

# Development Plan

## Section 2

## DEVELOPMENT PLAN

### A. VISION

The greatest value offered by MEREDITH INTERNATIONAL CENTRE is its location. With views of the San Gabriel Mountains in the distance, the property is located immediately north of Interstate 10, approximately ½-mile north of Ontario International Airport, and 2.75 miles west of Interstate 15. When creating a guiding vision for MEREDITH INTERNATIONAL CENTRE, the property's setting and proximity to major transportation corridors and facilities were given the highest consideration.

MEREDITH INTERNATIONAL CENTRE establishes a mixture of employment and residential land uses arranged in a sensible and efficient manner that allow ease of access and complement the surrounding community. To that end, MEREDITH INTERNATIONAL CENTRE is designed as a contemporary, horizontally mixed-use center containing industrial, urban commercial, and urban residential land uses that take advantage of the property's location near regional transportation corridors.

MEREDITH INTERNATIONAL CENTER is a place where businesses can locate and thrive and where multi-family housing opportunities are within walking distance to shopping, goods and services, employment opportunities, and transit. The mixture of uses and their orientation on the property are designed for the benefit of the residents, visitors, and employees of MEREDITH INTERNATIONAL CENTRE, the surrounding community, and the region.



## B. LAND USE PLAN

This Specific Plan specifies three land use districts: Industrial, Urban Commercial, Mixed-Use, and Urban Residential. For planning purposes, the 257.7-acre MEREDITH INTERNATIONAL CENTRE Specific Plan property is divided into six planning areas.

- ❖ Planning Area 1 Industrial
- ❖ Planning Area 1A Industrial
- ❖ Planning Area 2 Urban Commercial
- ❖ Planning Area 2A Mixed-Use Overlay
- ❖ Planning Area 3 Urban Commercial
- ❖ Planning Area 4 Urban Residential
- ❖ Planning Area 5 Urban Commercial

Figure 2-1, Land Use Plan, depicts the physical arrangement of land uses on the Specific Plan property. Table 2-1, Land Use Plan Statistical Summary, lists each planning area and their respective land use district, acreage, and development intensity. Buildout intensity of this Specific Plan is shown in the table below.

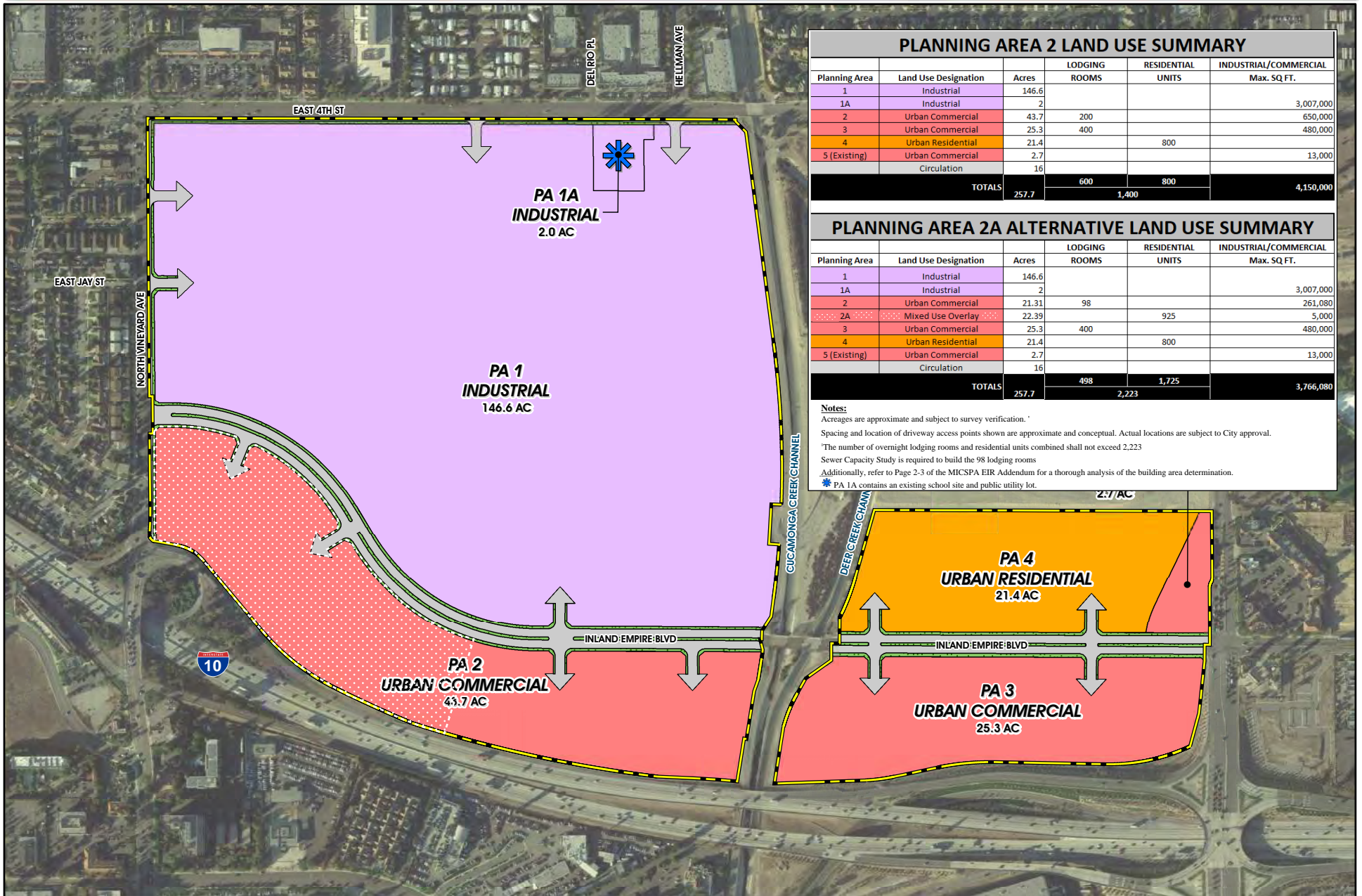
Use Category	Development Intensity	Planning Area(s)
Industrial	3,007,000 square feet	1 and 1A
Urban Commercial	1,143,000 square feet	2, 3, and 5
Mixed Use Overlay	925 units/ 5,000 sf	2A
Urban Residential	800 units	4

Refer to Figure 2-1, *Land Use Plan*, and Table 2-1, *Land Use Plan Statistical Summary*, for more detailed information.

Additionally, refer to Page 2-3 of the *MICSPA EIR Addendum* for a thorough analysis of the building area determination

Planning Area boundaries were determined by natural and man-made physical divisions formed by drainage channels, roads, and existing development. The Cucamonga Creek Channel and Deer Creek Channel (not a part of the Specific Plan area) divide the Specific Plan property in a north-south direction. The channels create a physical separation between the Industrial land uses in Planning Area 1 and the Urban Residential land uses in Planning Area 4. The channels also divide the two largest Urban Commercial and Mixed-Use Planning Areas (Planning Area 2/2A and Planning Area 3). In a similar manner, the alignment of Inland Empire Boulevard separates the Industrial land uses in Planning Area 1 from the Urban Commercial and Mixed-use land uses in Planning Area 2/2A and separates Planning Area 4 and Planning Area 5 from Planning Area 3.

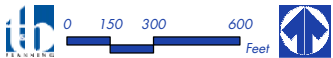
The boundaries of Planning Area 1A, and Planning Area 5 were determined by uses that were present in those areas at the time Specific Plan Amendment No. 1 was prepared (2015). Planning Area 1A (2.0 acres) was developed with an elementary school and public water utility use and Planning Area 5 (2.7 acres) was developed as a small retail commercial shopping center. The Planning Area 2A Mixed-Use Overlay 22.39 acres was determined by the remainder of the Planning Area 2 Urban Commercial.



Planning Area	Land Use Designation	Acres	LOGGING	RESIDENTIAL	INDUSTRIAL/COMMERCIAL
			ROOMS	UNITS	Max. SQ. FT.
1	Industrial	146.6			
1A	Industrial	2			3,007,000
2	Urban Commercial	43.7	200		650,000
3	Urban Commercial	25.3	400		480,000
4	Urban Residential	21.4		800	
5 (Existing)	Urban Commercial	2.7			13,000
	Circulation	16			
<b>TOTALS</b>		<b>257.7</b>	<b>600</b>	<b>800</b>	<b>4,150,000</b>

Planning Area	Land Use Designation	Acres	LOGGING	RESIDENTIAL	INDUSTRIAL/COMMERCIAL
			ROOMS	UNITS	Max. SQ. FT.
1	Industrial	146.6			
1A	Industrial	2			3,007,000
2	Urban Commercial	21.31	98		261,080
2A	Mixed Use Overlay	22.39		925	5,000
3	Urban Commercial	25.3	400		480,000
4	Urban Residential	21.4		800	
5 (Existing)	Urban Commercial	2.7			13,000
	Circulation	16			
<b>TOTALS</b>		<b>257.7</b>	<b>498</b>	<b>1,725</b>	<b>3,766,080</b>

**Notes:**  
 Acreages are approximate and subject to survey verification.  
 Spacing and location of driveway access points shown are approximate and conceptual. Actual locations are subject to City approval.  
 \*The number of overnight lodging rooms and residential units combined shall not exceed 2,223  
 Sewer Capacity Study is required to build the 98 lodging rooms  
 Additionally, refer to Page 2-3 of the MICSPE EIR Addendum for a thorough analysis of the building area determination.  
 \* PA 1A contains an existing school site and public utility lot.



*Table 2-1 Land Use Plan Statistical Summary*

Planning Area	Land Use	Acreage	Development Intensity		
			Units		Non-Residential Square Footage
			Overnight Lodging Room	Multi-Family Residential	
1	Industrial	146.6	--	--	3,007,000
1A	Industrial	2.0	--	--	
2	Urban Commercial	43.7	200 <sup>B</sup>	--	650,000
3	Urban Commercial	25.3	400 <sup>B</sup>	--	480,000
4	Urban Residential	21.4	--	800	--
5	Urban Commercial	2.7	--	--	13,000 <sup>A</sup>
--	Circulation	16.0	--	--	--
<b>TOTALS</b>		<b>257.7</b>	<b>600<sup>B</sup></b>	<b>800</b>	<b>4,150,000</b>
			<b>1,400<sup>C</sup></b>		

A - Planning Area 5 was fully constructed at the time Specific Plan Amendment No. 1 was prepared. The building square footage of 13,000 SF is approximate.

B - Overnight lodging rooms are included in the non-residential square footage.

C - Overnight lodging rooms may be swapped for additional multi-family residences in Planning Area 4 at a ratio of 1:1. Alternatively, multi-family residences may be swapped for additional overnight lodging rooms in Planning Areas 2 and/or 3 at a ratio of 1:1. The number of overnight lodging rooms and residential units combined shall not exceed 1,400.

*Table 2-2 Alternative Land Use Plan Statistical Summary*

Planning Area	Land Use	Acreage	Development Intensity		
			Units		Non-Residential Square Footage
			Overnight Lodging Room	Multi-Family Residential	
1	Industrial	146.6			3,007,000
1A	Industrial	2			
2	Urban Commercial	21.31	98 (B)(D)(E)		261,080
2A	Mixed-Use Overlay	22.39		925	5,000 (D)
3	Urban Commercial	25.3	400 (B)		480,000
4	Urban Residential	21.4		800	
5	Urban Commercial	2.7			13000 (A)
	Circulation	16			
<b>TOTALS</b>		257.7	498 (B)	1,725	3,766,080
			2223 (C) (E)		

A - Planning Area 5 was fully constructed at the time Specific Plan Amendment No. 1 was prepared. The building square footage of 13,000 SF is approximate.

B - Overnight lodging rooms are included in the non-residential square footage.

C - Overnight lodging rooms may be swapped for additional multi-family residences in Planning Area 4 at a ratio of 1:1. Alternatively, multi-family residences may be swapped for additional overnight lodging rooms in Planning Areas 2 and/or 3 at a ratio of 1:1. The number of overnight lodging rooms and residential units combined shall not exceed 2,223

D - Additionally, refer to Page 2-3 of the MICSPA EIR Addendum for a thorough analysis of the building area determination

E - Sewer Capacity Study is required to build the 98 lodging rooms.

## B.1. Planning Area 1 - Industrial

VISION



STATISTICS

LAND USE CATEGORY: Industrial

ACREAGE: 146.6 acres

BUILDING INTENSITY: 3,007,000 s.f.

Encompassing 146.6 acres in the northwestern quadrant of the Specific Plan area, Planning Area 1 is designated for Industrial land use and is the largest of the planning areas. As shown on Figure 2-2, *Planning Area 1*, this planning area is bordered on the north by East 4th Street, on the south by Inland Empire Boulevard, on the east by the Cucamonga Creek Channel, and on the west by North Vineyard Avenue.

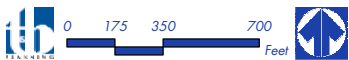
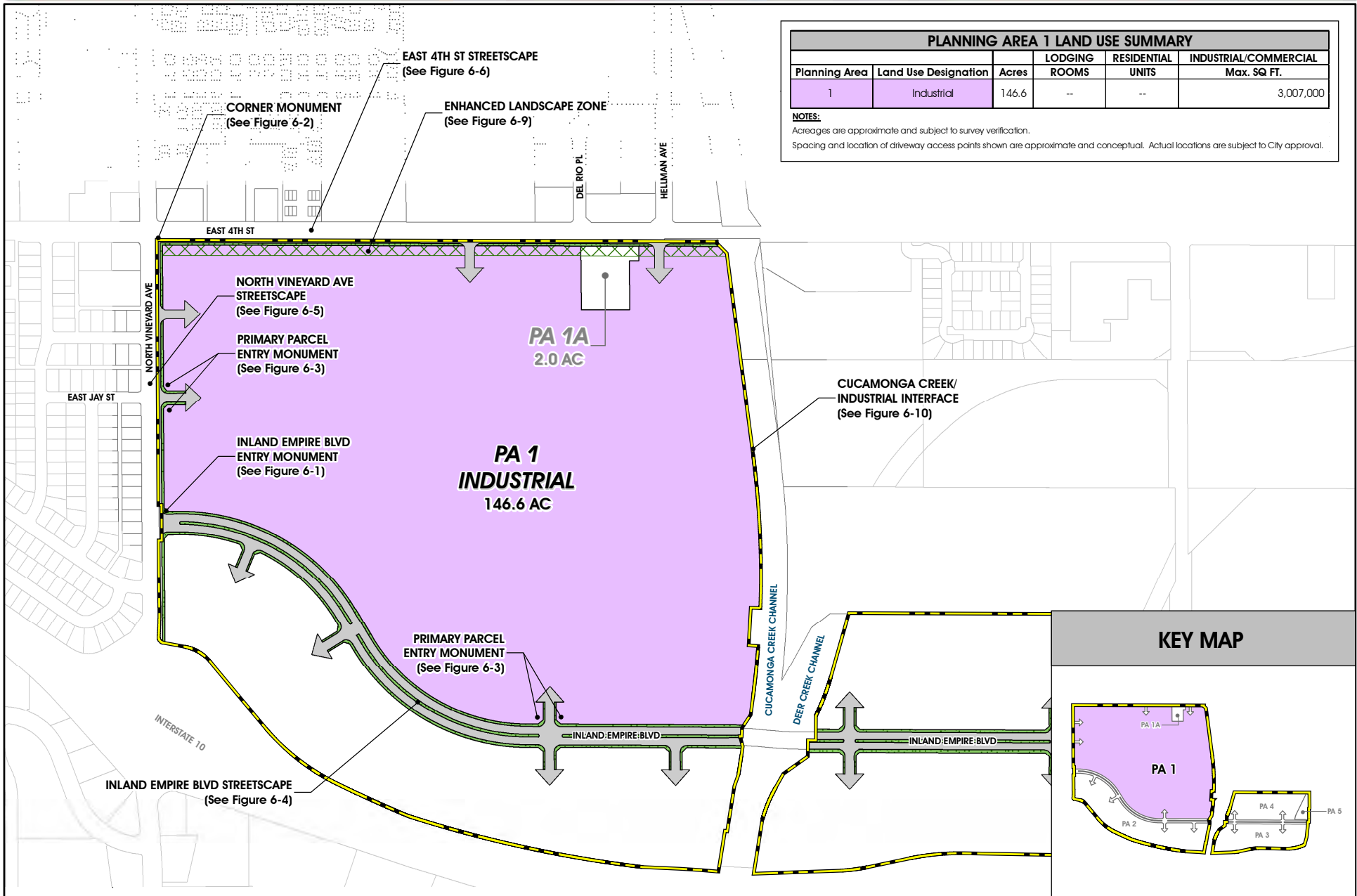
Planning Area 1 is designed to accommodate buildings of various sizes that house users such as general light industrial, manufacturing, and warehouse/distribution operations. These types of uses typically attract both passenger car and truck traffic that benefit from a short, direct route to the regional transportation (freeway) system. To facilitate vehicular access to the Interstate 10/North Vineyard Avenue on- and off-ramps, two local industrial streets, Del Rio Place and Jay Street, are provided within Planning Area 1. Del Rio Place and Jay Street traverse through Planning Area 1 and provide an interior connection between North Vineyard Avenue and Inland Empire Boulevard. Del Rio Place, a north-south oriented

street, provides access from the south via Inland Empire Boulevard. Jay Street, an east-west oriented street, provides access from the west via North Vineyard Avenue. Refer to Section 3, *Circulation Plan*, for more information about circulation to, from, and within Planning Area 1.

North of Planning Area 1, across East 4th Street, is a mixture of commercial and residential land uses. As shown on Figure 2-2, an enhanced landscape zone is provided along the northern boundary of Planning Area 1 as an amenity along East 4th Street. This landscape area provides a transitional area from the buildings in Planning Area 1 to East 4th Street and the land uses north of East 4th Street. The enhanced landscape zone provides a pleasant transition to soften and blend into the adjacent land uses.

The buildings in Planning Area 1 provide attractive building façades facing East 4th Street, with no visible loading bays. Along the western side of Planning Area 1, buildings facing North Vineyard Avenue also offer attractive architectural features with no visible loading bays. The buildings in the western portion of Planning Area 1 are smaller in scale compared to the larger structures elsewhere in Planning Area 1. As discussed in detail in Section 6, *Design Guidelines*, the placement, orientation, design, and architectural features of the building façades visible from surrounding roadways considers and respects the adjacent land uses.

Green spaces that include water quality/detention basins and landscape pockets are deliberately placed in the southern portion of Planning Area 1 along Inland Empire Boulevard. These green spaces serve a practical purpose as part of the development's storm drain system and also assist in providing a spacious and inviting landscape buffer along the roadway.





**B.2. Planning Area 1A - Industrial**

<b>VISION</b>	
<b>STATISTICS</b>	<p>LAND USE CATEGORY: Industrial</p> <p>ACREAGE: 2.0 acres</p> <p>BUILDING INTENSITY: N/A</p>

Planning Area 1A is a 2.0-acre area located south of East 4th Street, which is surrounded on its south, east, and west boundaries by Planning Area 1. Refer to Figure 2-3, *Planning Area 1A*. This area was not a part of the Specific Plan previously approved in 1981. Planning Area 1A was added to MEREDITH INTERNATIONAL CENTRE by the 2015 Specific Plan Amendment. At that time, the property contained the 6,767 s.f. Italo M. Bernt Elementary School (located on a 1.9-acre lot) and an adjacent 0.1-acre lot established by Ontario Municipal Utilities Company for use as a water treatment facility. This Specific Plan allows for the continuation of these uses.

Two build-out options are available to Planning Area 1A. These options are meant to work in collaboration with development of Planning Area 1.

In the event that Planning Area 1A redevelops in conjunction with the development of Planning Area 1 (Option A), no building square footage is allocated specifically to Planning Area 1A. Instead, the maximum building intensity of Planning Area 1 would be as

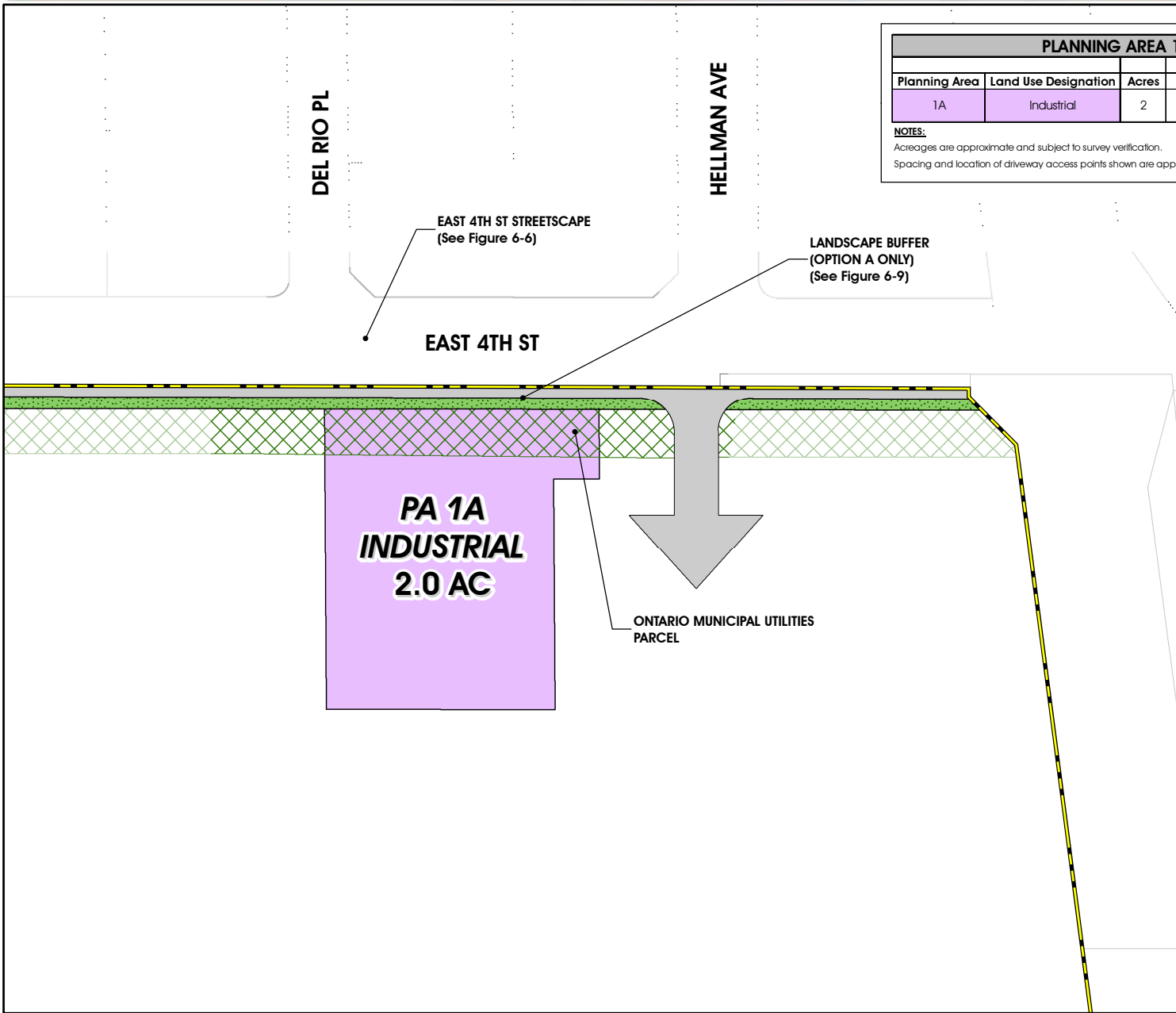
discussed previously in Subsection 2.B.1, *Planning Area 1 - Industrial*. Under Option A, build-out of Planning Area 1 and Planning Area 1A would occur seamlessly as if they were one planning area. With this coordinated planning, an enhanced landscape zone is designed along the northern boundary of Planning Area 1A extending from Planning Area 1 as an amenity along East 4th Street. Multiple buildings would be accommodated in the combined area of Planning Area 1 and Planning Area 1A, designed to house users such as general light industrial, manufacturing, and warehouse/distribution operations.

In the event that Planning Area 1A does not redevelop (remains a school, Option B), the existing 6,767 s.f. elementary school building located in Planning Area 1A at the time the 2015 Specific Plan Amendment was prepared would remain. Under Option B, Planning Area 1A would be buffered from adjacent industrial development in Planning Area 1 by a landscape buffer provided inside the boundary of Planning Area 1. Under Option B, access to Planning Area 1A would be provided via a driveway connecting to East 4th Street. There would be no internal vehicle through-way from Planning Area 1A to the rest of MEREDITH INTERNATIONAL CENTRE.

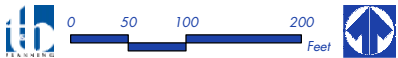
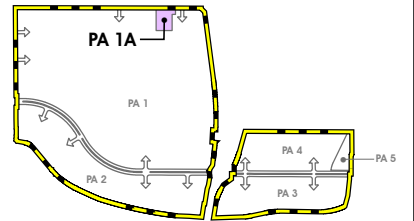
Both Options A and B accommodate the 0.1-acre Ontario Municipal Utilities Company water treatment facility at Planning Area 1A.

PLANNING AREA 1A LAND USE SUMMARY					
Planning Area	Land Use Designation	Acres	LODGING ROOMS	RESIDENTIAL UNITS	INDUSTRIAL/COMMERCIAL Max. SQ. FT.
1A	Industrial	2	--	--	3,007,000

**NOTES:**  
 Acreages are approximate and subject to survey verification.  
 Spacing and location of driveway access points shown are approximate and conceptual. Actual locations are subject to City approval.



**KEY MAP**



**B.3. Planning Area 2/2A - Urban Commercial/ Mixed Use**

VISION  
STATISTICS



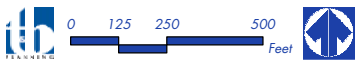
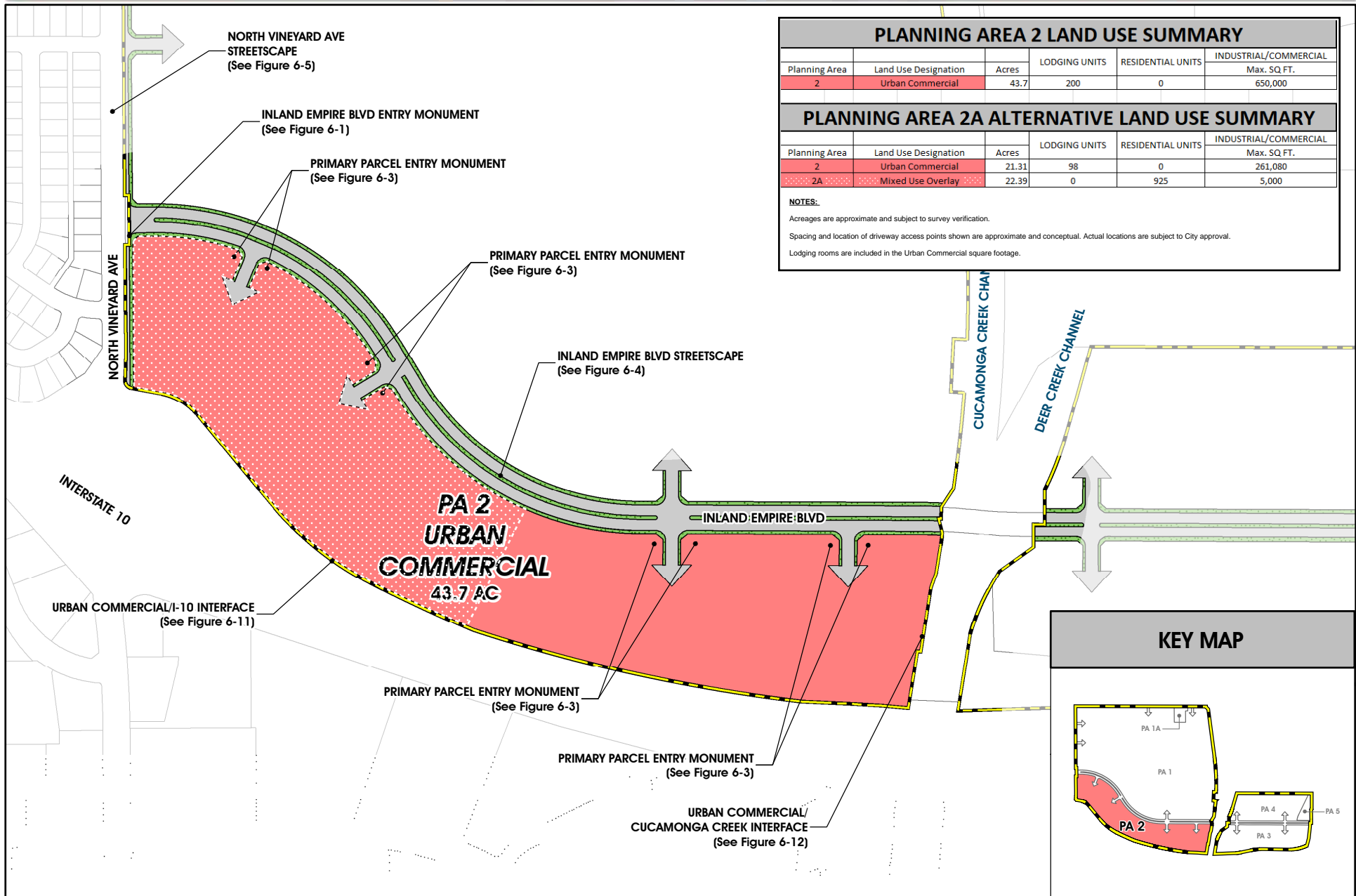
LAND USE CATEGORY: Urban Commercial/Mixed Use  
 ACREAGE: 43.7 acres  
 BUILDING INTENSITY: 650,000 s.f.

Planning Area 2 encompasses 43.7 acres of land located in the southwestern portion of the Specific Plan area. As shown on Figure 2-4, Planning Area 2, it is bordered on the north by Inland Empire Boulevard, on the south by Interstate 10, on the west by North Vineyard Avenue, and on the east by the Cucamonga Creek Channel.

Having a long, linear configuration adjacent to Interstate 10 with good visibility to passing motorists and easy access to the North Vineyard Avenue on- and off-ramps, Planning Area 2 is designed to accommodate Urban Commercial land uses, with a Residential Mixed Use Overlay. In concert with Planning Area 3, the uses in Planning Area 2 are designed to form an attractive visual gateway into MEREDITH INTERNATIONAL CENTRE.

The Urban Commercial and Mixed Use designation of Planning Area 2/2A allows for a range of commercial uses that benefit from the properties adjacency to Interstate 10 and approximately ½-mile distance to Ontario International Airport. Planning Area 2/2A is designed as an area offering a variety of market-driven commercial and residential uses to service nearby residents, visitors to the area, and travelers on Interstate 10. Two hundred (200) overnight lodging rooms are permitted in Planning Area 2. Nine hundred twenty five (925) residential units are permitted in the Planning Area 2 Mixed Use Overlay. 5,000 sf of commercial space is also permitted. The range of permitted land uses achieves this Specific Plans vision and intention to provide a mix of uses that take advantage of proximity to transportation corridors and serve the surrounding community and region.

Refer to Section 5, Development Standards, for a list of permitted uses and additional information. Vehicular access into Planning Area 2 is provided via Inland Empire Boulevard. The 4-way intersection at Del Rio Place and Inland Empire Boulevard provides a primary entry point to Planning Area 2 from Inland Empire Boulevard. Direct access into Planning Area 2 from North Vineyard Avenue is not permitted.



**B.4. Planning Area 3 – Urban Commercial**

**VISION**  
**STATISTICS**



LAND USE CATEGORY: Urban Commercial  
 ACREAGE: 25.3 acres  
 BUILDING INTENSITY: 480,000 s.f.

Planning Area 3 is comprised of 25.3 acres of land located in the southeastern portion of the Specific Plan property. Planning Area 3 is bordered on the north by Inland Empire Boulevard, on the south by Interstate 10, on the west by the Deer Creek Channel, and on the east by North Archibald Avenue. Refer to Figure 2-5, *Planning Area 3*.

Designed to complement the uses of Planning Area 2 also positioned along Interstate 10, Planning Area 3 has good visibility to passing motorists and access to the North Archibald Avenue on- and off-ramps. Planning Area 3 is designed to accommodate Urban Commercial land uses and, in concert with Planning Area 2, form an attractive visual gateway into MEREDITH INTERNATIONAL CENTRE.

Like Planning Area 2, the Urban Commercial designation of Planning Area 3 allows for a range of commercial uses that benefit from adjacency to Interstate 10 and approximately ½-mile distance to the Ontario International Airport. Located closer to MEREDITH INTERNATIONAL CENTRE’s Urban Residential area (Planning Area 4), and potential alignment of the Gold Line LRT corridor, Planning

Area 3 is afforded a greater opportunity than Planning Area 2 to offer smaller, pedestrian-oriented retail establishments. Four hundred (400) overnight lodging rooms are permitted in Planning Area 3 with the intention of serving the surrounding community and region, such as visitors to the nearby Ontario Convention Center and Ontario International Airport. Refer to Section 5, *Development Standards*, for a list of permitted uses and additional information.

Vehicular access into Planning Area 3 is provided via Inland Empire Boulevard. No direct driveway access into the planning area is permitted from North Archibald Avenue.

PLANNING AREA 3 LAND USE SUMMARY					
Planning Area	Land Use Designation	Acres	LODGING ROOMS	RESIDENTIAL UNITS	INDUSTRIAL/COMMERCIAL Max. SQ. FT.
3	Urban Commercial	25.3	400	--	480,000

**NOTES:**  
 Acreages are approximate and subject to survey verification.  
 Spacing and location of driveway access points shown are approximate and conceptual. Actual locations are subject to City approval.  
 Lodging rooms are included in the Urban Commercial square footage.

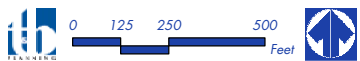
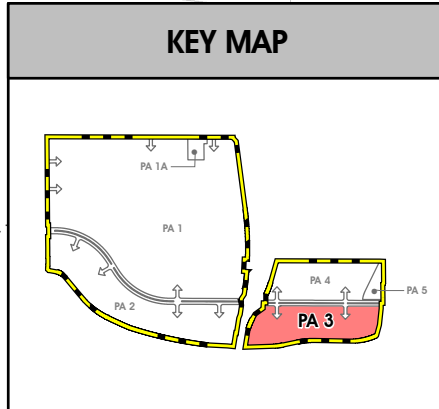
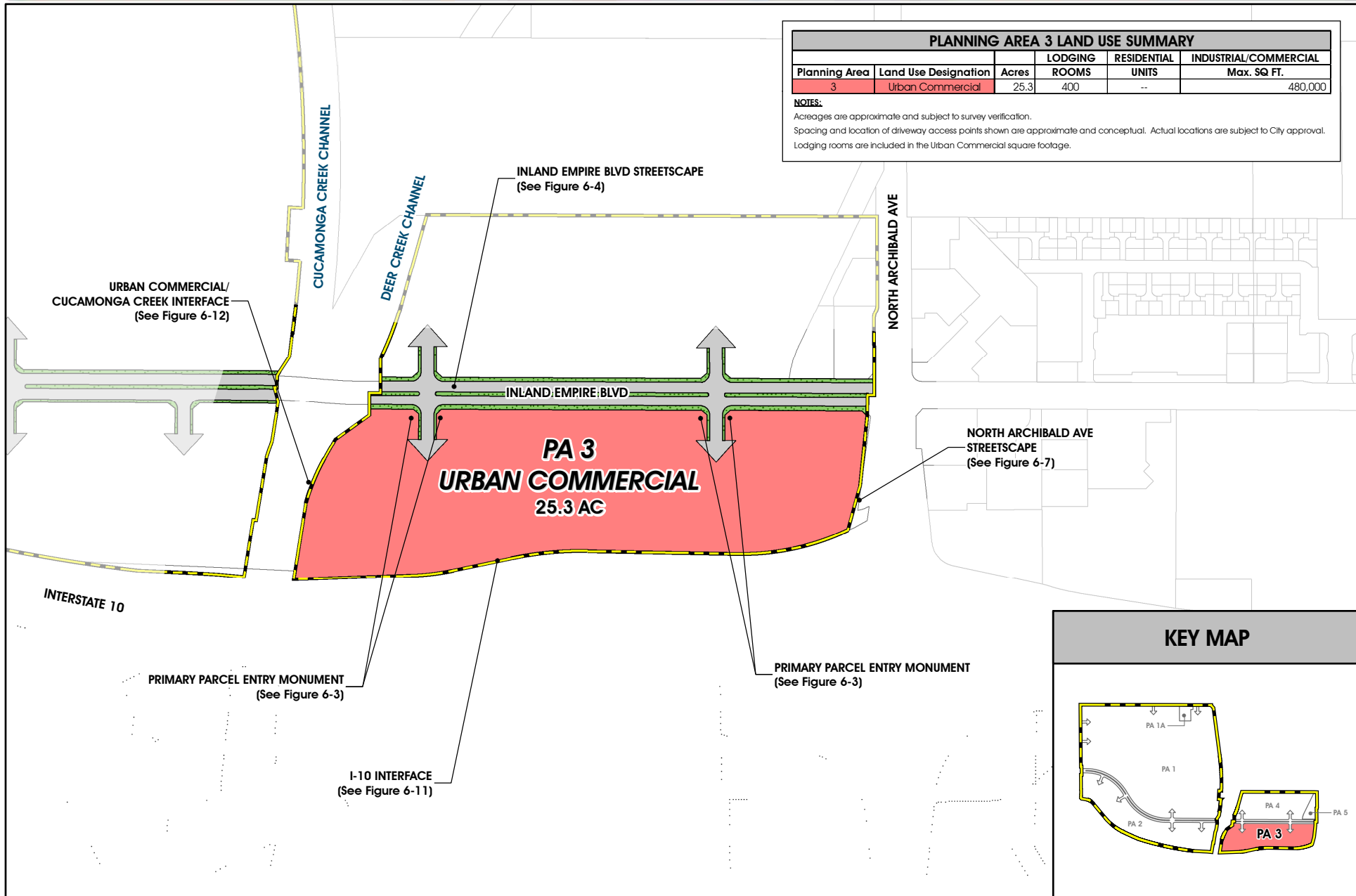


FIGURE 2-5

PLANNING AREA 3

**B.5. Planning Area 4 – Urban Residential**

<b>VISION</b>	
<b>STATISTICS</b>	<p>LAND USE CATEGORY: Urban Residential</p> <p>ACREAGE: 21.4 acres</p> <p>BUILDING INTENSITY: 800 dwelling units</p> <p>MINIMUM DENSITY: 25 dwelling units per acre</p>

offices serving the residential development. A landscaped area is designed to occur inside the eastern boundary of Planning Area 4 to provide a transition treatment between the residential units and the retail commercial uses in adjacent Planning Area 5.

Vehicular access to Planning Area 4 is provided exclusively by driveway connections to Inland Empire Boulevard.

Planning Area 4 is MEREDITH INTERNATIONAL CENTRE’s Urban Residential planning area. Planning Area 4 comprises 21.4 acres of land located in the southeastern portion of the Specific Plan area. As shown on Figure 2-6, *Planning Area 4*, this area is bordered on the north by San Bernardino County Flood Control District (SBFCD) facilities, on the south by Inland Empire Boulevard, on the west by the Deer Creek Channel, and on the east by Planning Area 5.

The Urban Residential designation of Planning Area 4 allows for high-density residential land uses (for-sale or for-rent multi-family residential units) within walking distance to a variety of shopping and employment opportunities, Cucamonga-Guasti Regional Park, and the potential Gold Line LRT corridor. The presence of residential uses contributes to the mixed-use lifestyle of MEREDITH INTERNATIONAL CENTRE and surrounding area.

Planning Area 4 is envisioned to include on-site amenities such as the integration of a private recreation facility, pedestrian connections, and ancillary uses such as carports, garages, and leasing

PLANNING AREA 4 LAND USE SUMMARY					
Planning Area	Land Use Designation	Acres	LODGING ROOMS	RESIDENTIAL UNITS	INDUSTRIAL/COMMERCIAL Max. SQ. FT.
4	Urban Residential	21.4	--	800	--

**NOTES:**  
 Acres are approximate and subject to survey verification.  
 Spacing and location of driveway access points shown are approximate and conceptual. Actual locations are subject to City approval.

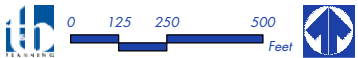
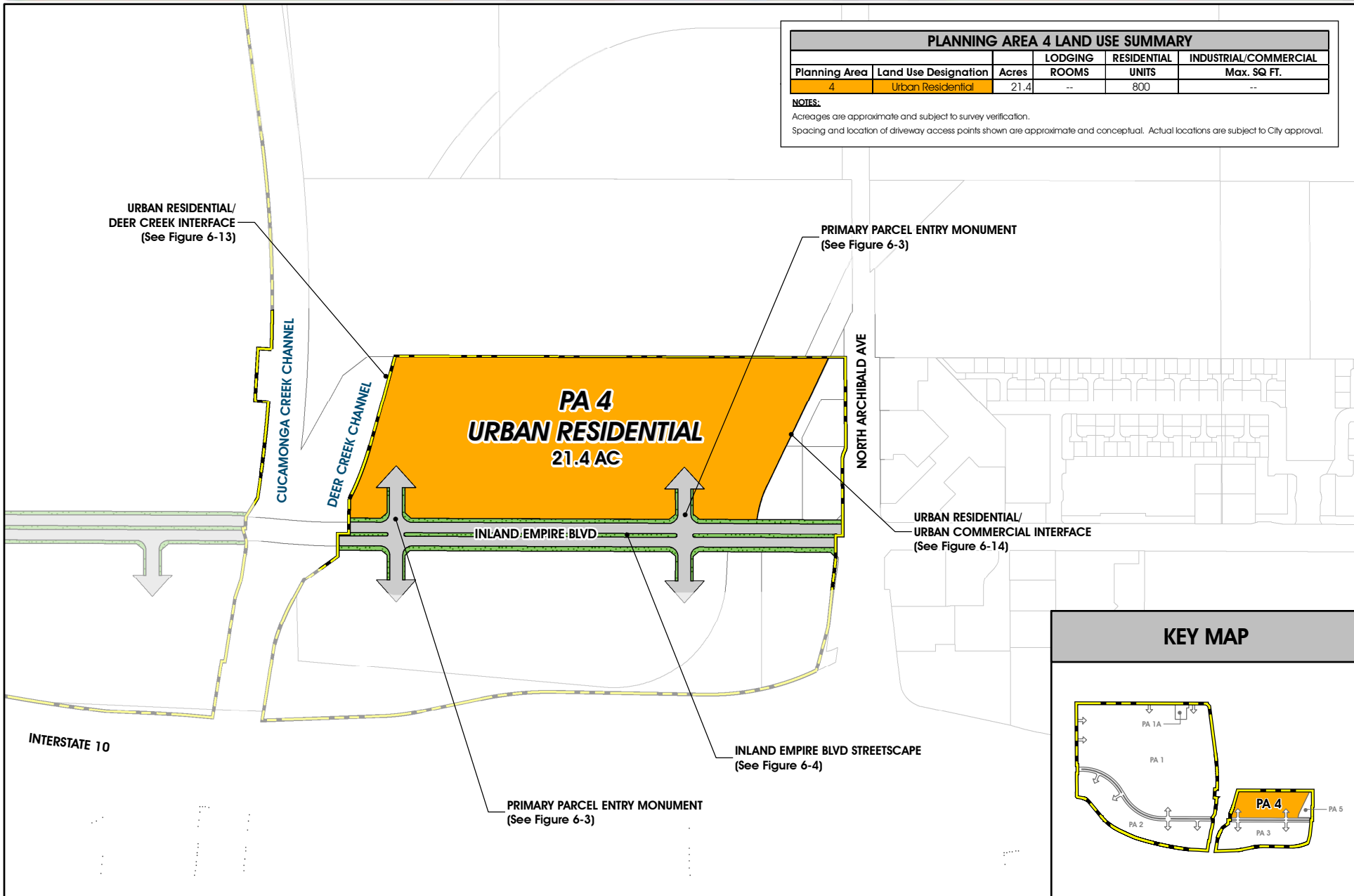



FIGURE 2-6

PLANNING AREA 4



**B.6. Planning Area 5 – Urban Commercial**

<b>VISION</b>	
<b>STATISTICS</b>	<p>LAND USE CATEGORY: Urban Commercial</p> <p>ACREAGE: 2.7 acres</p> <p>BUILDING INTENSITY: 13,000 s.f. (approximately)</p>

Planning Area 5 is situated on 2.7 acres of land in the eastern portion of the Specific Plan area, as shown on Figure 2-7, *Planning Area 5*. Planning Area 5 is bordered on the north by San Bernardino County Flood Control District (SBFCD) facilities, on the south by Inland Empire Boulevard, on the east by North Archibald Avenue, and on the west by Planning Area 4. Refer to Figure 2-7, *Planning Area 5*.

At the time the 2015 Specific Plan Amendment was prepared, Planning Area 5 was fully developed with approximately 13,000 s.f. of Urban Commercial uses; specifically, retail commercial uses and a gas station. This Specific Plan allows for the continuation of these commercial uses and future redevelopment of Planning Area 5 consistent with the standards set forth in Section 5, *Development Standards*.

PLANNING AREA 5 LAND USE SUMMARY					
Planning Area	Land Use Designation	Acres	LODGING ROOMS	RESIDENTIAL UNITS	INDUSTRIAL/COMMERCIAL Max. SQ. FT.
5 (Existing)	Urban Commercial	2.7	--	--	13,000

**NOTES:**  
 Acreages are approximate and subject to survey verification.  
 Spacing and location of driveway access points shown are approximate and conceptual. Actual locations are subject to City approval.

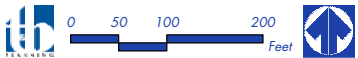
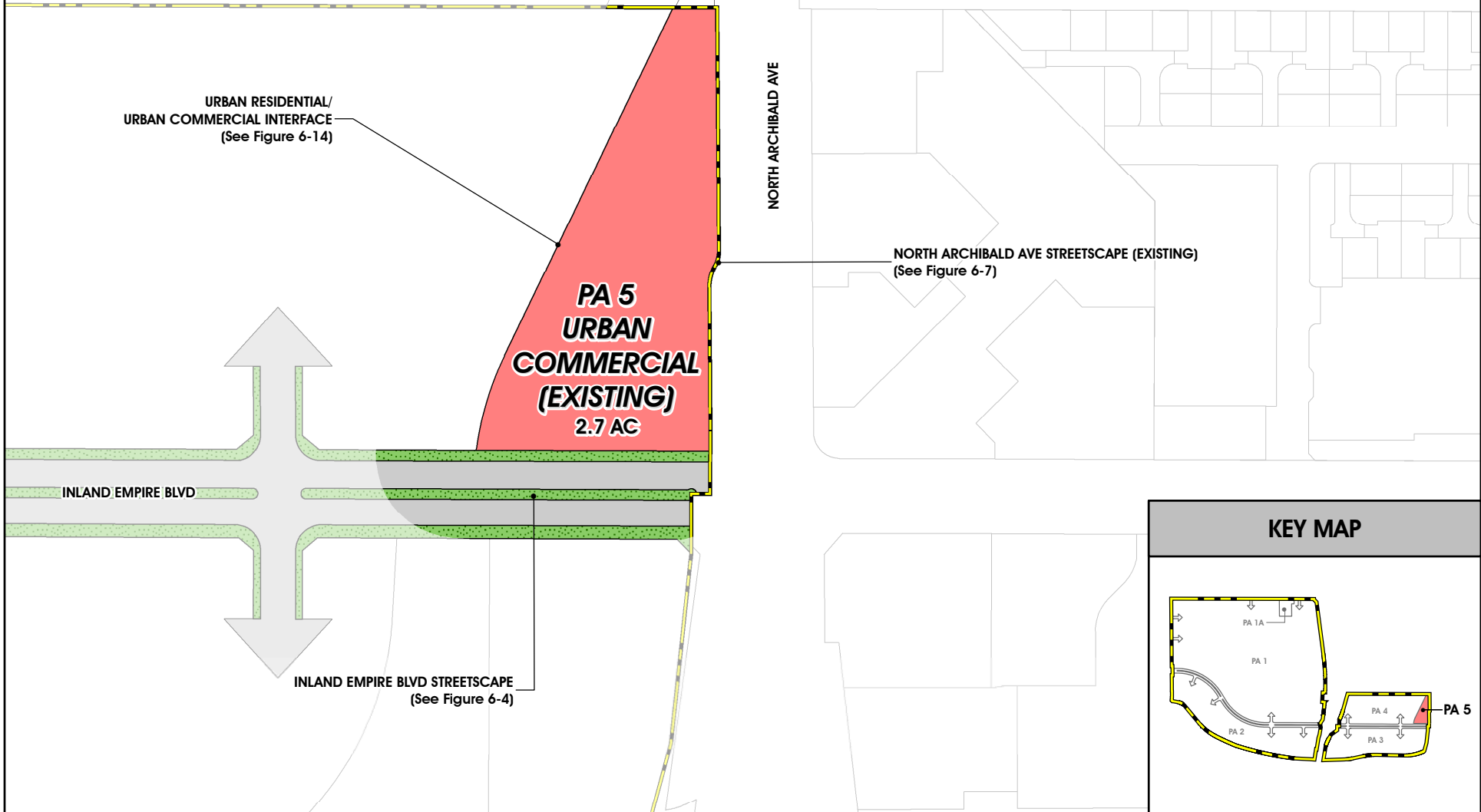


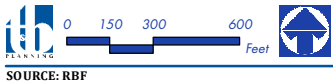
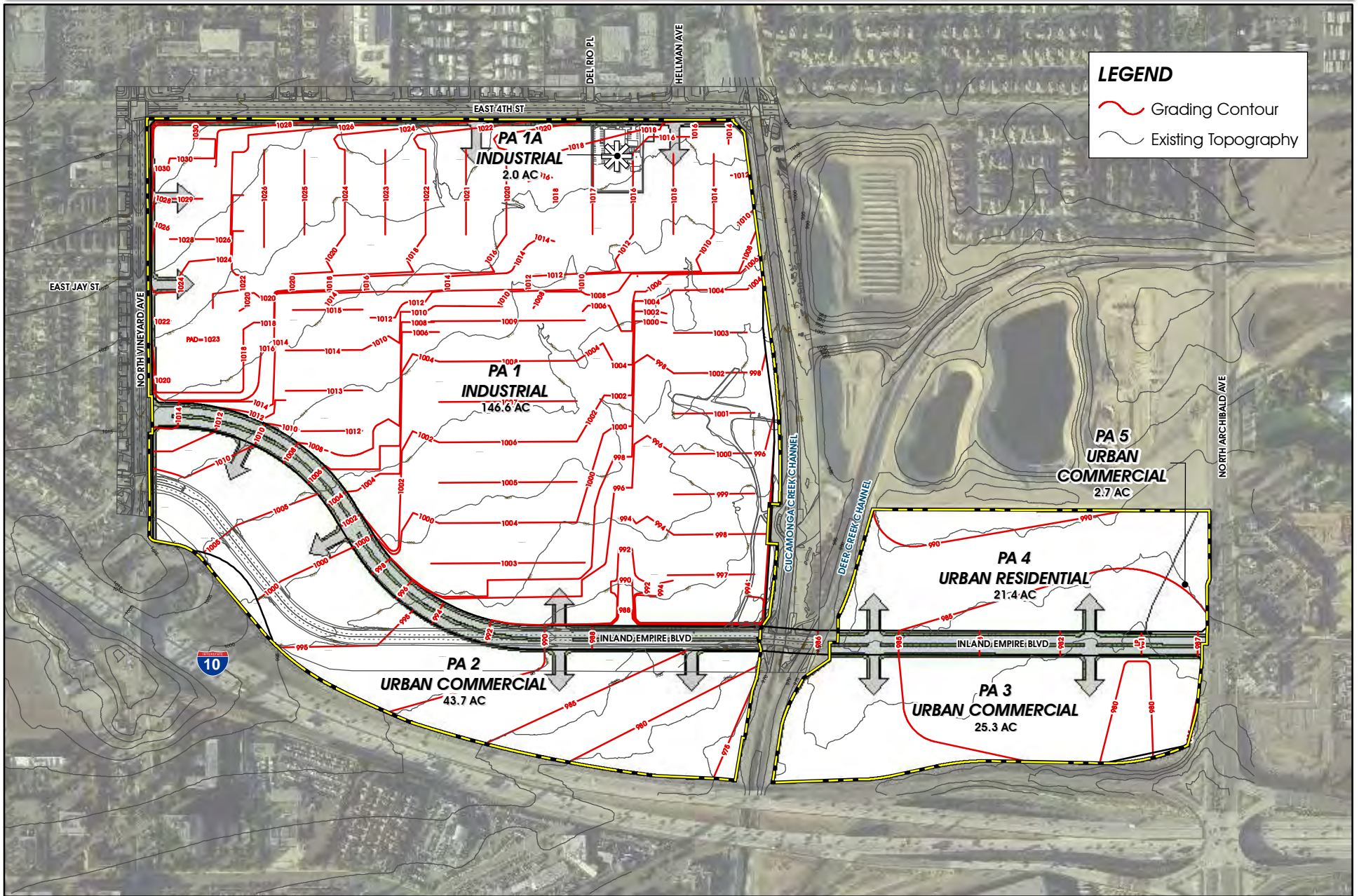
FIGURE 2-7

PLANNING AREA 5

## C. CONCEPTUAL GRADING PLAN

The natural topography of the MEREDITH INTERNATIONAL CENTRE property is relatively flat. No unusual grading conditions are present and substantial import or export of earth materials is not expected. The primary objectives of the grading plan are to: provide stable development pads for construction; balance the cut and fill grading quantities on-site; and meet City of Ontario building standards and acceptable infrastructure gradient requirements. Approximately 600,000 cubic yards (c.y.) of cut and fill plus additional remedial grading as required by a geotechnical engineer is anticipated. The grading plan is shown in Figure 2-8, *Conceptual Grading Plan*.

# MEREDITH INTERNATIONAL CENTRE



SOURCE: RBF

## D. LANDSCAPING AND GREEN SPACE PLAN

MEREDITH INTERNATIONAL CENTRE's landscaping and green space plan serves the dual purpose of adding visual appeal while being sensitive to the environment and Southern California climate by using low water use materials. Refer to Section 6, *Design Guidelines*, for detailed information on landscape design.

Landscaping occurs throughout the CENTRE, but most prominently at street corners and along roadways. Street corners include landscaping and identification monuments. Streetscape landscaping presents a combination of evergreen and deciduous trees, low shrubs, and masses of groundcovers to create a visually pleasing experience for pedestrians and passing motorists. Inland Empire Boulevard, North Vineyard Avenue, and East 4th Street also feature a raised, planted median with evenly spaced trees.

An enhanced landscape zone occurs along the CENTRE's frontage with East 4th Street. This spacious and inviting area features densely planted landscaping and a meandering decomposed granite pathway.

The southern boundary of the MEREDITH INTERNATIONAL CENTRE (Planning Areas 2 and 3) abuts Interstate 10. At this interface, layers of large canopied evergreen shade trees and shrub masses provide a green buffer between Urban Commercial development and the freeway.



