

3.0 DEVELOPMENT PLAN

This chapter provides the framework to guide development of the South Ontario Logistics Center Specific Plan. The chapter presents the proposed planning areas, infrastructure plans, and public services to support the project.

3.1 Land Use Plan

The Specific Plan consists of two Planning Areas accommodating a variety of industrial-serving commercial, low-intensity office, technology, light manufacturing, and warehouse/distribution uses that are compatible with the site's location within Safety Zone 1, 3 and 6 of the Chino Airport Influence Area. Chino Airport Runway 21 Runway Protection Zone and CFR Part 77 height restriction of the Chino Airport. The Land Use Plan implements the vision of The Ontario Plan by providing opportunities for employment in manufacturing, distribution, and research and development at intensities designed to meet the demand of current and future market conditions. A list of allowable uses by Planning Area is presented in Chapter 4 (Land Use and Development Standards).

Figure 3.1a (Land Use Plan) identifies the location of the land use designations for the proposed Project area. The Specific Plan zoning mirrors the TOP Land Use Districts and is identified along with the two Planning Areas in Figure 3.1b.

BP (Business Park) Zoning District: The BP zoning district accommodates industrial-serving commercial, low intensity office uses, and certain light industrial uses. Development within this district is typically multi-tenant in nature; however, single-tenant buildings are not precluded.

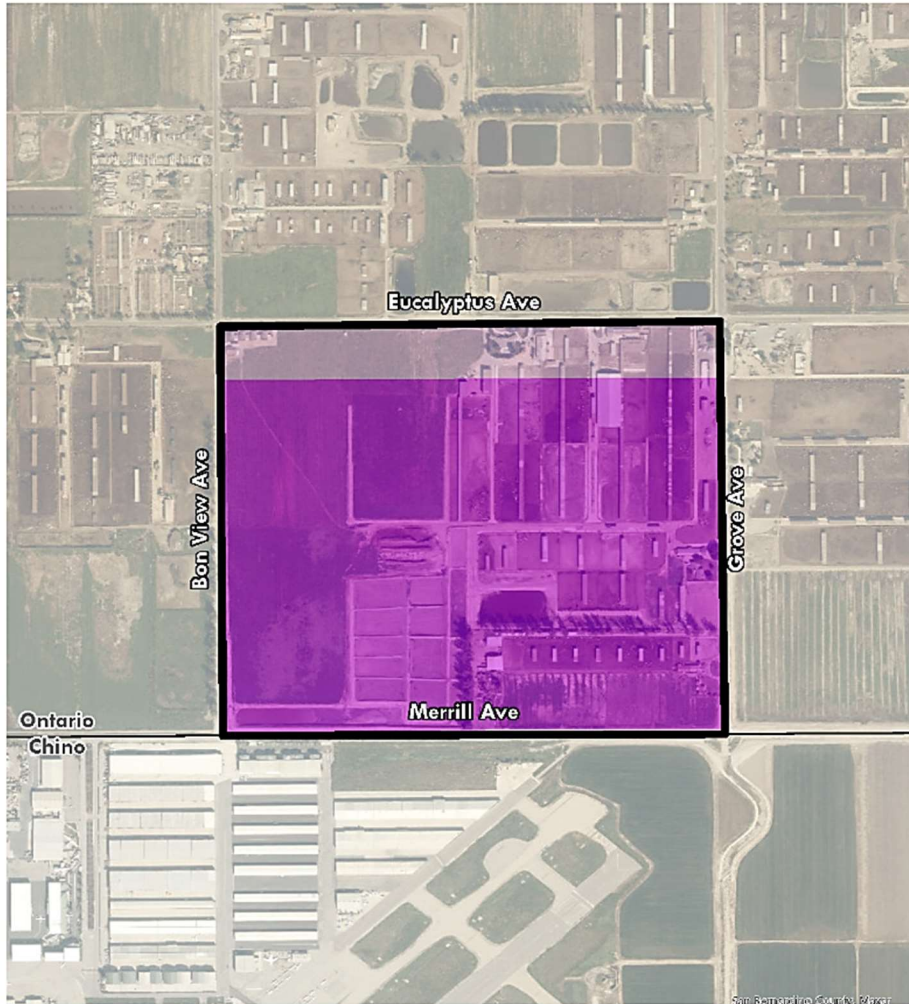
IG (Industrial - General) Zoning District: The IG zoning district accommodates storage and warehousing uses located in larger buildings on larger sites. Uses may include e-commerce, high cube warehouses, or distribution. A wide range of manufacturing and assembly uses are also permitted in this district.

Table 3.1 provides the maximum allowable gross building area for each Planning Area at its associated floor area ratio. Development standards (found in Chapter 4), such as setbacks, parking, landscaping, infrastructure, and site design, may reduce the maximum gross square footage.

Table 3.1 Maximum Specific Plan Build-Out

Planning Area	Maximum Floor Area Ratio ¹	Site Acreage	Maximum Building Square Footage
Planning Area 1: Business Park	0.60	23.65	618,116.4 SF
Planning Area 2: Industrial - General	0.55	124.20	2,975,583.6 SF
TOTAL		147.85	3,593,700
Note:			
1. Provided the General Plan Amendment application submitted in conjunction with this Specific Plan to designate PAs 1 as Business Park and PA 2 as Industrial – General is approved.			

Top Land Use Plan



TOP Land Use Designation

- Industrial General
- Business Park

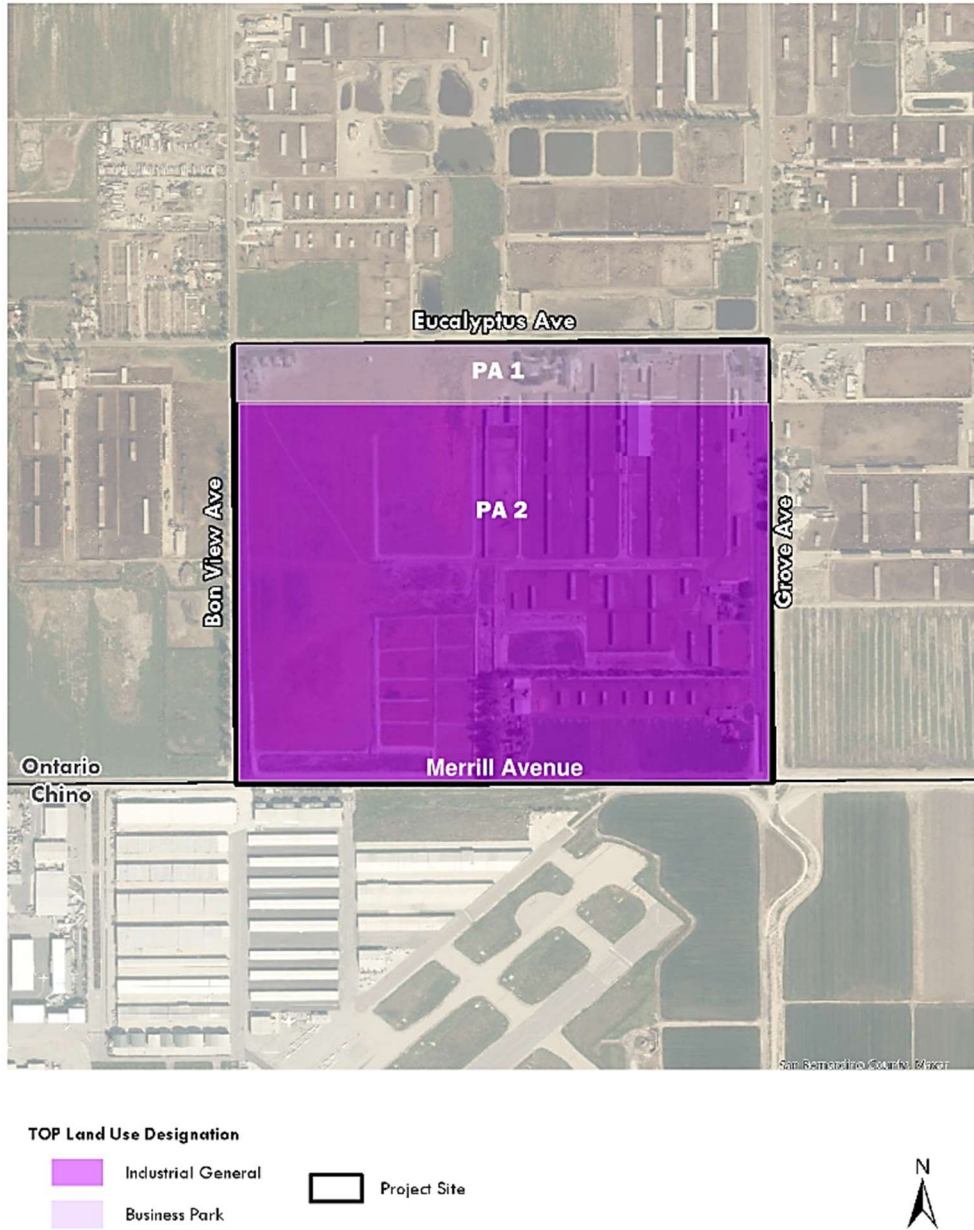
Project Site



South Ontario Logistics Center Specific Plan

Figure 3.1a

Proposed Specific Plan Land Use Plan



South Ontario Logistics Center Specific Plan

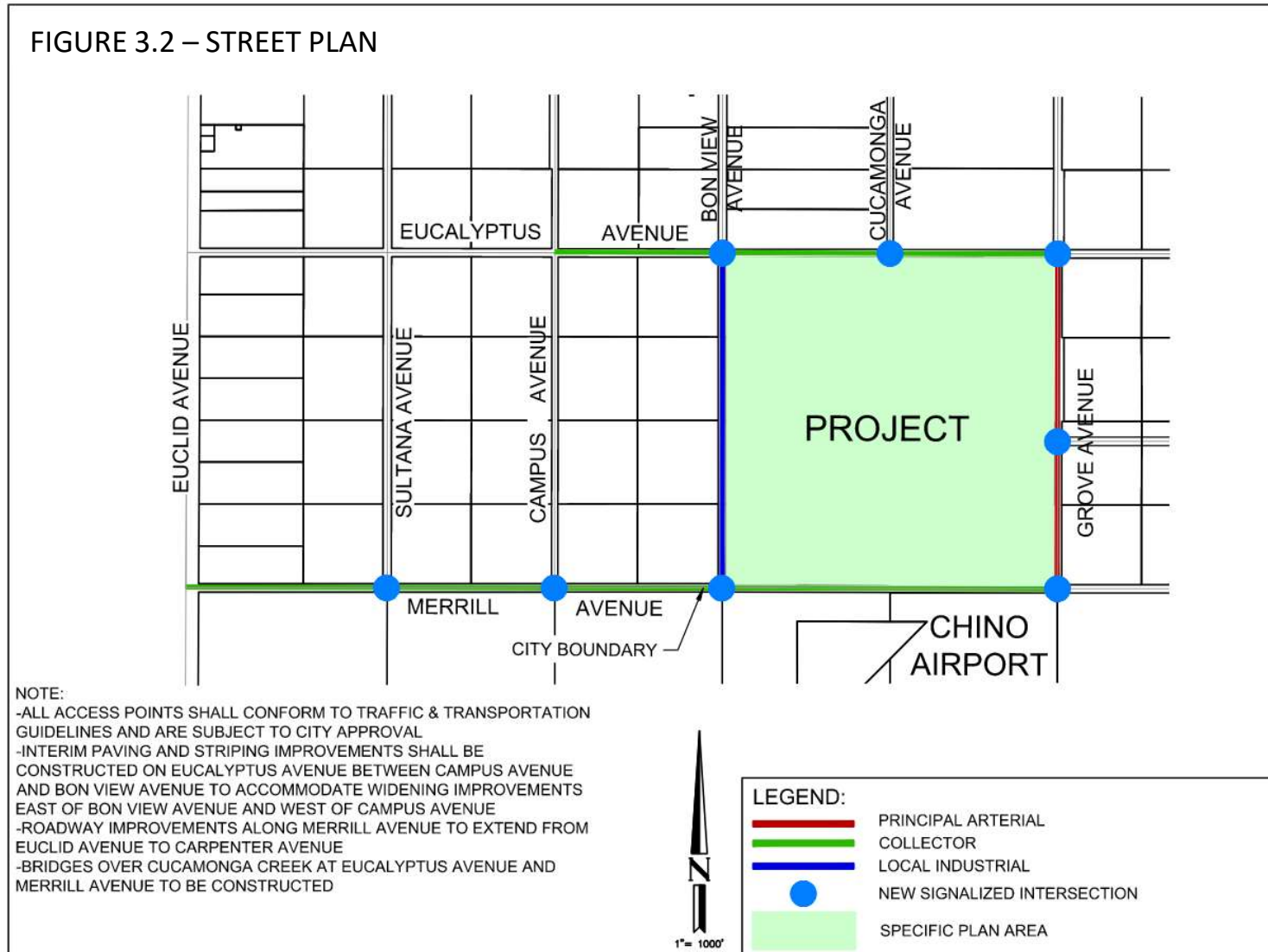
Figure 3.1b

3.2 Street Plan

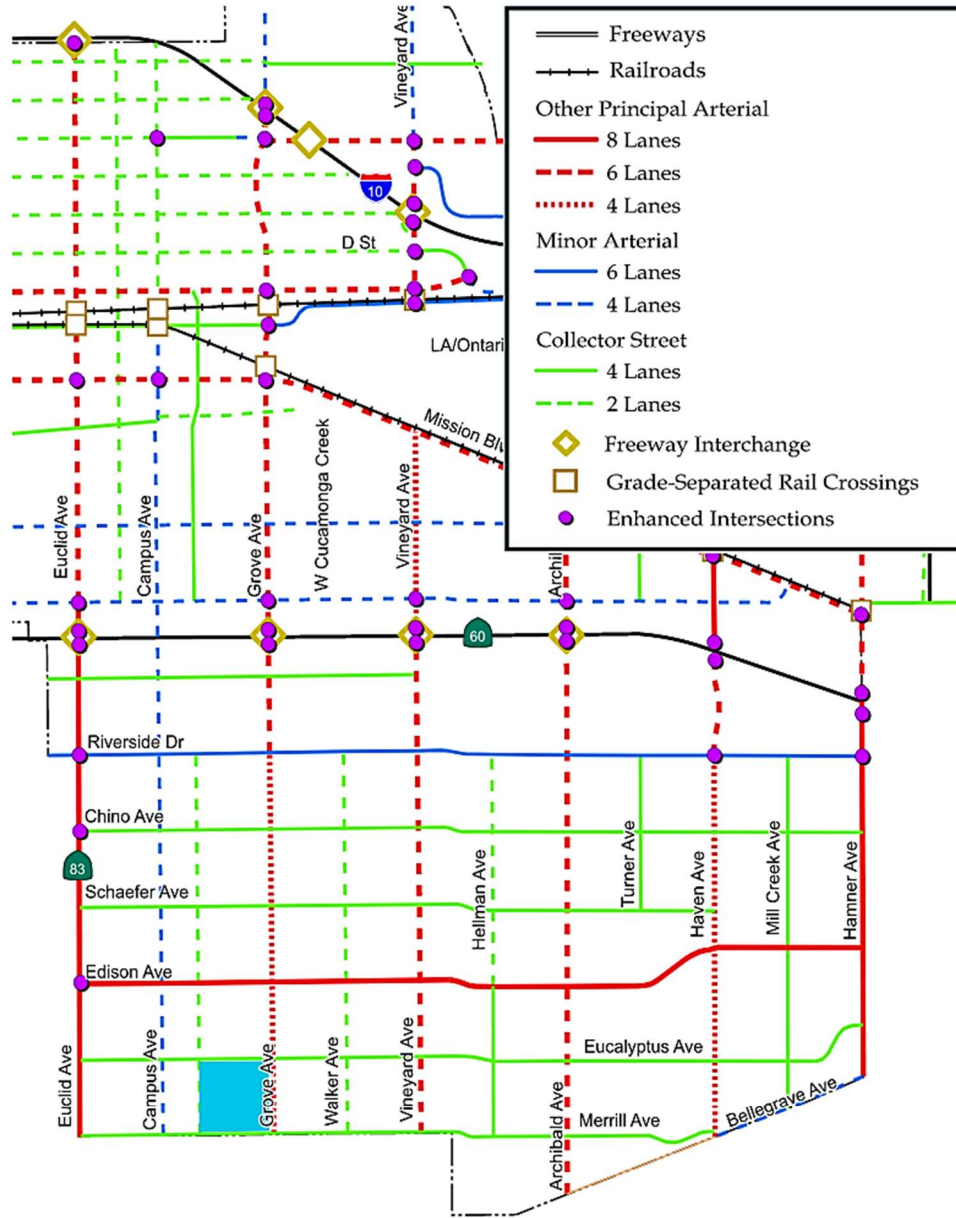
The Street Plan (Figure 3.2) facilitates movement of vehicles, pedestrians and cyclists within the Specific Plan area, consistent with the City of Ontario's Roadway Classification System, shown in Figure 3.3.

Figures 3.4 and 3.4a presents typical street cross sections for Grove, Eucalyptus, Merrill, and Bon View Avenue. Preliminary improvement responsibilities are indicated in the cross sections; however, final fair share responsibilities for street improvements shall be as determined in a Development Agreement with the City. Conceptual streetscape design is presented in Chapter 5 (Design Guidelines). Road surface, sidewalk, and trail improvements within the Specific Plan area shall be approved by the City's Engineering Department.

FIGURE 3.2 – STREET PLAN



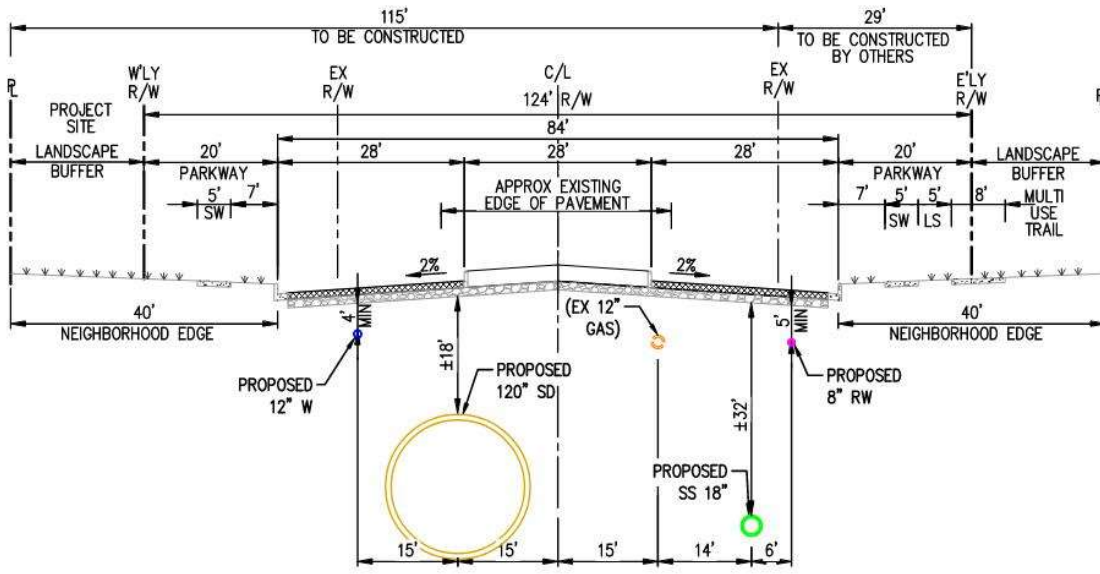
City of Ontario Roadway Classification Plan



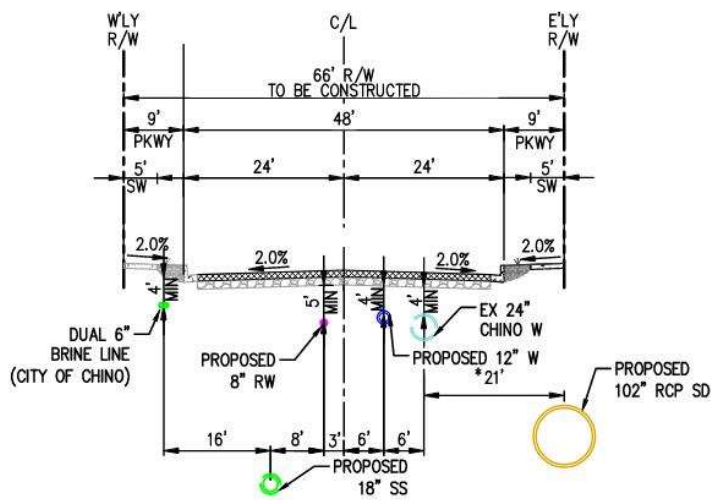
South Ontario Logistics Center Specific Plan

Figure 3.3

FIGURE 3.4 – STREET SECTIONS

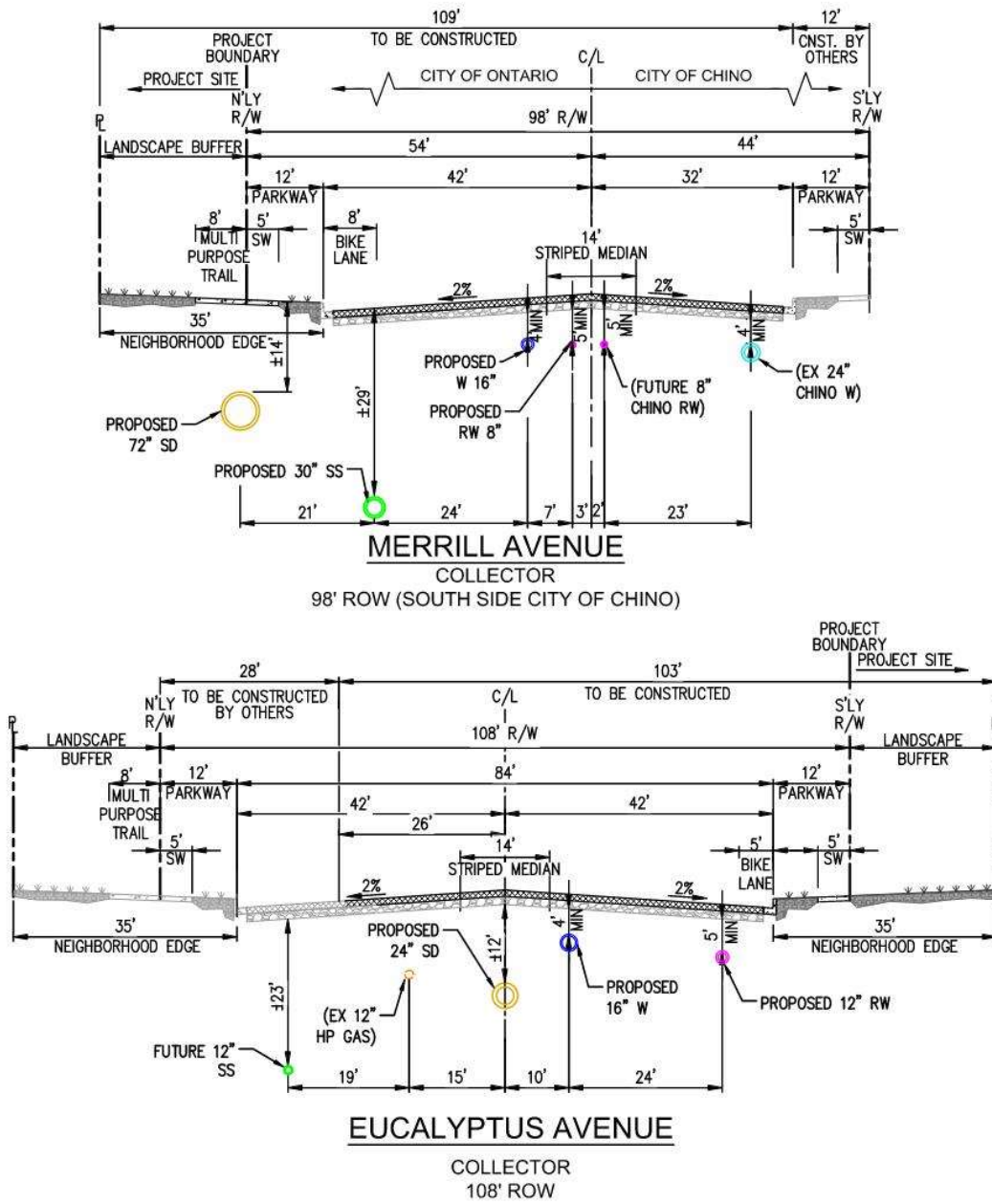


GROVE AVENUE
PRINCIPAL ARTERIAL
124' ROW



BON VIEW AVENUE
LOCAL INDUSTRIAL
66' ROW
*ADDITIONAL R/W OR AN EASEMENT
MAY BE REQUIRED

FIGURE 3.4a – STREET SECTIONS



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3.2.1 Grove Avenue

Grove Avenue is located along the eastern boundary of the Specific Plan area and is designated as an Other Principal Arterial in The Ontario Plan's Functional Roadway Classification Plan. Grove Avenue is designed with a 124-foot wide right-of-way, a 28-foot wide center median, and 28-foot wide pavement from median to curb face. The existing street right-of-way varies; therefore, a varied street dedication of 10 to 29 feet is required. An additional 20 feet dedication for the following neighborhood edge is required.

The Grove Avenue streetscape design illustrated in Chapter 5, Design Guidelines, specifies 20-foot wide parkways that include a five-foot wide sidewalk and 20-foot wide landscape buffer setbacks. The east side also includes an eight-foot wide decomposed granite multipurpose trail. Together, the parkways and landscape buffer setbacks create 40-foot wide neighborhood edges as specified in the *Ontario Ranch Colony Streetscape Master Plan*.

3.2.2 Eucalyptus Avenue

Eucalyptus Avenue is located along the northern boundary of the Specific Plan area, providing east/west access to the site. Eucalyptus Avenue is designated by the Functional Roadway Classification Plan as a four-lane Collector Street. The Specific Plan specifies an 108-foot wide right-of-way with 84 feet of pavement including curb/gutter. An additional 23ft dedication for the neighborhood edge is required.

The Eucalyptus Avenue streetscape design presented in Chapter 5, Design Guidelines, for the south side of the street adjacent to the project site specifies a 12-foot wide parkway including a seven-foot wide curb-adjacent landscaped area and a five-foot wide sidewalk. A five-foot wide Class II on-street bike lane at the edge of the street on the south side is proposed. The north side provides an eight-foot wide on-site multipurpose trail within a 23-foot wide landscape buffer setback. Together, the parkway and landscape buffer setback create a 35-foot wide neighborhood edge, as described in the *Ontario Ranch Colony Streetscape Master Plan*. A 21-foot dedication will be required for Eucalyptus Avenue.

3.2.3 Merrill Avenue

Merrill Avenue is designated as a four-lane Collector Street in the Functional Roadway Classification Plan and provides east-west access to the project's southern boundary. The centerline of this street forms the boundary between the City of Ontario to the north and the City of Chino to the south. The Specific Plan specifies a 98-foot wide right-of-way and 74 feet of pavement including curb and gutter for Merrill Avenue. An additional 23ft dedication for the neighborhood edge is required on the north side.

The Merrill Avenue streetscape design presented in Chapter 5, Design Guidelines, for the north side of the street includes an eight-foot wide Class II on-street bike lane at the edge of the street, a seven-foot wide curb-adjacent landscaped area, a five-foot wide sidewalk and an eight-foot wide multipurpose trail is located within a 23-foot wide landscape buffer setback. Together, these improvements establish a 35-

foot wide neighborhood edge, as specified in the *Ontario Ranch Streetscape Master Plan*. A 21-foot street dedication will be required for Merrill Avenue.

3.2.4 Bon View Avenue

Bon View Avenue is designated as a Local Industrial with a 66-foot wide right-of-way and 48 feet of pavement including curb and gutter. The Bon View Avenue right-of-way exists and no dedication is required.

The Bon View Avenue streetscape presented in Chapter 5, Design Guidelines, specifies 9-foot wide parkways including a five-foot wide sidewalk.

3.2.6 Local Circulation

Final site planning and off-site design shall be subject to City approval. In addition to the typical street section described and depicted, additional right-of-way and geometric enhancements such as additional left turn and right turn lanes, including but not limited to those at intersections, may be required to adequately mitigate impacts identified in the Traffic Impact Analysis/Specific Plan EIR. Local roadway circulation shall accommodate trucks with a double trailer combination wheelbase of 67 feet (known as the WB-67 design vehicle).

Driveways shall conform to access requirements of the City of Ontario Traffic and Transportation Design Guidelines and be to the satisfaction of the City Engineer. Driveway locations, specifically those that are in proximity to master-planned or future traffic signals, shall be located so as not to interfere with queues as projected in the Traffic Impact Analysis for the Specific Plan. The use of surrounding roads, drive aisles and truck parking lots to address the open land requirement for the Chino Airport Overlay zone is discussed in Chapter 2 (Section 2.2: Airport Influence Areas).

Fair share responsibilities for street improvements will be addressed in a Development Agreement with the City.

3.2.7 Traffic Control Devices

Traffic signs regulating, warning, and/or guiding traffic on public roads shall conform to the California Manual on Uniform Traffic Control Devices (MUTCD), latest edition. Traffic-control signs, whether on public or private property, shall conform to the California MUTCD.

3.2.8 Truck Routes

The City of Ontario designates and maintains a network of truck routes that provide for the effective transport of goods while minimizing negative impacts on local circulation and noise-sensitive land uses (Figure 3.5). Merrill Avenue, which runs along the southern boundary of the Specific Plan area, is a designated truck route from Euclid Avenue to Archibald Avenue. PCC pavement shall be installed per City Standard 1207 at signalized intersections along truck routes.

3.2.9 Pedestrian Circulation

Sidewalks will be provided along both sides of the street to improve safety and the pedestrian experience, connect the various parts of the Specific Plan area, and expand access to nearby land uses. Sidewalks shall be five-feet wide, constructed of concrete, and installed in conjunction with adjacent roadway improvements.

3.2.10 Trails and Bike Paths

Trails and bicycle paths provide an additional mode of circulation in and around the Specific Plan area.

The Ontario Plan Mobility Element specifies a Class II bikeway on the north side of Merrill Avenue, and the south side of Eucalyptus.. A Class II bikeway is defined as a dedicated (striped) lane along the street, with no parking allowed in the bike lane. This bike lane provides linkages to the City's bike path system (Figure 3.6).

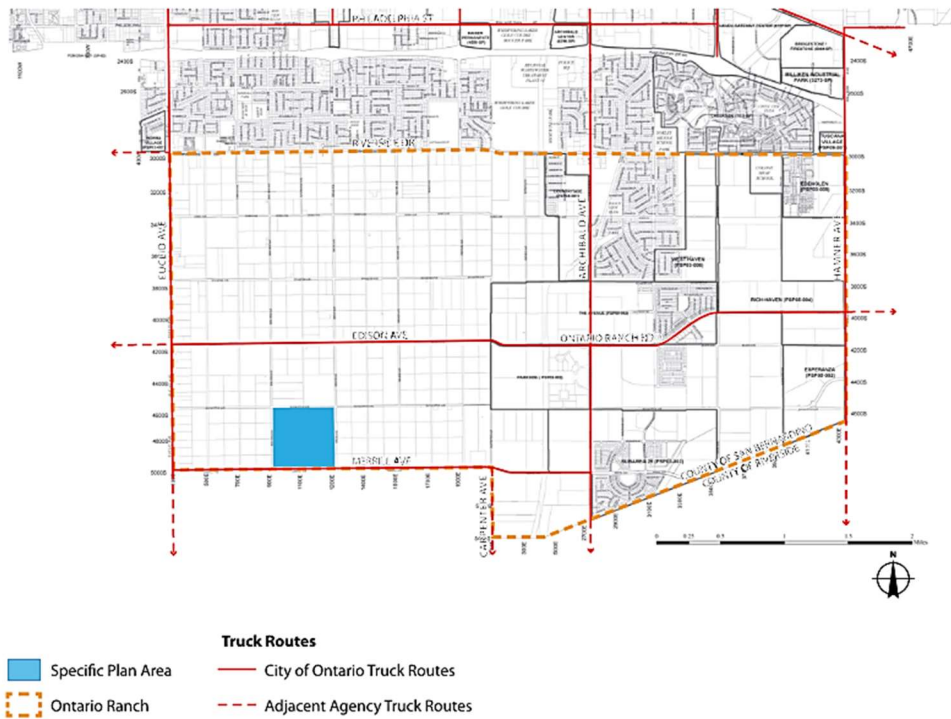
Eight-foot wide multipurpose trails are provided on the east side of Grove Avenue, the north side of Eucalyptus Avenue and the north side of Merrill Avenue (Figure 3.6).

The Specific Plan's trail and bikeway improvements shall be installed in conjunction with street improvements. The City reserves the right to implement bike lanes on Eucalyptus at the discretion of the Traffic and Transportation Division.

3.2.11 Transit

Transit options provide an alternative mode of transportation for motorists and a primary mode for the transit-dependent. The City is coordinating with regional transit agencies to implement Bus Rapid Transit (BRT) service to target destinations and along corridors, including Euclid Avenue west of the Specific Plan area.

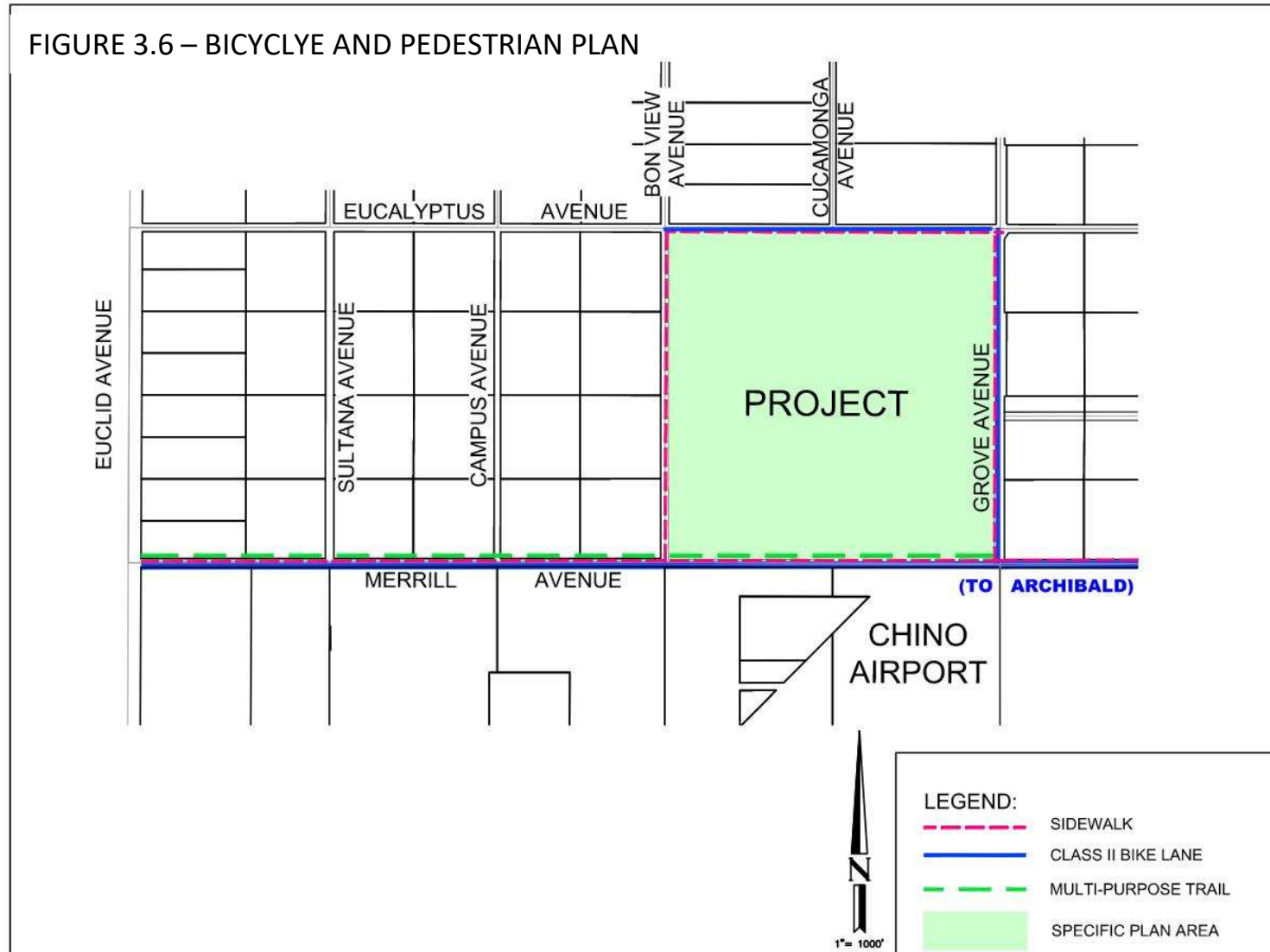
Truck Routes



South Ontario Logistics Center Specific Plan

Figure 3.5

FIGURE 3.6 – BICYCLE AND PEDESTRIAN PLAN



3.3 Potable Water Plan

Water service to the Specific Plan area will be provided by the City of Ontario. Potable water is provided by imported water from the Water Facilities Authority (WFA), Chino Basin Desalter Authority (CDA) and groundwater from the Chino Basin, extracted via the City's wells. The WFA was formed in 1980 as a Joint Powers Authority by the Cities of Chino, Chino Hills, Ontario and Upland, and the Monte Vista Water District. It was formed to construct and operate water treatment facilities that provide a supplemental supply of potable water to its member agencies.

Currently there are no City potable water mains or City potable water infrastructure in the vicinity of the Specific Plan Area. Potable Water System Improvements for the Specific Plan area (Figure 3.7a & Figure 3.7b) require the planning, design, and construction of the 925 Pressure Zone (PZ) Phase 2 West Backbone, which includes: Extending the 24-inch potable water main in Eucalyptus Avenue from Carpenter Avenue to Grove Avenue; installing a 30-inch to 42-inch potable water main in Grove Avenue connecting from the 24-inch potable water main in Eucalyptus Avenue and extending to Chino Avenue; installing an 18-inch potable water main in Chino Avenue connecting from the 30-inch to 42-inch potable water main in Grove Avenue and extending to connect to the end point of an existing 18-inch potable water main on the west side of the Cucamonga Channel; installing a 42-inch potable water main in Grove Avenue that ended at Chino Avenue and extending to Francis Street; installing a 42-inch potable water main in Francis Street connecting from the 42-inch potable water main in Grove Avenue and extending to Bon View Avenue; installing a 42-inch potable water main in Bon View Avenue connecting from the 42-inch potable water main in Francis Street and extending to Bon View Avenue Reservoir site and to the Reservoir. At the time this Specific Plan was prepared, the alignment of the 42-inch water line between Chino Avenue and the water reservoir site had not been finalized and is subject to change. This Specific Plan will be required to participate in the future Phase 2 Water System Improvements north of Chino Avenue, as detailed in the development agreement with the City.

In addition to the 925 Pressure Zone (PZ) Phase 2 West Backbone, the Specific Plan area requires the planning, design, and construction of a Secondary Industrial 16-inch Loop between the 925 Pressure Zone (PZ) Phase 2 West Backbone and the Specific Plan area which includes: installing a 16-inch potable water main in Eucalyptus Avenue connecting to the 30-inch to 42-inch 925 Pressure Zone (PZ) Phase 2 West Backbone main in Grove Avenue and extending to Bon View Avenue; installing a 16-inch potable water main in Merrill Avenue connecting from the 12-inch potable water main in Bon View Avenue and extending to Carpenter Avenue. The Specific Plan area also requires the planning, design, and construction of the Adjacent Potable Water System, which includes: installing a 12-inch potable water main in Bon View Avenue connecting to the 16-inch potable water main in Eucalyptus Avenue and extending to connect to the 16-inch potable water main in Merrill Avenue; and, installing a 12-inch potable water main in Grove Avenue connecting to the 16-inch potable water main in Eucalyptus Avenue and extending to connect to the 16-inch potable water main in Merrill Avenue.

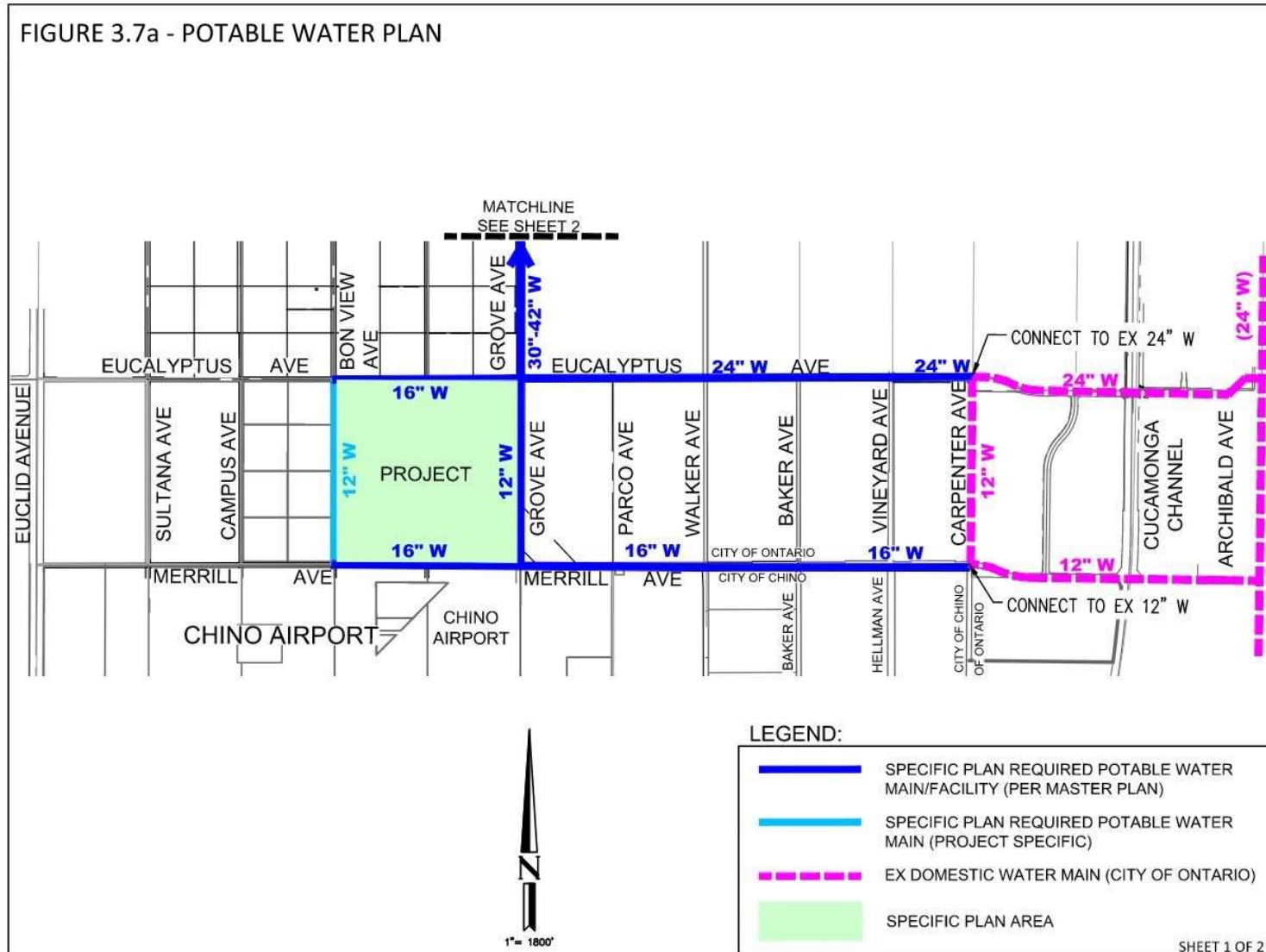
At minimum, the full Potable Water Infrastructure shown on Figure 3.7a and 3.7b shall be completed as Part of Phase 1 of the Specific Plan, which includes 925 Pressure Zone (PZ) Phase 2 West Backbone, the Secondary Loop, and the Adjacent Potable Water System

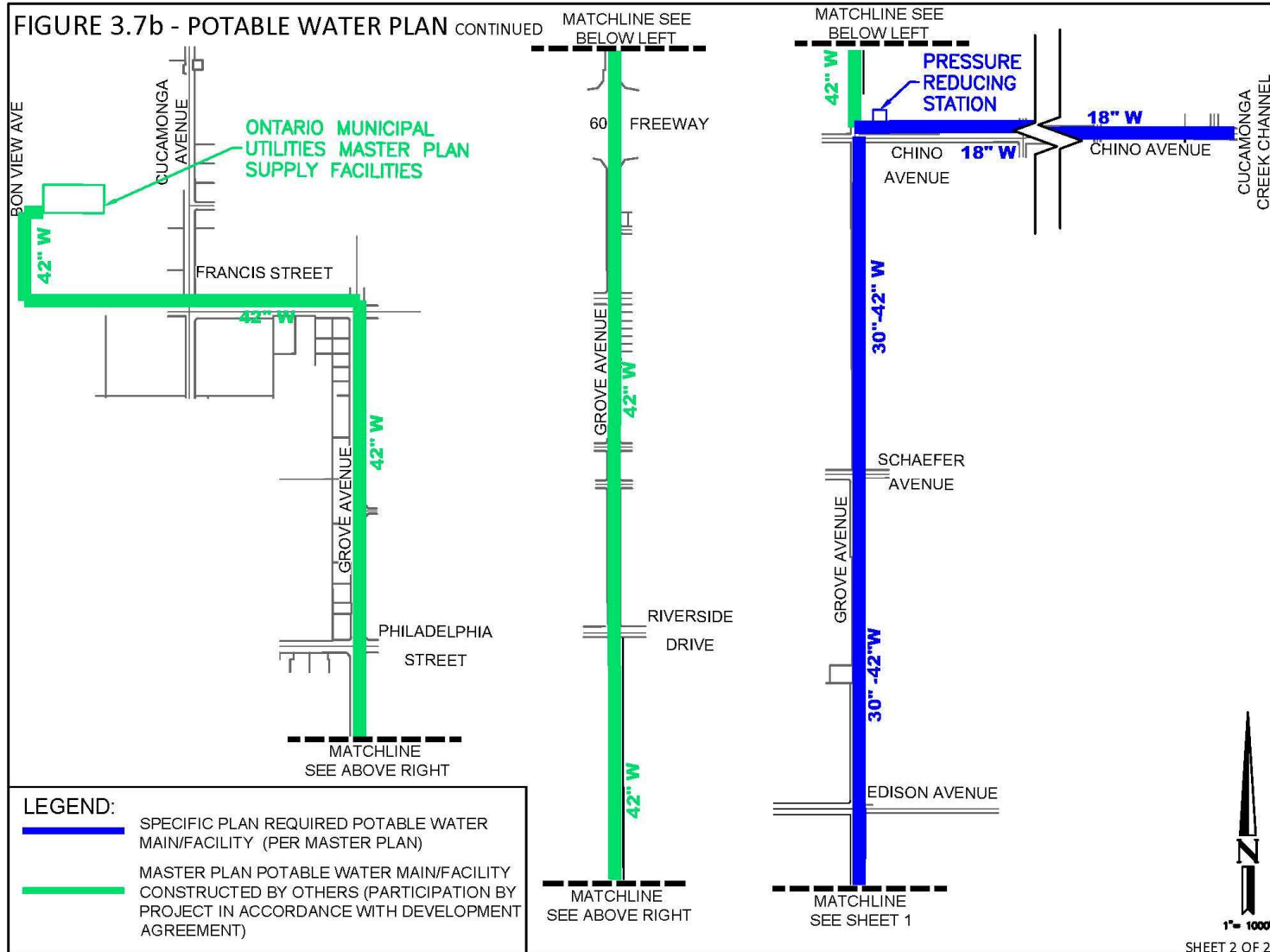
Water mains required to serve the project will need to be constructed prior to or concurrent with on-site water improvements. Within the project site, a network of 2-inch water lines for domestic water service and 8- to 10-inch water lines for fire service water will be installed. The on-site water system includes connections to the water main in Eucalyptus Avenue, Merrill Avenue, Grove Avenue, and Bon View Avenue.

Until the ultimate pipeline network for Ontario Ranch has been completed, there may be instances where construction of improvements to serve a project may not meet the required fire flow demands. Therefore, projects within the Specific Plan area may be required to construct additional pipelines not indicated in the Master Plan or upsize master planned pipelines to meet Fire Department fire flow requirements and/or Water Master Plan criteria. The developer shall submit a hydraulic analysis to the City for review and approval to demonstrate adequate fire flow and adherence to Potable Water Master Plan criteria.

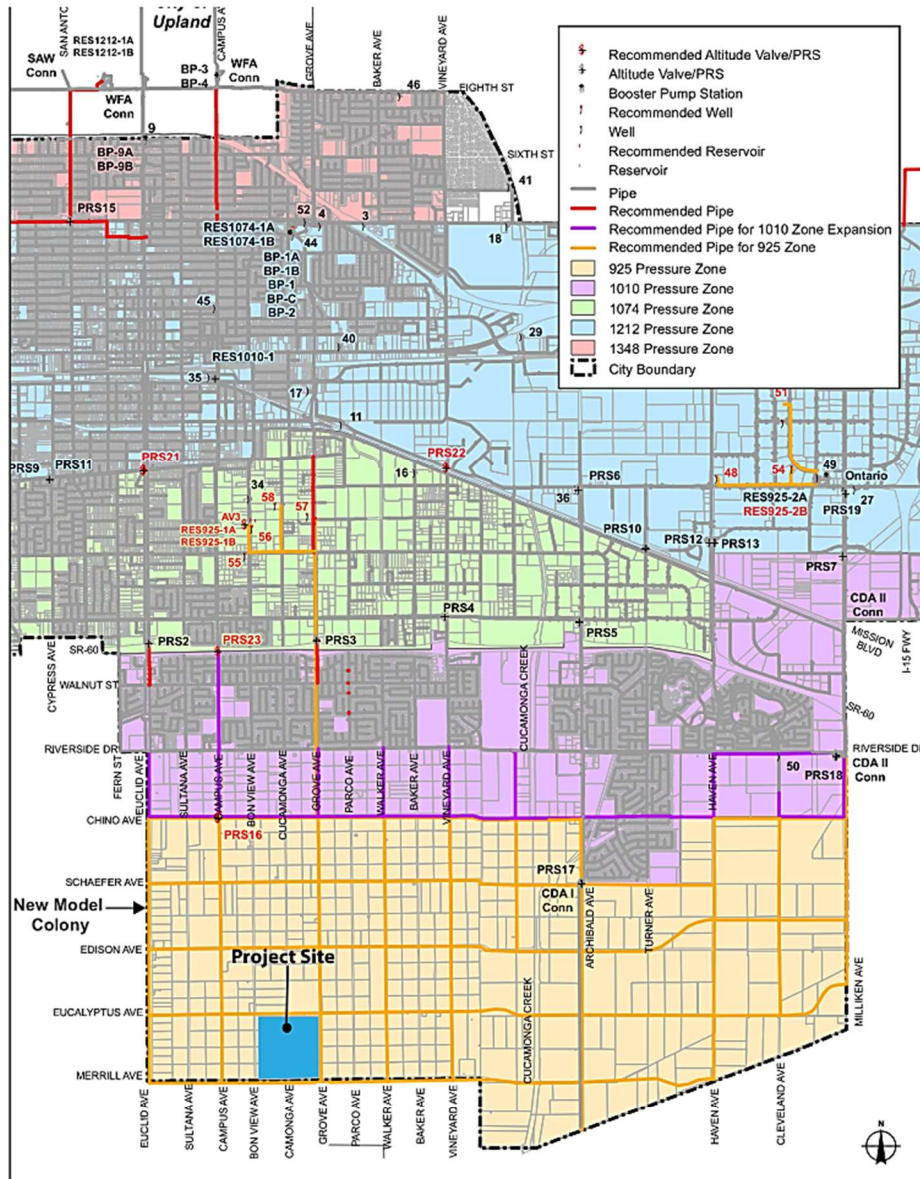
The overall water infrastructure plan to serve the City of Ontario is shown on Figure 3.8. The City's ultimate domestic water system consists of five pressure zones. Most of Ontario Ranch (including the Specific Plan area) is located within the 925 Pressure Zone. The sizing and alignment of potable water lines shall follow the current approved City of Ontario water system plan. Transmission line locations are subject to change, based on City conducted and approved hydraulic analysis. Required potable water infrastructure is subject to change based upon findings of the approved hydraulic study and master plan updates.

FIGURE 3.7a - POTABLE WATER PLAN





City of Ontario Ultimate Water System



South Ontario Logistics Center Specific Plan

Figure 3.8

Size and location of waterlines subject to change based on current approved Master Plan as well as City conducted and approved hydraulic analysis.

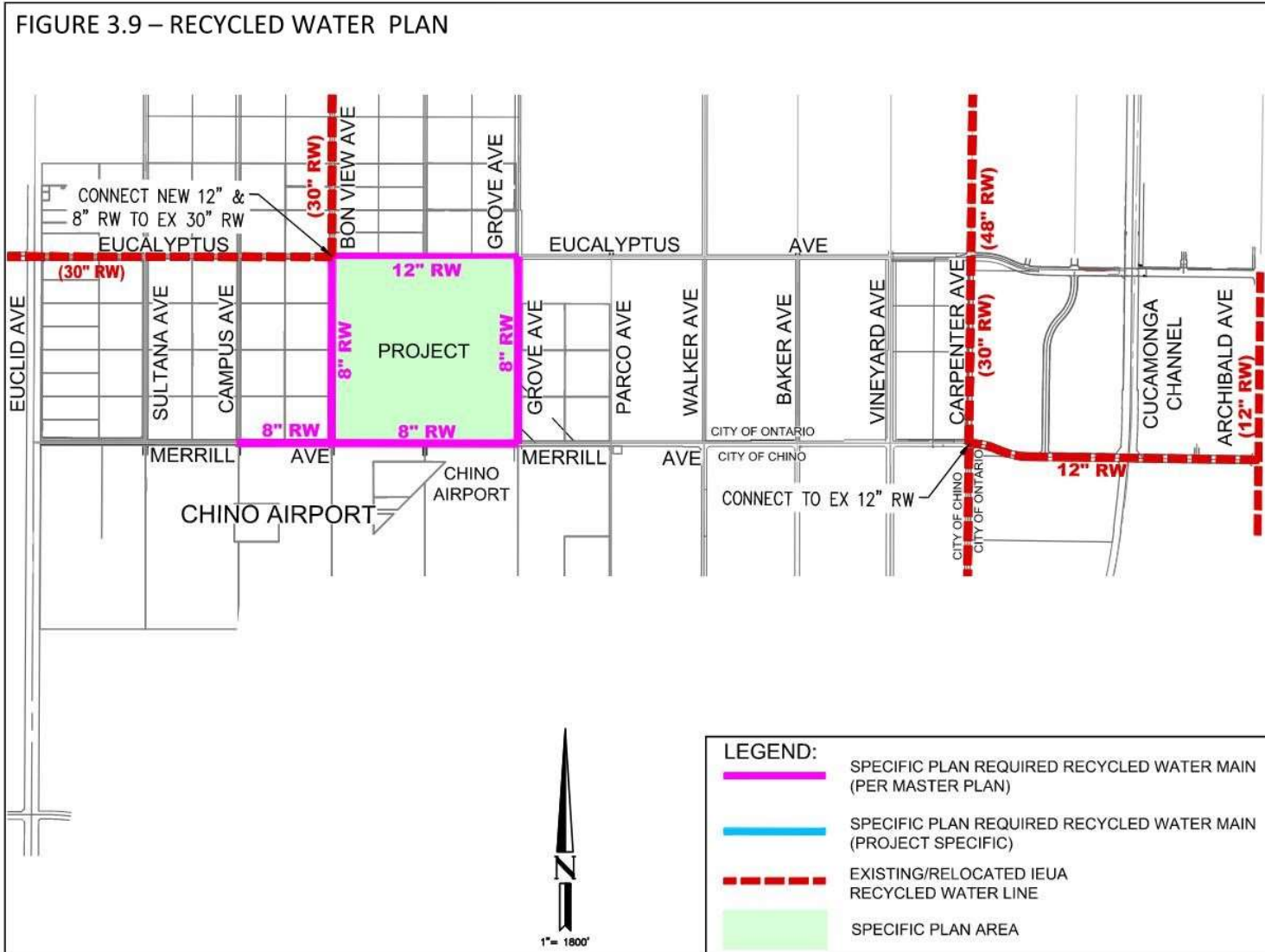
3.4 Recycled Water Plan

The City of Ontario Ordinance 2689 requires all new development in Ontario Ranch to connect to and use recycled water for all approved uses, including but not limited to landscape irrigation. Prior to use of recycled water, approval from the City of Ontario and State Water Resources Control Board (SWRCB) is required. Interim connection to potable water is not allowed.

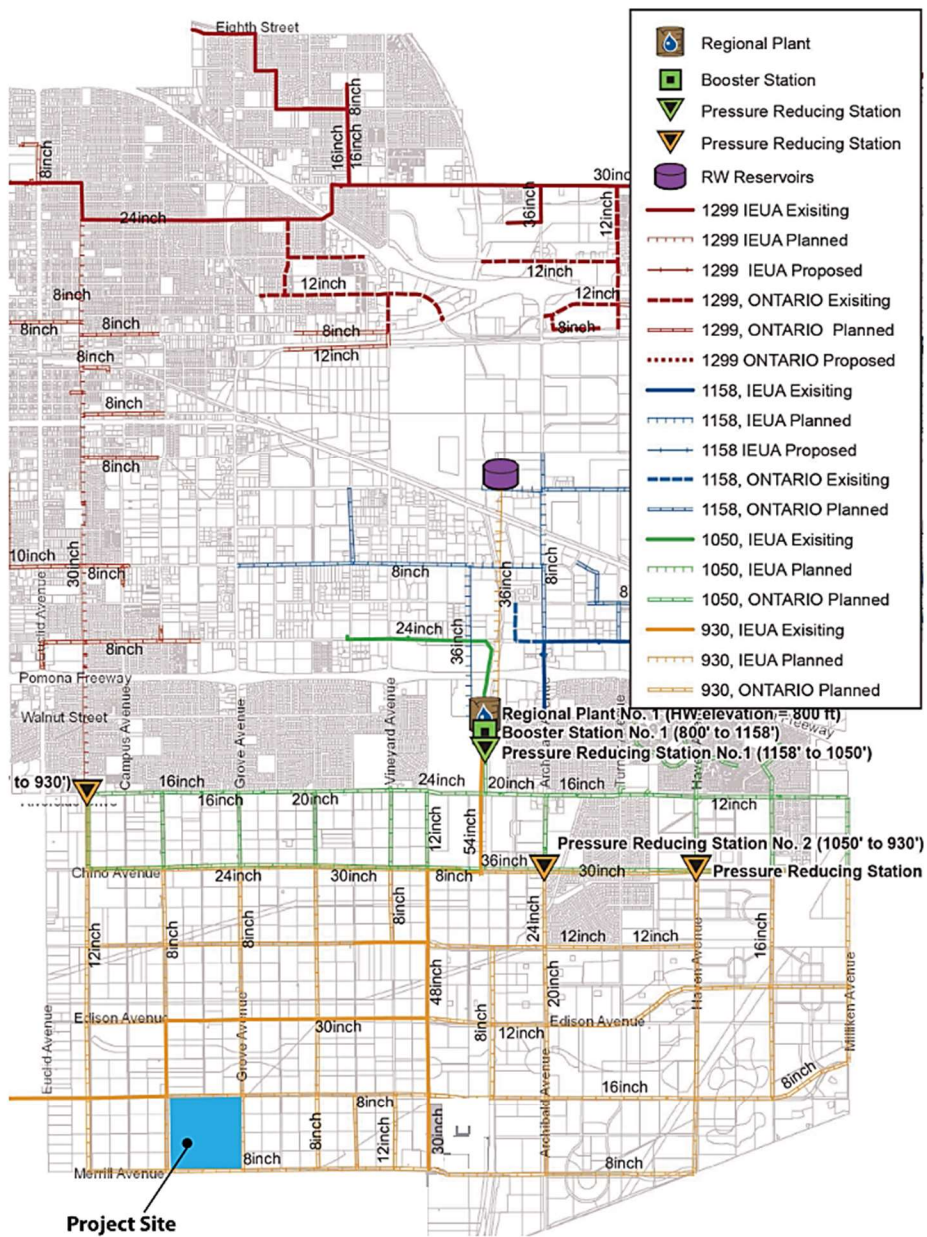
Currently there are no City recycled water mains or City recycled water infrastructure in the vicinity of the Specific Plan Area. Recycled Water is provided to the City of Ontario by the Inland Empire Utility Agency (IEUA) from its four wastewater reclamation plants. The entire Specific Plan area is within the City's master planned 930 Pressure Zone. Recycled Water Infrastructure Improvements for the Specific Plan area (Figure 3.9) require the planning, design, and construction of 930 Pressure Zone (PZ) Recycled Water Master Plan mains, which includes: installing a 12-inch recycled water main in Eucalyptus Avenue connecting to the 8-inch recycled water main in Grove Avenue and extending to connect to the IEUA 30-inch recycled water main in Eucalyptus Avenue at Bon View Avenue; installing an 8-inch recycled water main in Bon-View Avenue to connect to the IEUA 30-inch recycled water main in Eucalyptus Avenue and extending to connect to the 8-inch recycled water main in Merrill Avenue; installing an 8-inch recycled water main in Merrill Avenue connecting to the 8-inch recycled water main in Campus Avenue and extending to connect to the 8-inch recycled water main in Grove Avenue; installing an 8-inch recycled water main in Grove Avenue connecting to the 8-inch recycled water main in Merrill Avenue and extending to connect to the 12-inch recycled water main in Eucalyptus Avenue.

The future recycled water system to serve the City of Ontario is shown on Figure 3.10. The sizing and alignment of existing and proposed recycled water lines are shown in the exhibit.

Sizing and alignment of the recycled water lines will be consistent with the City of Ontario Recycled Water System Master Plan and/or a City approved hydraulic analysis.



City of Ontario Future Recycled Water System



South Ontario Logistics Center Specific Plan

Figure 3.10

3.5 Sewer Plan

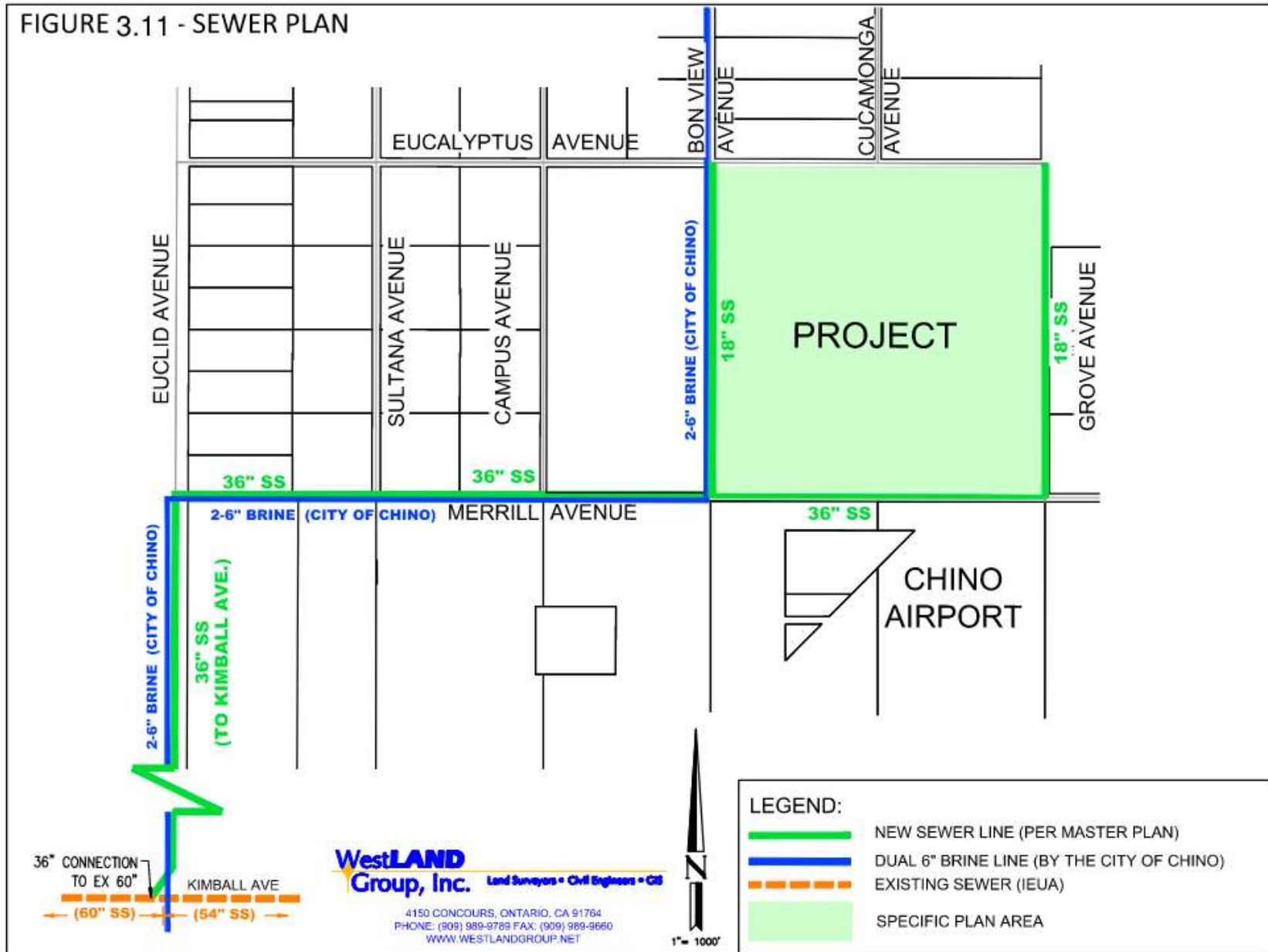
Regional wastewater treatment services are provided to the City of Ontario and its neighboring agencies by the Inland Empire Utilities Agency (IEUA). Several regional trunk sewers collect sewage generated in the city and transport it to IEUA's Regional Plant No.1 and Regional Plant No.5 for treatment. The City of Ontario's sewer service area is divided into eight sewer sheds, primarily based on the outlet points where the City's system ties into the IEUA downstream facility. Ontario Ranch is located in Sewer shed 8.

There are no sewer mains located within the broader vicinity of the Specific Plan area; therefore, the Specific Plan includes a network of new public sewer mains (Figure 3.11), consistent with the City of Ontario's Ultimate Sewer System Plan (Figure 3.12). A 36-inch sewer main will connect to an existing IEUA interceptor trunk main sewer located in Kimball Avenue to the south, run north in Euclid Avenue to Merrill Avenue, then stays a 36-inch sewer main east to Grove Avenue. The IEUA interceptor trunk sewer main is 54-inches east of Euclid and 60-inches west of Euclid Avenue. An 18-inch sewer main would run from Merrill Avenue north within Bon View Avenue and Grove Avenue to Eucalyptus Avenue.

The ultimate sizing and alignment of sewer infrastructure shall be consistent with the City of Ontario ultimate sewer system plan and/or a City conducted and approved hydraulic analysis.

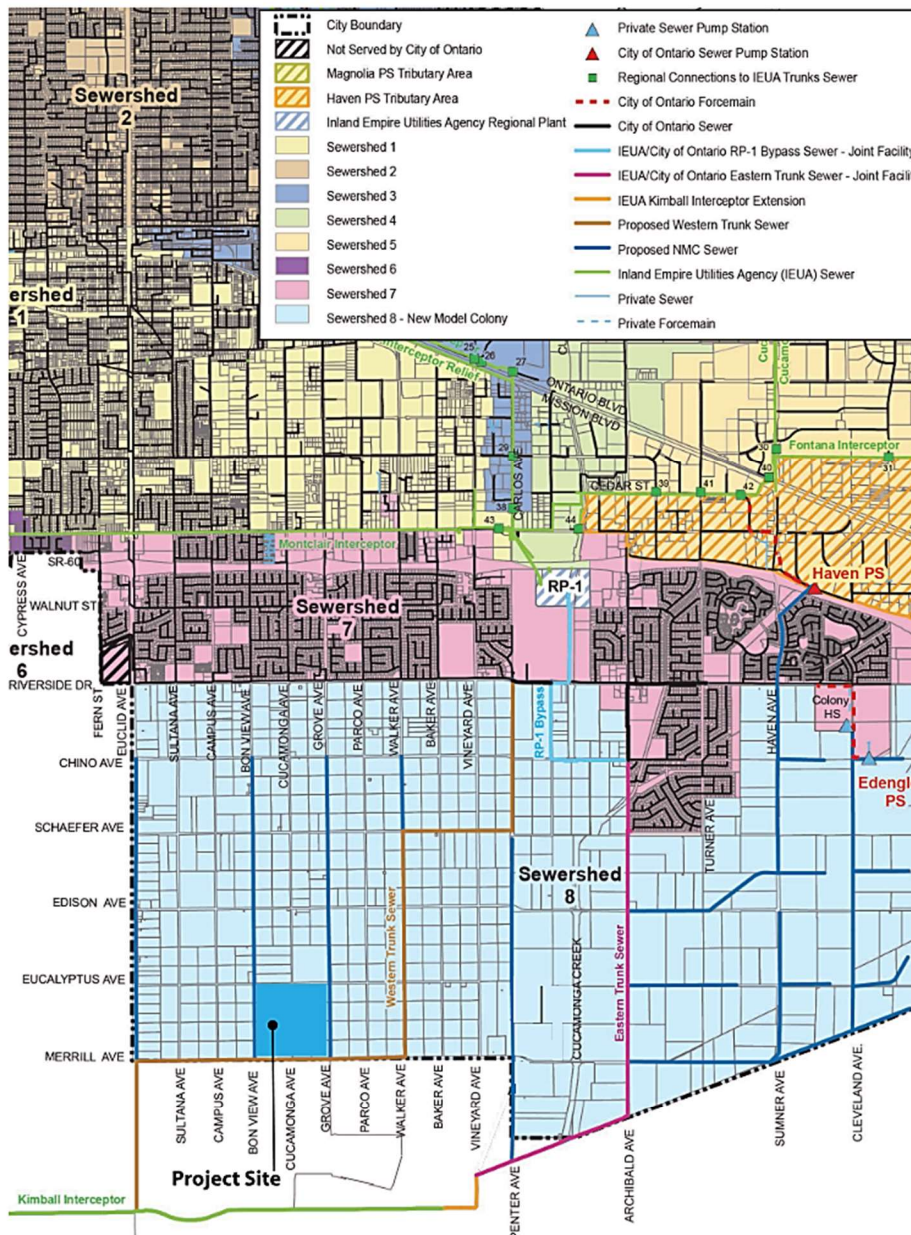
A Sewer Sub-Area Master Plan (SSAMP) shall be prepared for each tract map and development within the Specific Plan.

FIGURE 3.11 - SEWER PLAN



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City of Ontario Ultimate Sewer System



South Ontario Logistics Center Specific Plan

Figure 3.12

3.6 Conceptual Grading Plan

The topography of the site is moderately flat, sloping from the northeast to the southwest with approximately a 17-foot drop in elevation across the Specific Plan area.

The grading activities for the Specific Plan area will generally consist of clearing and grubbing, demolition of existing structures, and moving surface soils to construct building pads, driveways, and streets.

Conceptual grading and earthwork analysis indicate the project can be balanced onsite. Earthwork will include approximately 377,184 cubic yards (CY) of cut and 377,184 CY of fill with 525,500 CY of over-excavation. Geotechnical and/or environmental conditions encountered during grading operations may impact final earthwork calculations. Grading plans for each development project within the Specific Plan area shall be reviewed and approved by the City of Ontario prior to the issuance of grading permits. Grading plans and activities shall conform to the City's grading ordinance and dust and erosion control requirements.

All landscaped areas within the Specific Plan area shall be graded as shallow swales and designed to accept runoff water from impervious surfaces. Water quality retention basins, trenches, etc., (the exact location of which will be determined at the time of Water Quality Management Plan (WQMP) approval for individual implementing projects) will have a maximum side slope of 3:1.

3.7 Dry Utilities Plan

Utility services provided to the site shall be installed underground in accordance with City of Ontario guidelines.

Communication System

According to the City of Ontario 2013 Fiber Optic Master Plan, the fiber optic infrastructure, including approximately 23 miles of backbone fiber south of Riverside Drive, is an investment in a long-term capital asset using newly constructed and existing conduit to provide high speed communication links to key locations throughout the city. Developments within Ontario Ranch are required to install and provide fiber conduit to all improved lots. Proposed on-site facilities will be placed underground within a duct and structure system that will be installed by the developer. Pursuant to the City's Fiber Optic Master Plan, the fiber optic network will be owned and operated by the City and as such maintenance of the installed system will be the responsibility of the City and/or Special District fiber optic entity and not the developer. The Specific Plan area will be connected to the City's system as shown on Figure 3.13.

Natural Gas

Southern California Gas Company maintains 3" and 12" mains in Eucalyptus Avenue, a 12" main Grove Avenue and 2" mains located east and west of the site in Merrill Avenue. These mains are available to provide service to the project site. Gas mains will be installed to the individual development projects by the Gas Company, as necessary.

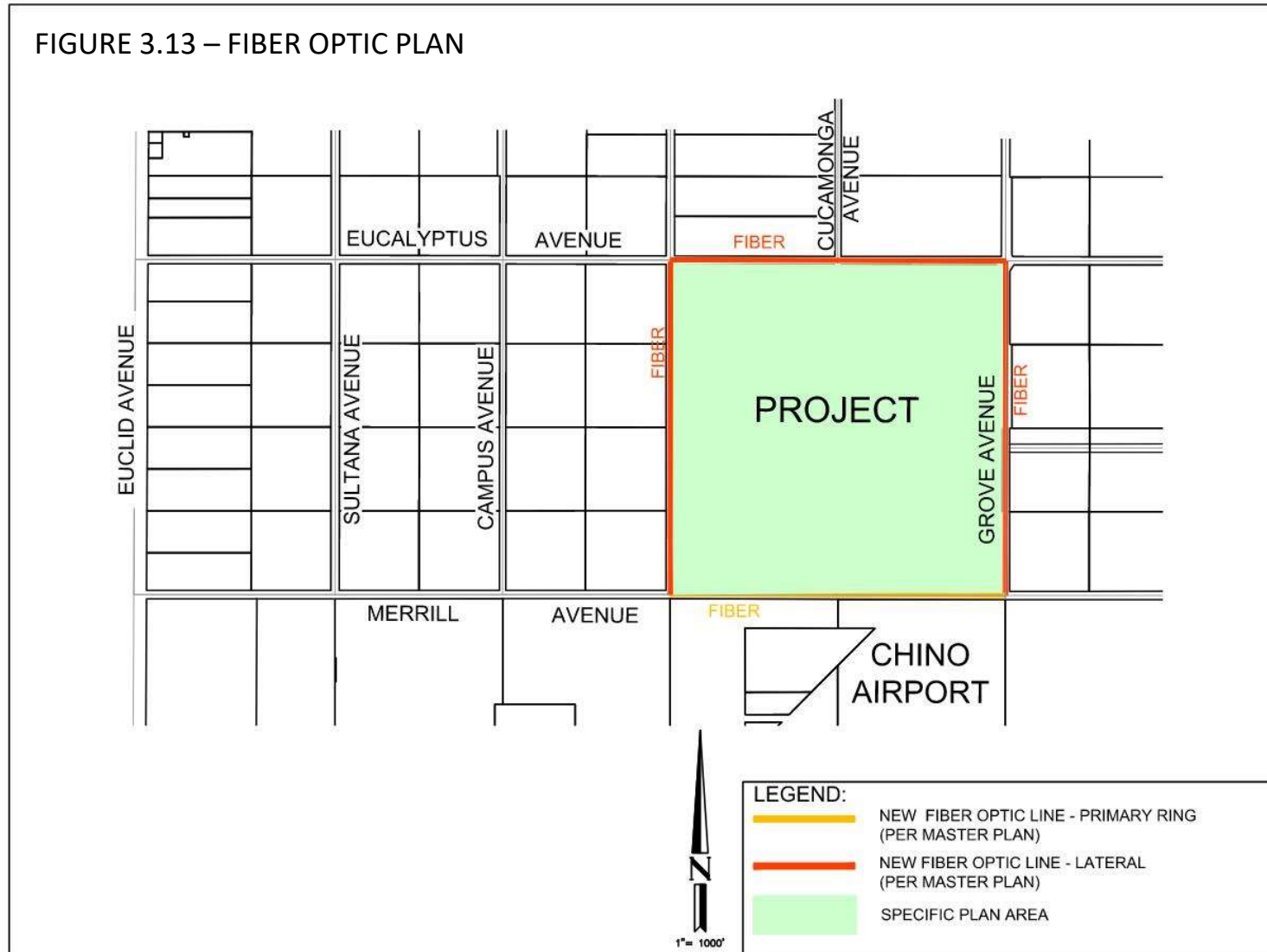
Electrical

Southern California Edison will provide electricity to the Specific Plan area from existing facilities in the vicinity. New lines within the Specific Plan area shall be installed according to City of Ontario requirements.

Cable

Charter does not maintain any facilities with the vicinity of the proposed project site. A main extension would be required if services are desired.

FIGURE 3.13 – FIBER OPTIC PLAN



3.8 Storm Drainage Plan

The Specific Plan area storm drain improvements (Figure 3.14) are consistent with the facilities specified in Drainage Area XIV of the City of Ontario Planned Drainage Facilities (Figure 3.15).

Catch basins located throughout the site will collect runoff. On-site storm drain systems will convey runoff to the following facilities:

- 120-inch storm drain in Grove Avenue leading south to existing facilities via modification of an existing channel.
- 102-inch reinforced concrete pipe in Bon View Avenue.
- 72-inch reinforced concrete pipe in Merrill Avenue adjacent to the Specific Plan area, which will transition to a 120-inch reinforced concrete pipe running west to a 9.5' x 9.5' reinforced concrete box, which will transition into a double 10' x 10' reinforced concrete box at the Merrill Avenue and Euclid Avenue intersection.
- The double 10'x10' reinforced concrete box will run south along Euclid Avenue approximately 600 feet and then transition to two 24-inch HDPE pipe bleeder lines. Additional on-site drain improvements would include storm water detention/ retention/ water quality basins, which would capture, treat, and/or gradually release storm water into the downstream public storm drain systems. Onsite storm water treatment would incorporate underground chambers installed within each building's parking area.

Development within this Specific Plan shall be responsible to construct the ultimate storm drain improvements if they are not installed at time of development.

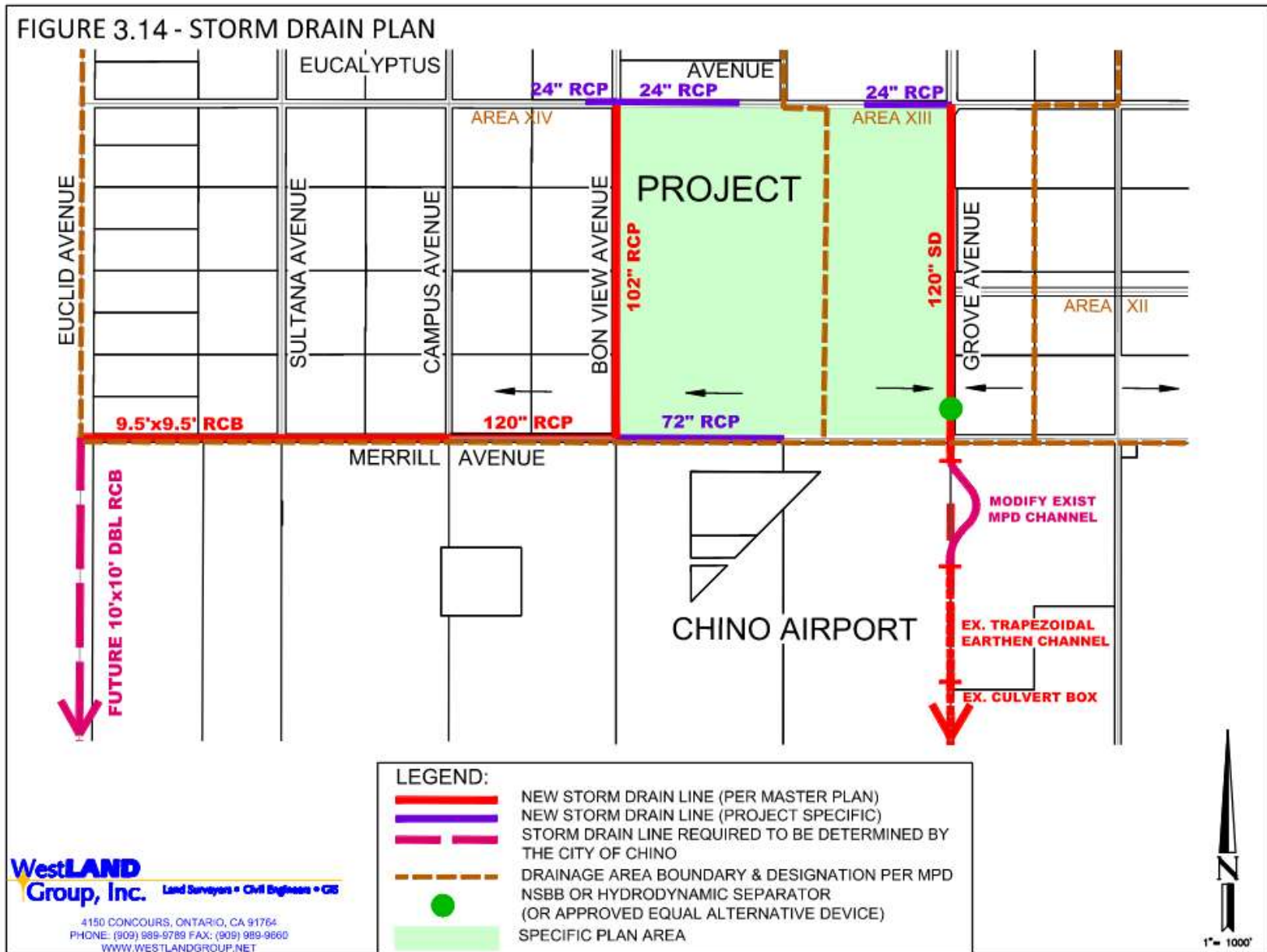
Each storm drain in, Bon View Avenue, Grove Avenue and Merrill Avenue will be equipped with a hydrodynamic separator, or series of hydrodynamic separators to satisfy the statewide trash mandate. Each device will be approved by and listed on the Certified Full Capture System List of Trash Treatment Control Devices of the State Water Resources Control Board (SWRCB).

NPDES Compliance

The grading and drainage of the Specific Plan area will be designed to retain/infiltrate, harvest, and re-use or biotreat surface runoff to comply with the current requirements of the San Bernardino County NPDES Stormwater Program's Water Quality Management Plan (WQMP) for significant new development projects. The objective of the WQMP for this project is to minimize the detrimental effects of urbanization on the beneficial uses of receiving waters, including effects caused by increased pollutants and changes in hydrology. These effects may be minimized through the implementation of site designs that reduce runoff and pollutant transport by minimizing impervious surfaces and maximizing on-site infiltration, employing Source Control Best Management Practices (BMP's), or using on-site structural Treatment Control BMP's where the infeasibility of installing Low Impact Development BMP's is demonstrated.

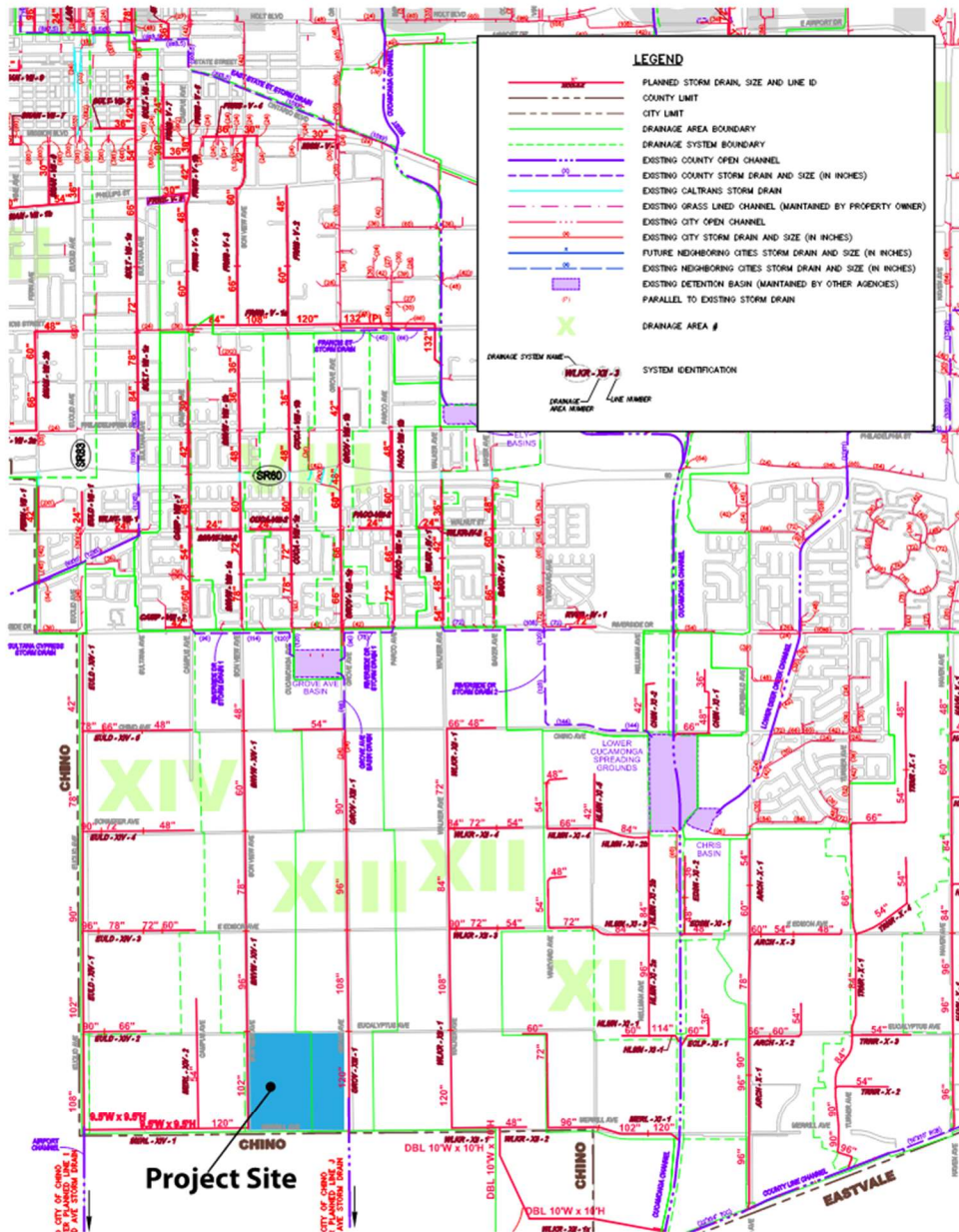
All Priority Land Use (PLU) areas within the Specific Plan Area shall comply with the statewide Trash Provisions adopted by State Water Resources Control Board (SWRCB) and trash requirements in the most current San Bernardino County Area-Wide MS4 Permit.

FIGURE 3.14 - STORM DRAIN PLAN



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City of Ontario Planned Drainage Facilities



South Ontario Logistics Center Specific Plan

Figure 3.15

New development within the Specific Plan area will utilize a variety of Low Impact Development site drainage designs to manage stormwater, including but not limited to retention/infiltration basins, trenches and swales, and above ground and/or below ground bio-treatment systems. Development projects within the Specific Plan area will comply with the latest Low Impact Development guidelines and incorporate features including but not limited to:

- Landscape designs that promote water retention and incorporation of water conservation elements such as use of native plants and drip irrigation systems;
- Permeable surface designs in areas with low traffic;
- Parking lots that drain to landscaped areas to provide retention and infiltration, or bio-treatment where infiltration is infeasible;
- Limited soil compaction during grading operations within landscaped storm water infiltration areas of not more than 80 percent compaction.

Prior to the issuance of a grading or construction permit, a Storm Water Pollution Prevention Plan (SWPPP), Erosion & Sediment Control Plan sheets, and a WQMP shall be prepared and approved. The SWPPP and Erosion & Sediment Control Plan Sheets shall identify and detail all appropriate BMP's to be implemented or installed during construction of the project, and the WQMP shall describe all post-construction BMP's designed to address water quality and quantity of runoff for the life of the project.

3.9 Public Services

This sub-section addresses public services within the Specific Plan area, including police, fire and solid waste disposal services.

3.9.1 Police

The City of Ontario will provide police services to the Specific Plan area. The closest police station is located approximately five miles north of the Specific Plan area at 2500 S. Archibald Avenue, just south of SR-60. This station is also the City of Ontario Police Department headquarters.

3.9.2 Fire

The City of Ontario will provide fire protection services to the Specific Plan area. The Ontario Fire Department currently has eight stations, which are staffed with eight four-person paramedic engine companies and two four-person truck companies. The closest operational fire station, Station 2, is located at 544 W. Francis Street, approximately four miles north of the Specific Plan area.

3.9.3 Solid Waste Disposal

The City of Ontario will provide solid waste services to the Specific Plan area. The City offers a full array of commercial and industrial services designed to meet the business community's needs. Solid waste facilities follow the "Solid Waste Department Refuse and Recycling Planning Manual." The Manual establishes the City of Ontario's requirements for refuse and recycling storage and access for service, as well as addresses the City's recycling goals. The Mid-Valley Landfill is the nearest County of San Bernardino landfill located at 2390 N. Alder Avenue in the City of Rialto, approximately 20 miles northeast of the Specific Plan area.

3.10 Phasing Plan

Development phasing of the Specific Plan area will be determined by the landowner and/or developer based upon real estate market conditions. Phasing sequencing is subject to change over time to respond to various market and local factors and as such, individual phases may overlap or develop concurrently. Phasing will occur as appropriate levels of infrastructure are provided. Infrastructure improvements, as required and approved by the City Engineer to support the development, shall be installed by the developer.

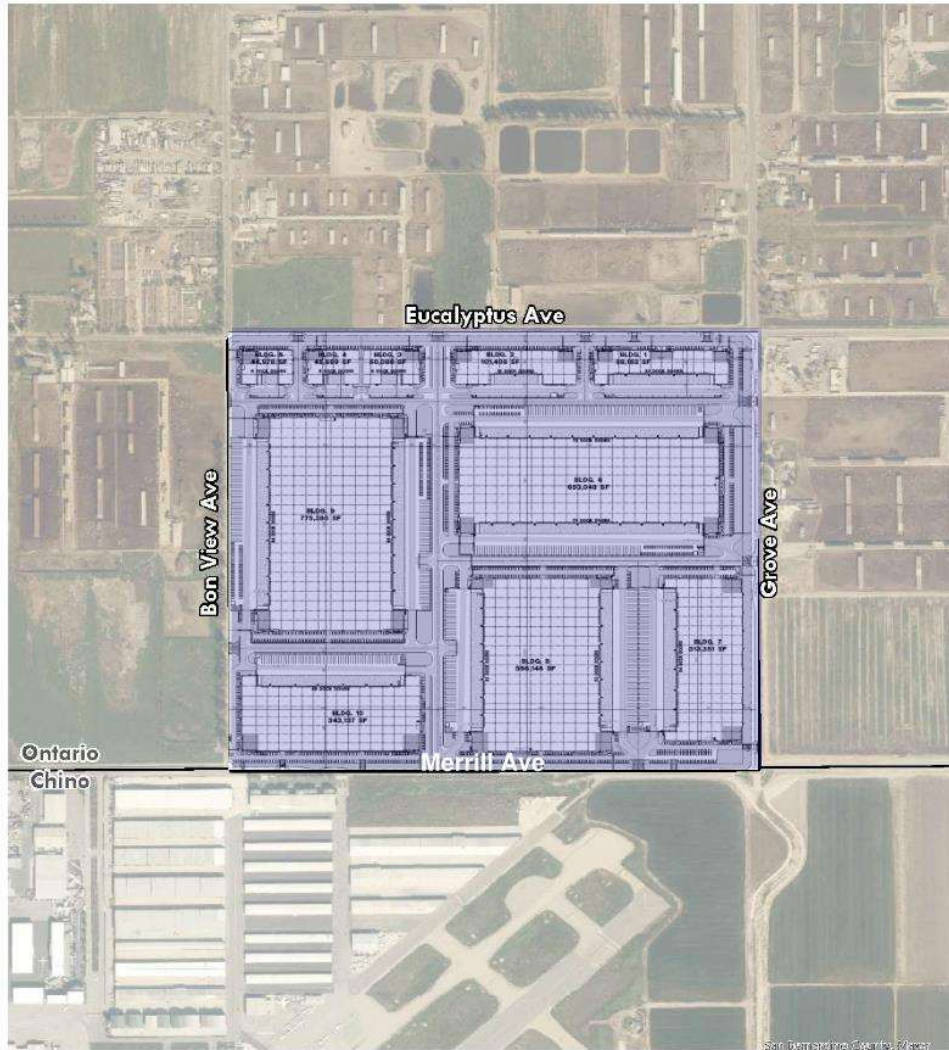
Figure 3.16, Conceptual Phasing Plan, describes one general phase of development for South Ontario Logistics Center.

Phase 1: Phase 1 consists of the construction of Buildings 1 through 10 as numbered in the Conceptual Site Plan and includes the Development Plan (PAs 1 and 2). This phase may be developed in several sub-phases in response to market demands and according to the logical and orderly completion of infrastructure improvements.

Specific Plan backbone infrastructure shall be installed by the project developer, in accordance with the applicable City-adopted infrastructure plan for the area, as well as the provisions of this Specific Plan and an approved Development Agreement. Final grading and infrastructure improvements will be completed in accordance with the approved Development Agreement and City Engineer approval. Fair share responsibilities for improvements shall be addressed in a Development Agreement with the City of Ontario.

All the Specific Plan required infrastructure can be found in Section 3.3 (Figure 3.7 and 3.7a) for Potable Water, Section 3.4 (Figure 3.9) for Recycled Water, Section 3.5 (Figure 3.11) for the Sewer Plan, Section 3.7 (Figure 3.13) for the Fiber Optic Plan, and Section 3.8 (Figure 3.14) for the Storm Drain Plan. Phasing will be determined per separate Development Agreement.

Conceptual Phasing Plan



 Phase 1



South Ontario Logistics Center Specific Plan

Figure 3.16