



**MERRILL COMMERCE CENTER  
SPECIFIC PLAN**

***CITY OF ONTARIO***

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DATE: SEPTEMBER 29, 2020

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ES.1 SPECIFIC PLAN OVERVIEW

ES.2 OTHER GOVERNING DOCUMENTS

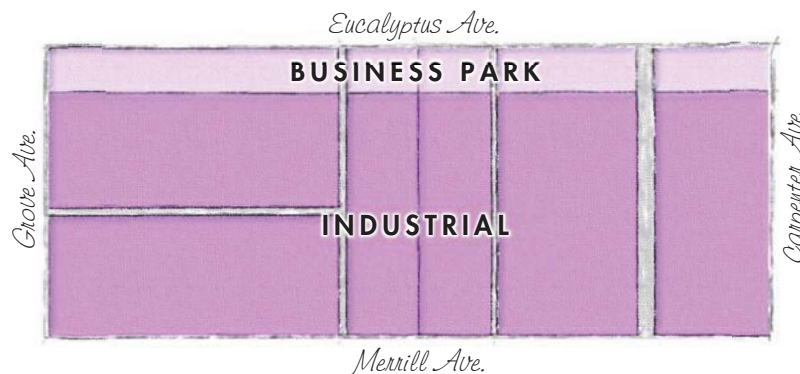
ES.3 SPECIFIC PLAN COMPONENTS

**ES.0 EXECUTIVE SUMMARY**

**ES.1 SPECIFIC PLAN OVERVIEW**

The MERRILL COMMERCE CENTER Specific Plan area covers approximately 376.3 acres in the southern portion of the City of Ontario. The Specific Plan property is located north of Merrill Avenue, south of Eucalyptus Avenue, east of Grove Avenue and west of Carpenter Avenue. Along the Specific Plan’s southern property line, Merrill Avenue forms the boundary between the City of Ontario and the City of Chino.

The location of the MERRILL COMMERCE CENTER in regional and local contexts is depicted in Figure ES-1, *Regional Map*, and Figure ES-2, *Vicinity Map*, which shows the relationship of the Specific Plan property with nearby cities, counties, and unincorporated communities.



The information contained in this Specific Plan provides guidance for the development of a contemporary, master-planned commerce center. The commerce center is envisioned to contain business park and industrial buildings supported by public roads and utility infrastructure systems, private driveways, parking lots, truck courts, lighting, landscaping, signage, and other functional and decorative features. Multi-purpose trails are provided at the perimeters of MERRILL COMMERCE CENTER to encourage connectivity and circulation by employees, visitors, and the passerby by means not completely dependent on a motorized vehicle.

The business park uses in smaller buildings are positioned along Eucalyptus Avenue while industrial uses in larger warehouse-style buildings comprise the balance of the site. As designed, building users are expected to be a mixture of local, national, and international businesses that bring job opportunities and economic growth to Ontario. A summary of the land uses is as follows:

**Table ES-1 Land Use Summary**

Land Use Designation	Acres <sup>1</sup>	Maximum Building Square Footage
Industrial Planning Areas	292.8 AC	7,014,000 SF
Business Park Planning Areas	55.1 AC	1,441,000 SF
Circulation	28.4 AC	--
<b>Total</b>	<b>376.3 AC</b>	<b>8,455,000 SF</b>

1. Acreages are approximate and subject to survey verification.

## ES.2 OTHER GOVERNING DOCUMENTS

In addition to this Specific Plan, which includes a Land Use Plan, Infrastructure Plan, Development Regulations, Design Guidelines, and an Implementation Plan tailored to the MERRILL COMMERCE CENTER, the following documents also contain applicable information:

- The Ontario Policy Plan (which serves as the City's State-mandated General Plan) that includes City-wide policies pertaining to land use, housing, mobility, safety, environmental resources, parks & recreation, community economics, community design, and social resources.
  - The City of Ontario Development Code, which governs over topics on which this Specific Plan's development regulations are silent. (Where the requirements of this Specific Plan differ from the requirements of the Ontario Development Code, this Specific Plan takes precedence.)
  - The MERRILL COMMERCE CENTER Development Agreement, which specifies methods for the financing, acquisition, and construction of infrastructure systems and provides assurance that development of the property may proceed subject to Ontario's rules and regulations in effect at the time of this Specific Plan's approval.
  - The Airport Land Use Compatibility Plan for Ontario International Airport (ONT) and the California Airport Land Use Planning Handbook published by Caltrans Division of Aeronautics, which contain criteria that apply to the development intensity of property located in the ONT's airport influence area.
- The Mitigation Monitoring and Reporting Program (MMRP) that is part of the MERRILL COMMERCE CENTER's Environmental Impact Report (EIR) prepared in compliance with the California Environmental Quality Act. The MMRP stipulates measures that are required to be implemented to mitigate the environmental effects of the commerce center's construction and operation.

**ES.3 SPECIFIC PLAN COMPONENTS**

This MERRILL COMMERCE CENTER Specific Plan is organized into the following chapters.

**Chapter 1 - Introduction:**

Describes the purpose and objectives of this Specific Plan, the related entitlement approvals for implementing development, and the general relationship of this Specific Plan to the Ontario Policy Plan (General Plan).

**Chapter 2 - Existing Conditions:**

Describes the physical setting of the MERRILL COMMERCE CENTER and the physical conditions on and surrounding the property at the time this Specific Plan was prepared.

**Chapter 3 - Land Use Plan:**

Describes the MERRILL COMMERCE CENTER's development plan, which includes six Industrial planning areas and five Business Park planning areas, with the Business Park areas concentrated along Eucalyptus Avenue in the northern portion of the Specific Plan area. Chapter 3 also specifies the acreages of each planning area and the maximum development intensities (amount of building square footage) permitted in each land use category.

**Chapter 4 - Infrastructure Plan:**

Provides information on vehicular and non-vehicular circulation improvements; the planned backbone water, sewer, recycled water, and storm drain systems; the planned dry utility network; and the preliminary grading concept for the development of the MERRILL COMMERCE CENTER.

**Chapter 5 - Development Regulations:**

Establishes the list of permitted and conditionally-permitted uses in the Specific Plan area, and presents the development regulations (zoning) that govern the uses. A discussion of the relationship of the MERRILL COMMERCE CENTER Specific Plan's development regulations to the City of Ontario Development Code also is provided.

**Chapter 6 - Design Guidelines:**

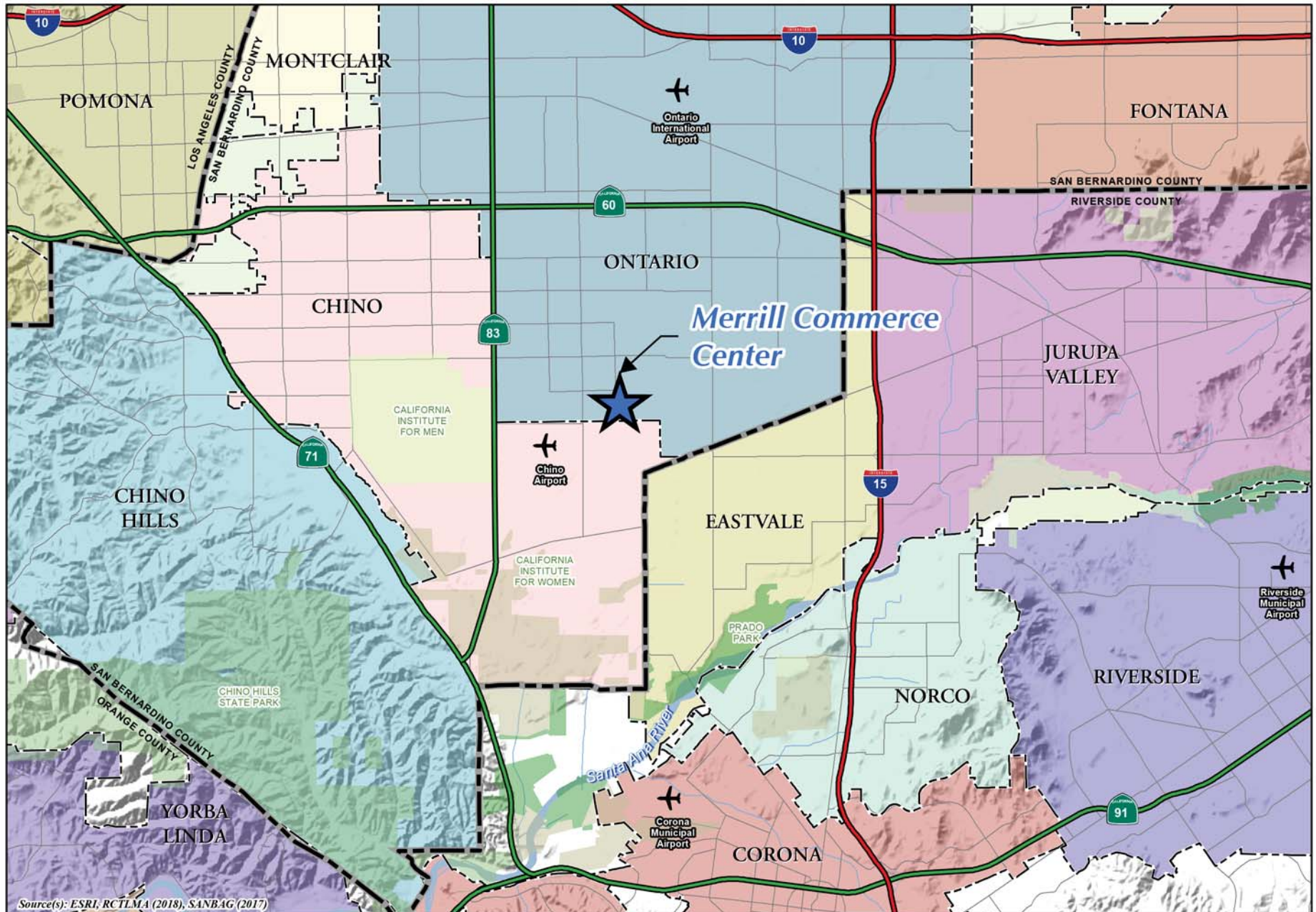
The MERRILL COMMERCE CENTER's design guidelines presented in Chapter 6 guide the site planning, landscaping, and architectural quality of implementing development within the Specific Plan area. Guidelines are included that address architectural design, landscape design, streetscapes, entry treatments and monuments, corner treatments, walls and fencing, lighting, and signage.

**Chapter 7 - Implementation Plan:**

Chapter 7 presents the policies and procedures for the City's review and approval of implementing projects within the MERRILL COMMERCE CENTER. This chapter also describes the methods and procedures for interpreting and amending the Specific Plan as necessary. A summary of maintenance responsibilities for development within the Specific Plan also is provided.

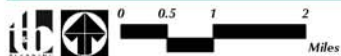
**Appendix A - General Plan Consistency:**

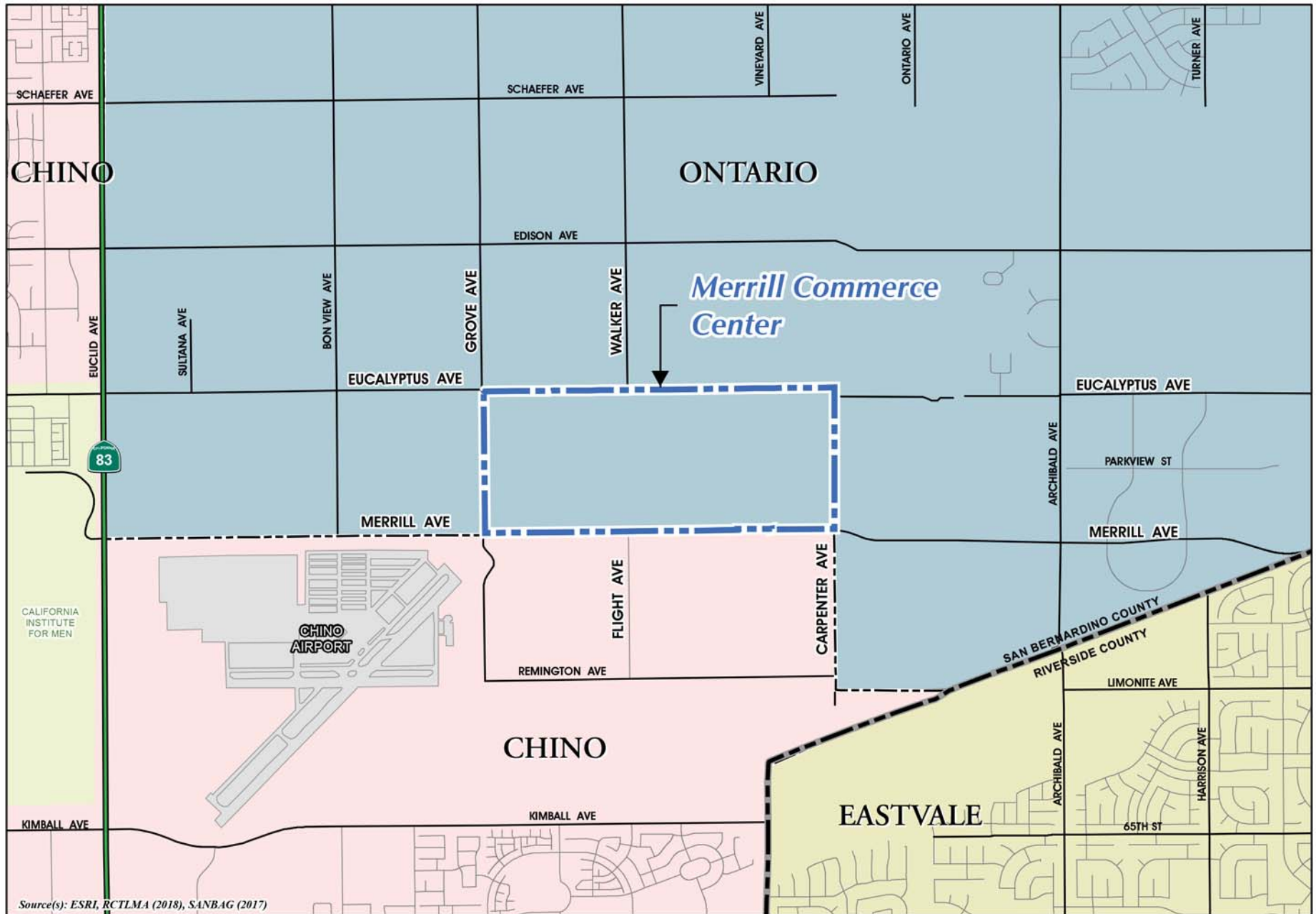
Includes a matrix evaluating the consistency of the MERRILL COMMERCE CENTER Specific Plan to each of the applicable policies of the Ontario Policy Plan (the City's General Plan).



Regional Map

Figure ES-1





Vicinity Map

Figure ES-2





# INTRODUCTION

## *CHAPTER 1*

1.1 SPECIFIC PLAN PURPOSE

1.2 SPECIFIC PLAN OBJECTIVES

1.3 AUTHORITY

1.4 SEVERABILITY

1.5 APPROVED PROCESS AND COMPANION ACTIONS

1.6 RELATIONSHIP TO THE ONTARIO PLAN

## CHAPTER 1 - INTRODUCTION

### 1.1 SPECIFIC PLAN PURPOSE

The underlying purpose of this Specific Plan is to guide the development of a 376.3-acre property into a master-planned commerce center, known as the MERRILL COMMERCE CENTER. The site is located within the southwestern portion of the City of Ontario (City) in an area known as Ontario Ranch. The Ontario Policy Plan requires the Ontario City Council to approve Specific Plans as part of the entitlement process for new development projects in this area.

Implementing development projects within the boundaries of the MERRILL COMMERCE CENTER are required to demonstrate substantial conformity with the information contained in this Specific Plan document.

Situated in proximity to three major freeways, the Ontario International Airport (ONT), and the Chino Airport, the MERRILL COMMERCE CENTER is poised to successfully accommodate uses that rely on access to the local and regional transportation network. The Specific Plan area is located approximately 1.8 miles east of State Route 83, 2.75 miles south of State Route 60, 3.25 miles west of Interstate 15, 4.0 miles southeast of the Chino Transit Center, and 7.0 miles north of State Route 91. Additionally, the Port of Long Beach and the Port of Los Angeles, which serve as major gateways to international trade, are located only ±53 miles to the southwest of the Specific Plan area. The property’s location at the junction of these major transportation facilities establishes a clear advantage for land uses that rely on proximity to the transportation network to remain competitive.

#### Why Ontario?

*Ontario is a thriving community with a strong business and employment hub located approximately 53 miles east of downtown Los Angeles in the western part of the Inland Empire metropolitan area. The City is conveniently located near regional transportation facilities and is home to the Ontario International Airport, making it an ideal location for a variety of business park and industrial land uses, including but limited to corporate headquarters, e-commerce fulfillment centers and other goods movement/ supply chain uses.*





## 1.2 SPECIFIC PLAN OBJECTIVES

This Specific Plan achieves the following objectives:

- To provide a land use plan for the development of a state-of-the-art commerce center that accommodates modern business and industrial activities.
- To attract and sustain industrial and business park uses within the Specific Plan area that are compatible with surrounding land uses.
- To locate businesses that rely on transportation efficiency in an area of south Ontario that offers convenient access to the state highway system.
- To provide opportunities for positive economic benefit to the City, including new net revenues to the General Fund which can be used for vital City services.
- To diversify the City's available range of employment-generating land uses.
- To improve connectivity in the area by providing multi-purpose trails and bike racks that encourage circulation by means not completely dependent on a motorized vehicle.
- To identify capital improvements for water, recycled water, sewer, storm drain, and circulation facilities that serve planned land uses within and adjacent to the Specific Plan area.
- To define guidelines and standards for architecture, landscaping, entry monuments/signage, and walls and fencing within the Specific Plan area.
- To set forth a development phasing sequence that is aligned with a logical sequence for the installation of supporting on-site and off-site infrastructure.



*The MERRILL COMMERCE CENTER Specific Plan brings jobs, sustainable economic growth, and business opportunities to the southern portion of Ontario. Its contemporary design is an attractive asset for the City and complements other surrounding employment and supply chain developments in Ontario and the surrounding metropolitan area.*

### 1.3 AUTHORITY

This Specific Plan is a regulatory document prepared pursuant to the provisions of California Government Code §§ 65450 through 65457, which grants local government agencies the authority to prepare Specific Plans for the systematic implementation of their General Plan for all or part of the area covered by the General Plan. While the Ontario Plan covers the entire City, the Specific Plan concentrates on the specific development of the approximately 376.3-acre MERRILL COMMERCE CENTER property.

California Government Code §§ 65450 through 65457 establish the authority to adopt a Specific Plan, identify the required contents of a Specific Plan, and mandate consistency with the General Plan. According to California Government Code § 65451:

- (a) A Specific Plan shall include text and a diagram which specify all the following in detail:
  - (1) The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
  - (2) The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan.
  - (3) Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.

- (4) A program of implementation measures including regulations, programs, public works projects, and financing measures, necessary to carry out items (1), (2), and (3).

- (b) The Specific Plan shall include a statement of the relationship of the Specific Plan to the General Plan.

This Specific Plan includes each of the required elements listed above and establishes the essential link between the policies of The Ontario Plan's Policy Plan and the MERRILL COMMERCE CENTER property. All future development plans and implementing construction activities within the MERRILL COMMERCE CENTER are required to be consistent with the requirements set forth in this Specific Plan and with all other applicable City regulations.

### 1.4 SEVERABILITY

This Specific Plan document enables the City of Ontario to facilitate the processing and approval of development plans and implementing permits to build out the MERRILL COMMERCE CENTER. If any regulation, condition, program, or portion of this Specific Plan is held invalid or unenforceable, such portions shall be deemed separate, distinct, and independent provisions, and the invalidity of such portions or provisions shall not affect the validity and enforceability of the remaining provisions contained herein.

### 1.5 APPROVAL PROCESS AND COMPANION ACTIONS

This Specific Plan is required to be considered by the City of Ontario Planning Commission and City Council, and adopted by ordinance of the City Council. Any future amendments thereto also require consideration by the Planning Commission and City Council, but may be adopted by ordinance of the City Council. More information on implementation procedures is contained in Chapter 7, *Implementation Plan*.

A Policy Plan (General Plan) Amendment (GPA) was processed concurrently with this Specific Plan. The GPA was a companion action to the approval of this Specific Plan that achieved consistency between the General Plan land use designations, this Specific Plan's land use designations, and zoning.

Additionally, a subdivision map will be approved by the City of Ontario for the Specific Plan area indicating the approximate boundaries and dimensions of parcels and streets and the proposed grading for the MERRILL COMMERCE CENTER. Following map recordation, the final map will become the legal document that identifies developable parcels within the Specific Plan.

Approval of the MERRILL COMMERCE CENTER Specific Plan also is accompanied by an application for the approval of a development agreement. California Government Code §§ 65864-65869.5 authorize the use of development agreements between any city, county, or city and county, with any person having a legal or equitable interest in real property for the development of the property.

In addition to this Specific Plan and the accompanying GPA, subdivision map, and development agreement, an Environmental Impact Report (EIR) was certified in compliance with the California

Environmental Quality Act (CEQA) to serve as the project-wide environmental assessment document. Together, this Specific Plan and the environmental mitigation measures contained in the accompanying EIR's Mitigation Monitoring and Reporting Program (MMRP) provide a path to develop the property taking into account all applicable goals, objectives, government requirements, and environmental regulations.

## 1.6 RELATIONSHIP TO THE ONTARIO PLAN

On January 26, 2010, the City adopted The Ontario Plan, which serves as the City's business plan and includes a long-term vision and a principle-based Policy Plan (General Plan). The Ontario Plan establishes the direction and vision for the City's future and provides a guidance system to shape the Ontario community of tomorrow. The Ontario Plan provides policies to accommodate change over a 30-year period commencing in 2010, the beginning of the planning period. The Ontario Plan consists of a six-part Component Framework: 1) Vision, 2) Governance Manual, 3) Policy Plan, 4) City Council Priorities, 5) Implementation, and 6) Tracking and Feedback.

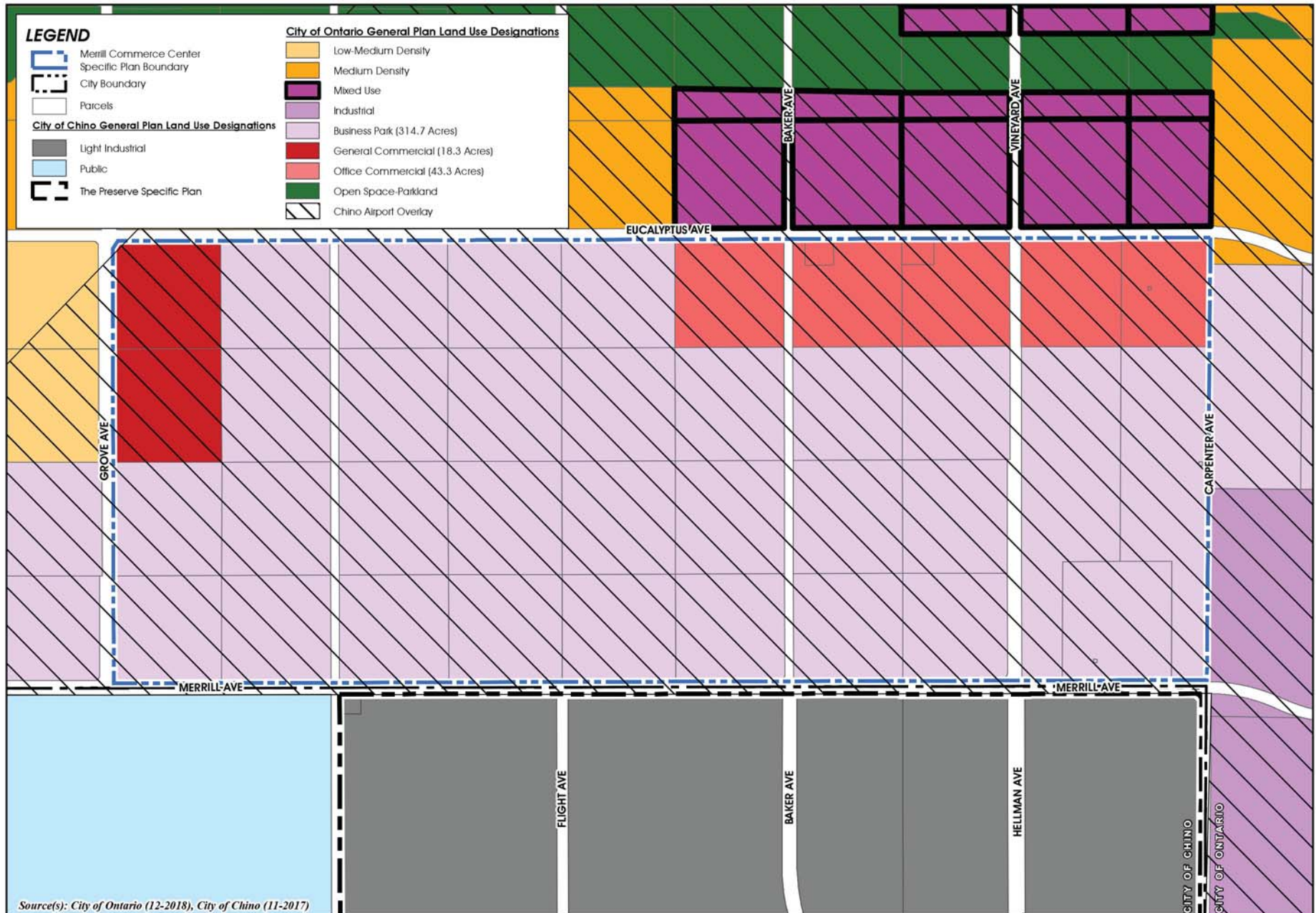
This Specific Plan defines the methods and requirements for the development of the MERRILL COMMERCE CENTER property to ensure that applicable policies from The Ontario Plan and its Policy Plan are implemented and, also, to ensure that development envisioned by this Specific Plan is consistent with applicable provisions of The Ontario Plan. The Ontario Plan sets forth long-term goals for the City's growth and development as mandated by State law. The Policy Plan is a long-term policy document that covers the topics of land use, housing, parks and recreation, environmental resources, community economics, safety, mobility, community design, and social resources.

As shown on Figure 1-1, *Existing General Plan Land Use Designations*, the Policy Plan component of The Ontario Plan designates approximately 314.7 acres of the 376.3-acre MERRILL COMMERCE CENTER property for "Business Park" land uses, 43.3 acres for "Office Commercial" land uses, and 18.3 acres for "General Commercial" land uses. The site also is located in the Chino Airport Overlay.

As shown Figure 1-2, *Proposed General Plan Land Use Designations*, this Specific Plan provides for a mix of business park and industrial uses that are generally consistent with The Ontario Plan's vision for the property but requires a GPA to change the site's land uses to the new land use designations of "Business Park" (55.1 acres) and "Industrial" (292.8 acres) in order to reflect the uses, development standards, design guidelines and implementation procedures described herein. The MERRILL COMMERCE CENTER Specific Plan would further the General Plan vision to transition areas formerly used for agricultural activities in Ontario Ranch to new development that would expand and diversify the City's economic base. Please refer also to Specific Plan Appendix A, *Policy Plan Consistency*, for more information.

As shown on Figure 1-3, *Existing Zoning Designations*, the City zoned the entire Specific Plan property "SP, Specific Plan" with an "AG, Agricultural" overlay. The zoning designation of AG-Specific Plan requires that a Specific Plan be approved by the Ontario City Council to guide the development of the property and to implement the goals and policies of The Policy Plan component of The Ontario Plan. As shown on Figure 1-3, *Proposed Zoning Designations*, a Zone Change is required to amend the City of Ontario's Zoning Map to change the site's zoning designation to "MERRILL COMMERCE CENTER Specific Plan" to allow for the development of a variety of industrial and business park uses.

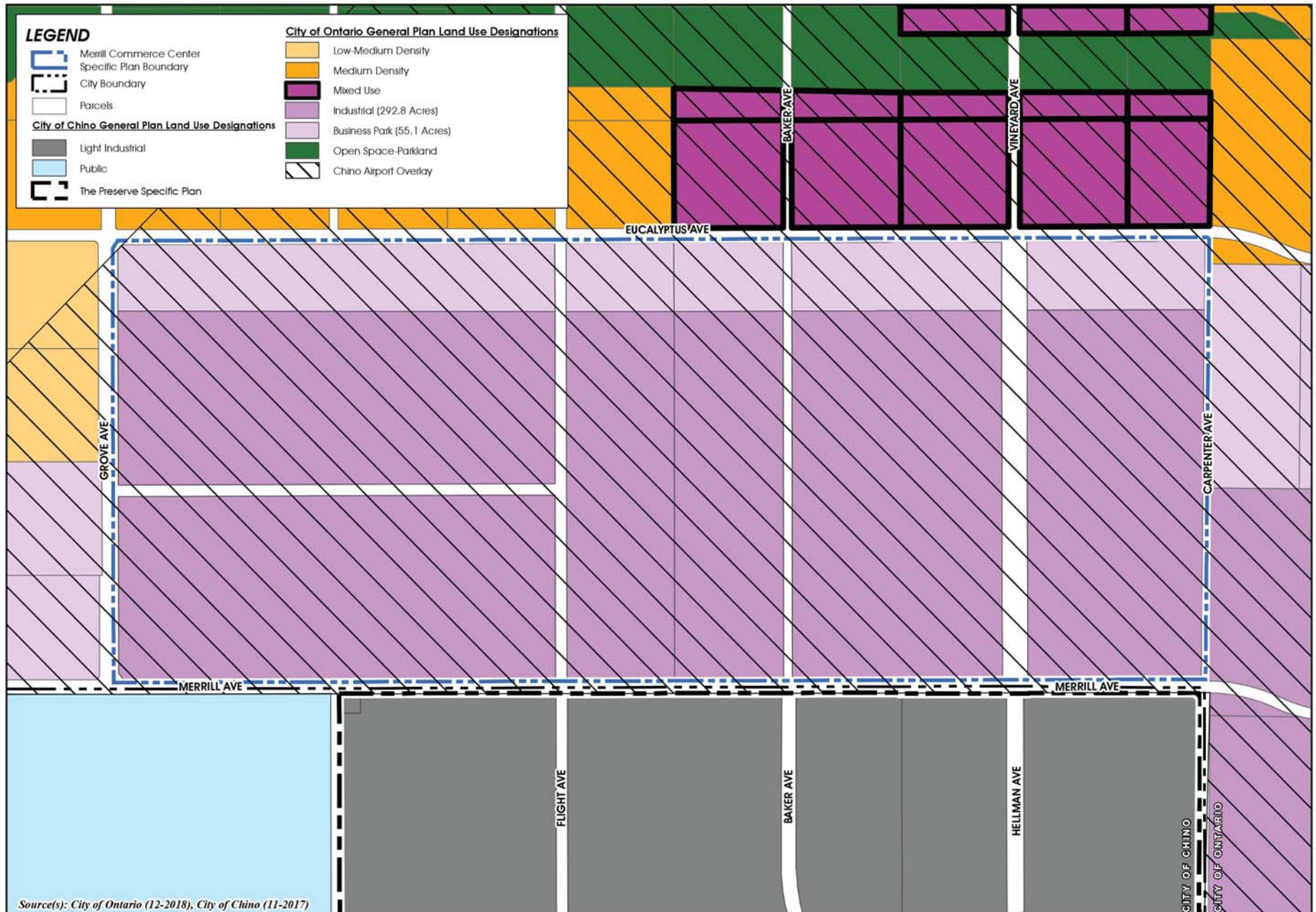
Pursuant to the City of Ontario's adoption of the MERRILL COMMERCE CENTER Specific Plan by ordinance, the Specific Plan will take precedence over the City's Development Code. In instances where the Specific Plan is silent on development standards, the City's Development Code shall prevail.



**Existing General Plan Land Use Designations**

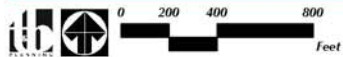
Figure 1-1

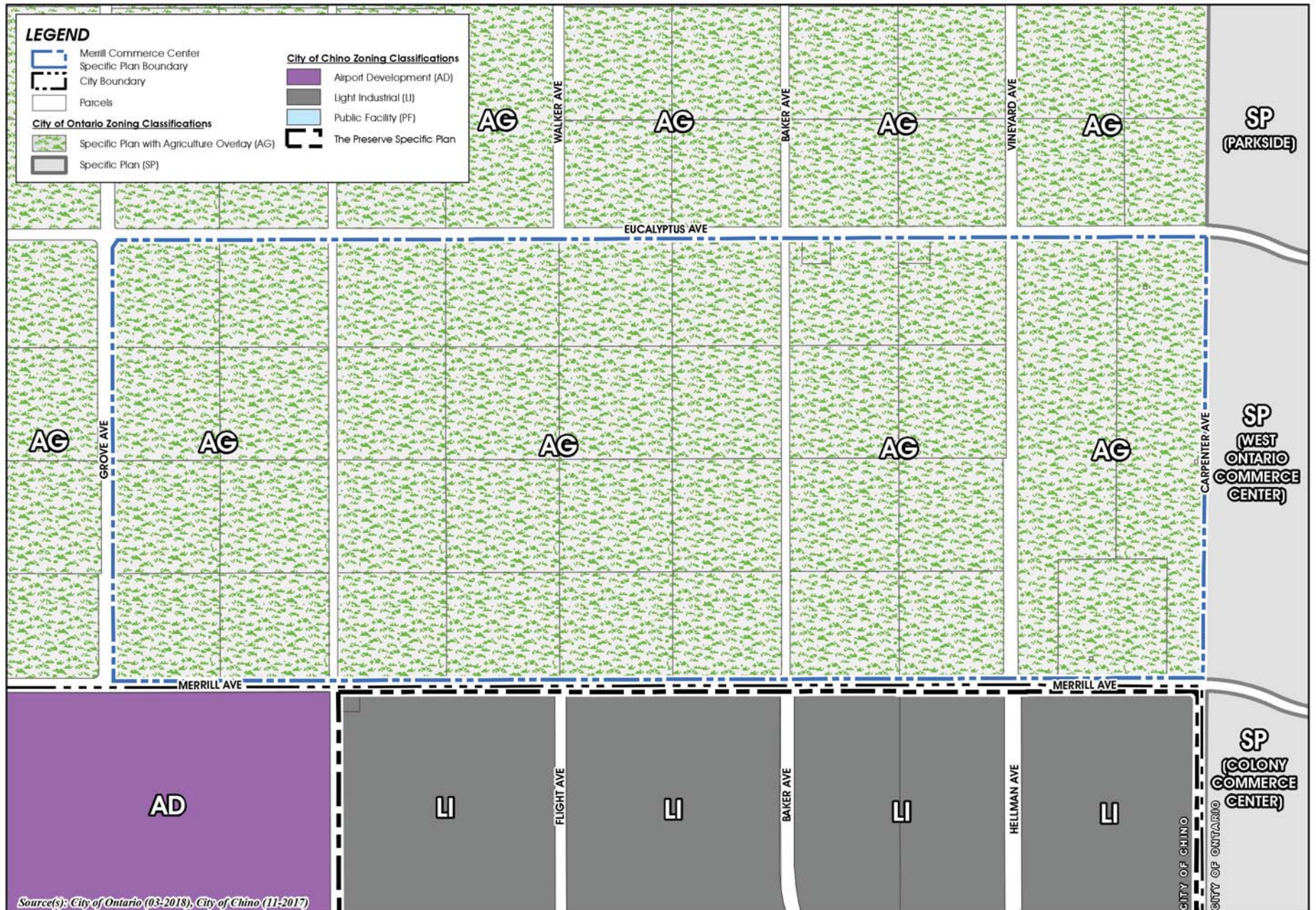




**Proposed General Plan Land Use Designations**

Figure 1-2

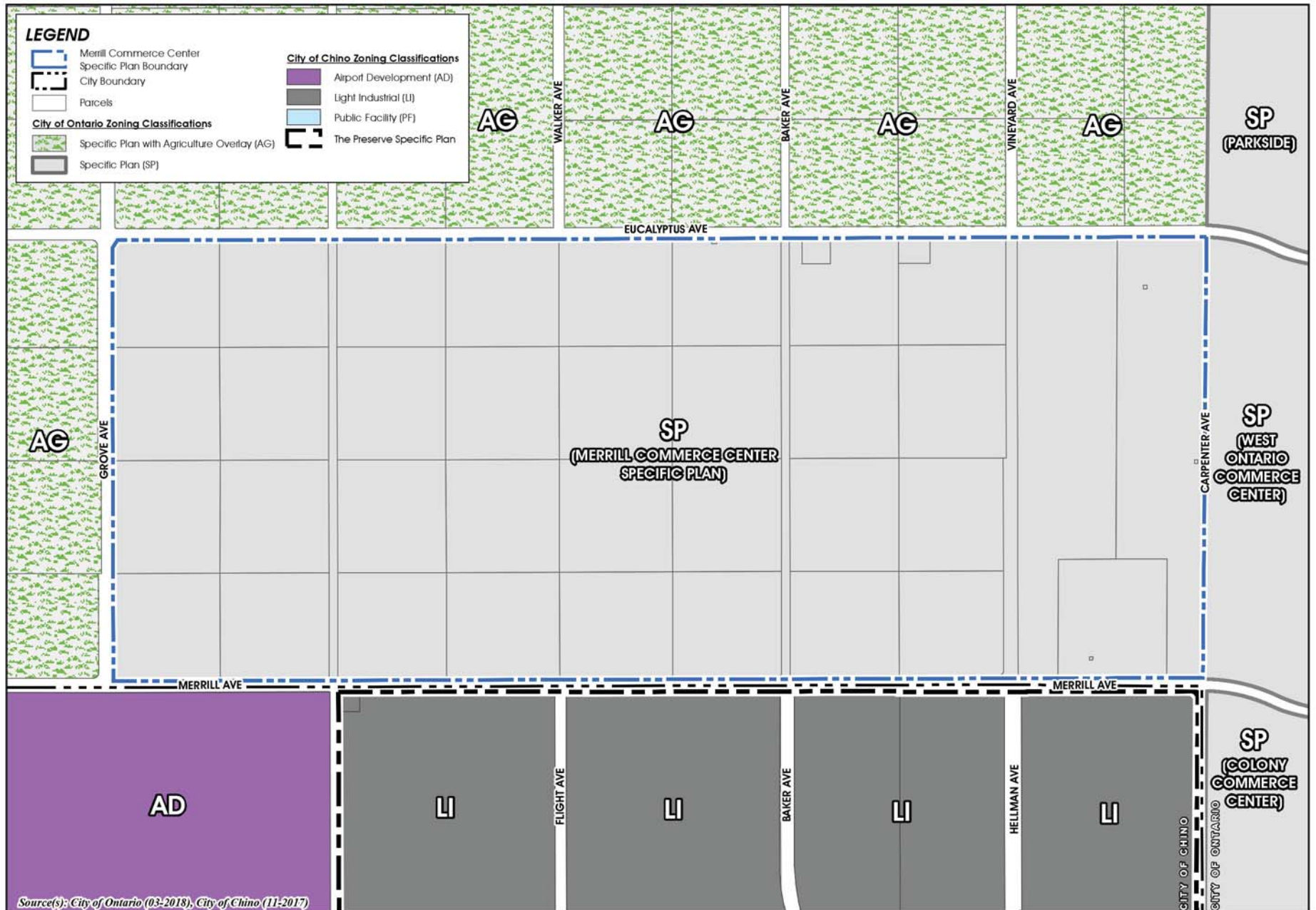




**Existing Zoning Classifications**

Figure 1-3





**Proposed Zoning Classifications**

Figure 1-4







# EXISTING CONDITIONS

## *CHAPTER 2*

2.1 EXISTING ASSESSOR PARCEL NUMBERS

2.2 EXISTING LAND USE

2.3 SURROUNDING LAND USES

2.4 WILLAMSON ACT

2.5 EXISTING CIRCULATION AND ACCESS

2.6 EXISTING PHYSICAL SITE CONDITIONS

## CHAPTER 2 - EXISTING CONDITIONS

### 2.1 EXISTING ASSESSOR PARCEL NUMBERS

At the time this Specific Plan was prepared (2020), the MERRILL COMMERCE CENTER Specific Plan area consists of the following Assessor Parcel Numbers (APNs), as listed on Table 2-1, *Existing APN List*, and shown on Figure 2-1, *Existing APNs*.

**Table 2-1 Existing APN List**

Planning Areas 1, 1A, and 2	
1054-111-01	1054-201-01
1054-111-02	1054-211-01
1054-121-01	1054-221-01
1054-121-02	1054-221-02
1054-131-01	1054-331-01
1054-141-01	1054-331-02
1054-151-01	1054-341-01
1054-161-01	1054-351-01
Planning Areas 3 and 3A	
1054-131-02	1054-211-02
1054-141-02	1054-341-02
Planning Areas 4 and 4A	
1054-151-02	1054-201-02
1054-161-02	1054-351-02
1054-161-03	
Planning Areas 5 and 5A	
1054-171-01	1054-181-02
1054-171-02	1054-361-01
1054-171-03	1054-361-02
1054-171-04	1054-161-02

1054-181-01	
Planning Areas 6 and 6A	
0218-261-27	0218-261-34
0218-261-28	0218-261-35
0218-261-29	0218-261-37

### 2.2 EXISTING LAND USE

The Specific Plan property was formerly used for agricultural purposes, primarily for dairy farming. At the time this Specific Plan was prepared (2020), the property contained agricultural dairy operations, several rural residential homes, dairy farm buildings, bio-retention basins for the dairy farms, and other ancillary facilities that occupy areas not in active dairy farm use. The easternmost part of the Specific Plan property west of Carpenter Avenue contained commercial/industrial structures and a truck trailer storage lot.

### 2.3 SURROUNDING LAND USES

As shown on Figure 2-3, *Surrounding Land Uses*, the Specific Plan property is bound by Eucalyptus Avenue and dairy farming activities and agricultural land uses to the north; Merrill Avenue, agricultural land uses, logistics warehouses, the Chino Airport, and vacant land to the south; Grove Avenue, and dairy farming activities to the west; Carpenter Avenue and dairy farming and agricultural land uses, and properties under development for warehouse uses to the east. Merrill Avenue, which forms the Specific Plan’s southern boundary, is the dividing line between the City of Ontario (north of Merrill Avenue) and the City of Chino (south of Merrill Avenue).

## 2.4 WILLIAMSON ACT

At the time this Specific Plan was prepared (2020), there is an active Williamson Act Contract (Contract #69-147, initiated in 1973) on APN 0218-261-35, a 29.05-acre property. Another Williamson Act Contract (#70-167 initiated in 1970) appears on title for APNs 1054-151-02, 1054-16102, 1054-161-03, 1054-201-02 and 1054-351-02. However, a notice of non-renewal dated September 14, 2017, and recorded, starting the process to terminate this Contract is effective January 1, 2018. As one of the discretionary actions associated with the MERRILL COMMERCE CENTER Specific Plan, these existing Williamson Act Contracts will be cancelled. Cancellation would comply with provisions and requirements identified at Government Code (GC) §51280 et seq. The City would be required to make the required statutory findings (GC §51282(a)). The landowner would be required to pay the requisite cancellation fee.

## 2.5 EXISTING CIRCULATION AND ACCESS

### 2.5.1 Regional Circulation

Interstate 15 (I-15) is located approximately 7.0 miles east of the MERRILL COMMERCE CENTER Specific Plan. The Specific Plan area is accessible to and from I-15 via the Cantu-Galleano Ranch Road and Limonite Avenue on- and off-ramps. State Route 60 (SR-60) is located approximately 2.7 miles north of the Specific Plan area, with access to and from SR-60 provided by the Grove Avenue (abuts the Specific Plan to the west) and Archibald Avenue on- and off-ramps. State Route 83 (SR-83/Euclid Avenue) is located approximately 1.8 miles to the west of the Specific Plan area, with access available from Merrill Avenue which abuts the Specific Plan property on the south.

### 2.5.2 Local Circulation

Access to the MERRILL COMMERCE CENTER Specific Plan area is provided from Grove Avenue, Eucalyptus Avenue, Vineyard Avenue, Baker Avenue, Carpenter Avenue, Walker Avenue, and Flight Avenue. Merrill Avenue, Edison Avenue, and Euclid Avenue are City of Ontario designated truck routes that provide truck access to the MERRILL COMMERCE CENTER. Additionally, the City of Chino designates Carpenter Avenue, Walker Avenue, and Flight Avenue as truck routes, which provide access to MERRILL COMMERCE CENTER from the south.

Merrill Avenue abuts the Specific Plan on the south and consisted of two paved travel lanes at the time this Specific Plan was prepared. The Ontario Policy Plan designates Merrill Avenue as a 4-lane Collector Street.

Eucalyptus Avenue abuts the Specific Plan to the north, is designated as a 4-lane Collector Street by the Policy Plan, and consisted of two paved travel lanes at the time this Specific Plan was prepared.

Grove Avenue abuts the Specific Plan to the west and consisted of two paved travel lanes at the time this Specific Plan was prepared. The Policy Plan designates Grove Avenue as a 4-lane Principal Arterial.

Walker Avenue is a north-south oriented roadway that traverses the west-central portion of the Specific Plan and consisted of two paved travel lanes north of its intersection with Eucalyptus at the time this Specific Plan was prepared. The Policy Plan designates Walker Avenue as a 2-lane Collector Street.

Baker Avenue consisted of two paved travel lanes south of its intersection with Merrill Avenue at the time this Specific Plan was

prepared. Figure M-2, *Functional Roadway Classification Plan*, of the Policy Plan does not show a roadway classification for Baker Avenue. The Specific Plan will construct the on-site segment of Baker Avenue to be consistent with the segment of Baker Avenue located south of Merrill Avenue within the City of Chino.

Vineyard Avenue is a north-south oriented street that traverses the easterly portion of the Specific Plan that is designated as a 6-lane Principal Arterial by the Policy Plan. At the time this Specific Plan was prepared, no segments of Vineyard Avenue on-site or immediately abutting the Specific Plan had been constructed.

Carpenter Avenue abuts the Specific Plan to the east, with the segment of Carpenter Avenue that abuts the Specific Plan consisting of an unstriped semi-paved single travel lane. Figure M-2, *Functional Roadway Classification Plan*, of the Policy Plan does not depict a roadway classification for Carpenter Avenue. The Specific Plan will construct frontage improvements along Carpenter Avenue to be consistent with the segment of Carpenter Avenue located south of Merrill Avenue, at its ultimate right-of-way build-out.

## 2.6 EXISTING PHYSICAL SITE CONDITIONS

### 2.6.1 Topography

The MERRILL COMMERCE CENTER Specific Plan property is relatively flat and gently falls to the south, with elevations ranging from approximately 670 feet above mean sea level (AMSL) in the north to approximately 645 feet AMSL in the south. The existing topographic conditions for the Specific Plan property are illustrated on Figure 2-4, *USGS Topographic Map*.

### 2.6.2 Hydrology

Due to use of the majority of the Specific Plan property for agricultural and dairy farm operations, only a limited portion of the property was covered with impervious surfaces at the time this Specific Plan was prepared. Stormwater mostly percolated through on-site soils and did not result in high volumes of surface runoff that are associated with urban environments having predominantly impervious surfaces. The storm drain system throughout the Specific Plan property was generally unimproved and consisted primarily of open earthen swales along area roadways or curbed roadway surfaces. The MERRILL COMMERCE CENTER Specific Plan EIR includes additional detailed hydrology information for the property.

The MERRILL COMMERCE CENTER Specific Plan is located within the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map (FIRM) Numbers 06071C9375H and 06071C9335H. As shown on FIRM No. 06071C9375H, a large portion of the eastern area of the Specific Plan property is located within Flood Zone X, indicating it is protected by the Cucamonga Creek flood channel levees from hazards associated with a 1% annual chance flood event.

### 2.6.3 Geology and Soils

The Specific Plan property is located in the Upper Santa Ana Valley, a broad alluvial and fluvial plain located within the Los Angeles, Orange, Riverside, and San Bernardino Counties. The Upper Santa Ana Valley is a southwesterly draining basin bounded by the San Gabriel Mountains and San Bernardino Mountains on the north and east, the Puente and San Jose Hills on the west and the Jurupa Hills and the Santa Ana Mountains to the south.

Subsurface lithology in the general vicinity is mapped as recent-age alluvium and colluvium. Soil types at the Specific Plan property consist of cattle manure, artificial fill soils, and native alluvial soils.

According to the United States Department of Agriculture (USDA) Web Soil Survey database, the majority of the Specific Plan property is mapped as containing Delhi fine sand, with the remainder of the soils mapped as Hilmar loamy fine sand, Tujunga loamy sand, and Chino silt loam.

The property is not underlain by any seismic fault lines, with the nearest fault (Chino Fault) occurring 4.3 miles to the southwest.

#### **2.6.4 Vegetation and Biological Resources**

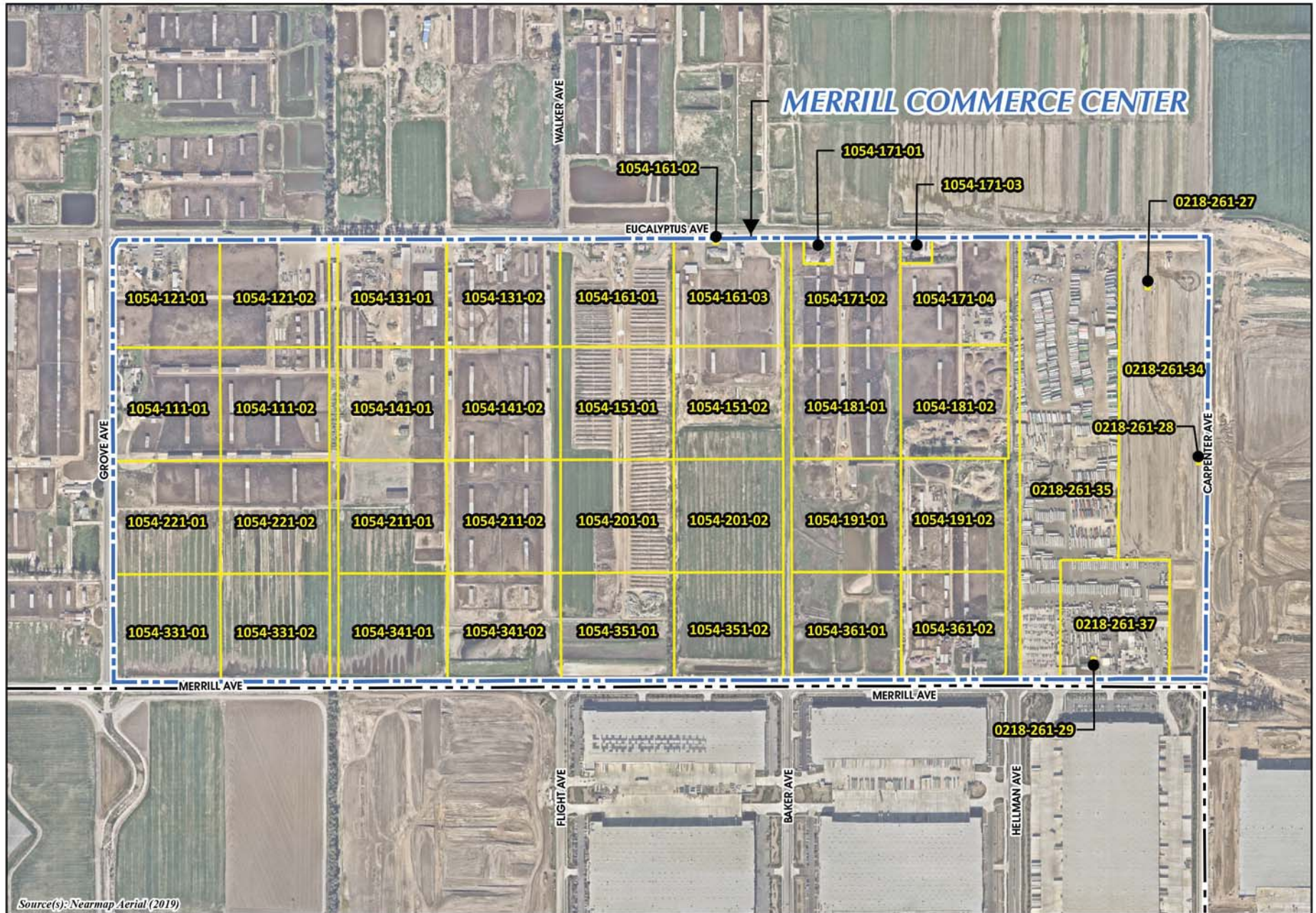
The Specific Plan property has been extensively used for agricultural operations (including dairy and row crop uses), residential uses, and a trucking operation, and therefore consisted of entirely disturbed/developed and agricultural vegetation/land cover types with little to no native vegetation at the time this specific plan was prepared. The MERRILL COMMERCE CENTER Specific Plan EIR includes a detailed evaluation of vegetation and biological resources.

#### **2.6.5 Existing Ground Water Wells**

In compliance with the Chino Basin Watermaster's Well Procedure for Developers, documentation that a well use/destruction plan and schedule for all existing private/agricultural wells is in-process shall be submitted to the City of Ontario prior to any construction activities. If a private well is actively used for water supply, the Developer shall submit a plan to abandon such well and connect users to the City's water system (residential to the domestic water system and agricultural to the recycled water system) when available.

Wells shall be destroyed/abandoned per the California Water Resource Guidelines and require permitting from the County Health Department. The location of existing water wells are shown in Figure 2-5, *Existing Water Well Locations*.

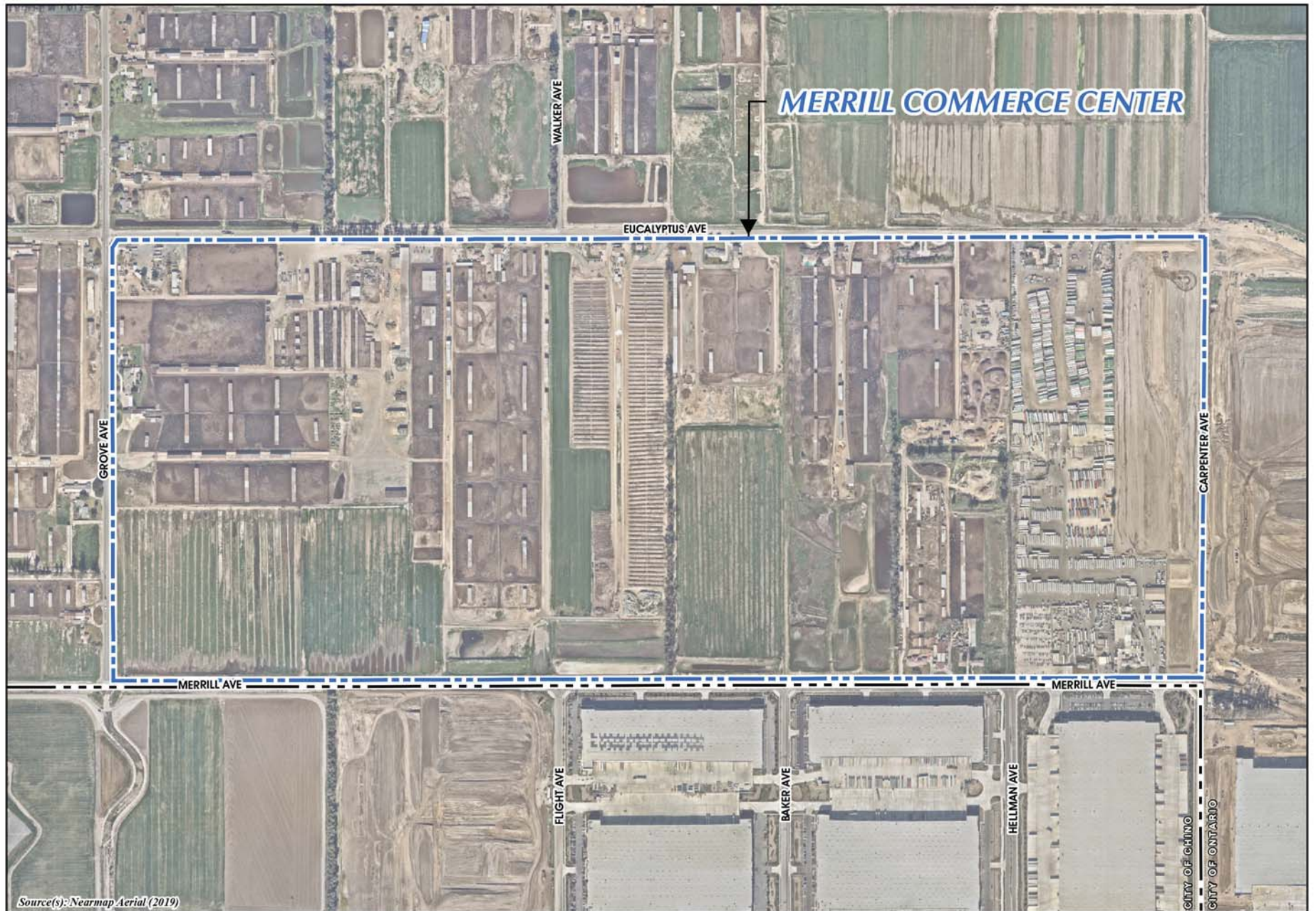
Documentation that the well abandonment process is underway shall be provided to the Development Engineering Department and the Utilities Engineering Department at the time of a grading permit and/or building permit being issued. If the Developer proposes temporary use of an existing agricultural well for purposes other than agriculture, such as grading, dust control, etc., the Developer shall make a formal request to the City of Ontario for such use prior to the issuance of permits for any construction activity. Upon approval, the Developer shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by the agreement.



Existing APNs

Figure 2-1



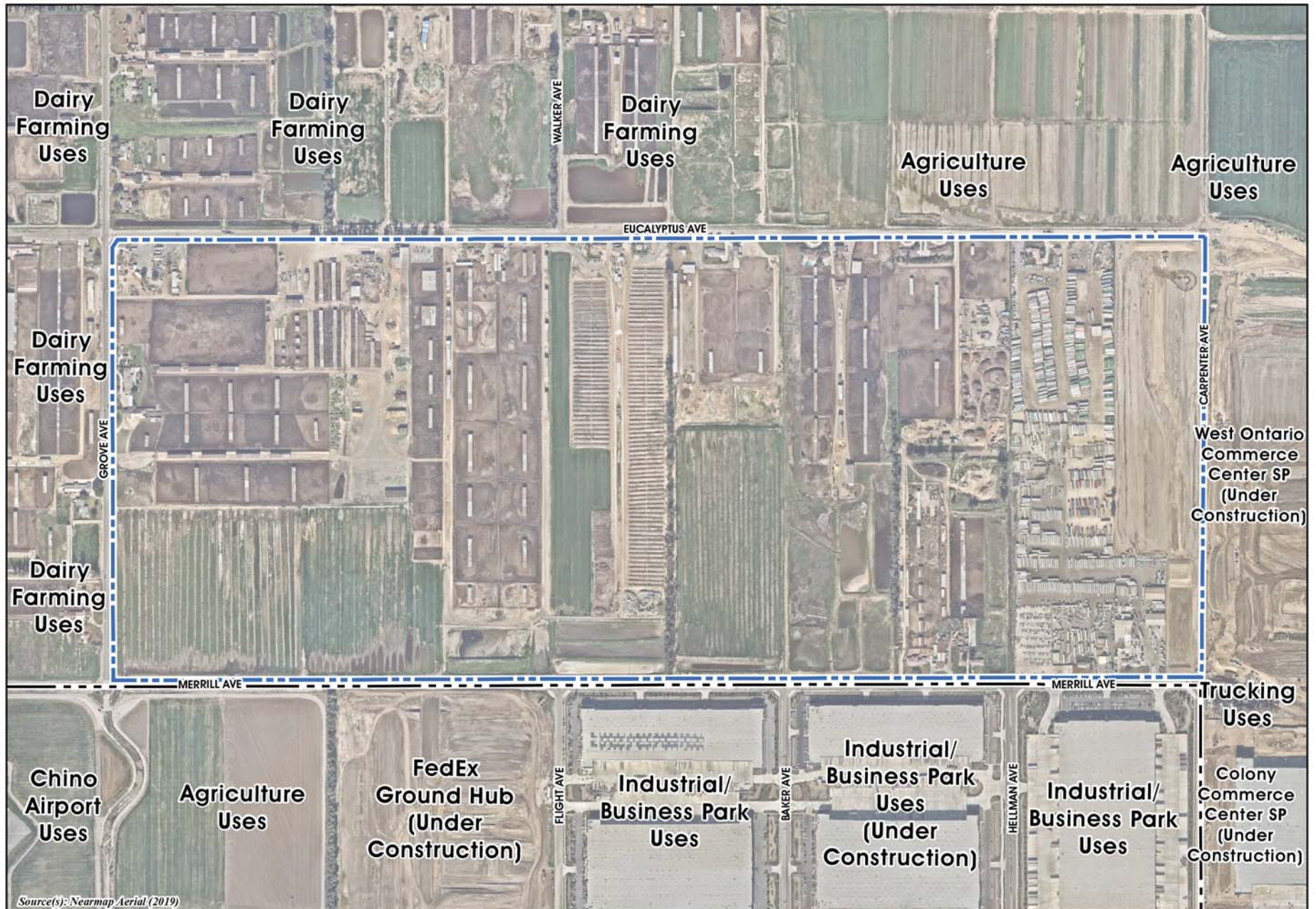


Source(s): Nearmap Aerial (2019)

Aerial Photograph

Figure 2-2



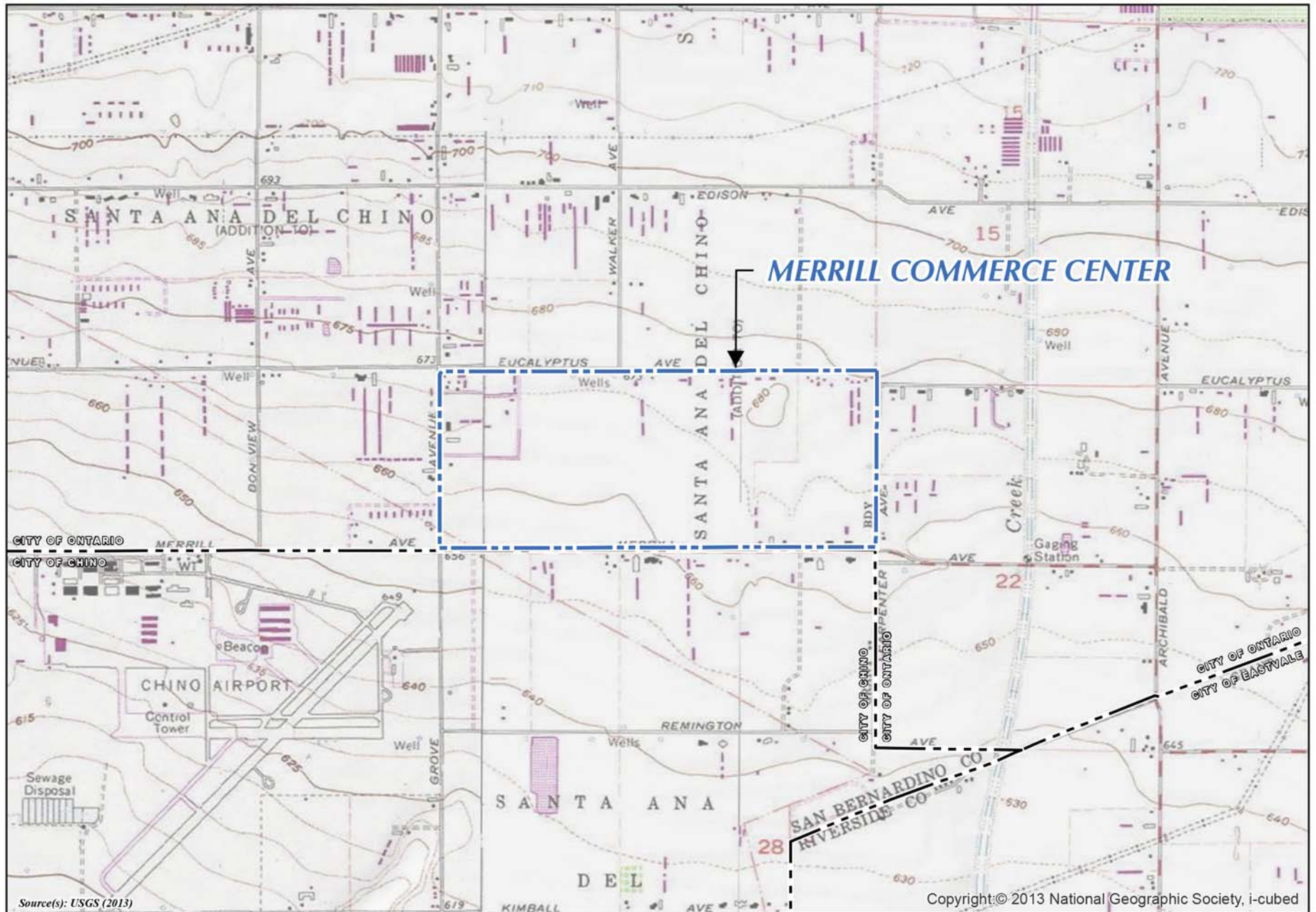


Surrounding Land Uses

Figure 2-3



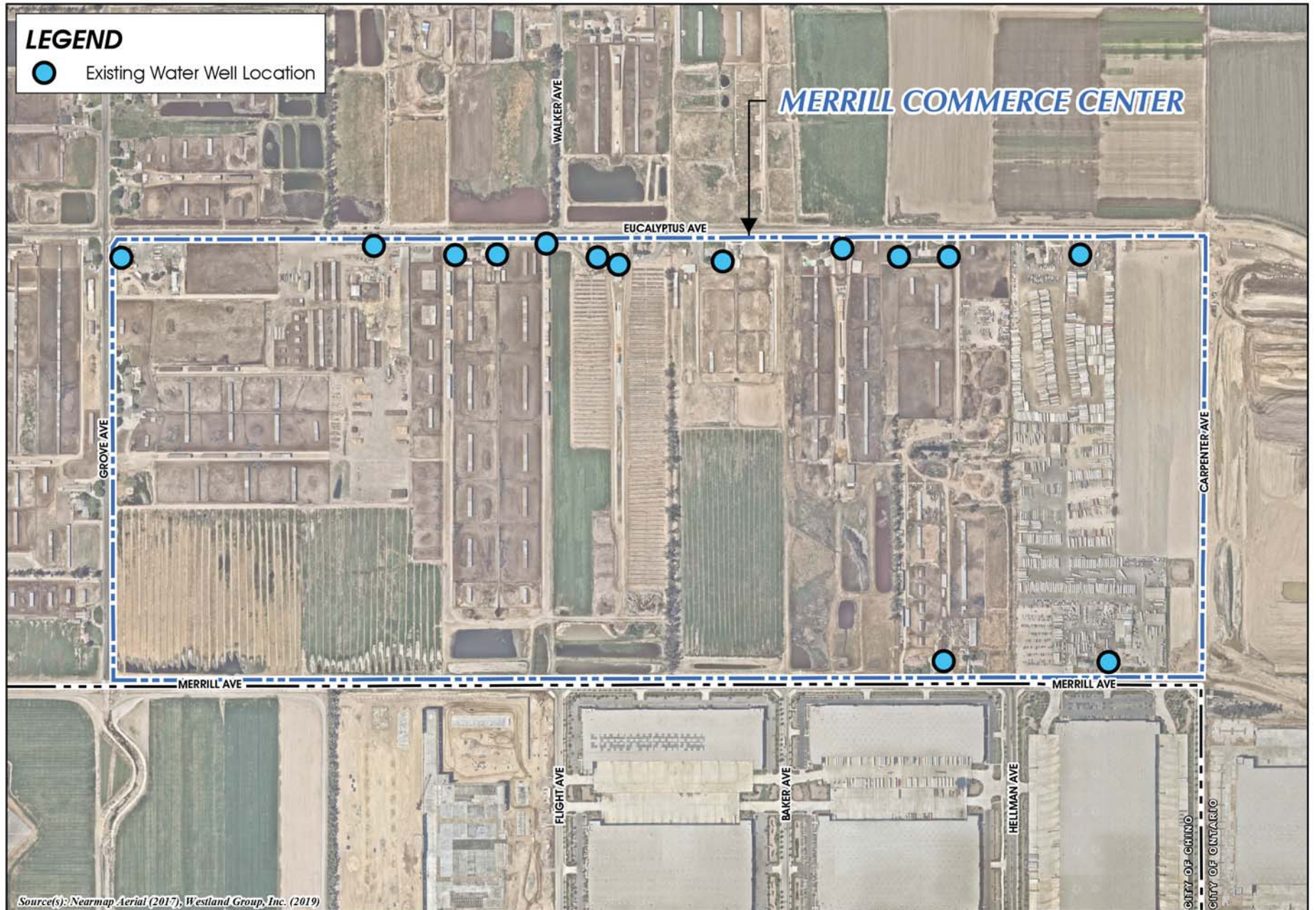




USGS Topographic Map

Figure 2-4





Existing Water Well Locations

Figure 2-5





**LAND USE PLAN**

***CHAPTER 3***

3.1 VISION

3.2 LAND USE PLAN

## CHAPTER 3 - LAND USE PLAN

### 3.1 VISION

The MERRILL COMMERCE CENTER is designed as a contemporary employment center laid out in a master-planned, campus-like setting. Located in the southern section of the City of Ontario in the heart of the Inland Empire, the MERRILL COMMERCE CENTER is positioned to attract a variety of business types and sizes, ranging from local enterprises to international corporations. With distant views of the San Gabriel Mountains to the north, the Chino Hills to the southwest, and the Santa Ana Mountains to the south, the MERRILL COMMERCE CENTER is envisioned as an attractive place where businesses can prosper, attract economic investment, and provide goods, services, and job opportunities to the surrounding community and region.

### 3.2 LAND USE PLAN

This Specific Plan establishes two land use designations: Industrial and Business Park. For planning purposes, the 376.3-acre Specific Plan area is divided into 11 planning areas. A “planning area” is a specific geographic area to which development standards are uniformly applied. Figure 3-1, *Conceptual Land Use Plan*, depicts the physical arrangement of the planning areas and the major roads within and abutting the Specific Plan area.

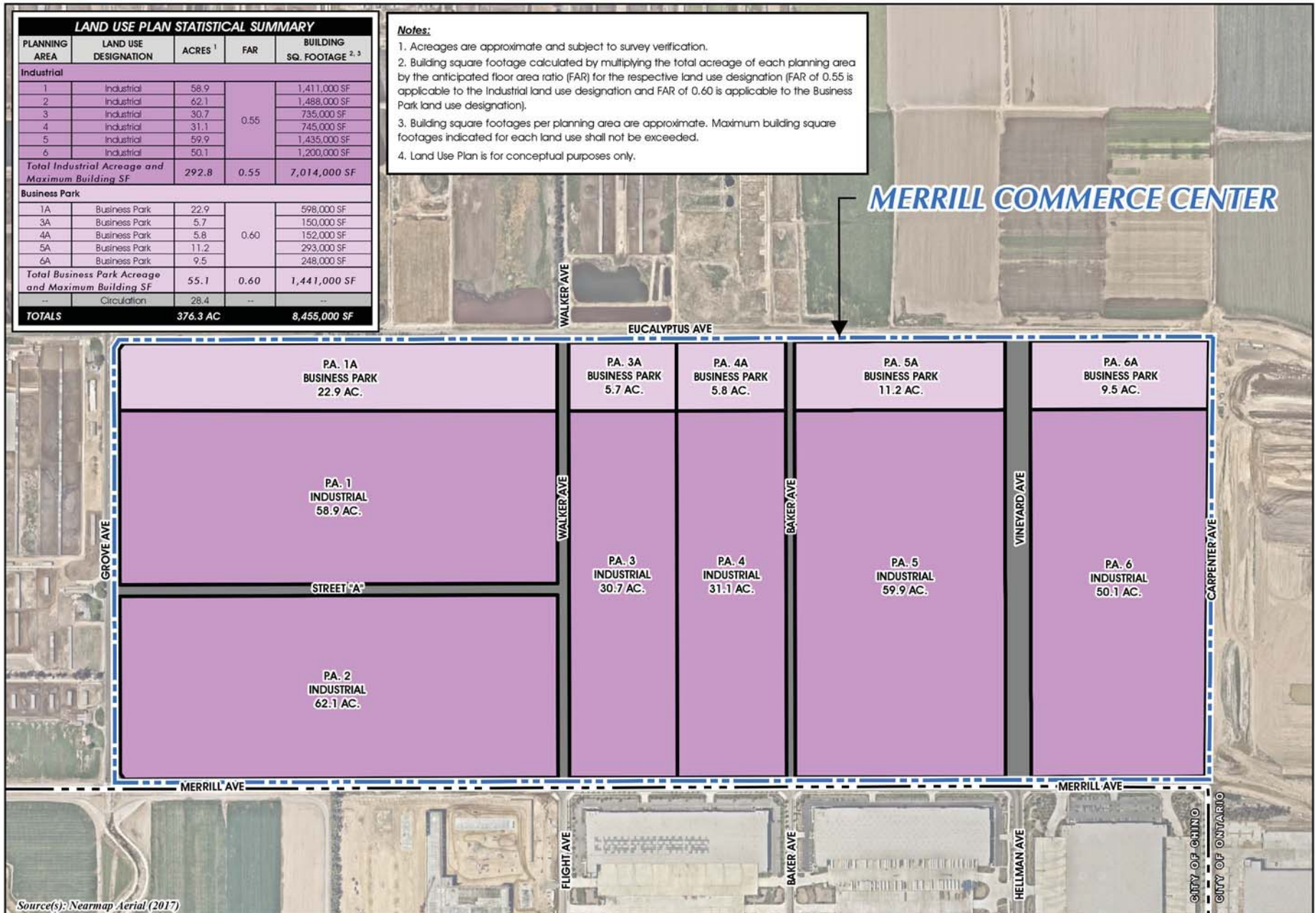
Table 3-1, *Land Use Plan Statistical Summary*, lists each planning area and their respective land use designation, acreage, and target development intensity (amount of building square footage targeted for each planning area). The maximum building square footage in the Specific Plan area is 8,455,000 s.f.

Table 3-1 Land Use Plan Statistical Summary

PLANNING AREA	LAND USE DESIGNATION	ACRES <sup>1</sup>	FAR	BUILDING SQ. FOOTAGE <sup>2,3</sup>
<b>Industrial</b>				
1	Industrial	58.9	0.55	1,411,000 SF
2	Industrial	62.1		1,488,000 SF
3	Industrial	30.7		735,000 SF
4	Industrial	31.1		745,000 SF
5	Industrial	59.9		1,435,000 SF
6	Industrial	50.1		1,200,000 SF
<b>Total Industrial Acreage and Maximum Building SF</b>		<b>292.8</b>	<b>0.55</b>	<b>7,014,000 SF</b>
<b>Business Park</b>				
1A	Business Park	22.9	0.60	598,000 SF
3A	Business Park	5.7		150,000 SF
4A	Business Park	5.8		152,000 SF
5A	Business Park	11.2		293,000 SF
6A	Business Park	9.5		248,000 SF
<b>Total Business Park Acreage and Maximum Building SF</b>		<b>55.1</b>	<b>0.60</b>	<b>1,441,000 SF</b>
--	Circulation	28.4	--	--
<b>TOTALS</b>		<b>376.3 AC</b>		<b>8,455,000 SF</b>

Notes:

1. Acreages are approximate and subject to survey verification.
2. Building square footage calculated by multiplying the total acreage of each planning area by the anticipated floor area ratio (FAR) for the respective land use designation (FAR of 0.55 is applicable to the Industrial land use designation and FAR of 0.60 is applicable to the Business Park land use designation).
3. Building square footages per planning area are approximate. Maximum building square footages indicated for each land use category (maximum of 7,104,000 s.f. for Industrial and maximum of 1,441,000 s.f. for Business Park) shall not be exceeded.



Conceptual Land Use Plan

Figure 3-1



### 3.2.1 Industrial Planning Areas (292.8 Acres)

Six planning areas (Planning Areas 1, 2, 3, 4, 5 and 6) covering a total of 292.8 acres are designated “Industrial” and located in the southerly portion of the Specific Plan area. Up to 7,014,000 square(sq.) feet of building space is permitted across Planning Areas 1, 2, 3, 4, 5 and 6.

Industrial buildings are envisioned to range from approximately 100,000 sq. feet in size up to 1,500,000 sq. feet in size and house users such as general light industrial, manufacturing, warehouse/storage, fulfillment center, and e-commerce operations. planning area boundaries.

To facilitate vehicular access to and from the uses in these planning areas, Street “A” provides an interior connection between Grove Avenue and Walker Avenue, with its ultimate alignment to be determined and designed in conjunction with implementing projects. The other perimeter and interior public streets form a grid pattern, as called for by the Ontario Policy Plan.

### 3.2.2 Business Park Planning Areas (55.1 Acres)

Encompassing 55.1 acres in the northerly portion of the Specific Plan area along its frontage with Eucalyptus Avenue, Planning Areas 1A, 3A, 4A, 5A and 6A are designated “Business Park.” Up to 1,441,000 sq. feet of building space is permitted across Planning Areas 1A, 3A, 4A, 5A and 6A.

The buildings constructed in Business Park planning areas are envisioned to be smaller than 150,000 sq. feet, oriented toward Eucalyptus Avenue, and primarily provide for merchant wholesalers, professional services, professional office, small-scale warehousing/storage, and research and development uses. Primary vehicular access is from Eucalyptus Avenue and private driveways interior to the planning areas.

*Refer to Section 5, Development Regulations, for the specific land use and development standards applicable to each planning area, and Section 6, Design Guidelines, for information about architecture, landscaping, lighting, and signage.*



# INFRASTRUCTURE PLAN

## *CHAPTER 4*

4.1 CIRCULATION AND ACCESS

4.2 UTILITY INFRASTRUCTURE PLAN

4.3 CONCEPTUAL GRADING

## CHAPTER 4 - INFRASTRUCTURE PLAN

### 4.1 CIRCULATION AND ACCESS PLAN

The MERRILL COMMERCE CENTER Circulation and Access Plan provides direct, safe, and convenient access for visitors, employees, and goods movement to and from the Specific Plan's 11 planning areas. Components of the Specific Plan's Circulation and Access Plan are discussed and illustrated on the following pages.

#### 4.1.1 Vehicular Circulation

Direct access to the MERRILL COMMERCE CENTER property is provided by several roadways. Merrill Avenue forms the Specific Plan's southern boundary and Eucalyptus Avenue forms the northern boundary. Grove Avenue forms the Specific Plan's western boundary and Carpenter Avenue forms the eastern boundary. Baker Avenue, Walker Avenue, and Vineyard Avenue traverse through the middle of the Specific Plan area in north-south orientations.

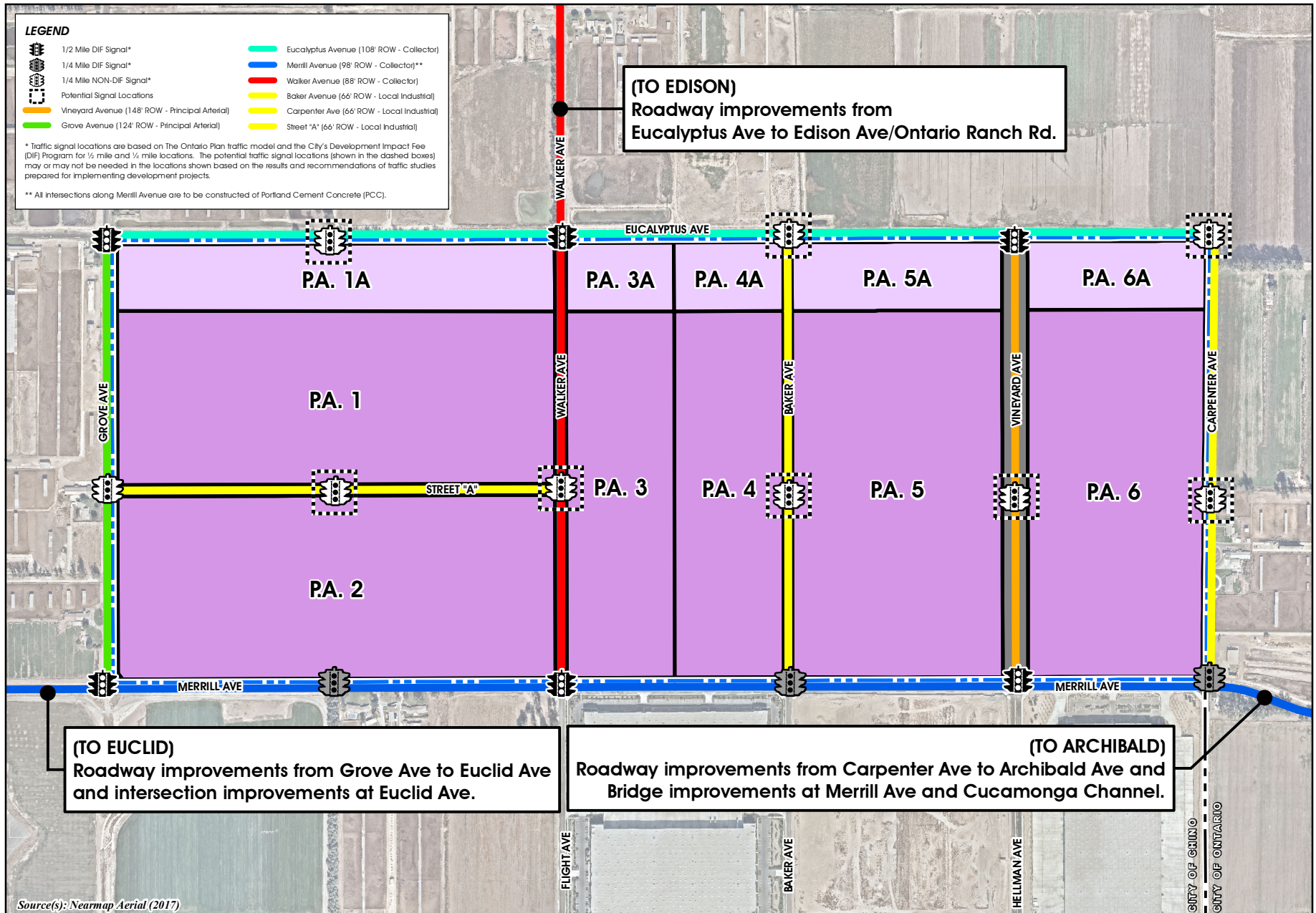
Additionally, Street "A," is an internal east-west oriented roadway positioned between Planning Areas 1 and 2 connecting Grove Avenue and Walker Avenue. The ultimate alignment of Street "A" is to be determined and designed in conjunction with implementing development plans. In addition, the installation of traffic controls (traffic signals and stop signs) and the location and orientation of private driveways serving individual buildings will be determined as part of implementing development plans.

Figure 4-1, *Conceptual Vehicular Circulation and Access Plan*, illustrates the vehicular circulation plan. Final intersection designs, intersection spacing, and traffic controls must conform to the City of Ontario engineering standards. In addition, the design of roadway

intersections and traffic controls along Merrill Avenue also must conform to the City of Chino's engineering standards, as the southern half of Merrill Avenue is located in the City of Chino.

The following pages describe and illustrate the primary components of the MERRILL COMMERCE CENTER Specific Plan's vehicular circulation network. Refer to Chapter 7, *Implementation Plan*, for additional circulation improvement standards pertaining to phasing.





Conceptual Vehicular Circulation and Access Plan

Figure 4-1

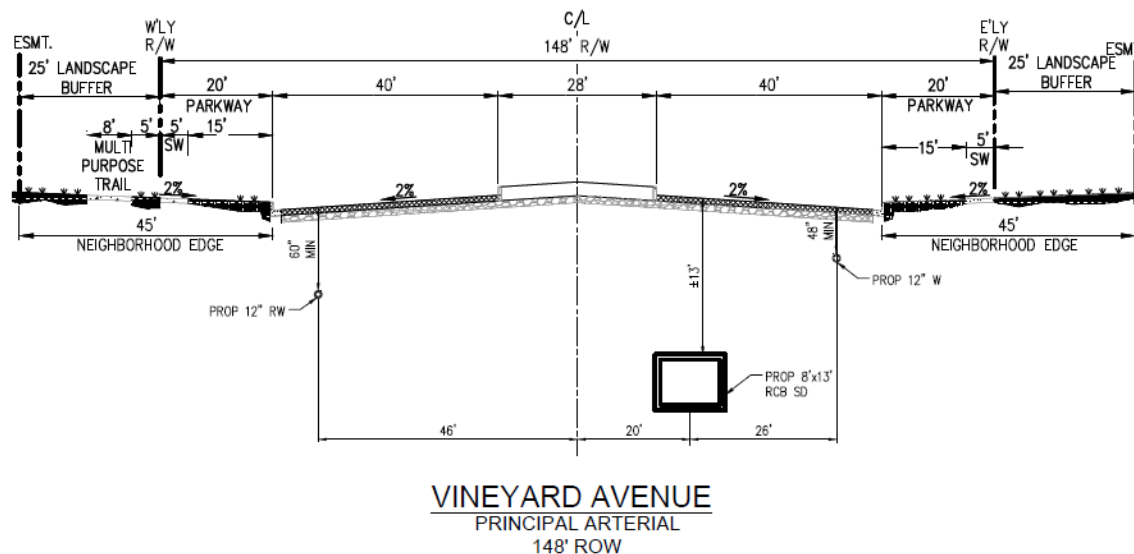


**A. Vineyard Avenue (148-foot ROW Principal Arterial)**

Vineyard Avenue forms the eastern boundary of Planning Areas 5 and 5A and forms the western boundary of Planning Areas 6 and 6A. This public roadway provides access to the Specific Plan area from off-site areas to the north and south. Along the segment of Vineyard Avenue located between Eucalyptus Avenue and Merrill Avenue (includes the segment that abuts the eastern boundary of Planning Areas 5 and 5A and the western boundary of Planning Areas 6 and 6A), the developer(s) of the MERRILL COMMERCE CENTER will construct the entirety of the 148-foot Principal Arterial, which includes 80 feet of paved roadway, a 28-foot wide raised landscaped median, and a 20-foot wide parkway on the east and west sides of the roadway. The 20-foot wide parkway on both sides of Vineyard Avenue contain 15 feet of curb-adjacent landscaping and a 5-foot wide sidewalk.

A 25-foot wide landscape buffer abuts the east and west sides of the Vineyard Avenue right-of-way, with the westerly landscape buffer including an 8-foot wide multi-purpose trail.

Traffic signals will be located at the intersection of Vineyard Avenue with Eucalyptus Avenue (1/2-mile DIF) and the intersection of Vineyard Avenue with Merrill Avenue (1/2-mile DIF). Also, a 1/4-mile non-DIF signal may be needed on Vineyard Avenue mid-way between Eucalyptus Avenue and Merrill Avenue depending on need. Traffic signal locations are subject to change based on the results and recommendations of a traffic study. All improvements to Vineyard Avenue shall comply with applicable City of Ontario requirements, including sight distance requirements. Buildings located in Planning Areas 5, 5A, 6, and 6A connect to Vineyard Avenue via direct driveway connections. Final driveway locations connecting with Vineyard Avenue will be determined in conjunction with the design of implementing development plans.



## B. Grove Avenue (124-foot ROW Principal Arterial)

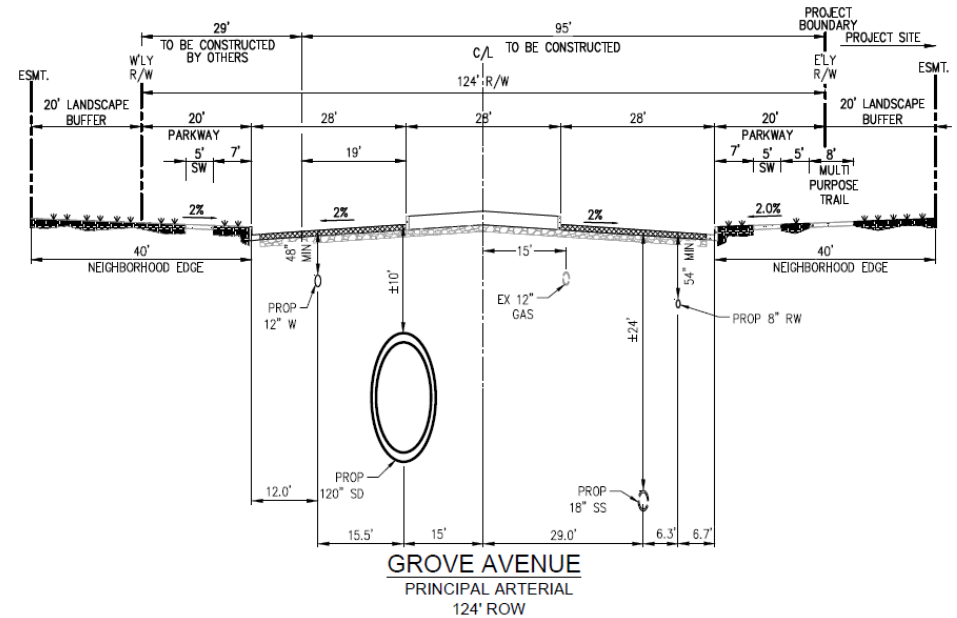
Grove Avenue forms the western boundary of Planning Areas 1, 1A, and 2. Along the segment of Grove Avenue located between Eucalyptus Avenue and Merrill Avenue, the developer(s) of the MERRILL COMMERCE CENTER will construct 95 feet of the ROW of this 124-foot wide Principal Arterial. The portions of Grove Avenue to be constructed by the MERRILL COMMERCE CENTER's developer(s) include 47 feet of paved roadway, a 28-foot-wide raised landscaped median, and a 20-foot wide parkway on the eastern side of the street that contains 7 feet of landscaping, a 5-foot sidewalk, a 5-foot landscape buffer and 3 feet of the 8-foot wide multi-purpose trail (the other 5 feet of the multi-purpose trail are located outside of the Grove Avenue ROW and will be constructed as part of the MERRILL COMMERCE CENTER). The remaining portions of the western side of the Grove Avenue ROW will be constructed by others and are anticipated to include 9 feet of roadway and curb-and-gutter improvements, and a 20-foot wide parkway containing a 7-foot landscaped parkway, 5-foot sidewalk, and 8 feet of landscaping.

A landscape buffer abuts the east and west sides of the Grove Avenue ROW, with the easterly 20-foot-wide landscape buffer to be located in the MERRILL COMMERCE CENTER and including 5 feet of an 8-foot wide multi-purpose trail (the other 3 feet of which are located within the Grove Avenue public ROW).

Traffic signals will be located at the intersections of Grove Avenue with Eucalyptus Avenue (1/2-mile DIF), Street "A" (1/4-mile non-DIF) and Merrill Avenue (1/2-mile DIF). Traffic signal locations are subject to change based on the results and recommendations of a traffic study. All improvements to Grove Avenue are required to

comply with applicable City of Ontario requirements, including sight distance requirements.

Buildings in Planning Areas 1, 1A, and 2 connect to Grove Avenue via direct driveway connections along Grove Avenue. Final driveway locations will be determined in conjunction with the design of implementing development projects, in accordance with Chapter 2.0 Access Guidelines of the City of Ontario's Traffic and Transportation Design Guidelines.



**C. Eucalyptus Avenue (108-foot ROW Collector)**

Eucalyptus Avenue forms the northern boundary of Planning Areas 1A, 3A, 4A, 5A and 6A. This public roadway provides access to the Specific Plan area from the west and east. Along the segment of Eucalyptus Avenue that abuts the northern boundary of Planning Areas 1A, 3A, 4A, 5A, and 6A, the developer(s) of the MERRILL COMMERCE CENTER will construct 79 feet of the Eucalyptus Avenue ROW including 67 feet of paved roadway, and a 12-foot-wide parkway on the south side of the street that contains 7 feet of curb-adjacent landscaping and a 5-foot-wide sidewalk. The remaining portions of the northern side of the Eucalyptus Avenue ROW will be constructed by others and will include 17 feet of roadway and a 12-foot parkway containing 7 feet of curb-adjacent landscaping and a 5-foot sidewalk.

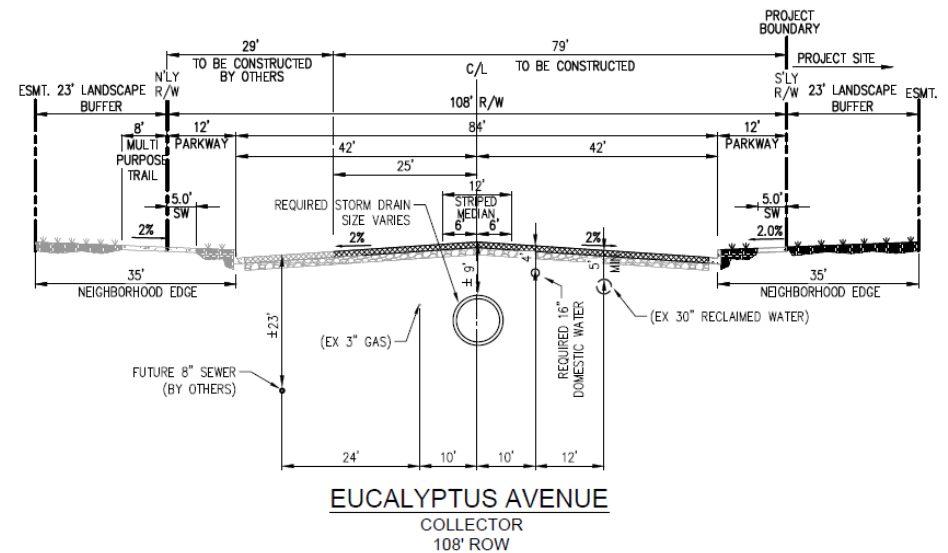
Eucalyptus Avenue is designated by the Chino Airport Compatibility Plan to satisfy open land requirements. For this reason, the median is painted and not raised and light poles and street trees are designed to maintain a clear width of about 75 feet. The light poles will be spaced at approximately 250 feet and staggered on the opposite side of the street.

Traffic signals will be located along Eucalyptus Avenue at its intersections with Grove Avenue, Walker Avenue, and Vineyard Avenue (1/2-mile DIF), and potentially at Baker Avenue, Carpenter Avenue, and mid-way between Grove Avenue and Walker Avenue (1/4-mile non-DIF) depending on need.

Traffic signal locations are subject to change based on the results and recommendations of a traffic study. All improvements to Eucalyptus Avenue are required to comply with applicable City of Ontario requirements, including sight distance requirements.

A landscape buffer abuts the north and south sides of the Eucalyptus Avenue right-of-way, with the southernly 23-foot wide landscape buffer located in the MERRILL COMMERCE CENTER outside of the public right-of-way.

Buildings in Planning Areas 1A, 3A, 4A, 5A, and 6A connect to Eucalyptus Avenue via direct driveway connections along Eucalyptus Avenue and via Grove Avenue, Walker Avenue, Baker Avenue, Vineyard Avenue, and Carpenter Avenue. Final driveway locations along Eucalyptus Avenue will be determined in conjunction with the design of implementing development projects, in accordance with Chapter 2.0 Access Guidelines of the City of Ontario’s Traffic and Transportation Design Guidelines.

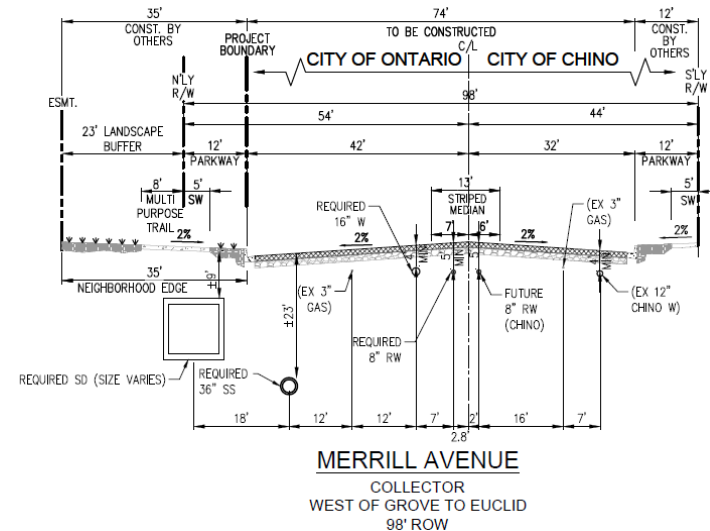
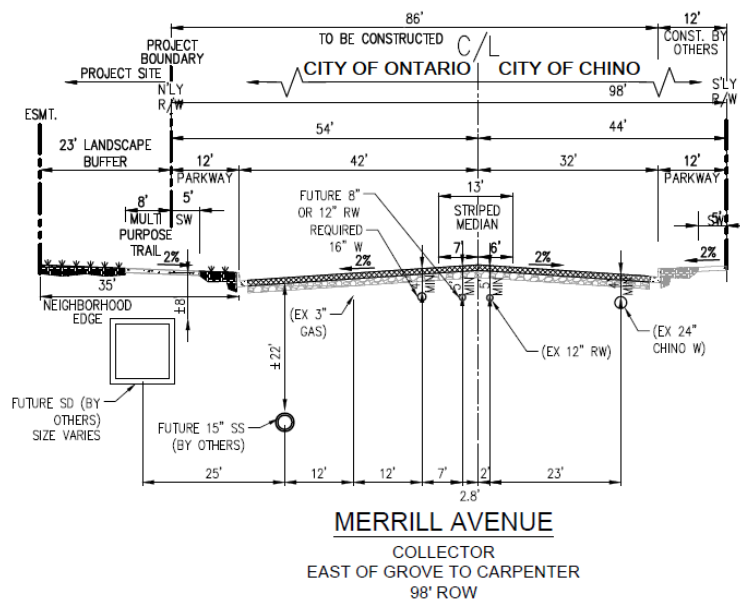


## D. Merrill Avenue (98-foot ROW Collector)

Merrill Avenue forms the southern boundary of Planning Areas 2, 3, 4, 5, and 6 and is the dividing line between the City of Ontario to the north and the City of Chino to the south. This public roadway provides access to the Specific Plan area from off-site areas to the east, south, and west. Along the segment of Merrill Avenue located between Grove Avenue and Carpenter Avenue, the developer(s) of the MERRILL COMMERCE CENTER will construct 86 feet of the ROW of this 98-foot wide Collector. The portions of Merrill Avenue between Grove Avenue and Carpenter that will be constructed as part of the MERRILL COMMERCE CENTER include 74 feet of paved roadway, a 13-foot wide striped (painted) median, and a 12-foot-wide parkway on the north side of the street that contains 7 feet of curb-adjacent landscaping and a 5-foot-wide sidewalk.

The remaining portions of the Merrill Avenue ROW (southerly side of the roadway in the City of Chino) will be constructed by others and is anticipated to include a 12-foot parkway containing 7 feet of curb-adjacent landscaping and a 5-foot wide sidewalk. An approximate 23-foot landscape buffer abuts the northern side of the Merrill Avenue ROW in the MERRILL COMMERCE CENTER, with the landscape buffer including an 8-foot wide multi-purpose trail.

Along the segment of Merrill Avenue located between Euclid Avenue and Grove Avenue, the developer(s) of the MERRILL COMMERCE CENTER will construct 74 feet of the ROW of this 98-foot wide Collector. The portions of Merrill Avenue between Euclid Avenue and Grove Avenue that will be constructed as part of the MERRILL COMMERCE CENTER include 74 feet of paved roadway and a 13-foot wide striped (painted) median.



Merrill Avenue is designated by the Chino Airport Compatibility Plan to satisfy open land requirements. For this reason, the median is painted and not raised and light poles and street trees are designed to maintain a clear width of about 75 feet. The light poles will be spaced at approximately 250 feet and staggered on the opposite side of the street. The portion of Merrill Avenue traversing Airport Safety Zone 1 (the southwest portion of Planning Area 1) must remain clear of permanent aboveground objects, and as such may contain frangible/break-away light poles.

Traffic signals will be located at the intersections of Merrill Avenue with Grove Avenue, Walker Avenue, and Vineyard Avenue (1/2-mile DIF), and at Baker Avenue, Carpenter Avenue, and the mid-way point between Grove and Walker (1/4-mile non-DIF) depending on need. Traffic signal locations are subject to change based on the results and recommendations of a traffic study.

As noted on Figure 4-1, the development of the MERRILL COMMERCE CENTER also triggers the need to modify the existing traffic signal at the intersection of Euclid Avenue and Merrill Avenue and a need to construct bridge improvements at the intersection of Merrill Avenue and the Cucamonga Channel. All improvements to Merrill Avenue are required to comply with applicable City of Ontario requirements, including sight distance requirements, in addition to requirements of the City of Chino pertaining to roadway improvements in the City of Chino's jurisdiction.

Buildings constructed in Planning Areas 2, 3, 4, 5 and 6 connect to Merrill Avenue via direct driveway connections along Merrill Avenue and via Walker Avenue, Baker Avenue, Grove Avenue, Vineyard Avenue, and Carpenter Avenue. Final driveway locations along Merrill Avenue will be determined in conjunction with the design of implementing development projects, in accordance with Chapter 2.0

Access Guidelines of the City of Ontario's Traffic and Transportation Design Guidelines.

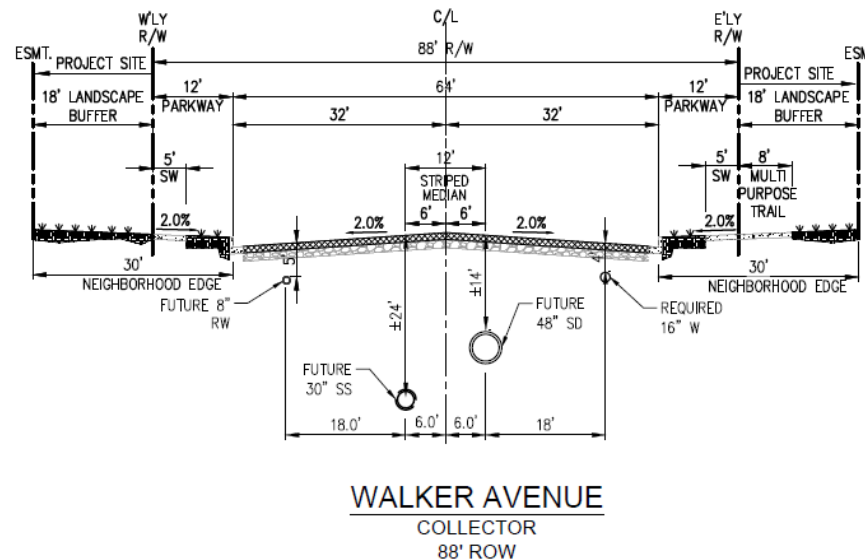
**E. Walker Avenue (88-foot ROW Collector)**

Walker Avenue forms the eastern boundary of Planning Areas 1, 1A and 2, and forms the western boundary of Planning Areas 3 and 3A. This public roadway provides access to the Specific Plan area from off-site areas to the north and south. Along the segment of Walker Avenue located between Edison Avenue and Merrill Avenue, the developer(s) of the MERRILL COMMERCE CENTER will construct the entirety of the 88-foot Collector, which consists of 64 feet of paved roadway and a 12-foot wide striped (painted) median. Additionally, the Walker Avenue ROW includes a 12-foot wide parkway on both sides of the roadway containing 7 feet of curb-adjacent landscaping and a 5-foot wide parkway-adjacent sidewalk.

An approximate 18-foot wide landscape buffer abuts the west and east sides of the Walker Avenue ROW inside the MERRILL COMMERCE CENTER Specific Plan, with the easterly landscape buffer including an 8-foot wide multi-purpose trail.

Traffic signals will be located at the intersections of Walker Avenue with Eucalyptus Avenue, Merrill Avenue, and potentially Street "A" (1/2-mile DIF) depending on need. Traffic signal locations are subject to change based on the results and recommendations of a traffic study. All improvements to Walker Avenue are required to comply with applicable City of Ontario requirements, including sight distance requirements.

Buildings in Planning Areas 1, 1A, 2, 3, and 3A connect to Walker Avenue via direct driveway connections along Walker Avenue. Final driveway locations will be determined in conjunction with the design of implementing development projects, in accordance with Chapter 2.0 Access Guidelines of the City of Ontario's Traffic and Transportation Design Guidelines.

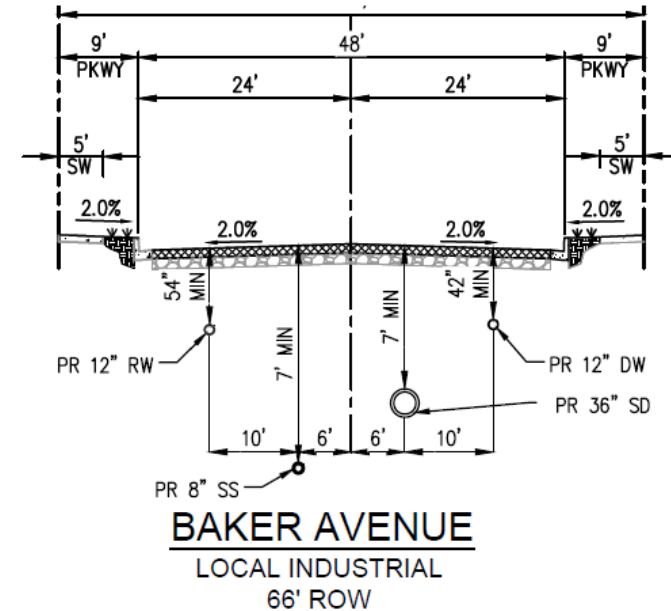


**F. Baker Avenue (66-foot ROW Local Industrial)**

Baker Avenue forms the eastern boundary of Planning Areas 4 and 4A, and forms the western boundary of Planning Areas 5 and 5A. This public roadway provides access to the Specific Plan area from off-site areas to the north and south. Along the segment of Baker Avenue located between Eucalyptus Avenue and Merrill Avenue, the developer(s) of the MERRILL COMMERCE CENTER will construct the entirety of the 66-foot wide Local Industrial Street, which consists of 48 feet of pavement and a 9-foot wide parkway on either side of the roadway containing 4 feet of curb-adjacent landscaping and a 5-foot wide sidewalk.

A traffic signal will be located at the intersection of Baker Avenue with Merrill Avenue (1/4-mile DIF) and potentially at the intersection of Baker Avenue with Eucalyptus Avenue (1/4-mile non-DIF and at the mid-way point between Eucalyptus and Merrill (1/4-mile non-DIF) depending on need. Traffic signal locations are subject to change based on the results and recommendations of a traffic study. All improvements to Baker Avenue are required to comply with applicable City of Ontario requirements, including sight distance requirements.

Planning Areas 4, 4A, 5, and 5A connect to Baker Avenue via direct driveway connections along Baker Avenue. Final driveway locations will be determined in conjunction with the design of implementing development projects, in accordance with Chapter 2.0 Access Guidelines of the City of Ontario’s Traffic and Transportation Design Guidelines.

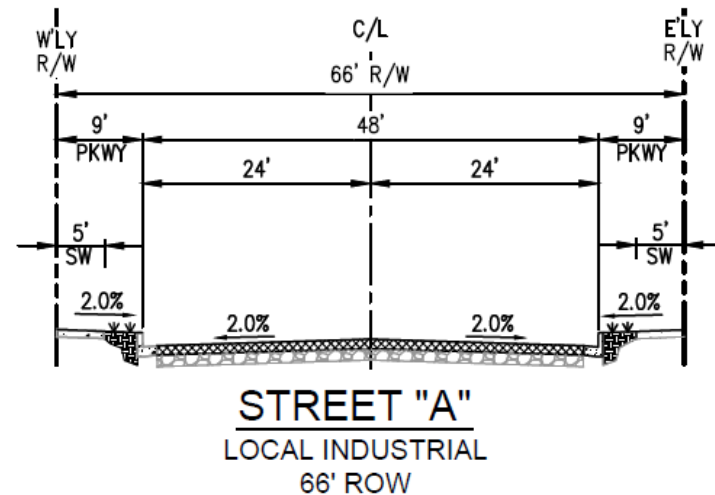




**G. Street "A" (66-foot ROW Local Industrial)**

Street "A" forms the southern boundary of Planning Area 1 and the northern boundary of Planning Area 2 and its exact alignment will be determined in conjunction with implementing development plans. This public roadway provides access to the Specific Plan area from off-site areas to the west and facilitates internal east-west circulation within the westerly portions of the Specific Plan area. Along the segment of Street "A" located between Grove Avenue and Walker Avenue, the developer(s) of the MERRILL COMMERCE CENTER will construct the entirety of the 66-foot wide Local Industrial Street, which consists of 48 feet of pavement and a 9-foot wide parkway on either side of the roadway containing 4 feet of curb-adjacent landscaping and a 5-foot wide sidewalk. A traffic signal will be located at the intersection of Street "A" with Grove Avenue (1/4-mile non-DIF), and potentially with Walker Avenue (1/2-mile DIF) and at the mid-way point between Gove and Walker (1/4-mile non-DIF). Traffic signal locations are subject to change based on the results and recommendations of a traffic study. All improvements to Street "A" are required to comply with applicable City of Ontario requirements, including sight distance requirements.

Planning Areas 1 and 2 connect to Street "A" via direct driveway connections along Street "A." Final driveway locations will be determined in conjunction with the design of implementing development projects, in accordance with Chapter 2.0 Access Guidelines of the City of Ontario's Traffic and Transportation Design Guidelines.



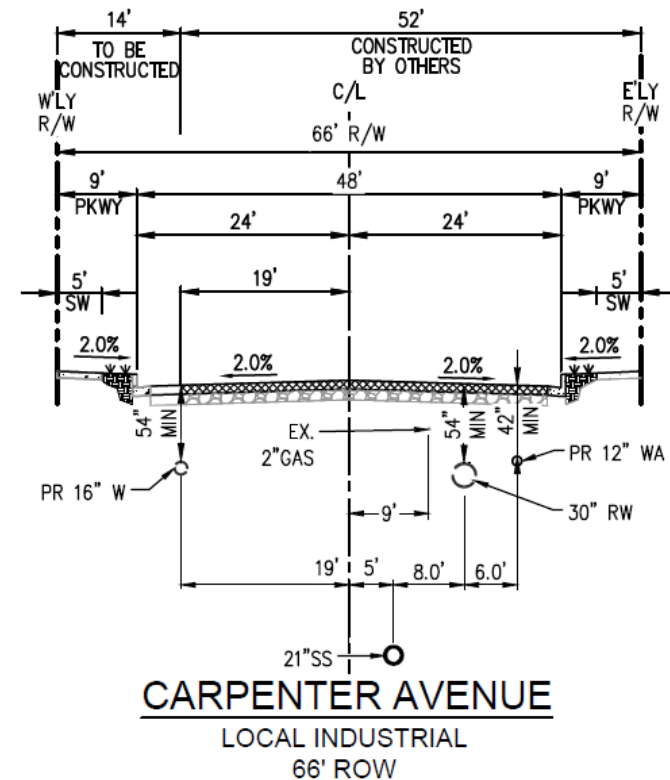
**H. Carpenter Avenue (66-foot ROW Local Industrial)**

Carpenter Avenue forms the eastern boundary of Planning Areas 6 and 6A. This public roadway provides access to the Specific Plan area from off-site areas to the north, south, and east. Along the segment of Carpenter Avenue located between Eucalyptus Avenue and Merrill Avenue (including the segment that abuts the eastern boundary of Planning Areas 6 and 6A, the developer(s) of the MERRILL COMMERCE CENTER will construct 14 feet of the ROW of this 66-foot-wide Local Industrial Street. The portions of Carpenter Avenue to be constructed as part of the Specific Plan include 5 feet of paved roadway (including curb and gutter) and a 9-foot-wide parkway on the western side of the street that contains 4 feet of curb-adjacent landscaping and a 5-foot sidewalk. The remaining portions of the eastern side of the Carpenter Avenue ROW will be constructed by others (the development project located on the east side of the roadway) which will include 43 feet of roadway and curb and gutter improvements, and a 9-foot-wide parkway containing 4 feet of landscaping and a 5-foot parkway-adjacent sidewalk.

Traffic signals will be located at the intersection of Carpenter Avenue with Merrill Avenue (1/4-mile DIF) and potentially with Eucalyptus Avenue and at the mid-way point between Eucalyptus and Merrill (1/4-mile non-DIF) depending on need. Traffic signal locations are subject to change based on the results and recommendations of a traffic study. All improvements to Carpenter Avenue are required to comply with applicable City of Ontario requirements, including sight distance requirements.

Planning Areas 6 and 6A connect to Carpenter Avenue via direct driveway connections along Carpenter Avenue. Final driveway

locations will be determined in conjunction with the design of implementing development projects, in accordance with Chapter 2.0 Access Guidelines of the City of Ontario’s Traffic and Transportation Design Guidelines.



### ***I. Private Drive Aisles***

Private Drive Aisles connect individual planning areas to the roadways described above. Within each planning area, Private Drive Aisles provide vehicular access for automobiles and trucks to parking lots, truck courts, loading dock areas, etc. Private Drive Aisles will have pavement widths that range between 24 and 50 feet. Private Drive Aisles are not depicted on Figure 4-1 because their locations, alignments, and widths will be determined in conjunction with the design of implementing development projects.

### ***J. Truck Routes***

Access to the MERRILL COMMERCE CENTER Specific Plan area is provided from Grove Avenue, Eucalyptus Avenue, Vineyard Avenue, Baker Avenue, Carpenter Avenue, Walker Avenue, and Flight Avenue.

Truck traffic to and from the MERRILL COMMERCE CENTER will use the City of Ontario designated truck routes, which include Merrill Avenue, Edison Avenue, and Euclid Avenue. Additionally, truck traffic to and from the MERRILL COMMERCE CENTER will use the City of Chino's designated truck routes, which include Carpenter Avenue, Walker Avenue, and Flight Avenue.

Signalized intersections along truck routes in the City of Ontario will be constructed with PCC pavement per City standards, along with signalized intersections along the truck routes on Merrill Avenue, as shown on Figure 4-1, *Conceptual Vehicular Circulation and Access Plan*.

### 4.1.2 Non-Vehicular Circulation

The MERRILL COMMERCE CENTER Specific Plan encourages circulation by employees and visitors via non-motorized means. Pedestrian circulation is encouraged interior to the Specific Plan area through an integrated network of sidewalks, bikeways, and trails. Additional pedestrian and bike facilities will be designed on individual building sites at the time buildings are designed and positioned in each planning area as part of implementing development projects.

As illustrated on Figure 4-2, *Non-Vehicular Circulation and Mobility Plan*, the MERRILL COMMERCE CENTER Specific Plan provides for sidewalks in the public rights-of-way along the sides of the following streets that front the Specific Plan: Eucalyptus Avenue, Merrill Avenue, Grove Avenue, and Carpenter Avenue. Additionally, the MERRILL COMMERCE CENTER Specific Plan provides for sidewalks in the public rights-of-way along both sides of Baker Avenue (segment located between Eucalyptus Avenue and Merrill Avenue), Walker Avenue (segment located between Edison Avenue/Ontario Ranch Road and Merrill Avenue), Vineyard Avenue (segment located between Eucalyptus Avenue and Merrill Avenue), and Street "A." Pedestrian Crosswalks are designed at signalized intersections (as depicted on Figure 4-2) to ensure pedestrian safety.

As illustrated on Figure 4-2, *Non-Vehicular Circulation and Mobility Plan*, the Specific Plan provides for 8-foot-wide multi-purpose trails along the north side of the segment of Merrill Avenue located between Euclid Avenue and Archibald Avenue; along the east side of the segment of Grove Avenue located between Eucalyptus Avenue and Merrill Avenue; along the east side of the segment of Walker Avenue located between Edison Avenue/Ontario Ranch Road and

Merrill Avenue; and along the west side of the segment of Vineyard Avenue located between Eucalyptus Avenue and Merrill Avenue.

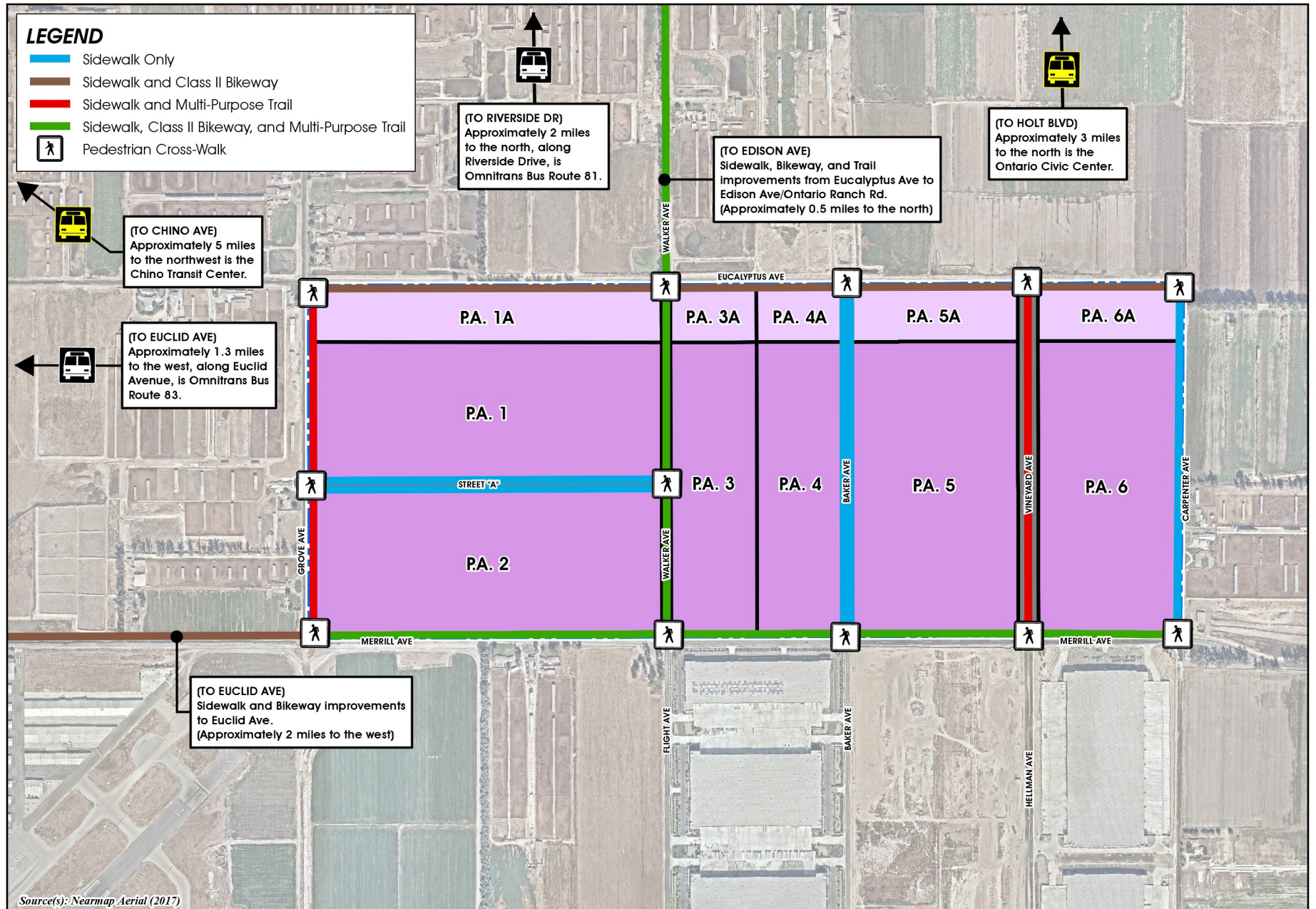
As illustrated on Figure 4-2, *Non-Vehicular Circulation and Mobility Plan*, the Specific Plan provides Class II bike lanes in the public right-of-way along both sides of the segment of Merrill Avenue located between Carpenter Avenue and Euclid Avenue; along both sides of the segment of Walker Avenue between Merrill Avenue and Edison Avenue/Ontario Ranch Road; along the south side of the segment of Eucalyptus Avenue that abuts the northern boundaries of Planning Areas 3A, 4A, 5A, and 6A; and along the north side of Eucalyptus Avenue between Grove Avenue and Walker Avenue.

Given the volume of passenger car and truck traffic circulating on the site, pedestrian and bicycle safety was given due consideration when preparing the design standards for the MERRILL COMMERCE CENTER. Provisions for sidewalks and pedestrian walkways, bicycle storage facilities, and employee and visitor gathering areas interior to the planning areas are set forth in Chapter 6, *Design Guidelines*.

As shown on Figure 4-2, the following bus and transit facilities are located to the west and north of MERRILL COMMERCE CENTER:

- A. Omnitrans Bus Route 83, located approximately 1.3 miles to the west along Euclid Avenue.
- B. Chino Transit Center, located approximately 5 miles to the northwest at Chino Avenue.
- C. Omnitrans Bus Route 81, located approximately 2 miles to the north along Riverside Drive.
- D. Ontario Civic Center/Transfer Station, located approximately 3 miles to the north at Holt Boulevard.

Future bus stops and shelters will be required to be installed to the satisfaction of the City of Ontario and Omnitrans.



Non-Vehicular Circulation and Mobility Plan

Figure 4-2

## 4.2 UTILITY INFRASTRUCTURE PLAN

Buildout of the MERRILL COMMERCE CENTER requires the installation of water, sewer, drainage, and other utility infrastructure, as described in this chapter. All utility infrastructure improvements shall be constructed in accordance with applicable City of Ontario design standards and specifications.

### 4.2.1 Potable Water Plan

Currently, there are no City potable water mains or City potable water infrastructure in the vicinity of the Specific Plan area. Potable Water System Improvements for the Specific Plan area (as shown on Figure 4-3) require the planning, design, and construction of the 925 Pressure Zone (PZ) Phase 2 West Backbone, which includes: extending the 24-inch potable water main in Eucalyptus Avenue from Carpenter Avenue to Grove Avenue; and, installing a 30-inch to 42-inch potable water main in Grove Avenue connecting from the 24-inch potable water main in Eucalyptus Avenue and extending to Chino Avenue; installing an 18-inch to 24-inch potable water main in Chino Avenue and connecting to the existing 18-inch potable water main located on the west side of the Cucamonga Creek Channel; and installing a Pressure Reducing Station between the 1010 PZ and 925 PZ near the intersection of Grove Avenue and Chino Avenue.

Master Plan Phase 2 facilities that are required to serve the Project but that will be constructed by others include installing a 42-inch potable water main in Grove Avenue connecting from the 30-inch to 42-inch potable water main in Grove Avenue at Chino Ave and extending to Francis Avenue; and, installing a 42-inch potable main in Francis Avenue connecting from the 42-inch potable water main in Grove Avenue and extending to Bon View Avenue; and, installing

a 42-inch potable water main in Bon View Avenue connecting from the 42-inch potable water main in Francis Avenue and extending to the Bon View Avenue Reservoir site and to the Reservoir; and, installing a 9 million gallon reservoir on the Bon View Reservoir site; and, installing two 2,500 gpm wells with any treatment necessary to meet water quality standards and the 16-inch to 42-inch well collection mains from the wells to the reservoirs. At the time this Specific Plan was prepared, the alignment of the 42-inch water line between Chino Avenue and the water reservoir site had not been finalized and is subject to change. The Project will be required to participate in the future Phase 2 Water System Improvements north of Chino Avenue, as detailed in the development agreement with the City.

In addition to the 925 Pressure Zone (PZ) Phase 2 West Backbone, the Specific Plan area requires the planning, design, and construction of a Secondary Loop between the 925 Pressure Zone (PZ) Phase 2 West Backbone and the Specific Plan area which includes: installing a 24-inch potable water main in Eucalyptus Avenue connecting to the 30-inch to 42-inch 925 Pressure Zone (PZ) Phase 2 West Backbone main in Grove Avenue; installing a 16-inch potable water main in Merrill Avenue connecting from the 12-inch to 16-inch potable water main in Grove Avenue and extending to Vineyard Avenue; and, installing a 16-inch potable water main in Vineyard Avenue connecting from the 16-inch potable water main in Merrill Avenue and extending to connect to the 24-inch potable water main in Eucalyptus Avenue; and, installing a 12-inch potable water main in Merrill Avenue connecting from the 16-inch potable water main in Vineyard Avenue and extending east to connect to the 12-inch potable water main in Carpenter Avenue.

The Specific Plan area also requires the planning, design, and construction of the Local Adjacent Potable Water System, which

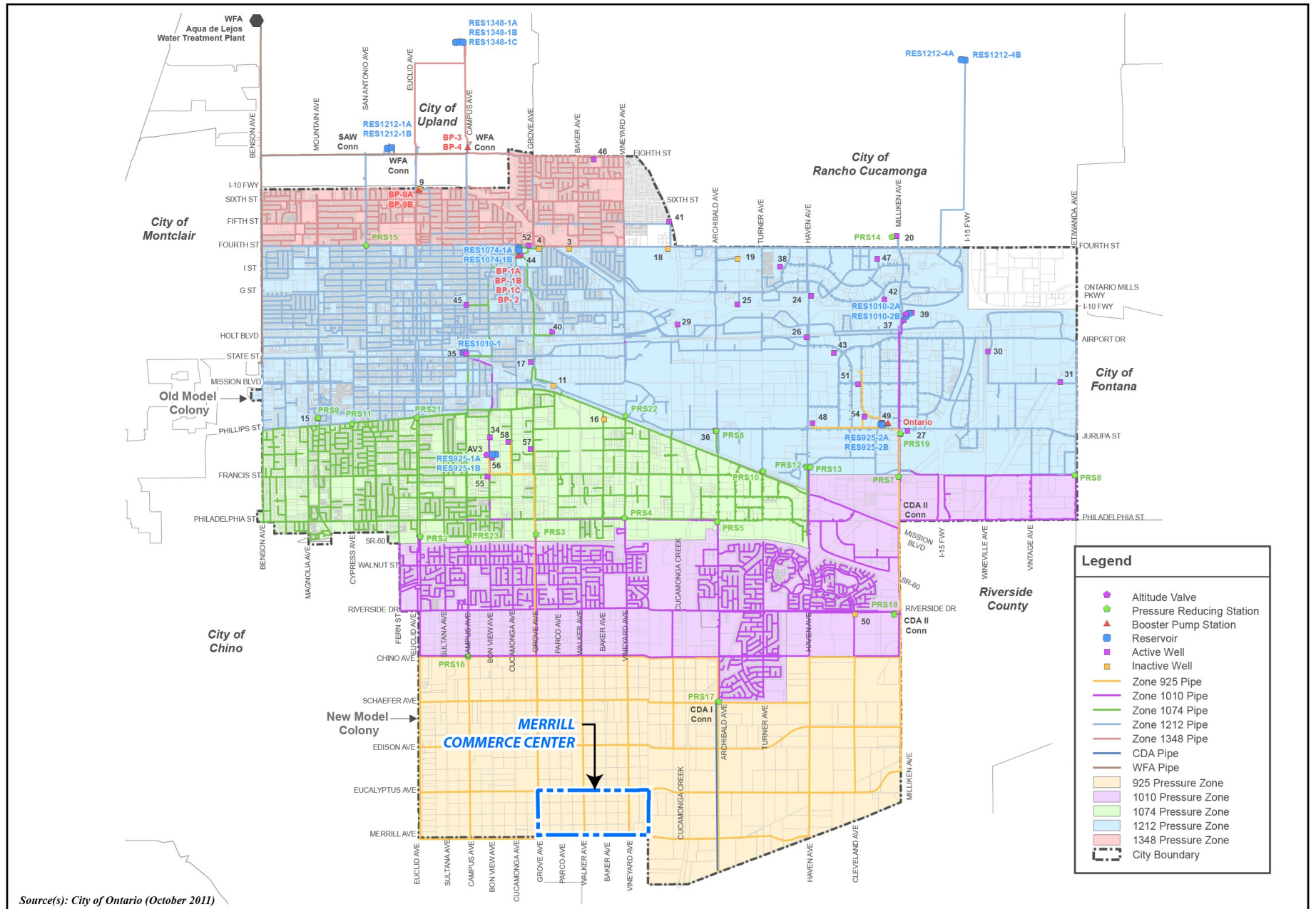
includes: installing a 12-inch to 16-inch potable water main in Grove Avenue connecting to the 24-inch potable water main in Eucalyptus Avenue and extending to connect to the 16-inch potable water main in Merrill Avenue; and, installing a 12-inch to 16-inch potable water main in Walker Avenue connecting to the 24-inch potable water main in Eucalyptus Avenue and extending to connect to the 16-inch potable water main in Merrill Avenue; and, installing a 12-inch potable water main in Baker Avenue connecting to the 24-inch potable water main in Eucalyptus Avenue and extending to connect to the 16-inch potable water main in Merrill Avenue; and, installing a 12-inch potable water main in "Street A" connecting to the 12-inch potable water main in Grove Avenue and extending to connect to the 12-inch to 16-inch potable water main in Walker Avenue.

Required Potable Water Infrastructure is subject to change based upon findings of City approved hydraulic studies, master plan updates, and project final designs; and, potable water main locations are also subject to change based upon the developer-conducted and City-approved Conceptual Design Report. Any existing utilities, including IEUA water mains, that do not meet minimum depths, standard alignment locations, and/or minimum horizontal and vertical separation requirements shall be subject to relocation/ replacement by the developer(s) of the Specific Plan.

Within the project site, on individual private property, the onsite potable, domestic, and fire systems shall be private and be privately maintained.







Source(s): City of Ontario (October 2011)

City of Ontario Ultimate Water System

Figure 4-3A

### 4.2.2 Recycled Water Plan

The City of Ontario/ Ontario Municipal Utilities Company (OMUC) supplies recycled water to the Specific Plan area. Existing City recycled water infrastructure is located to the east of the Specific Plan Area in Carpenter Avenue, Eucalyptus Avenue, and Merrill Avenue. Recycled Water supplied by OMUC is produced by the Inland Empire Utility Agency (IEUA) from its four wastewater reclamation plants.

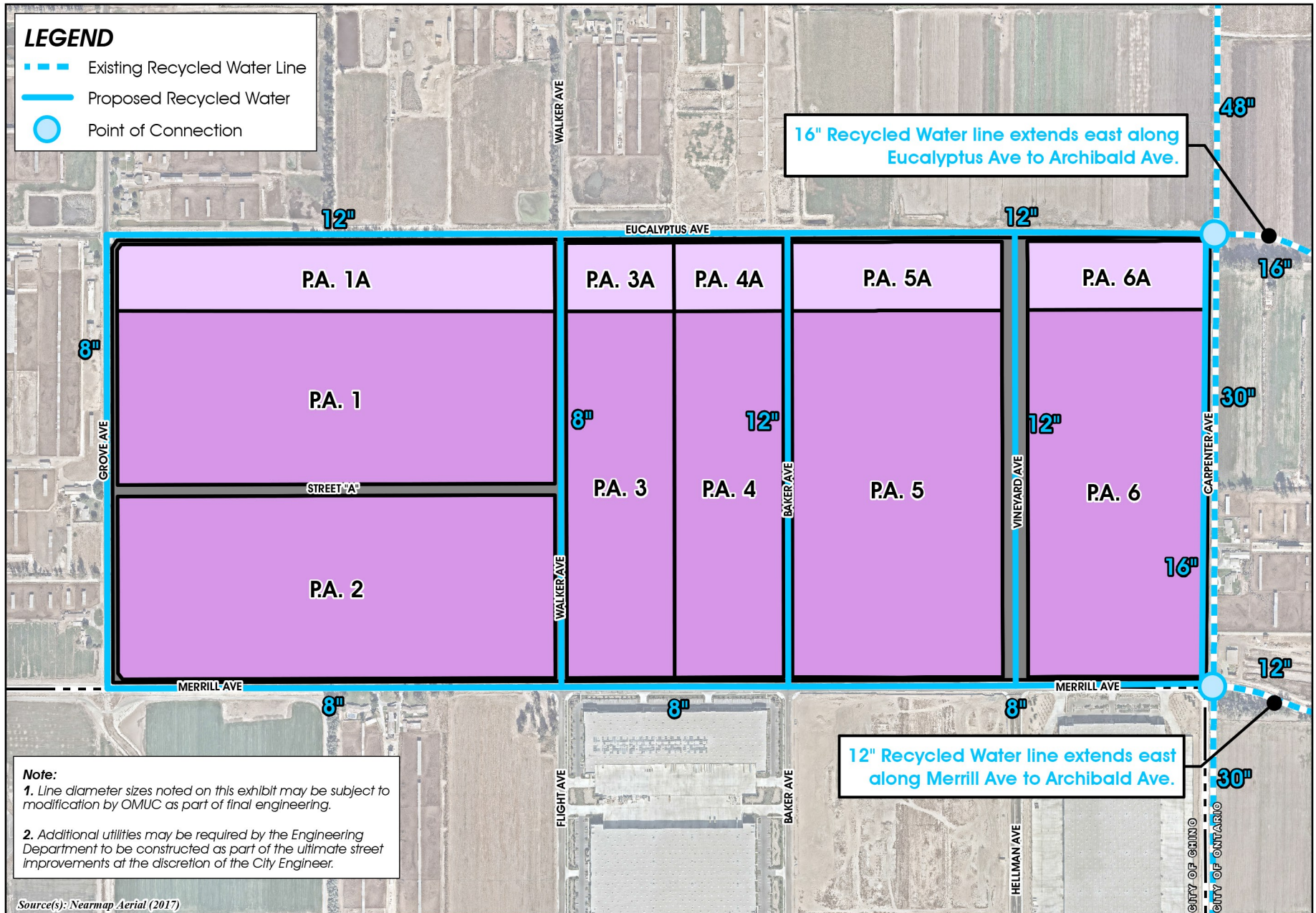
The entire Specific Plan area is within the City's master planned 930 Pressure Zone. Recycled Water Infrastructure Improvements for the Specific Plan area (as shown on Figure 4-4) require the planning, design, and construction of the Primary 930 Pressure Zone (PZ) Recycled Water Master Plan Backbone mains, which includes: installing a 16-inch recycled water main in Carpenter Avenue connecting to the 16-inch 930 Pressure Zone Recycled Water main in Eucalyptus Avenue and extending it to connect to the 8-inch 930 Pressure Zone Recycled Water main in Merrill Avenue; installing a 12-inch recycled water main in Eucalyptus Avenue connecting to the existing 16-inch 930 Pressure Zone recycled water main at the intersection of Carpenter Avenue & Eucalyptus Avenue and extending to Grove Avenue; and, installing an 8-inch recycled water main in Grove Avenue connecting to the 12-inch recycled water main in Eucalyptus Avenue and extending in Grove Avenue to Merrill Avenue; and, installing an 8-inch recycled water main in Merrill Avenue connecting to the existing City 12-inch 930 Pressure Zone Recycled Water main in Merrill Avenue at the intersection of Merrill Avenue and Carpenter Avenue and extending it west to Baker Avenue; and, installing an 8-inch recycled water main in Merrill Avenue connecting to the 12-inch recycled water main in Merrill Avenue at Baker Avenue and extending west to Grove Avenue.

In addition to the Primary 930 Pressure Zone (PZ) Recycled Water Master Plan Backbone mains, the Specific Plan area requires the planning, design, and construction of a Secondary Loop Improvements which includes: installing an 8-inch recycled water main in Merrill Avenue connecting to the 8-inch recycled water main in Merrill Avenue at Grove Avenue and extending west to Euclid Avenue.

The Specific Plan area also requires the planning, design, and construction of the Adjacent Recycled Water System, which includes: installing a 12-inch recycled water main in Vineyard Avenue connecting to the 8-inch recycled water main in Merrill Avenue and extending it to connect to the 12-inch main in Eucalyptus Avenue; and, installing a 12-inch recycled water main in Baker Avenue connecting to the 8-inch recycled water main in Merrill Avenue and extending it to connect to the 12-inch main in Eucalyptus Avenue; and, installing an 8-inch recycled water main in Walker Avenue connecting to the 8-inch recycled water main in Merrill Avenue and extending it to connect to the 12-inch main in Eucalyptus Avenue.

Required Recycled Water Infrastructure is subject to change based upon findings of City-approved hydraulic studies, master plan updates, and project final designs; and, Recycled Water main locations are also subject to change based upon the developer-conducted and City-approved Conceptual Design Report. Any existing utilities, including IEUA Recycled Water mains, that do not meet minimum depths, standard alignment locations, and/or minimum horizontal and vertical separation requirements shall be subject to relocation/replacement by the developer(s) of the Specific Plan.

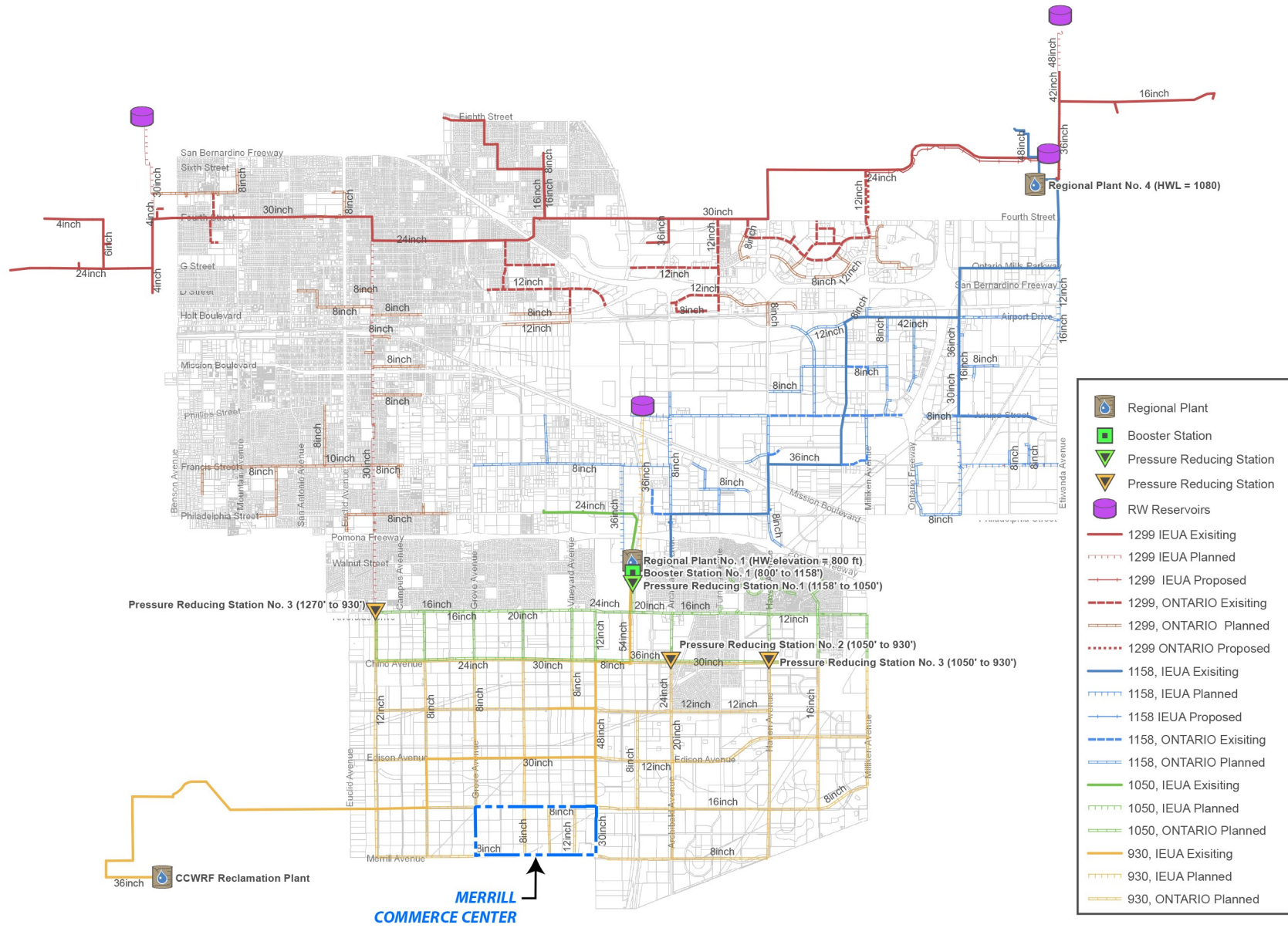
Within the project site, on individual private property, the onsite recycled water and irrigation systems shall be private and be privately maintained.



Recycled Water Infrastructure Plan

Figure 4-4





Source(s): City of Ontario (October 2011)

## City of Ontario Future Recycled Water System

Figure 4-4A

### 4.2.3 Sanitary Sewer Plan

Sanitary Sewer/Wastewater Collections for the Specific Plan area is served by the City of Ontario/ Ontario Municipal Utilities Company (OMUC), which conveys wastewater to the Inland Empire Utility Agency (IEUA) for transmission and treatment.

Currently, existing 21-inch and existing 24-inch City sanitary sewer mains are located in Carpenter Avenue to the east and south of the Specific Plan area. The entire Specific Plan area is included within the City's Sewer Master Plan. The areas west of Vineyard Avenue are Tributary to the Western Trunk Sewer, which connect to IEUA's system at Kimball Avenue and Euclid Avenue; and the areas east of Vineyard Avenue are Tributary to the Eastern Trunk Sewer (ETS), through the City's Carpenter Trunk Sewer which connect to IEUA's system at Vineyard/Hellman Avenue and the San Bernardino/ Riverside County line. Planning Areas 1 to 5 and 1A to 5A are within the Western Trunk Sewer tributary area and Planning Area 6 and 6A are within the Eastern Trunk Sewer tributary area.

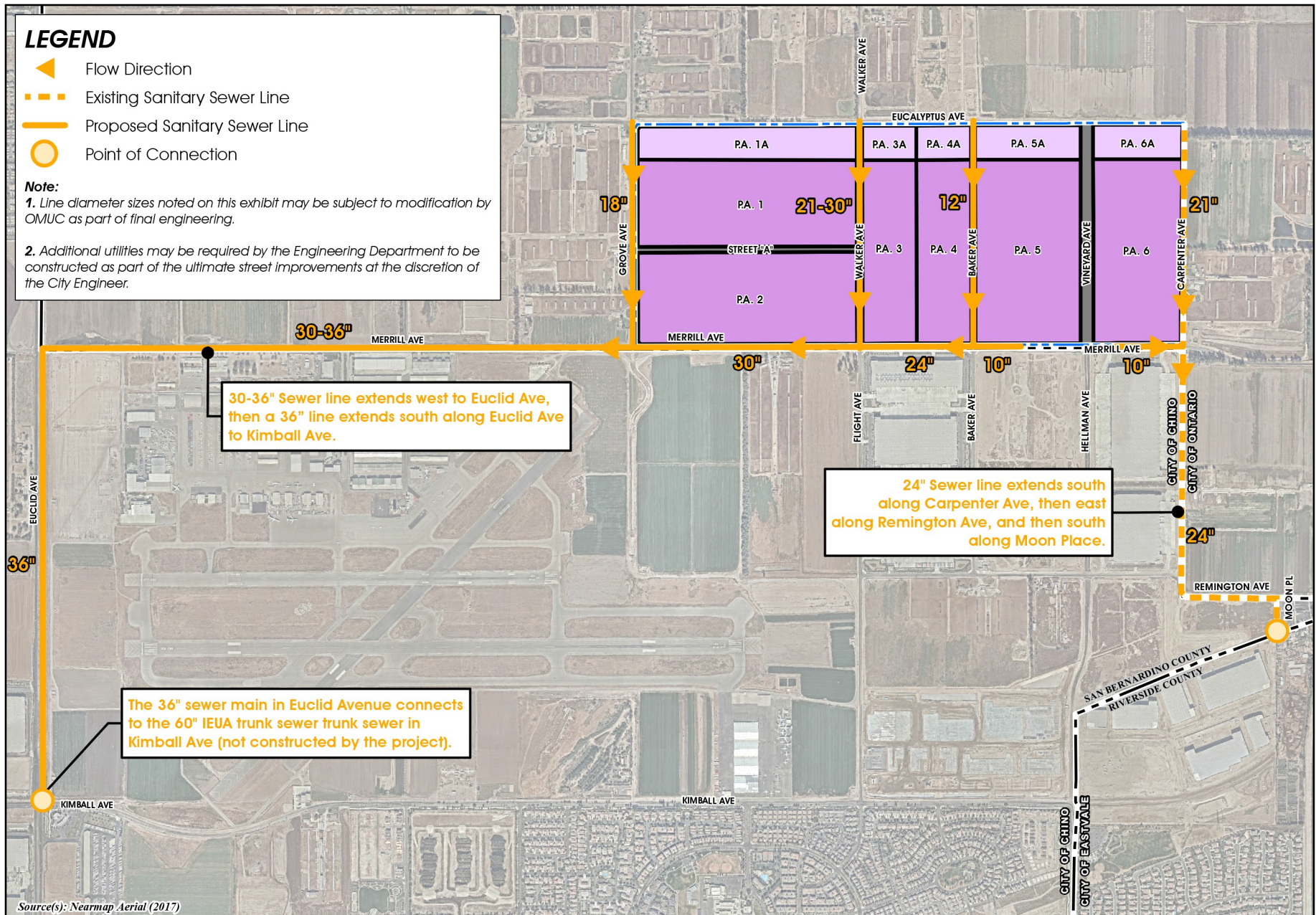
Sanitary Sewer Improvements for the Specific Plan area (as shown on Figure 4-5) require the planning, design, and construction of the following Primary Sewer Master Plan Backbone mains of the Western Trunk Sewer (WTS), which includes: installing a 36-inch sewer main in Euclid Avenue connecting to the IEUA's 60-inch Kimball Interceptor at the intersection of Kimball Avenue and Euclid Avenue and extending north to Merrill Avenue; and, installing a 30-inch to 36-inch sewer main in Merrill Ave from Euclid Avenue to Grove Avenue; and, installing a 30-inch sewer main in Merrill Avenue from Grove Avenue to Walker Avenue; and, installing a 21-inch to 30-inch sewer main in Walker Avenue from Merrill Avenue to Eucalyptus Avenue.

In addition to the Primary Sewer Master Plan Backbone mains, the Specific Plan area requires the planning, design, and construction of a Secondary Master Plan Trunk Sewer, which includes: installing an 18-inch Grove Trunk Sewer main in Grove Avenue from the WTS in Merrill Avenue and extending north in Grove Avenue to Eucalyptus Avenue.

The Specific Plan area also requires the planning, design, and construction of the Adjacent Local Sewer Systems, which includes: installing a 10-inch sewer main in Merrill Avenue from Carpenter Avenue west towards Vineyard Avenue; and, installing a 24-inch sewer main in Merrill Avenue from the WTS in Walker Avenue and extending east to Baker Avenue; and, installing a 10-inch sewer main in Merrill Avenue from Baker Avenue easterly towards Vineyard Avenue; and, installing a 12-inch sewer main in Baker Avenue from Merrill Avenue northerly toward Eucalyptus Avenue.

Required Sanitary Sewer Infrastructure is subject to change based upon findings of City-approved hydraulic studies, master plan updates, and project final designs; and, sewer main locations are also subject to change based upon the developer-conducted and City-approved Conceptual Design Report. Any existing utilities, including IEUA Recycled Water mains, that do not minimum depth, standard alignment locations, and/or minimum horizontal and vertical separation requirements shall be subject to relocation/replacement by the developer(s) of the Specific Plan.

Within the project site, on individual private property, the onsite sewer systems shall be private and be privately maintained.

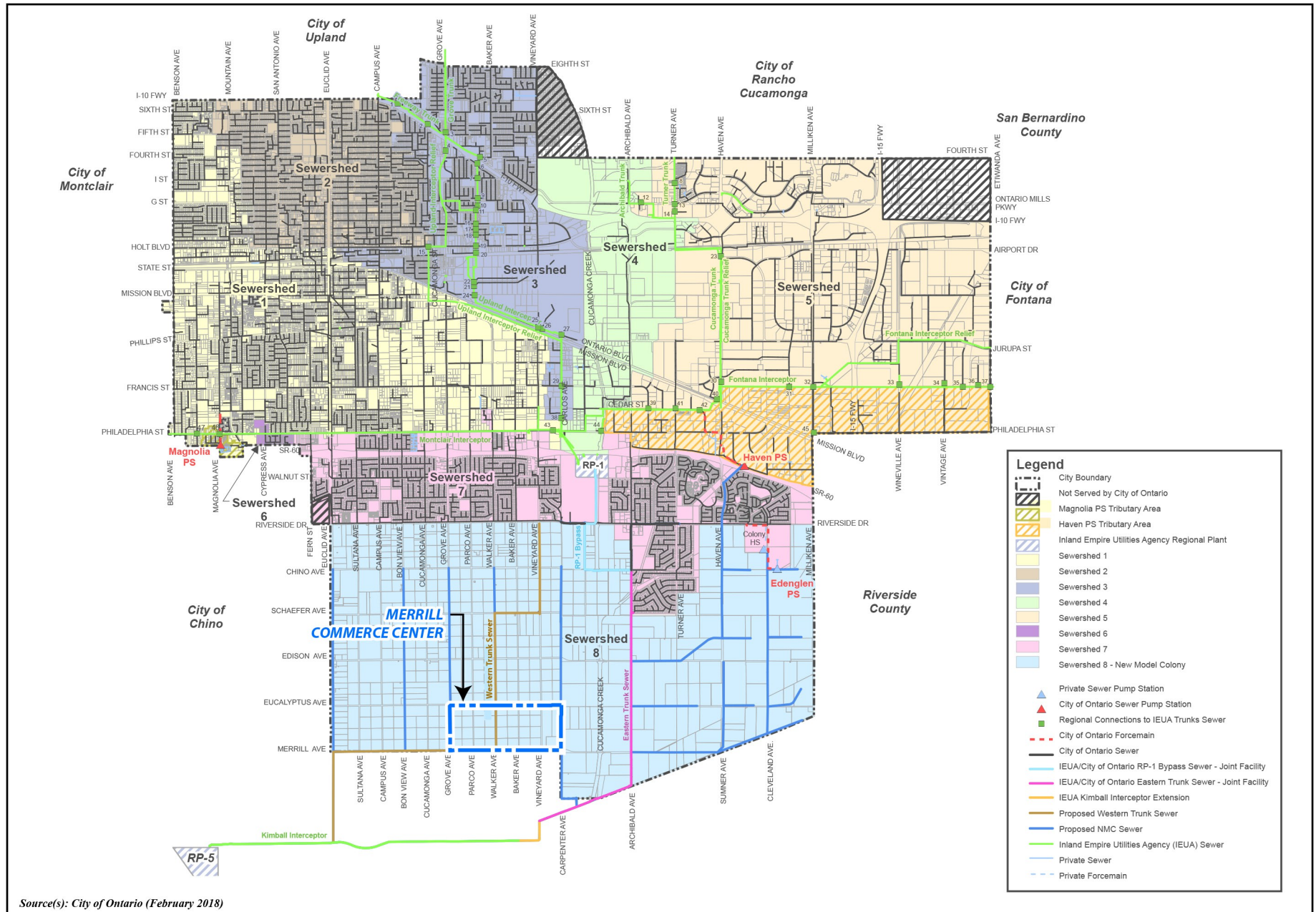


**Sanitary Sewer Infrastructure Plan**

Figure 4-5







City of Ontario Ultimate Sewer System

Figure 4-5A

#### 4.2.4 Storm Water Management Plan

The master storm drain plan for the MERRILL COMMERCE CENTER is shown on Figure 4-6, *Storm Drain Infrastructure Plan*. Improvements include the construction of the following storm drain improvements: an 8-foot by 13-foot Reinforced Concrete Box (RCB) in the segment of Eucalyptus Avenue located between Walker Avenue and Vineyard Avenue; a 3-foot by 6-foot RCB, a double 4-foot by 8-foot RCB, a double 8-foot by 9-foot RCB, and a double 12-foot by 10-foot RCB in various segments of Merrill Avenue between the midpoint of the southerly boundary of Planning Area 2 and Carpenter Avenue; a 24-inch storm drain line in the segment of Walker Avenue located between the southerly boundary of Planning Area 1A and Merrill Avenue; a 120-inch storm drain line in the segment of Grove Avenue located between Eucalyptus Avenue and Merrill Avenue (with a point of connection to the existing open flood channel located south of the intersection of Merrill Avenue and Grove Avenue); and an 8-foot by 13-foot RCB in the segment of Vineyard Avenue located between Merrill Avenue and Eucalyptus Avenue.

Additionally, as indicated on Figure 4-6, the developer(s) of the MERRILL COMMERCE CENTER may be conditioned to improve the existing open flood channel located south of the intersection of Merrill Avenue and Grove Avenue, which may consist of either lowering the elevation of the existing earthen channel or installing a double 10-foot by 6-foot RCB within the existing earthen channel to connect to an existing RCB located at the southerly terminus of the existing earthen flood channel. The ultimate solution will be determined during the final engineering process.

Planning Areas 1, 1A, and 2 drain in a southerly direction and the drainage ultimately flows into either a water quality basin located in

the southwest portion of Planning Area 2, the existing flood channel located south of the intersection of Merrill Avenue and Grove Avenue, or to the RCB drainage system in Merrill Avenue which will convey flows easterly to the Cucamonga Channel.

Stormwater flows from Planning Areas 3 and 3A drain in a southerly direction and the drainage ultimately flows into either the 24-inch line within Walker Avenue or to the RCB system in Merrill Avenue. Planning Areas 4 and 4A also drain in a southerly direction with the drainage ultimately flowing to either a storm drain line installed in Baker Avenue or to the RCB system in Merrill Avenue. Planning Areas 5, 5A, 6, and 6A drain in a southerly direction as well, and the drainage ultimately flows to the 8-foot by 13-foot RCB in Vineyard Avenue or the double 8-foot by 9-foot RCB in Merrill Avenue. Flows from Planning Areas 3, 3A, 4, 4A, 5, 5A, 6, and 6A ultimately drain easterly to an existing inlet connection to the Cucamonga Creek Channel via the existing double 12-foot by 10-foot RCB in Merrill Avenue (east of Carpenter Avenue).

Each storm drain in Grove Avenue and Merrill Avenue will be equipped with a hydrodynamic separator or City approved equal device to satisfy the statewide trash mandate. Each device will be approved by and listed on the Certified Full Capture System List of Trash Treatment Control Devices of the State Water Resources Control Board (SWRCB).

Line diameter sizes and other storm drain facility sizes noted herein may be subject to modification by the City of Ontario and/or the San Bernardino Flood Control District as part of final engineering.

### ***A. National Pollutant Discharge Elimination System (NPDES) and Water Quality Management Plan (WQMP)***

Design for the on-site improvements within the MERRILL COMMERCE CENTER Specific Plan will utilize a variety of Low Impact Development (LID) design concepts to detain, filter, and treat surface runoff in a practical manner to comply with the requirements of the San Bernardino County NPDES Storm Water Program's current Water Quality Management Plan (WQMP) for new development projects.

The objective of the project-specific WQMP is to minimize the detrimental effects of urbanization on the beneficial uses of receiving waters, including effects caused by increased pollutants and changes in hydrology. These effects shall be minimized through the implementation of on-site and off-site Low Impact Development (LID) Site Design Best Management Practices (BMP's) that retain/infiltrate or biotreat 85th percentile storm event runoff from the Specific Plan area.

In addition, non-structural and structural Source Control BMP's will be implemented and documented in the approved Water Quality Management Plan(s) to reduce pollutant generation and transport from the Specific Plan area.

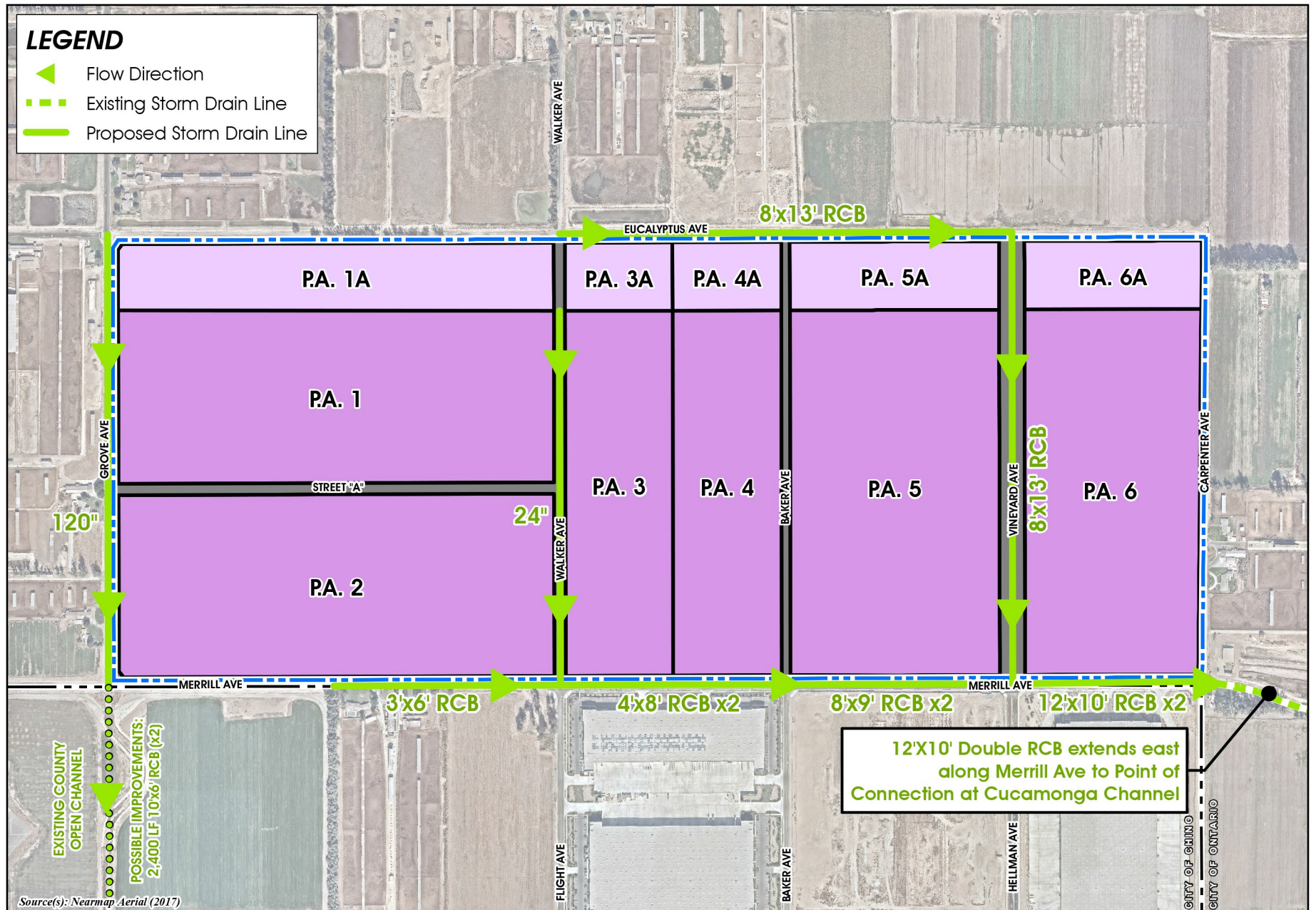
All Priority Land Use (PLU) areas, defined by the State Water Resources Board (SWRCB) as high trash generating areas such as industrial uses, within the Specific Plan area shall comply with the statewide Trash Provisions adopted by the SWRCB and trash requirements in the most current San Bernardino County Area-Wide MS4 permit.

Prior to the issuance of any grading or construction permits for any parcel map or area that disturbs one (1) acre or more of land within

the MERRILL COMMERCE CENTER Specific Plan, an Erosion Control & Sedimentation Plan and Storm Water Pollution Prevention Plans (SWPPP) will be prepared to comply with California State Water Resources Control Board's (State Water Board) current "General Permit to Discharge Storm Water Associated with Construction Activity" and current "Area Wide Urban Storm Water Runoff (Regional NPDES) Permit." SWPPPs are required to identify and detail all appropriate Best Management Practices (BMP's) to be implemented or installed during construction.

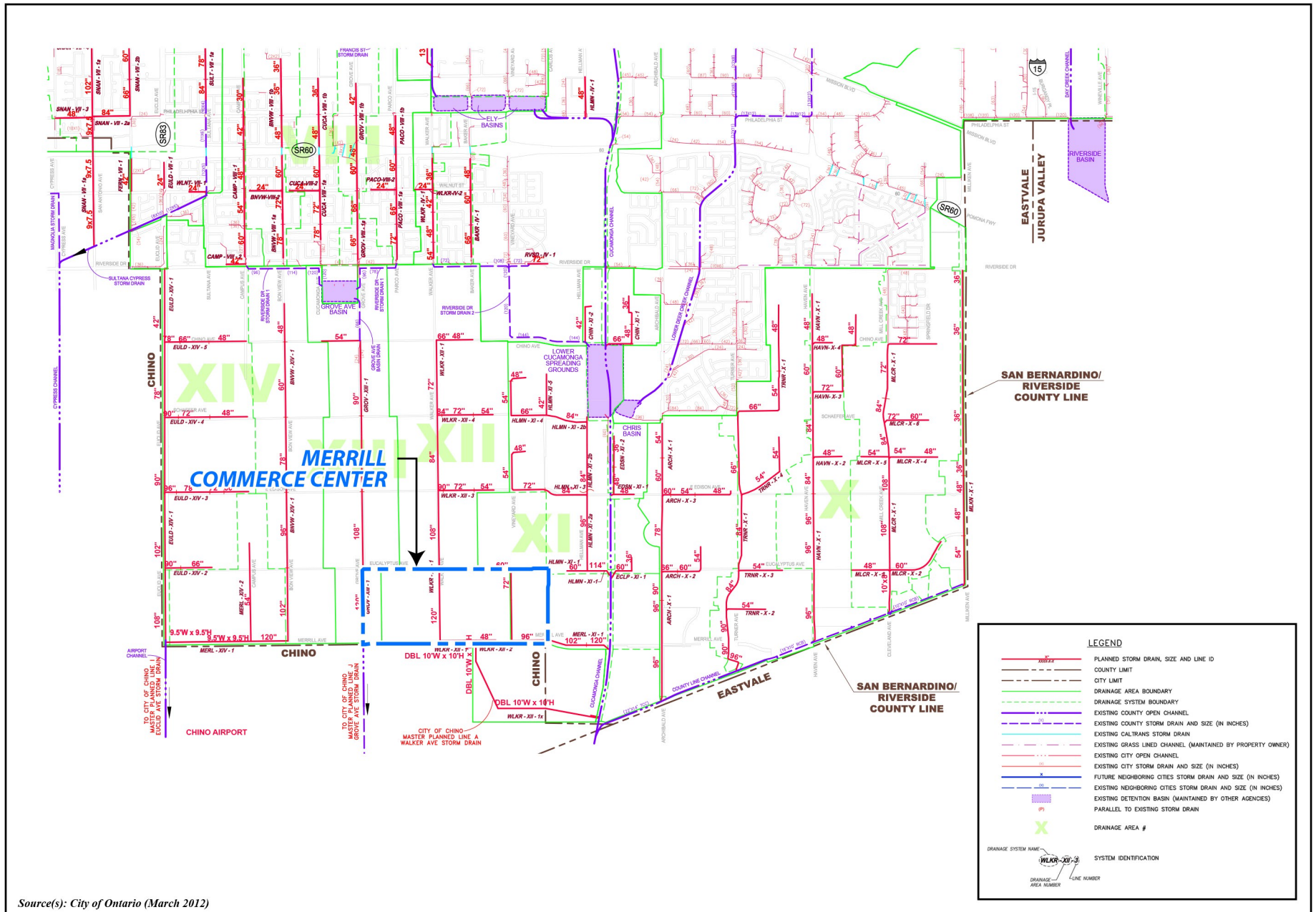
### ***B. Airport Compatibility***

The Specific Plan Area is located in the Airport Influence Areas (AIAs) of the Ontario International Airport and the Chino Airport. The Federal Aviation Administration (FAA) identifies stormwater management facilities as one of the greatest attractants to wildlife hazardous to airport operations. For this reason, all new stormwater management facilities located within the MERRILL COMMERCE CENTER will be designed to avoid the creation of open water and habitat by being designed to drain completely within a maximum 48-hour period following design storm event (i.e., 24-hour storm) and remain totally dry between storm events.



Storm Drain Infrastructure Plan

Figure 4-6



Source(s): City of Ontario (March 2012)

City of Ontario Planned Drainage Facilities

Figure 4-6A

#### 4.2.5 Dry Utilities Plan

As shown on Figure 4-7, *Dry Utilities Infrastructure Plan*, primary dry utility lines within joint trenches in Merrill Avenue will be installed to connect to existing dry utility lines at Merrill Avenue to the west of Grove Avenue and at Merrill Avenue to the east of Carpenter Avenue to fully service the Specific Plan area. Lateral dry utility lines within joint trenches will be installed in Grove Avenue, Vineyard Avenue, and Eucalyptus Avenue. The lateral dry utility line within Eucalyptus Avenue will connect to existing dry utility lines at Merrill Avenue and Archibald Avenue to the east. The lateral dry utility lines within Grove Avenue and Vineyard Avenue connect to the primary dry utility lines within Merrill Avenue.

All other dry utilities internal to the MERRILL COMMERCE CENTER will be installed underground in conjunction with the development of the MERRILL COMMERCE CENTER in accordance with applicable public utility standards and specifications and to the satisfaction of the Ontario City Engineer. The locations of other lateral connections, transformers, switches, pull boxes, and dry utility manholes will be determined at the time buildings are positioned in each planning area in conjunction with implementing development.

##### A. Fiber Optics Plan

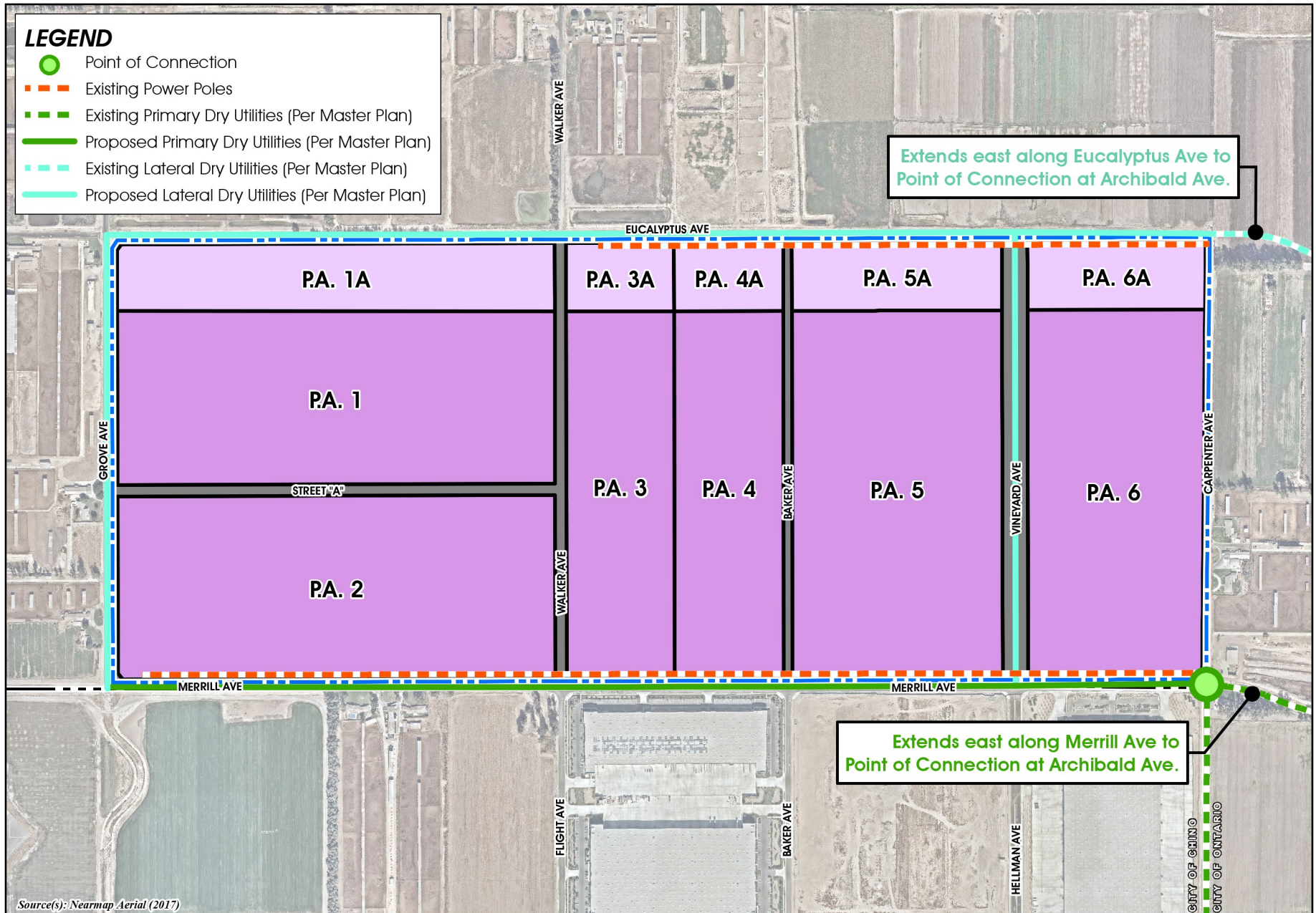
As shown on Figure 4-8, *Fiber Optics Plan*, fiber optic lines within joint trenches, per the City of Ontario's Master Fiber Optic Plan, will be installed in Merrill Avenue between Grove Avenue and Carpenter Avenue, Grove Avenue abutting Planning Areas 1 and 2, in

Eucalyptus Avenue from Grove Avenue to Carpenter Avenue, and in Vineyard Avenue abutting Planning Areas 5 and 6. The backbone street fiber optics (conduits, hand holes, tracer wire, and fiber) will be placed underground within a duct and structure system to be installed by the Master Developer in a joint trench. In-tract fiber and conduit will be installed by the Developer per the in-tract fiber optic design guidelines. Maintenance of the installed system will be the responsibility of the City/Special District. Development of the MERRILL COMMERCE CENTER requires Developer installation of all fiber optic infrastructure and peripheral equipment necessary to service the Specific Plan as a stand-alone development.

Existing power poles located along Eucalyptus Avenue and Merrill Avenue will be undergrounded as part of the Specific Plan's buildout.

#### 4.3 CONCEPTUAL GRADING PLAN

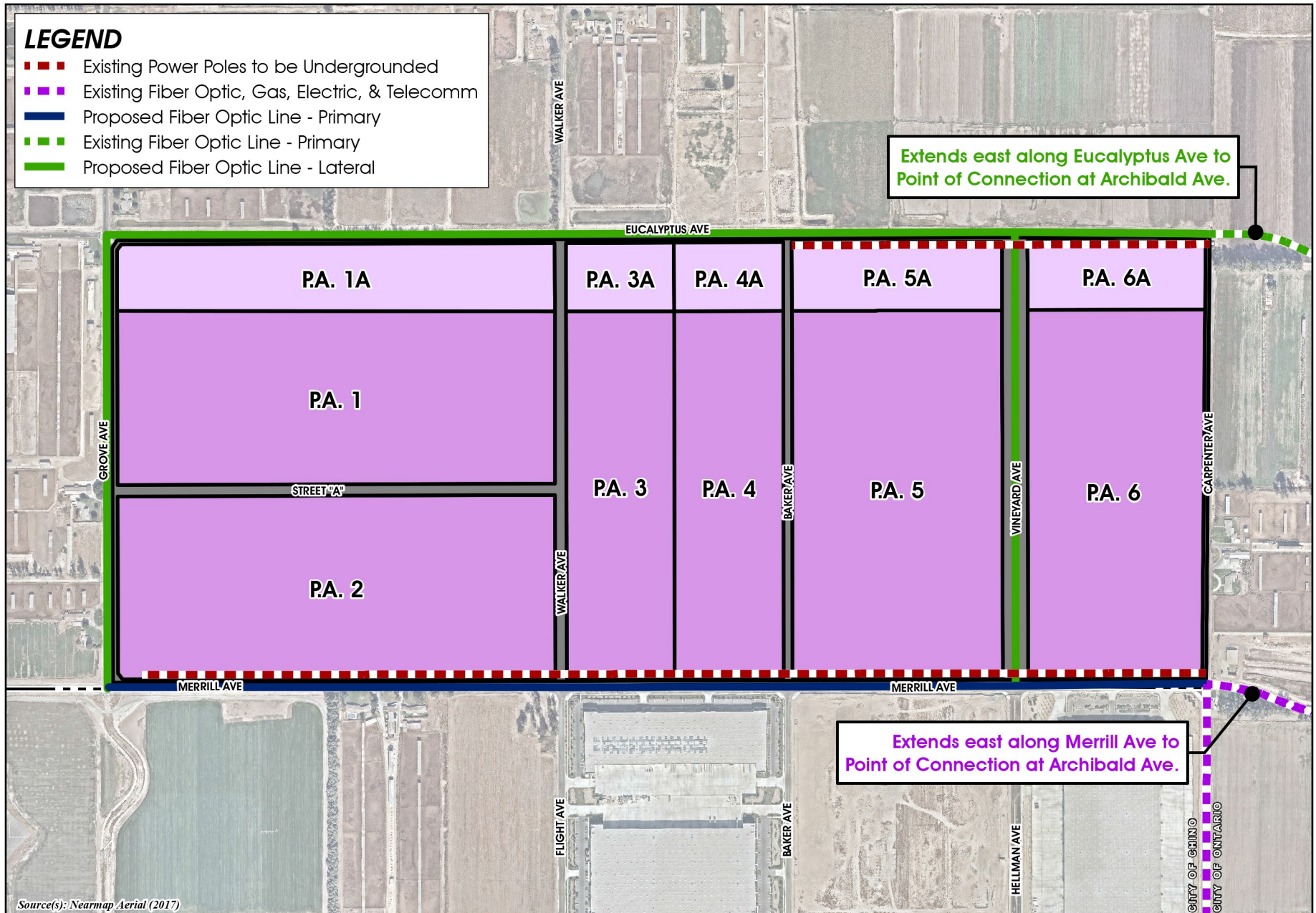
The natural topography of the MERRILL COMMERCE CENTER property is relatively flat. No unusual grading conditions are present and substantial import or export of earth materials is not expected. The primary objectives of the grading plan are to: provide stable development pads for construction; balance the cut and fill grading quantities on-site, and meet City of Ontario building standards and acceptable infrastructure gradient requirements.



Dry Utilities Infrastructure Plan

Figure 4-7





Fiber Optics Plan

Figure 4-8







# DEVELOPMENT REGULATIONS

## *CHAPTER 5*

5.1 PURPOSE AND INTENT

5.2 DEFINITION OF TERMS

5.3 APPLICABILITY

5.4 PERMITTED, CONDITIONAL, AND ANCILLARY USES

5.5 DEVELOPMENT STANDARDS

## CHAPTER 5 - DEVELOPMENT REGULATIONS

### 5.1 PURPOSE AND INTENT

This chapter formally establishes the use permissions and development standards (zoning) for the MERRILL COMMERCE CENTER property. The regulations provided herein work in concert with the architectural and landscape guidelines set forth in Chapter 6, *Design Guidelines*, to achieve the vision of the MERRILL COMMERCE CENTER.

### 5.2 DEFINITION OF TERMS

The meaning of words, phrases, titles, and terms shall be the same as provided in the City of Ontario Development Code (hereafter referred to as "Development Code"), unless otherwise specifically defined in this Specific Plan.

### 5.3 APPLICABILITY

The regulations set forth in the chapter shall apply to all development plans or agreements, tract or parcel maps, site plans, or any other action requiring administrative or discretionary approval within the 376.3-acre MERRILL COMMERCE CENTER Specific Plan area. Whenever the development standards contained herein differ from those contained in the Development Code, the provisions of this Specific Plan shall take precedence. Any development standard, condition, or situation not specifically addressed herein shall be subject to the applicable requirements of the Development Code.

### 5.4 PERMITTED, CONDITIONAL, AND ANCILLARY USES

Land within the MERRILL COMMERCE CENTER Specific Plan area and structures/facilities thereon may be developed, divided, and/or used for those activities listed in Table 5-1, *Permitted Uses*. Table 5-1 lists the permitted, conditionally-permitted, and administratively permitted land uses for each land use district established by this Specific Plan (Business Park and Industrial). A use that is not listed in Table 5-1 is a prohibited use unless otherwise allowed pursuant to the procedures described in Chapter 7, *Implementation Plan*, or applicable interpretations and determinations established by the City of Ontario Development Code.

*The symbols shown in Table 5-1 have the following meanings:*

*"P" means the land use is permitted by right of being in the proper land use district, subject to the development standards applicable to that land use district.*

*"C" means the land use is conditionally permitted, subject to the filing of a Conditional Use Permit in accordance with the requirements of the City of Ontario Development Code and must be approved by the City of Ontario Zoning Administrator or Planning Commission before the use can be established within a particular land use district.*

*"A" means an administratively permitted use is permitted in the proper land use district, subject to the granting of an Administrative Use Permit.*

Table 5-1 Permitted Uses

Use	Land Use District		Notes
	Industrial (PAs 1, 2, 3, 4, 5 & 6)	Business Park (PAs 1A, 3A, 4A, 5A & 6A)	
<b>Agricultural, Forestry, Fishing, Hunting</b>			
Temporary and Interim Agricultural Uses	P	P	Restricted to existing agriculture and dairy uses.
<b>Manufacturing</b>			
Apparel	P	P	
Beverage	C	---	
Chemical	C	---	
Computer and Electronic Products	P	P	
Electrical Equipment, Appliance, and Component	P	P	
Fabricated Metal Products	P	---	
Food	C	---	
Footwear	P	P	
Furniture and Related Products	P	P	
Leather and Allied Products	P	P	Excludes leather and hide finishing/tanning.
Machinery	P	---	
Miscellaneous Manufacturing	P	P	Includes medical equipment and supplies; jewelry and silverware; sporting and athletic goods; dolls, toys and games; office supplies; signs; and other miscellaneous manufacturing.
Plastic Products (Manufacturing)	C	---	
Rubber Products (Manufacturing)	C	---	
Plastics and Rubber Products (Assembly)	P	P	
Printing and Related Support Activities	C	C	
Textile Mills	C	---	Transforms basic fiber into fabric.

Use Legend: Permitted Use = "P" Conditional Use = "C" Administratively Permitted Use = "A" Prohibited Use = "---"

Use	Land Use District		Notes
	Industrial (PAS 1, 2, 3, 4, 5 & 6)	Business Park (PAS 1A, 3A, 4A, 5A & 6A)	
Textile Products	C	C	Transforms fabric into product, except apparel.
Wood Products	C	---	
<b>Wholesale Trade</b>			
Motor Vehicles and Motor Vehicle Parts and Supplies	P	---	
Furniture and Home Furnishings	P	P	
Professional and Commercial Equipment and Supplies	P	P	
Household Appliances and Electrical/Electronic Goods	P	P	
Hardware, Plumbing/Heating Equipment and Supplies	P	P	
Machinery Equipment and Supplies	P	---	
Miscellaneous Durable Goods	P	---	
<b>Transportation and Warehousing</b>			
Within a Wholly Enclosed Building	P	P	Includes indoor motor vehicle storage.
Outside Materials and Equipment Storage	A	---	Outdoor motor vehicle storage permitted in the Industrial land use district. Refer to Section 5.5.1(2) of this Specific Plan for outdoor storage provisions.
Fulfillment Center	P	---	
Refrigerated Warehousing and Storage	C	---	A maximum of 10% of building square footage of entire Specific Plan.
<b>Industrial Retail Sales</b>			
Maximum 15% of Building Gross Floor Area or 8,000 s.f., whichever is less	A	A	
Up to 15% of Building Gross Floor Area or 8,000 s.f., whichever is greater	C	C	
Package and Parcel Sorting and Delivery	P	---	
<b>Information</b>			
Telecommunication Facilities	A	A	

Use Legend: Permitted Use = "P" Conditional Use = "C" Administratively Permitted Use = "A" Prohibited Use = "---"

Use	Land Use District		Notes
	Industrial (PAS 1, 2, 3, 4, 5 & 6)	Business Park (PAS 1A, 3A, 4A, 5A & 6A)	
<b>Professional, Scientific, Tech. Services</b>			
Including but not limited to professional offices of legal, accounting, tax preparation, bookkeeping, payroll, architecture, engineering, and specialized design services; systems design; management, scientific, and technical consulting services; administrative and business support services; and advertising and public relations services.	A	P	
Management of Companies and Enterprises	A	P	Including corporate, subsidiary, and regional managing offices.
Scientific Research and Development Services	---	C	
Sound (Audio) Recording Facilities	---	P	
<b>Accommodation and Food Services</b>			
Food Service Contractors	---	C	
Caterers	---	C	
Mobile Food Services	---	C	
<b>Other Services (except Pubic Administration)</b>			
Motor Vehicle Cleaning, Repair and Maintenance	C	C	
Linen and Uniform Supply	C	C	Includes linen supply and industrial launders.
Motion Picture and Video Industries	---	P	Excludes movie theaters.
Auction Houses	C	C	
Couriers and Messengers	P	P	
Data Processing, Hosting and Related Services	P	P	
Electronic (Internet) Shopping and Auctions, and Mail Order Houses	P	P	Includes direct business to consumer internet retail sales, auction houses, and/or mail order retail sales.

5.5 DEVELOPMENT STANDARDS

The following standards establish the development criteria that shall apply within the Business Park and Industrial land use districts of this Specific Plan.

Table 5-2 Development Standards

Development Standards		
Site Requirements		
	BP	I
Minimum Lot Size	1.0 acre	1.0 acre
Maximum Floor Area Ratio	0.60 <sup>1</sup>	0.55 <sup>1</sup>
Overall Minimum Landscape Coverage (Landscaping shall include plantings (trees, shrubs, groundcovers, vines) and may include walkways, benches, trellises, thematic fencing, walls, and related amenities.)	10% <sup>2,3</sup>	10% <sup>2,3</sup>
Minimum Landscape Coverage on Parcels at Principal Arterial Corners	10% <sup>4</sup>	10% <sup>4</sup>

1. Maximum Floor Area Ratio shall be determined based on the gross total acreage of all parcels developed in all Planning Areas of the same land use category (Business Park (BP) or Industrial (I)), and the total gross floor area of all buildings developed in the same land use category. The FAR maximum shall not apply on a planning area by planning area or parcel by parcel basis.
2. The Overall Minimum Landscape Coverage shall be determined based on the net total acreage of all parcels in the same land use category. The minimum landscape coverage requirement shall not apply on a planning area by planning area or parcel by parcel basis.
3. The landscaped portions of Water Quality Basins shall be counted towards the total landscape coverage. Non-landscaped portions of Water Quality Basins shall not be counted towards the total landscape coverage.
4. Minimum Landscape Coverage at Principal Arterial Corners shall be determined based on the net total acreage of parcels adjoining the corner. Refer to Specific Plan Figure 6-1 "Primary Entry Treatment" for locations.

Table 5-3 Minimum Setback Requirements at Public Streets

- As measured from the public right-of-way.
- Architecture features such as cornices, eaves, canopies, decorative wall elements may encroach up to 4 feet into the setback.

	BP	I
<i>Eucalyptus Avenue</i>		
Building	23 feet	N/A
Drive Aisle and Passenger Car Parking	23 feet	N/A
Screened Loading and Storage Yards	23 feet	N/A
<i>Vineyard Avenue</i>		
Building	25 feet	25 feet
Drive Aisle and Passenger Car Parking	25 feet	25 feet
Screened Loading and Storage Yards	25 feet	25 feet
<i>Grove Avenue</i>		
Building	20 feet	20 feet
Drive Aisle and Passenger Car Parking	20 feet	20 feet
Screened Loading and Storage Yards	20 feet	20 feet
<i>Walker Avenue</i>		
Building	18 feet	18 feet
Drive Aisle and Passenger Car Parking	18 feet	18 feet
Screened Loading and Storage Yards	18 feet	18 feet
<i>Baker Avenue</i>		
Building	10 feet	10 feet
Drive Aisle and Passenger Car Parking	10 feet	10 feet
Screened Loading and Storage Yards	10 feet	10 feet

TABLE 5-3 CONTINUED

	BP	I
<i>Carpenter Avenue</i>		
Building	10 feet	10 feet
Drive Aisle and Passenger Car Parking	10 feet	10 feet
Screened Loading and Storage Yards	10 feet	10 feet
<i>Merrill Avenue</i>		
Building	N/A	23 feet
Drive Aisle and Passenger Car Parking	N/A	23 feet
Screened Loading and Storage Yards	N/A	23 feet
<i>Street "A"</i>		
Building	N/A	10 feet
Drive Aisle and Passenger Car Parking	N/A	10 feet
Screened Loading and Storage Yards	N/A	10 feet

**Table 5-4 Minimum Setback Requirements at Interior Side Yards**

- As measured from the property line.

	BP	I
<i>Interior Side Yard</i>		
Building	5 feet	0 feet
Drive Aisle and Passenger Car and Truck Parking	5 feet	5 feet
Screened Loading and Storage Yards	0 feet	0 feet

**Table 5-5 Minimum Drive Aisle and Parking Space Separation Requirements**

- As measured from the edge of pavement.

	BP	I
Adjacent to building office element	10 feet	10 feet
Adjacent to solid building wall or screen wall/fence, and <u>not</u> within a screened/ enclosed yard.	5 feet	5 feet
Adjacent to solid building wall or screen wall/fence, and within a screened/enclosed yard.	0 feet	0 feet

**Table 5-6 Maximum Building Height Requirements**

- As measured from the building's finished floor.  
 - Maximum building height in the southwest portion of Planning Area 1 is required to be lower than 95 feet subject to consistency with the Chino Airport Compatibility Plan, or in absence of an adopted Compatibility Plan, the California Airport Land Use Planning Handbook.

	BP	I
Building Height	45 feet	85 feet
Vertical Architectural Projections (towers, focal elements, cupolas, etc.)	55 feet	95 feet
Building Height and Vertical Architectural Projections (Planning Areas 5 and 6)	N/A	110 feet

### 5.5.1 Other Development Standards

- (1) Loading docks and truck parking areas shall be visually screened from Eucalyptus Avenue, Grove Avenue, Walker Avenue, Baker Avenue, Vineyard Avenue, Merrill Avenue, Street "A", and Carpenter Avenue by walls and landscaping features.
- (2) The outdoor storage of materials and equipment shall be permitted ancillary to the land uses allowed pursuant to Table 5-1. Outdoor loading and storage areas and loading doors shall be screened from view from public streets by concrete or masonry walls and landscaping. Any gates shall be lockable. Such walls and landscaping used as screening shall be a minimum eight feet (8') in height and shall be of sufficient height to screen all outdoor materials and equipment, tractors and trailers, and loading doors from view of public streets and shall not exceed 14 feet in height. The storage of outdoor materials shall not exceed the height of the screen wall as measured from the interior side of the wall.
- (3) Ground- and roof-mounted exterior mechanical equipment, heating and ventilating, air conditioning, tanks, and other mechanical devices shall be of an architecturally compatible design with the primary structure and screened when visible to the public.
- (4) Exterior lighting fixtures shall be downward directed. Pole-mounted lights shall be shielded with the light source oriented away from public streets and/or adjacent properties.
- (5) All manufacturing and processing activities shall be conducted within a wholly-enclosed building.
- (6) The following open land and occupancy limit requirements shall apply in Chino Airport Safety Zones, as established by the Chino Airport Compatibility Plan.
  - a. Zone 1: The southwestern corner of Planning Area 2 is located in Chino Airport Safety Zone 1. No buildings shall be located in Safety Zone 1.
  - b. Zone 2: Portions of Planning Areas 1 and 2 are located in Chino Airport Safety Zone 2. At least 25% of the zone shall remain as open land\* and occupancy shall be limited to 60 people per acre on average and a maximum of 120 people in any one acre.
  - c. Zone 3: Portions of Planning Areas 1, 1A, and 2 are located in Chino Airport Safety Zone 3. At least 15% of the zone shall remain as open land\* and occupancy shall be limited to 100 people per acre on average and a maximum of 300 people in any one acre.
  - d. Zone 4: Portions of Planning Areas 1 and 1A are located in Chino Airport Safety Zone 4. At least 15% of the zone shall remain as open land\* and occupancy shall be limited to 150 people per acre on average and a maximum of 450 people in any one acre.
  - e. Zone 6: Portions of all Planning Areas are located in Chino Airport Safety Zone 6. At least 10% of the zone shall remain as open land\* or an open area every ¼



mile to ½ mile is required; occupancy shall be limited to 300 people per acre on average and a maximum of 1,200 people in any one acre.

\* Open land is defined as areas at least 300 feet long by 75 feet wide (about 0.5 acre) that are relatively level and free of tall vertical objects such as structures, overhead lines/wires, and large trees and poles greater than 4 inches in diameter and taller than 4 feet above the ground. Parking lots can be considered as acceptable open lands.

- (7) The Specific Plan area is located in the Airport Influence Area (AIA) established by the Ontario International Airport Land Use Compatibility Plan (ONT ALUCP) and the AIA established by the Chino Airport Compatibility Plan (CNO ALUCP). All development in MERRILL COMMERCE CENTER shall be subject to the mandatory requirements and standards of those applicable ALUCPs, or the absence of an adopted ALUCP, the California Airport Land Use Planning Handbook.



# DESIGN GUIDELINES

## *CHAPTER 6*

6.1 PURPOSE AND INTENT

6.2 DESIGN THEME

6.3 SPECIFIC PLAN-WIDE DESIGN GUIDELINES

6.4 TRUCK COURTS AND LOADING DOCKS

6.5 GROUND OR WALL-MOUNTED EQUIPMENT

6.6 ROOFTOP EQUIPMENT

6.7 TRASH ENCLOSURES

6.8 OUTDOOR EMPLOYEE AMENITIES

6.9 OUTDOOR LIGHTING

6.10 SIGNAGE GUIDELINES

6.11 LANDSCAPE DESIGN GUIDELINES

## CHAPTER 6 - DESIGN GUIDELINES

### 6.1 PURPOSE AND INTENT

The design guidelines presented in this chapter describe the quality and character of the built environment expected for the MERRILL COMMERCE CENTER. While the design guidelines provide direction, they are meant to provide a certain level of flexibility to allow creative expression during the design of implementing development projects.

The guidelines provide criteria for architecture, lighting, energy efficiency, signage, and landscape design.

The MERRILL COMMERCE CENTER's visual identity will be expressed primarily through landscape, hardscape, and signage elements. The architectural design guidelines contained herein are presented in a manner that ensures consistent architectural expression across the Specific Plan area, while allowing for flexibility in modern-day building design.

*All photographs, illustrations, and diagrams contained in these Design Guidelines serve as visual aids to convey the overall theme. Exact replication of the examples is neither required nor anticipated.*



*Conceptual design theme of the MERRILL COMMERCE CENTER.*

The objectives of these Design Guidelines are as follows:

- To provide the City of Ontario with the assurance that the MERRILL COMMERCE CENTER will develop in accordance with the quality and character described within this Specific Plan.
- To provide guidance to developers, builders, engineers, architects, landscape architects, and other professionals to achieve and maintain the desired design quality.
- To provide an aesthetic benchmark for City staff, the Planning Commission, and the City Council in their review of the design of future implementing development projects in the Specific Plan area.
- To provide guidelines that steer the MERRILL COMMERCE CENTER to convey a contemporary aesthetic theme and character while allowing flexibility for practical application and creative expression.
- To provide guidelines for energy efficiency that can be implemented in the site planning, design, and construction phases of the Specific Plan's implementation to minimize waste deposited at landfills, decrease fossil fuel consumption, and reduce domestic water consumption.
- To ensure that the Specific Plan implements the intent of the Ontario Plan and the City's Development Code.

*Guidelines that promote energy efficiency are indicated with an ("E") throughout this chapter.*

The Design Guidelines presented in this chapter apply to all development within MERRILL COMMERCE CENTER, regardless of the land use category. These guidelines may be subject to modification and contemporary interpretation to allow for responses to unanticipated conditions, including but not limited to changes in the real estate market, needs and desires of building users, technology advancements, and fluctuations in economic conditions.

## 6.2 DESIGN THEME

The MERRILL COMMERCE CENTER is a contemporary employment center containing Industrial and Business Park land uses. It will provide businesses easy access to the regional transportation network, proximity to workers, and proximity to the Ports of LA and Long Beach (approximately 53 miles to the southwest).

The theme features a contemporary design aesthetic, which provides architectural styling with attractive detailing, steel accents, a light-toned color palette, and timeless features. Signs are modern, lighting is focused and directed, landscaping is colorful and drought-tolerant, and design features are applied that lower energy use demands of building operations.



### 6.3 SPECIFIC PLAN-WIDE DESIGN GUIDELINES

This section sets forth design guidelines that apply to all planning areas within MERRILL COMMERCE CENTER. Developers, builders, engineers, architects, landscape architects, and other design professionals should utilize these guidelines in order to maintain design continuity throughout the Specific Plan area.

#### 6.3.1 Architecture Design Guidelines

##### A. Design Theme

The architectural style of the MERRILL COMMERCE CENTER emphasizes building massing over structural articulation. Buildings are characterized by simple and distinct cubic masses with interlocking volumes of wall planes, colors, and materials to create visual appeal. Exterior building colors are light and gray tones with stone, glass, or steel materials at focal points, such as around building entrances and near outdoor gathering spaces. Additionally, architectural designs may mix colors, materials, and textures to articulate façades and create visual appeal.

Design elements are selected to be compatible in character, massing, and materials in order to promote a clean and contemporary feel. Individual creativity and identity is encouraged, but design integrity and compatibility must be maintained among all buildings and planning areas to reinforce a unified image and campus-like setting within the MERRILL COMMERCE CENTER.

##### B. Building Form

Building form is one of the primary elements of architecture. Numerous design aspects, including shape, mass (size), scale, proportion, and articulation, are elements of a building's "form."



*Although provided for illustrative purposes only, the image above shows how building façades visible from public roadways are to incorporate angular changes in massing, building materials, color, texture, and accents.*

Building forms are especially important for building façades that are visible along the following public view corridors:

- Building façades in Planning Areas 1, 1A, and 2 that are visible from Grove Avenue, a principal arterial;
- Building façades in Planning Areas 1A, 3A, 4A, 5A and 6A that are visible from Eucalyptus Avenue, a collector road;
- Building façades in Planning Areas 5, 5A, 6, and 6A that are visible from Vineyard Avenue, a principal arterial; and
- Building façades in Planning Areas 2, 3, 4, 5, and 6 that are visible from Merrill Avenue, a collector road.

The following guidelines apply to buildings within the MERRILL COMMERCE CENTER to ensure that development is visually consistent, appealing, and inviting: Note that building faces that orient inward to truck courts or service areas and that are not visible from public roads, or publicly accessible viewing areas, are not required to adhere to the below building form guidelines.

- (1) Use simple geometric shapes as the overall building form. Rectangular forms are encouraged to promote balance and visual interest. Avoid arbitrary, complicated building forms.
- (2) Long horizontal wall planes visible from a public street should include periodic changes in exterior building materials, color, decorative accents, and/or articulated features.
- (3) Modulation and variation of building masses between adjacent buildings visible from public streets is encouraged.
- (4) Make pedestrian entrances to buildings (with the exception of service doors and emergency exit doors) obvious through changes in massing, color, and/or building materials.

- (5) Pedestrian and ground-level building entries intended for visitor use should be recessed or covered by architectural projections, roofs, or arcades in order to provide shade and visual relief.
- (6) Architectural and trim detailing on building façades shall be clean, simplistic, and not overly complicated
- (7) Materials applied to any elevations shall turn the corner of the building to a logical termination point in relation to architectural features or massing.



*Although provided for illustrative purposes only, the image above shows an example of how the architecture of buildings is to be articulated at pedestrian entrances.*

### C. Building Materials, Colors, and Textures

Building materials and colors play a key role in developing a clean, contemporary visual environment; therefore, the selected exterior materials, colors, and textures should complement one another throughout the MERRILL COMMERCE CENTER. Slight variations are encouraged to provide visual interest.

- (1) Appropriate primary exterior building materials include concrete and similar materials, as well as tilt-up panels. The primary materials should be accented by secondary materials including but not limited to natural or fabricated stone, Fire resistant wood siding (horizontal or vertical), and metal.
- (2) Trim details may include metal finished in a consistent color, plaster, or concrete elements finished consistently with the building treatment. Use of overly extraneous “themed” detailing, like oversized or excessive foam cornice caps, foam moulding and window detailing is discouraged.
- (3) Material changes should occur at intersecting planes, preferably at the inside corners of change of wall planes, or where architectural elements intersect.
- (4) Primary exterior building colors should be light and gray tones. Darker and/or more vibrant accent colors may be provided in focal point areas, such as around building entrances and near outdoor gathering spaces.
- (5) Bright primary colors, garish use of color, and arbitrary patterns or stripes that will clash with this color palette are discouraged, except in signage logos.
- (6) Exposed downspouts (only permitted if not in public view), service doors and mechanical screen colors shall be the same color as the adjacent wall.
- (7) If downspouts are needed in areas of public view, they shall be designed as internal downspouts.

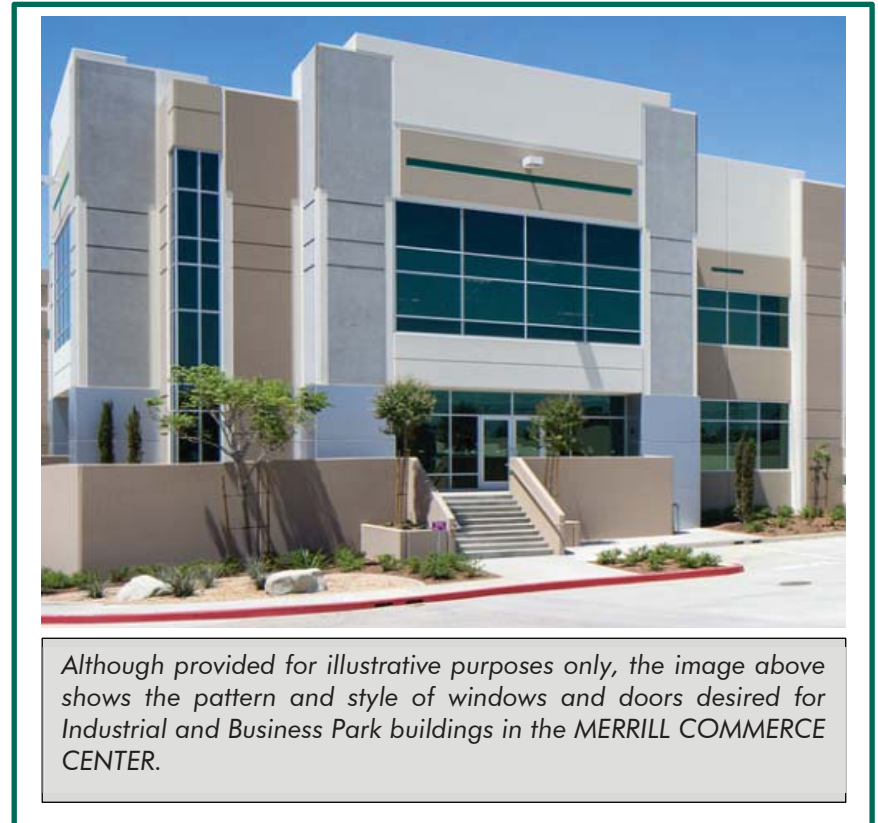


*Although provided for illustrative purposes only, the image above shows an example of the conceptual accent building materials, colors, and textures desired for the Industrial and Business Park buildings within the MERRILL COMMERCE CENTER.*

### D. Windows and Doors

The patterns of window and door openings shall correspond with the overall rhythm of the building and should be consistent in form, pattern, and color within each planning area. Guidelines for windows and doors within the MERRILL COMMERCE CENTER are as follows:

- (1) When possible, the positioning of doors and windows on individual building façades should occur in a symmetrical and repetitive pattern to create continuity.
- (2) Window styles and trims shall be consistent in form and color in each planning area. Window trims shall be finished in a consistent color on each building.
- (3) Gold or unfinished/untreated metal window or door frames are prohibited. Clear silver anodized frames are allowed.
- (4) Glass shall be clear or colored with subtle reflectiveness. Silver/reflective glass is prohibited. Green tinted windows with subtle reflectiveness are allowed.
- (5) Pedestrian entry doors to buildings shall be clearly defined by features such as overhangs, awnings, and canopies or embellished with decorative framing treatments – including but not limited to accent trim. Dark and confined entries, flush doorways (except emergency exit and service doors), and tacked-on entry alcoves are discouraged.





### F. Walls and Fences

The following guidelines for walls and fencing will ensure that these features complement the overall the MERRILL COMMERCE CENTER design theme, and are attractive from public viewing areas, scaled appropriately, durable, and integrated consistently within the Specific Plan area.

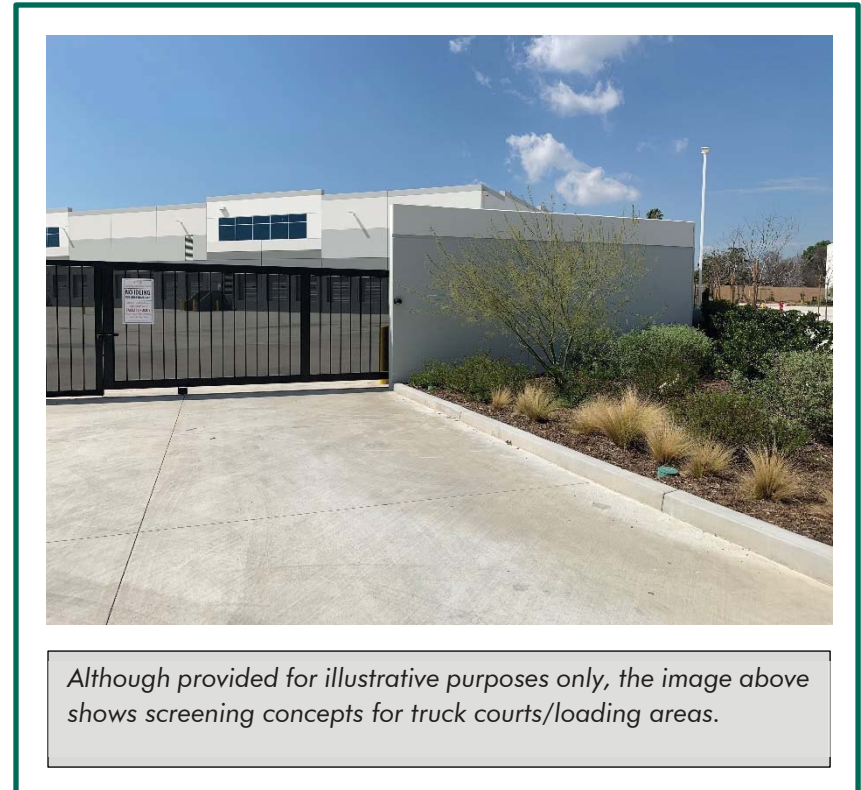
- (1) Freestanding walls and fences should not exceed a height of 14 feet, measured from the base of the wall/fence to the top of the wall/fence.
- (2) Landscaping may be used for visual screening instead of walls and fences in locations where a solid physical barrier is not needed.
- (3) Walls and fences in public view should be built with attractive, durable materials.
- (4) Chain-link fencing is not permitted as perimeter fencing and/or within public view.
- (5) Along public street frontages, long expanses of wall surfaces should be offset and/or architecturally treated to prevent monotony. Techniques to accomplish this may include, but are not limited to: openings, material changes, pilasters and posts, and staggered sections.
- (6) Wall and fencing materials shall be compatible with other design elements of the MERRILL COMMERCE CENTER.



*Although wall and fence design may vary, the concepts shown above provide examples of an ornamental iron fence (top), architecturally-enhanced screen wall (bottom-left), and freestanding wall (bottom-right).*

## 6.4 TRUCK COURTS AND LOADING DOCKS

- (1) Loading doors, service docks, and equipment areas should be oriented or screened to reduce visibility from public roads and publicly accessible locations within the MERRILL COMMERCE CENTER. Screening may be accomplished with solid walls or fences that are compatible with the architectural expression of the building. Screening may also be accomplished by landscaping.
- (2) Business park buildings located along Eucalyptus Avenue shall not have loading docks on the building façade(s) facing Eucalyptus Avenue.
- (3) No loading or unloading activity is permitted to take place from public streets/view.
- (4) Adequate queuing distance should be provided on-site in front of security gates to avoid the circumstance of trucks stacking on public streets waiting to enter at gates.
- (5) Truck and service vehicle entries should be designed to provide clear and convenient access to truck courts and loading areas such that passenger vehicle, pedestrian, and bicycle circulation is not adversely affected by truck movements.
- (6) Loading bays that are utilized by refrigerated trailers shall have dock seals and be equipped with plug-in electrical outlets. (E)
- (7) Conduit should be installed in truck courts in logical locations that would allow for the future installation of charging stations for electric trucks, in anticipation of this technology becoming available in the lifetime of the MERRILL COMMERCE CENTER. (E)



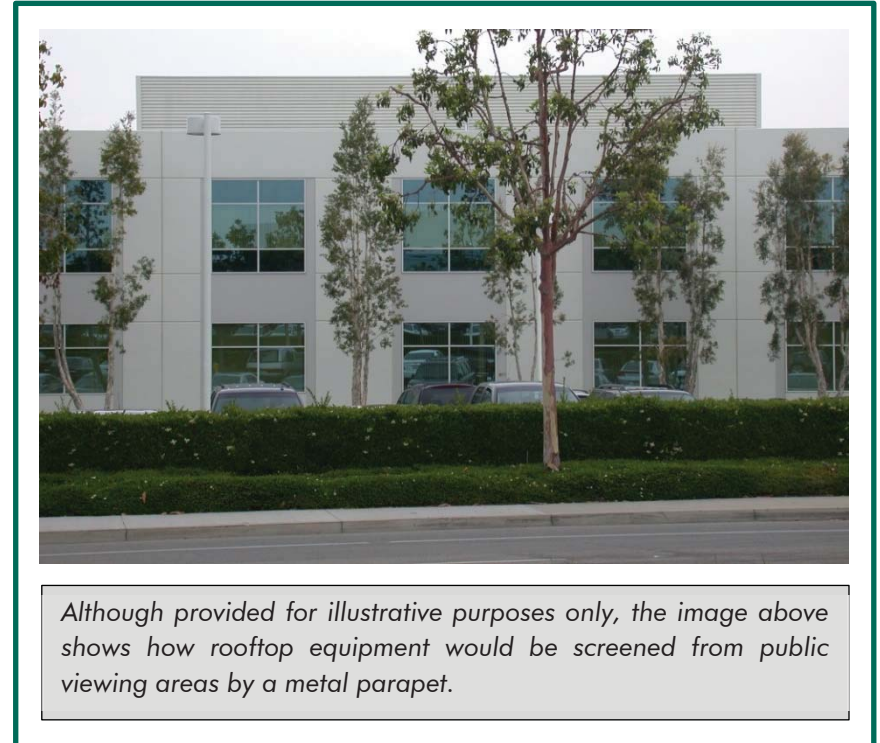
### 6.5 GROUND OR WALL-MOUNTED EQUIPMENT

- (1) Ground-mounted equipment, including but not limited to mechanical or electrical equipment, emergency generators, boilers, storage tanks, risers, and electrical conduits, should be screened from public viewing areas including adjacent public roads. Screening may be accomplished with solid walls, or landscaping.
- (2) Electrical equipment rooms should be located within the building envelope. Pop-outs or shed-like additions are discouraged.
- (3) Wall-mounted items, such as electrical panels, should not be located on the building façade facing adjacent public roads/view. Wall-mounted items should be screened or incorporated into the architectural elements of the building so as not to be visually apparent from the street or other public areas.

### 6.6 ROOFTOP EQUIPMENT

- (1) Rooftop equipment, including but limited to mechanical equipment, electrical equipment, storage tanks, cellular telephone facilities, satellite dishes, vents, exhaust fans, smoke hatches, and mechanical ducts, shall be screened by rooftop screens or parapet walls so as not to be visible by the public.
- (2) Integrate rooftop screens (i.e. parapet walls) into the architecture of the main building. Wood finished rooftop screens are prohibited.

- (3) Design the roofs of Industrial buildings to support the future installation of solar panels (E).
- (4) Roof access (via roof ladders or other means) must be located interior to the building.

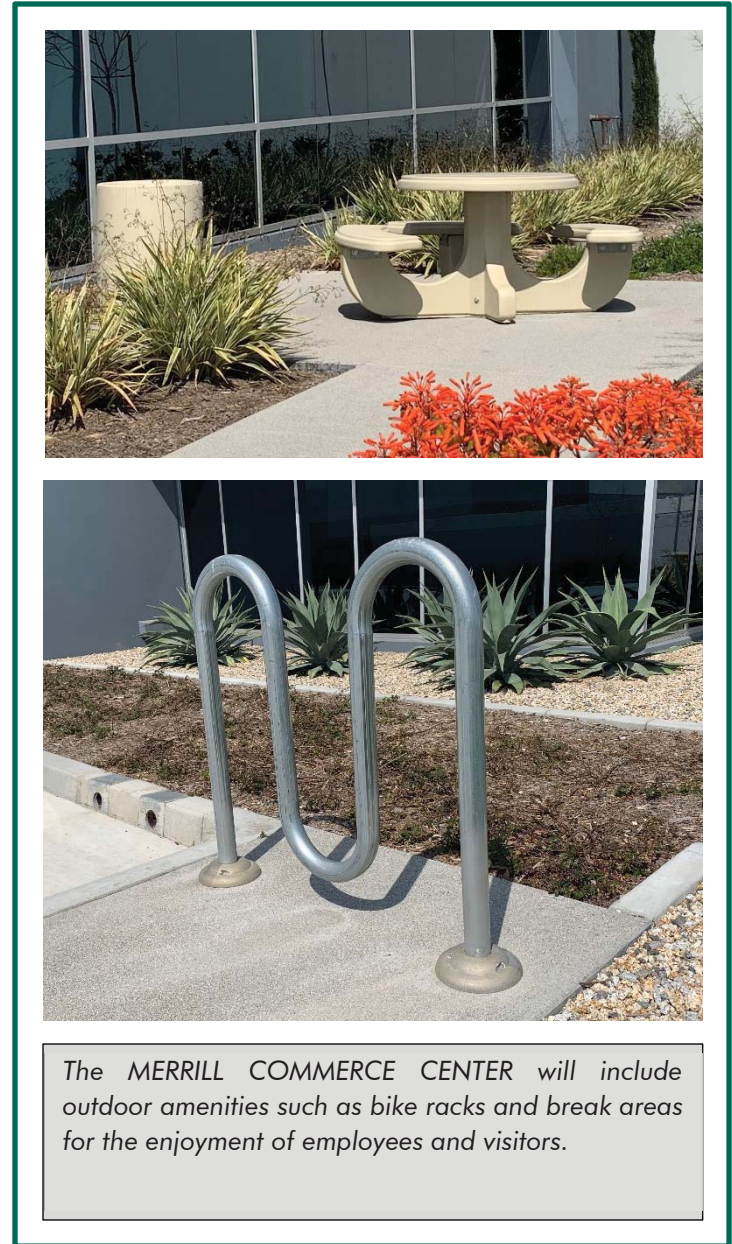


## 6.7 TRASH ENCLOSURES

- (1) All outdoor refuse containers shall be screened with locks within a permanent, durable enclosure and should be oriented so they are not visible from public roads/views. The enclosure's design shall reflect the architectural style of adjacent buildings and use similar, high-quality materials.
- (2) All outdoor trash enclosures shall be constructed with solid roofs to prevent exposure of dumpster contents to rainfall and prevent polluted stormwater runoff from these structures. (E)
- (3) Refuse collection areas shall be located behind or to the side of buildings, away from the building's main entrance and public view.
- (4) Buildings shall be designed to meet all Integrated Waste Department requirements, including the requirements for Sizing of Storage, Location of Collection Area, Accessibility for Collection Vehicles, and Collection of Sorted/Diverted Waste Types.

## 6.8 OUTDOOR EMPLOYEE AMENITIES

- (1) Bicycle racks should be provided at each building or in a common area that serves multiple buildings to encourage non-vehicular circulation.
- (2) Industrial buildings should include an outdoor employee amenity area, including tables and chairs so that workers do not have to travel off-site for outdoor enjoyment.



*The MERRILL COMMERCE CENTER will include outdoor amenities such as bike racks and break areas for the enjoyment of employees and visitors.*

## 6.9 OUTDOOR LIGHTING

Outdoor lighting of the MERRILL COMMERCE CENTER is an essential architectural component that provides aesthetic appeal, enhances safe pedestrian and vehicular circulation, and adds to security. Lighting within the public rights-of-way shall adhere to applicable City of Ontario requirements.

All other lighting on private property in the Specific Plan should adhere to the following guidelines.

- (1) Minimize glare and “spill over” light onto public streets and adjacent properties by using downward-directed lights and/or cutoff devices on outdoor lighting fixtures, including spotlights, floodlights, electrical reflectors, and other means of illumination for signs, structures, parking, loading, unloading, and similar areas. Where desired, illuminate trees and other landscape features by concealed uplight fixtures. Limit light spillover or trespass to one-quarter foot-candle or less, measured from within five feet of any adjacent property line.
- (2) Select all lighting fixtures used in the Specific Plan area from the same – or complementary – family of fixtures with respect to design, materials, fixture color, and light color. Use of LED lighting is encouraged. (E)
- (3) Lights should be unbreakable plastic, recessed, or otherwise designed to reduce the problems associated with damage and replacement of fixtures.
- (4) Neon and similar types of lighting are prohibited in all areas within the MERRILL COMMERCE CENTER.
- (5) Locate all electrical meter pedestals and light switch/control equipment in areas with minimum public visibility or screen them with appropriate plant materials.



*Although provided for illustrative purposes only, the image above conceptually show lighting fixtures that are consistent with the overall theme of the Specific Plan and minimize glare and spill over light onto public streets and adjacent properties.*

- (6) Illuminate parking lots, loading dock areas, pedestrian walkways, building entrances, and public sidewalks to the level necessary for building operation and security reasons. Dimmers and motion detectors are permitted.
- (7) Along sidewalks and walkways, the use of low mounted fixtures (ground or bollard height), which reinforce the pedestrian-scaled, are encouraged.
- (8) Use exterior lights to accent entrances, plazas, activity areas, and special features.
- (9) To illuminate parking lots or parking structures and their pedestrian links that provide more than five parking spaces for use by the general public, provide minimum coverage of one foot-candle of light with a maximum of eight foot-candles on the parking or walkway surface, unless otherwise approved by the City of Ontario for visibility and security.
- (10) To illuminate aisles and passageways within a building complex, provide a maximum of one-half to one foot-candle of maintained lighting.
- (11) High-Pressure Sodium (HPS) light fixtures are prohibited for site lighting.
- (12) Lighting is prohibited that could be mistaken for airport lighting or that would create glare in the eyes of pilots of aircraft using the nearby Chino Airport or Ontario International Airport.



*Although provided for illustrative purposes only, the image above demonstrates appropriate use of exterior lights to accent entrances and special features.*

### 6.10 SIGNAGE GUIDELINES

Signage within the Specific Plan area serves a variety of purposes. Signs will identify the MERRILL COMMERCE CENTER and its building occupants and ensure the efficient circulation of vehicle traffic within the site by identifying vehicular entry points and directing vehicles to their on-site destinations. Also, signage will enhance the pedestrian experience through the design of wayfinding components: directories, directional signage, and destination identifiers.

As such, clear, concise, and easy-to-understand signage that is also visually appealing is vitally important for positive worker and visitor experiences at the MERRILL COMMERCE CENTER. General design requirements for signage are as follows:

- (1) Signage should be compatible with and complementary to the building's exterior materials, colors, and finishes.
- (2) The dimensions and shape of free-standing signs and sign panels or elements mounted on building façades or marquees shall be scaled proportionately to the architecture.
- (3) All signs shall be contained within the parcel to which applicable and shall be so oriented as to preclude hazardous obstructions to person and/or vision of pedestrians and/or vehicle operators.
- (4) Building occupant identification signage shall be in keeping with the character established for the MERRILL COMMERCE CENTER with variations allowed to accommodate individual user identities/corporate branding standards.



*Although provided for illustrative purposes only, the image above demonstrates integration of building occupant signage with the architectural style and color palette of the building.*

- (5) All signs are expected to be of the highest quality to pass eye-level examination and scrutiny, and shall comply with the following fabrication specifications:
  - (a) Signs should be constructed to eliminate burrs, dents, cutting edges and sharp corners;
  - (b) Welds on exposed surfaces should be imperceptible in the finished work;

- (c) Surfaces which are intended to be flat should be without dents, bulges, oil canning, gaps, or other physical deformities;
  - (d) All fasteners shall be concealed;
  - (e) Access panels shall be tight-fitting, light-proof and flush with adjacent surfaces;
  - (f) Manufacturers' recommended fabrication procedures regarding expansion/contraction, fastening and restraining of acrylic plastic shall be followed; and
  - (g) Painted, polished and plated surfaces shall be unblemished in the finished work.
- (6) Prohibited sign components include the following:
- (a) Letters with exposed fastening and unfinished edges (unless architecturally consistent);
  - (b) Paper, cardboard, Styrofoam or untreated cloth;
  - (c) Visible moving parts or simulated moving parts by means of fluttering, rotation, or reflecting devices; and
  - (d) Flashing and strobing.
- (7) All conductors, transformers, cabinets, housing, and other equipment for the illumination of signs shall be concealed and/or incorporated into the building architecture.
- (8) Signs shall be constructed so as to not have exposed wiring, raceways, ballasts, conduit, transformers, or the like.
- (9) Direction signs may be located at any vehicular or pedestrian decision point.
- (10) Vehicular direction signs shall clearly direct to destination anchors within the MERRILL COMMERCE CENTER, on-site parking areas, and truck routes.
- (11) Vehicular direction signs shall be consistent in size, shape, and design throughout the MERRILL COMMERCE CENTER.
- (12) Typography on vehicular direction signs should be legible and have enough contrast to be read from an appropriate windshield viewing distance.
- (13) Vehicular direction signs shall incorporate reflective vinyl copy for night-time illumination.
- (14) All direction signs and general information signs (e.g., restrooms, telephones, fire extinguishers, elevators, escalators, stairs) throughout the MERRILL COMMERCE CENTER shall incorporate the appropriate identity symbol as established by the Society of Environmental Graphic Design (SEGD) and comply with all State, local and federal regulations.
- (15) All traffic control signs, whether on public or private property, shall conform to the California Manual on Uniform Traffic Control Devices (MUTCD).
- (16) All signage shall comply with the City of Ontario Development Code's Sign Regulations.



### 6.11 LANDSCAPE DESIGN GUIDELINES

These *Landscape Design Guidelines* establish landscape principles and standards that apply to all planning areas within the MERRILL COMMERCE CENTER. The intent is to ensure that plant materials, entries and monuments, streetscapes, and other amenities are compatible with the overall design theme and that all implementing development projects are united under a common design vocabulary. These *Landscape Design Guidelines*, when taken with the companion *Architectural Design Guidelines* provided herein, establish an identity for the MERRILL COMMERCE CENTER that is contemporary, visually appealing, and contextually sensitive to the surrounding area.

Although a great deal of specific design information is presented herein, these Guidelines are not intended to establish a set of rigid landscaping requirements for the MERRILL COMMERCE CENTER. It is recognized that, at times, there will be a need to adapt these Guidelines to meet certain parcel-specific or user-identity requirements. As such, these *Landscape Guidelines* are intended to be flexible, and are subject to modification over time. However, it is critical to the MERRILL COMMERCE CENTER's long-term design integrity that any deviations from these Landscape Guidelines are in keeping with the spirit of the core elements of the overall theme described herein to ensure a cohesive and unified landscape concept across the MERRILL COMMERCE CENTER.

The landscaping plan serves the dual purpose of adding visual appeal while being sensitive to the environment and the Southern California climate by using drought-tolerant materials. Landscaping occurs throughout the MERRILL COMMERCE CENTER, but most prominently at street corners, along roadways, and at building entrances and in passenger car parking lots.

Primary Entry Treatments and Secondary Entry Treatments provided at entry corners welcome employees and visitors to the MERRILL COMMERCE CENTER. Primary entry treatments featuring signs and landscaping occur at the corners of Eucalyptus Avenue with Grove Avenue and with Vineyard Avenue, and Merrill Avenue with Grove Avenue and with Vineyard Avenue. Secondary Entry Treatments featuring colorful accent trees, shrubs, and groundcover occur at the corners of entrances into the MERRILL COMMERCE CENTER.

Streetscape landscaping presents a combination of evergreen and deciduous trees, low shrubs, and masses of groundcovers to create a visually pleasing experience for pedestrians and passing motorists.

As identified on Figure 6-1, *Conceptual Landscape and Greenspace Plan*, the MERRILL COMMERCE CENTER's thematic identity is reinforced by the landscape design of interfaces, monumentation, streetscapes, and pedestrian paths. Furthermore, the recommended plant palette, community elements, and hardscape materials work in concert to reinforce and emphasize the MERRILL COMMERCE CENTER's landscape theme.



*Landscape theme for drought-tolerant planting areas in the MERRILL COMMERCE CENTER, provided for illustrative purposes only.*

### 6.11.1 Plant Palette

The plant palette for the MERRILL COMMERCE CENTER includes colorful shrubs and groundcovers, ornamental grasses and succulents, and evergreen and deciduous trees – including flowering varieties – that are commonly used throughout Southern California and complement the MERRILL COMMERCE CENTER’s design theme and setting. Many of the plant materials are water-efficient species native to Southern California or naturalized to the arid Southern California climate.

Table 6-1, Plant Palette, provides a list of plant materials approved for use in the MERRILL COMMERCE CENTER. The plants listed in Table 6-1 establish a base palette for the MERRILL COMMERCE CENTER landscape design. Other similar plant materials may be substituted for species listed in Table 6-1, provided the alternative plants are drought-tolerant and complement the MERRILL COMMERCE CENTER design theme.

### 6.11.2 Irrigation

The following general irrigation concepts shall be considered in the design and installation of irrigation systems within the MERRILL COMMERCE CENTER:

- (1) All landscaped areas should be equipped with a permanent, automatic, underground irrigation system. Drip systems are encouraged in all areas needing irrigation. Irrigation systems must conform to all City of Ontario requirements. (E)
- (2) Irrigation systems should be designed to apply water slowly, allowing plants to be deep soaked and to reduce run-off. (E)
- (3) Connect the irrigation system to the recycled water conveyance system (E)
- (4) “Pop-up” type sprinkler heads may be used adjacent to all walks, drives, curbs (car overhangs), parking areas and public right-of-way but must be designed to prevent all run-off and overspray
- (5) The design of irrigation systems, particularly the location of controller boxes, valves, and other above-ground equipment (e.g., backflow prevention devices), shall be incorporated into the overall landscaping design. Where aboveground equipment is provided, it shall be screened or otherwise removed from public view, to the extent possible.
- (6) The irrigation system shall be programmed to operate between 6:00pm and 6:00am. (E)

Table 6-1 Plant Palette

BOTANICAL NAME	COMMON NAME	APPLICATION	WATER USE	BOTANICAL NAME	COMMON NAME	APPLICATION	WATER USE
<b>TREES</b>				<b>SHRUBS AND GROUNDCOVERS</b>			
ARBUTUS 'MARINA'	MARINA STRAWBERRY TREE	ACCENT TREE / CANOPY TREE	L	CALLISTEMON 'LITTLE JOHN'	DWARF CALLISTEMON	MIDGROUND / FOREGROUND	L
CERCIDIUM X 'DESERT MUSEUM'	DESERT MUSEUM PALO VERDE	ACCENT TREE	L	CARISSA M. 'GREEN CARPET'	DWARF NATAL PLUM	GROUNDCOVER	M
CERCIS C. 'FOREST PANSY'	EASTERN REDBUD	STREET TREE / WATER QUALITY	M	CAREX DIVULSA	BERKELEY SEDGE	GROUNDCOVER / FOREGROUND	L
CHITALPA T. 'MORNING CLOUD'	CHITALPA	STREET TREE	L	CISTUS PURPUREUS	PURPLE ROCK ROSE	MID-GROUND / WATER QUALITY	L
GEIJERA PARVIFLORA	AUSTRALIAN WILLOW	CANOPY TREE / BACKGROUND	L	COTYLEDON ORBICULATA	PIG'S EAR	FOREGROUND / ACCENT	L
KOELRUTERIA BIPINNATA	CHINESE FLAME TREE	STREET TREE	L	DIANELLA 'LITTLE REV'	LITTLE REV FLAX LILY	FOREGROUND	L
LAGERSTROEMIA F. 'NATCHEZ'	CRAPE MYRTLE	STREET TREE / CANOPY TREE	M	DIANELLA T. 'VARIEGATA'	VARIEGATED FLAX LILY	FOREGROUND / MID-GROUND	M
MAGNOLIA SPP.	ST. MARY'S MAGNOLIA	STREET TREE / CANOPY TREE	M	ECHEVERIA A. 'LIPSTICK'	ECHEVERIA	ACCENT	L
OLEA EUROPAEA – SWAN HILL	SWAN HILL FRUITLESS OLIVE	ACCENT TREE	L	FESTUCA MAIREI	ATLAS FESCUE	FOREGROUND / MID-GROUND	L
PINUS ELДАРICA	AFGHAN PINE	STREET TREE / BACKGROUND	M	HESPERALOE 'BRAKELIGHTS'	BRAKELIGHTS YUCCA	MID-GROUND / ACCENT	L
PISTACHIA CHINENSIS	CHINESE PISTACHE	STREET TREE	M	JUNCUS PATENS	CALIFORNIA GRAY RUSH	WATER QUALITY	L
PLATANUS A. 'BLOODGOOD'	LONDON PLANE TREE	STREET TREE / BACKGROUND	M	KURAPIA	KURAPIA	GROUNDCOVER	L
PLATANUS RACEMOSA	CALIFORNIA SYCAMORE	WATER QUALITY / CANOPY TREE	M	LANTANA MONTEVIDENSIS	PURPLE LANTANA	GROUNDCOVER	L
PODOCARPUS GRACILIOR	FERN PINE	STREET TREE / CANOPY TREE	M	LANTANA 'NEW GOLD'	NEW GOLD LANTANA	GROUNDCOVER / FOREGROUND	L
TRISTANIA CONFERTA	BRISBANE BOX	CANOPY TREE / BACKGROUND	M	LAVANDULA X I. 'PROVENCE'	PROVENCE FRENCH LAVENDER	FOREGROUND	L
QUERCUS AGRIFOLIA	COAST LIVE OAK	STREET TREE / ACCENT TREE	L	LEYMUS C. 'CANYON PRINCE'	CANYON PRINCE WILD RYE	MID-GROUND / WATER QUALITY	L
QUERCUS SUBER	CORK OAK	STREET TREE	L	MUHLENBERGIA C. 'REGAL MIST'	PINK MUHLY	MID-GROUND / WATER QUALITY	L
QUERCUS ILEX	HOLLY OAK	STREET TREE / CANOPY TREE	L	OLEA 'MONTRA'	LITTLE OLLIE	MID-GROUND	L
<b>SHRUBS AND GROUNDCOVERS</b>				RHAPHIOLEPIS INDICA 'CLARA'	INDIAN HAWTHORN	MID-GROUND / BACKGROUND	M
ACACIA REDOLENS	ACACIA	GROUNDCOVER	L	ROSMARINUS O. 'PROSTRATUS'	PROSTRATE ROSEMARY	GROUNDCOVER	L
ALOE ARBORESCENS	TORCH ALOE	ACCENT / BACKGROUND	L	SALVIA GREGGII	AUTUMN SAGE	MID-GROUND	L
ALOE PETRICOLA	STONE ALOE	ACCENT / BACKGROUND	L	SALVIA L. 'SANTA BARBARA'	SANTA BARBARA MEXICAN SAGE	MID-GROUND / WATER QUALITY	L
ALOE STRIATA	CORAL ALOE	ACCENT	L	TURF 'MARATHON IIE'	SODDED TURF	GROUNDCOVER	H
ARBUTUS U. 'COMPACTA'	DWARF STRAWBERRY TREE	BACKGROUND	L	WESTRINGIA 'WYNYABBIE GEM'	COAST ROSEMARY	BACKGROUND	L
BACCHARIS X 'CENTENNIAL'	CENTENNIAL COYOTE BRUSH	WATER QUALITY	L				

### 6.11.3 Streetscapes

Streetscape landscaping plays an important role in creating a sense of place. In addition, streetscapes serve functional purposes, including screening undesirable views from public view. Within the MERRILL COMMERCE CENTER, streetscapes are planted with a combination of evergreen and deciduous trees, low shrubs, and masses of groundcovers to create a visually pleasing experience for pedestrians and passing motorists. The landscaping plant palette for streetscapes should link the roadways to the rest of the MERRILL COMMERCE CENTER and should reflect the CENTER's landscape design theme.

The conceptual streetscape landscape treatments within the MERRILL COMMERCE CENTER are presented on the following pages.

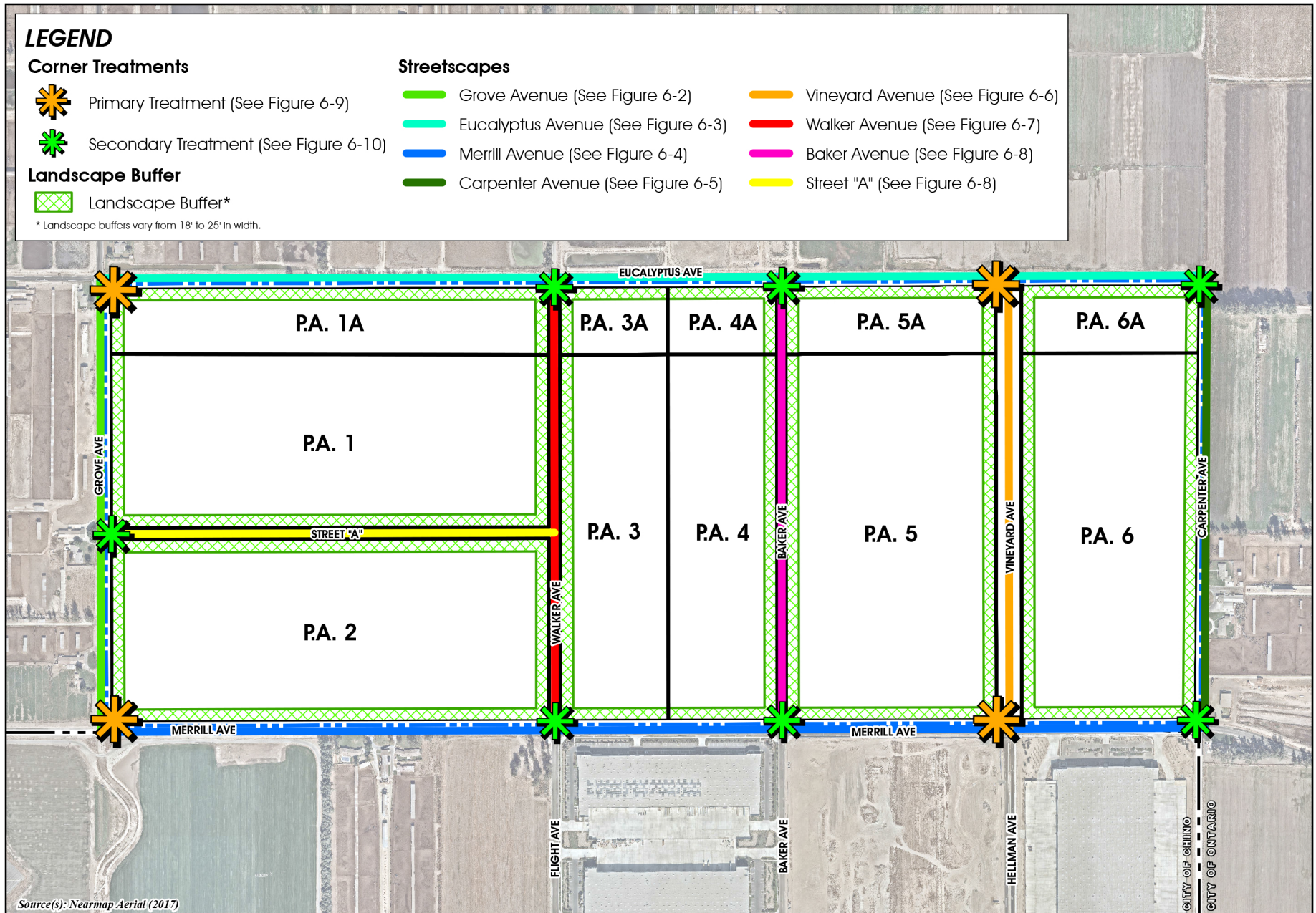
#### A. Grove Avenue Streetscape

The segment of Grove Avenue abutting the MERRILL COMMERCE CENTER features two landscaped components: 1) a 28-foot wide landscaped raised median and 2) parkways on both sides of the street. The landscaped raised median is planted with thematic accent trees at regular intervals – to reinforce the theme established at entries and monuments – and colorful groundcovers and succulents. Parkway include a curb-adjacent park strip planted with deciduous and/or evergreen trees, and low flowering groundcovers and succulents, as well as a 5-foot wide sidewalk. Evergreen trees are planted outside of the right-of-way on both sides of the street to provide pedestrians using the sidewalk with additional opportunities for shade and to screen undesirable views. An 8-foot wide multi-purpose trail is also provided along the eastern side of Grove Avenue to provide passive recreational opportunities. The typical Grove

Avenue Streetscape is illustrated on Figure 6-2, *Grove Avenue Streetscape*.

#### B. Eucalyptus Avenue Streetscape

The segment of Eucalyptus Avenue abutting the MERRILL COMMERCE CENTER features landscaping within parkways on both sides of the street. Parkway include a curb-adjacent park strip planted with deciduous and/or evergreen trees, and low flowering groundcovers and succulents, as well as a 5-foot wide sidewalk. Evergreen trees are planted outside of the right-of-way on both sides of the street to provide pedestrians using the sidewalk with additional opportunities for shade and to screen undesirable views. Because Eucalyptus Avenue is designated by the Chino Airport Compatibility Plan to satisfy open land requirements, street trees are spaced to maintain a clear width of about 75 feet. An 8-foot wide multi-purpose trail is also provided along the northern side of Eucalyptus Avenue to offer passive recreational opportunities. The typical Eucalyptus Avenue Streetscape is illustrated on Figure 6-3, *Eucalyptus Avenue Streetscape*.



Conceptual Landscape and Greenspace Plan

Figure 6-1

### C. Merrill Avenue Streetscape

The segment of Merrill Avenue abutting the MERRILL COMMERCE CENTER features landscaping within parkways on both sides of the street. Parkway include a curb-adjacent park strip planted with deciduous and/or evergreen trees, and low flowering groundcovers and succulents, as well as a 5-foot wide sidewalk. Because Merrill Avenue is designated by the Chino Airport Compatibility Plan to satisfy open land requirements, street trees are spaced to maintain a clear width of about 75 feet, except within Airport Safety Zone 1 (southwest portion of Planning Area 1), where no trees are permitted. Along other segments of Merrill Avenue abutting the Specific Plan boundary, additional evergreen and/or deciduous trees are planted outside of the right-of-way on both sides of the street to provide pedestrians using the sidewalk with additional opportunities for shade and to screen undesirable views. An 8-foot wide multi-purpose trail is also provided along the northern side of Merrill Avenue outside of the right-of-way to offer pedestrians passive recreational opportunities. The typical Merrill Avenue Streetscape is illustrated on Figure 6-4, *Merrill Avenue Streetscape*.

### D. Carpenter Avenue Streetscape

The segment of Carpenter Avenue abutting the MERRILL COMMERCE CENTER features landscaping within parkways on the western side of the street. Parkway include a curb-adjacent park strip planted with deciduous and/or evergreen trees, and low flowering groundcovers and succulents, as well as a 5-foot wide sidewalk. Evergreen and/or deciduous trees are planted outside of the right-of-way on both sides of the street to provide pedestrians using the sidewalk with additional opportunities for shade and to screen undesirable views. The typical Carpenter Avenue Streetscape is illustrated on Figure 6-5, *Carpenter Avenue Streetscape*.

### E. Vineyard Avenue Streetscape

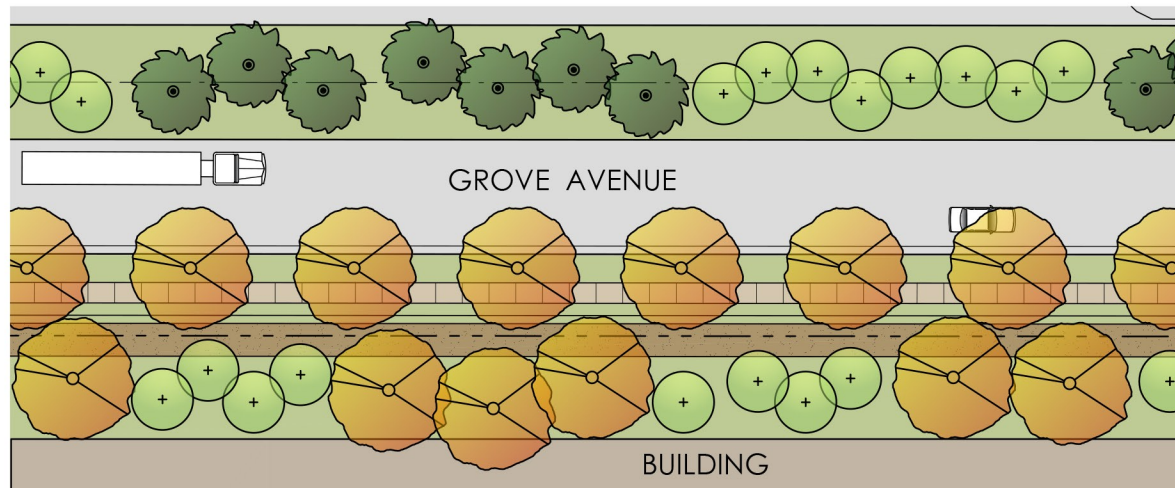
The segment of Vineyard Avenue abutting the MERRILL COMMERCE CENTER features two landscaped components: 1) a 28-foot wide landscaped raised median and 2) parkways on both sides of the street. The landscaped raised median is planted with thematic accent trees at regular intervals – to reinforce the theme established at entries and monuments – and colorful groundcovers and succulents. Parkway include a curb-adjacent park strip planted with deciduous and/or evergreen trees, and low flowering groundcovers and succulents, as well as a 5-foot wide sidewalk. An 8-foot wide Multi-Purpose Trail is also provided along the western side of Vineyard Avenue to provide passive recreational opportunities for pedestrians. Evergreen and/or deciduous trees are planted outside of the right-of-way on both sides of the street to provide pedestrians using the sidewalk and Multi-Purpose Trail with additional opportunities for shade and to screen undesirable views. The typical Vineyard Avenue Streetscape is illustrated on Figure 6-6, *Vineyard Avenue Streetscape*.

### F. Walker Avenue Streetscape


The typical section of Walker Avenue features landscaping within parkways on both sides of the street. Parkway include a curb-adjacent park strip planted with deciduous and/or evergreen trees, and low flowering groundcovers and succulents, as well as a 5-foot wide sidewalk. Evergreen and/or deciduous trees are planted outside of the right-of-way on both sides of the street to provide pedestrians using the sidewalk with additional opportunities for shade and to screen undesirable views. An 8-foot wide Multi-Purpose Trail is also provided along the eastern side of Walker Avenue outside of the right-of-way to offer pedestrians passive recreational opportunities. The typical Walker Avenue Streetscape is illustrated on Figure 6-7, *Walker Avenue Streetscape*.


### ***G. Street 'A' and Baker Avenue Streetscapes***


The typical sections of Street 'A' and Baker Avenue features landscaping within parkways on both sides of the street. Parkway include a curb-adjacent park strip planted with deciduous and/or evergreen trees, and low flowering groundcovers and succulents, as well as a 5-foot wide sidewalk. Evergreen and/or deciduous trees are planted outside of the right-of-way on both sides of the street to provide pedestrians using the sidewalk with additional opportunities for shade and to screen undesirable views. The typical Street 'A' and Baker Avenue Streetscape is illustrated on Figure 6-8, *Baker Avenue and Street "A" Streetscape*.



GROVE AVENUE STREETSCAPE

- 

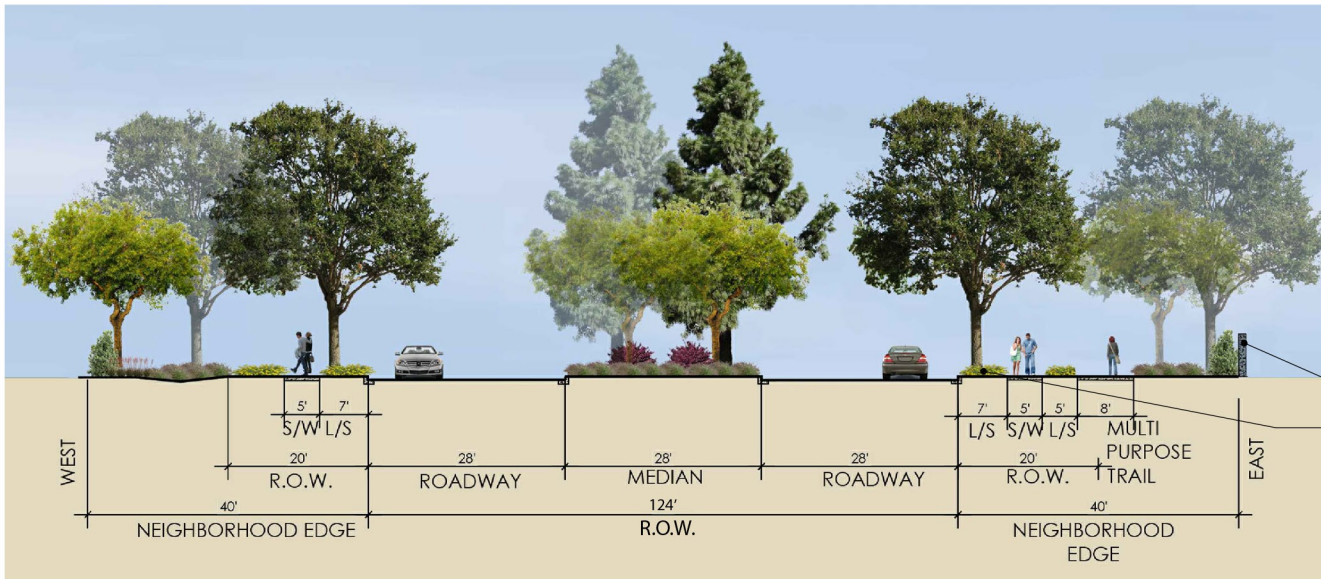
PLATANUS A. 'BLOODGOOD' (LONDON PLANE TREE)  
 HEIGHT X SPREAD: 40' X 30'  
 SPACING: 40' O.C.  
 DESCRIPTION: DECIDUOUS
- 

CERCIS C. 'FOREST PANSY' (EASTERN REDBUD)  
 HEIGHT X SPREAD: 20' X 15'  
 SPACING: 15' O.C.  
 DESCRIPTION: DECIDUOUS
- 

PINUS ELДАРICA (AFGHAN PINE)  
 HEIGHT X SPREAD: 40' X 30'  
 SPACING: 30' O.C.  
 DESCRIPTION: EVERGREEN

7' WIDE PARKWAY  
 5' WIDE SIDEWALK  
 5' WIDE LANDSCAPE SETBACK  
 8' WIDE MULTI-PURPOSE TRAIL (EAST SIDE ONLY)  
 LANDSCAPE SETBACK

Plan View



Section

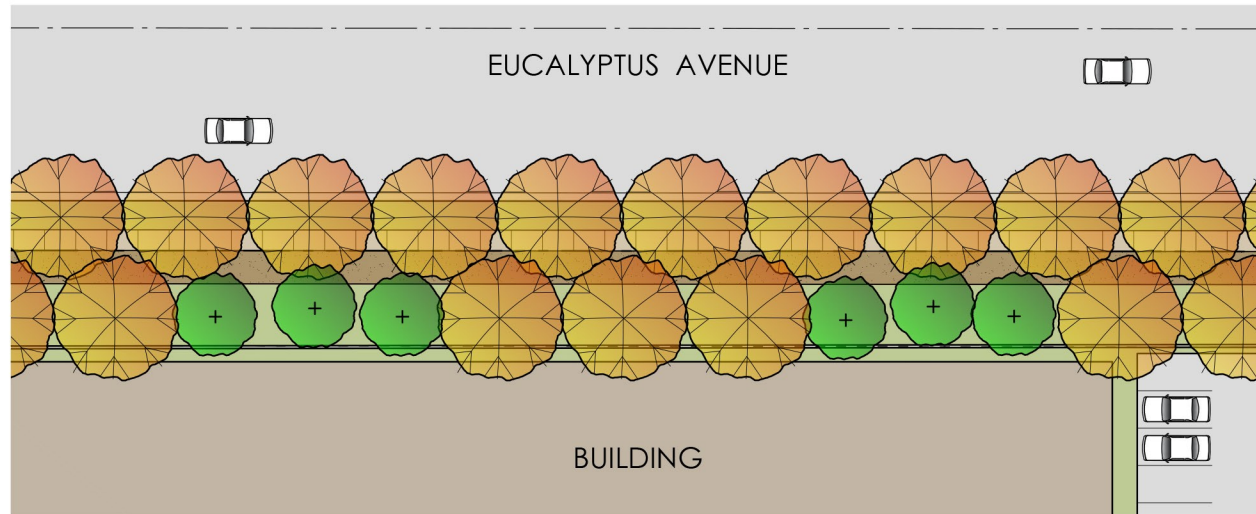
PROJECT PERIMETER WALL  
 PARKWAY PLANTING SHALL NOT EXCEED 18' IN HEIGHT, TYP.

Source(s): RLA (06-12-2019)

Grove Avenue Streetscape

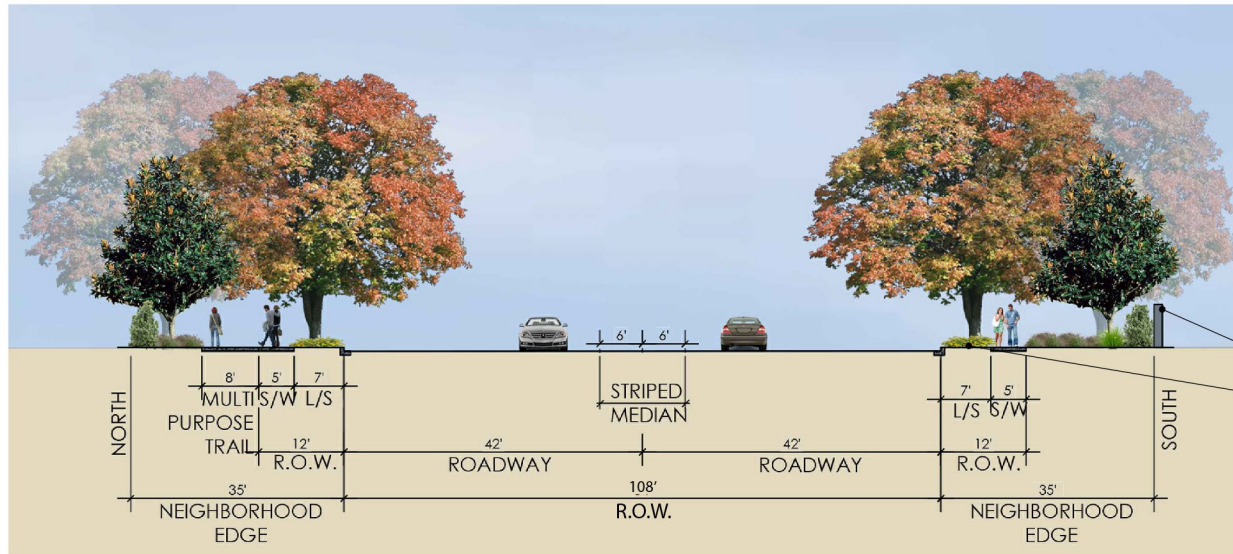
Figure 6-2



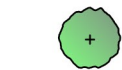


7' WIDE PARKWAY  
 5' WIDE SIDEWALK  
 8' WIDE MULTI-PURPOSE TRAIL  
 LANDSCAPE SETBACK

Plan View

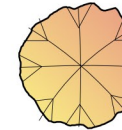


EUCALYPTUS AVENUE STREETSCAPE



PODOCARPUS  
 GRACILIOR  
 (FERN PINE)

HEIGHT X SPREAD:  
 40' X 30'  
 SPACING:  
 30' O.C.  
 DESCRIPTION:  
 EVERGREEN



PISTACIA CHINENSIS  
 (CHINESE PISTACHE)

HEIGHT X SPREAD:  
 40' X 40'  
 SPACING:  
 30' O.C.  
 DESCRIPTION:  
 DECIDUOUS

PROJECT PERIMETER WALL

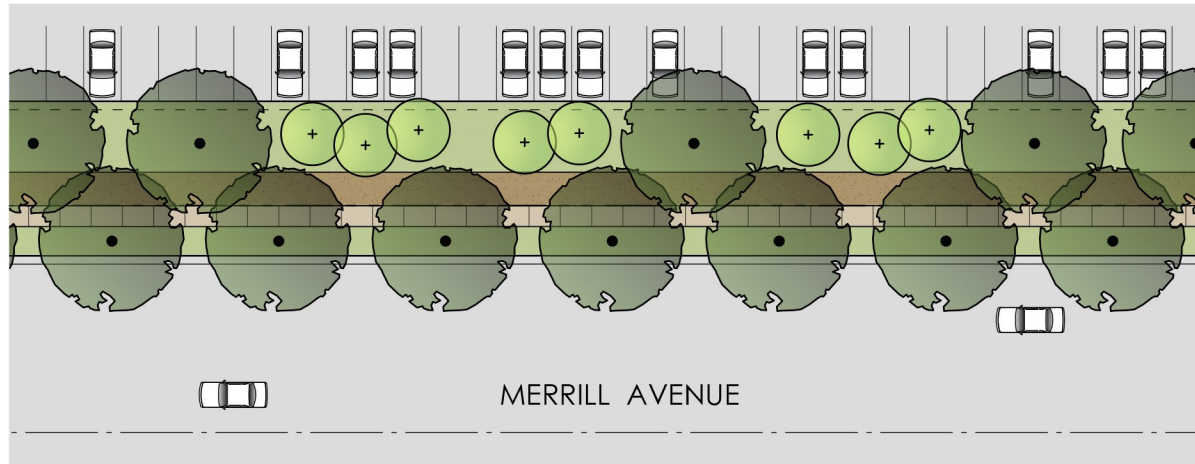
PARKWAY PLANTING SHALL  
 NOT EXCEED 18" IN HEIGHT, TYP.

Section

Source(s): RLA (06-12-2019)

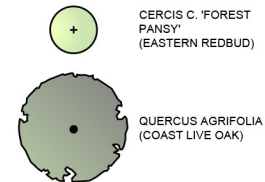
Eucalyptus Avenue Streetscape

Figure 6-3



LANDSCAPE SETBACK  
 8' WIDE MULTI-PURPOSE TRAIL  
 5' WIDE SIDEWALK  
 7' WIDE PARKWAY

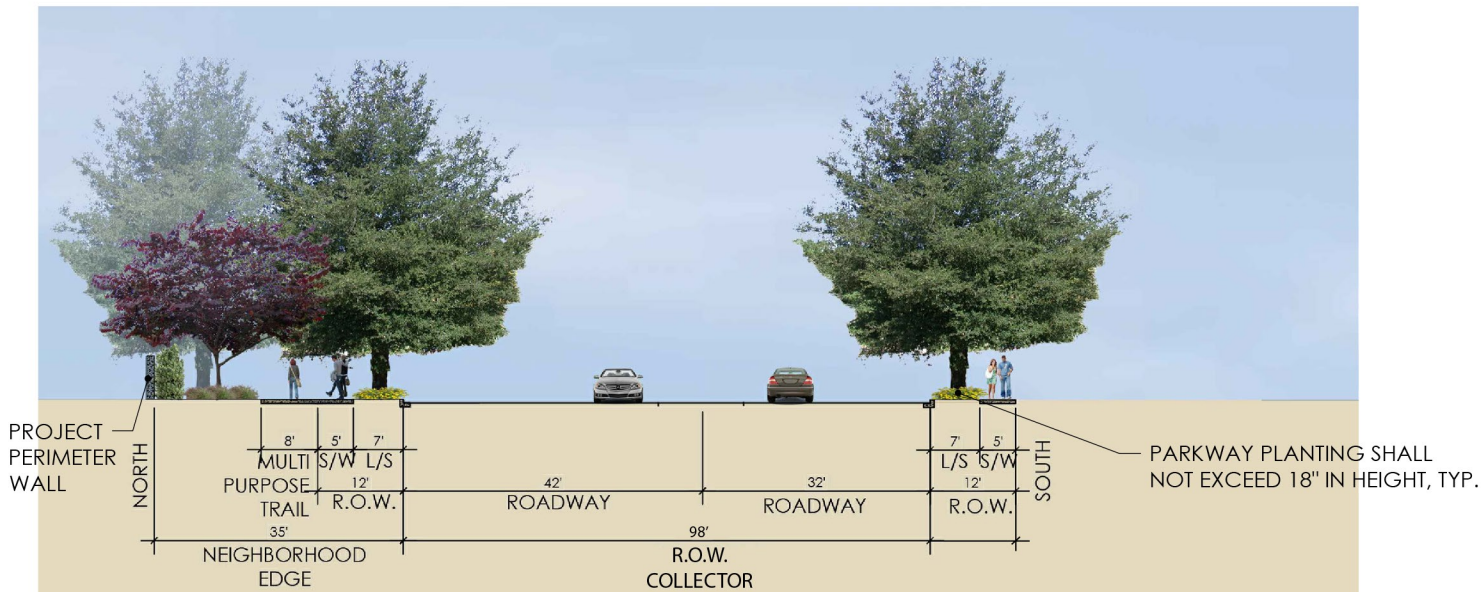
MERRILL AVENUE STREETSCAPE



HEIGHT X SPREAD:  
 20' X 15'  
 SPACING:  
 15' O.C.  
 DESCRIPTION:  
 DECIDUOUS

HEIGHT X SPREAD:  
 35' X 35'  
 SPACING:  
 40' O.C.  
 DESCRIPTION:  
 EVERGREEN

Plan View

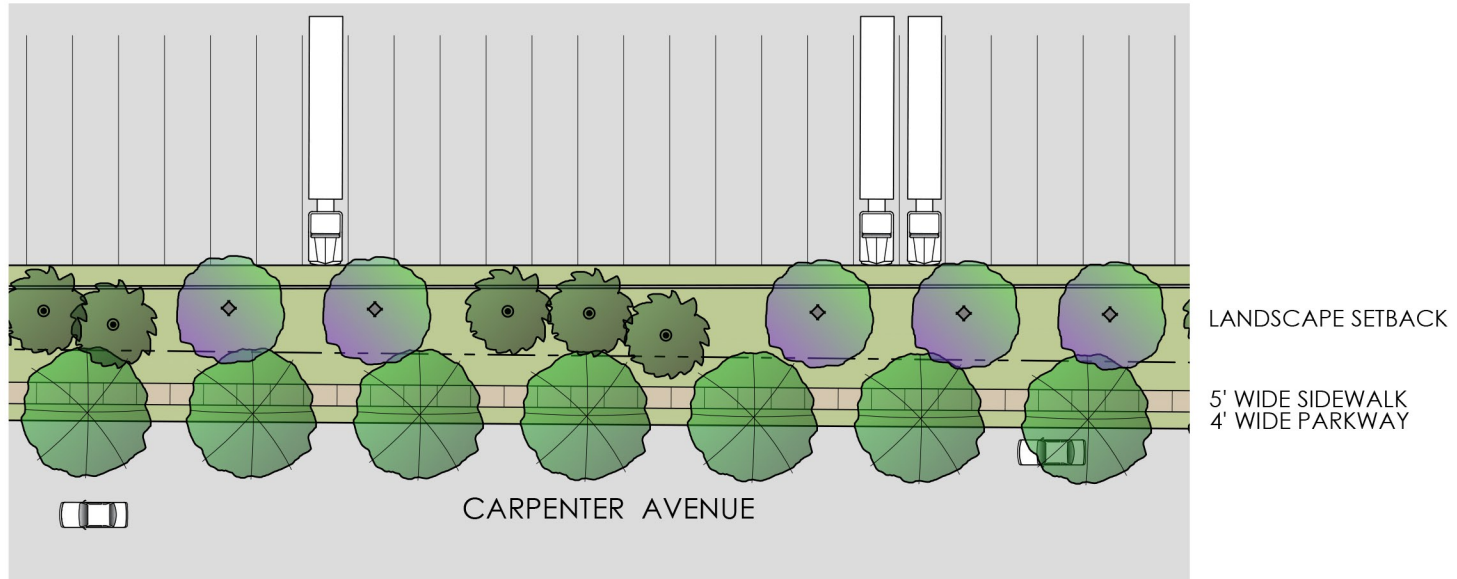


Section

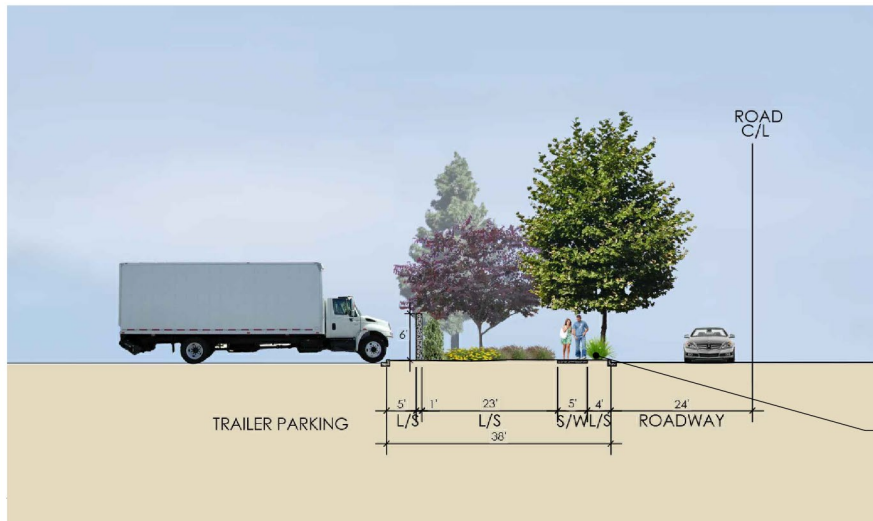
Source(s): RLA (06-12-2019)

Merrill Avenue Streetscape

Figure 6-4






Plan View



Section

CARPENTER AVENUE STREETSCAPE

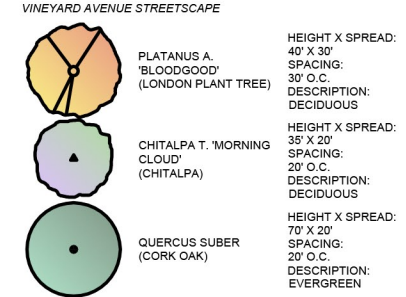
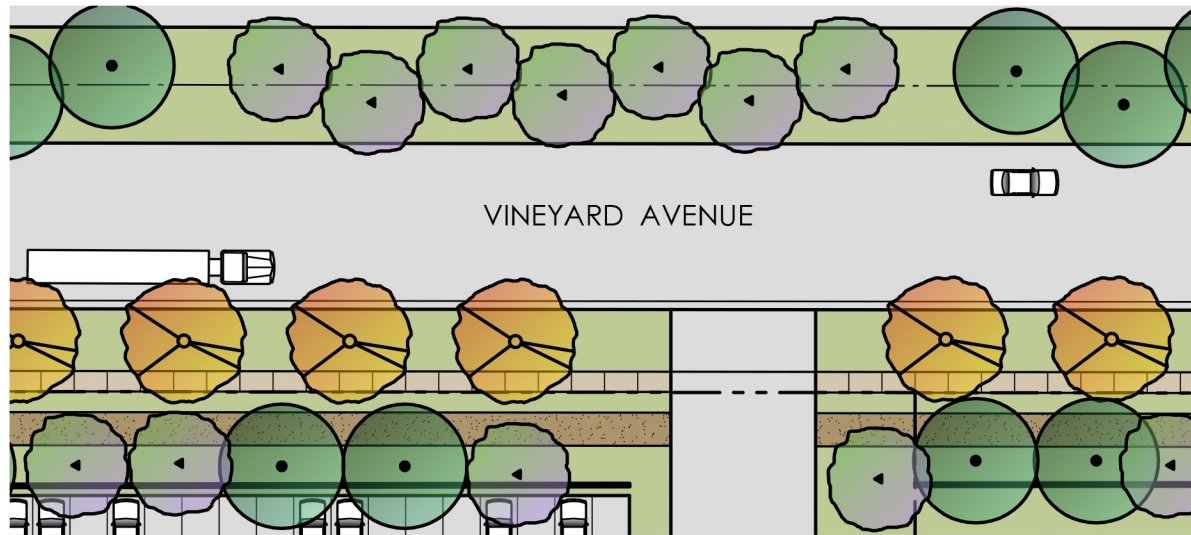
-  LIRIODENDRON TULIPIFERA (TULIP TREE) HEIGHT X SPREAD: 60' X 30' SPACING: 40' O.C. DESCRIPTION: DECIDUOUS
-  PINUS ELДАРICA (AFGHAN PINE) -OR- PODOCARPUS GRACILIOR (FERN PINE) HEIGHT X SPREAD: 40' X 30' SPACING: 30' O.C. DESCRIPTION: EVERGREEN
-  PRUNUS CERASIFERA (FLOWERING PLUM) HEIGHT X SPREAD: 25' X 25' SPACING: 35' O.C. DESCRIPTION: DECIDUOUS

PARKWAY PLANTING SHALL NOT EXCEED 18" IN HEIGHT, TYP.

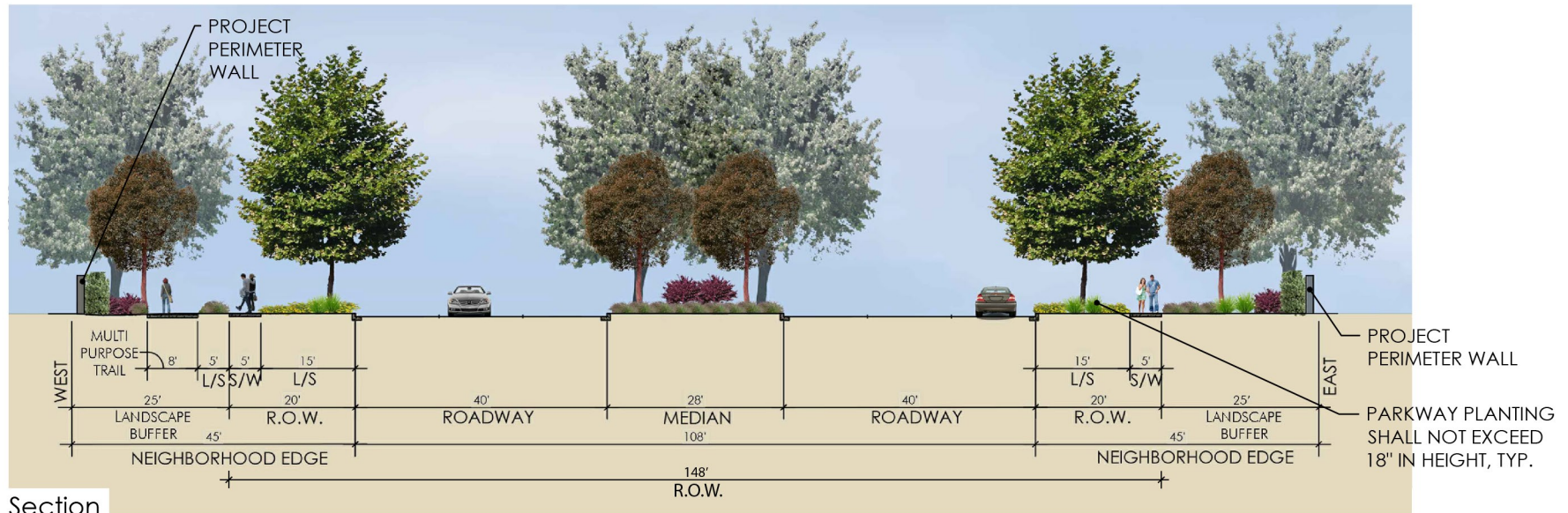
Source(s): RLA (06-12-2019)

Carpenter Avenue Streetscape

Figure 6-5



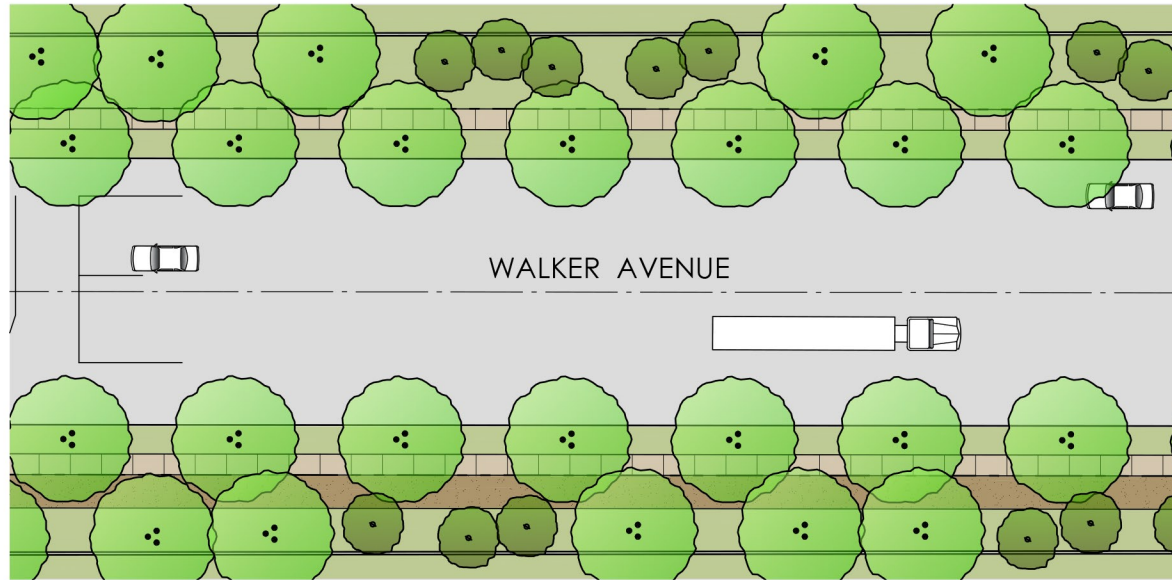
15' WIDE PARKWAY  
 5' WIDE SIDEWALK  
 5' WIDE LANDSCAPE AREA  
 8' WIDE MULTI-PURPOSE TRAIL  
 LANDSCAPE SETBACK



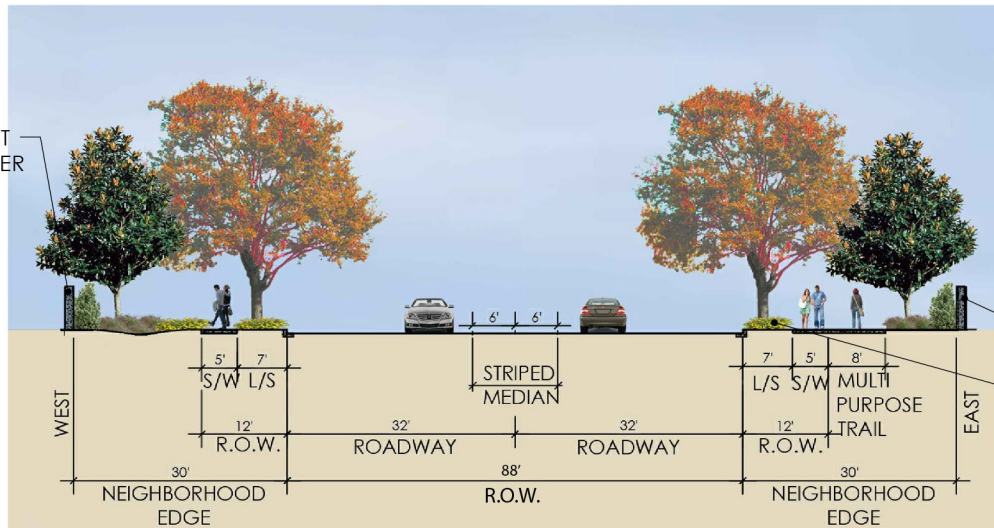
Source(s): RLA (12-18-2019)

Vineyard Avenue Streetscape

Figure 6-6



Plan View



Section

WALKER AVENUE STREETSCAPE



- KOELREUTERIA BIPPINATA (CHINESE FLAME TREE)  
HEIGHT X SPREAD: 30' X 30'  
SPACING: 40' O.C.  
DESCRIPTION: DECIDUOUS
- TRISTANIA LAURINA (WATER GUM)  
HEIGHT X SPREAD: 15' X 15'  
SPACING: 20' O.C.  
DESCRIPTION: EVERGREEN

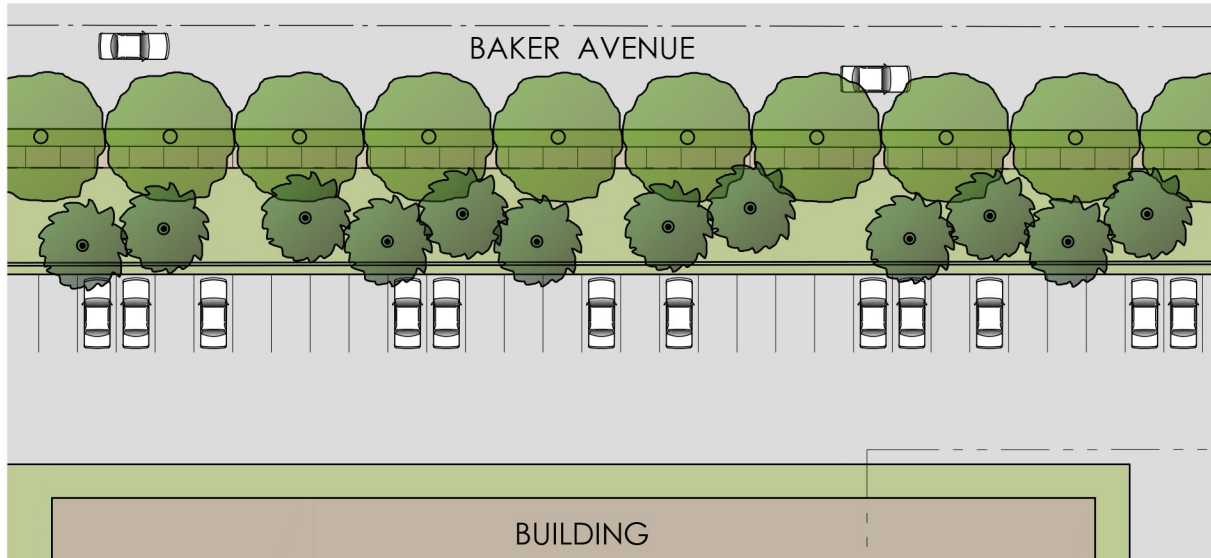
PROJECT PERIMETER WALL

PARKWAY PLANTING SHALL NOT EXCEED 18" IN HEIGHT, TYP.

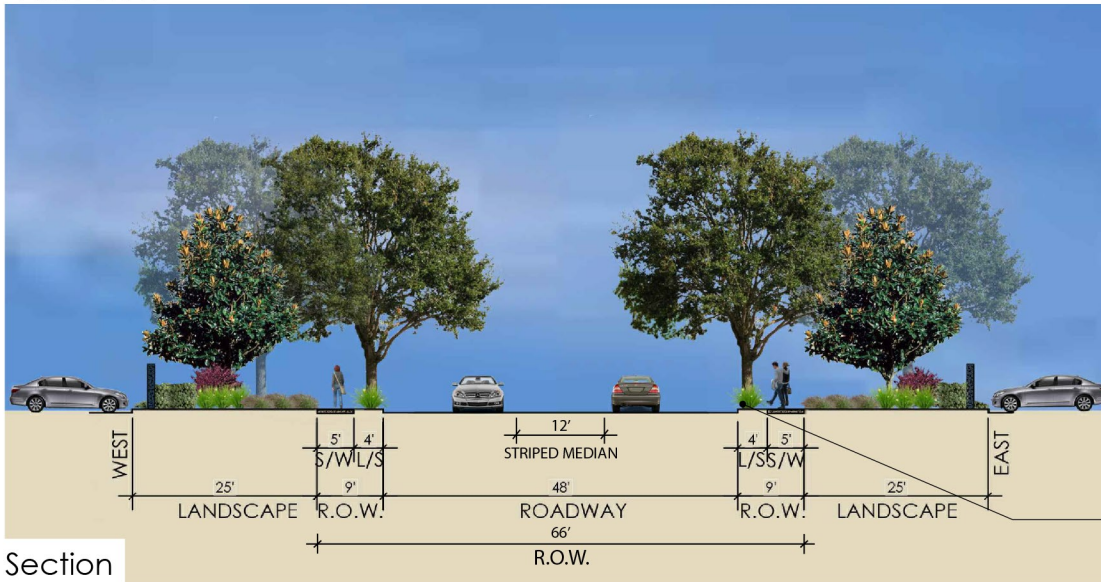
Source(s): RLA (06-12-2019)

Walker Avenue Streetscape

Figure 6-7



Plan View



STREET 'A' / BAKER AVENUE STREETSCAPE



QUERCUS ILEX  
(HOLLY OAK)

PINUS ELДАРICA  
(AFGHAN PINE)  
-OR-  
PODOCARPUS  
GRACILIOR  
(FERN PINE)

HEIGHT X SPREAD:  
40' X 30'  
SPACING:  
40' O.C.  
DESCRIPTION:  
EVERGREEN

HEIGHT X SPREAD:  
40' X 30'  
SPACING:  
30' O.C.  
DESCRIPTION:  
EVERGREEN

PARKWAY PLANTING SHALL NOT EXCEED 18" IN HEIGHT, TYP.

Source(s): RLA (12-18-2019)

Baker Avenue and Street "A" Streetscape

Figure 6-8

#### 6.11.4 Entries and Monuments

The MERRILL COMMERCE CENTER provides a three-tiered hierarchy of entry and corner treatments to identify the CENTER and distinguish individual planning areas. The entry and corner treatments are designed to provide distinctive visual statements and emphasize the Specific Plan's contemporary aesthetic. All hardscape and landscape features at entry and monument locations shall provide adequate "line-of-sight" for motorists and shall comply with applicable City design standards and specifications. Monumentation shall not be located within the public street right-of-way.

The entry and corner concepts described and illustrated on the following pages have been designed to provide a prominent reminder of the quality and distinctiveness of the MERRILL COMMERCE CENTER and to complement and reinforce the CENTER's general architectural and landscape theme. Implemented entry and corner treatments should be flexible to respond to physical contexts and the needs and desires of specific tenants and may differ slightly from the concepts presented herein; however, all entry and corner treatments within the MERRILL COMMERCE CENTER shall be consistent in theme and character.

##### A. Primary Corner Treatments

Primary Corner Monuments may be located at the intersections of Eucalyptus Avenue and Grove Avenue (southeast corner), Eucalyptus Avenue and Vineyard Avenue (southwest corner), and Merrill Avenue and Grove Avenue (northeast corner), and Merrill Avenue and Vineyard Avenue (northwest corner) to announce arrival to the MERRILL COMMERCE CENTER. These Primary Corner Monuments are the largest of the entry/monument family. The conceptual design for these monuments includes a gabion style frame monument with

filled native rock, freestanding steel letters, monument lighting, and a laser-cut steel panel with a masonry wall. Landscaping at the Primary Corner Treatments include low foreground plants with accent trees in orchard rows behind the monument. Figure 6-9, *Conceptual Primary Corner Treatment*, conceptually illustrates these monuments. Note that a modified version of the Primary Corner Treatment is required at the northeast corner of the Merrill Avenue and Grove Avenue intersection, which is located in Chino Airport Safety Zone 1, in which tall vertical objects including trees greater than 4 inches in diameter and taller than 4 feet above the ground are not permitted.

##### B. Secondary Corner Treatments

Secondary Corner Treatments may be located at the entrances into the MERRILL COMMERCE CENTER from Grove Avenue, Eucalyptus Avenue, Carpenter Avenue, and Merrill Avenue to subtly promote the CENTER's design theme at key focal points. These Secondary Corner Treatments feature accent trees in orchard rows, shrub planting to echo rows, and background shrubs on both corners of the street. Figure 6-10, *Conceptual Secondary Corner Treatment*, conceptually illustrates these monuments.

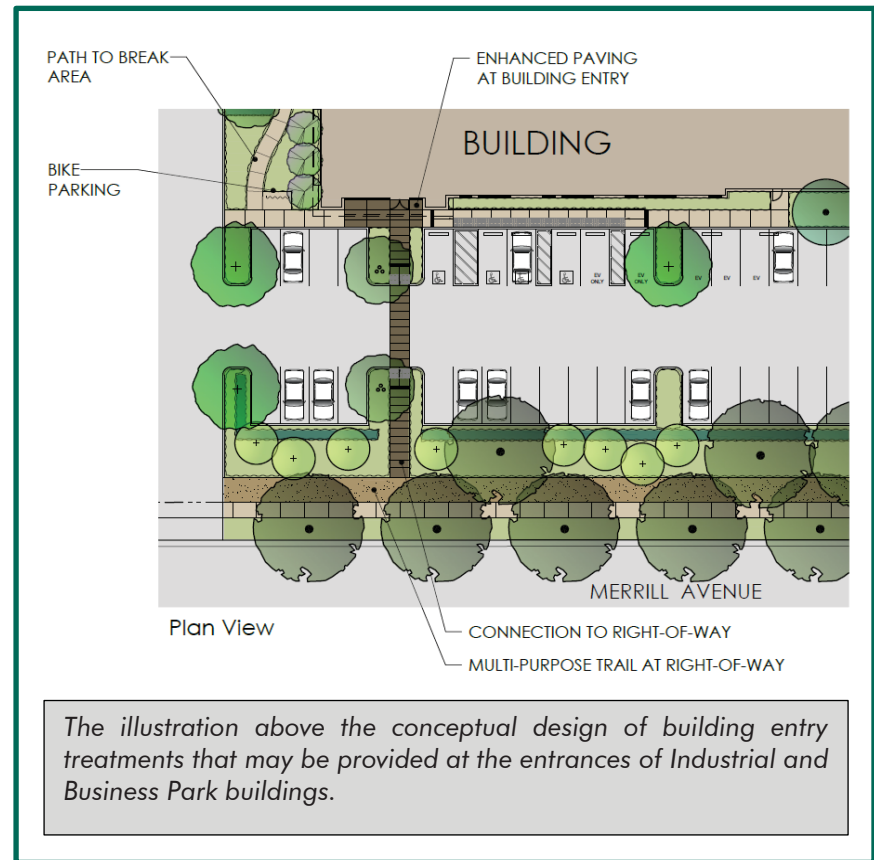
##### C. Building User Monument Treatments

Tenant Monument Treatments may be provided at the corners of driveways connecting to public roads. The locations of such driveways will be determined at the time buildings are designed and oriented in the Specific Plan area as part of implementing development projects. Tenant Monument Treatments are meant to identify building occupants and welcome employees and visitors to the site. The designs of typical Tenant Monument Treatments are conceptually shown below, and may include a tenant monument,

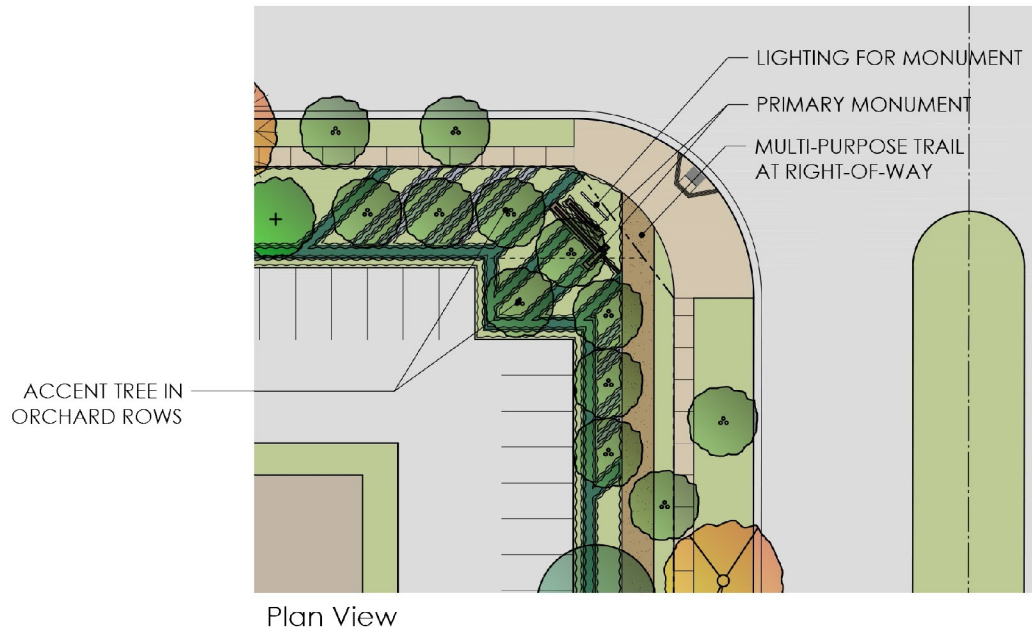
monument lighting, a masonry wall with pin mounted letters, and a gabion style frame filled with native rock. Landscaping at these Treatments may include accent trees, decorative row planting, and background shrubs. Figure 6-11, *Conceptual Building User Monument Treatment*, conceptually illustrates these monuments.

**D. Building Entry Treatments**

Building Entry Treatments may be provided at the entrances of Industrial or Business Park buildings. Building Entry Treatments are meant to welcome employees and visitors to Industrial or Business Park buildings. The designs of typical Building Entry Treatments are conceptually shown below, and may include tenant signage on the building façade, an enhanced paving at building entry, connection to the public road, and Multi-Purpose Trail, bike parking, and pathways to break areas.



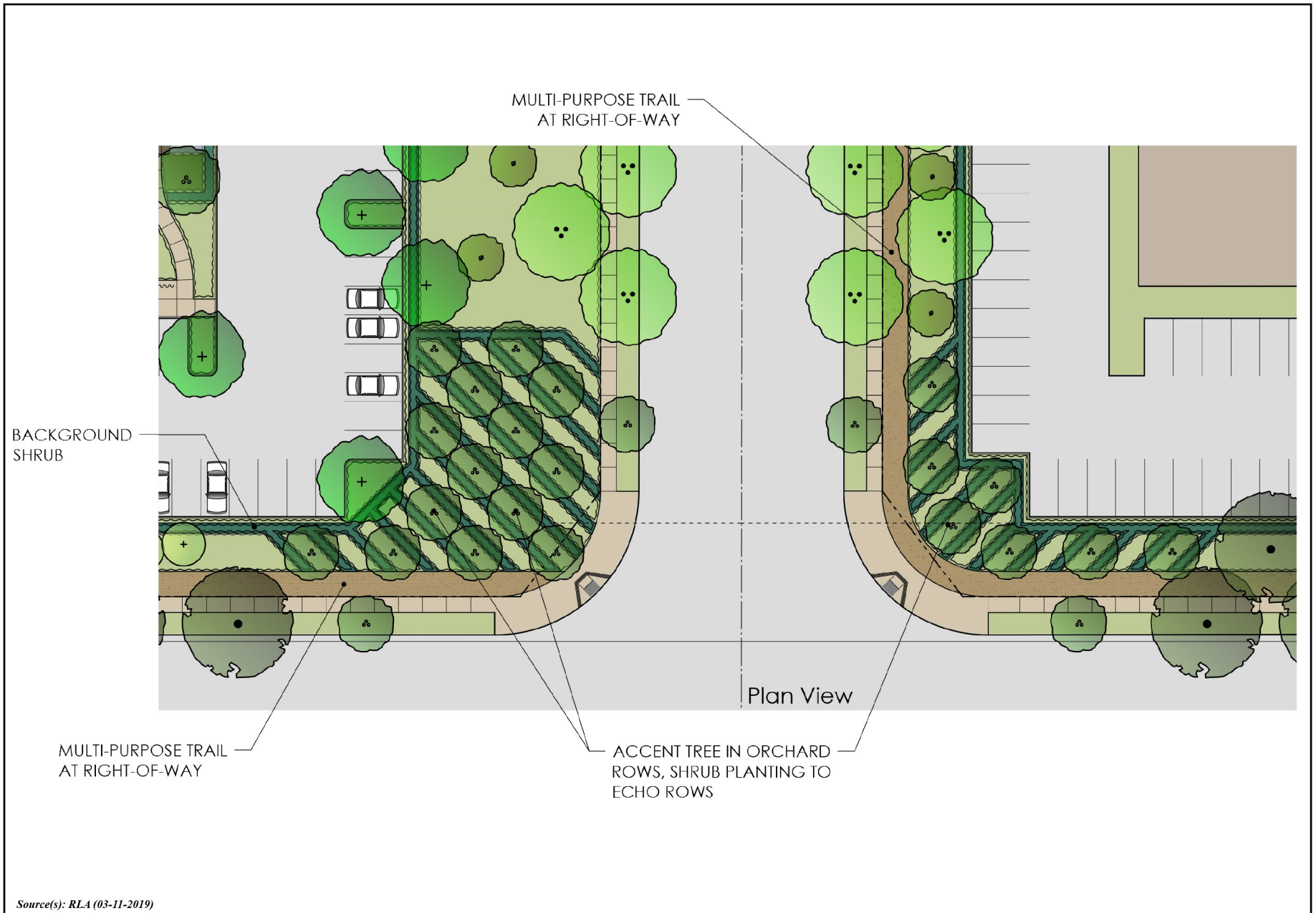




Source(s): RLA (03-11-2019)

Conceptual Primary Corner Treatment

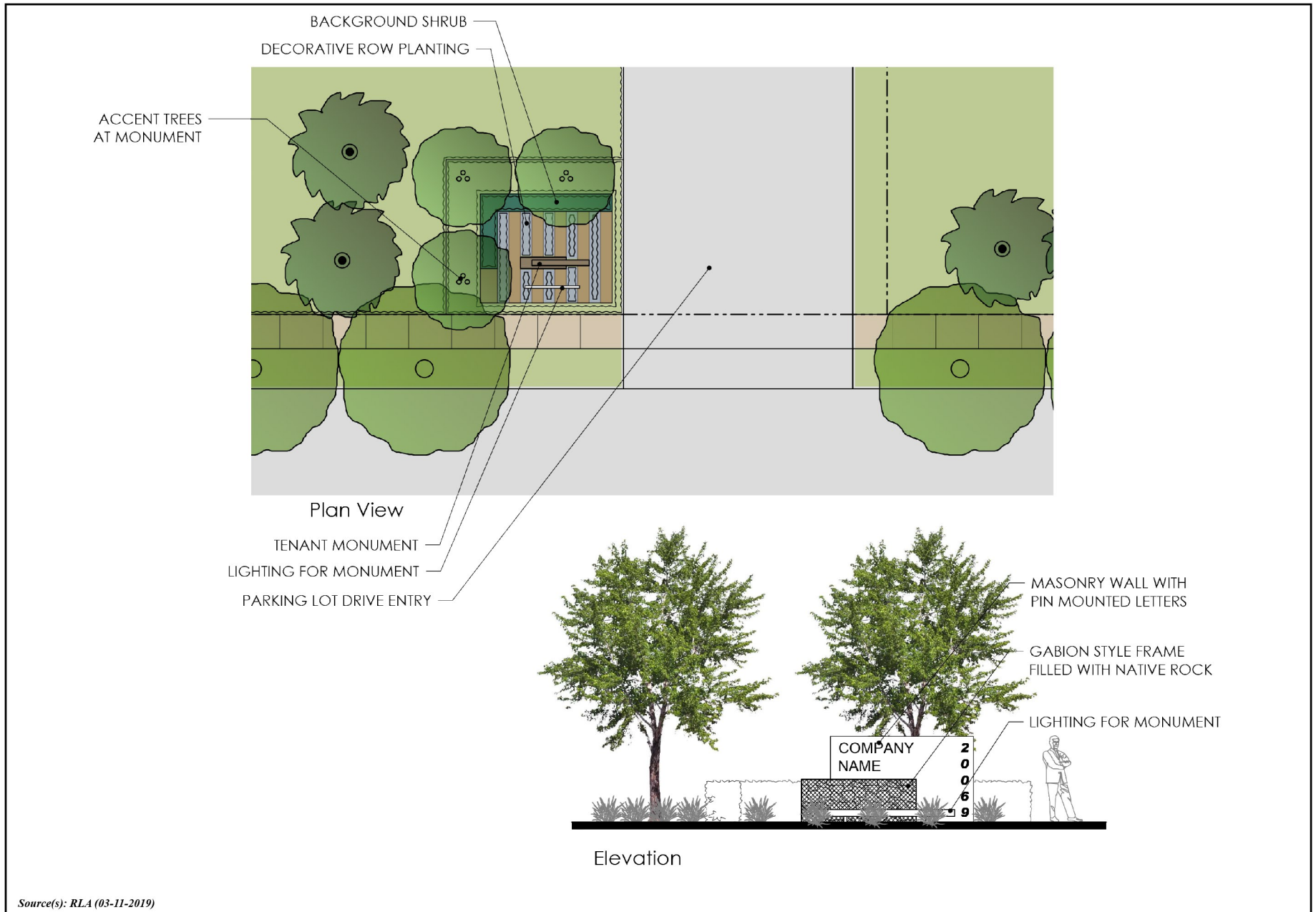
Figure 6-9



Source(s): RLA (03-11-2019)

Conceptual Secondary Corner Treatment

Figure 6-10



Conceptual Building User Monument Treatment

Figure 6-11

### 6.11.5 Walls and Fences

Along building site perimeters and interior to building sites, the installation of fences and walls will be necessary. The final locations and details of these fences and walls will be determined when buildings are designed and oriented during the implementation of the MERRILL COMMERCE CENTER.

As shown on Figure 6-12, *Conceptual Screening Wall Illustration*, a 8 to 14- foot high screen wall may be provided around the perimeters of individual buildings sites and around loading and dock areas, trailer parking areas, and parking lots to screen on-site industrial uses from public views and public roads. In addition, landscaping within rights-of-way and outside of rights-of-way serve as additional screening between on-site industrial uses and public roads.

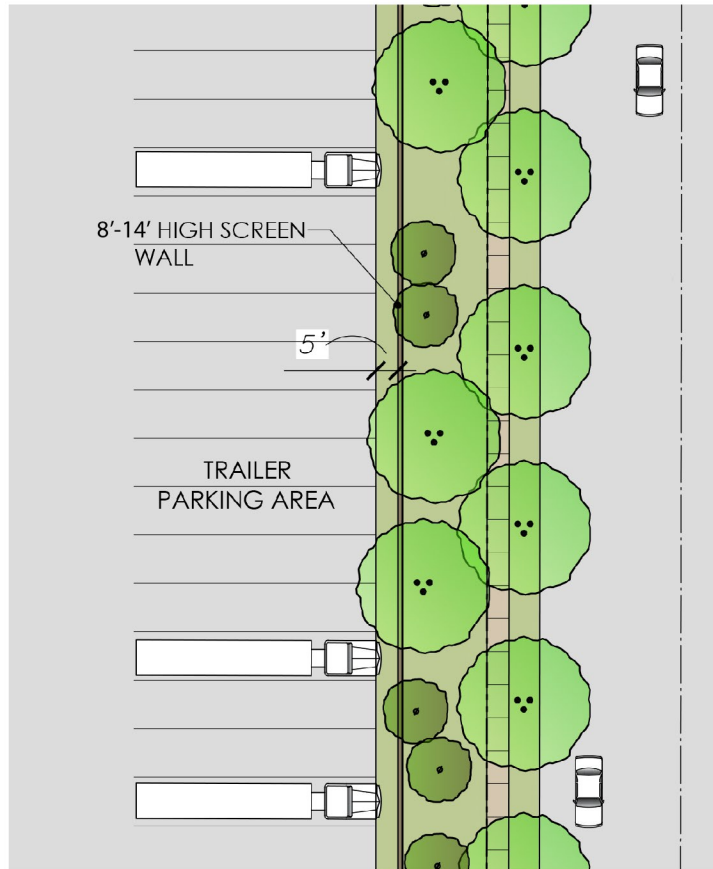
### 6.11.6 Open Space Areas

As shown on Figure 6-13, *Conceptual Open Space Area Illustration*, shared open space areas may be provided within the MERRILL COMMERCE CENTER to offer employees and visitors a recreational amenity in proximity to the Industrial/Business Park uses. The shared amenities that may be provided within open space areas include seating areas, meandering decomposed granite walkways, overhead structures above decomposed granite dining areas, open turf areas, and a natural planting scheme with boulders. The final locations and details of these open space areas will be determined when buildings are designed and oriented during the implementation of the MERRILL COMMERCE CENTER.

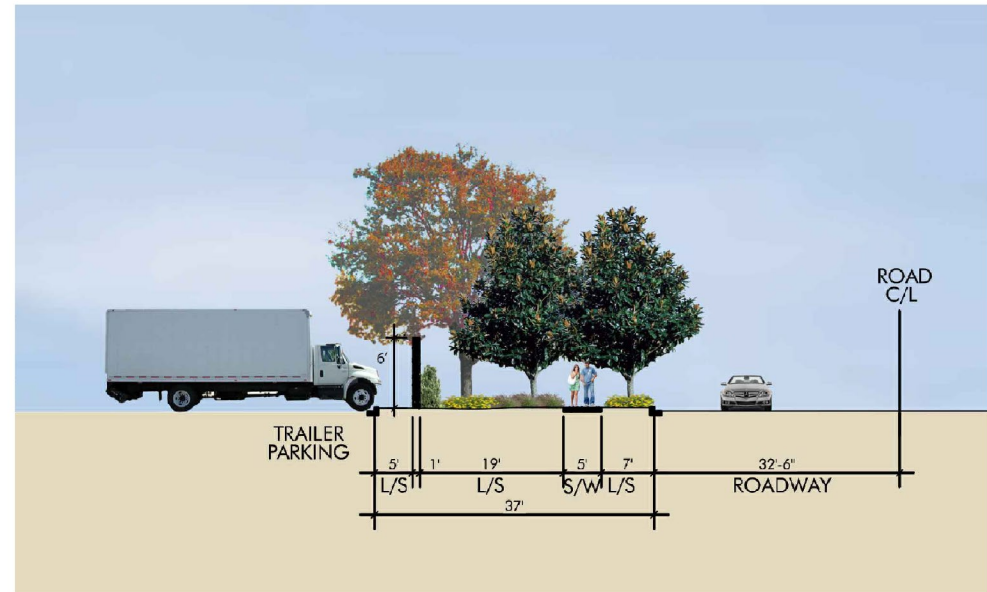
### 6.11.7 Water Quality Basins

As shown on Figure 6-14, *Conceptual Water Quality Basin Illustration*, water quality basins may be provided within the MERRILL COMMERCE CENTER to treat stormwater before the flows ultimately drain into the storm drain facilities, as described in Section 4.2.4, *Storm Water Management Plan*. The water quality basins may include contoured basin edges for a natural look, and landscaping around the perimeter of the basin to screen public views of the basin. Swales may be no greater than 40% of the landscape area width to allow for ornamental landscaping, although landscaping may have limitations where necessary to deter the attraction of birds to the basins, which can be a hazard to aircraft using the nearby Chino Airport and Ontario International Airport. Other wildlife deterrents that may be considered include floating covers, bird balls, netting, and basins designed to be linear and narrow with steep sides and rip-rap lining.

The final locations and design details of the water quality basins will be determined when buildings are designed and oriented during the implementation of the MERRILL COMMERCE CENTER.

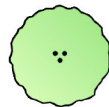


Plan View



Section

WALKER AVENUE STREETScape



KOELREUTERIA  
BIPINNATA  
(CHINESE FLAME TREE)

HEIGHT X SPREAD:  
30' X 30'  
SPACING:  
40' O.C.  
DESCRIPTION:  
DECIDUOUS



MAGNOLIA G. 'ST.  
MARY'  
(ST. MARY'S MAGNOLIA)

HEIGHT X SPREAD:  
15' X 15'  
SPACING:  
20' O.C.  
DESCRIPTION:  
EVERGREEN

Source(s): RLA (03-11-2019)

Conceptual Screening Wall Illustration

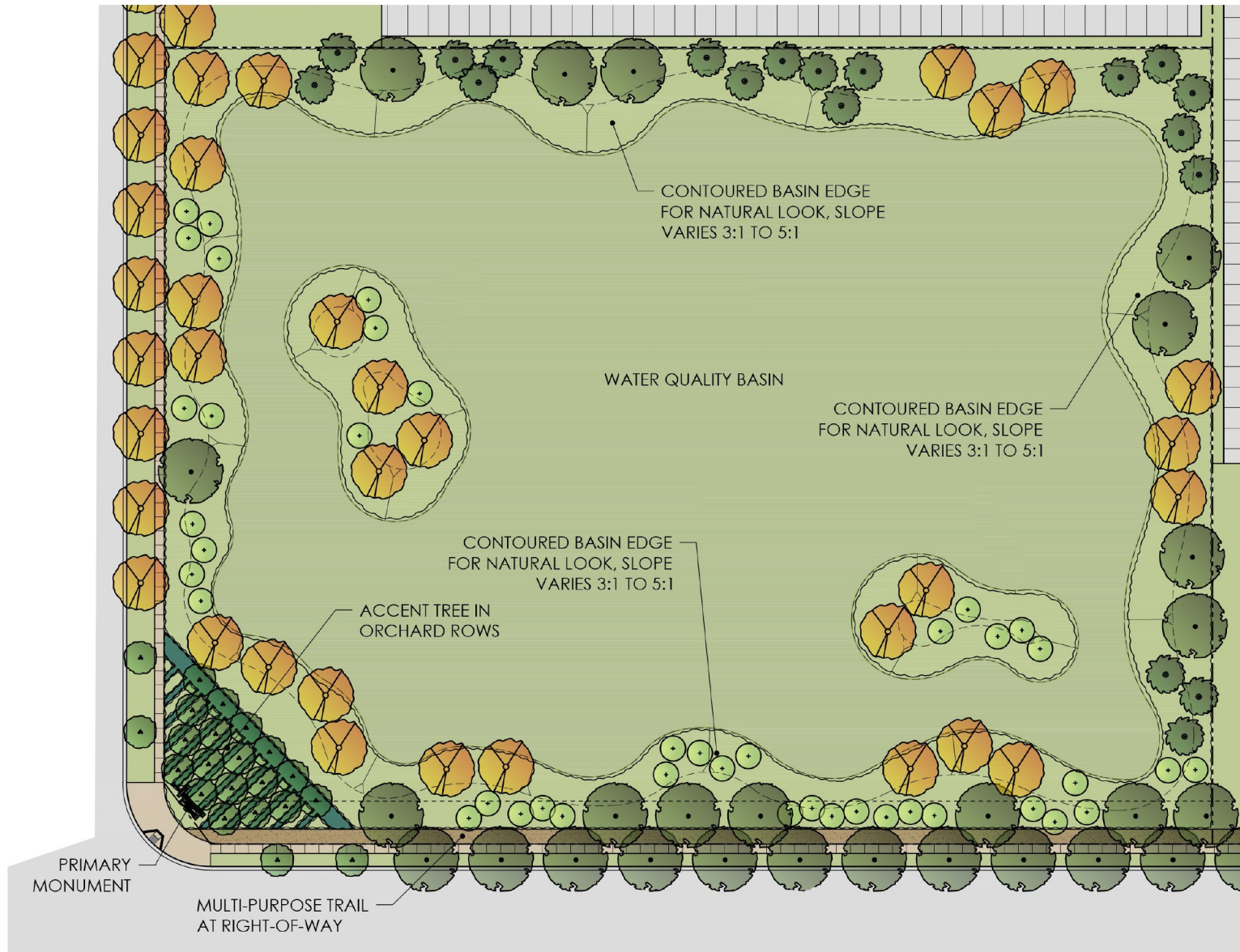
Figure 6-12



Source(s): RLA (03-11-2019)

Conceptual Open Space Area Illustration

Figure 6-13



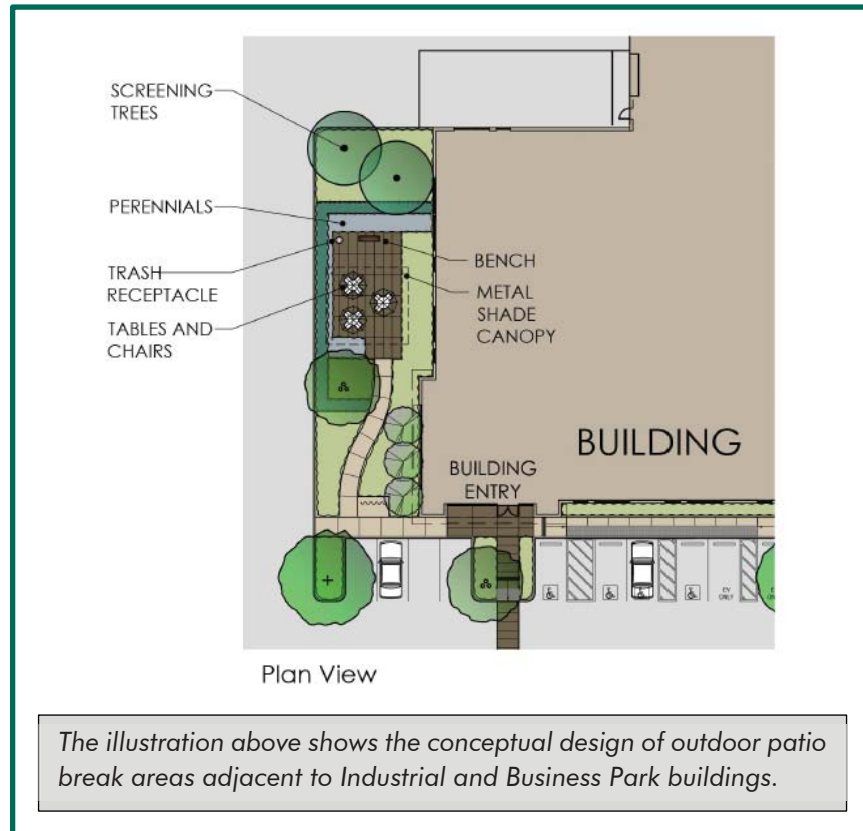
Source(s): RLA (03-11-2019)

Conceptual Water Quality Basin Illustration

Figure 6-14

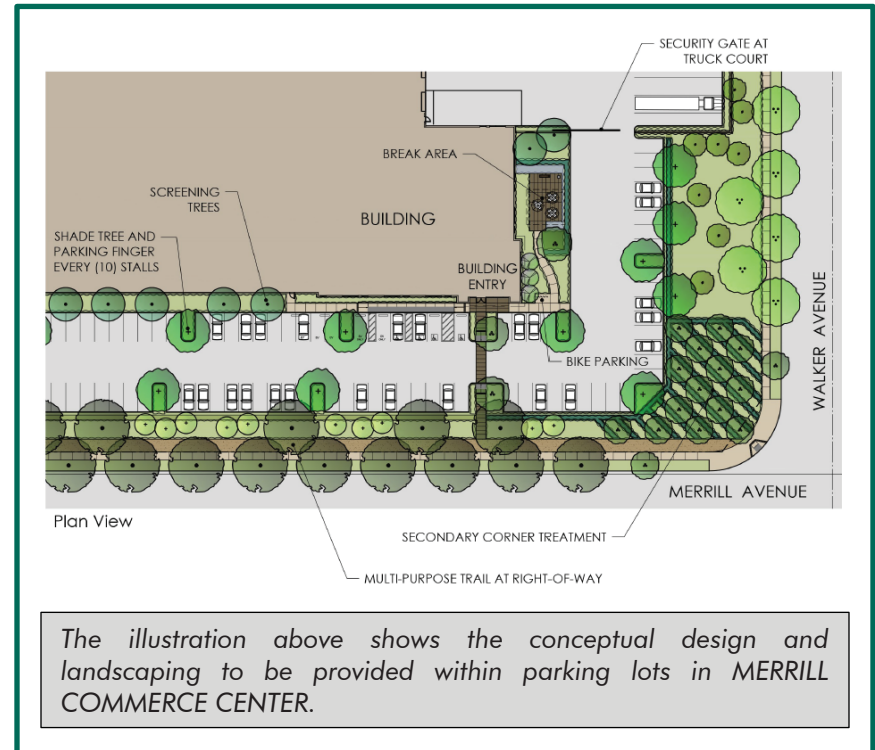
**6.11.8 Shared Outdoor Patio Break Areas**

As shown in the illustration below, shared outdoor patio break areas should be provided within Industrial and Business Park planning areas to offer employees with outdoor areas adjacent to their work buildings. Shared outdoor patio break areas may include amenities such as benches, metal shade canopies, tables, chairs, perennials/shrubs, and trash receptables. Trees may also be provided to shade the outdoor patio break areas.



**6.11.9 Parking Lots**

As shown in the illustration below, passenger car parking lots within Industrial and Business Park sites provide landscaping that consist of shade trees and parking finger islands for every ten (10) stalls, and landscaping outside/inside of the abutting public road’s right-of-way. Bicycle parking is also provided near the entrances of buildings. Trees and parking fingers are not required in truck courts, to minimize truck turning movement conflicts.







# IMPLEMENTATION PLAN

## *CHAPTER 7*

7.1 SEVERABILITY

7.2 APPLICABILITY

7.3 INTERPRETATION

7.4 DEVELOPMENT REVIEW PROCESS

7.5 SUBSTANTIAL CONFORMANCE

7.6 FORMAL AMENDMENTS TO THE SPECIFIC PLAN

7.7 APPEALS

7.8 COMPLIANCE WITH MITIGATION MONITORING AND REPORTING PROGRAM

7.9 PROJECT FINANCING

7.10 PHASING PLAN

7.11 ADDITIONAL ENTITLEMENT REQUIREMENTS

7.12 MAINTENANCE PLAN

## CHAPTER 7 - IMPLEMENTATION PLAN

### 7.1 SEVERABILITY

This Specific Plan document enables the City of Ontario to facilitate the processing and approval of development plans and implementing permits to build out the MERRILL COMMERCE CENTER. If any regulation, condition, program, or portion of this Specific Plan is held invalid or unenforceable, such portions shall be deemed separate, distinct, and independent provisions, and the invalidity of such portions or provisions shall not affect the validity and enforceability of the remaining provisions contained herein.

### 7.2 APPLICABILITY

Approval of the MERRILL COMMERCE CENTER Specific Plan indicates acceptance by the City of Ontario City Council of a general framework for the development of the MERRILL COMMERCE CENTER property. Part of that framework establishes specific development standards that constitute the zoning regulations for the Specific Plan (refer to Chapter 5, *Development Standards*). The provisions contained herein are intended to regulate development within the Specific Plan area.

Development within the MERRILL COMMERCE CENTER Specific Plan boundary shall be implemented through the City's approval of tentative and final parcel maps and the Development Plan Review process as established in the City's Development Code. The implementation process described herein provides the mechanisms for review and approval of development projects within MERRILL COMMERCE CENTER.

### 7.3 INTERPRETATION

Unless otherwise provided, any ambiguity concerning the content or application of the Specific Plan shall be resolved by the City's Planning Director, or his/her designee, in a manner consistent with the goals, policies, purpose, and intent established in this Specific Plan.

### 7.4 DEVELOPMENT REVIEW PROCESS

#### 7.4.1 Subdivision Maps

Approval of future tentative subdivision maps within the MERRILL COMMERCE CENTER Specific Plan may occur concurrently with or subsequently to the adoption of the Specific Plan. All tentative and final subdivision maps shall be reviewed and approved pursuant to the applicable provisions of the City's Subdivision Ordinance and consistent with the applicable provisions established within the Land Use, Infrastructure, Design Guidelines, and Development Regulations chapters of this Specific Plan.

#### 7.4.2 Development Plan Review

All development within the MERRILL COMMERCE CENTER property shall be subject to the Development Plan Review Process established in the City of Ontario Development Code. Adoption of this Specific Plan by the City includes the design guidelines contained in Chapter 6, which shall be the design criteria by which development projects with the Specific Plan shall be reviewed during Development Plan Review. Topics on which these design guidelines are silent, the applicable design guidelines contained within the City's Development Code shall apply. The design guidelines are intended to be flexible in nature while establishing rudimentary evaluation

criteria for the review by the City of development projects during design review.

### 7.4.3 Development Agreements

Approval of statutory Development Agreements, per individual property owner, authorized pursuant to California Government Code Sections 65864 et seq., is required as part of the approval of the Specific Plan and prior to approval of the first Final Map. The Development Agreements shall include, but not be limited to, methods for financing, acquisition, and construction of infrastructure. The MERRILL COMMERCE CENTER Development Agreement shall be fully executed prior to the issuance of the first building permits for development within the Specific Plan.

### 7.4.4 Conditional Use Permits

Uses specified as conditionally permitted uses within Chapter 5, *Development Regulations*, of this Specific Plan shall be reviewed and approved by the City pursuant to the requirements of the City's Development Code, Article 9, "Conditional Use Permits."

### 7.4.5 Variances

Variances and Administrative Exceptions to the development regulations contained in Chapter 5, *Development Regulations*, of this Specific Plan with respect to the site area, setback dimensions, building heights, distances between buildings, landscape percentage and off-street parking and loading shall be reviewed pursuant to "Variances and Administrative Exceptions" of the City's Development Code.

## 7.5 SUBSTANTIAL CONFORMANCE

All development under the Specific Plan is subject to a Substantial Conformance Determination, considered and approved ministerially by the Planning Director or designee. The Substantial Conformance Determination is also a mechanism that allows for the approval of ministerial minor modifications for development under the Specific Plan. The City recognizes that modifications to the text and exhibits of this document may be needed over time. Upon direction by the City of Ontario Planning Department, certain modifications to text, exhibits, and/or development standards and design guidelines may not require a formal Specific Plan Amendment (i.e., through public hearing) and occur ministerially. The following minor modifications to this document do not require a formal Specific Plan Amendment and are subject to review and approval by the Planning Director. The Planning Director shall have the discretion to defer to any request for modification to the Planning Commission or City Council, either for ministerial direction and guidance, or determination that the required change requires a formal amendment as discussed below in Section 7.6. Ministerial substantial conformance decisions are not subject to CEQA compliance.

- Expansions or reductions of the net acreage covered by a given Planning Area.
- A decrease in development intensity/density (building square footage).
- Modification of design criteria such as architectural details, landscape treatments, fencing, lighting, and entry treatments.
- Changes to the Phasing Plan, provided infrastructure is available to serve the phase as determined by the City Engineer.

- Implementation of alternative landscape materials, wall materials, entry monument design, primary/secondary corner treatments, and streetscape design that are generally consistent with the conceptual design guidelines contained within this Specific Plan.
- Modifications to Architectural Design Guidelines, such as variation of architectural style and variations in materials and colors.
- Final infrastructure facility sizing and precise location of dry utilities, water, sewer, and storm drainage improvements as approved by the City Engineer, OMUC, or San Bernardino Flood Control District.
- Roadway ROW design, when the changes are warranted and approved by the City Engineer.
- Revisions to exhibits which do not substantially change the intent of the Specific Plan.
- Modification, deletions, and additions to the list of permitted and conditional uses.
- Specific modifications of a similar nature to those listed above which are deemed minor by the Planning Director, which are in keeping with the intent of this Specific Plan and which are in conformance with the Policy Plan component of The Ontario Plan.

## 7.6 FORMAL AMENDMENTS TO THE SPECIFIC PLAN

All modifications to this document which do not meet the criteria of a Substantial Conformance (as defined in the previous subsection) shall be deemed to require a formal Specific Plan Amendment. This document was prepared pursuant to California Government Code

§65450, et. seq. Amendments shall be processed in accordance with the applicable requirements of the law, which include §65450, et. seq. of the California Government Code.

Formal Specific Plan Amendments shall be subject to the review and approval of the City Council. The Planning Commission should first hear and consider all applications for formal Specific Plan Amendments and provide a recommendation to the City Council. As required by the California Government Code, all government agencies significantly affected by the proposed Amendment shall be notified of the proposed action prior to the approval. In addition, and as required by CEQA, formal Specific Plan Amendments shall be appropriately reviewed in accordance with the State CEQA Guidelines, with the City of Ontario serving as the CEQA Lead Agency.

Any formal Specific Plan Amendment initiated by an applicant requires preliminary review by the Planning Director, filing of an official application and required materials supporting the Amendment, submittal of a fee deposit, Planning Commission review and recommendations, and City Council review and final decision.

Amendments also may be initiated by the City Council or Planning Commission by majority vote. All Planning Commission-requested amendments shall be submitted and considered by the City Council and accepted for processing by a majority vote. City staff may initiate an amendment by submitting the requested amendment to the Planning Commission for a vote. Only amendments accepted by a majority vote of the Planning Commission shall be submitted to the City Council for consideration; however, the Applicant shall have the right of appeal to City Council if the Planning Commission fails to approve.

In considering approval or disapproval of formal Specific Plan Amendments, the City Council shall find that the request is:

- Consistent with the Policy Plan component of The Ontario Plan (General Plan) in effect at the time of consideration.
- Compatible with surrounding land uses with respect to use, development standards, density, or issues of health, public safety, and general welfare.
- Consistent with the overall design character and general structure of the MERRILL COMMERCE CENTER as set forth in Chapter 6, *Design Guidelines*, of this Specific Plan.

### **7.7 APPEALS**

Appeals from any determination of the Planning Director, Zoning Administrator or the Planning Commission, may be made by the applicant or any other aggrieved party by filing an application on forms provided by the City of Ontario and accompanied by the appropriate filing fee, where applicable, within ten (10) days following the final date of action for which an appeal is made. Appeals shall be processed consistent with the provisions of Article 5, "Appeals" of the City of Ontario Development Code.

### **7.8 COMPLIANCE WITH MITIGATION MONITORING AND REPORTING PROGRAM**

Certification of an EIR shall be required prior to the approval of the Specific Plan. Development within the MERRILL COMMERCE CENTER Specific Plan shall comply with all approved mitigation measures as described in the MMRP included as part of the EIR.

### **7.9 PROJECT FINANCING**

The financing of construction, operation, and maintenance of public improvements, facilities, and public services shall include funding through a combination of financing mechanisms. Prior to the recordation of final maps, a final determination shall be made by City staff and confirmed by the Planning Director and City Engineer regarding the responsibility for construction and maintenance of public facilities, whether publicly or privately maintained.

Implementation of the MERRILL COMMERCE CENTER Specific Plan may involve financing options including, but not limited to, the following:

#### **7.9.1 Facilities and Services**

Construction of public improvements and facilities and the provision of public services may be financed through private capital investment, a Community Facilities District (CFD), or other special district, pursuant to the Mello-Roos Community Facilities District Act of 1982.

#### **7.9.2 Operation and Maintenance**

Options for operation and maintenance of public improvements and facilities include, but are not limited to, the following:

- Individual private property owners
- Private Property Owners Association
- CFD or other special district (will be at the City's sole discretion)

### **7.10 PHASING PLAN**

As shown on Figure 7-1, *Conceptual Phasing Plan*, the development of the MERRILL COMMERCE CENTER is expected to occur in three (3) phases in response to market demands and according to a logical and orderly extension of roadways, public utilities, and

infrastructure. Phase A includes Planning Areas 4 and 5; Phase B includes Planning Areas 1, 2, 3, and 6; and Phase C includes Planning Areas 1A, 3A, 4A, 5A, and 6A. These phases may be developed as subphases and may occur either sequentially or concurrently with one another. Phasing of the Specific Plan shall be permitted; however, improvements will be dictated by the City of Ontario's Land Development Section and the approved MERRILL COMMERCE CENTER Development Agreement.

### **7.11 ADDITIONAL ENTITLEMENT REQUIREMENTS**

Currently, there are no existing City utilities, nor any improvement plans for City utilities in the vicinity of the Specific Plan area. Several miles of new infrastructure is required to provide City utility services to the Specific Plan area. In order to ensure orderly expansion of the City utility systems and other City infrastructure, the City has imposed the following requirements all Subdivision Maps and implementing developments within the Specific Plan area.

#### **A. Utilities System Map (USM)**

Prior to approval of any entitlement application (subdivision maps, Development Plan Reviews, etc.) in the Specific Plan area, as part of the entitlement application a Conceptual Utilities Systems Map (USM) shall be prepared and submitted to the Development Agency Engineering Department and the Utilities Engineering Department for review and approval. The USM is a summary plan sheet exhibit that shows all the public offsite infrastructure requirements and demands for the development project (and/or subdivision), the onsite private infrastructure improvements, and the interaction between the public and private utilities systems. As a Condition of Approval for the development project (and/or subdivision), the Conceptual Utilities Systems Map shall be updated into a Final Utilities Systems Map to reflect the changes that occur between

entitlement and Final Plan and Permit Approval. Reference the Ontario Municipal Utilities Company Utilities Engineering Department's Utilities Systems Map (USM) Requirements for details.

#### **7.11.1 Integrated Waste Management Report (IWMR) and Solid Waste Handling Plan (SWHP)**

Prior to approval of any entitlement application (subdivision maps, Development Plan Reviews, etc.) in the Specific Plan area, as part of the entitlement application a Conceptual Integrated Waste Management Report (IWMR) and a Conceptual Solid Waste Handling Plan (SWHP) shall be prepared and submitted to the Development Agency Engineering Department and the Utilities Engineering Department for review and approval. The SWHP summary plan sheet exhibit that demonstrates the project site's design conformance with the Integrated Waste Department's requirements, including the requirements for Sizing of Storage, Location of Collection Areas, Accessibility for Collection Vehicles, and Collection of Sorted/Diverted Waste Types. The IWMR is a report that presents project specific information that is not able to be demonstrated within the Solid Waste Handling Plan, including discussions on: project solid waste operations; project compliance with all applicable laws, statutes, policies, and requirements; and, conformance with all the Integrated Waste Department's requirements, including the requirements for Sizing of Storage, Location of Collection Areas, Accessibility for Collection Vehicles, and Collection of Sorted/Diverted Waste Types. As a Condition of Approval for the development project (and/or subdivision), the Conceptual SWHP and the Conceptual IWMR shall be updated into a Final SWHP and Final IWMR to reflect the changes that occur between entitlement and Final Plan and Permit Approval. The SWHP and IWMR may be required to be updated from whenever there are new occupants, new uses or changes to existing uses, Tenant

Improvements, Business Licenses, and Certificates of Occupancy. Reference the Ontario Municipal Utilities Company Utilities Engineering Department's Integrated Waste Management Report (IWMR) and Solid Waste Handling Plan (SWHP) Requirements for details.

### 7.11.2 Conceptual Design Report

Prior to approval of any entitlement application (subdivision maps, Development Plan Reviews, etc.), a conceptual design report shall be prepared and submitted to the Development Agency Engineering Department and the Utilities Engineering Department for review and approval for the established extent of all public improvements required for the project. The study shall identify existing and future rights-of-ways (ROW) and infrastructure improvements and establish all vertical and horizontal alignments for each utility. The report shall include cross-sections, profiles, and any supporting details needed to demonstrate that utilities can be adequately accommodated in the public ROW. The study shall account for all utility conflicts, right-of-way variations, existing obstructions, and the timing of utility installation. *Utilities cannot be located along an alignment that conflicts with existing conditions (e.g. electrical poles, private property, etc.) unless that applicant is accepting the responsibility of modifying the existing conditions (e.g. undergrounding, relocation, ROW acquisition, etc.).*

#### A. Western Trunk Sewershed Scope

In order to assure that the Western Trunk Sewer (WTS) is designed to serve its entire Sewer Master Plan Tributary Area, all the Master Plan Trunk Sewers connecting to the WTS and the WTS shall be included in the Conceptual Design Report. This shall include: the Western Trunk Sewer from the Inland Empire Utility Agency's Kimball

Interceptor to the Whispering Lakes Pump Station Riverside Drive and Carpenter Avenue; the Euclid Trunk Sewer from the Western Trunk Sewer in Merrill Avenue to Chino Avenue; the Bon View Trunk Sewer from the Western Trunk Sewer in Merrill Avenue to Chino Avenue; the Grove Trunk Sewer from the Western Trunk Sewer in Merrill Avenue to Chino Avenue; the Walker Trunk Sewer from the Western Trunk Sewer in Schaeffer Avenue to Chino Avenue.

### 7.11.3 Preliminary Design Report

As a condition of entitlements (subdivision maps, Development Plan Reviews, etc.) within the Specific Plan and prior to submittal of Infrastructure Improvements Plans, a Preliminary Design Report (PDR) for all public infrastructure shall be submitted and approved by the Development Agency Engineering Department and the Utilities Engineering Department. The PDR shall include the following:

#### A. Conceptual Design Compliance

A discussion modifying or confirming the conceptual design established with the Project's Conceptual Design Report. The study shall confirm all rights-of-ways (ROW), infrastructure improvements, and vertical and horizontal alignments for each utility.

#### B. Street Cross Sections and Profiles

Street Cross Sections and Profiles shall be provided for each public street, private street, and Public Utility Easement (PUE) containing a public utility and at any point along the alignments where the ROW varies. The cross-sections shall show the location and size of each utility and shall annotate the property/ROW lines, the type of finished surface material, the distance of each utility from the centerline, the depth from the finished surface to the top of the pipe, and the distance between utilities (outside wall to outside wall).

### ***C. Constructability Review***

The report shall include a discussion of the constructability issues along the proposed alignment and identify the recommended construction methods that may be utilized. The study shall perform field investigation (field survey and potholing) in order to identify potential utility conflicts, right-of-way variations, existing obstructions, and constructability issues created by the timing of utility installation.

### ***D. Supporting Details***

The PDR shall include any supporting details needed to demonstrate that utilities can be adequately accommodated in the public ROW, including the placement of large appurtenances, clearance from existing obstructions, etc.

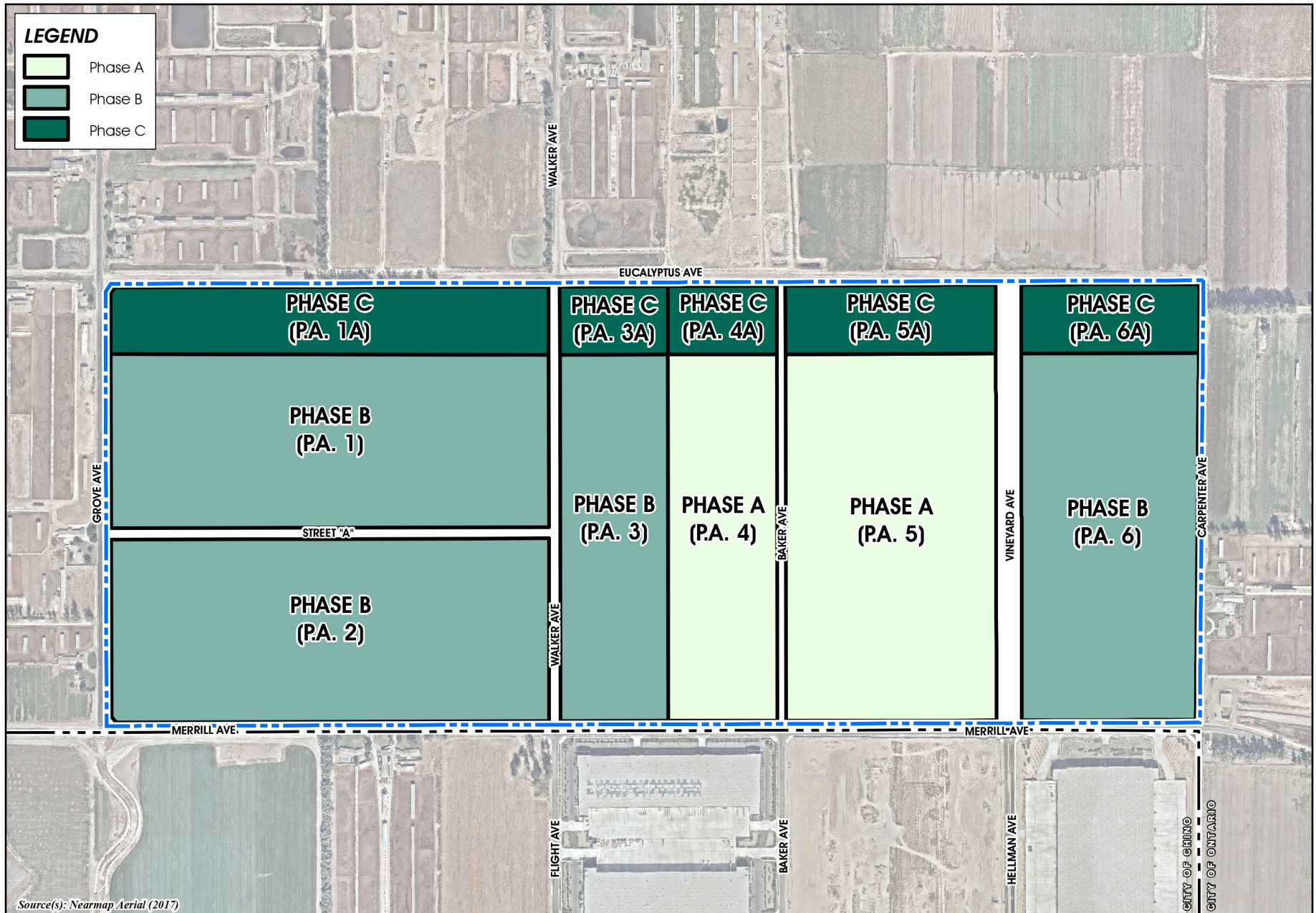
### ***E. 30% Design Drawings***

The PDR findings shall be incorporated into a 30% design plan set and included in the PDR.

### ***F. Western Trunk Sewershed Scope***

In order to qualify for Development Impact Fees (DIF) and to assure that the Western Trunk Sewer (WTS) is designed to serve its entire Sewer Master Plan Tributary Area, the PDR shall include all the Master Plan Trunk Sewers connecting to the WTS and the WTS. The PDR shall include: the Western Trunk Sewer from the Kimball Interceptor to the Whispering Lakes Pump Station Riverside Drive and Carpenter Avenue; the Euclid Trunk Sewer from the Western Trunk Sewer in Merrill Avenue to Chino Avenue; the Bon View Trunk Sewer from the Western Trunk Sewer in Merrill Avenue to Chino Avenue; the Grove Trunk Sewer from the





Source(s): Nearmap Aerial (2017)

Conceptual Phasing Plan

Figure 7-1



## 7.12 MAINTENANCE PLAN

The public and private improvements constructed within the MERRILL COMMERCE CENTER shall be maintained through a combination of public and private entities as described in Table 7-1, *Maintenance Responsibilities*. Table 7-1 provides a list of maintenance entity options that may fund and/or maintain facilities within the Specific Plan, however, the ultimate maintenance entity for each facility will be determined by the MERRILL COMMERCE CENTER's Development Agreement. A Property Owners Association (POA) shall be established for the maintenance of common area landscape improvements and private roadways within areas of the MERRILL COMMERCE CENTER. For areas in public ownership (such as public roadway ROWs), municipal maintenance districts may fund the maintenance of these areas.

Table 7-1 Maintenance Responsibilities

Facility	City and/or CFD	Property Owners Association	Property Owner or Occupant	Public Utility
Master Plan Roadways (Eucalyptus Avenue, Merrill Avenue, Grove Avenue, Walker Avenue, Baker Avenue, Vineyard Avenue, and Carpenter Avenue)				
Curb-to-curb improvements	✓			
Parkways within public right-of-way (ROW)	✓			
Neighborhood Edges, Master Plan Trails and Medians	✓			
Landscape buffer located outside public ROW		✓		
Interior public streets (Street A)				
Curb-to-curb improvements	✓			
Parkways within public right-of-way (ROW)	✓			
Sidewalks/Trails	✓			
Landscape buffer located outside public ROW		✓		
Interior private streets located outside of the public ROW (including parkways, landscaping, non-Master Plan Trails, and sidewalks)		✓		
Off-street parking areas			✓	
Traffic signals	✓			
Traffic control signs – in the public ROW	✓			

Facility	City and/or CFD	Property Owners Association	Property Owner or Occupant	Public Utility
Traffic control signs – not in the public ROW		✓		
Street lights – in the public ROW	✓			
Street lights – not in the public ROW		✓		
Public water, sewer, and storm drain improvements within public ROW (excluding laterals)	✓			
On-site landscaping and irrigation		✓	✓	
Common open space		✓		
Walls and fences		✓	✓	
Corner and Entry Monuments		✓		
Tenant Signage		✓	✓	
Off-street lighting		✓	✓	
Storm Water Drainage/Water Quality Facilities – within the curb-to-curb area of a public street	✓			
Permanent Storm Water Drainage/Water Quality Facilities (swales, basins, biotreatment filters, etc.) – behind the curb line of the public street		✓		
Dry Utilities (electricity, natural gas, communications systems)				✓
Fiber Optic communication system in the public ROW	✓			✓



**GENERAL PLAN CONSISTENCY ANALYSIS**

***APPENDIX A***

## APPENDIX A POLICY PLAN CONSISTENCY

California Government Code (Title 7, Division 1, Chapter 3, Article 8, §§ 65450 through 65457) allows local governments to adopt and administer specific plans as tools to implement their general plan; however, specific plans must demonstrate consistency with the goals and policies set forth in the local general plan. This appendix provides a summary discussion to demonstrate that this MERRILL COMMERCE CENTER Specific Plan is consistent with, and results in the implementation of, applicable primary goals and policies of the Policy Plan (General Plan) component of The Ontario Plan.

Ontario Plan Policy

Specific Plan Consistency

Land Use Element

**Goal LU1:** *A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.*

**Policy LU1-1: Strategic Growth.** We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.

**Consistent.** The MERRILL COMMERCE CENTER Specific Plan area is located in the Ontario Ranch area of the City, which is an area planned for future growth. Although existing infrastructure improvements are limited on and adjacent to the Specific Plan property, the City’s Master Plans for water, sewer, and storm drainage identify planned infrastructure facilities to support growth in this area. The developer(s) of the MERRILL COMMERCE CENTER will be required to participate in the installation of the Master Plan infrastructure. Immediately south of the MERRILL COMMERCE CENTER are existing and planned industrial warehouse developments and the Chino Airport, in the City of Chino. Development of the MERRILL COMMERCE CENTER will help to establish an identity for the City of Ontario along its southern border in this location, as envisioned by the Policy Plan’s Land Use Element. The Policy Plan designates the MERRILL COMMERCE CENTER property as “Business Park,” “Office Commercial,” and “General Commercial” land uses. This Specific Plan calls for “Business Park” and “Industrial” uses that are generally compatible with the Policy Plan’s intent for employment growth and development.

**Policy LU1-2: Sustainable Community Strategy.** We integrate state, regional and local Sustainable Community/Smart Growth principles into the development and entitlement process.

**Consistent.** The MERRILL COMMERCE CENTER features numerous sustainable features. The Specific Plan encourages non-motorized circulation by employees and visitors via its provision of an integrated network of sidewalks, bikeways, and trails. In accordance with the Exhibit M-3, *Multipurpose Trails & Bikeway Corridor Plan*, of the Policy Plan, Class II bike lanes are provided along both sides of the segment of Merrill Avenue located between Carpenter Avenue and Euclid Avenue; along both sides of the segment of Walker Avenue between Merrill Avenue and Edison Avenue; and along the south side of the segment of Eucalyptus Avenue that abuts the northern boundaries of Planning Areas 3A, 4A, 5A and 6A. Additionally, the MERRILL COMMERCE CENTER includes an 8-foot multi-purpose trail along segments of Merrill Avenue, Eucalyptus Avenue, Grove Avenue, Walker Avenue, and Vineyard Avenue which promotes pedestrian and bicycle connectivity.

Ontario Plan Policy	Specific Plan Consistency
	<p>All development in the Specific Plan area is required to conform to the California Green Building Standards Code (CalGreen). In addition, the roofs of industrial buildings will be structurally designed to support solar panels. The plant palette for the MERRILL COMMERCE CENTER is comprised of water-efficient species native to southern California or naturalized to the arid southern California climate, and the use of turf will be minimized. As such, the MERRILL COMMERCE CENTER is consistent with, and results in the implementation of, this policy.</p>
<p><b>Policy LU1-3: Adequate Capacity.</b> We require adequate infrastructure and services for all development.</p>	<p><b>Consistent.</b> The developer(s) of the MERRILL COMMERCE CENTER are required to install adequate roadway and utility infrastructure improvements to meet the demands of the Specific Plan while maintaining adequate service levels for existing, surrounding development. Refer to the Environmental Impact Report (EIR) prepared for the MERRILL COMMERCE CENTER Specific Plan for a detailed analysis of the adequacy of the Specific Plan’s infrastructure improvements.</p>
<p><b>Policy LU1-4: Mobility.</b> We require development and urban design, where appropriate, that reduces reliance on the automobile and capitalizes on multi-modal transportation opportunities.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER offers numerous opportunities for non-vehicular circulation, including multi-purpose trails, bikeways, and sidewalks. Class II Bikeways are designed along both sides of the segment of Merrill Avenue located between Carpenter Avenue and Euclid Avenue; along both sides of the segment of Walker Avenue between Merrill Avenue and Edison Avenue; and along the south side of the segment of Eucalyptus Avenue that abuts the northern boundaries of Planning Areas 3A, 4A, 5A, and 6A, as well as sidewalks, multi-purpose trails, and pathways to promote non-vehicular transportation. Multi-purpose trails are provided at the perimeters of MERRILL COMMERCE CENTER to encourage connectivity and circulation by employees, visitors, and the passerby by means not completely dependent on a motorized vehicle.</p>
<p><b>Policy LU1-5: Jobs-Housing Balance.</b> We coordinate land use, infrastructure, and transportation planning and analysis with regional, county and other local agencies to</p>	<p><b>Consistent.</b> The developer(s) of the MERRILL COMMERCE CENTER will construct roadway and utility improvements in accordance with the City’s infrastructure master plans. Furthermore, the MERRILL COMMERCE CENTER will generate employment opportunities for residents of the City of Ontario (City) and surrounding jurisdictions, and reduce the need for residents in the region to commute to other regions for employment</p>



Ontario Plan Policy	Specific Plan Consistency
<p>further regional and subregional goals for jobs-housing balance.</p>	<p>opportunities. Accordingly, implementation of the MERRILL COMMERCE CENTER Specific Plan will serve to improve the jobs-housing balance within the City and the Inland Empire region.</p>
<p><b>Policy LU1-6: Complete Community.</b> We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER land use plan provides for Industrial and Business Park land uses that will accommodate a variety of employment uses arranged in a sensible and efficient manner that allow ease of access and complement the surrounding community. With a mixture of Business Park and Industrial uses, the MERRILL COMMERCE CENTER is envisioned to attract local, national, and international businesses, and will assist in diversifying the mix of employment uses available in the City.</p>
<p><b>Policy LU1-7: Revenues and Cost.</b> We require future amendments to our Land Use Plan to be accompanied by analyses of fiscal impacts.</p>	<p><b>Consistent.</b> A fiscal impact analysis was prepared in support of the MERRILL COMMERCE CENTER. Refer to the MERRILL COMMERCE CENTER Specific Plan EIR for a detailed analysis of potential fiscal effects associated with the implementation of the Specific Plan.</p>
<p><b><u>Goal LU2:</u></b> <i>Compatibility between a wide range of uses.</i></p>	
<p><b>Policy LU2-1: Land Use Decisions.</b> We minimize adverse impacts on adjacent properties when considering land use and zoning requests.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER Specific Plan EIR evaluated the potential physical environmental impacts of the implementation of the Specific Plan on the surrounding community in accordance with the California Environmental Quality Act (CEQA). Refer to the MERRILL COMMERCE CENTER Specific Plan EIR for a detailed analysis of impacts to adjacent properties associated with the implementation of the Specific Plan.</p>
<p><b>Policy LU2-2: Buffers.</b> We require new uses to provide mitigation or buffers between existing uses where potential adverse impacts could occur.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER provides streetscapes for Eucalyptus Avenue, Merrill Avenue, Carpenter Avenue, and Grove Avenue; primary and secondary corner treatments at Specific Plan entry points; and landscaped screening walls between truck courts and parking areas and adjacent public rights-of-way. Implementation of these features provides a visual buffer between planned on-site Industrial and Business Park</p>

Ontario Plan Policy	Specific Plan Consistency
	land uses, and existing and planned off-site land uses. As such, the MERRILL COMMERCE CENTER is consistent with, and results in the implementation of, this policy.
<p><b>Policy LU2-3: Hazardous Uses.</b> We regulate the development of industrial and similar uses that use, store, produce or transport toxic substances, air emissions, other pollutants or hazardous materials.</p>	<p><b>Consistent.</b> Building occupants within the MERRILL COMMERCE CENTER Specific Plan are obligated to comply with all local and State requirements for using, storing, producing, or transporting toxic substances, air emissions, other pollutants, or hazardous materials. As such, the MERRILL COMMERCE CENTER results in the implementation of this policy.</p>
<p><b>Policy LU2-5: Regulation of Uses.</b> We regulate the location, concentration and operations of uses that have impacts on surrounding land uses.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER Specific Plan EIR evaluated the potential physical environmental impacts of the implementation of the Specific Plan on the surrounding community in accordance with CEQA. Refer to the MERRILL COMMERCE CENTER Specific Plan EIR for a detailed analysis of impacts to adjacent properties associated with the implementation of the Specific Plan.</p>
<p><b>Policy LU2-6: Infrastructure Compatibility.</b> We require infrastructure to be aesthetically pleasing and in context with the community character.</p>	<p><b>Consistent.</b> Water, sewer, storm drain, and dry utility improvements will be located underground and out of view, with the exception of detention basins and storm water inlets and channels that are required to be exposed at the surface. Several above-ground utility lines will be undergrounded with the implementation of development in the Specific Plan area. Roadway infrastructure improvements are designed to be landscaped with a combination of evergreen and deciduous trees – including flowering varieties – shrubs and groundcovers in an aesthetically-pleasing manner to establish the MERRILL COMMERCE CENTER design theme. Entry corners are designed with an “orchard row” landscape treatment as a nod to south Ontario’s agricultural history.</p>
<p><b>Policy LU2-8: Transitional Areas.</b> We require development in transitional areas to protect the quality of life of current residents.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER property is surrounded by agricultural uses, dairy operations, the Chino Airport, and existing and planned industrial warehouse uses. Nonetheless, the Specific Plan stipulates landscaping and building setbacks along all of the Specific Plan’s perimeter streets that separate the Specific Plan area from adjacent properties. Trucks traveling to and from the MERRILL COMMERCE CENTER are required to utilize the City’s approved truck routes, thereby minimizing exposure of nearby residents to truck-related impacts. Development within the MERRILL COMMERCE CENTER</p>

Ontario Plan Policy	Specific Plan Consistency
	must adhere to the Design Guidelines set forth in Chapter 6 of this Specific Plan, which will provide for a high-quality, attractive, and contemporary environment.
<p><b>Policy LU2-9: Methane Gas Sites.</b> We require sensitive land uses and new uses on former dairy farms or other methane-producing sites be designed to minimize health risks.</p>	<p><b>Consistent.</b> If determined to be necessary, the MERRILL COMMERCE CENTER will be required to implement the mitigation measures identified in the MERRILL COMMERCE CENTER Specific Plan EIR addressing soil remediation and building venting requirements related to methane gas hazards. As such, the MERRILL COMMERCE CENTER Specific Plan is consistent with this policy.</p>
<p><b>Goal LU4:</b> <i>Development that provides short-term value only when the opportunity to achieve our Vision can be preserved.</i></p>	
<p><b>Policy LU4-2: Interim Development.</b> We allow development in growth areas that is not immediately reflective of our ultimate Vision provided it can be modified or replaced when circumstances are right. We will not allow development that impedes, precludes or compromises our ability to achieve our Vision.</p>	<p><b>Consistent.</b> The central theme of the Ontario Vision is “A sustained, community-wide prosperity which continuously adds value and yields benefits.” The phased development of the MERRILL COMMERCE CENTER Specific Plan will attract new businesses, provide employment opportunities for residents, contribute the City’s tax base, and construct circulation and utility infrastructure improvements that are critical to allow for the growth of the region to continue. As such, the MERRILL COMMERCE CENTER will help the City achieve its Vision by continuously adding value, yielding benefits, and contributing to the prosperity of the City and the Inland Empire region.</p>
<p><b>Policy LU4-3: Infrastructure Timing.</b> We require that the necessary infrastructure and services be in place prior to or concurrently with development.</p>	<p><b>Consistent.</b> Chapter 7, <i>Implementation Plan</i>, of the MERRILL COMMERCE CENTER Specific Plan includes a development phasing plan which requires that infrastructure to support the buildout of the Specific Plan be adequately phased concurrently with development.</p>
<p><b>Goal LU5:</b> <i>Integrated airport systems and facilities that minimize negative impacts to the community and maximize economic benefits.</i></p>	
<p><b>Policy LU5-5: Airport Compatibility Planning for ONT.</b> We create and maintain the Airport Land Use Compatibility Plan for ONT.</p>	<p><b>Consistent.</b> The land uses provided by the MERRILL COMMERCE CENTER Specific Plan are consistent with the Airport Land Use Compatibility Plan (ALUCP) requirements for the Ontario International Airport and the Chino Airport. Furthermore, future development on</p>

Ontario Plan Policy	Specific Plan Consistency
<p><b>Policy LU5-7: ALUCP Consistency with Land Use Regulations.</b> We comply with state law that requires general plans, specific plans and all new development be consistent with the policies and criteria set forth within an Airport Land Use Compatibility Plan for any public use airport.</p>	<p>the Specific Plan property would be required to comply with the development standards and design guidelines established in this Specific Plan, as well as the applicable requirements of the City of Ontario Development Code, which would preclude any potential inconsistencies with the Ontario International Airport ALUCP.</p>

**Housing Element**

***Goal H1:** Stable neighborhoods of quality housing, ample community services and public facilities, well-maintained infrastructure, and public safety that foster a positive sense of identity.*

<p><b>Policy H1-4: Historical Preservation.</b> We support the preservation and enhancement of residential structures, properties, street designs, lot configurations, and other reminders of Ontario’s past that are considered to be local historical or cultural resources.</p>	<p><b>Consistent.</b> Refer to the MERRILL COMMERCE CENTER Specific Plan EIR for a detailed analysis of impacts to historical and cultural resources that would occur as a result of the implementation of the Specific Plan. If determined to be necessary, development projects within the MERRILL COMMERCE CENTER Specific Plan will be required to implement mitigation measures to reduce impacts to historical and cultural resources to the maximum extent feasible.</p>
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**Parks & Recreation Element**

***Goal PR1:** A system of safe and accessible parks that meets the needs of the community.*

<p><b>Policy PR1-15: Trail Connectivity.</b> We strengthen and improve equestrian, bike and multipurpose trail connections within the City and work to improve trail connections into adjacent jurisdictions.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER includes an 8-foot-wide multi-purpose trail along segments of Merrill Avenue, Eucalyptus Avenue, Grove Avenue, Walker Avenue, and Vineyard Avenue which promotes pedestrian and bicycle connectivity between the Planning Areas and with the surrounding land uses. The multi-purpose trail system provided by the MERRILL COMMERCE CENTER implements the multipurpose trail system shown in Exhibit M-3, <i>Multipurpose Trails &amp; Bikeway Corridor Plan</i>, of The Policy Plan.</p>
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Ontario Plan Policy

Specific Plan Consistency

Environmental Resources Element

*Goal ER1: A reliable and cost-effective system that permits the City to manage its diverse water resources and needs.*

**Policy ER1-3: Conservation.** We require conservation strategies that reduce water usage.

**Consistent.** The MERRILL COMMERCE CENTER incorporates a drought-tolerant plant palette and water-efficient irrigation system design to minimize the water demands of planned development. In addition, implementing development projects will be required to comply with the water-efficiency mandates of the California Green Building Standards Code (Title 24), including the provision of water-efficient fixtures. Accordingly, the MERRILL COMMERCE CENTER Specific Plan incorporates water conservation strategies and is consistent with this policy.

**Policy ER1-4: Supply-Demand Balance.** We require that available water supply and demands be balanced.

**Consistent.** The MERRILL COMMERCE CENTER Specific Plan EIR evaluates the impacts of the Specific Plan’s implementation related to water supply based on the results of a project-specific water supply assessment. If deemed necessary by the EIR, development within the Specific Plan is required to implement mitigation measures to reduce significant impacts to the water supply to the maximum extent feasible. Therefore, the MERRILL COMMERCE CENTER is consistent with this policy.

**Policy ER1-5: Groundwater Management.** We protect groundwater quality by incorporating strategies that prevent pollution, require remediation where necessary, capture and treat urban run-off, and recharge the aquifer.

**Consistent.** As discussed in detail in the MERRILL COMMERCE CENTER Specific Plan EIR, implementation of the Specific Plan will not result in new, substantial sources of urban runoff, and also will not violate applicable stormwater quality requirements. Also, the MERRILL COMMERCE CENTER includes permeable surfaces (e.g., landscaped areas, water quality/infiltration basins) that would allow surface water to percolate and contribute to recharge of the aquifer.

**Policy ER1-6: Urban Run-off Quantity.** We encourage the use of low impact development strategies to intercept run-off, slow the discharge rate, increase infiltration and ultimately reduce discharge volumes to traditional storm drain systems.

Ontario Plan Policy	Specific Plan Consistency
<p><b>Policy ER1-7: Urban Run-off Quality.</b> We require the control and management of urban run-off, consistent with Regional Water Quality Control Board regulations.</p>	
<p><b>Policy ER1-8: Wastewater Management.</b> We require the management of wastewater discharge and collection consistent with waste discharge requirements adopted by the Regional Water Quality Control Board.</p>	<p><b>Consistent.</b> All sewer conveyance infrastructure needed to serve the MERRILL COMMERCE CENTER will be installed in accordance with City design standards. Accordingly, implementation of the Specific Plan will not violate any applicable waste discharge requirements as detailed in the MERRILL COMMERCE CENTER Specific Plan EIR.</p>
<p><b>Goal ER3:</b> <i>Cost-effective and reliable energy system sustained through a combination of low impact building, site and neighborhood energy conservation and diverse sources of energy generation that collectively helps to minimize the region’s carbon footprint.</i></p>	
<p><b>Policy ER3-3: Building and Site Design.</b> We require new construction to incorporate energy efficient building and site design strategies, which could include appropriate solar orientation, maximum use of natural daylight, passive solar and natural ventilation.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER Specific Plan’s design guidelines encourage all new construction to utilize design features, fixtures, appliances, and heating and cooling controls to conserve energy and water. In addition, all development is required to comply with the California Green Building Standards Code (CalGreen).</p>
<p><b>Policy ER3-6: Generation- Renewable Sources.</b> We promote the use of renewable energy sources to serve public and private sector development.</p>	<p><b>Consistent.</b> Buildings roofs in the Industrial planning areas will be designed to structurally support the installation of solar panels.</p>
<p><b>Goal ER4:</b> <i>Improved indoor and outdoor air quality and reduced locally generated pollutant emissions.</i></p>	
<p><b>Policy ER4-1: Land Use.</b> We reduce GHG and other local pollutant emissions through compact, mixed use, and transit-oriented</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER provides employment land uses in close proximity to major transportation corridors (Interstate 10, Interstate 15, State Route 60, State Route 83, State Route 71 and State Route 91) and near BRT Corridors along Edison Avenue and Euclid Avenue. Therefore, the MERRILL COMMERCE CENTER provides local</p>

Ontario Plan Policy	Specific Plan Consistency
<p>development and development that improves the regional jobs-housing balance.</p>	<p>employment opportunities for residents and serves to improve the regional jobs-housing balance. Additionally, the MERRILL COMMERCE CENTER provides an integrated system of sidewalks, bikeways, and multi-purpose trails on internal and perimeter roadways to encourage non-vehicular modes of transportation. Based on the foregoing, the MERRILL COMMERCE CENTER has the potential to reduce vehicle miles traveled, which would reduce tailpipe emissions – a major source of greenhouse gases (GHGs). As such, the MERRILL COMMERCE CENTER would not prevent the City from achieving this Policy Plan goal.</p>
<p><b>Policy ER4-3: Greenhouse Gases (GHG) Emissions Reductions.</b> We will reduce GHG emissions in accordance with regional, state and federal regulations.</p>	<p><b>Consistent.</b> As described in the MERRILL COMMERCE CENTER Specific Plan EIR, the Specific Plan would be consistent with applicable regional, State, and federal regulations related to the reduction of GHG emissions and would not obstruct implementation of any GHG reduction plans/programs. The MERRILL COMMERCE CENTER is consistent with Policy ER4-3.</p>
<p><b>Policy ER4-8: Tree Planting.</b> We protect healthy trees within the City and plant new trees to increase carbon sequestration and help the regional/local air quality.</p>	<p><b>Consistent.</b> As part of the development of the MERRILL COMMERCE CENTER, a substantial number of trees will be planted within the Specific Plan area, and in particular along roadway street frontages, near building entrances, and in passenger car parking lots. At primary entry corners, trees will be planted in “orchard rows.”</p>
<p><i><b>Goal ER5:</b> Protected high value habitat and farming and mineral resource extraction activities that are compatible with adjacent development.</i></p>	
<p><b>Policy ER5-1: Habitat Conservation Areas.</b> We support the protection of biological resources through the establishment, restoration and conservation of high-quality habitat areas.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER property was historically used for agricultural uses, dairy farming, and a commercial trucking operation. Natural habitat is nil. The Specific Plan EIR evaluates impacts to biological resources (including sensitive/protected habitats and species) as a result of the implementation of the Specific Plan and establishes mitigation measures required to reduce significant impacts to the maximum extent feasible.</p>
<p><b>Policy ER5-2: Entitlement and Permitting Process.</b> We comply with state and federal regulations regarding protected species.</p>	

Ontario Plan Policy	Specific Plan Consistency
<p><b>Policy ER5-3: Right to Farm.</b> We support the right of existing farms to continue their operations within the New Model Colony.</p>	<p><b>Consistent.</b> Implementation of the MERRILL COMMERCE CENTER Specific Plan would not preclude the continued operation of existing farms within the New Model Colony/Ontario Ranch. Furthermore, the MERRILL COMMERCE CENTER Specific Plan EIR evaluates impacts to surrounding land uses that could potentially occur as a result of the implementation of the MERRILL COMMERCE CENTER Specific Plan and establishes mitigation measures required to reduce significant impacts to the maximum extent feasible.</p>
<p><b>Policy ER5-4: Transition of Farms.</b> We protect both existing farms and sensitive uses around them as agricultural areas transition to urban uses.</p>	

**Community Economics Element**

**Goal CE1:** *A complete community that provides for all incomes and stages of life.*

<p><b>Policy CE1-1: Jobs-Housing Balance.</b> We pursue improvement to the Inland Empire’s balance between jobs and housing by promoting job growth that reduces the regional economy’s reliance on out-commuting.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER would provide over 376.3 acres of new employment-generating land uses (up to 8,455,000 square feet of building area) in close driving distance to existing and planned residential land uses. The new job opportunities provided within the MERRILL COMMERCE CENTER Specific Plan area will assist the City’s efforts to promote job growth and improve the balance between jobs and housing within the City limits.</p>
<p><b>Policy CE1-12: Circulation.</b> We continuously plan and improve public transit and non-vehicular circulation for the mobility of all, including those with limited or no access to private automobiles.</p>	<p><b>Consistent.</b> BRT Corridors are located along Edison Avenue and Euclid Avenue in close proximity to the Specific Plan. Additionally, the MERRILL COMMERCE CENTER provides Class II Bikeways along both sides of the segment of Merrill Avenue located between Carpenter Avenue and Euclid Avenue; along both sides of the segment of Walker Avenue between Merrill Avenue and Edison Avenue; and along the south side of the segment of Eucalyptus Avenue that abuts the northern boundaries of Planning Areas 3A, 4A, 5A, and 6A, as well as sidewalks, multi-purpose trails, and pathways to promote non-vehicular transportation.</p>



Ontario Plan Policy	Specific Plan Consistency
<p><b>Goal CE2:</b> <i>A City of distinctive neighborhoods, districts, and corridors, where people choose to be.</i></p>	
<p><b>Policy CE2-1: Development Projects.</b> We require new development and redevelopment to create unique, high-quality places that add value to the community.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER Specific Plan provides for the development of the Specific Plan property as a high-quality and contemporary industrial/business park. This Specific Plan’s Design Guidelines (refer to Chapter 6) establish criteria for architecture, lighting, signage, and landscape design to promote the development of an attractive industrial/business park with timeless design features that are perceived as an inviting place to work and conduct business.</p>
<p><b>Policy CE2-2: Development Review.</b> We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER Specific Plan requires implementing development projects within the Specific Plan area to be reviewed and approved pursuant to the provisions of the City’s Subdivision Ordinance and Development Plan Review process which provides for review by the City’s Planning Department which may require the development to demonstrate how each project will create appropriately unique, functional and sustainable places. Furthermore, implementing development projects within the MERRILL COMMERCE CENTER Specific Plan are required to be designed in accordance with the architectural and landscape design guidelines established in Chapter 6, <i>Design Guidelines</i>, of this Specific Plan, which will result in a unique, aesthetically attractive, and contemporary industrial/business park that will be highly competitive with similar developments in the region.</p>
<p><b>Policy CE2-4: Protection of Investment.</b> We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.</p>	<p><b>Consistent.</b> Implementing development projects within the MERRILL COMMERCE CENTER Specific Plan are required to be designed in accordance with the architectural and landscape design guidelines established in Chapter 6, <i>Design Guidelines</i>, of this Specific Plan, which will result in a unique, aesthetically-attractive, and contemporary industrial/business park that reflects high-quality architectural design. As such, the MERRILL COMMERCE CENTER will implement this policy.</p>
<p><b>Policy CE2-5: Private Maintenance.</b> We require adequate maintenance, upkeep, and investment in private property because proper</p>	<p><b>Consistent.</b> This Specific Plan defines the entities responsible for the maintenance of publicly and privately-owned improvements within the MERRILL COMMERCE CENTER, including roadways and utility infrastructure. Compliance with the maintenance</p>

Ontario Plan Policy	Specific Plan Consistency
<p>maintenance on private property protects property values.</p> <p><b>Policy CE2-6: Public Maintenance.</b> We require the establishment and operation of maintenance districts or other vehicles to fund the long-term operation and maintenance of the public realm whether on private land, in rights-of-way, or on publicly-owned property.</p>	<p>responsibility matrix established in Chapter 7, <i>Implementation Plan</i>, will ensure that all improvements within the Specific Plan area are properly and consistently maintained.</p>
<p><b>Goal CE3:</b> <i>Decision-making deliberations that incorporate the full short-term and long-term economic and fiscal implications of proposed City Council actions.</i></p>	
<p><b>Policy CE3-2: General Plan Amendments.</b> We require those proposing General Plan amendments to disclose reasonably foreseeable impacts through a fiscal analysis.</p>	<p><b>Consistent.</b> A fiscal impact analysis was prepared in support of the MERRILL COMMERCE CENTER. Refer to the MERRILL COMMERCE CENTER Specific Plan EIR for a detailed analysis of potential reasonably foreseeable fiscal effects associated with implementation of this Specific Plan.</p>

**Safety Element**

<p><b>Goal S1:</b> <i>Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.</i></p>	
<p><b>Policy S1-1: Implementation of Regulations and Standards.</b> We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.</p>	<p><b>Consistent.</b> Implementing development within the MERRILL COMMERCE CENTER Specific Plan will be required by law to comply with the California Green Building Standards Code as adopted and implemented by the City. Geotechnical studies were required for the Specific Plan, and are contained as appendices to the Specific Plan EIR.</p>
<p><b>Policy S1-2: Entitlement and Permitting Process.</b> We follow state guidelines and the California</p>	

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<p>Building Code to determine when development proposals must conduct geotechnical and geological investigations.</p>	
<p><b>Goal S2:</b> <i>Minimized risk of injury, loss of life, property damage and economic and social disruption caused by flooding and inundation hazards.</i></p>	
<p><b>Policy S2-1: Entitlement and Permitting Process.</b> We follow State guidelines and building code to determine when development proposals require hydrological studies prepared by a State-certified engineer to assess the impact that the new development will have on the flooding potential of existing development down-gradient.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER Specific Plan EIR evaluates flooding impacts that could result from implementation of the MERRILL COMMERCE CENTER Specific Plan, primarily based on the results of a project-specific hydrology study. Where necessary, the MERRILL COMMERCE CENTER Specific Plan EIR incorporates mitigation measures to reduce significant flood hazard-related impacts to the maximum extent feasible. Furthermore, the MERRILL COMMERCE CENTER is required to improve the public storm drain system in accordance with the City’s master plan of drainage and as described in Specific Plan Chapter 4, <i>Infrastructure Plan</i>.</p>
<p><b>Policy S2-2: Flood Insurance.</b> We will limit development in flood plains and participate in the National Flood Insurance Program.</p>	
<p><b>Policy S2-5: Storm Drain System.</b> We maintain and improve the storm drain system to minimize flooding.</p>	
<p><b>Goal S3:</b> <i>Reduced risk of death, injury, property damage and economic loss due to fires, accidents and normal everyday occurrences through prompt and capable emergency response.</i></p>	
<p><b>Policy S3-8: Fire Prevention through Environmental Design.</b> We require new development to incorporate fire prevention</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER Specific Plan requires implementing development within the Specific Plan area to be reviewed and approved pursuant to the provisions of the City’s Subdivision Ordinance and Development Plan Review process which provides for review by the City’s Fire Department which may require the</p>

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consideration in the design of streetscapes, sites, open spaces and buildings.	incorporation of fire prevention design elements in streetscapes, sites, open spaces and buildings.
<b>Goal S4:</b> <i>An environment where noise does not adversely affect the public’s health, safety, and welfare.</i>	
<p><b>Policy S4-1: Noise Mitigation.</b> We utilize the City’s Noise Ordinance, building codes and subdivision and development codes to mitigate noise impacts.</p>	<p><b>Consistent.</b> Development within the MERRILL COMMERCE CENTER Specific Plan is required to comply with the City’s Noise Ordinance, building codes, and roadway design standards. Trucks traveling to and from the Specific Plan are required to utilize City-approved truck routes. Furthermore, the MERRILL COMMERCE CENTER Specific Plan EIR evaluates noise impacts that could result from construction and operation of the MERRILL COMMERCE CENTER Specific Plan, primarily based on the results of a project-specific noise study. Where necessary, the MERRILL COMMERCE CENTER Specific Plan EIR incorporates mitigation measures to reduce significant noise impacts to the maximum extent feasible.</p>
<p><b>Policy S4-4: Truck Traffic.</b> We manage truck traffic to minimize noise impacts on sensitive land uses.</p>	
<p><b>Policy S4-5: Roadway Design.</b> We design streets and highways to minimize noise impacts.</p>	
<p><b>Policy S4-6: Airport Noise Compatibility.</b> We utilize information from Airport Land Use Compatibility Plans to prevent the construction of new noise sensitive land uses within airport noise impact zones.</p>	
<b>Goal S5:</b> <i>Reduced risk of injury, property damage and economic loss resulting from windstorms and wind-related hazards.</i>	
<p><b>Policy S5-2: Dust Control Measures.</b> We require the implementation of Best Management Practices for dust control at all excavation and grading projects.</p>	<p><b>Consistent.</b> Construction activities within the Specific Plan will comply with a City-approved construction management plan and building code requirements related to dust control, and will implement all best management practices and mitigation measures identified in the MERRILL COMMERCE CENTER Specific Plan EIR with regard to dust control.</p>
<p><b>Policy S5-3: Grading in High Winds.</b> We prohibit excavation and grading during strong</p>	

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wind conditions, as defined by the Building Code.	
<b>Goal S6:</b> <i>Reduced potential for hazardous materials exposure and contamination.</i>	
<b>Policy S6-6: Location of Sensitive Land Uses.</b> We prohibit new sensitive land uses from locating within airport Safety Zones and near existing sites that use, store, or generate large quantities of hazardous materials.	<b>Consistent.</b> The MERRILL COMMERCE CENTER Specific Plan includes Industrial and Business Park land uses, which are generally not considered sensitive land uses. These land uses are consistent with the ALUCP requirements for the Ontario International Airport and the Chino Airport. Therefore, the MERRILL COMMERCE CENTER is consistent with this policy.
<b>Policy S6-9: Remediation of Methane.</b> We require development to assess and mitigate the presence of methane, per regulatory standards and guidelines.	<b>Consistent.</b> If determined to be necessary, the MERRILL COMMERCE CENTER will be required to implement the mitigation measures identified in the MERRILL COMMERCE CENTER Specific Plan EIR addressing soil remediation and building venting requirements related to methane gas hazards.
<b>Goal S7:</b> <i>Neighborhoods and commercial and industrial districts that are kept safe through a multi-faceted approach of prevention, suppression, community involvement and a system of continuous monitoring.</i>	
<b>Policy S7-4: Crime Prevention through Environmental Design (CPTED).</b> We require new development to incorporate CPTED in the design of streetscapes, sites, open spaces and buildings.	<b>Consistent.</b> Development within the MERRILL COMMERCE CENTER Specific Plan is required to be reviewed pursuant to the provisions of the City’s Subdivision Ordinance and Development Plan Review process which provides for review by the City’s Police Department which may require the development to incorporate CPTED in the design of streetscapes, sites, open spaces and buildings.

**Mobility Element**

**Goal M1:** *A system of roadways that meets the mobility needs of a dynamic and prosperous Ontario.*

<b>Policy M1-1: Roadway Design and Maintenance.</b> We require our roadways to:	<b>Consistent.</b> The developer(s) of MERRILL COMMERCE CENTER will improve all perimeter streets and new internal streets in accordance with the City’s Master Plan of Streets and Highways and City design standards, and would comply with the San Bernardino County
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<ul style="list-style-type: none"> <li>❑ Comply with federal, state and local design and safety standards.</li> <li>❑ Meet the needs of multiple transportation modes and users.</li> <li>❑ Handle the capacity envisioned in the Functional Roadway Classification Plan.</li> <li>❑ Maintain a peak hour Level of Service (LOS) E or better at all intersections.</li> <li>❑ Be compatible with the streetscape and surrounding land uses.</li> <li>❑ Be maintained in accordance with best practices and our Right-of-Way Management Plan.</li> </ul>	<p>Municipal Separate Storm Sewer System (MS4) Permit and Water Quality Management Plan. As described in Chapter 4, <i>Infrastructure Plan</i>, of this Specific Plan, the MERRILL COMMERCE CENTER includes roadway, bikeway, sidewalk, and multi-purpose trail improvements to facilitate efficient vehicular and non-vehicular transportation through and around the Specific Plan area. The MERRILL COMMERCE CENTER Specific Plan EIR is supported by a traffic study that identifies roadway facility improvements and fair share payments that will be the responsibility of the Specific Plan developer(s). roadway network is designed to operate at acceptable levels of service. All public roadways will be maintained in accordance with City requirements.</p>
<p><b>Policy M1-2: Mitigation of Impacts.</b> We require development to mitigate its traffic impacts.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER Specific Plan EIR is supported by a traffic study that stipulates the mitigation measures that the Specific Plan’s developer(s) will need to implement to address the traffic impacts of implementing development projects.</p>
<p><b>Policy M1-5: Complete Streets.</b> We work to provide a balanced context sensitive, multimodal transportation network that meets the needs of all users of streets, roads, and highways, including motorists, pedestrians, bicyclists, children, persons with disabilities, seniors, movers of commercial goods and users of public transportation.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER’s circulation plan encourages mobility via a variety of means. BRT Corridors are located along Edison Avenue and Euclid Avenue in close proximity to the Specific Plan. Additionally, the MERRILL COMMERCE CENTER provides Class II Bikeways along both sides of the segment of Merrill Avenue located between Archibald Avenue and Euclid Avenue; along both sides of the segment of Walker Avenue between Merrill Avenue and Edison Avenue; and along the south side of the segment of Eucalyptus Avenue that abuts the northern boundaries of Planning Areas 3A, 4A, 5A, and 6A, as well as sidewalks, multi-purpose trails, and pathways to promote non-vehicular transportation. Additionally, development within the MERRILL COMMERCE CENTER is required to comply with applicable building codes and standards to accommodate persons with disabilities.</p>

**Goal M2:** A system of trails and corridors that facilitate and encourage bicycling and walking.

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<p><b>Policy M2-1: Bikeway Plan.</b> We maintain our Multipurpose Trails &amp; Bikeway Corridor Plan to create a comprehensive system of on- and off-street bikeways that connect residential areas, businesses, schools, parks, and other key destination points.</p> <p><b>Policy M2-2: Bicycle System.</b> We provide off-street multipurpose trails and Class II bikeways as our primary paths of travel and use the Class III for connectivity in constrained circumstances.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER includes the construction of Class II Bikeways along both sides of the segment of Merrill Avenue located between Carpenter Avenue and Euclid Avenue; along both sides of the segment of Walker Avenue between Merrill Avenue and Edison Avenue; and along the south side of the segment of Eucalyptus Avenue that abuts the northern boundaries of Planning Areas 3A, 4A, 5A, and 6A. The bikeways would be provided in conformance with the City’s Multipurpose Trails &amp; Bikeway Corridor Plan.</p>
<p><b>Policy M2-3: Pedestrian Walkways.</b> We require walkways that promote safe and convenient travel between residential areas, businesses, schools, parks, recreation areas, and other key destination points.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER provides sidewalks along all perimeter and internal public streets to facilitate safe and convenient pedestrian travel to the Specific Plan area and between the planned Business Park and Industrial land uses. Additionally, the MERRILL COMMERCE CENTER includes an 8-foot multi-purpose trail along segments of Merrill Avenue, Eucalyptus Avenue, Grove Avenue, Walker Avenue, and Vineyard Avenue. All sidewalks and trails will be constructed in accordance with City standards, and landscaping will be spaced to provide motorists and pedestrians with adequate sight lines to promote safe travel.</p>
<p><b>Goal M4:</b> <i>An efficient flow of goods through the City that maximizes economic benefits and minimizes negative impacts.</i></p>	
<p><b>Policy M4-1: Truck Routes.</b> We designate and maintain a network of City truck routes that provide for the effective transport of goods while minimizing negative impacts on local circulation and noise-sensitive land uses, as shown in the Truck Routes Plan.</p>	<p><b>Consistent.</b> Trucks traveling to and from the MERRILL COMMERCE CENTER will be required to utilize City-designated truck routes. Merrill Avenue is a City-designated truck route. The MERRILL COMMERCE CENTER Specific Plan EIR evaluates the traffic- and noise-related impacts resulting from operation of the Specific Plan and identifies mitigation measures to reduce significant impacts to the maximum extent feasible.</p>

Community Design Element

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<p><b>Goal CD1:</b> <i>A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.</i></p>	
<p><b>Policy CD1-2: Growth Areas.</b> We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.</p>	<p><b>Consistent.</b> This Specific Plan includes detailed architectural and landscape design guidelines (refer to Chapter 6) that address all aspects of land development, including site design, architectural design, landscape materials, monuments/entries, signage and lighting to ensure future development within the Specific Plan is aesthetically pleasing, cohesive, and distinctive, yet also complements the existing surrounding development.</p>
<p><b>Policy CD1-3: Neighborhood Improvement.</b> We require viable existing residential and non-residential neighborhoods to be preserved, protected and enhanced in accordance with our land use policies.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER is designed to protect the integrity of existing residential land uses within the vicinity of the Specific Plan. Streetscapes along the perimeter roadways include landscape buffers which physically and visually separate off-site areas from planned on-site Business Park and Industrial uses. Additionally, buildings would be sited to minimize adverse effects to nearby residential uses. For example, loading docks would be screened by a combination of building orientation, walls, and landscaping. Furthermore, exterior lighting fixtures would be focused on the Specific Plan property and focused/shielded to prevent light trespass on adjacent properties. The design features provided by the MERRILL COMMERCE CENTER to prevent “edge effects” with surrounding land uses are listed in Chapter 5, <i>Development Standards</i>, and Chapter 6, <i>Design Guidelines</i>, of this Specific Plan. Accordingly, the MERRILL COMMERCE CENTER will preserve and protect existing sensitive land uses near the Specific Plan property.</p>
<p><b>Policy CD1-4: Transportation Corridors.</b> We will enhance our major transportation corridors within the City through landscape, hardscape, signage and lighting.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER includes streetscapes that include landscaping – including evergreen and deciduous trees, low shrubs, and groundcovers – along perimeter and interior streets concurrent with development. The MERRILL COMMERCE CENTER also includes an 8-foot multi-purpose trail along segments of Merrill Avenue, Eucalyptus Avenue, Grove Avenue, Walker Avenue, and Vineyard Avenue. Additionally, Class II bike lanes are provided along both sides of the segment of Merrill Avenue located between Carpenter Avenue and Euclid Avenue; along both sides of the segment of Walker Avenue between Merrill Avenue and Edison Avenue; and along the south side of the segment of Eucalyptus Avenue that abuts the northern boundaries of</p>



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	<p>Planning Areas 3A, 4A, 5A, and 6A. The MERRILL COMMERCE CENTER also provides a series of entry monuments, tenant signage, and corner treatments, which incorporate architectural features (e.g., monument signs) and landscaping to welcome employees and visitors and establish the Specific Plan’s design theme.</p>
<p><b>Policy CD1-5: View Corridors.</b> We require all major north-south streets be designed and redeveloped to feature views of the San Gabriel Mountains, which are part of the City’s visual identity and a key to geographic orientation. Such views should be free of visual clutter, including billboards and may be enhanced by framing with trees.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER does not include any design components that would detract from views of the San Gabriel Mountains from major north-south street corridors. As part of build-out of this Specific Plan, landscaping – including trees – would be planted along the major north-south interior and perimeter streets in conjunction with improvements to these roadways.</p>
<p><b>Goal CD2:</b> <i>A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.</i></p>	
<p><b>Policy CD2-1: Quality Architecture.</b> We encourage all development projects to convey visual interest and character through:</p> <ul style="list-style-type: none"> <li>□ Building volume, massing, and height to provide appropriate scale and proportion;</li> <li>□ A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and</li> <li>□ Exterior building materials that are visually interesting, high quality,</li> </ul>	<p><b>Consistent.</b> Future development of the MERRILL COMMERCE CENTER will be guided by the Specific Plan’s Design Guidelines, which include comprehensive architectural criteria that provide for the development of an attractive, contemporary industrial/business park. The Design Guidelines specifically address architectural style, building form (shape, mass, scale, proportion, articulation), and building materials, colors, and textures to ensure that development is visually appealing and inviting to pedestrians and motorists. The MERRILL COMMERCE CENTER’s design theme complements the City of Ontario’s character and would not conflict with this policy.</p>

Ontario Plan Policy	Specific Plan Consistency
<p>durable, and appropriate for the architectural style.</p>	
<p><b>Policy CD2-5: Streetscapes.</b> We design new and, when necessary, retrofit existing streets to improve walkability, bicycling and transit integration, strengthen connectivity, and enhance community identity through improvements to the public right of way such as sidewalks, street trees, parkways, curbs, street lighting and street furniture.</p>	<p><b>Consistent.</b> As part of implementation of the MERRILL COMMERCE CENTER, existing public streets (Grove Avenue, Walker Avenue, Baker Avenue, Vineyard Avenue, Carpenter Avenue, Merrill Avenue, and Eucalyptus Avenue) will improved with new travel lanes, medians, bicycle lanes, sidewalks, and multi-purpose trails (to the extent shown in Chapter 4, <i>Infrastructure Plan</i>) to ensure safe vehicular and non-vehicular transportation. In addition, the MERRILL COMMERCE CENTER will provide landscaping (trees, shrubs, groundcovers, etc.) along all exterior street frontages and along interior streets to establish the Specific Plan’s design theme and create a welcoming visual environment for employees and visitors.</p>
<p><b>Policy CD2-7: Sustainability.</b> We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.</p>	<p><b>Consistent.</b> By nature of its location near regional transportation corridors, close driving distance to residential communities, and its planned mix of employment land uses to serve the surrounding community and region, the MERRILL COMMERCE CENTER is consistent with sustainable, “smart growth” principles. Building roofs of Industrial buildings will be constructed to support the installation of solar panels. Also, the plant palette for the MERRILL COMMERCE CENTER is comprised of drought-tolerant plant species native to Southern California or naturalized to the arid Southern California climate, and the use of turf will be minimized throughout the Specific Plan area, to minimize water use.</p>
<p><b>Policy CD2-8: Safe Design.</b> We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER Specific Plan serves as the framework for implementing development projects in the Specific Plan area, and includes design specifications for sidewalks, building entrances, and lighting.</p>

Ontario Plan Policy	Specific Plan Consistency
<p><b>Policy CD2-9: Landscape Design.</b> We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.</p>	<p><b>Consistent.</b> The landscape concept for the MERRILL COMMERCE CENTER incorporates the use of attractive, durable landscaping materials, and an irrigation system designed to keep plant materials in good health while conserving water. Landscaping will be provided throughout the MERRILL COMMERCE CENTER, including along roadways, within passenger car parking lots, at monuments/entries, within common open space areas, and adjacent to buildings.</p>
<p><b>Policy CD2-10: Surface Parking Areas.</b> We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.</p>	<p><b>Consistent.</b> Parking lots, truck courts, and loading areas within the MERRILL COMMERCE CENTER will be designed and constructed in accordance with the requirements of the City’s Development Code, and will include landscaping, screening walls, lighting, and well-defined drive aisles and parking spaces.</p>
<p><b>Policy CD2-11: Entry Statements.</b> We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER includes building entry treatments, primary and secondary corner treatments, and tenant monument treatments to identify the development and distinguish individual planning areas, in conformance with the intent of Policy CD2-11. The entry treatments, corner treatments, and tenant monuments which are illustrated in Chapter 6, <i>Design Guidelines</i>, incorporate landscaping and enhanced signage to provide attractive and distinctive visual statements.</p>
<p><b>Policy CD2-12: Site and Building Signage.</b> We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.</p>	<p><b>Consistent.</b> The Design Guidelines for the MERRILL COMMERCE CENTER (refer to Chapter 6 of this Specific Plan) include signage guidelines to ensure that future development will construct clear, concise, easy-to-read signs that reflect and complement the Specific Plan’s design theme, provide for safe and efficient circulation of vehicle traffic, and facilitate pedestrian travel. Signs will be of high-quality and the use of distracting sign elements, such as flashing lights or moving parts, is prohibited.</p>

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<p><b>Goal CD3:</b> <i>Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.</i></p>	
<p><b>Policy CD3-1: Design.</b> We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER provides a coordinated, interconnected circulation network for vehicles, bicycles, and pedestrians. All public roadways constructed by the MERRILL COMMERCE CENTER, including sidewalks, trails, and parkways, will be improved as illustrated in Chapter 4, <i>Infrastructure Plan</i>, and Chapter 6, <i>Design Guidelines</i>, of this Specific Plan and in accordance with City standards. Future development also will be required to comply with City standards related to the location of landscape plantings to ensure that adequate sight lines are provided for motorists and pedestrians.</p>
<p><b>Policy CD3-2: Connectivity between Streets, Sidewalks, Walkways and Plazas.</b> We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER Design Guidelines establish site planning and landscaping measures to provide efficient, well-defined pedestrian connections that follow a cohesive design theme.</p>
<p><b>Policy CD3-3: Building Entrances.</b> We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER Specific Plan includes an integrated network of sidewalks, multipurpose trails and bikeways that facilitates access to buildings located throughout the Specific Plan using non-vehicular means of transportation. Where appropriate and implemented, the building entry treatment (as depicted in Chapter 6, <i>Design Guidelines</i>, of this Specific Plan) provides an enhanced paved path that serves as a direct connection to the adjacent right-of-way.</p>
<p><b>Policy CD3-5: Paving.</b> We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER Specific Plan requires implementing development within the Specific Plan area to be reviewed and approved pursuant to the provisions of the City's Subdivision Ordinance and Development Plan Review process, which provides for review by the City's Engineering Department which will ensure roadways and sidewalks are designed and constructed to comply with the City's roadway design standards.</p>

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<p><b>Policy CD3-6: Landscaping.</b> We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.</p>	<p><b>Consistent.</b> The MERRILL COMMERCE CENTER utilizes landscaping to establish an attractive, cohesive design theme, as a focal point at building entry treatments, entrance monuments and corner treatments, to buffer on-site land from off-site land uses and adjacent roadway facilities, and to screen objectionable views from public views. Specific landscape design concepts for the MERRILL COMMERCE CENTER are described and illustrated in Chapter 6, <i>Design Guidelines</i>, of this Specific Plan.</p>
<p><b>Goal CD5:</b> <i>A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.</i></p>	
<p><b>Policy CD5-1: Maintenance of Buildings and Property.</b> We require all public and privately-owned buildings and property (including trails and easements) to be properly and consistently maintained.</p> <p><b>Policy CD5-2: Maintenance of Infrastructure.</b> We require the continual maintenance of infrastructure.</p>	<p><b>Consistent.</b> This Specific Plan defines the entities responsible for maintenance of publicly and privately-owned improvements within the MERRILL COMMERCE CENTER, including roadways and utility infrastructure. Compliance with the maintenance responsibility matrix established in Chapter 7, <i>Implementation Plan</i>, will ensure that all improvements within the Specific Plan area are properly and consistently maintained.</p>