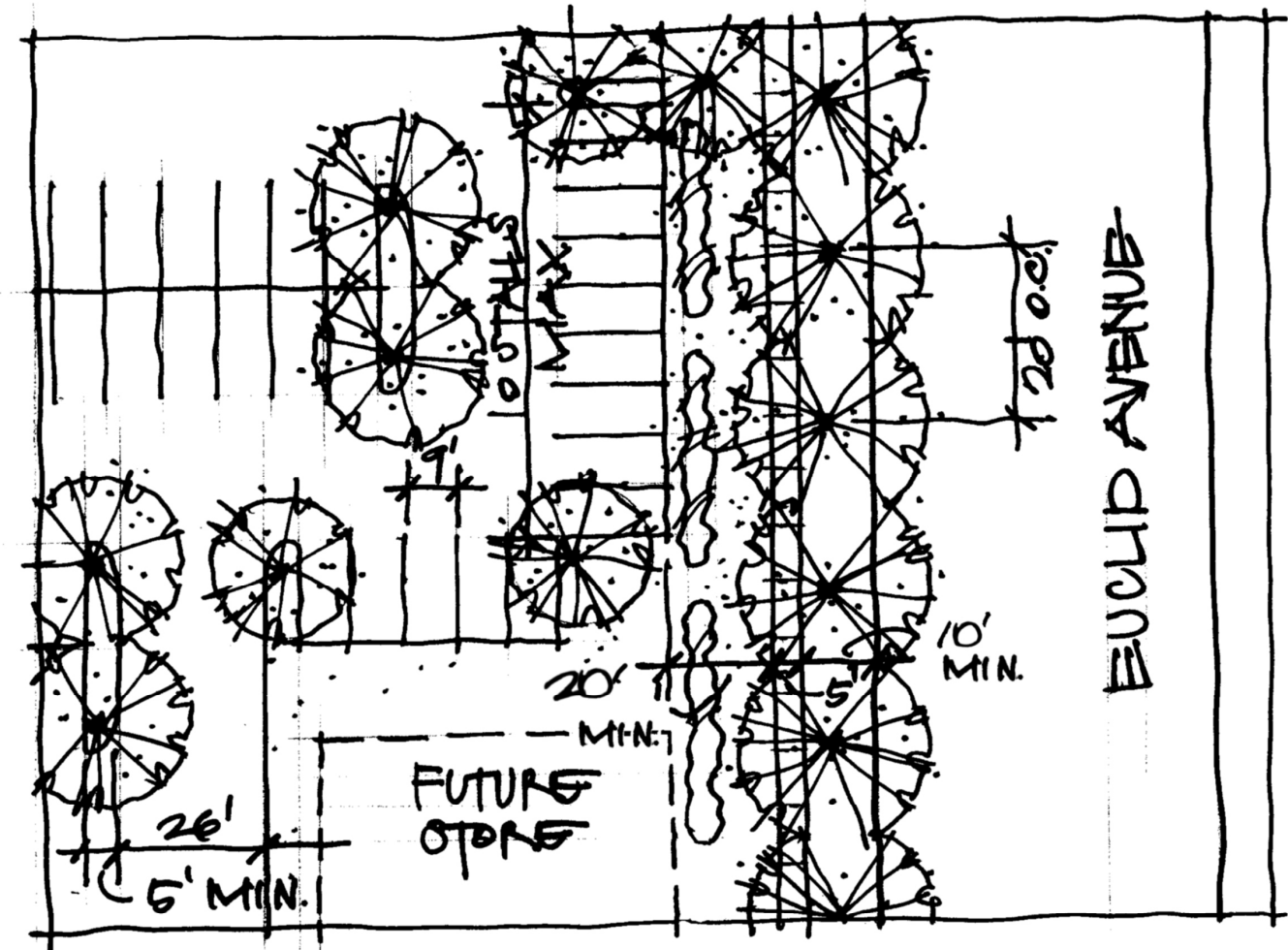
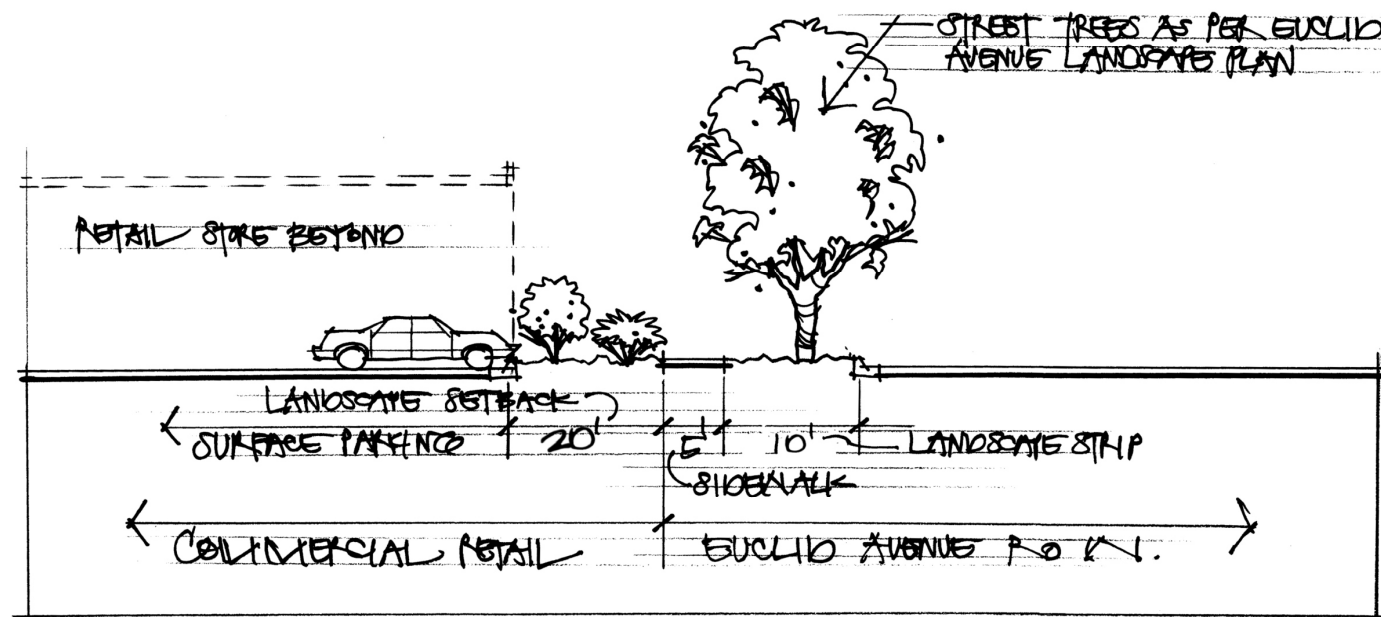


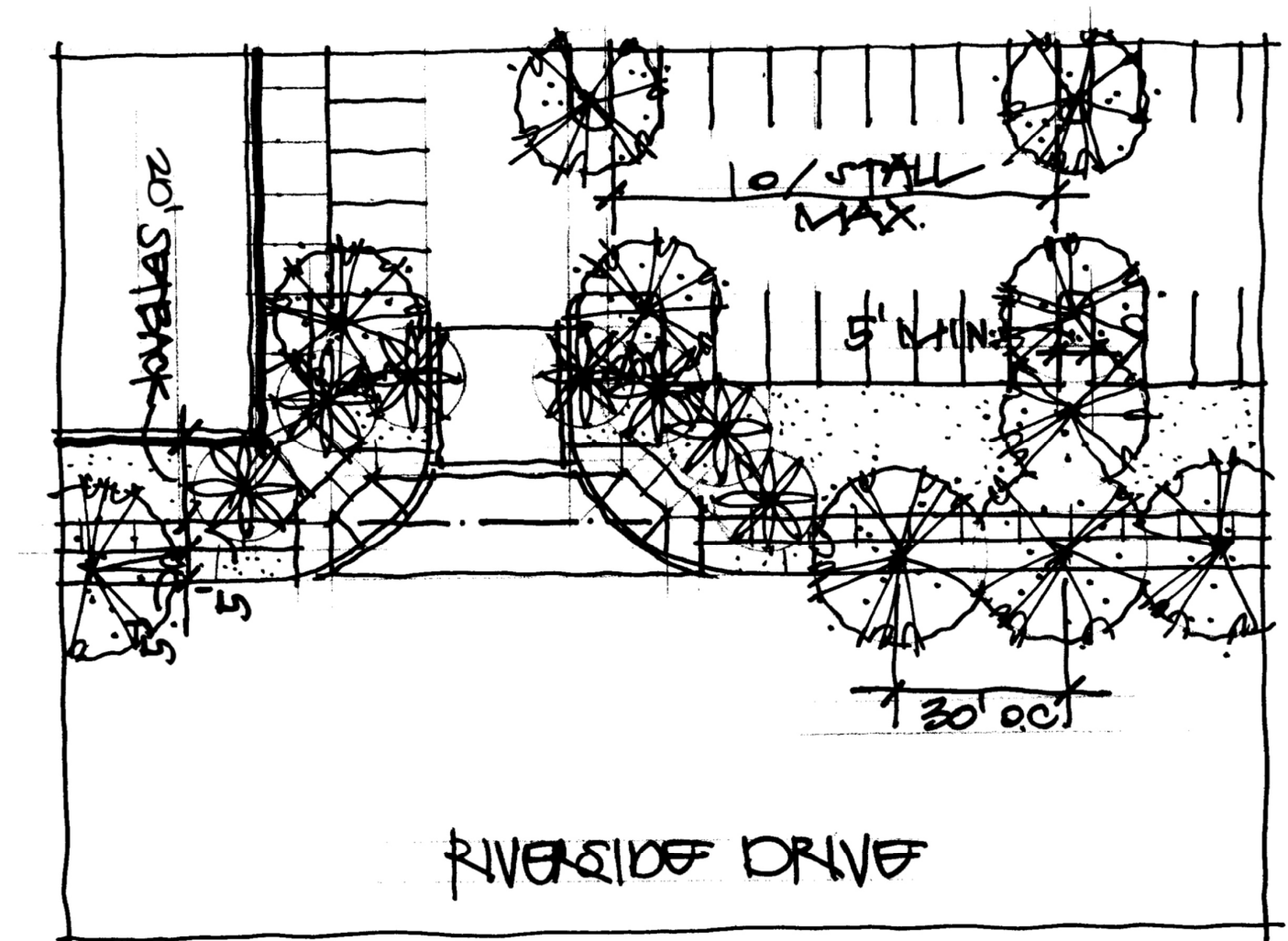
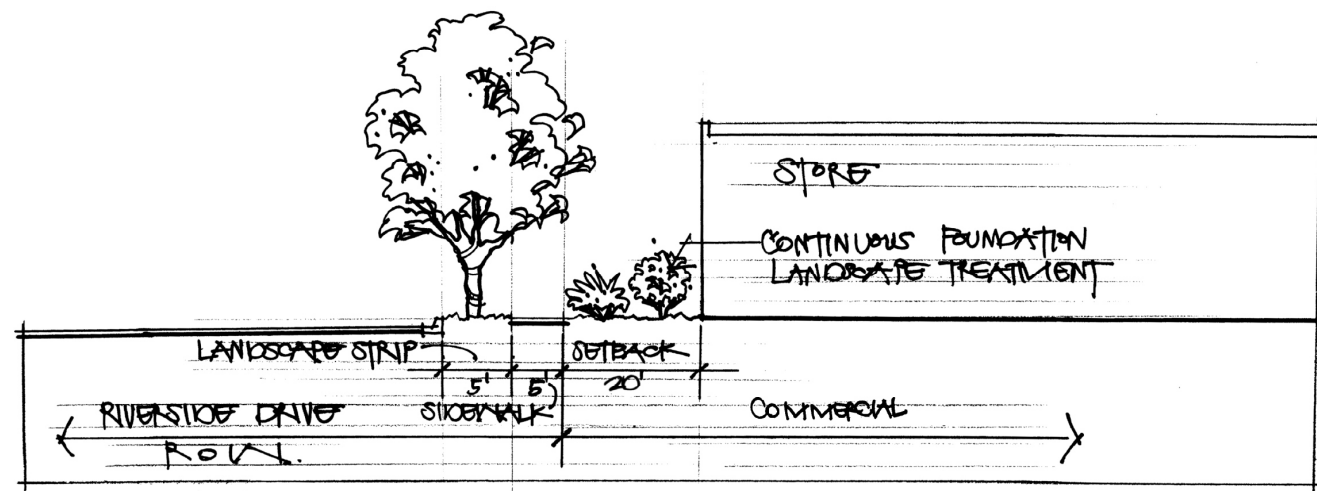
EXHIBIT 16

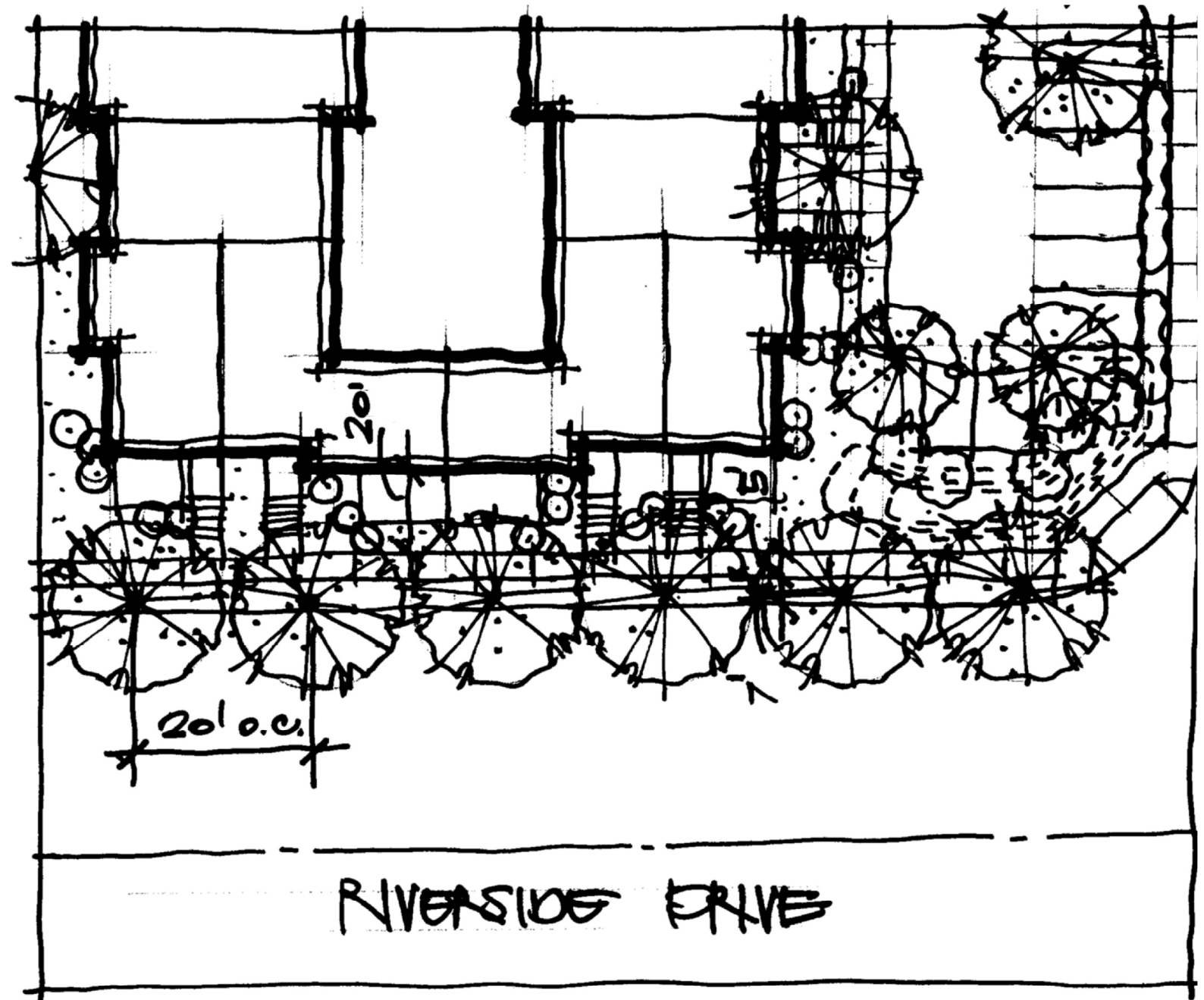
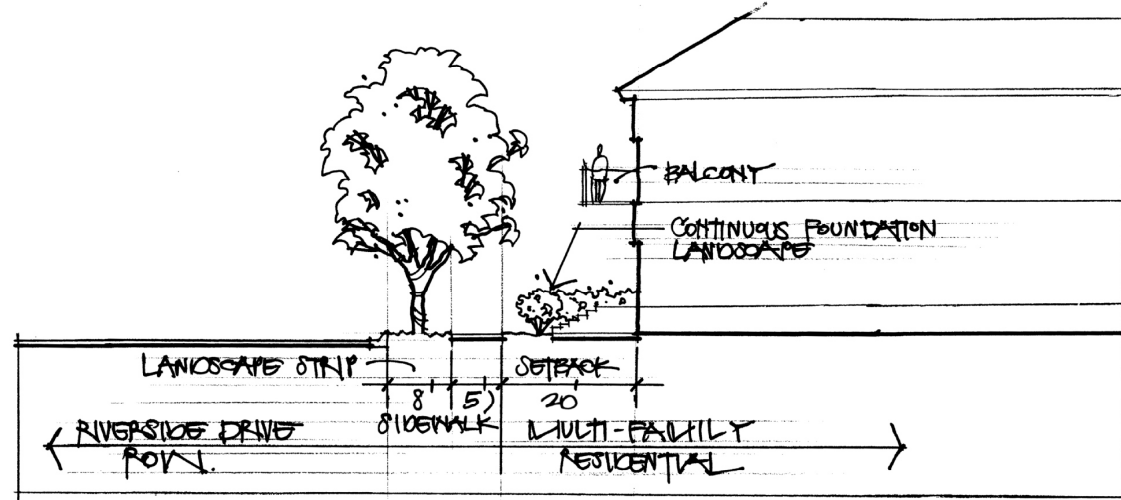
EUCLID AVENUE STREETScape ADJACENT TO RESIDENTIAL



5.2.2 Riverside Drive

Both residential multi-family and commercial land uses front on Riverside Drive, a major east/west arterial in the City of Ontario. Public right of way improvements to Riverside Drive adjacent to both residential and commercial service uses include a 5 foot wide sidewalk separated from the street by a 5 foot wide landscaped area. Buffering of land uses along Riverside Drive will include an additional 20 foot wide landscaped area between the public right of way and buildings. This buffer will consist of the use of a continuous “wall” of dense landscape materials to soften the visual perception of the street edge where buildings become visible. Within the residential area balconies may encroach into this setback area and porches at the ground level may be setback 10 feet to create a closer relationship of the residence to the street. The Riverside Drive streetscape plan is illustrated on Exhibits 18 and 19.





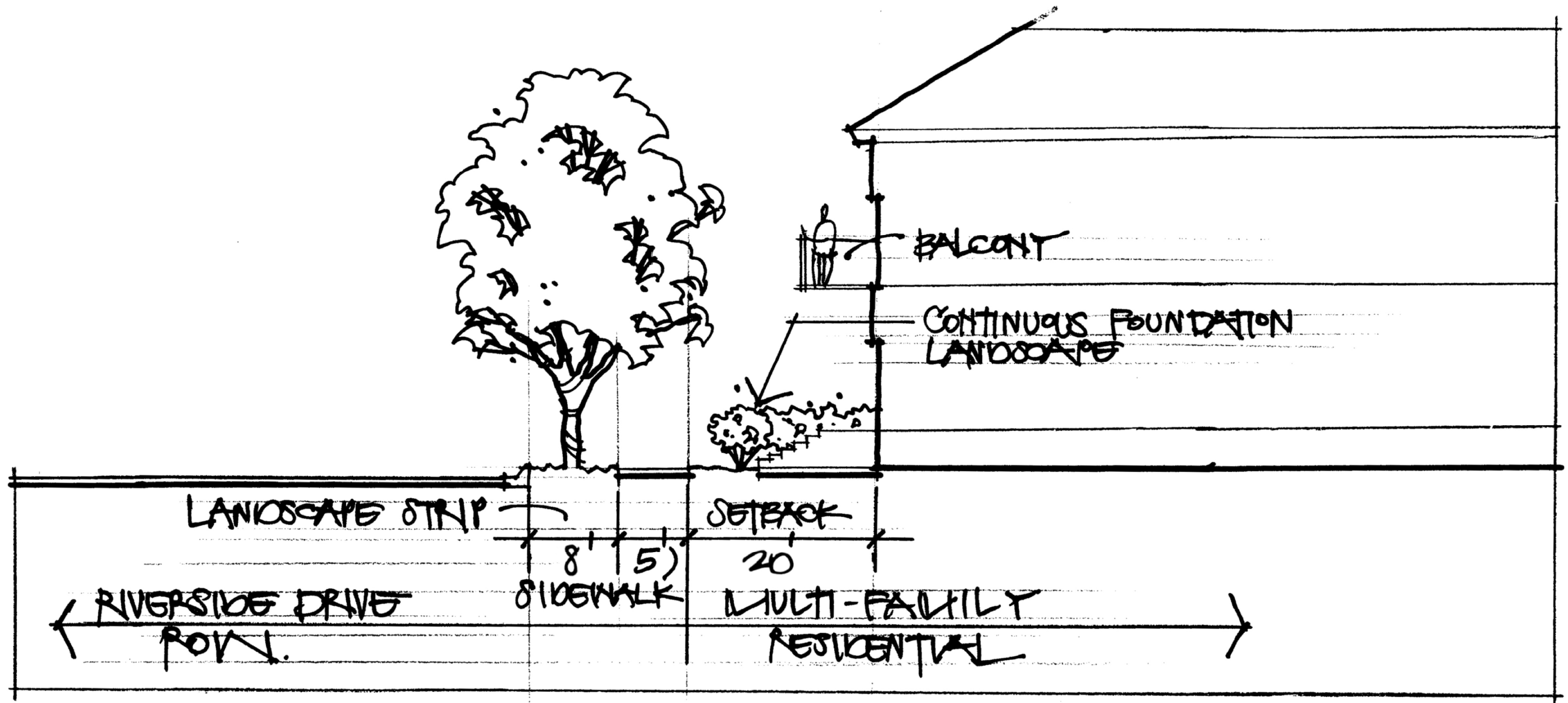
5.2.3 Fern Avenue

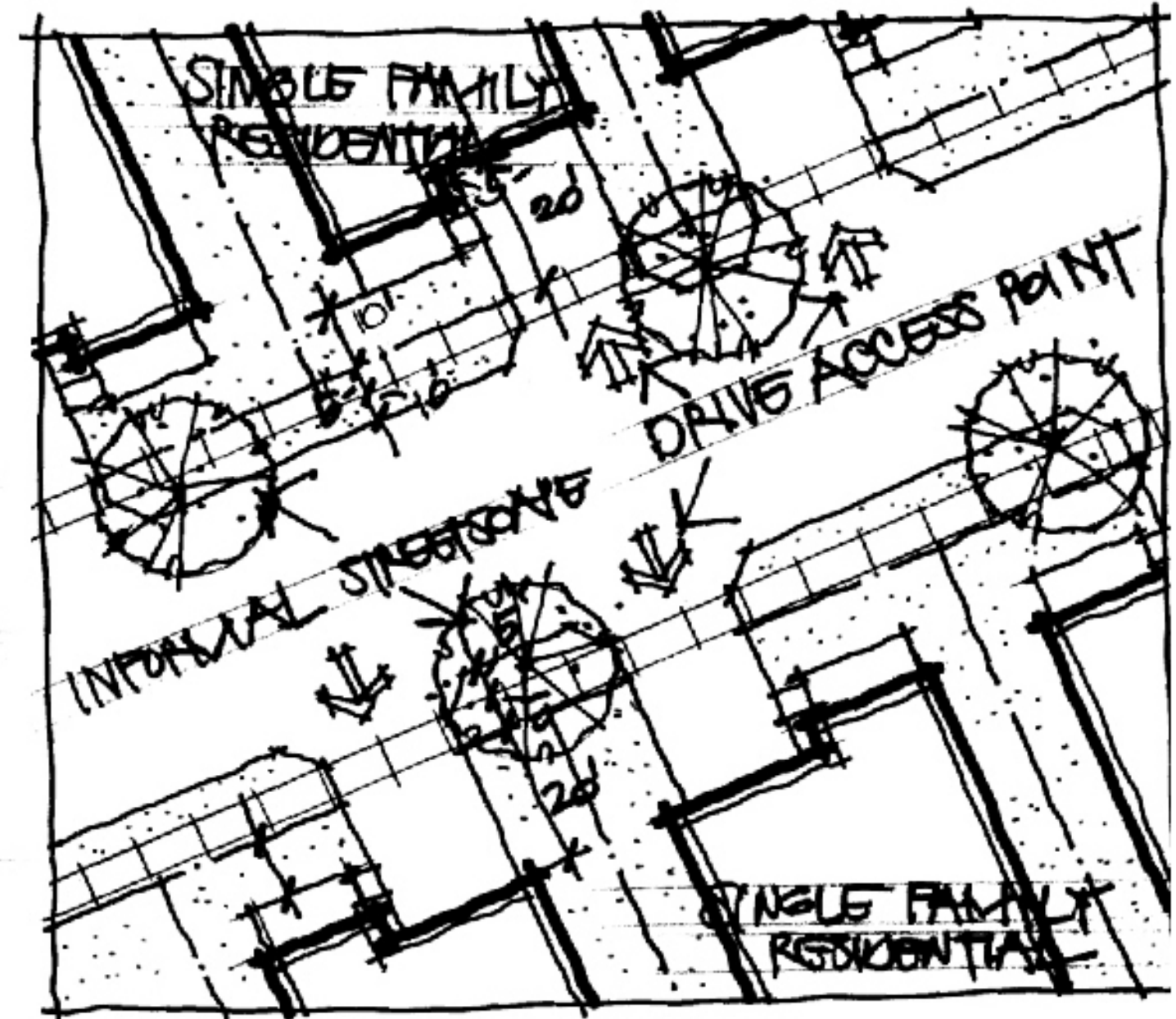
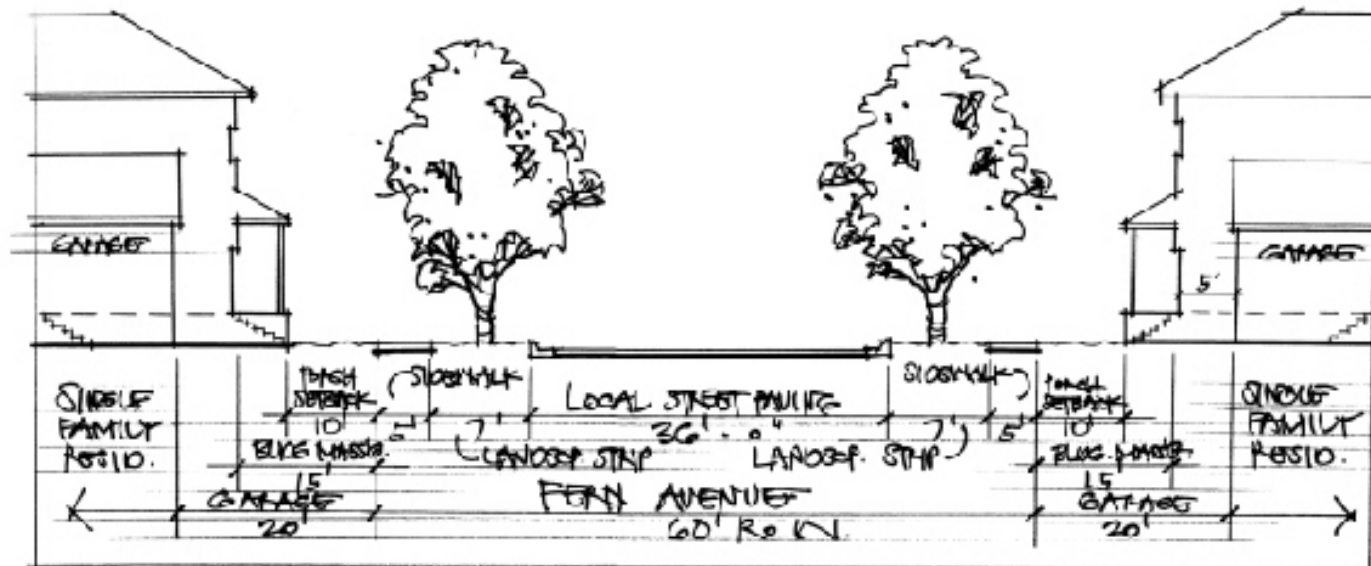
Fern Avenue, an existing north/south residential collector street will provide access to the multifamily residential area and is planned to extend in a northeasterly direction to serve as the local street serving the single family residential area. Additional right of way improvements to the existing portion of Fern Avenue as part of the development of Borba Village include construction of a 5 foot sidewalk separated from the street by an 8 foot wide landscaped parkway.

Within multifamily areas, a 20 foot landscaped setback area will be provided from back of sidewalk to residential building face. Patios and balconies may encroach into this setback area and porches at the ground level may be setback 10 feet to create a closer relationship of the residence to the street. This portion of the Fern Avenue streetscape is illustrated in Exhibit 20.

5.2.4 Street "A"

Within the single family residential area a new street, Street "A" will be constructed with a total 60 foot wide right of way providing 36 feet of paved travel area, a 5 foot wide sidewalk separated from the street by a 7 foot landscaped parkway on each side of the street. Homes will be brought closer to the street by permitting porches to be setback 10 feet from the front property line and by requiring garages to be set back a minimum of 20 feet from the back of sidewalk with a minimum 5 foot separation between the garage and the front elevation of the main structure. Exhibit 21 illustrates the condition for proposed Street "A" within the project site.





5.3 ENTRIES

The design of entries to the Borba Village Specific Plan area is intended to reinforce the overall community identity through the use of thematic landscaping and signage.

5.3.1 Euclid Avenue at Entry to Single Family and Multifamily Residential area

A right in/right out only entry is planned for the single family and multifamily area from Euclid Avenue. The entry will consist of enhanced paving such as scored and/or colored concrete or specialty paving materials compatible with the architectural style and materials utilized on the home improvement building. Planting areas should be provided on the south side of the entry to accommodate thematic landscaping.

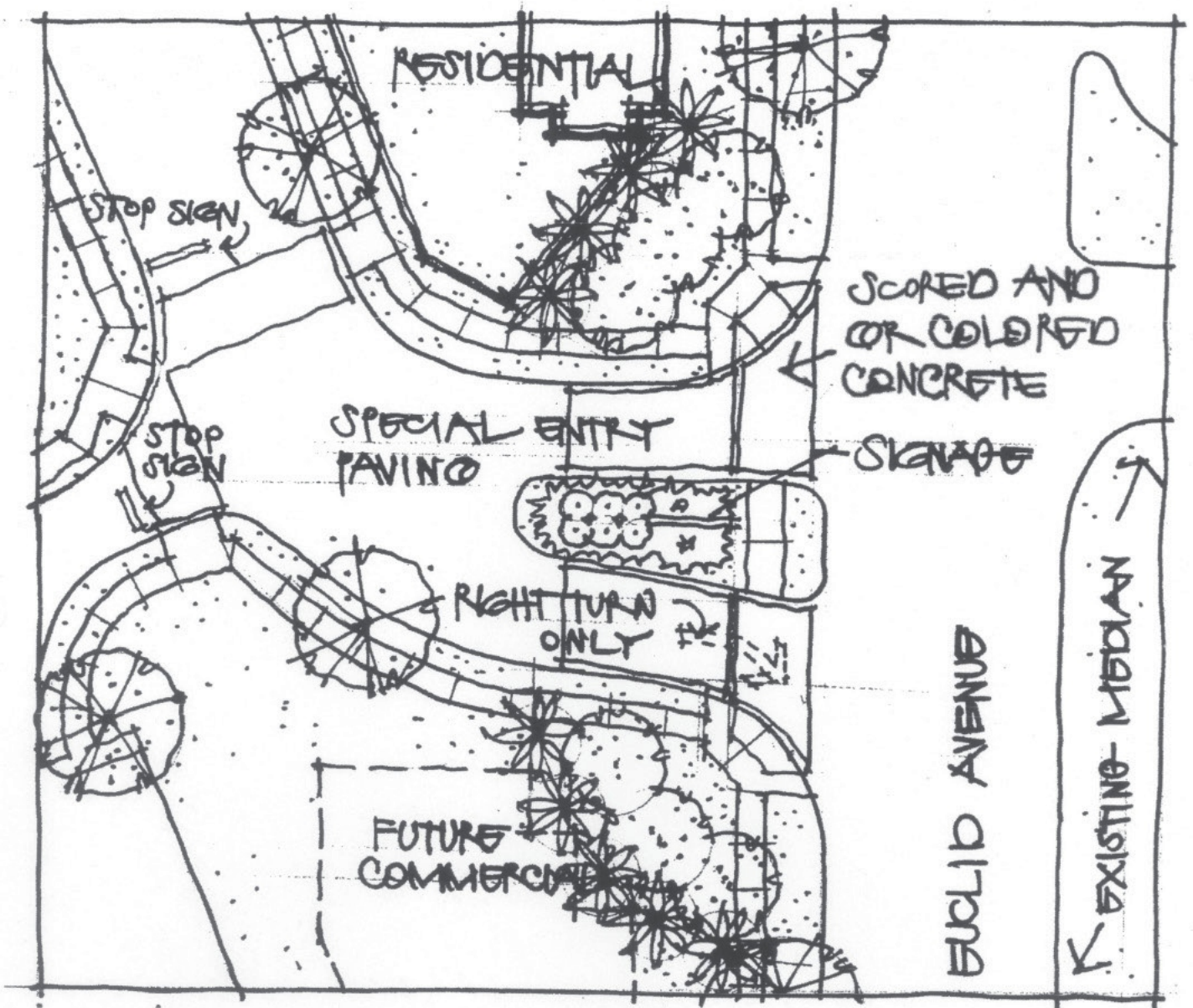


EXHIBIT 22

RESIDENTIAL ENTRY
AT EUCLID AVENUE

5.3.2 Commercial Service Center Entryways.

The main entry to the commercial service center from Euclid Avenue will be designed to provide two ingress lanes and two egress lanes to be striped as a left and left/through/right combination lane with a minimum driveway width of 48 feet. A northbound left turn lane should be provided with a pocket length of at least 120 feet, and a separate southbound right turn lane is recommended. This entry will be signalized. This entry will provide a community entry statement for the entire Borba Village community. This entry will consist of enhanced paving such as scored and/or colored concrete or specialty paving materials compatible with the architectural style and materials utilized in the commercial buildings within the travel lanes. Monument planting areas should be provided on both sides of the home improvement building. Planting areas should be provided on the south side of the entry to accommodate thematic landscaping and low walls. An entry median should be provided to include thematic landscaping and monumentation signage. Other secondary entries to the commercial service center from Euclid Avenue and Riverside Drive will incorporate the same enhanced paving as the main entry. The conceptual design of the main Euclid Avenue entry at the neighborhood commercial center is illustrated in Exhibit 23.

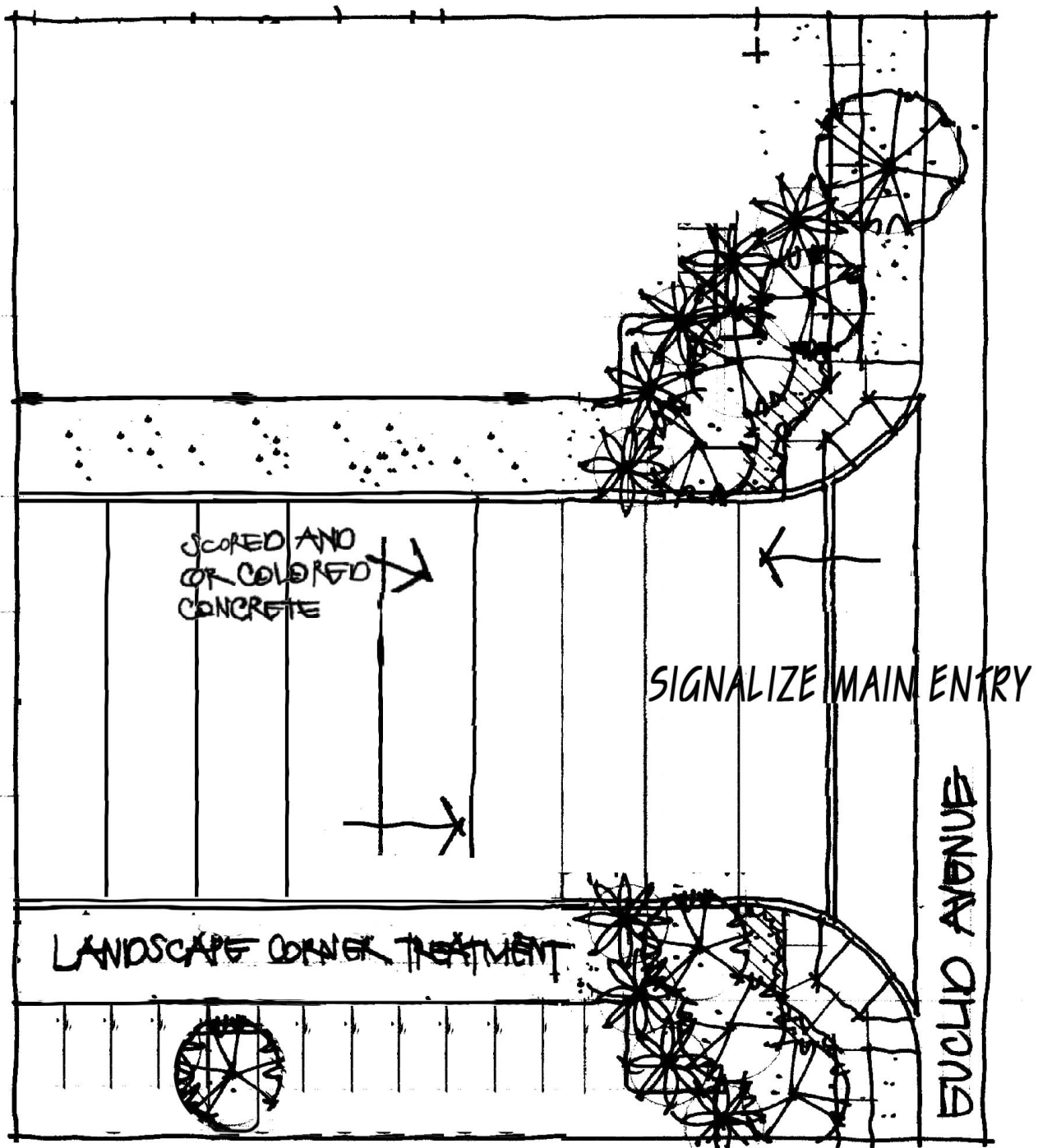


EXHIBIT 23

**COMMERCIAL SERVICE CENTER ENTRY
AT EUCLID AVENUE**

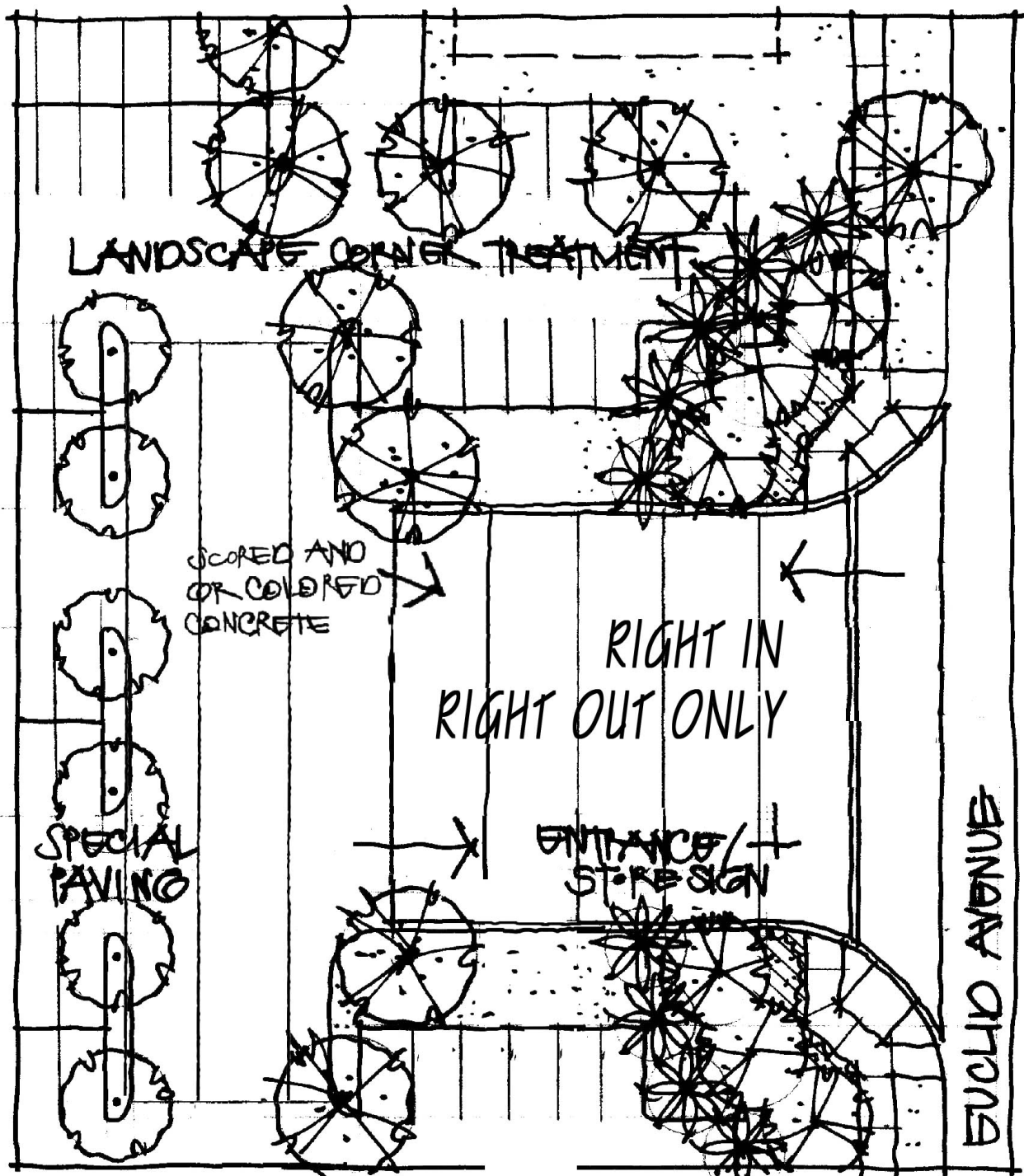


EXHIBIT 23

COMMERCIAL SERVICE CENTER ENTRY
AT EUCLID AVENUE

5.3.3 Fern Avenue at Multifamily Residential Area.

Three entries are proposed from Fern Avenue to serve the multifamily residential area. A primary entry will be centrally located along Fern Avenue. This entry is considered a “gateway” entry to the multifamily residential area due to its relationship to the east/west link of the pedestrian corridor planned as a central amenity to the Borba Village community. As such, this entry will consist of enhanced paving materials, such as scored and/or colored concrete or special paving stones, and thematic landscaping and hardscape treatment along each side of the entry. Secondary entries to the multifamily residential area from Fern Avenue will be designed with enhanced paving similar to that used in the main entry. The main entry will be landscaped in a manner consistent with the landscaping for the pedestrian corridor. The pedestrian corridor, designed to serve as a promenade through the community from the multifamily area through the commercial center to Euclid Avenue and single family areas and will connect the residential uses to the spaces outside of the complex. The pedestrian corridor is a central theme of the development design concept and as such will be lushly landscaped to a premium. The conceptual design of the primary Fern Avenue entry at the multifamily residential area is illustrated in Exhibit 24.

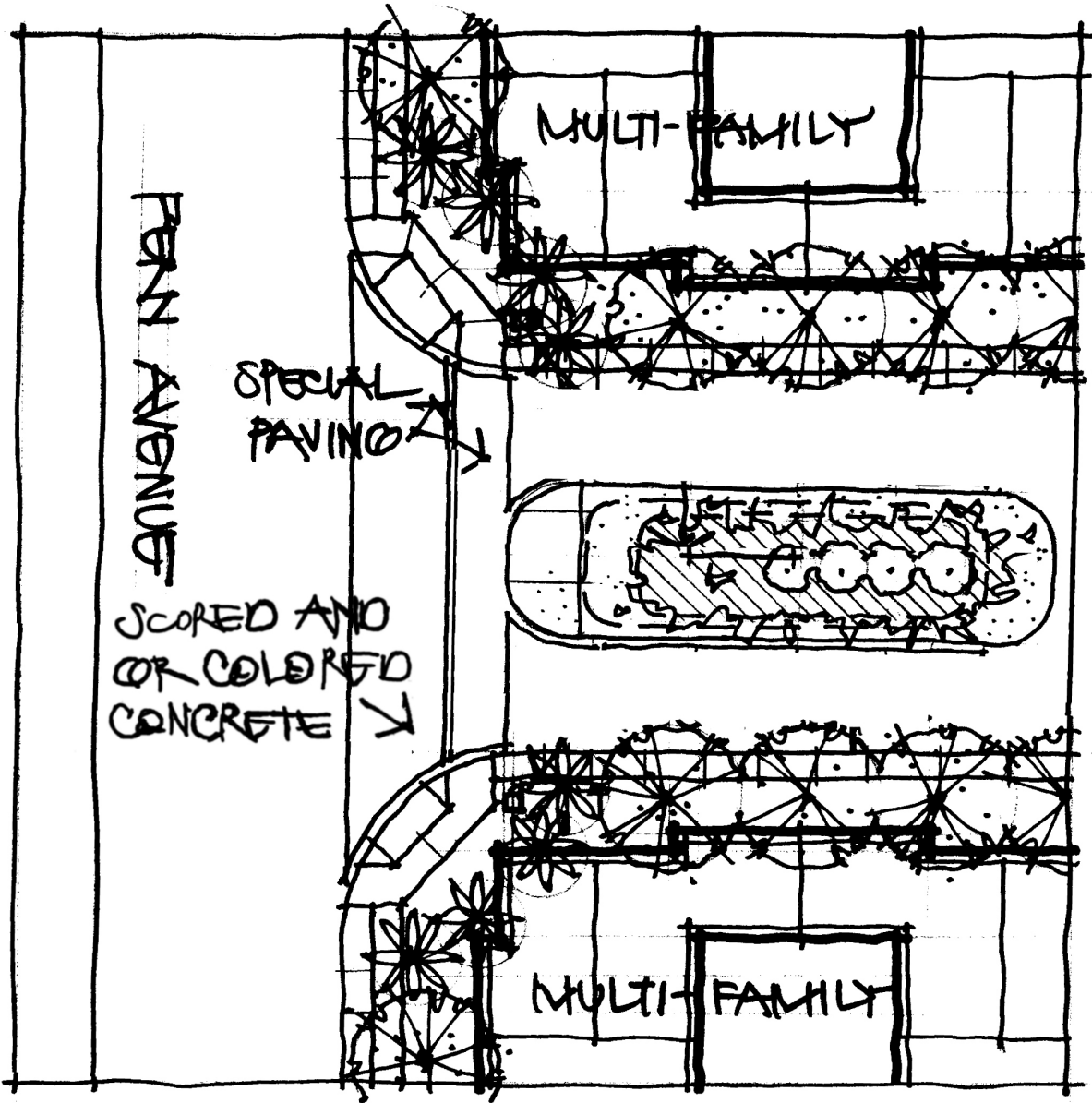


EXHIBIT 24

MULTIFAMILY ENTRY
AT FERN AVENUE

5.4 PEDESTRIAN CORRIDOR, OPEN SPACE AND RECREATION AREAS

A central feature of the Borba Village community is the pedestrian corridor extending throughout the residential community. A north/south alignment of a pedestrian corridor is planned through the central green connecting to another pedestrian corridor extending in an east/west alignment connecting residential areas to one another and to public streets where public transit stops are planned. The pedestrian corridor will take the form of a promenade by connecting to a community focal point within the residential area such as a public plaza and providing a gathering place for residents. The exact location and design of the pedestrian corridor and central green will be determined at the time of project development, however the following guidelines are provided for consideration during the Development Review process.

The corridor will be designed in a north/south alignment through the multifamily area connecting with the public sidewalk along Street A and the public sidewalk located in Riverside Drive. This link of the pedestrian corridor may vary in width and should be located within open space areas of the multifamily development wherever possible, however portions of this pedestrian corridor may also extend through open space areas provided within the parking area. Where the pedestrian corridor is located within the parking area a minimum width of 20 feet and an average width of 50 feet should be provided.

The pedestrian corridor will connect with a central green to be located within the multifamily residential area offering opportunities for passive recreation and casual interaction among neighbors. The central green will contain a signature park planned to include active recreational facilities such as a pool, spa, and tot lot. Larger open space areas within the multifamily residential development will also be improved as part of the central green providing passive recreational opportunities such as picnic and barbeque facilities.

The pedestrian corridor will extend into the neighborhood commercial center taking the form of a promenade and connecting to a community focal point within the commercial center such as a public plaza providing a gathering place for visitors to the neighborhood center and residents alike. From this focal point the pedestrian corridor will continue to Euclid Avenue through the commercial parking area providing a clearly designated pathway for pedestrians to access Euclid Avenue. The pedestrian corridor through the commercial area should incorporate landscaping and street furniture.

Exhibits 25-30 illustrate the conceptual design for the pedestrian corridor, open space, and recreational areas for Borba Village.



EXHIBIT 25

MULTIFAMILY PEDESTRIAN CORRIDOR CONCEPT NORTH / SOUTH EXTENSION

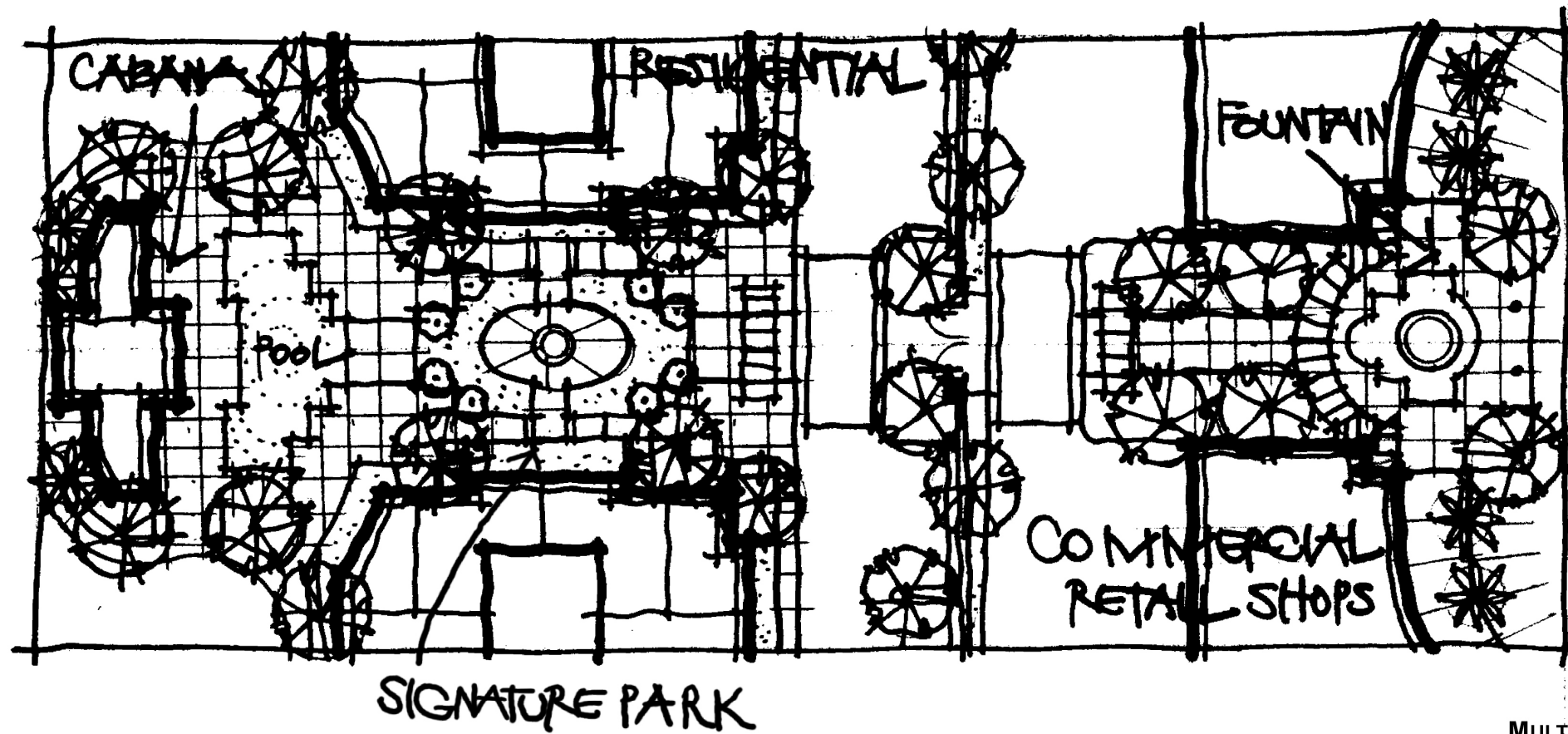
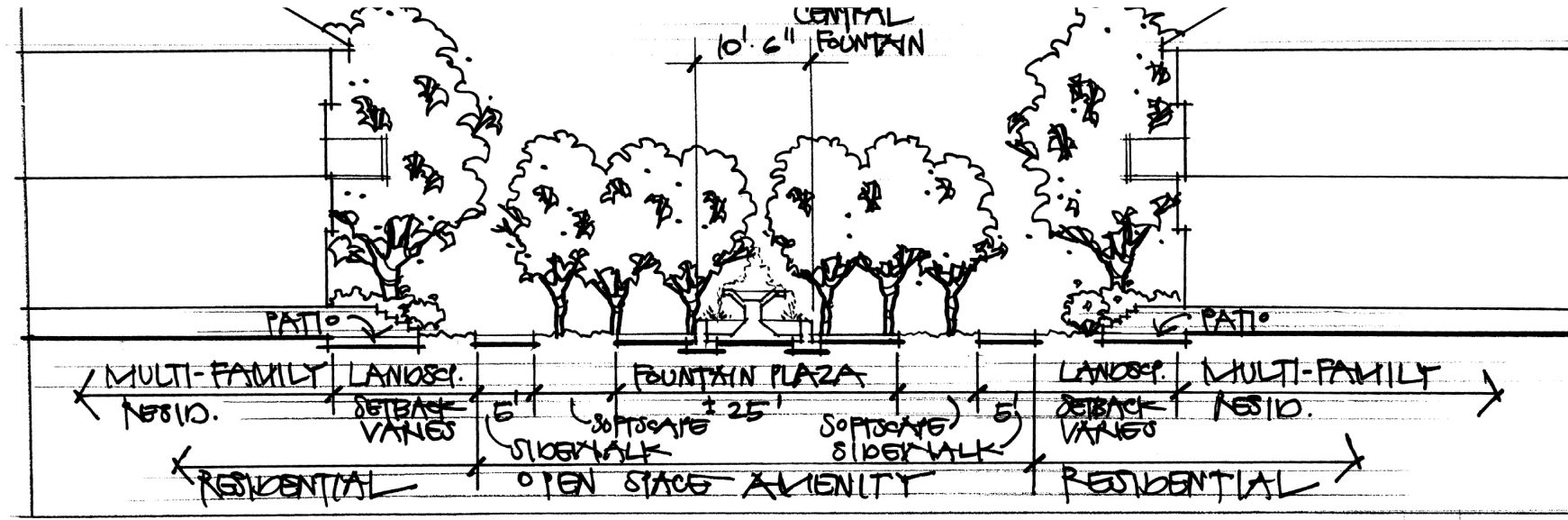


EXHIBIT 26
MULTIFAMILY CENTRAL RECREATION CONCEPT

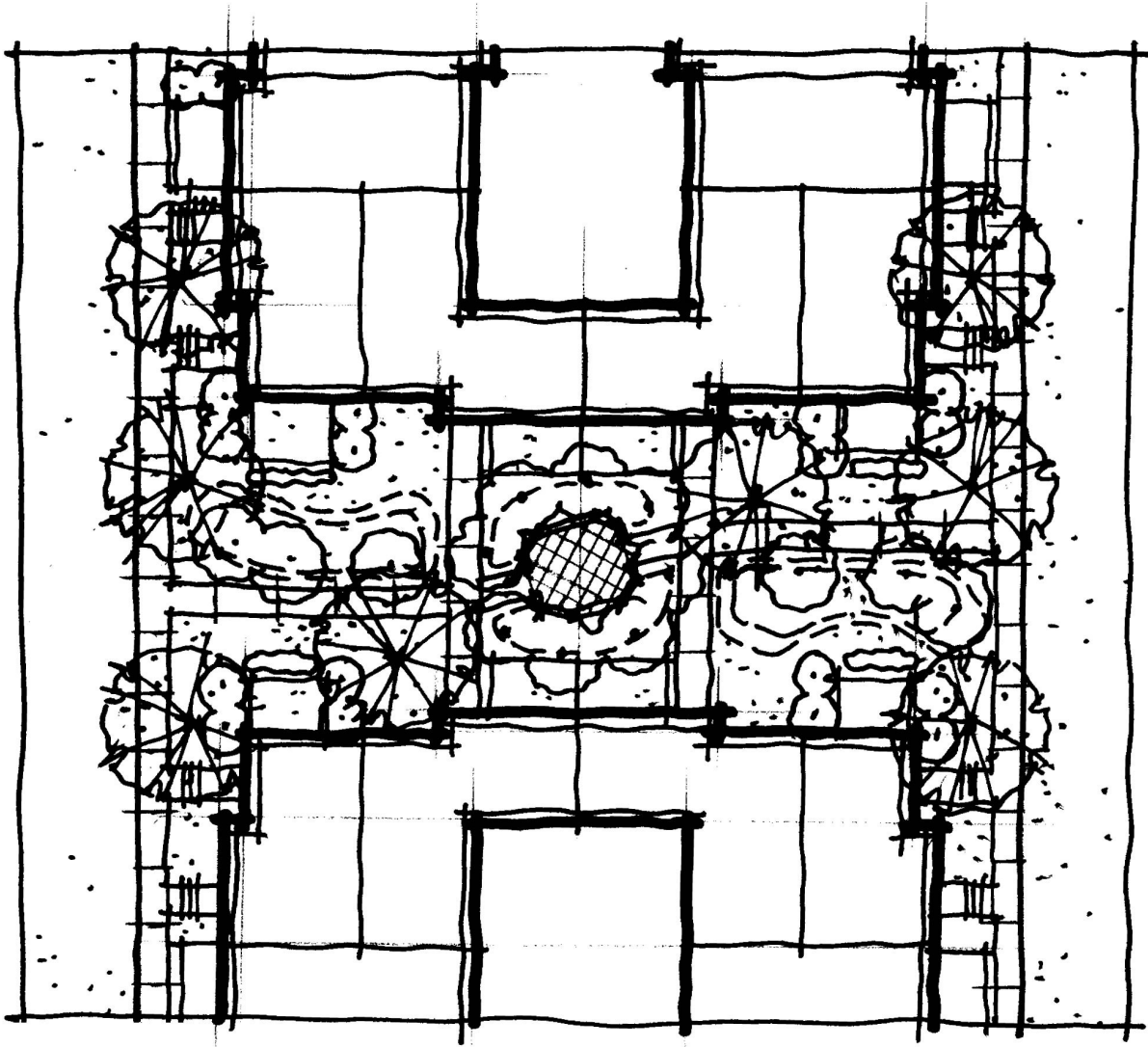


EXHIBIT 27
MULTIFAMILY
PASSIVE RECREATION CONCEPT

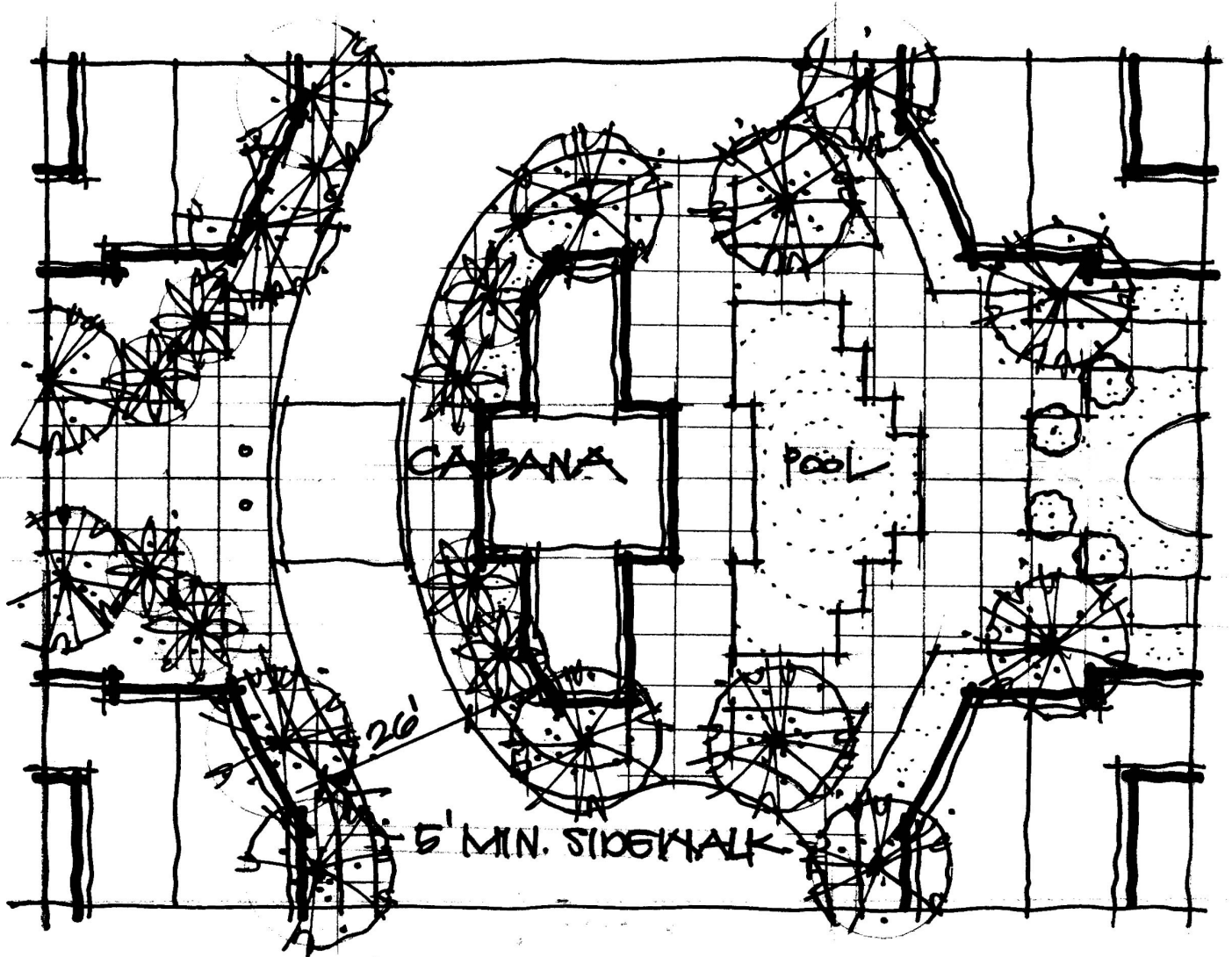


EXHIBIT 28

**MULTIFAMILY ACTIVE
RECREATION AREA CONCEPT**

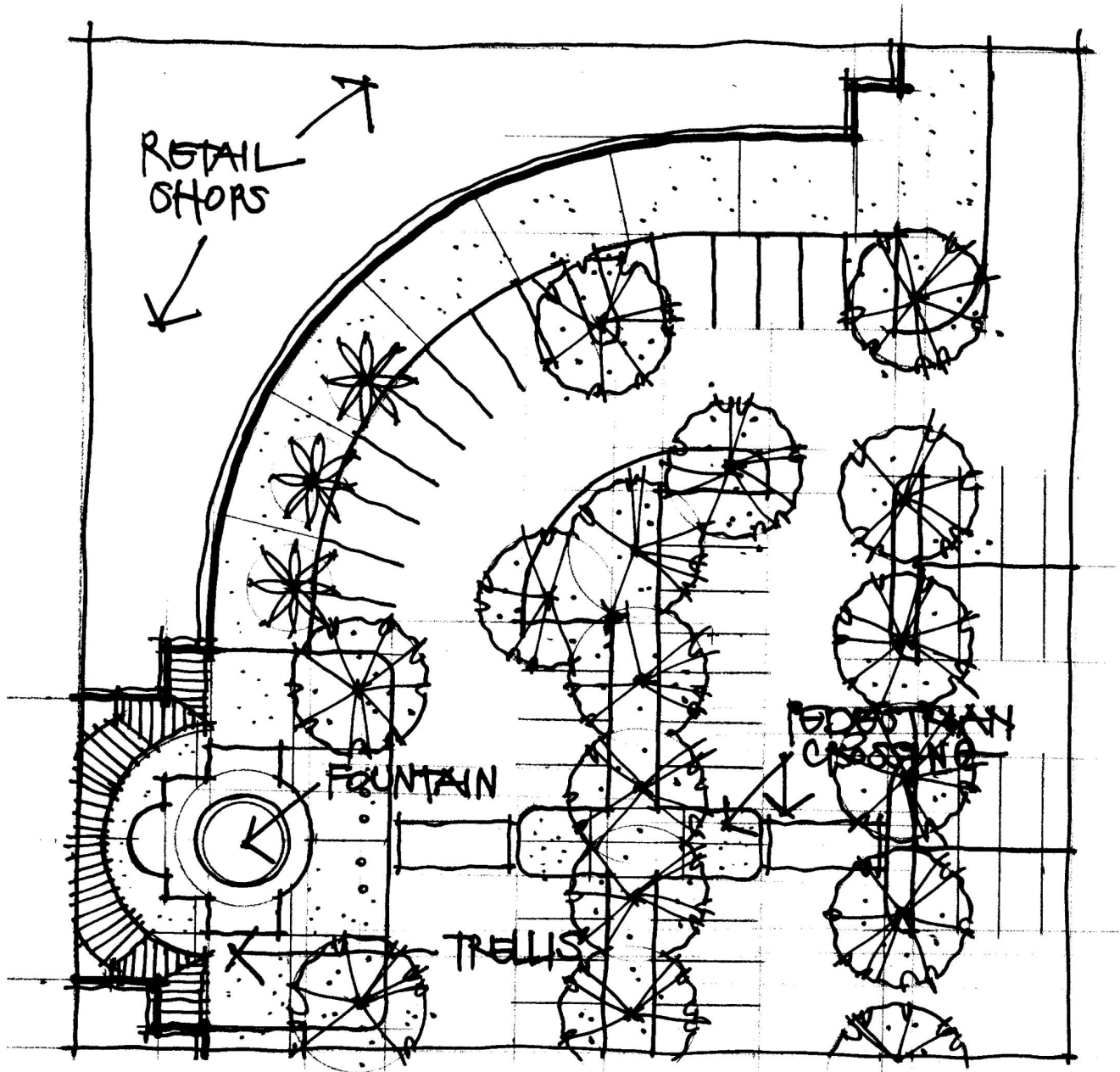
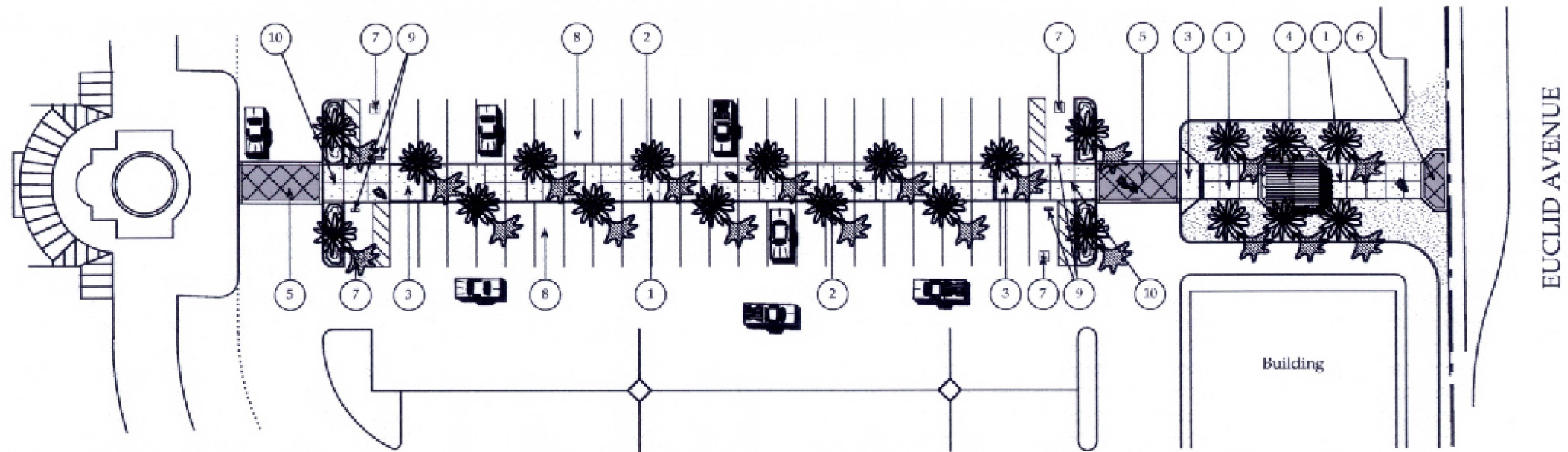





EXHIBIT 29

COMMERCIAL AREA
PUBLIC PLAZA CONCEPT

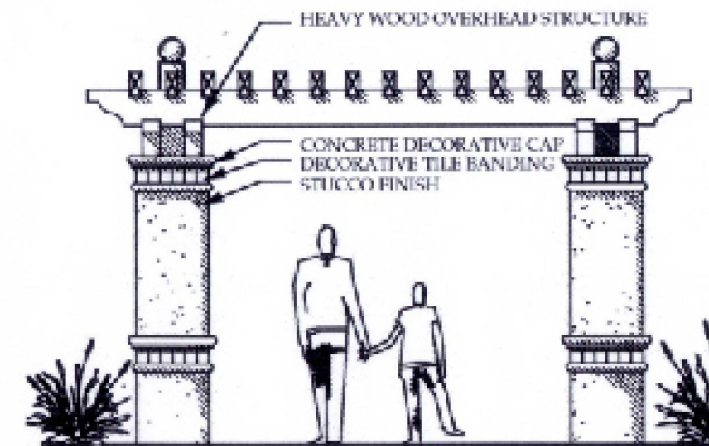


PLANT PALETTE

-  Street Palm Trees Along Pedestrian Walkway
WASHINGTONIA ROBUSTA • MEXICAN FAN PALM
-  Turf Area
-  Shrubs and Ground Covers

FEATURE LEGEND:

1. Medium Broom Concrete Pedestrian Access Walkway with Decorative Score joints.
2. Parking Area Diamond Planters.
3. Handicap Access Ramp.
4. Main Entry Wood Overhead Structure.
5. Enhanced Driveway with Bands.
6. Pedestrian Walk Enhanced Paving with Bands.
7. Handicap Parking Stalls.
8. 9' Wide by 20' Deep Parking Stalls.
9. Concrete Wheelstop.
10. Medium Broom Concrete & Curbing Set Flush with Asphalt Paving.



PEDESTRIAN WALKWAY OVERHEAD STRUCTURE
NOT TO SCALE

5.5 BUFFER ZONE

The relationship between the commercial service center and multifamily residential uses should include adequate buffering between the two land uses to assure security and privacy for residents of Borba Village. Commercial service uses should be oriented toward Euclid Avenue allowing for the service drive located behind the commercial service center to become a part of the buffering between land uses. A minimum 5-foot wide landscaped buffer area will be located (installed at the property line between the residential areas and the commercial service area) adjacent to the service drive. A decorative screen wall placed at the property line will occur along the wall providing a visual connection between the residential area and the landscaped buffer area. The residential units will be set back from the property line by an additional 19-foot wide landscape buffer. Within this 19-foot setback area sidewalks will be provided and balconies and patios/porches will be allowed to encroach. By fronting units adjacent to the buffer area rather than having units back up to the buffer area, residents can easily observe the area and interact with their neighbors with the goal of creating a safer and friendlier atmosphere. Exhibit 31 illustrates the concept for this buffer area.

Buffering for the single family residential area adjacent to Euclid Avenue will be provided by means of a minimum 15 foot wide landscaped buffer, to include earth berming, located adjacent to the 15 foot parkway as part of Euclid Avenue improvements. Together the parkway and landscaped buffer zone provide a 30 foot landscaped area between residential units and Euclid Avenue. Exhibit 32 illustrates the concept for this buffer area.

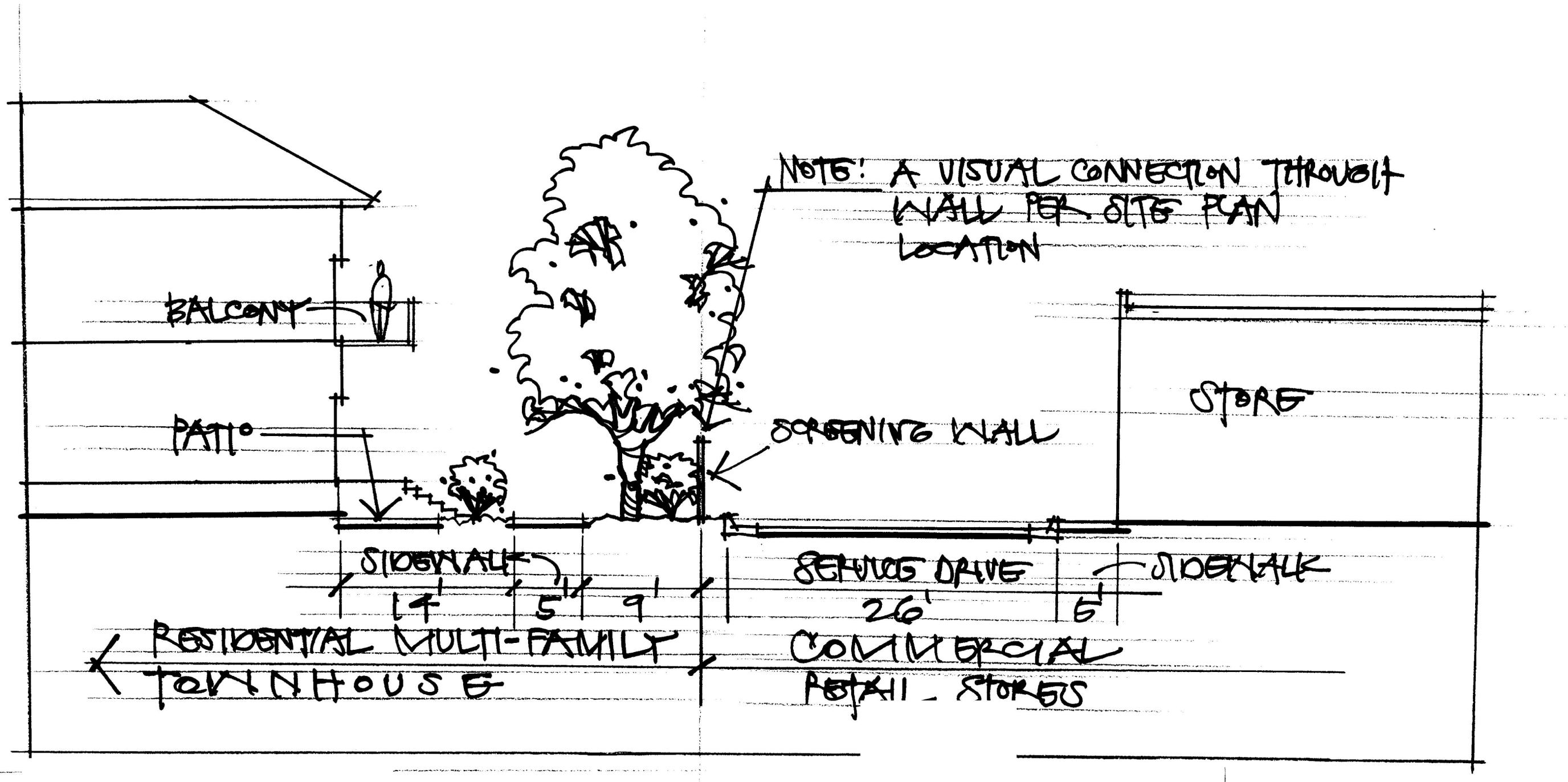


EXHIBIT 31

LAND USE BUFFER

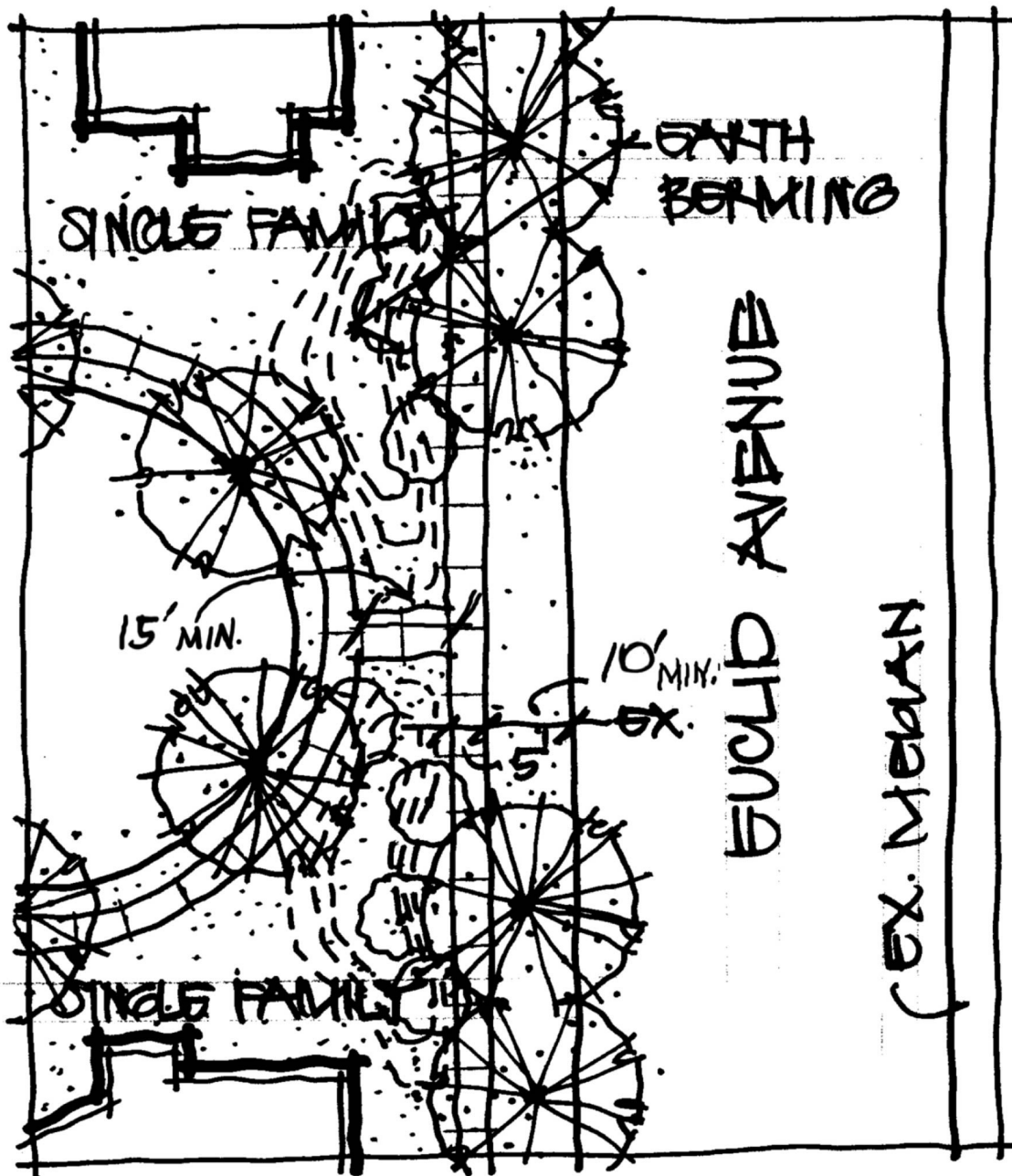


EXHIBIT 32

LAND USE BUFFER
SINGLE FAMILY DETACHED

5.6 SITE PLANNING CRITERIA

Development within the Borba Village Specific Plan will adhere to the adopted City of Ontario architectural design guidelines as contained in Articles 14 and 16 of the City's Development Code. The following site planning principles are included to augment the City's design guidelines to insure a unified and cohesive community that reinforces the traditional neighborhood concept.

- Emphasis of the street as the principle design element.
- De-emphasis of the visual impact of garages and carports.
- Attention to composition of building mass.
- The use of durable materials that retain their original beauty.
- Orienting residents to the street to encourage social interaction and sense of community.

5.6.1 RESIDENTIAL SITE PLANNING

Streets

- The street layout within residential neighborhoods should provide view corridors to open space where feasible. In addition, physical access to this community amenity should be provided where feasible.
- The use of parkways is required in the design of neighborhood streetscapes.
- The view from arterial streets should be perceived as pleasant neighborhood spaces in which the architectural character of the neighborhoods contributes positively to the overall street scene, therefore, the use of community walls is discouraged along these streets. The desired neighborhood character along these streets can be achieved through the use of appropriate plotting concepts, setbacks, and landscaping.

Plotting

- Locating buildings closer to streets creates safer and more active streets. Residents can more easily watch over the street and know their neighbors. Porches will be permitted to be setback 10 feet; bays and balconies should be allowed to project 6 feet into front setbacks in single family areas and 5 feet in multifamily areas along streetside setbacks to contribute to a street's human scale and activity.

- Single family residential units abutting Euclid Avenue should be plotted fronting Euclid Avenue along an enhanced sideyard setback as illustrated in Exhibit 33.
- Single story plates are encouraged on front elevations to enhance the pedestrian scale of neighborhood streets.
- No more than two single family detached dwelling units with the same floor plan shall be plotted adjacent to one another, and the floor plans shall be reversed with different elevation options to avoid a repetitious street scene.
- Varying lot widths should be provided within the single family residential area.

Garages and Carports

- Residential garages should be positioned to reduce their visual impact on the street. This will allow the active, visually interesting features of the house, to dominate the streetscape. Within single family areas, garages will be set back a minimum of 5 feet from the massing of the house. Exhibit 34 illustrates the typical plotting within the single family area. Exhibit 35 illustrates various garage concepts for placement on a single family lot designed to de-emphasize the garage from the street.
- Where garages are adjacent to one another along interior lot lines, their front elevations should be offset a minimum of five feet from one another. Garages that are setback further from the street than adjoining living areas are encouraged. Garages should be recessed a minimum 12 inches from adjacent wall planes to provide shadow pattern. A maximum of twenty percent (20%) of the single family residential units shall be permitted to be plotted with three car garages fronting the street.
- Carports should be limited to four cars and be open trellis or solid roof design as illustrated in Exhibit 36.

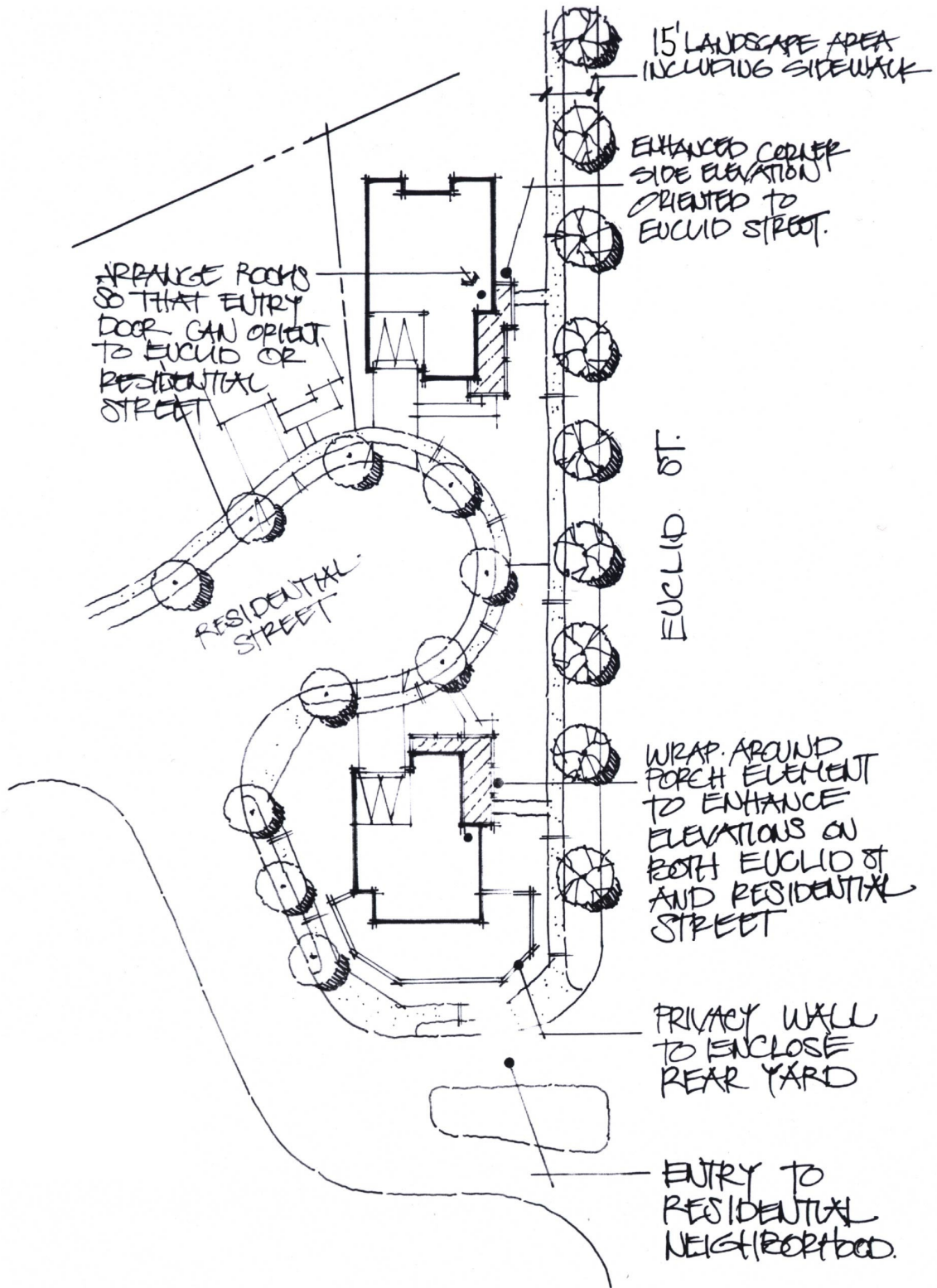
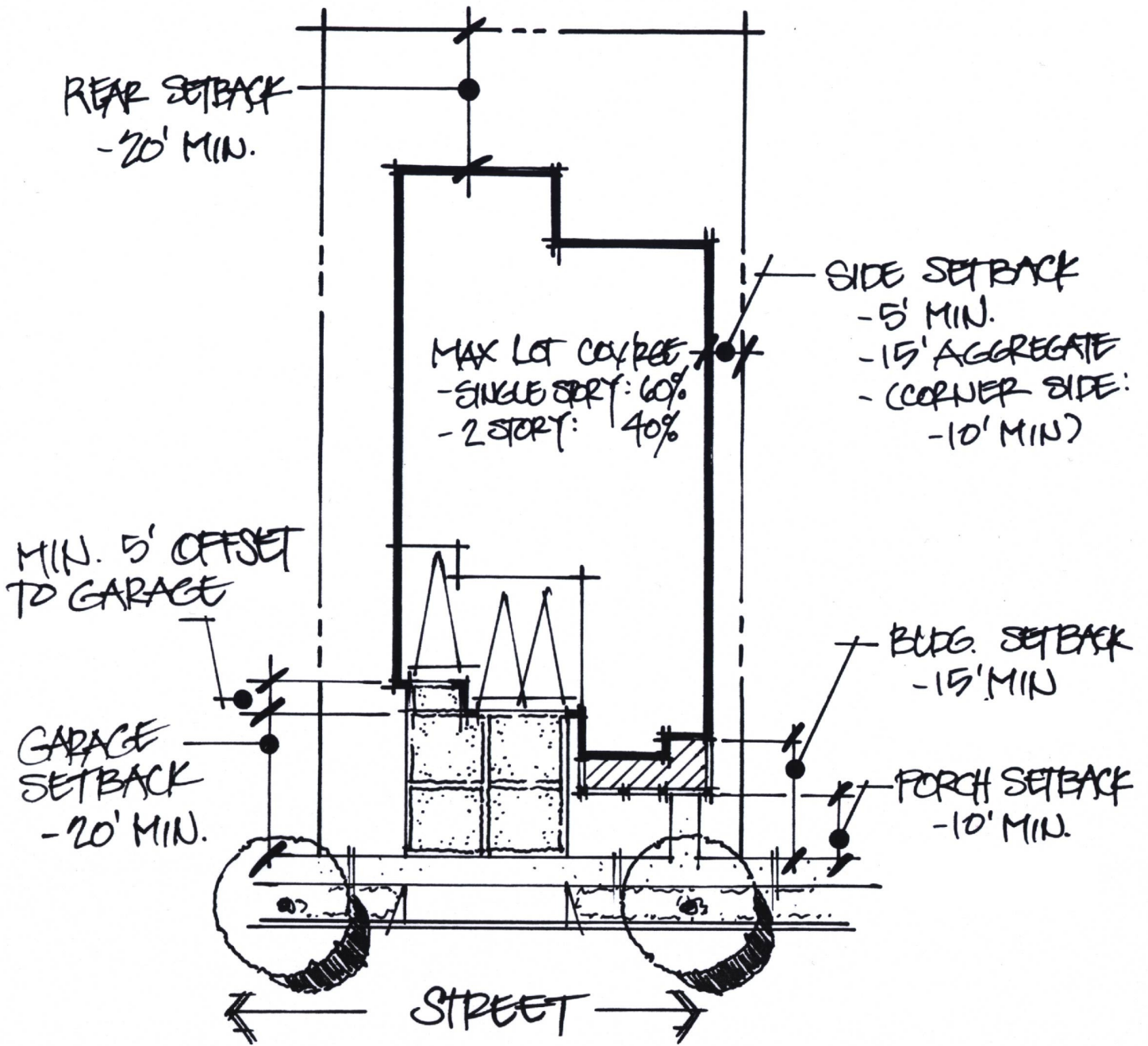


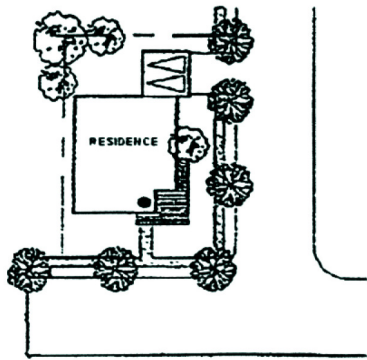
EXHIBIT 33

**RESIDENTIAL PLOTTING
ABUTTING EUCLID AVENUE**

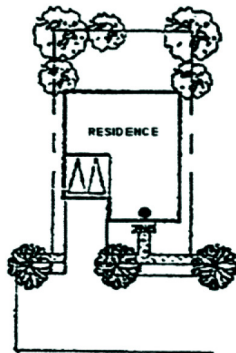


NOTE: A MAXIMUM OF 20% OF SINGLE FAMILY UNITS PERMITTED WITH 3 CAR GARAGE FRONTAGE

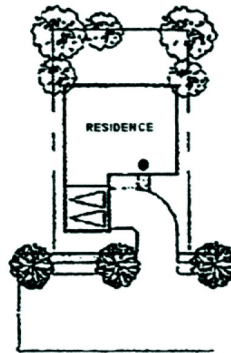
EXHIBIT 34
**SINGLE FAMILY
RESIDENTIAL PLOTTING**



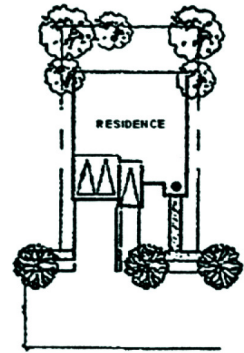
"SIDE ENTRY AT CORNER"



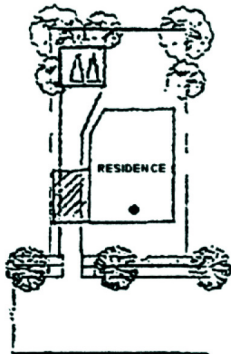
"PUSH BACK GARAGE"



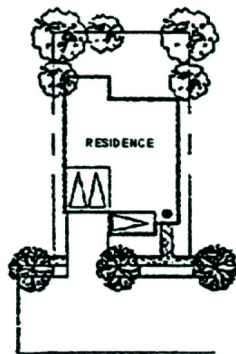
"TURN-IN GARAGE"



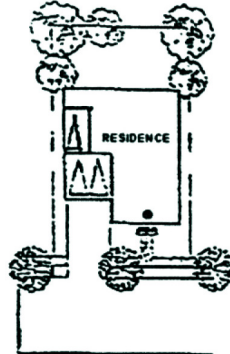
"OFF-SET GARAGE"



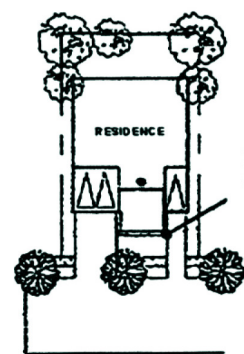
"PUSH BACK GARAGE WITH PORTE COCHERE OR ARCHWAY"



"SPLIT GARAGE"

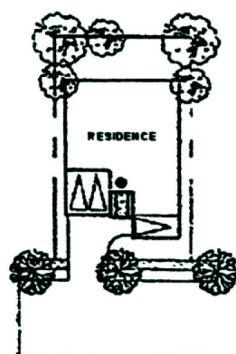


"PUSHED BACK GARAGE"

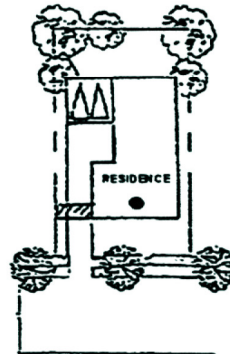


"SPLIT GARAGE"

Porch or Courtyard



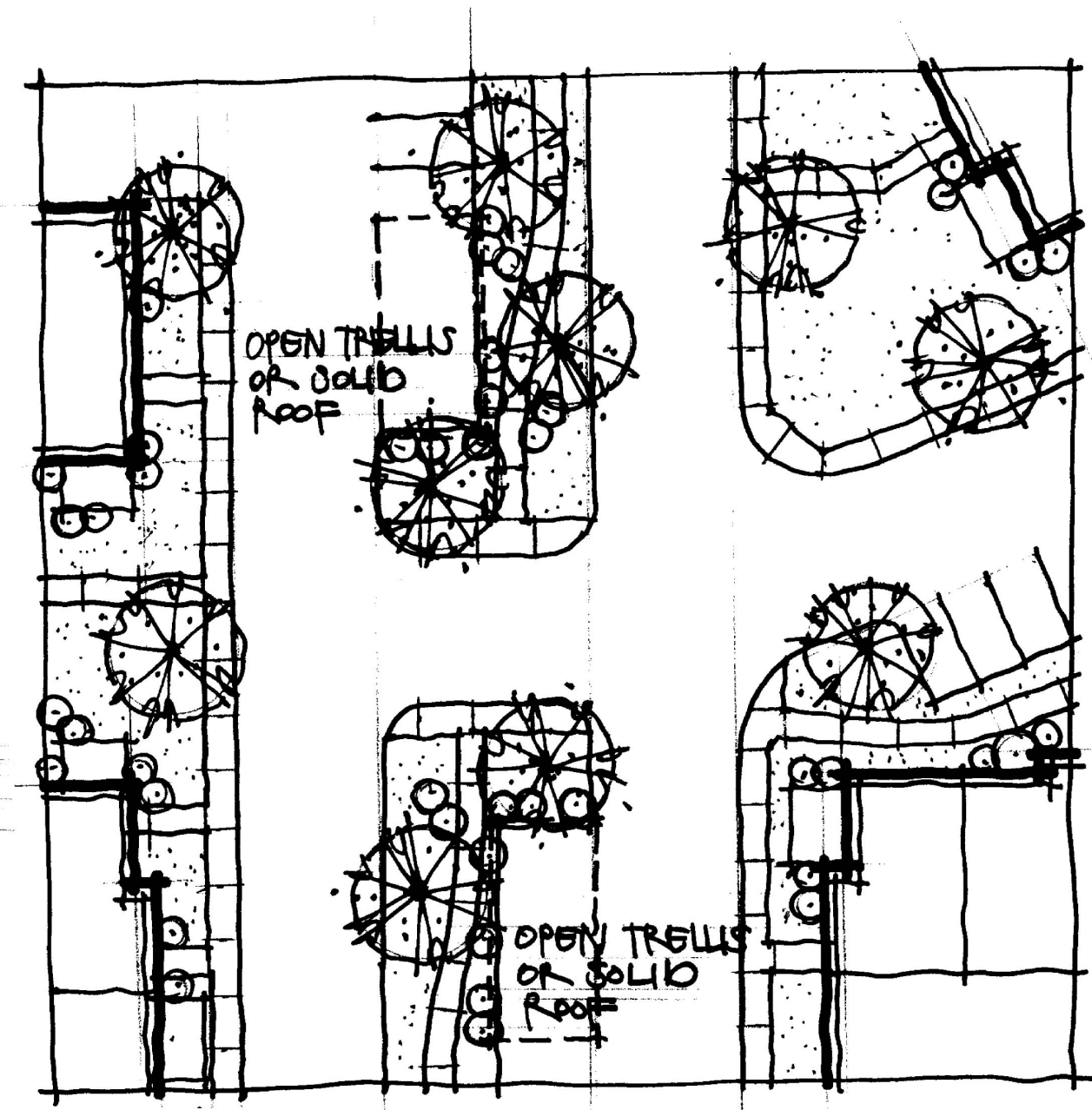
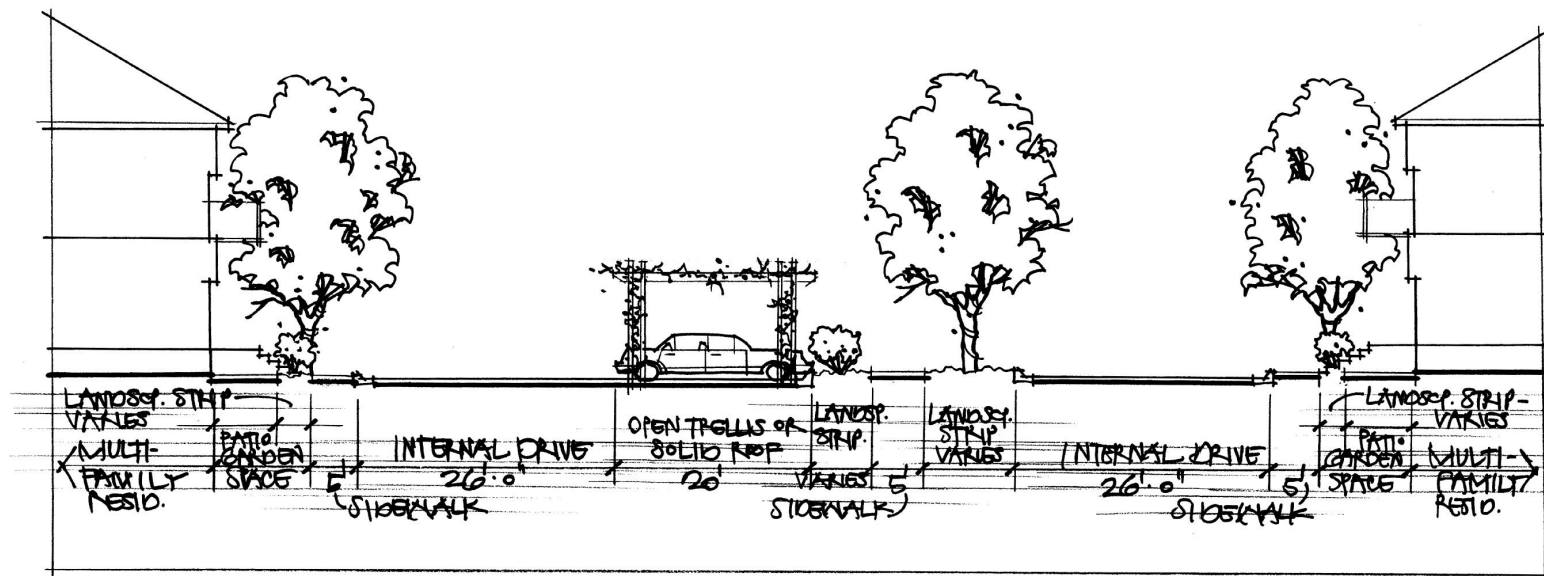
"SPLIT GARAGE"



"PUSHED BACK GARAGE WITH PORTE COCHERE OR ARCHWAY"

NOTE: A MAXIMUM OF 20% OF SINGLE FAMILY UNITS PERMITTED WITH 3 CAR GARAGE FRONTAGE

EXHIBIT 35
GARAGE CONCEPTS



5.6.2 COMMERCIAL SERVICE

- Shade should be provided on the commercial and support facilities of grouped and freestanding buildings arranged around shaded areas, where feasible to provide a pleasant pedestrian environment.
- Off-street parking should be screened from view through the use of plant material or low walls which are compatible with the building architecture.
- Landscape materials should be used within parking areas to visually minimize the impact of parking areas, as well as to provide shade cover for automobiles as illustrated in Exhibit 37.
- Commercial service drives and entries should be located in a manner that separates automobile traffic from truck and service vehicle traffic.
- To the extent possible, pedestrian pathways should be buffered from vehicular traffic through the use of plant material, paving, bollards, or other design features to ensure pedestrian safety.

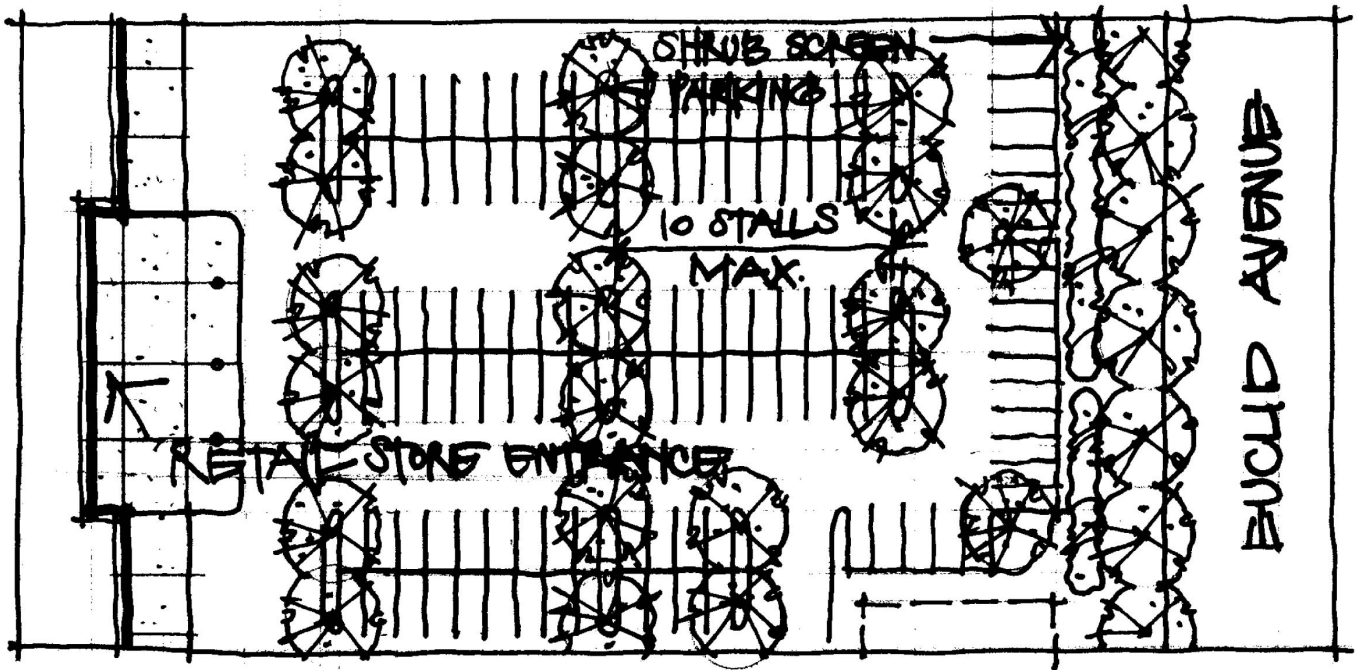


EXHIBIT 37

COMMERCIAL PARKING AREA

5.6.3 LIGHTING

- Entrances to building should be well lighted and void of any large shrubbery or obstructions to aid in public safety.
- All exterior lighting should be adequately controlled and shielded to prevent glare and undesirable illumination to adjacent uses or streets.
- On-site lights should provide a safe, functional and aesthetic design. Enough lighting shall be provided to ensure a safe environment while at the same time not cause areas of intense light or glare.
- Light fixtures and poles shall of decorative design and placed in a manner consistent and compatible with the overall site and building design.
- Lighting design shall comply with the City of Ontario Police Department security requirements.
- Lighting for pedestrian walkways shall be low profile and have a human scale.
- Freestanding parking lot light poles shall not exceed the height of a single story commercial structure.
- The use of wall packs as lighting fixtures is prohibited.

5.6.4 UTILITIES

- Temporary overhead power and telephone facilities are permitted only during construction.
- Transformers, utility pads and telephone boxes shall be located away from public streets, screened with decorative walls, earthen berms, vegetation, or a combination of these.
- Transformer enclosures shall be designed of durable materials with finishes and colors used which are compatible and harmonious with the overall architectural character of the commercial center and landscaped whenever possible.
- All exterior on-site utilities including, but not limited to drainage systems, sewers, gas lines, water lines, and electrical, telephone, and communications wires and equipment shall be installed and maintained underground. Items which must be placed above ground for function and safety reasons shall be landscaped and screened from view of public streets by using durable materials with finishes and colors compatible and harmonious with the overall character of the development.

5.6.5 TRASH ENCLOSURES

Trash bins shall be fully enclosed within 6 foot high walls and solid gates using steel or other similar material, be of an architectural design compatible with the overall design character of the project and should be softened with landscaping. Recommended locations include at the ends of carports or parking bays. The location of trash enclosures should be conveniently accessible for trash collection and maintenance. Exhibit 38 illustrates a concept for the treatment of trash enclosures.

5.6.6 SIGNS

Signs for the Borba Village Specific Plan will be reviewed and approved under separate permit and shall be consistent with Article 31: Signs, of the City's Development Code. The hierarchy of signs for Borba Village is anticipated to include monument signs wall signs, tenant identification signs, way-finding signs, and temporary signs. All signs shall be compatible with the architectural character of the project.

5.6.7 WALLS AND FENCES

Consistent with the traditional development concept, the use of community walls and fences is discouraged in favor of a more open community. However, in limited instances, walls are required for sound attenuation and/or privacy for individual residences. In such cases, walls should include decorative material and niches for landscape features and openings to allow visibility to key vistas. Decorative masonry walls will be provided along the perimeter of the single family residential area abutting streets. Within the single family residential neighborhood return side yard walls shall be provided. Where side yard return walls front the street these walls be decorative masonry walls. The conceptual plan for community walls and fencing is illustrated in Exhibit 41. Examples of wall and fencing details are illustrated in Exhibit 42.

5.6.8 BUS SHELTERS

Bus shelters should be designed to be compatible with the commercial or residential areas incorporating materials and elements representative of the commercial and/or residential architecture of the adjacent use. Minimum improvements for bus shelters should include seating.

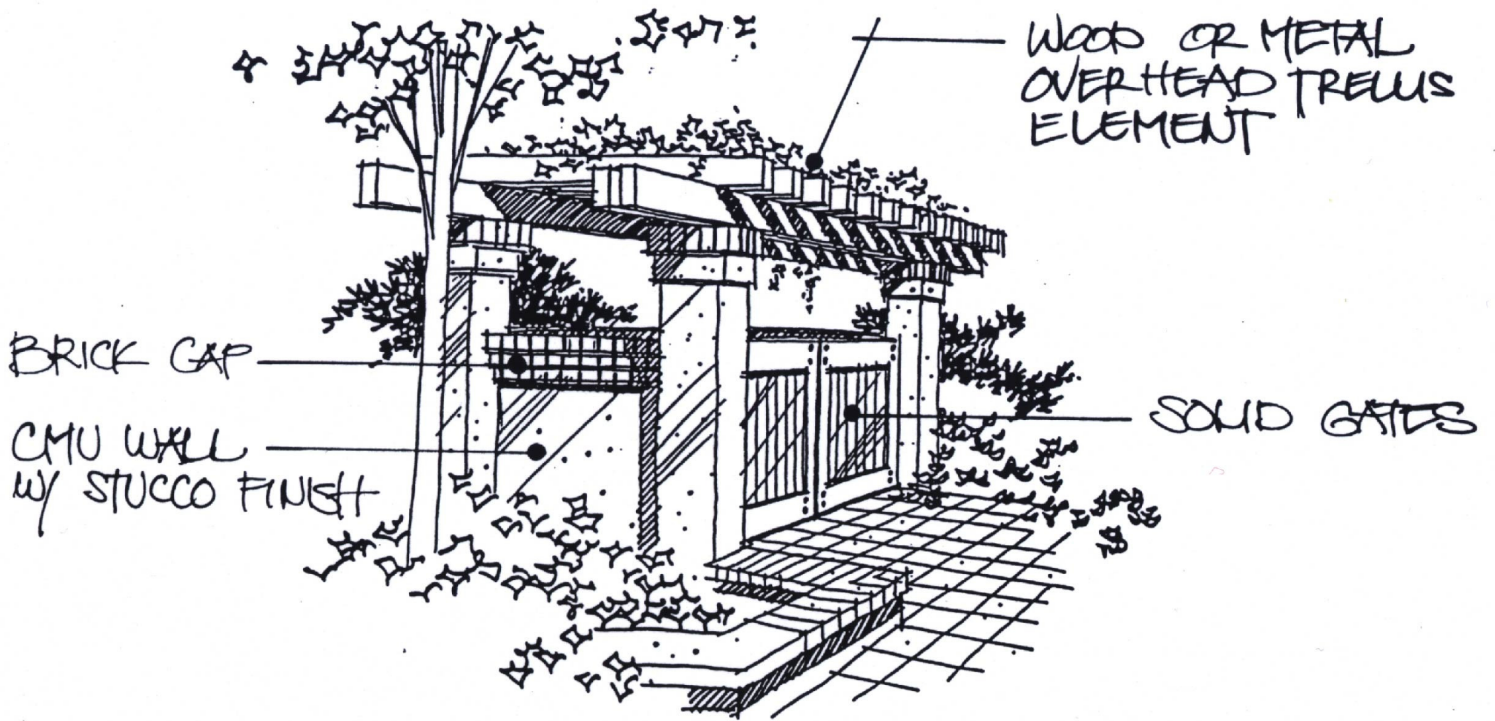
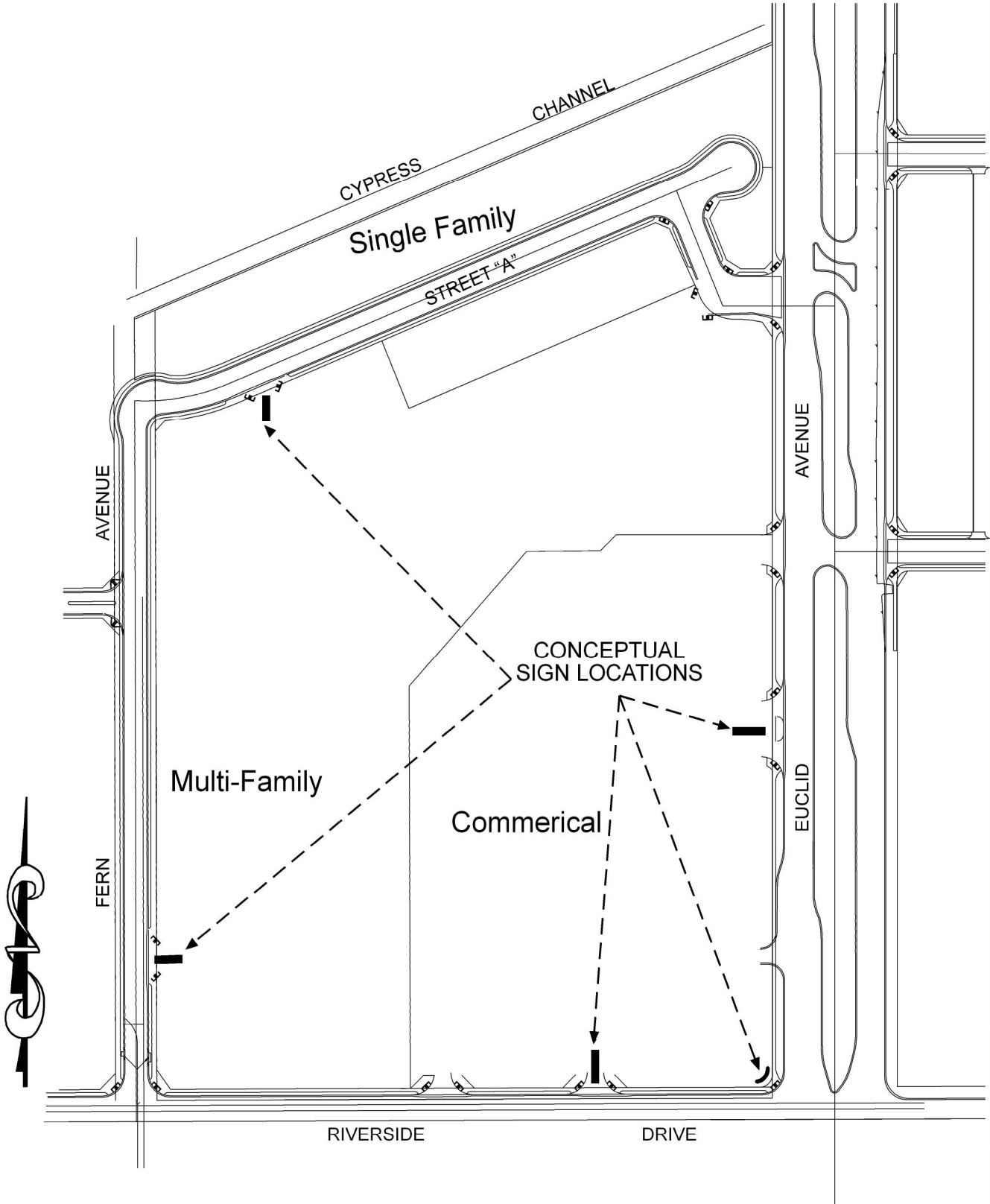


EXHIBIT 38

TRASH ENCLOSURES



(NOT TO SCALE)

EXHIBIT 39
CONCEPTUAL MONUMENT SIGN PLAN

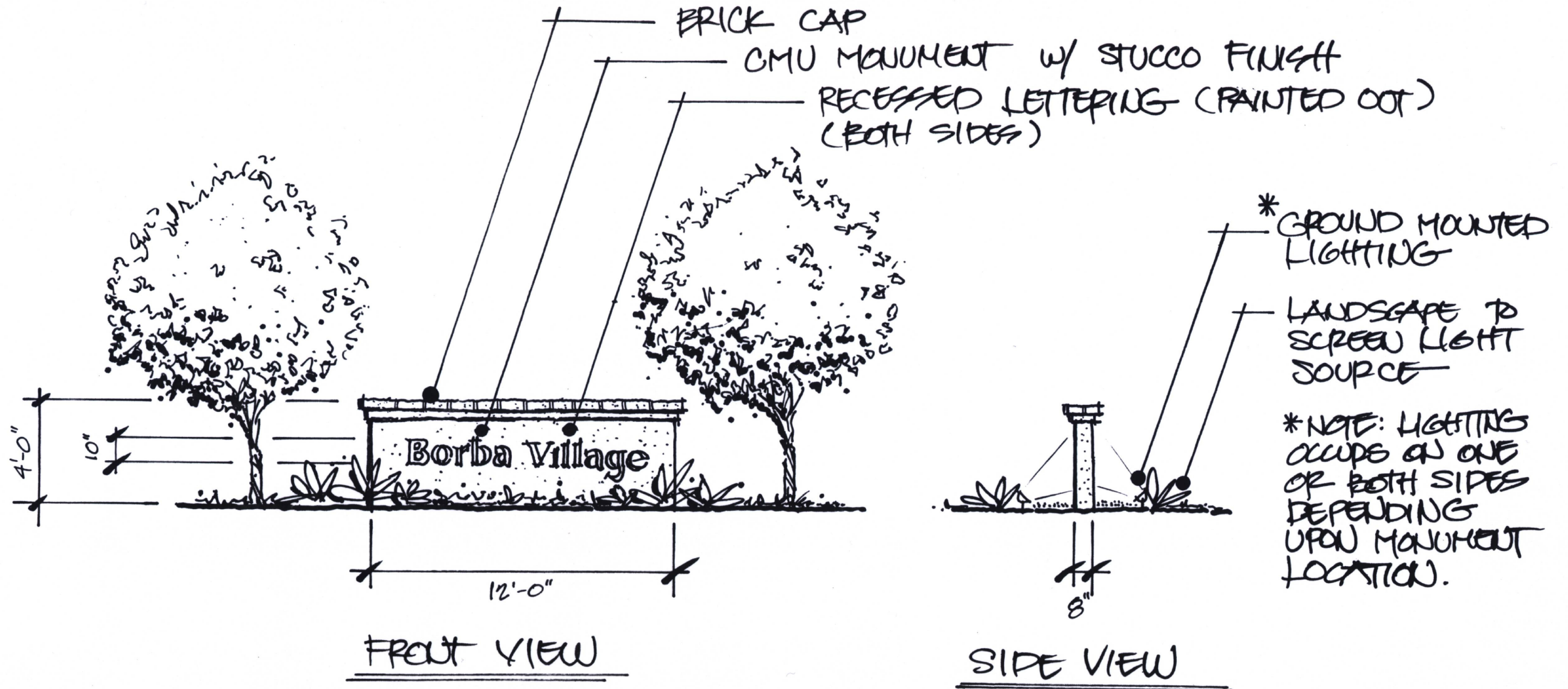
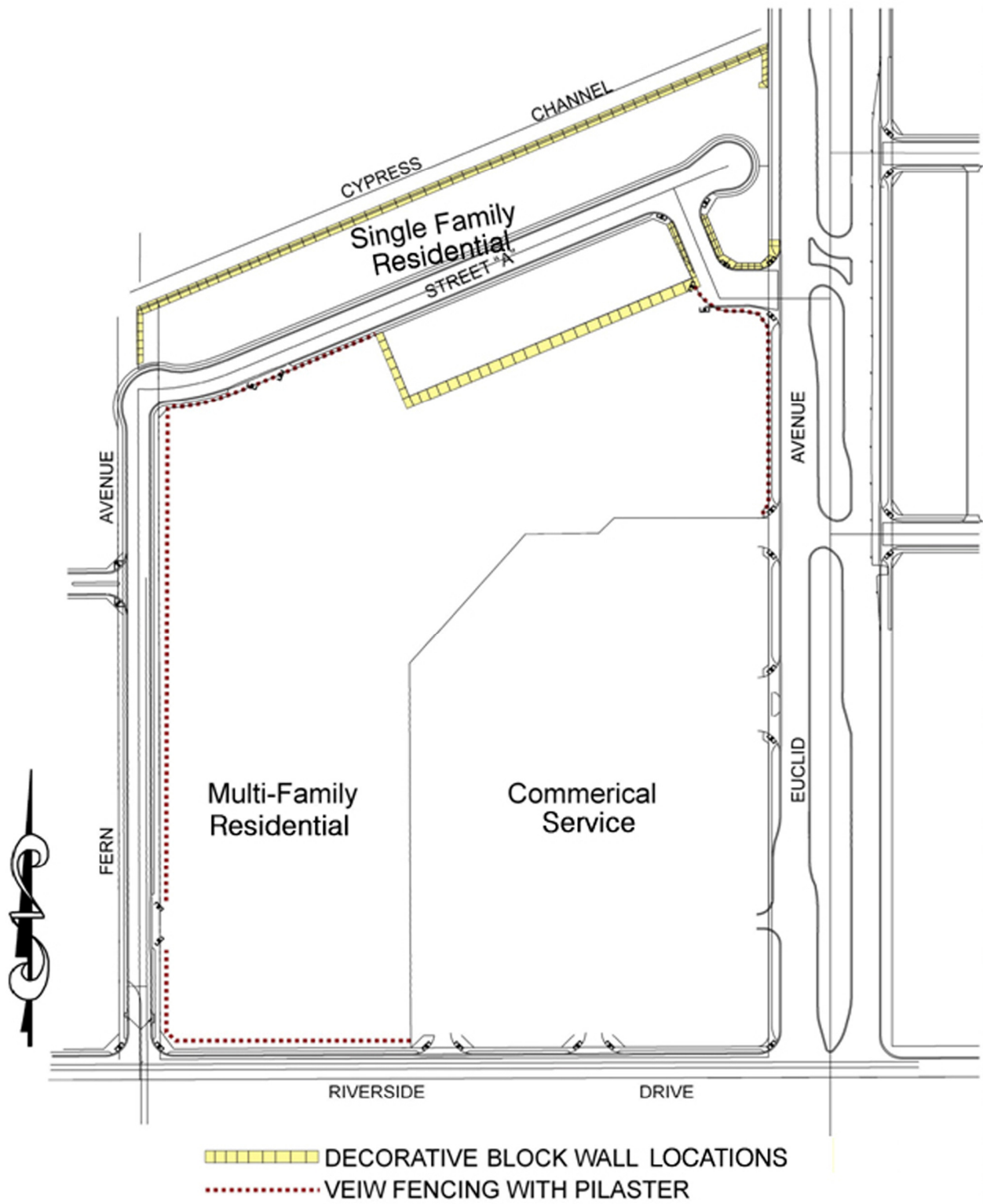


EXHIBIT 40
MONUMENT SIGN DETAIL



(NOT TO SCALE)

EXHIBIT 41
COMMUNITY WALLS AND FENCES

