

3. DEVELOPMENT PLAN

This Section provides the framework to guide development of the Euclid Mixed-Use Specific Plan. The Section presents the proposed planning areas, infrastructure plans, and public services to support the project.

3.1 Land Use Plan

The Specific Plan consists of three (3) Land Use Designations divided into five (5) Planning Areas to accommodate the development of a mix of land uses including retail commercial, general commercial/office, business parks, low intensity office/warehouse, technology and incubator space, light manufacturing/distribution uses, high density residential uses and open space areas that are compatible with the site's location and surrounding existing and proposed land uses. The Land Use Plan implements the vision and goals of the Ontario Plan by providing opportunities for employment in a variety of commercial and business industries designed to meet the demand of current and future market conditions. A list of allowable uses by Planning Area is presented in Section 4 (Development Criteria and Standards).

Table 3.1 provides the maximum development intensity (allowable gross building area and maximum residential development for each Planning Area based on the allowable floor area ratio or residential density respectively. Development standards (found in Section 4), such as setback requirements, parking, open space, minimum landscaping, infrastructure, and site design, may reduce the maximum gross square footage or density.

Figure 3.1 (Land Use Plan) identifies the location of the Planning Areas. Each Land Use Designation is described below:

BP (BUSINESS PARK) ZONING DISTRICT:

The Business Park District accommodates business park and related commercial, low intensity office uses, and certain light industrial uses. Development within this district is typically multi-tenant in nature; however, single-tenant buildings are not precluded.

Development within the Business Park District will be set back from Euclid Avenue, separated by a 50 ft. landscaped Neighborhood Edge with an 8 ft. wide multi-purpose trail. Smaller scale buildings will face Euclid Avenue and will be designed to create an attractive frontage that reflects a commercial/office character within the allowable building heights. Parking and vehicle storage areas will be sited behind buildings or will be heavily screened from perimeter street views.

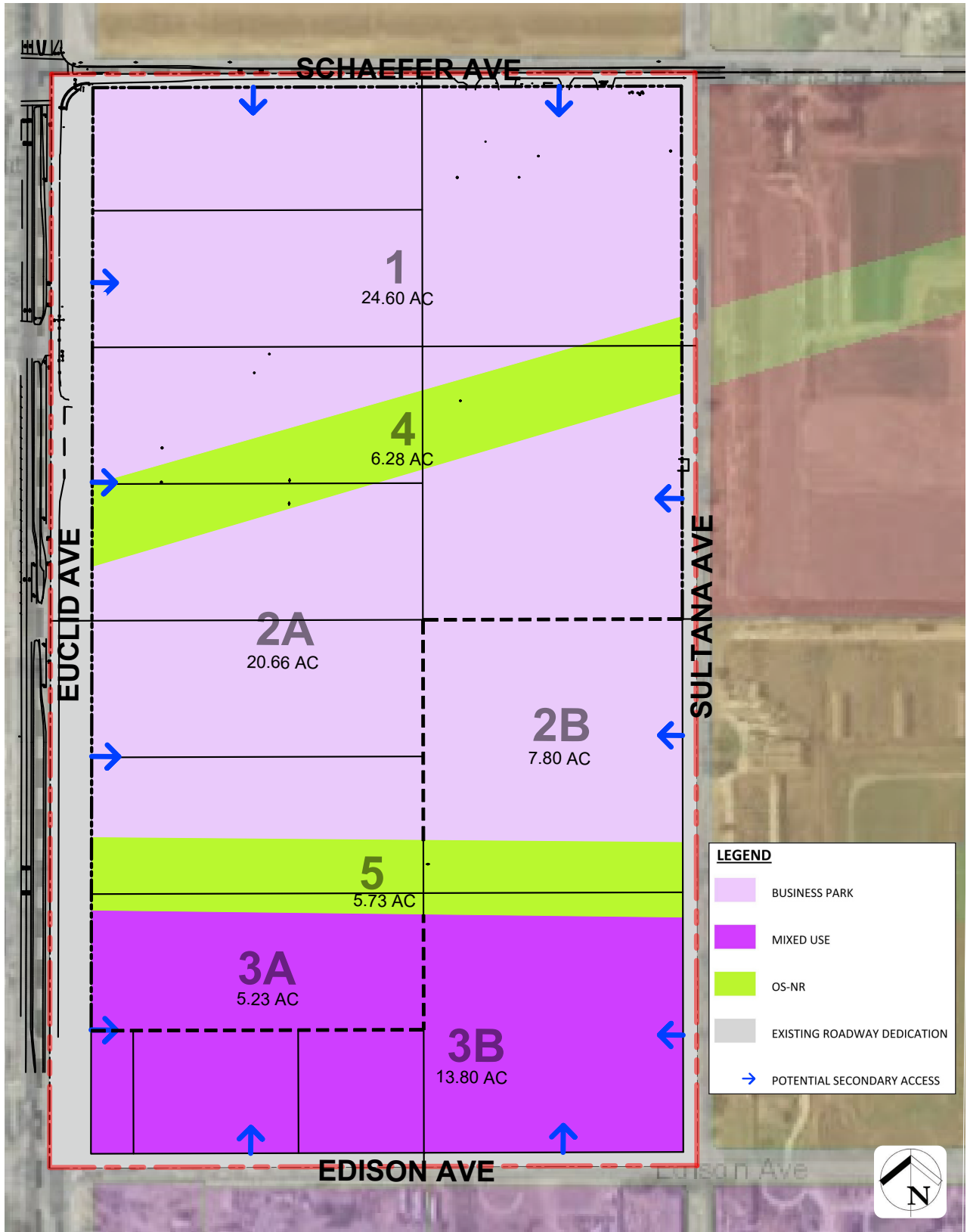


FIGURE 3.1 LAND USE PLAN

**EUCLID MIXED USE SPECIFIC PLAN
LAND USE DEVELOPMENT TABLE**

	<u>PA</u>	<u>TOP USE</u>	<u>ACRES</u>	<u>ALLOWABLE DENSITY</u>	<u>ALLOWABLE FAR</u>	<u>MAXIMUM UNITS</u>	<u>MAXIMUM SQ. FT.</u>	<u>MIXED USE OPTIONS</u>
	1	Business Park	24.60	N/A	0.6	N/A	642,946	N/A
	2A	Business Park	20.66	N/A	0.6	N/A	539,970	N/A
	2B	Business Park	7.80	N/A	0.6	N/A	203,861	N/A
TOTAL BUSINESS PARK			53.06				1,386,776	

	3	Mixed Use *	1.90	N/A	1.5	N/A	124,146	OFFICE
	3	Mixed Use *	3.81	N/A	1.0	N/A	165,964 **	RETAIL
	3	Mixed Use *	13.32	35	N/A	N/A	N/A	RESIDENTIAL
TOTAL MIXED USE*			19.03			466	290,110	

	4	OS-NR	6.28	N/A	N/A	N/A	N/A	N/A
	5	OS-NR	5.73	N/A	N/A	N/A	N/A	N/A
TOTAL OPEN SPACE			12.01					

TOTAL			84.1			466	1,676,886	
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* PA 3 Totals allow a combination of Office, Retail and High Density Residential Development. For Traffic Calculations, Mixed Uses are assumed to be 10% Office, 20% Retail and 70% Residential. Assume a maximum of 124,146 s.f. OFFICE, 165,964 s.f. RETAIL and 466 D.U.

** SP proposes a minimum of 20,000 s.f. of RETAIL within PA 3. Maximum density allowed may exceed 35 du/ac on any parcel as long as a maximum of 466 du is not exceeded within the Specific Plan area.

TABLE 3.1 DEVELOPMENT TABLE

MU (MIXED-USE) ZONING DISTRICT:

The MU zoning district accommodates a wide variety of retail commercial, office and high density residential development in conformance with The Ontario Plan 2050. It is anticipated that the Mixed-Use development will be comprised of 10% Office, 20% Retail and 70% Residential.

The southern portion of the site, comprised of approximately 20 acres along the north side of Edison Avenue, is zoned for Mixed Use development. This area is part of a much larger mixed use area identified in The Ontario Plan 2050 as the Great Park Mixed Use Area. Approximately 170 additional contiguous acres is located south of Edison Avenue. This Mixed Use Area is intended to encourage commercial/retail, office, and higher density residential development opportunities to serve the western portion of the Ontario Ranch and to provide a west anchor to the City of Ontario Great Park.

OS-NR (OPEN SPACE/NON-RECREATIONAL) ZONING DISTRICT:

The OS-NR zoning district defines the utility corridors (Southern California Edison) easement areas within the City. This zoning district allows low intensity uses such as vehicle and truck storage, nursery and limited agricultural uses, parking, etc. subject to written approval from Southern California Edison.

This “District” encompasses the utility easement corridors within the Euclid Mixed Use Specific Plan boundary. These areas are designated for non-recreational uses. While no buildings are proposed within this district, it is suitable for uses such as landscape plant nurseries, recreational vehicle and truck/trailer storage and other uses allowed by the City zoning.

3.2 Circulation Plan

The Circulation Plan facilitates movement of vehicles, pedestrians and cyclists around and within the Specific Plan area, consistent with the City of Ontario Roadway Classification System, shown in **Figure 3.2**.

Figures 3.4 to 3.7 present typical street cross sections for Euclid Avenue, Schaefer Avenue, Sultana Avenue, and Edison Avenue. Conceptual streetscape design is presented in Section 5 (Design Guidelines). Roadway, sidewalk, and trail improvements within the Specific Plan area must be approved by the City’s Engineering Department.

3.2.1 Local Circulation

Final site planning and off-site design shall be subject to City approval. In addition to the typical street section described and depicted, additional right-of-way and geometric enhancements such as an additional left turn and right turn lanes, including but not limited to those at intersections, may be required to adequately mitigate impacts identified in the Traffic Impact Analysis/Specific Plan EIR.

Driveways shall conform to access requirements of the Traffic and Transportation Design Guidelines. Driveway locations, specifically those that are in proximity to master planned or future traffic signals, shall be located so as not to interfere with traffic stacking and queues as projected in the The Ontario Plan 2050 SEIR Traffic Impact Analysis. Fair share responsibilities for street improvements will be addressed in a Development Agreement with the City.

3.2.2 Traffic Control Devices

All traffic signs regulating, warning, and/or guiding traffic on public roads will conform to the California Manual on Uniform Traffic Control Devices (MUTCD), latest edition. All traffic control signs, whether on public or private property, shall conform to the California MUTCD.

Additional traffic signals may be required based on locations of future access points and as identified in the Traffic Impact Analysis/Specific Plan EIR.

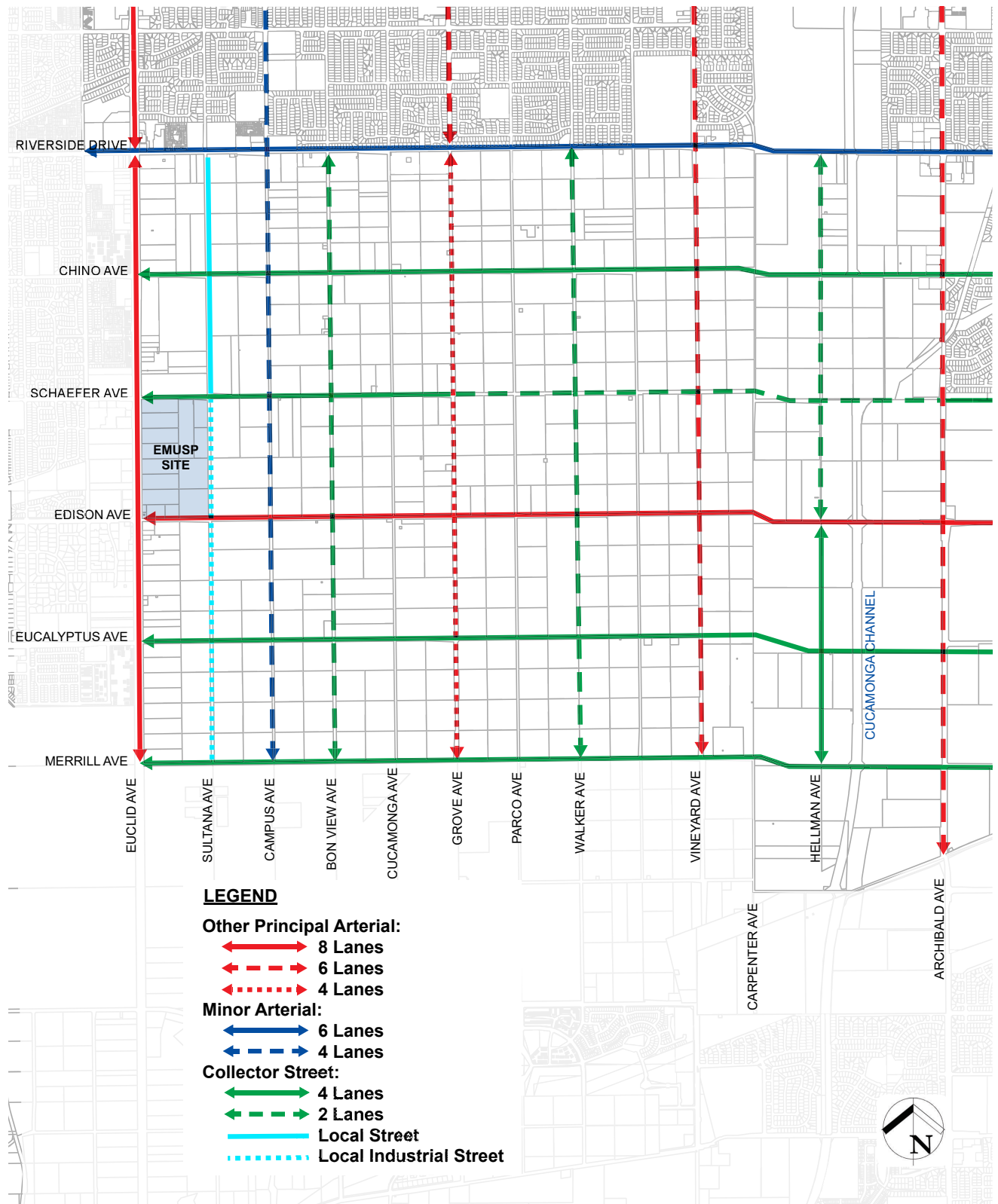


FIGURE 3.2 CITY OF ONTARIO ROAD CLASSIFICATION SYSTEM

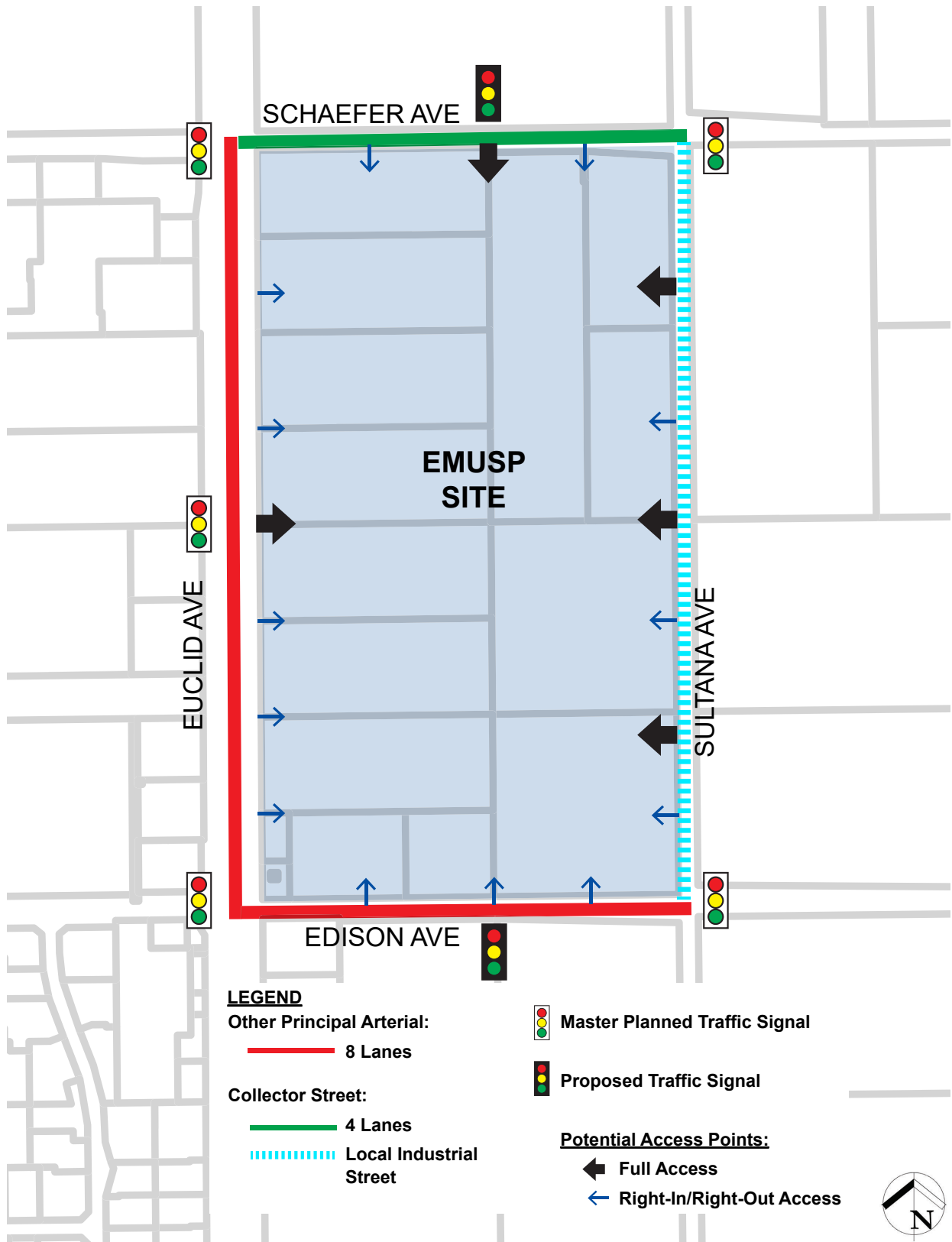
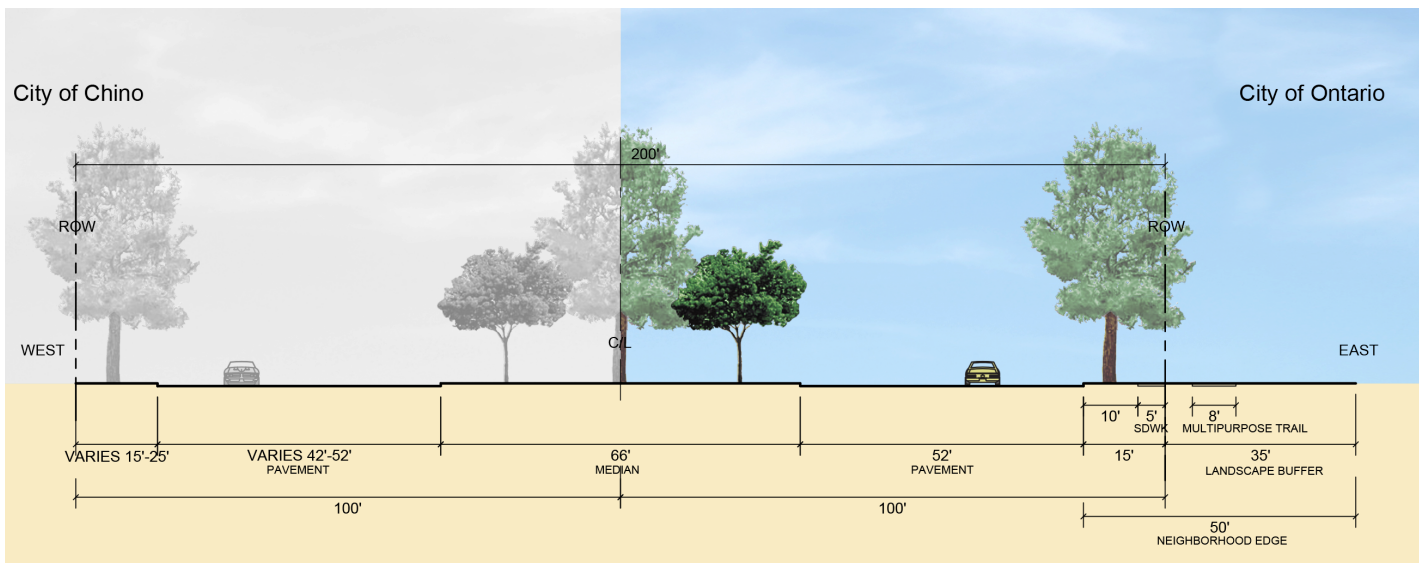


FIGURE 3.3 CIRCULATION PLAN

3.2.3 Euclid Avenue (Route 83)

Euclid Avenue is a State Route under Caltrans' jurisdiction that is designated as an eight lane Other Principal Arterial in The Ontario Plan 2050. The centerline of this street forms the boundary between the City of Ontario to the east and the City of Chino to the west. Euclid Avenue is designed with a 200 ft. wide right-of-way, a 66 ft. wide center median, and 52 ft. of pavement including curbs and gutter. The existing half-width street right-of-way is 100 ft.; therefore, no dedication is required.

The Euclid Avenue streetscape design illustrated in Section 5, Design Guidelines, for the east side of the street adjacent to the project site specifies a 15 ft. wide parkway including a 5 ft. wide sidewalk and an 8 ft. wide on-site multipurpose trail within a 35 ft. wide landscape buffer, creating a 50 ft. wide neighborhood edge as specified in the Ontario Ranch Colony Streetscape Master Plan.



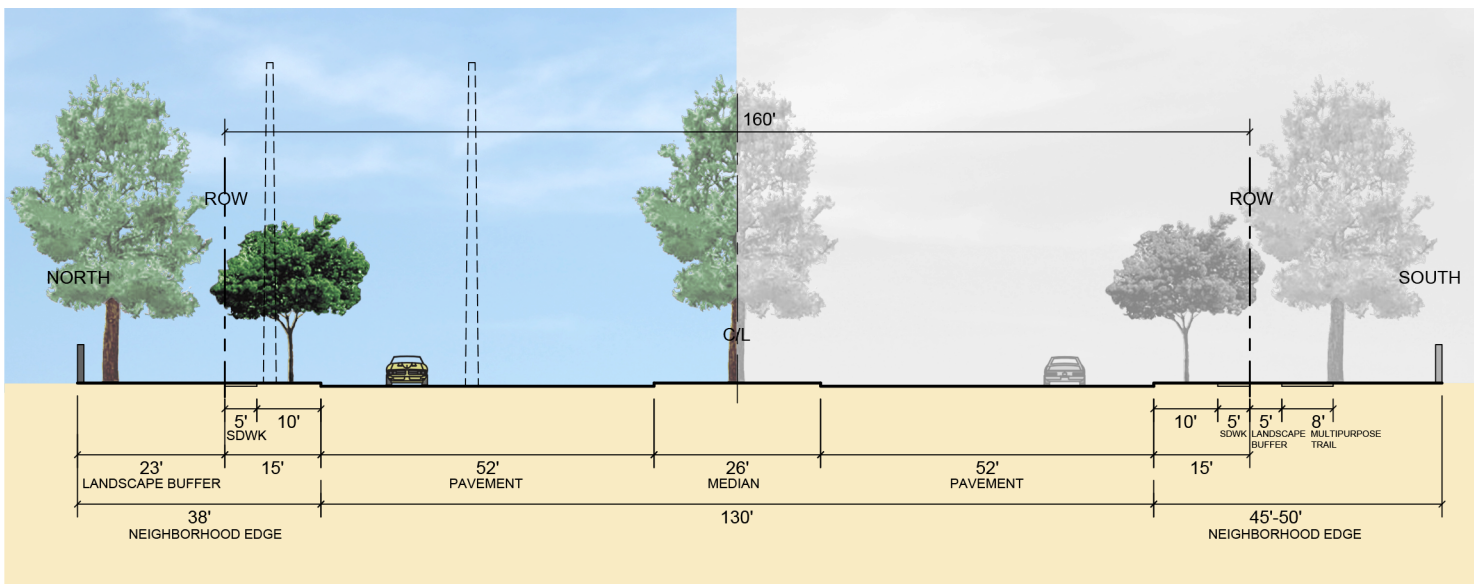
EUCLID AVENUE

FIGURE 3.4 EUCLID AVENUE STREET SECTION

3.2.4 Edison Avenue

Edison Avenue is located along the southern boundary of the Specific Plan area, providing east/west access to the site. Edison Avenue is designated as a Other Principal Arterial Street in The Ontario Plan 2050. The Specific Plan specifies an 160 ft. wide right-of-way with a 26 ft. landscaped median and a 52 ft. paved section on each side.

The Edison Avenue streetscape design presented in Section 5, Design Guidelines, for the north side of the street adjacent to the project site specifies a 15 ft. wide parkway including a 10 ft. wide curb-adjacent landscaped area and a 5 ft. wide sidewalk. The south side also provides an 8 ft. wide on-site multipurpose trail within a 23 ft. wide landscape buffer setback. Together, the parkway and landscape buffer setback create a 38 ft. wide neighborhood edge, per in the City’s Master Plan of Streets.



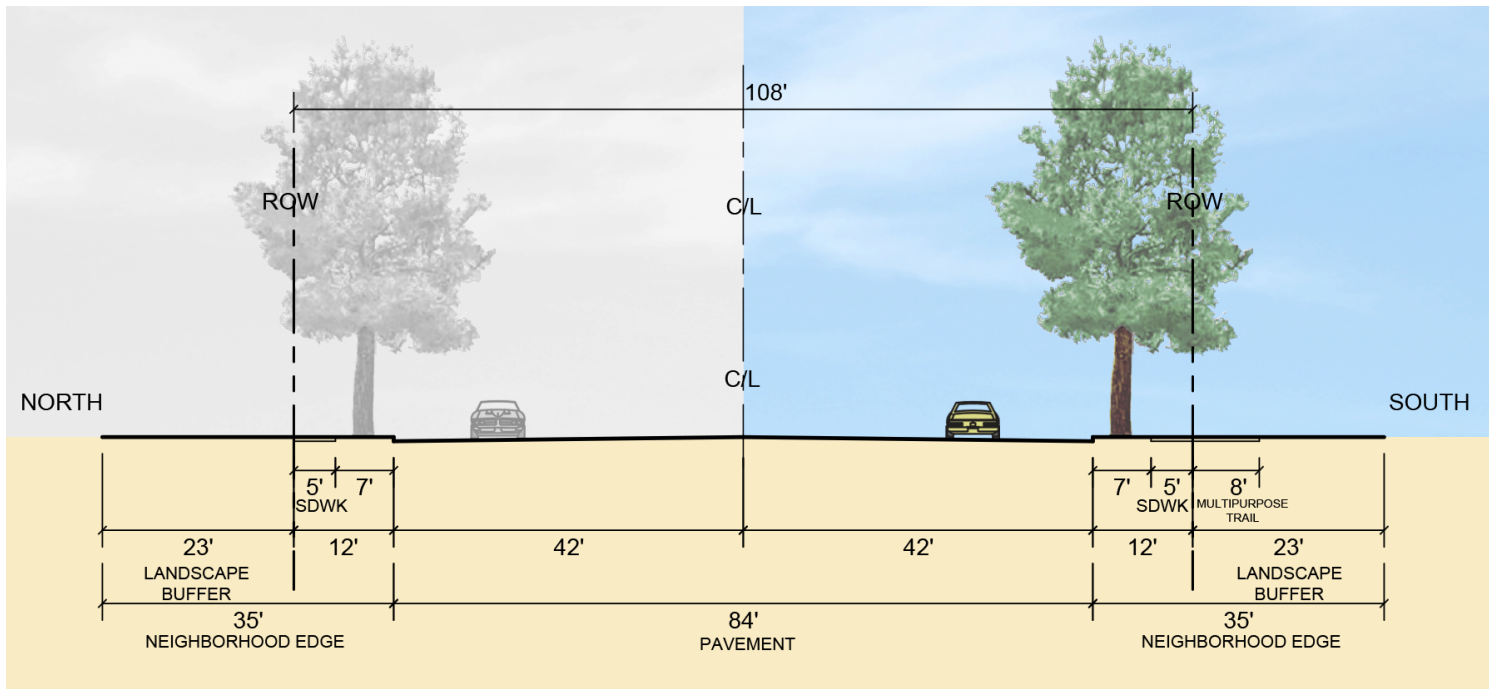
EDISON AVENUE

FIGURE 3.5 EDISON AVENUE STREET SECTION

3.2.5 Schaefer Avenue

Schaefer Avenue is designated as a four lane Collector Street per the City's Master Plan of Streets and provides east-west access to the project's northern boundary. The Specific Plan specifies an 108 ft. wide right-of-way and 84 ft. of pavement including curb and gutter for Schaefer Avenue.

The Schaefer Avenue streetscape design presented in Section 5, Design Guidelines, for the south side of the street adjacent to the project site includes a Class II on-street bike lane at the edge of the street, a 7 ft. wide curb-adjacent landscaped area, and a 5 ft. wide sidewalk. Together, these improvements establish a 35 ft. wide neighborhood edge, per in the City's Master Plan of Streets.

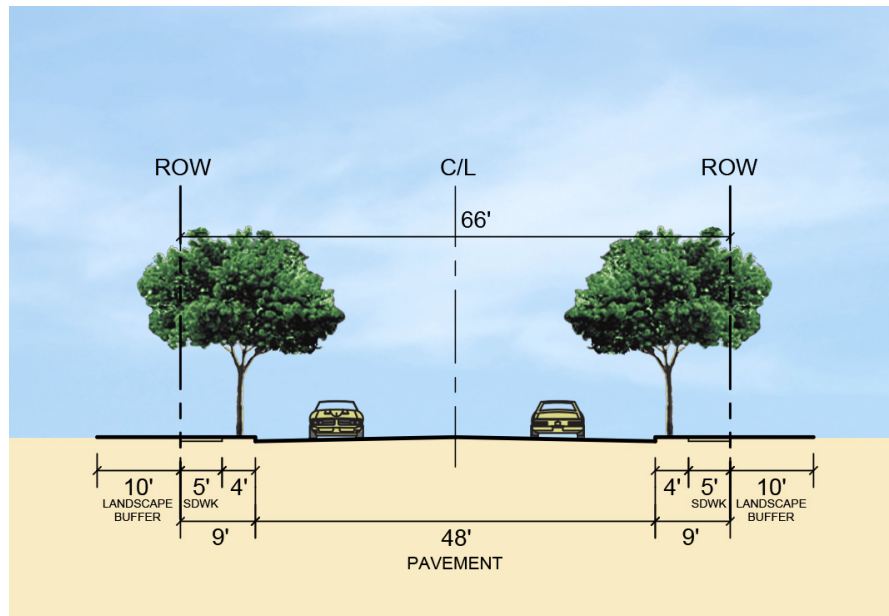


SCHAEFER AVENUE

FIGURE 3.6 SCHAEFER AVENUE STREET SECTION

3.2.6 Sultana Avenue

Sultana Avenue is designated as a local industrial street with a 66 ft. wide right-of-way and 48 ft. of pavement including curb and gutter. The Sultana Avenue streetscape presented in Section 5, Design Guidelines, specifies a 9 ft. wide parkway including a 4 ft. landscape and a 5 ft. wide sidewalk. The west side of the street adjacent to the project site provides a 10 ft. wide landscape buffer setback. Sultana Avenue is not yet developed adjacent to the Specific Plan area. However, the right-of-way exists, and no dedication is required.



SULTANA AVENUE

FIGURE 3.7 SULTANA AVENUE STREET SECTION

3.2.7 Pedestrian Circulation

To improve safety and the pedestrian experience, connect the various parts of the Specific Plan area, and expand access to nearby land uses, sidewalks will be provided along all streets abutting the Specific Plan area.

3.2.8 Trails and Bicycle Paths

The Ontario Plan Mobility Element specifies a Class II bicycle lane on the north side of Schaefer Avenue adjacent to the Specific Plan area. Class II bicycle lanes are defined as dedicated (striped) lanes along streets, with no parking allowed in the bicycle lane. This bicycle lane provides linkages to the City's bicycle path system (*Figure 3.8*).

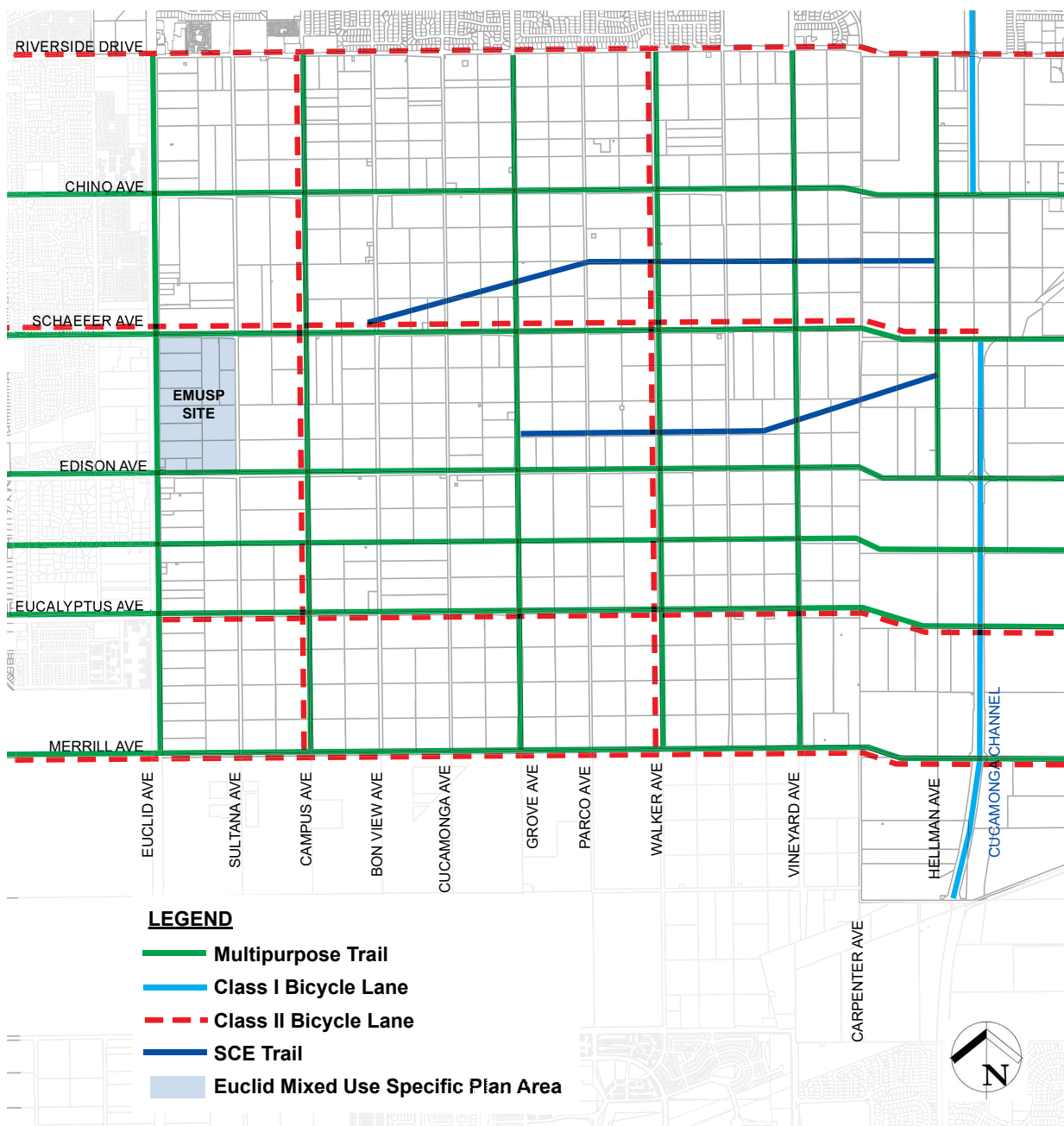


FIGURE 3.8 CITY OF ONTARIO TRAIL AND BICYCLE PATHS PLAN

Trails and bicycle paths will provide an additional mode of circulation in and around the Specific Plan area. Multipurpose trails will be provided on the east side of Euclid Avenue, on the south side of Schaefer Avenue and on the south side of Edison Avenue (**Figure 3.9**).

The trail and bicycle path improvements will be installed along the project frontages in conjunction with street improvements.

3.2.9 Transit

Transit options provide an alternative mode of transportation for motorists and a primary mode for the transit dependent. The City is coordinating with regional transit agencies to implement Bus Rapid Transit (BRT) service to target destinations and along corridors, including Euclid Avenue on the western boundary of the Specific Plan area.

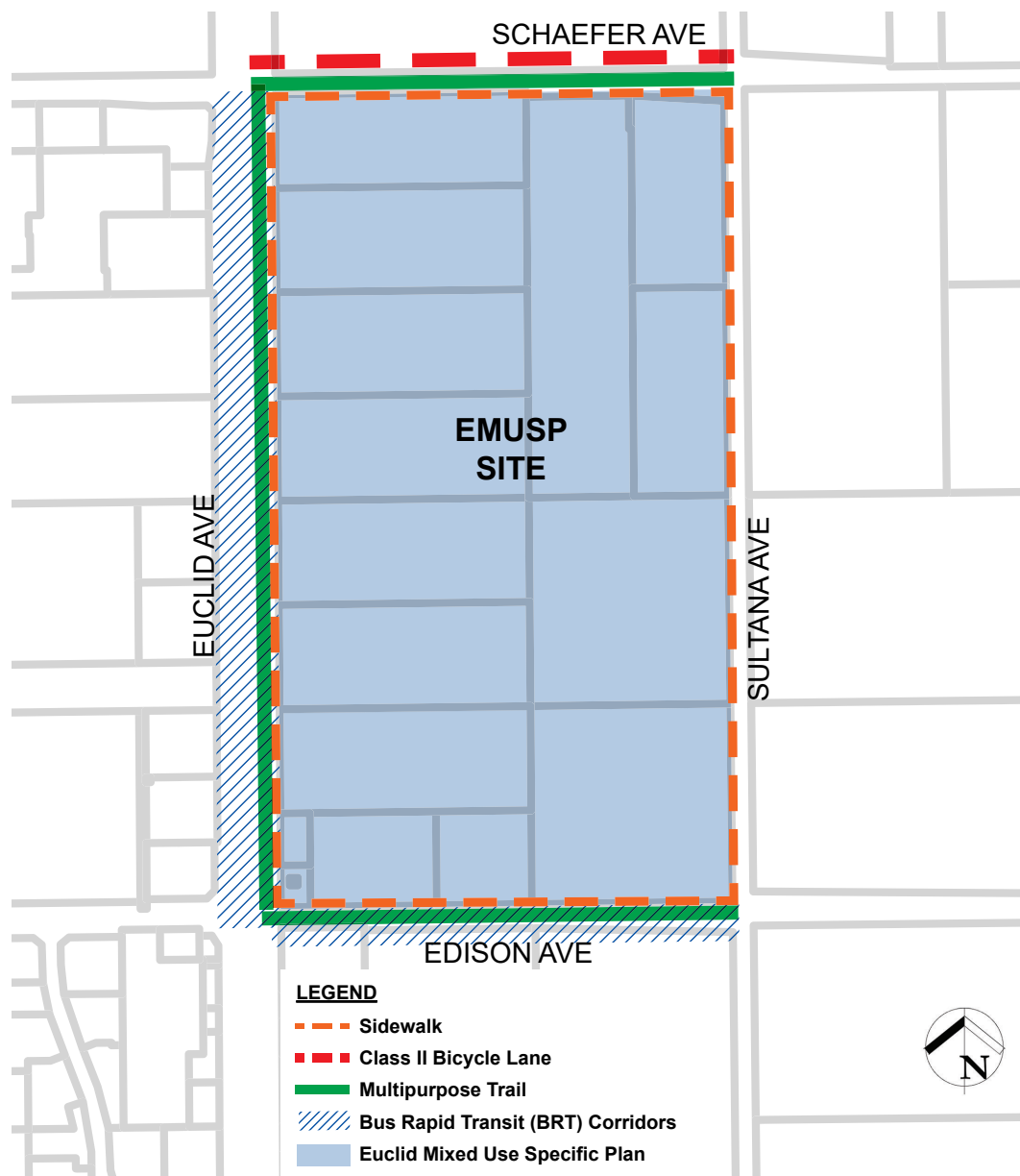


FIGURE 3.9 BICYCLE AND PEDESTRIAN PLAN

3.3 Potable Water Plan

The City's ultimate domestic water system will consist of five pressure zones. Most of Ontario Ranch (including the Specific Plan area) is in the 925 Pressure Zone. The overall water infrastructure plan to serve the City of Ontario as well as the required water infrastructure required to be constructed to serve the Euclid Mixed-Use Specific Plan area is shown on **Figure 3.10**. The developer of the Euclid Mixed-Use Specific Plan will be responsible to provide the required water lines to connect to the existing improvements constructed by the Ontario Ranch Business Park development to the south of the site.

925 Pressure Zone (PZ) Phase 2 Potable Water System facilities will need to be constructed, by others, to serve the Project. These facilities include installing a 42-inch potable water main in Grove Avenue connecting from the 30-inch to the 42-inch potable water main in Grove Avenue at Chino Ave; installing a 42-inch potable main in Chino Avenue connecting from the 42-inch potable water main in Campus Avenue and extending to Francis Street; and, installing a 42-inch potable water main in Bon View Avenue connecting from the 42-inch potable water main in Francis Street and extending to the Bon View Avenue Reservoir site and to the Reservoir; and, installing a 9 million gallon reservoir on the Bon View Reservoir site; and, installing two 2,500 gpm wells with any treatment necessary to meet water quality standards and the 16-inch to 42-inch well collection mains from the wells to the reservoirs. The EUCLID MIXED-USE Project will be required to participate in the future Phase 2 Water System Improvements as detailed in the development agreement with the City.

In addition to extending the 925 Pressure Zone (PZ) Phase 2 West Backbone, described above, the Specific Plan area requires a connection between the 925 Pressure Zone (PZ) Phase 2 West Backbone and the 1010 Pressure Zone (PZ). This will supply a second source of potable water to the Specific Plan area. The connection to the 1010 Pressure Zone (PZ) will require extending the Phase 2 West Backbone at Eucalyptus Avenue and Grove Avenue by installing a 30-inch potable water main north on Grove Avenue to Chino Avenue. The connection to the 1010 Pressure Zone will require installing an 18-inch potable water main in Chino Avenue easterly to the existing 18-inch potable water main located on the west side of the Cucamonga Creek channel and installing a Pressure Reducing Station between the 1010 PZ and 925 PZ near the intersection of Grove Avenue and Chino Avenue.

Other elements of the Phase 2 Water System are shown on **Figure 3.10**. The elements shown north of Chino Avenue will be constructed by others. The balance of Phase 2 Water System will be completed as required by future development of Ontario Ranch. The Project will be required to participate in the future Phase 2 Water System improvements, as detailed in the development agreement with the City.

Water service to the Specific Plan area will be provided by the City of Ontario. Currently there are no City domestic water mains or City water infrastructure in the vicinity of the Specific Plan area. The extension of City master planned domestic water infrastructure is being developed within the western portion of Ontario Ranch. The Euclid Mixed-Use Specific Plan is responsible to provide domestic water service to serve future development and will construct a 16-inch line in Euclid Avenue and Schaefer Avenue, and a 12-inch line along the remaining perimeter of the Specific Plan in Sultana Avenue and Edison Avenue. The Specific Plan will extend the 16-inch line in Schaefer Avenue to connect to the future line at Grove Avenue and the 16-inch line in Euclid Avenue south to connect to the future line in Eucalyptus Avenue.

Water mains required to serve the project will need to be constructed prior to or concurrent with on-site water improvements. Within the project site, a private network of 2-inch to 4-inch water lines for domestic water service and 10-inch to 12-inch water lines for fire service water will be installed. The on-site water system includes connections to the water main in Edison Avenue, Euclid Avenue, Schaefer Avenue and Sultana Avenue.

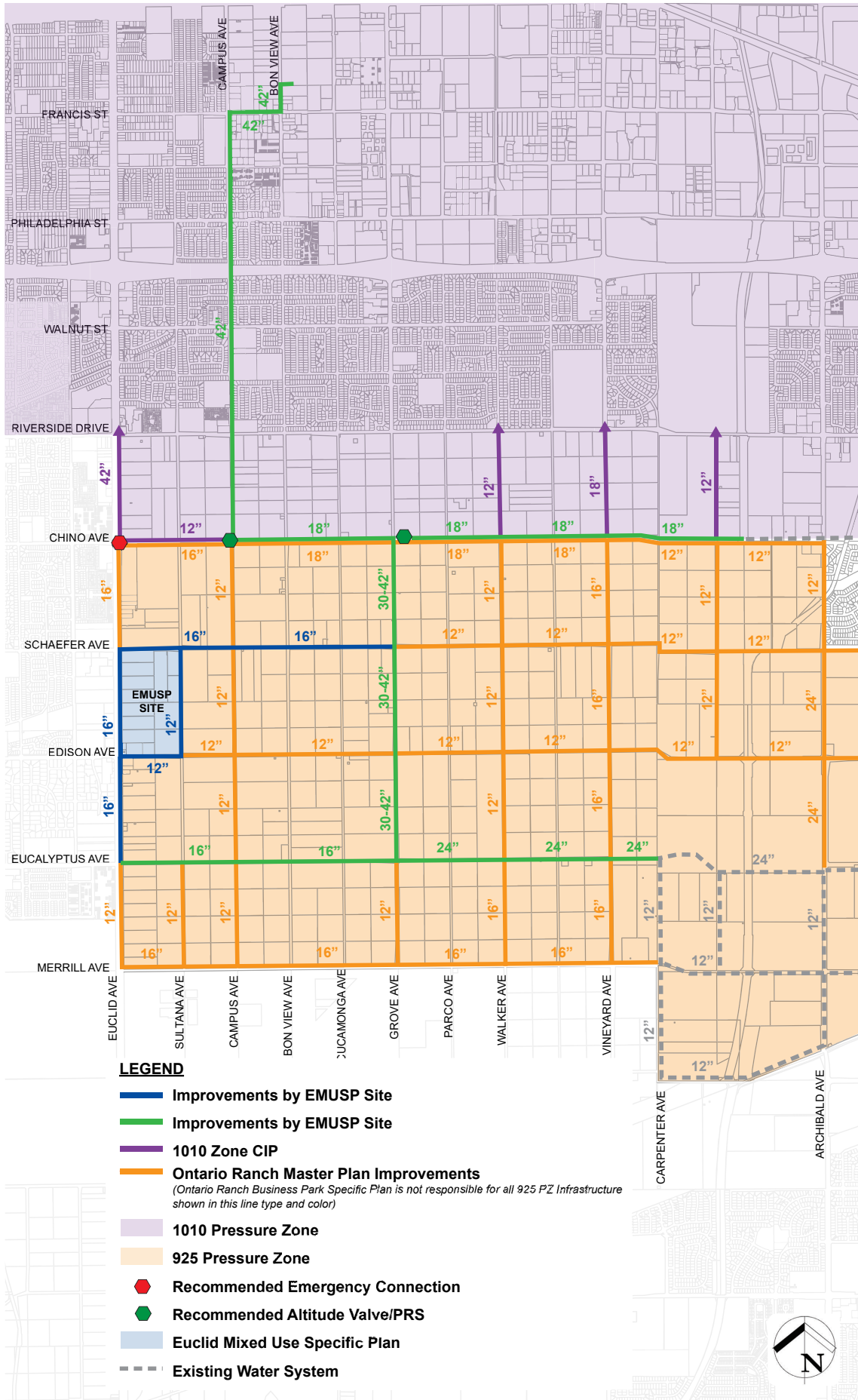


FIGURE 3.10 CITY OF ONTARIO ULTIMATE WATER SYSTEM

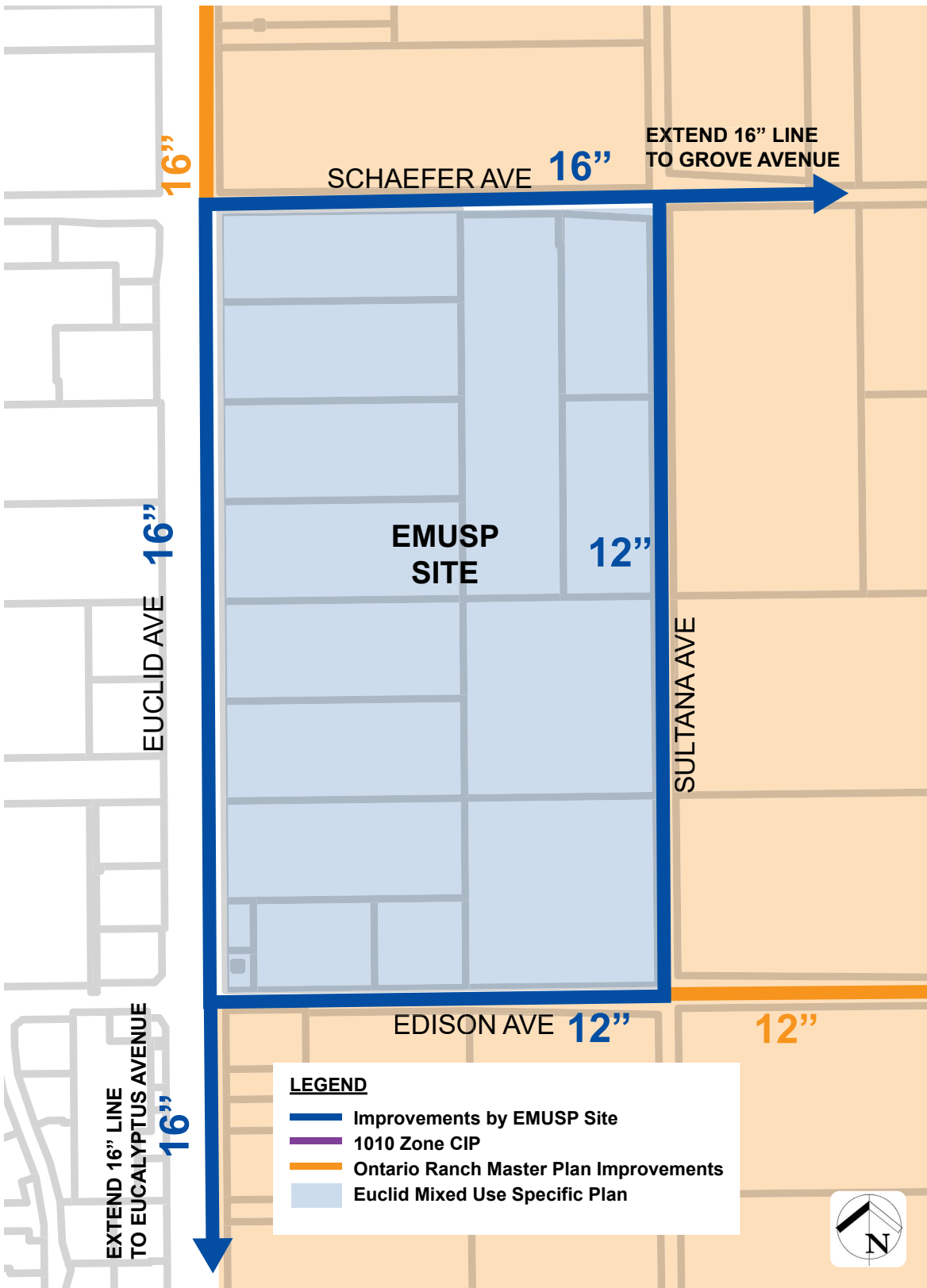


FIGURE 3.11 SPECIFIC PLAN DOMESTIC WATER PLAN

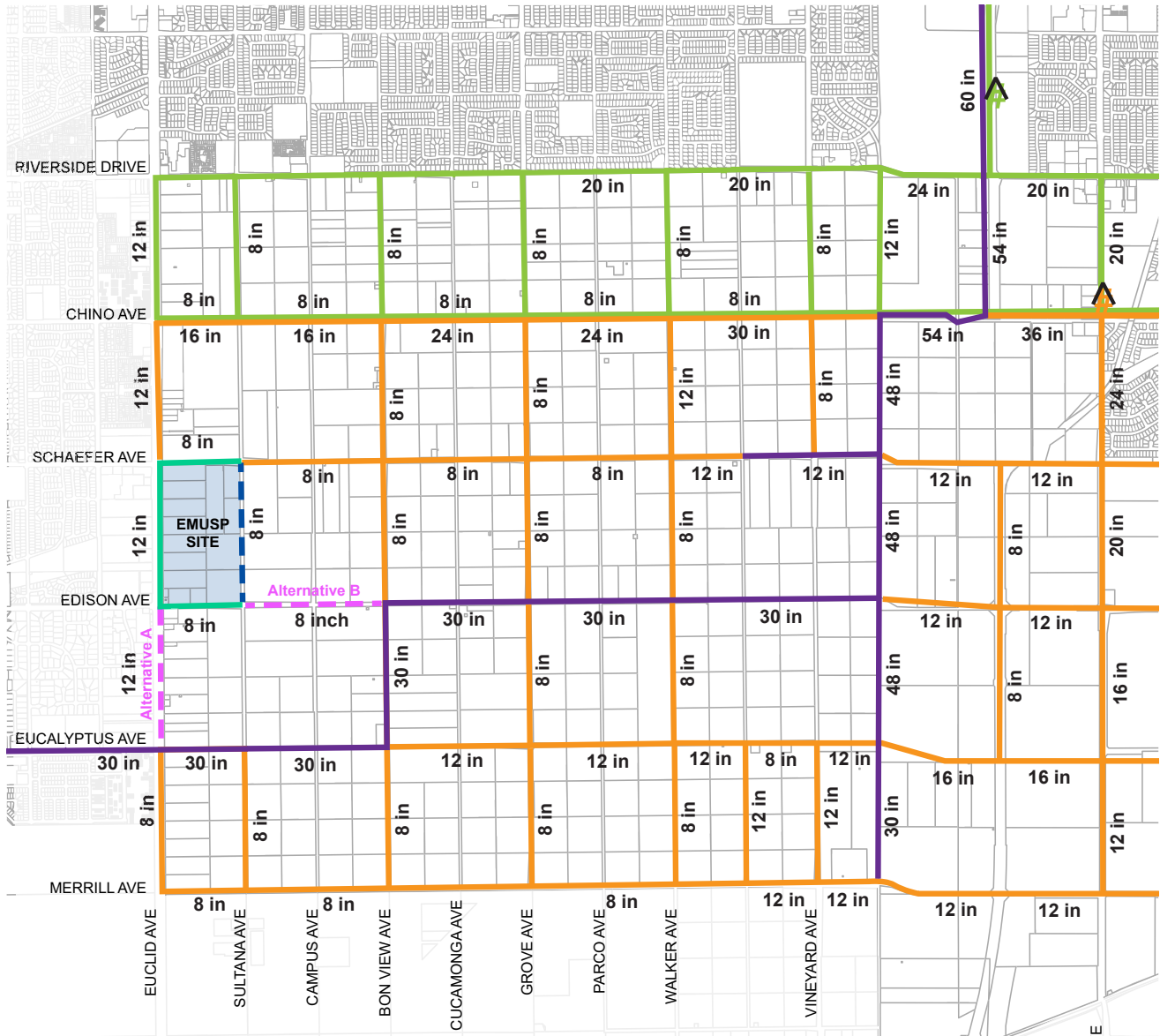
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3.4 *Recycled Water Plan*

Recycled Water is provided to the City of Ontario by the IEUA from its four wastewater reclamation plants. The entire Specific Plan area is within the City's master planned 930 Pressure Zone. Recycled water infrastructure improvements requiring the planning, design, and construction of new 930 Pressure Zone (PZ) Recycled Water Master Plan main lines area will be required. Refer to **Figure 3.12**.

The City requires all new development in Ontario Ranch to connect to and use recycled water for all approved uses, including but not limited to landscape irrigation. Currently there are no City owned recycled water mains or City recycled water infrastructure in the vicinity of the Specific Plan area. There is an existing 30 inch Inland Empire Utility Agency (IEUA) recycled water main in Eucalyptus Avenue south of the Specific Plan area. The Euclid Mixed-Use Specific Plan is responsible to provide recycled water service to serve future development and will construct a 12" line in Euclid Avenue and a 8" line in Schaefer Avenue, Sultana Avenue and Edison Avenue. The Specific Plan will extend the 12" line in Euclid Avenue south to connect to the existing line in Eucalyptus Avenue.

An 8" line may be required in Sultana Ave to provide service to parcels that only front on Sultana Ave.



LEGEND




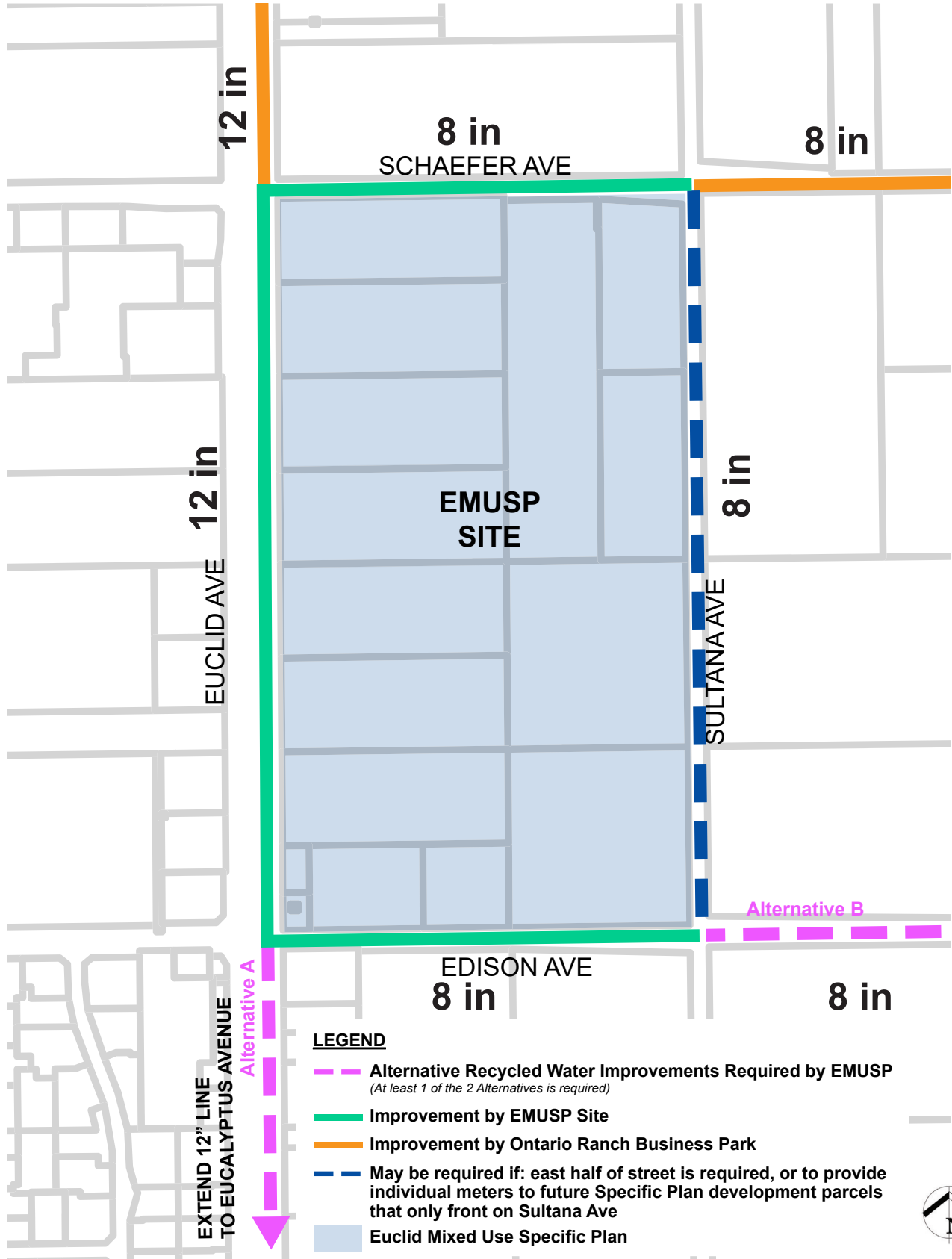
- **Alternative Recycled Water Improvements Required by EMUSP**
(At least 1 of the 2 Alternatives is required)
- **Improvement by EMUSP Site**
- **Improvement by Ontario Ranch Business Park**
- **May be required if: east half of street is required, or to provide individual meters to future Specific Plan development parcels that only front on Suitana Ave**
- **1050 Zone CIP**
- **Existing IEUA Pipeline**
-  **Pressure Reducing Station**
-  **Pressure Reducing Station**
-  **Euclid Mixed Use Specific Plan**



FIGURE 3.12 CITY OF ONTARIO ULTIMATE RECYCLED WATER SYSTEM



- LEGEND**
- Alternative Recycled Water Improvements Required by EMUSP
(At least 1 of the 2 Alternatives is required)
 - Improvement by EMUSP Site
 - Improvement by Ontario Ranch Business Park
 - May be required if: east half of street is required, or to provide individual meters to future Specific Plan development parcels that only front on Sultana Ave
 - Euclid Mixed Use Specific Plan

FIGURE 3.13 SPECIFIC PLAN RECYCLED WATER PLAN

3.5 Sewer Plan

Regional wastewater treatment services are provided to the City of Ontario by the Inland Empire Utilities Agency (IEUA). Several regional trunk sewers collect sewage generated in the City and transport it to IEUA's Regional Plant No.1 and Regional Plant No.5 for treatment. Sewer Main Sizes to be determined by final design requirements and criteria.

There are no sewer mains located in Euclid Avenue or the other streets adjacent to the Specific Plan area. The extension of City master planned sewer infrastructure is being developed within the western portion of Ontario Ranch. The City's master planned sewer system has been constructed in Euclid Avenue at Eucalyptus Avenue and along Eucalyptus Avenue between Euclid and Sultana Avenues.

The Euclid Mixed-Use Specific Plan is responsible to provide sewer service to serve future development and will construct a 16" line approximately 2,650 ft. along the entire frontage of the project in Euclid and extend an 18" main line along the Euclid Avenue to the south to connect to the Kimball Interceptor Sewer. The Specific Plan will construct an 8" line along approximately 1,320 ft. of the eastern boundary of the Specific Plan in Sultana Avenue to Edison Avenue and extend an 8" line in Edison Avenue to connect to the 18" main line in Euclid Avenue.

Actual sewer pipe size will be determined during final design by hydraulic calculations through a Sewer Study or Sewer Sub-Area Master Plan (SSAMP). Refer to **Figure 3.15**.

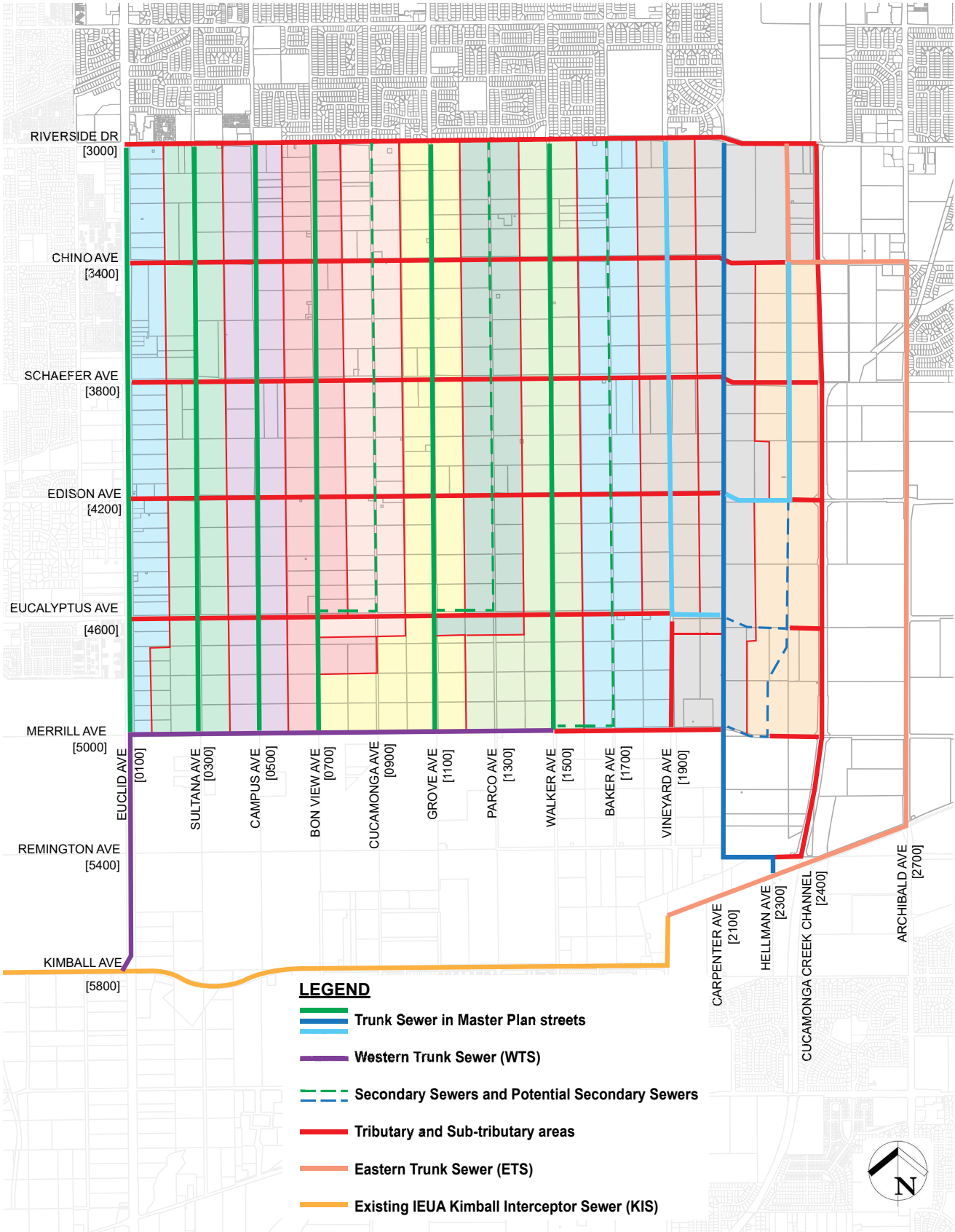


FIGURE 3.14 CITY OF ONTARIO OVERALL SEWER PLAN

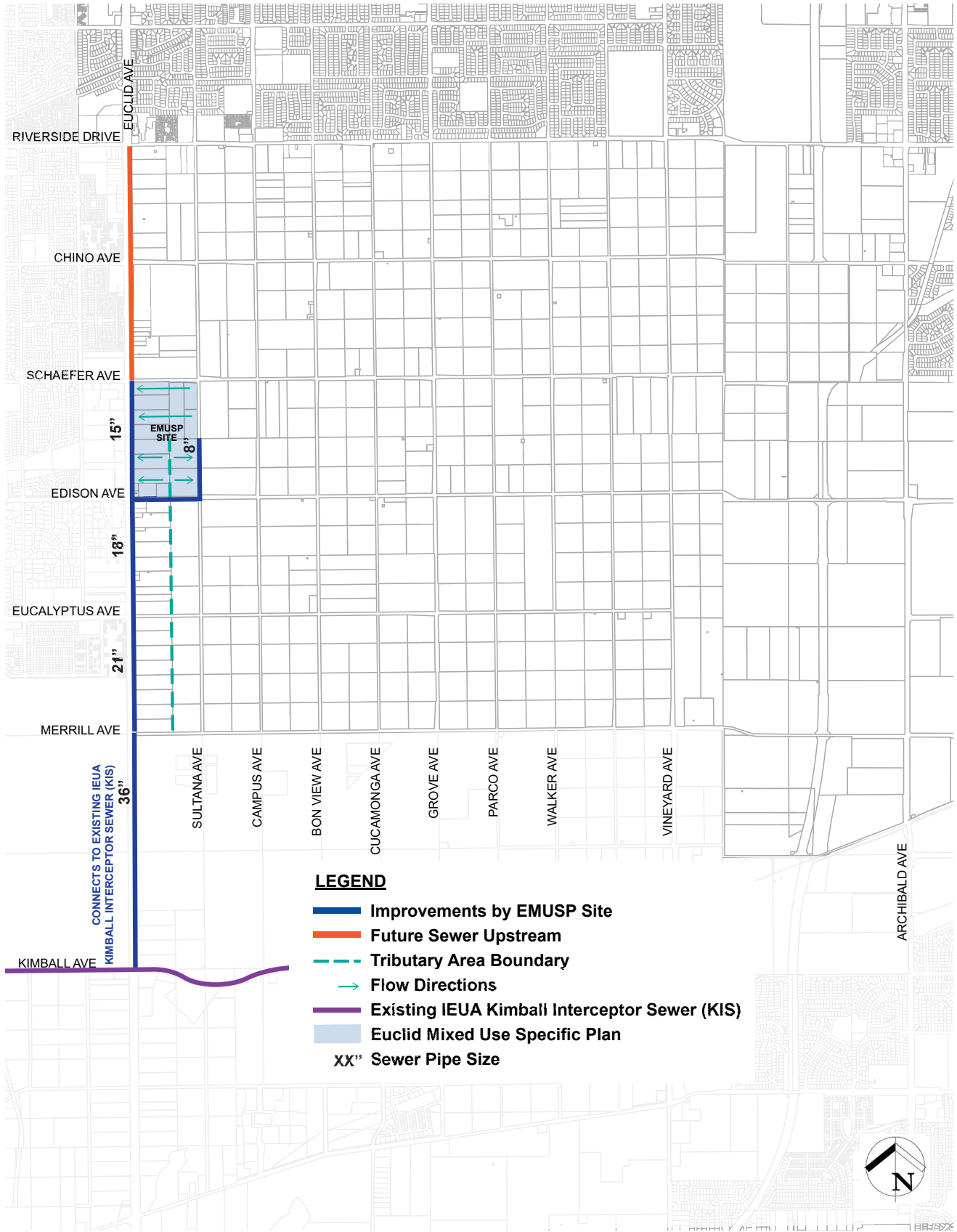


FIGURE 3.15 CITY OF ONTARIO ULTIMATE SEWER SYSTEM

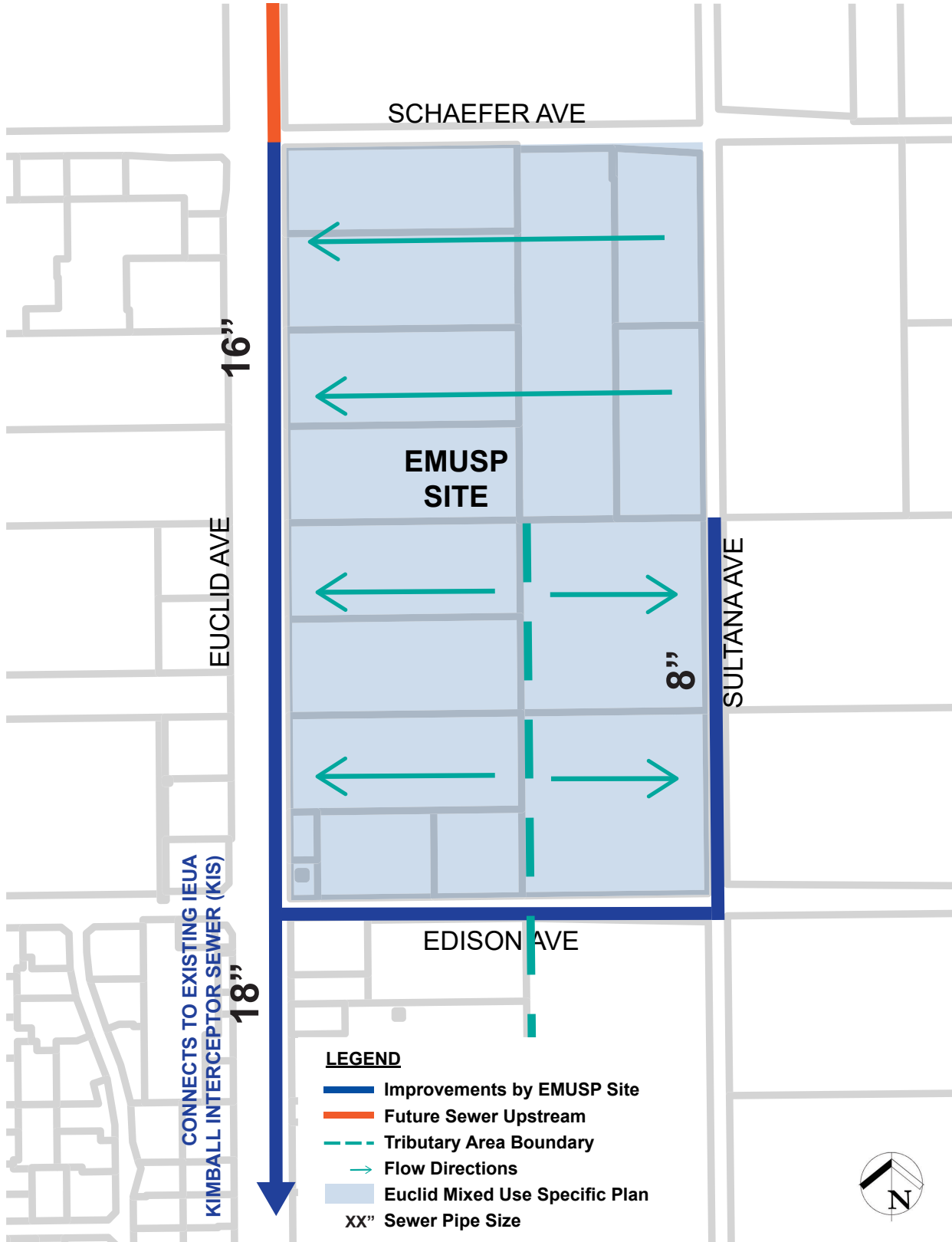


FIGURE 3.16 SPECIFIC PLAN SEWER PLAN



3.6 Conceptual Grading Plan

Site topography is moderately flat, sloping from the north to the south. There is an approximately 25 ft. elevation change across the Specific Plan area.

The grading activities for the Specific Plan area will generally consist of clearing and grubbing, demolition of existing structures, and moving surface soils to construct building pads, driveways and streets.

A Conceptual Grading Plan will be prepared in conjunction with future subdivision mapping and approvals. The grading plan will provide a balance of cut and fill for the Specific Plan area. Grading plans for each development project within the Specific Plan area will be reviewed and approved by the City of Ontario prior to the issuance of grading permits. Grading plans and activities will conform to the City's grading ordinance and dust and erosion control requirements.

All landscaped areas within the Specific Plan area shall be designed to accept runoff water from impervious surfaces. Landscape slopes shall not be greater than 3:1 on all perimeter slopes, including within neighborhood edge areas and slopes over 5 ft. in height.

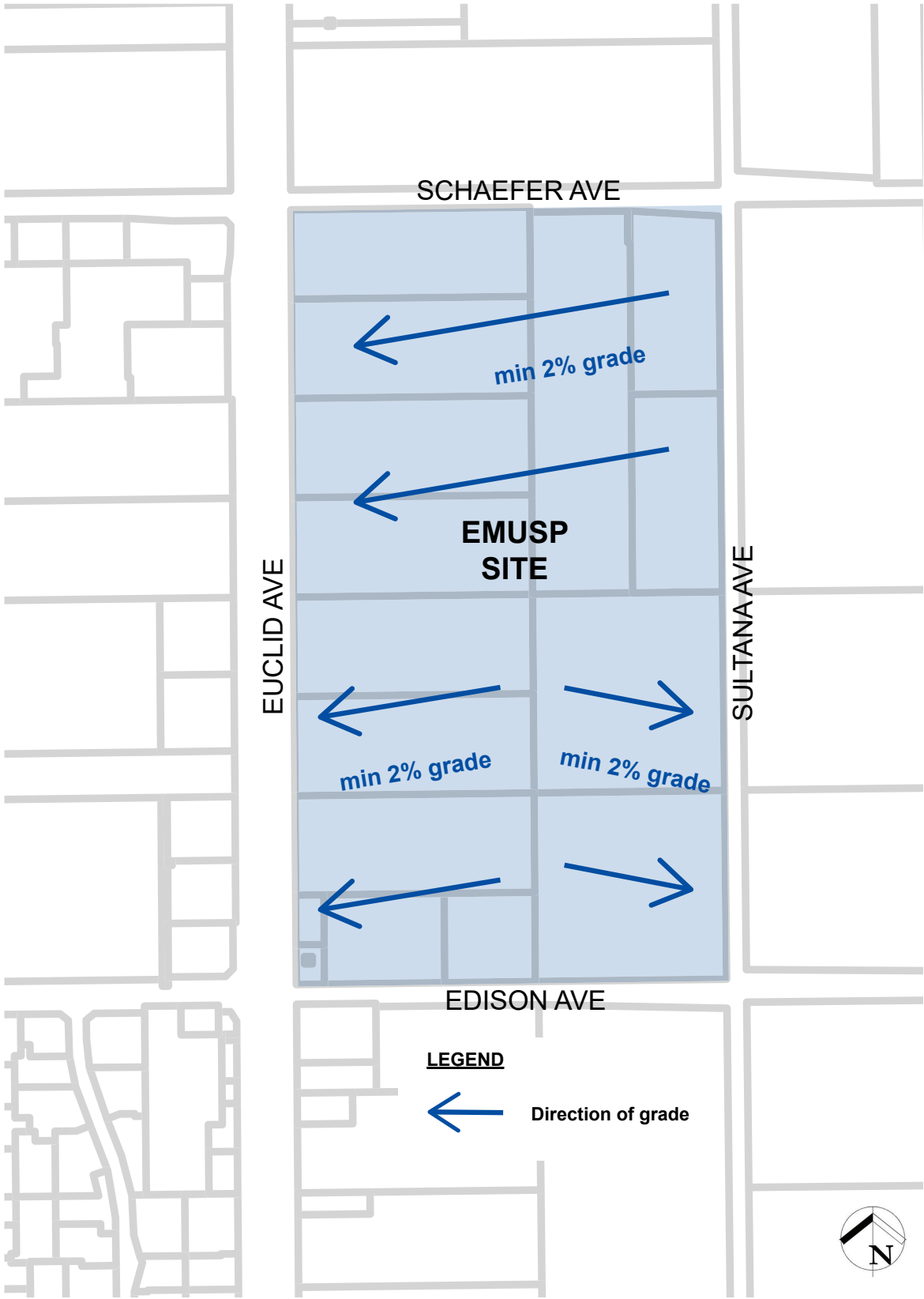


FIGURE 3.17 SPECIFIC PLAN CONCEPTUAL GRADING PLAN

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3.7 Dry Utilities Plan

Utility services provided to the site will be installed underground in accordance with City of Ontario guidelines.

3.7.1 Communication System

Developments in Ontario Ranch are required to install and provide fiber conduit to all improved lots. Proposed on-site facilities will be placed underground within a duct and structure system that will be installed by the developer. Pursuant to the City of Ontario 2013 Fiber Optic Master Plan, the fiber optic network will be owned and operated by the City of Ontario and as such maintenance of the installed system will be the responsibility of the City and/or Special District fiber optic entity and not the developer. According to the City's Fiber Optic Master Plan, the proposed fiber optic infrastructure, including approximately 23 miles of backbone fiber south of Riverside Drive, is an investment into a long term capital asset using newly constructed and existing conduit to provide high speed communication links to key locations throughout the City. The Specific Plan area will be connected to the City's system as shown on **Figure 3.18**.

3.7.2 Natural Gas

The Gas Company will provide natural gas to the Specific Plan area. Gas mains will be installed to the individual development projects by the Gas Company, as necessary.

3.7.3 Electricity

Southern California Edison will provide electricity to the Specific Plan area from existing facilities in the vicinity. All new lines within the Specific Plan area shall be installed according to City of Ontario requirements.

Undergrounding of existing overhead power lines will be subject to Section 7-7 of the Ontario Municipal Code. Existing power poles along Edison Avenue will need to be relocated. The existing location is within the ultimate roadway.

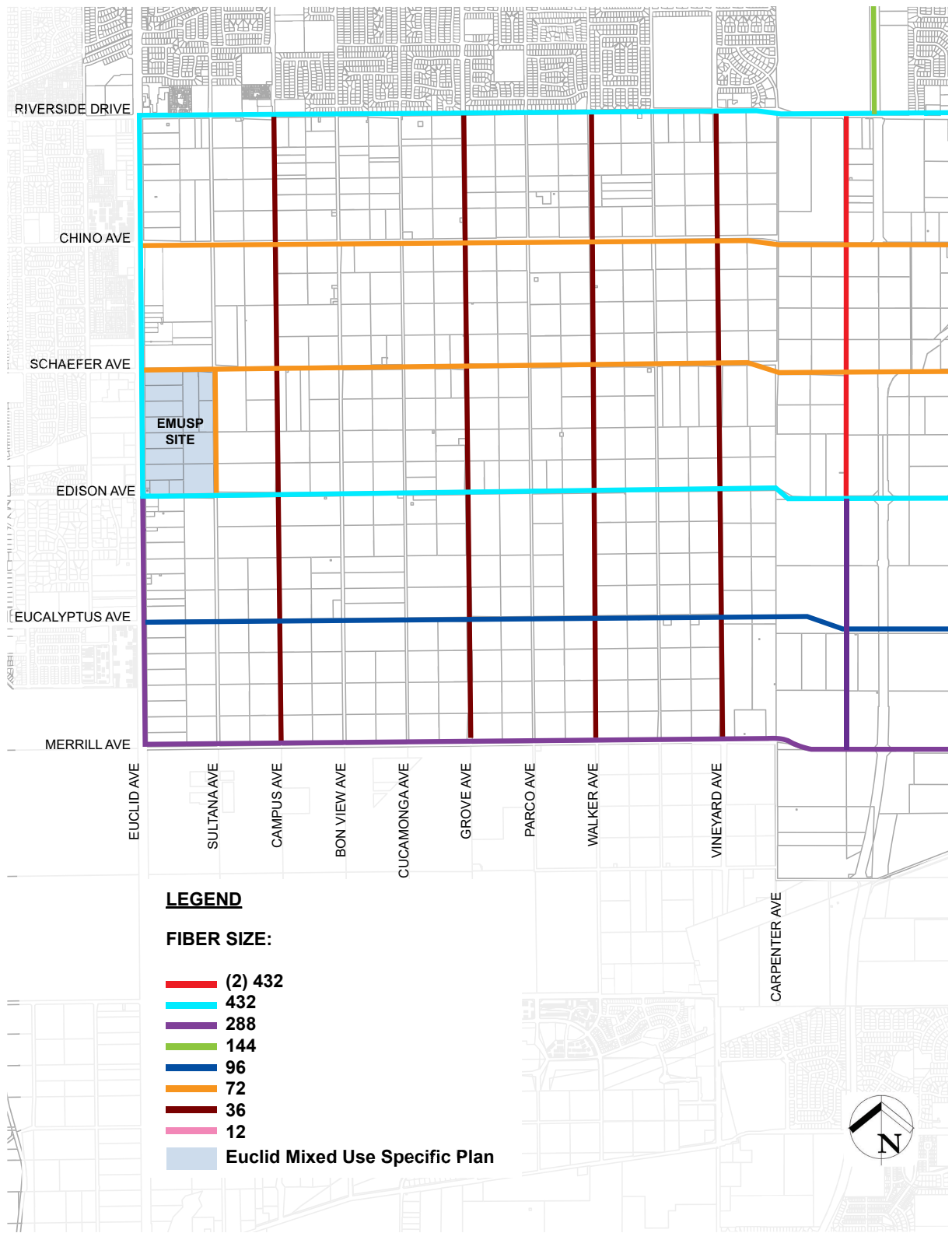


FIGURE 3.18 CITY OF ONTARIO ULTIMATE FIBER OPTICAL SYSTEM

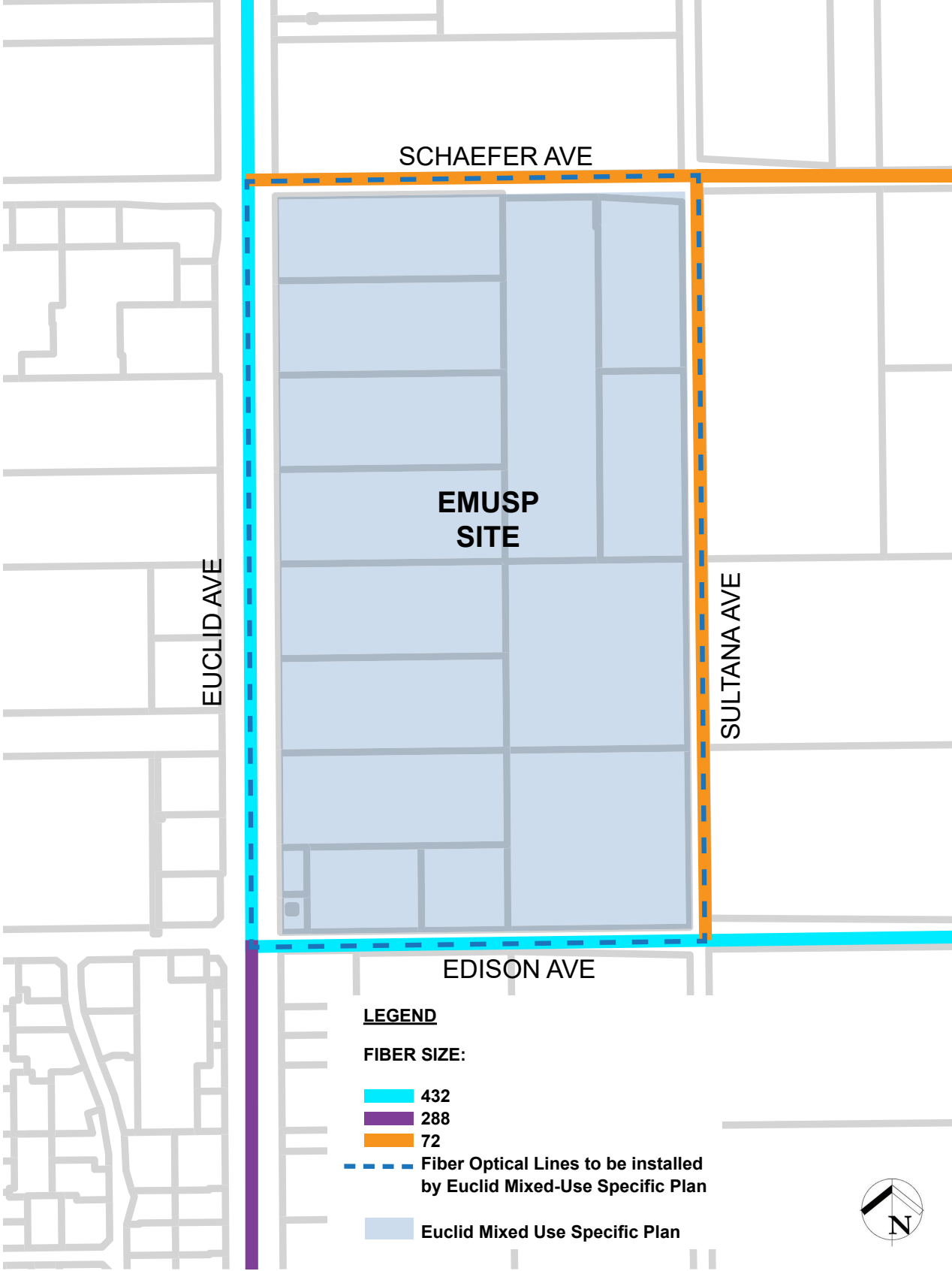


FIGURE 3.19 SPECIFIC PLAN FIBER OPTICAL PLAN

3.8 Storm Drain Plan

Storm Drain systems to serve the Specific Plan area will be installed according to the City of Ontario Storm Drain Master Plan. As of the establishment of the Specific Plan, there are no City domestic storm drain trunk lines in the vicinity of the Specific Plan area. The extension of City master planned storm drain infrastructure is being developed within the western portion of Ontario Ranch. The Euclid Mixed-Use Specific Plan is responsible to provide storm drain to serve future development and will construct a 90" line in Euclid along the western perimeter of the Specific Plan, a 48" to 90" line in Schaefer Avenue along the northern perimeter of the Specific Plan and a 78' to 96" in Edison Avenue along the southern perimeter of the Specific Plan. The Specific Plan will construct a 102" trunk line in Euclid Avenue south to connect to the existing 108" line at Euclid Avenue and Eucalyptus Avenue.

Catch basins located throughout the site will collect runoff. On-site storm drain systems serving the Specific Plan will connect to the master planned system in Euclid Avenue to serve the Specific Plan development. The Specific Plan area storm drain improvements are shown in **Figure 3.20**.

3.8.1 NPDES Compliance

The grading and drainage of the Specific Plan area will be designed to retain/filter, harvest, and reuse or treat surface runoff to comply with the current requirements of the San Bernardino County NPDES Stormwater Program's Water Quality Management Plan (WQMP) for significant new development projects. Water quality impacts may be minimized through the implementation of site designs that reduce runoff and pollutant transport by minimizing impervious surfaces and maximizing on-site infiltration, employing Source Control Best Management Practices (BMP's), or using on-site structural Treatment Control BMP's where the infeasibility of installing Low Impact Development BMP's is demonstrated.

New development within the Specific Plan area will utilize a variety of Low Impact Development site drainage designs to manage stormwater, including but not limited to retention/filtration basins, trenches and swales, and above ground bio-retention systems. Development projects within the Specific Plan area will incorporate features including but not limited to:

- Landscape designs that promote water retention and incorporation of water conservation elements such as use of native plants and drip irrigation systems;
- Permeable surface designs in areas with low traffic;
- Parking lots that drain to landscaped areas to provide retention and infiltration, or bio-treatment where infiltration is infeasible; and
- Limit soil compaction during grading operations within landscaped storm water infiltration areas to no more than 80 percent compaction.

Prior to the issuance of a grading or construction permit, a Storm Water Pollution Prevention Plan (SWPPP), Erosion & Sediment Control Plan sheets, and a WQMP will be prepared and approved. The SWPPP and Erosion & Sediment Control Plan Sheets will identify and detail all appropriate BMP's to be implemented or installed during construction of the project, and the WQMP will describe all post-construction BMP's designed to address water quality and quantity of runoff for the life of the project.

All Priority Land Use (PLU) areas within the Specific Plan Area shall comply with the statewide Trash Provisions adopted by the State Water Resources Control Board (SWRCB) and trash requirements in the most current San Bernardino County Area-Wide MS4 Permit. Priority Land Use (PLU) includes high-density residential (defined as a land use with at least ten (10) dwelling units per acre), industrial, commercial, mixed urban, and public transportation station. Drainage from the PLU shall be designed with conveyance tributary to a sub-regional Full Trash Capture System such as a Debris Separating Baffle Box (DSBB), Continuous Deflective System (CDS), or an equivalent hydrodynamic separator which has been approved by the SWRCB.

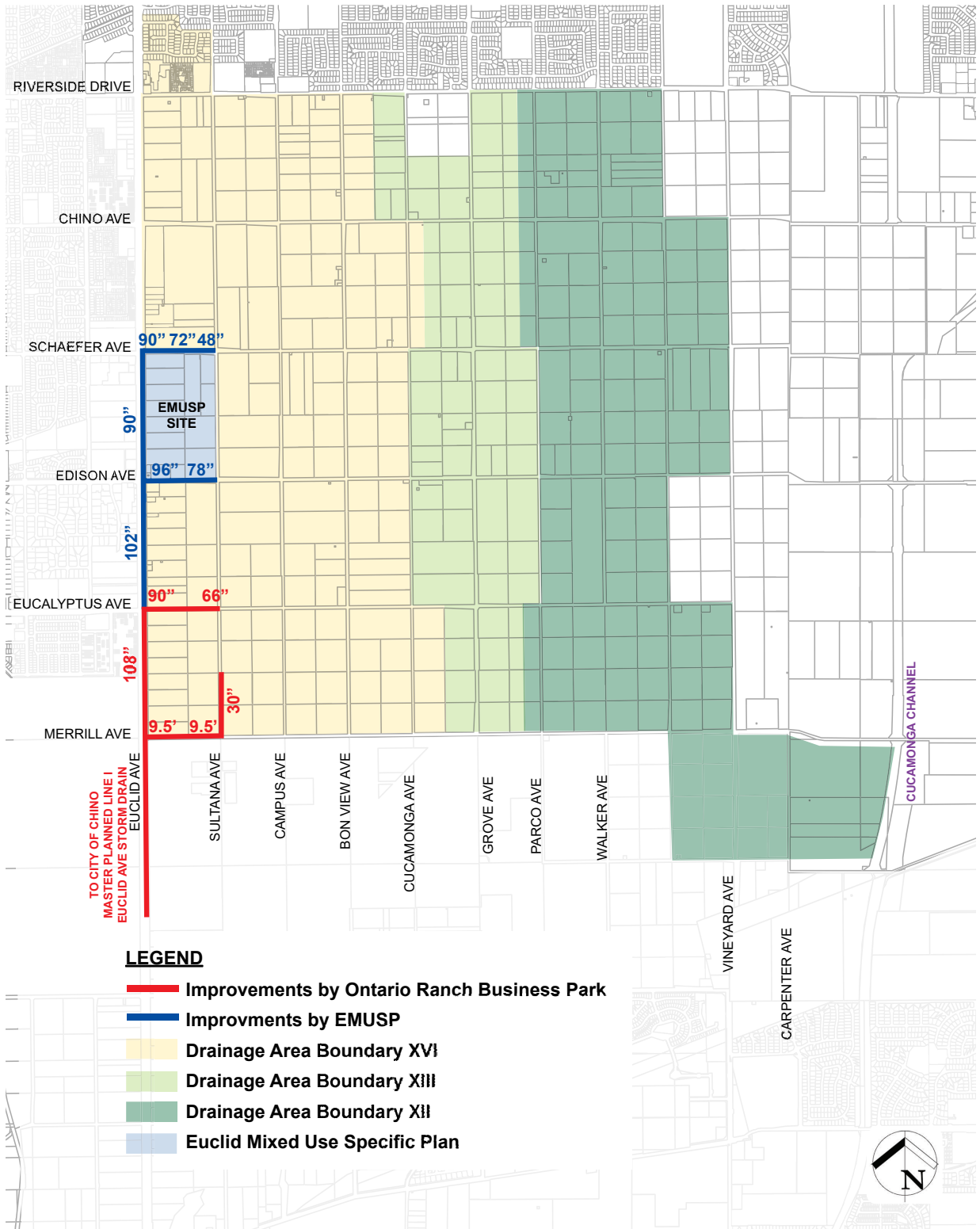


FIGURE 3.20 CITY OF ONTARIO ULTIMATE STORM DRAIN SYSTEM

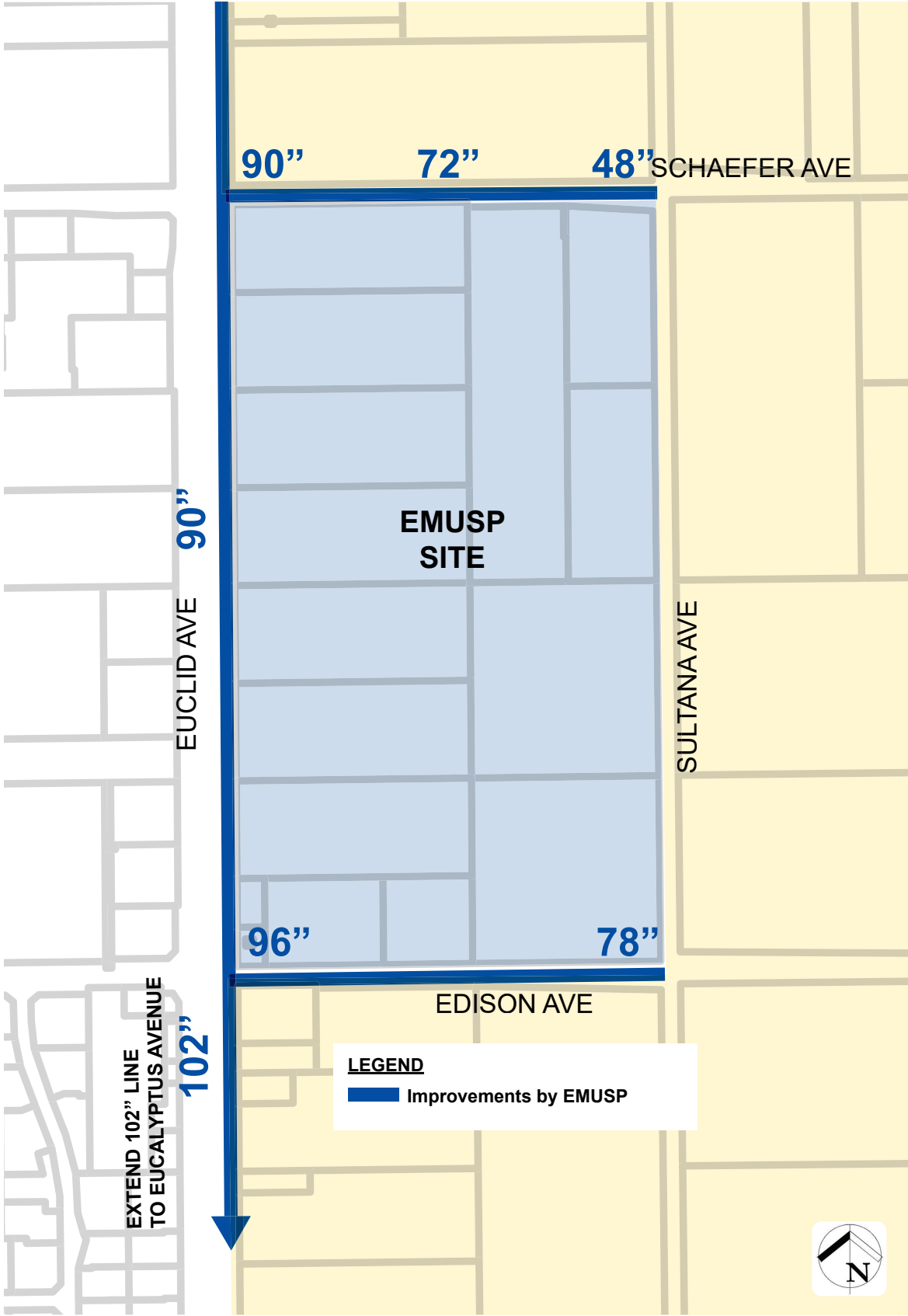


FIGURE 3.21 SPECIFIC PLAN STORM DRAIN PLAN

3.9 Public Services

Public services within the Specific Plan area, including police, fire and solid waste disposal services will be provided as follows:

3.9.1 Police

The City of Ontario will provide police services to the Specific Plan area. The closest police station is located approximately four miles north of the Specific Plan area at 2500 S. Archibald Avenue, just south of SR-60. This station is also the City of Ontario Police Department headquarters.

3.9.2 Fire

The City of Ontario will provide fire protection services to the Specific Plan area. The Ontario Fire Department currently has ten stations, which are staffed with eight four-man paramedic engine companies and two four-man truck companies. The closest operational fire station, Station 2, is located at 544 W. Francis Street, approximately three miles north of the Specific Plan area.

3.9.3 Solid Waste Disposal

The City of Ontario will provide solid waste services to the Specific Plan area. Solid waste facilities will follow the “Solid Waste Department Refuse and Recycling Planning Manual.” The Manual establishes the City of Ontario’s requirements for refuse and recycling storage and access for service, as well as addresses the City’s recycling goals. The Mid-Valley Landfill is the nearest County of San Bernardino landfill located at 2390 N. Alder Avenue in the City of Rialto, approximately 20 miles northeast of the Specific Plan area.

3.10 Phasing Plan

Development phasing of the project site will be determined by the landowner and/or developer based upon real estate market conditions. Phasing will occur as appropriate levels of infrastructure are provided. Phasing sequencing is subject to change over time to respond to various market and local factors and as such, individual phases may overlap or develop concurrently. Infrastructure improvements, as required and approved by the City Engineer to support the development, will be installed by the developer. **Figure 3.22**, Conceptual Phasing Plan, describes the general phases of development anticipated for the Euclid Mixed-Use Specific Plan area.

Specific Plan backbone infrastructure will be installed by the project developer, in accordance with the applicable City adopted infrastructure plan for the area, as well as the provisions of this Specific Plan and an approved Development Agreement. Fair share responsibilities for improvements will be addressed in a Development Agreement with the City of Ontario. The timing for installation of infrastructure and utilities within the Specific Plan area will be determined as part of the City’s approval of future subdivision maps and development plans. Infrastructure will be constructed and made available in a timely manner as development progresses. Phasing of required infrastructure will be determined per separate Development Agreement.

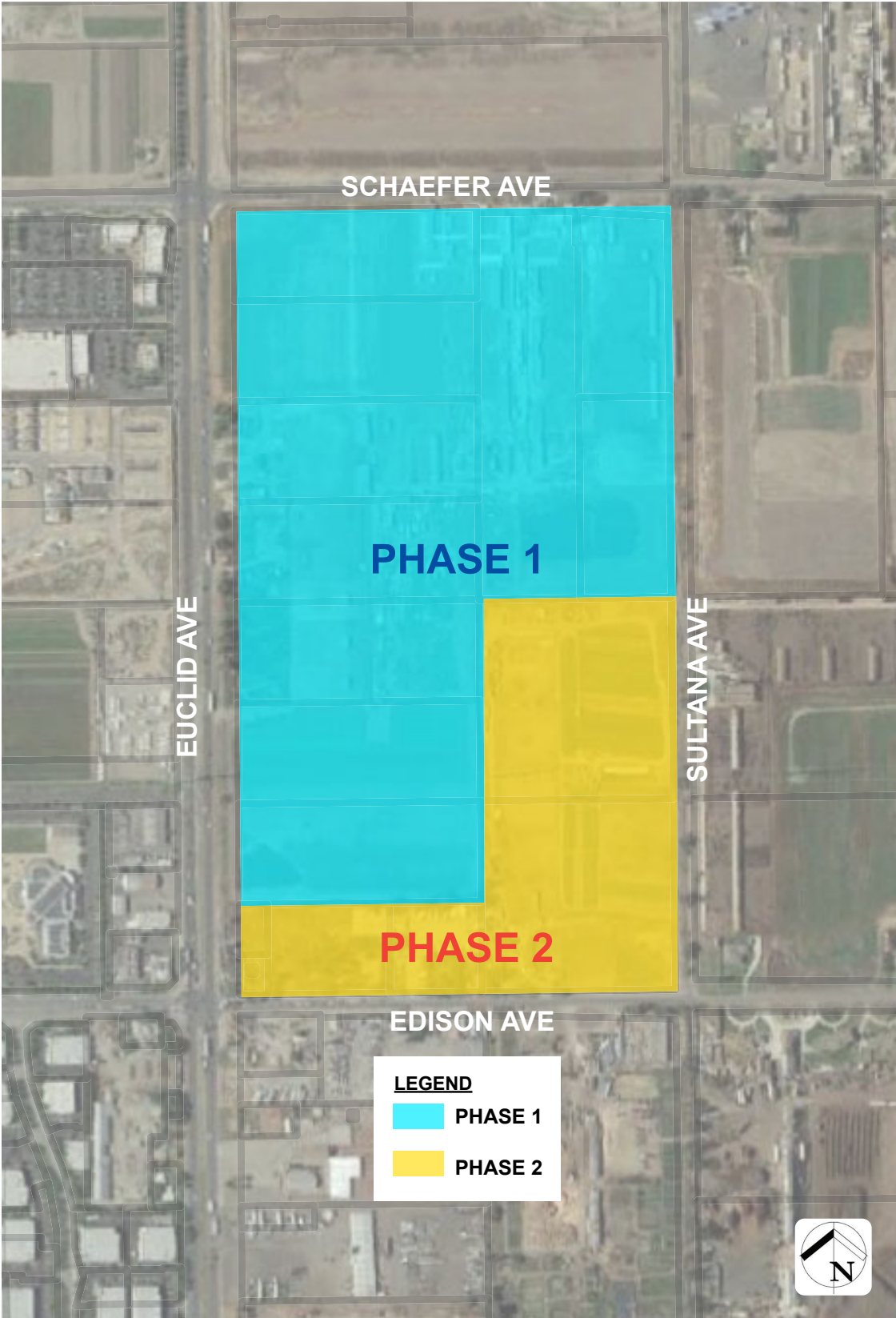


FIGURE 3.22 SPECIFIC PLAN PHASING PLAN

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