

DOWNTOWN

CIVIC CENTER

PLANNED UNIT DEVELOPMENT



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City of Ontario Planning Department
303 East B Street
Ontario, California 91764
Ph: (909) 395-2036 / Fax: (909) 395-2420
www.ontarioca.gov

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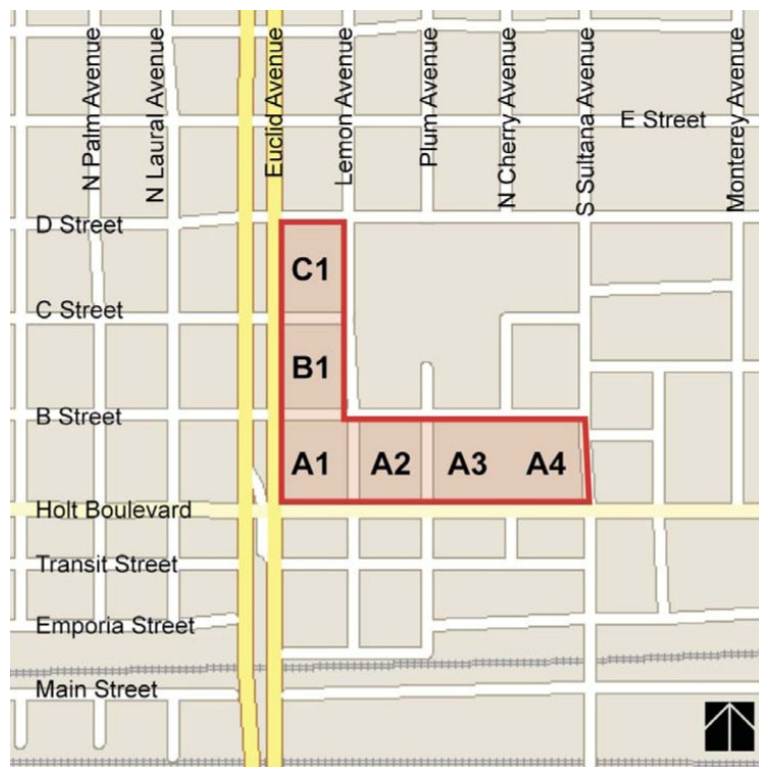
1.0 INTRODUCTION

The Downtown Civic Center Planned Unit Development (PUD) is intended to function as a set of planning and design principles, development regulations, and performance standards to guide and govern the development of a 6-block area of the City of Ontario, which consists of a portion of the City’s original downtown and its current civic center area (see Exhibit 1-1, Project Location Map). This PUD will facilitate the development of a mixture of high-density attached housing units, restaurants, retail shops, commercial services, and a public commons area that will help achieve the City’s goals to economically revitalize and aesthetically enhance the project area.

This document is organized to fulfill the requirements for a PUD, as set forth in the Ontario Development Code. As such, this PUD will replace the existing zone district designations and zoning standards that apply to the affected properties. Unless otherwise defined herein, definitions and interpretations contained in the Development Code shall apply.

City staff and private developers will rely upon this PUD to determine whether precise plans for development (“Development Plans”) will adequately meet the City’s land use and design objectives for this key part of Ontario’s original downtown. The objectives have been defined over the last 20+ years in the Redevelopment Plan for the Center City Redevelopment Project (1983), Ontario General Plan (1992), Downtown Civic Center Project EIR (2004), and The Ontario Plan (2010) and are summarized below.

Exhibit 1-1: Project Location Map



1.1 GENERAL PLAN OBJECTIVES

- 1.1.1 The Ontario Plan (TOP) defines the project area as a Policy Growth Area named the “Historic Downtown and Civic Center.” This area is the historic heart of Ontario and is a unique blend of historic, social and cultural uses set in a compact street grid. It includes our Civic Center, Library, diverse residential neighborhoods, and retail opportunities along Euclid Avenue, Holt Boulevard and B Street. The Downtown Civic Center PUD is consistent with the goals and policies of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. The goals and policies furthered by the PUD and objectives set forth in the Center City Redevelopment Plan, both summarized below. The Ontario Plan Goals & Policies

Land Use

Goal—LU1 A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.

Policy—LU1-1 *Strategic Growth.* We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.

Community Design

Goal—CD1 A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

Policy—CD1-1 *City Identity.* We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

Policy—CD1-2 *Growth Areas.* We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

Goal—CD2 A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

Policy—CD2-1 *Quality Architecture.* We encourage all development projects to convey visual interest and character through:

- building volume, massing, and height to provide appropriate scale and proportion;
- a true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

Policy—CD2-5 *Streetscapes.* We design new and, when necessary, retrofit existing streets to improve walkability, bicycling and transit integration, strengthen connectivity, and enhance community identity through improvements to the public right of way such as sidewalks, street trees, parkways, curbs, street lighting and street furniture.

Policy—CD2-8 *Safe Design.* We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

Goal—CD3 Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

Policy—CD3-1 *Design.* We require that pedestrian, vehicular, bicycle and equestrian circulation (where appropriate) on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

Goal—CD4 Historic buildings, streets, landscapes and neighborhoods, as well as the story of Ontario’s people, businesses, and social and community organizations, that have been preserved and serve as a focal point for civic pride and identity.

Policy—CD4-2 *Collaboration with Property Owners and Developers.* We educate and collaborate with property owners and developers to implement strategies and best practices that preserve the character of our historic buildings, streetscapes and unique neighborhoods.

Housing

Goal—H2 Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

Policy—H2-1 *Corridor Housing.* We revitalize transportation corridors by encouraging the production of higher density residential and mixed-uses that are architecturally, functionally and aesthetically suited to corridors.

Policy—H2-2 *Historic Downtown.* We foster a vibrant historic downtown through facilitating a wide range of housing types and affordability levels for households of all ages, housing preferences, and income levels.

Policy—H2-5 *Housing Design.* We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.

Policy—H2-6 *Infill Development.* We support the revitalization of neighborhoods through the construction of higher-density residential developments on underutilized residential and commercial sites.

Goal—H4 Increased opportunities for low and moderate-income households and families to afford and maintain quality ownership and rental housing opportunities, including move-up opportunities.

Policy—H4-4 *Mixed-income Housing.* We encourage the integration of affordable housing in the New Model Colony, Ontario Airport Metro Center Area, and existing neighborhoods.

Community Economics

Goal—CE1 A complete community that provides for all incomes and stages of life

Policy—CE1-6 *Diversity of Housing.* We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

Policy—CE1-7 *Retail Goods and Services.* We seek to ensure a mix of retail businesses that provide the full continuum of goods and services for the community.

1.1.2 CENTER CITY REDEVELOPMENT PLAN OBJECTIVES

- Encourage development of a high intensity, multi-use central business district and surrounding neighborhoods that maximize the economic productivity of the commercial areas and maximize the housing opportunities of the residential areas.
- Apply innovative mixed-use urban design that maintains a flexible approach to allow for changing opportunities over a long-term, phased revitalization effort.
- Create a healthy and exciting urban environment, with the ability to work, live, shop and play within a small area, combine daytime and nighttime use and conserve energy and resources through mixed-use development
- Cluster activity centers within walking distance of each other, supported by a pedestrian network that provides an enjoyable pedestrian flow

1.2 DOWNTOWN CIVIC CENTER PROJECT OBJECTIVES

- Revitalize the downtown area and enhance its economic growth by creating a mixed-use neighborhood with a mixture of housing, retail, academic and office uses within a historic downtown setting
- Develop high quality, mixed-use housing, consisting of market rate and affordable multiple- family, senior housing, offices, academic classrooms and retail
- Establish appropriate relationships among new residential neighborhoods as well as with existing adjacent land use
- Provide for a circulation network which promotes pedestrian walkways and bicycle activity as alternative modes of travel while also providing for safe and efficient movement of automobile travel through the project site
- Ensure that development addresses the Ontario General Plan and Redevelopment Plan for the Center City Redevelopment Project policies and objectives

2.0 LAND USE PLAN

Four land use districts are established for the project limits, as shown in Exhibit 2-1, Land Use Plan, and as described below. Placement of buildings, structures, fences, walls, utility facilities, perimeter yards, etc. will be based on the street rights-of-way and property line dimensions shown in Exhibits 2-2 through 2-6.

2.1 COMMERCIAL/RESIDENTIAL MIXED USE (BLOCKS A1 AND C1)

Blocks A1 and C1 will be developed with vertically mixed commercial and residential uses, in a mid-rise configuration, with commercial uses on the ground floor level, and attached, single level housing units on 3 upper levels. Structured parking will serve commercial and residential uses, with a subterranean parking level or upper levels of a parking structure facility for residents and ground-level parking for the commercial uses. Ground-level spaces along Euclid and Holt Avenues will provide opportunities for specialty shopping, eating/drinking, and limited personal and business services that cater to local residents and local employers/employees, which will draw from pedestrian traffic along Euclid Avenue and the adjacent streets in the Civic Center area. A satellite police station may also be located in a ground-level space within either block area. Because of the vertical integration of housing over commercial, and the potential for conflicts involving noise and intrusion on the residential environment, entertainment venues such as standalone bars and nightclubs will not be permitted. Refer to Chapter 5, Zoning and Land Use, of the Ontario Development Code, for additional land use restrictions and protection corridors.

The residential units will be designed to accommodate a variety of households that prefer a low-maintenance, urban lifestyle, within a pedestrian-friendly environment, and conveniently located shopping, dining, and personal services. As transit linkages improve in the downtown area, these units may attract residents that are reliant upon the regional transit system to commute to jobs outside of the Ontario/Western San Bernardino County area.

2.1.1 PERMITTED USES

- Refer to Table 2-1 for a list of the uses permitted by right, by Conditional Use Permit, and as ancillary to an on-site permitted use.
- Home occupations within residential units, in accordance with the Ontario Development Code
 - Small household pets, with a maximum combination of 2 cats, dogs, or other small domesticated animals as may be allowed by the Ontario Development Code
 - Structured parking

*Table 2.1: Permitted Land Uses in Blocks A1 and C1*Legend:

- “P”: Means the use is permitted by right of being in the correct land use district.
- “C”: Means the use is allowed in the land use district, subject to the filing and approval of a Conditional Use Permit in accordance with the Ontario Development Code, prior to the establishment of the land use.
- “A”: Means the use is permitted by right of being in the correct land use district, provided it is subordinate to the primary land use.
- “AP”: Means administratively permitted land use, activity, or facility within the specified zoning district is subject to the granting of an Administrative Use Permit in accordance with the Ontario Development Code, prior to the establishment of the land use.

Uses	Permitted/Conditionally Permitted Uses
Residential (Ground Floor and Above)	
Apartments, Condominiums, Duplexes, and Other Multiple-Family Dwellings (includes “for sale” and “for rent” dwellings)	P
Senior Housing	P
Institutional (Ground Floor Only)	
Medical	
Pharmacies and Drug Stores	P
Public/Recreational/Cultural Facilities	
Cultural Facility	P
Museum	P
Government Offices	P
Other Government Facilities	C
Library	P
Police/Fire Station	P
Police Storefront/Substation	P
Recreation Center	P
Senior Citizen Center	P
Church	C
Non-Profit/Service Organizations	
Campaign Offices	P
Charitable, Philanthropic, Service, and other Nonprofit Organization Offices	P
Commercial	
Alcohol	
Alcoholic Beverage Sales	AP/C

Uses	Permitted/Conditionally Permitted Uses
Automobile Related Services	
Parking: Commercial Lot/Garage	C
Parking: Publicly Owned Facilities	P
Communications	
Radio and Television Broadcasting Studios (prohibited on ground/first floor of buildings located along the Euclid Avenue Protection Corridor)	C
Satellite Dishes/Ham Radio Antennas and other facilities (for private use only, must be screened from public view)	P
Eating/Drinking Places & Food Services	
Restaurants (sit down/full service)	P
Outdoor Dining	A
Entertainment and Recreation	
Auditoriums & Other Public Assembly Facilities	C
Dance Hall/Studio (instructional)	P
Health Club/Gymnasium (prohibited on ground/first floor of buildings located along the Euclid Avenue Protection Corridor)	C
Health Club/Gymnasium if less than 10,000 sq. ft. (prohibited on ground/first floor of buildings located along the Euclid Avenue Protection Corridor)	P
Live Entertainment (Non-Amplified)	See Table 5.02-1 Land Use Matrix in ODC
Recreational Facilities (as part of a residential development)	A
Theaters	C
Lodging	
Bed and Breakfast	C
Non-Medical Offices	
Administrative, Professional, and Other Offices	P
Architectural, Engineering, and Related Services	P
Business Management Offices	P
Insurance Agents, Brokers, and Related Offices	P
Real Estate and Related Offices	P
Stock Brokers and Related Offices	P
Retail	
Antique Stores	P
Art Galleries and Art Supply Store	P
Beauty Supply Store	P
Book Stores	P
Camera and Photographic Supply Store	P
Clothing & Accessory Stores	P
Computer and Home Electronic Stores	P

Uses	Permitted/Conditionally Permitted Uses
Bakery (Retail)	P
Convenience Market	P
Delicatessen	P
Grocery Store/Super Market	P
Specialty Food Stores	P
Furniture and Home Furnishing Stores	P
Department Store	P
Hobby, Toy and Game Store	P
Jewelry Store	P
Luggage and Leather Goods	P
Music and Video Stores	P
Office Supply, Stationery & Gift Stores	P
Pet and Pet Supply Store	P
Shoe Store	P
Services	
Advertising Agency	P
Photocopying and Duplicating Services	P
Photography Studio	P
Banks, Credit Unions and other Depository Institutions	P
Banks, Credit Unions and other Depository Institutions with Drive-Through (drive-thru prohibited on Euclid Avenue and Holt Boulevard)	C
Check Cashing	A
Barber Shop & Beauty/Nail Salon	P
Tailor	P
Travel Agency	P
Jewelry and Watches/Clocks	P
Locksmith/Key Shop	P
Shoe Repair	P
Temporary and Interim Uses	
Parking Lot Sales	P
Street/Craft Fair and Farmers Markets	P

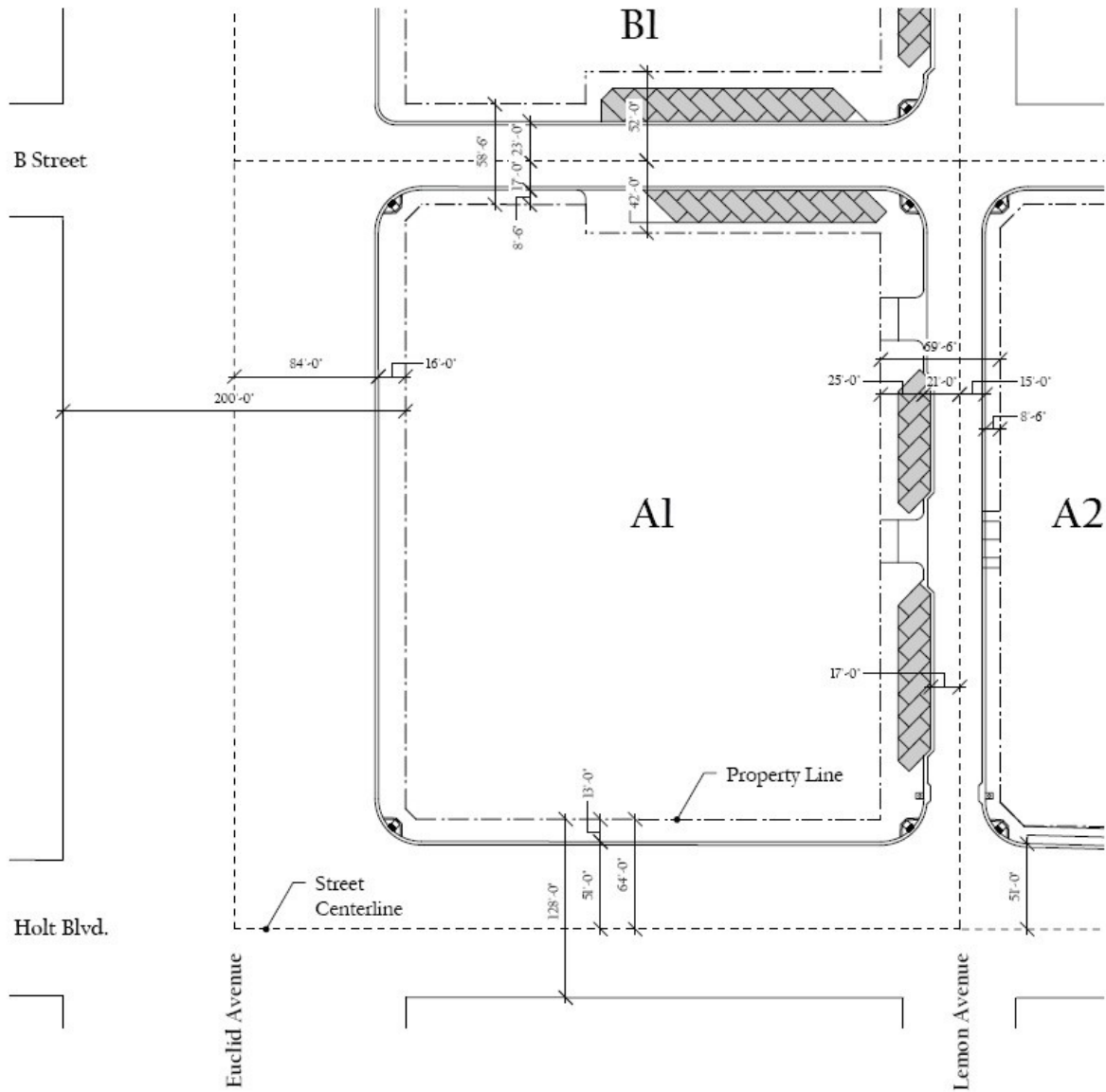


- Legend**
- TH Town Homes
 - APT Apartments
 - MU Mixed Use
 - CP Civic Plaza

Exhibit
Land Use Plan
Downtown Civic Center P.U.D. 2-1

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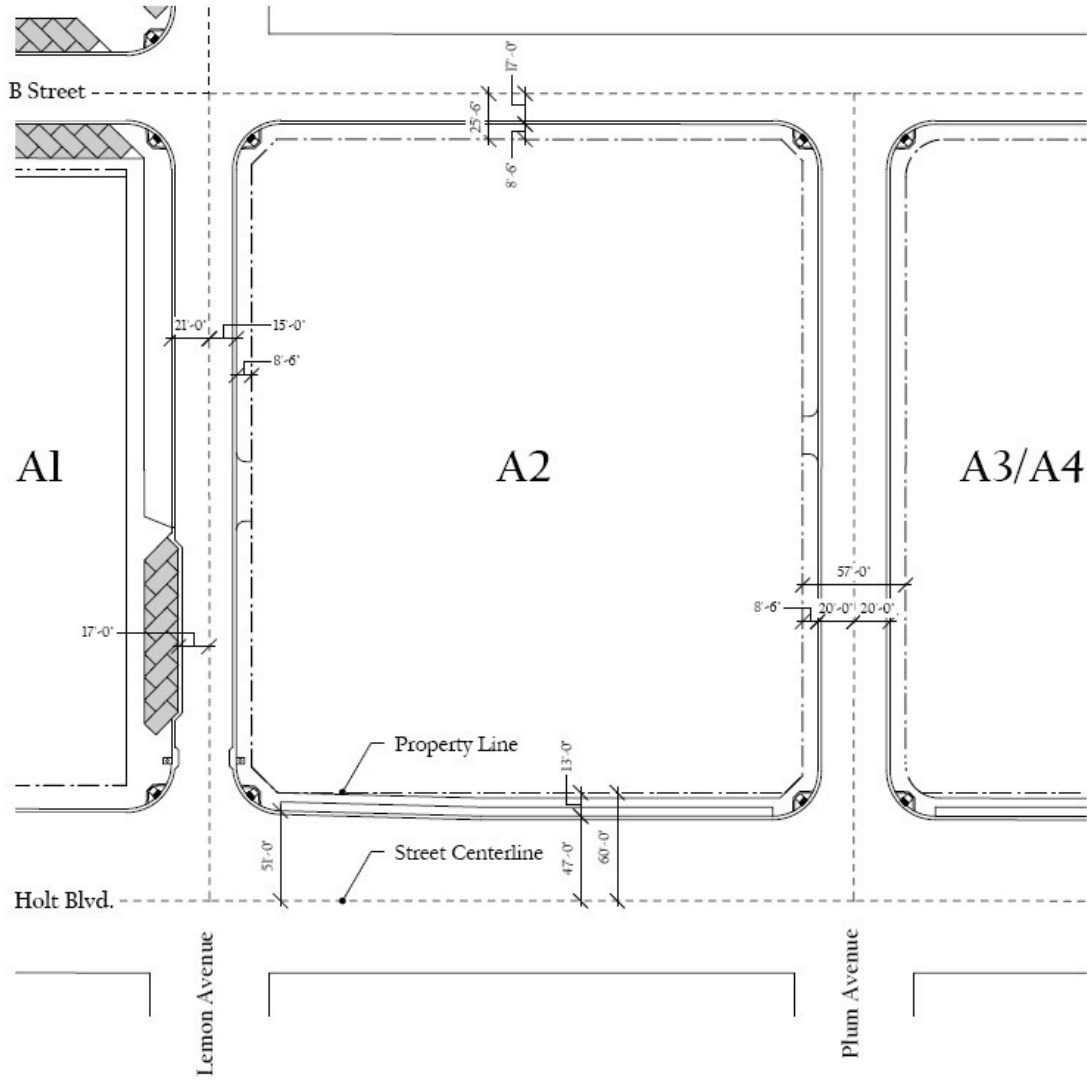
LEGEND

Conceptual angled parking layout




Street Rights of Way/Property Lines - Block A1
Downtown Civic Center P.U.D.

Exhibit
2-2



LEGEND

-  Conceptual angled parking layout



Street Rights of Way/Property Lines - Block A2
Downtown Civic Center P.U.D.

Exhibit

2-3

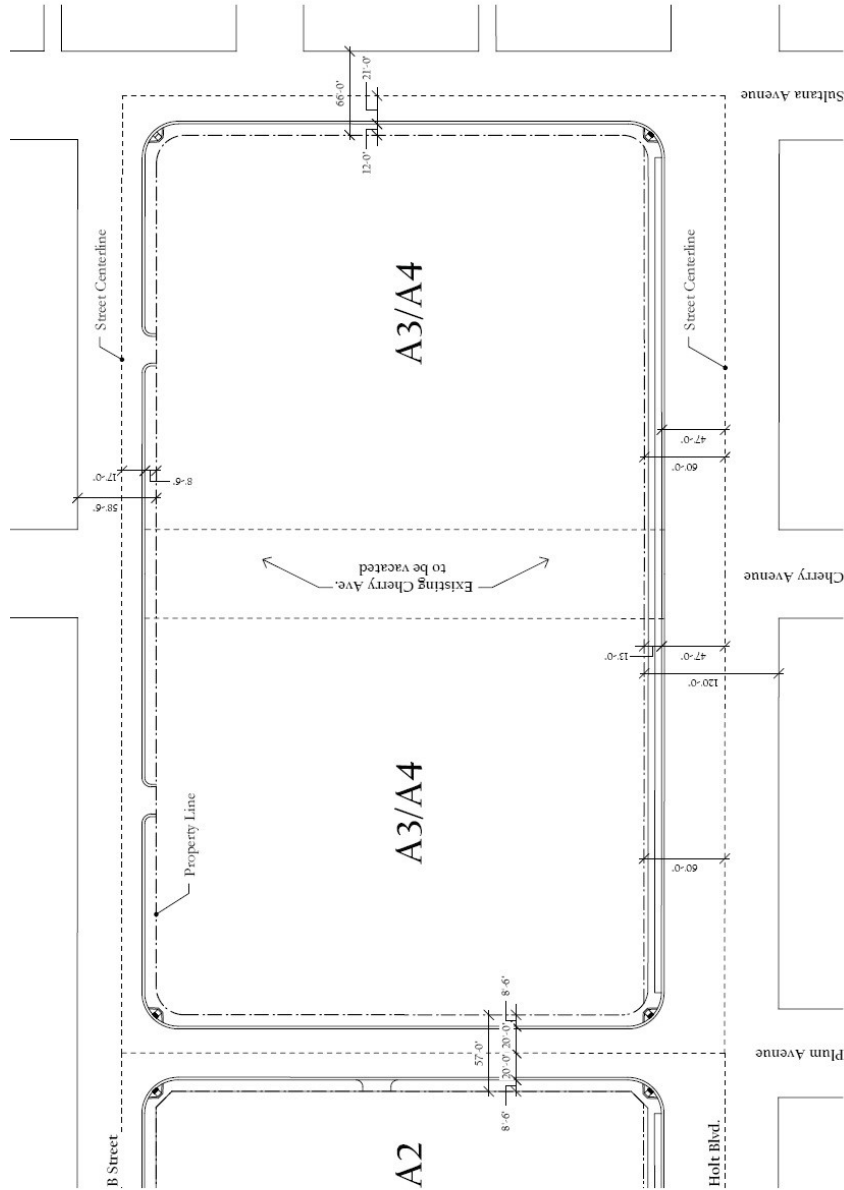
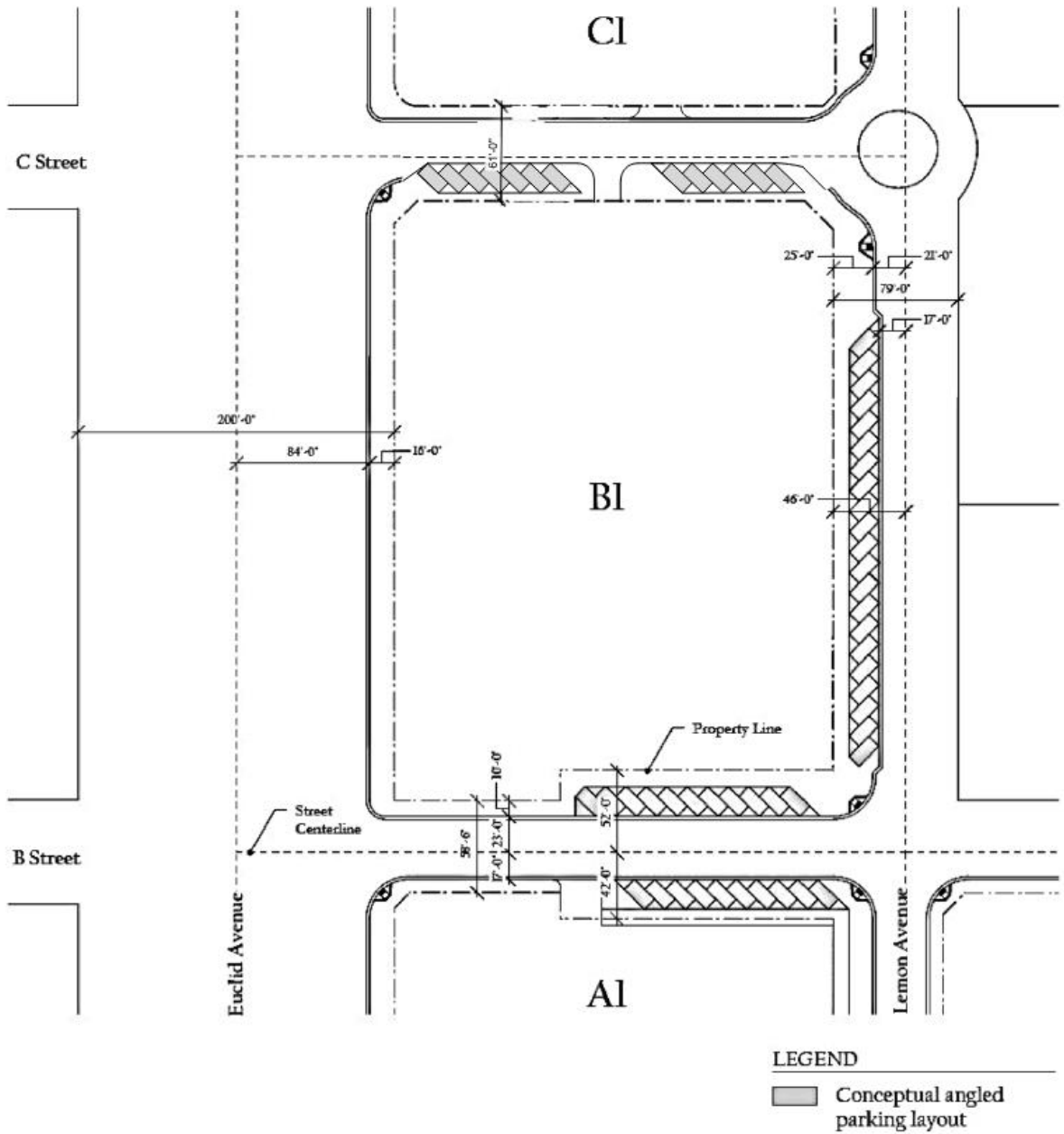


Exhibit
2-4

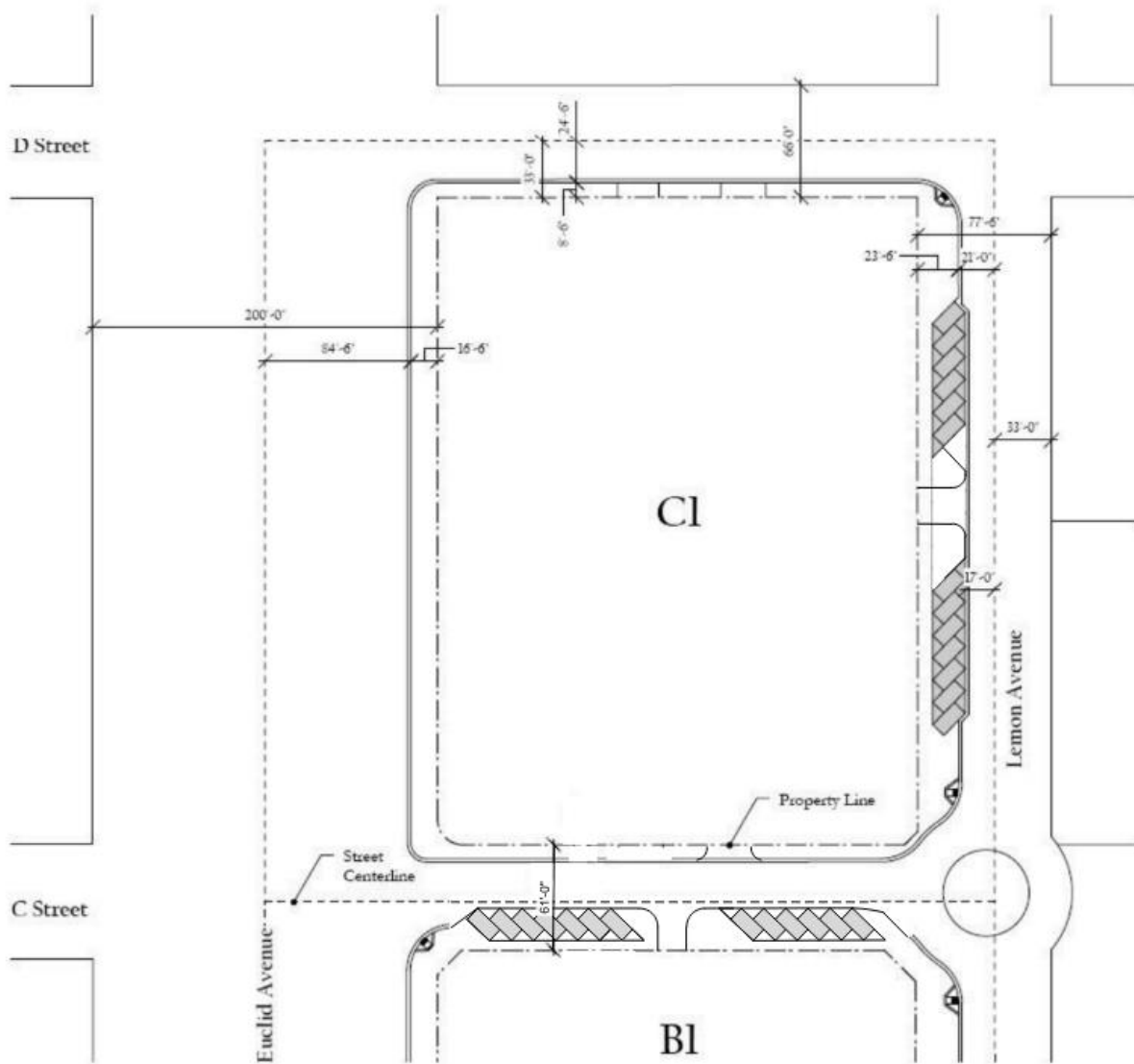
Street Rights of Way/Property Lines - Block A3/A4
Downtown Civic Center P.U.D.





Street Rights of Way/Property Lines - Block BI
Downtown Civic Center PUD.

Exhibit
2-5



LEGEND

▨ Conceptual angled parking layout



not to scale

Street Rights of Way/Property Lines - Block CI

Downtown Civic Center P.U.D.

Exhibit

2-6

2.1.2 INTENSITIES

- Total building area of commercial uses at Blocks A1 and C1 shall not exceed a 0.4 floor area ratio, based on the net site area
- Residential units in Block A1 shall be developed at a density of 45 to 65 dwelling units/acre
- Residential units in Block C1 shall be developed at a density of 45 to 65 dwelling units/acre
- The total floor area ratio (as defined in the Ontario Development Code) for all development in each block shall not exceed 2.7

2.1.3 SIMILAR USES

Other land uses may be allowed, provided that the Zoning Administrator or his or her designee makes a finding that such other use is similar to or no more objectionable than those uses listed in Table 2-1.

2.2 CIVIC PLAZA (BLOCK B1)

Block B1 will be developed as a public plaza area, with 2 restaurant/retail pads at the northwest and southwest corners. The open plaza will contain decorative hardscape and landscape elements, along with seating and possibly other pedestrian amenities, to provide passive recreation opportunities for downtown area employees and shoppers and function as a visual window and pedestrian linkage into the civic center area. The Bank of Italy building, located at the southwest corner of Block B1, is considered historically significant and will be preserved/rehabilitated to enable occupancy by a restaurant and/or other businesses targeting local workers, shoppers, and residents. A new building will be constructed at the northwest corner, also to provide space for businesses catering to local shopping, dining and possibly entertainment opportunities. Outdoor dining areas may be established adjacent to each of the commercial buildings. On-site surface parking will not be permitted unless a parking demand/supply study indicates there is insufficient on-street parking or other nearby public parking resources. This is intended to maintain the open character and public plaza functions of this block.

Pursuant to the Ontario Development Code, a Development Plan shall be submitted for approval of a site plan, landscaping, parking, building footprints, architectural style, materials and colors, signs, public amenities, street trees, streetlights and public furniture.

2.2.1 PERMITTED USES

- Restaurants only on the ground floor; however, sales and professional offices (excluding medical services) are permitted as interim uses. On the second floor, all commercial uses allowed in the Mixed-Use District are permitted
- Outdoor public recreation and civic activities
- Surface parking for public use

2.2.2 INTENSITIES

- Ground floor commercial uses shall not exceed a total of 10,000 square feet; second floor space shall not exceed 5,000 square feet
- Surface parking, if required, shall not cover more than 10 percent of the net site area

2.3 APARTMENTS (BLOCK A2)

This block will be developed with high quality, attached rental housing units, with 3 levels of housing over a partially underground parking structure that is screened to minimize visual impacts. Indoor and outdoor recreation amenities will be provided in one common area serving all residents. One and 2-bedroom floor plans will meet a variety of household lifestyle preferences. These homes will provide opportunities for people who work in the downtown area and cannot afford or prefer not to buy a home, to live within walking or biking distance of their jobs.

2.3.1 PERMITTED USES

- Multiple-family dwellings (i.e. apartments)
- Home occupations, in accordance with the Ontario Development Code
- Small, domesticated animal keeping, with a maximum combination of 2 cats, dogs and/or other small domesticated animals
- Outdoor and indoor recreation facilities and amenities for use by on-site residents and their guests
- Structured vehicle parking

2.3.2 INTENSITY

- Up to 65 dwelling units/acre, not to exceed a total of 160 units

2.4 TOWNHOMES (BLOCKS A3 AND A4)

Cherry Avenue will be vacated and Blocks A3 and A4 will be merged into a single development site. It will contain groupings of attractively designed, attached dwellings that will provide opportunities for work force housing in the downtown area. Two levels of living space will be built over ground floor garages, in a 3-story 'walk-up' configuration for all units. A ground-level common area will provide outdoor and indoor recreation amenities for all residents.

2.4.1 PERMITTED USES

- Multiple-family dwellings, with attached two-stall garages (includes "for sale" and "for rent" dwellings)
- Home occupations, in accordance with the Ontario Development Code

- Household pets, limited to a maximum of 3 animals
- Private and common outdoor and indoor recreation amenities for use by on-site residents and their guests
- Small family day care facilities, in accordance with the Ontario Development Code

2.4.2 INTENSITY

- 25-35 dwelling units/acre, not to exceed 140 total dwelling units

3.0 REGULATIONS FOR DEVELOPMENT

This section sets forth standards to govern the placement, height and bulk of permitted buildings and other structures, to establish locational criteria for vehicular access to development sites, to establish parking and loading requirements, to specify minimum requirements for recreational amenities for residential uses, outline sign standards for commercial uses and identify historic preservation features to be incorporated into Development Plans. Unless otherwise stated herein, all development within the project area shall meet the applicable standards and requirements of the Ontario Development Code.

3.1 COMMERCIAL/RESIDENTIAL MIXED USE (BLOCKS A1 AND C1)

3.1.1 BUILDING ENVELOPE

Commercial, public safety, and/or academic uses shall be on the ground floor level.

Residential units shall be located primarily on the second floor and higher levels. Residential units may be located on the ground floor behind commercial units which front Euclid Avenue.

Structured parking for residential uses shall be underground or on the upper levels of a parking structure facility, and parking for other uses shall be at ground level.

Total building height (to flat roofline) shall not exceed 50 feet above the highest existing sidewalk elevation. Architectural projections, such as parapets, roofed towers and gable roofs, or elevator towers may extend up to 65 feet above the highest existing sidewalk elevation. Any rooftop mechanical, venting or communications equipment shall be built within or otherwise screened by the roof structure, so that they do not project above parapets and are not visible from any public rights-of-way or neighboring buildings. Temporary construction and/or maintenance machinery shall not exceed a height of 122 feet, measured from existing grade.

Buildings along Euclid Avenue (Block C1) and Holt Avenue (Block A1) shall be placed at the edge of the public right-of-way. Buildings sited along the other frontages of Block C1 shall have the minimum setbacks shown on Exhibits 3-1 and 3-2, and as specified in Table 3-1. Building setbacks and projections therefrom for Block A1 shall be specified in the Development Plan submitted for that block. Awnings, canopies, residential balconies, commercial signs, and decorative architectural elements may encroach into the public rights-of-way, as specified in Table 3-1.

A minimum 6-foot wide clear area shall be maintained for pedestrian circulation within the public right-of-way. This may be reduced to 4 feet in width adjacent to City-approved outdoor dining facilities or tree grates.

Any yard areas provided between the main building and the adjacent right-of-way may be finished with a hardscape or with landscape elements. Landscaping shall include materials selected from the master plant palette described in Table 4-1 herein.

Above ground utility closets, electrical transformers, backflow prevention devices, fire department connections and similar items may be placed anywhere within a perimeter yard area,

and may require screening measures, as approved by the Planning Director. Underground utility enclosures, such as electrical transformer vaults, may be placed within the street right-of-way, subject to approval by the City Engineer Director.

3.1.2 VEHICULAR ACCESS AND PARKING FACILITIES

- For Block A1, vehicular access shall be limited to no more than 2 locations along Lemon Avenue and no more than 2 on B Street, as approved by the City Engineer
- For Block C1, vehicular access shall be limited to one location on “C” Street, one location of Lemon Avenue, and no more than 2 locations on D Street, as approved by the City Engineer
- All parking required to meet the quantitative standards set forth in Section 3.1.3 shall be provided on site. Inoperable vehicles shall not be stored in any parking area.
- Angled parking spaces shall be incorporated into the western side of Lemon Avenue and both sides of “B” Street, as approved by the City Engineer

3.1.3 PARKING SUPPLY AND DESIGN

- Commercial: Minimum 1 space/250 square feet gross floor area for all permitted or conditionally permitted uses.
- Residential Condominiums: 1.2 spaces/bedroom, including guest parking
- Standard and tandem parking spaces are permitted. Tandem spaces may comprise up to 20 percent of the total spaces, if supported by a parking study approved by the Planning Director. All pairs of tandem spaces shall be assigned to the same dwelling unit. A tandem space is defined as the smaller of 2 parking spaces, paired end-to-end, which is blocked by the standard-size space (see Exhibit 3-9).
- Parking space dimensions, drive aisles, etc. shall generally conform to the design standards set forth in Division 6.03 (Off-Street Parking and Loading) of the Ontario Development Code. Standard dimensions for the structured parking areas shall be as shown in Exhibit 3-9.
- A parking management plan shall be submitted for Planning Director approval, prior to issuance of building permits. This plan shall justify the mix and sizing of compact and tandem spaces, shall indicate how the residential parking will be restricted to avoid overflow into the commercial parking area, and shall ensure that tandem parking spaces will be assigned to residents of the same homes.
- Parking spaces shall not obstruct access to building entrances, trash storage areas or loading areas

3.1.4 RESIDENTIAL RECREATION AREA

- Private outdoor open space (balconies) shall be provided for at least 90 percent of all homes. At least 80 percent of all balconies shall have a minimum dimension of 6 feet and the remainder shall have a minimum dimension of 3 feet. The average area of all balconies shall be 56 square feet. The minimum area for a balcony shall be 50 square feet.

▪ Common open space/recreation amenities shall be provided on the podium or ground level of the project, at a minimum ratio of 200 square feet per dwelling unit. This may include a combination of passive and active, indoor and outdoor spaces, and landscaped and hardscaped ground surfaces, and shall include an outdoor pool. Such common area does not include walkways and corridors between buildings, areas occupied by utility equipment or any perimeter yards abutting the public right-of-way. Outdoor recreation areas shall comprise a minimum of 80 percent of the total common recreation area.

3.1.5 SERVICE FACILITIES

▪ Loading/Unloading areas shall be provided on site, at the ground level, adjacent to commercial buildings. One such area shall be provided for each building 7,500 square feet GFA or larger. Design shall be as approved by the Planning and Public Works Directors.

▪ Loading/Unloading for residential uses shall be limited to the C Street and Lemon Avenue frontages. Location, signage and any hourly restrictions to be imposed on such areas shall be approved by the City Engineer.

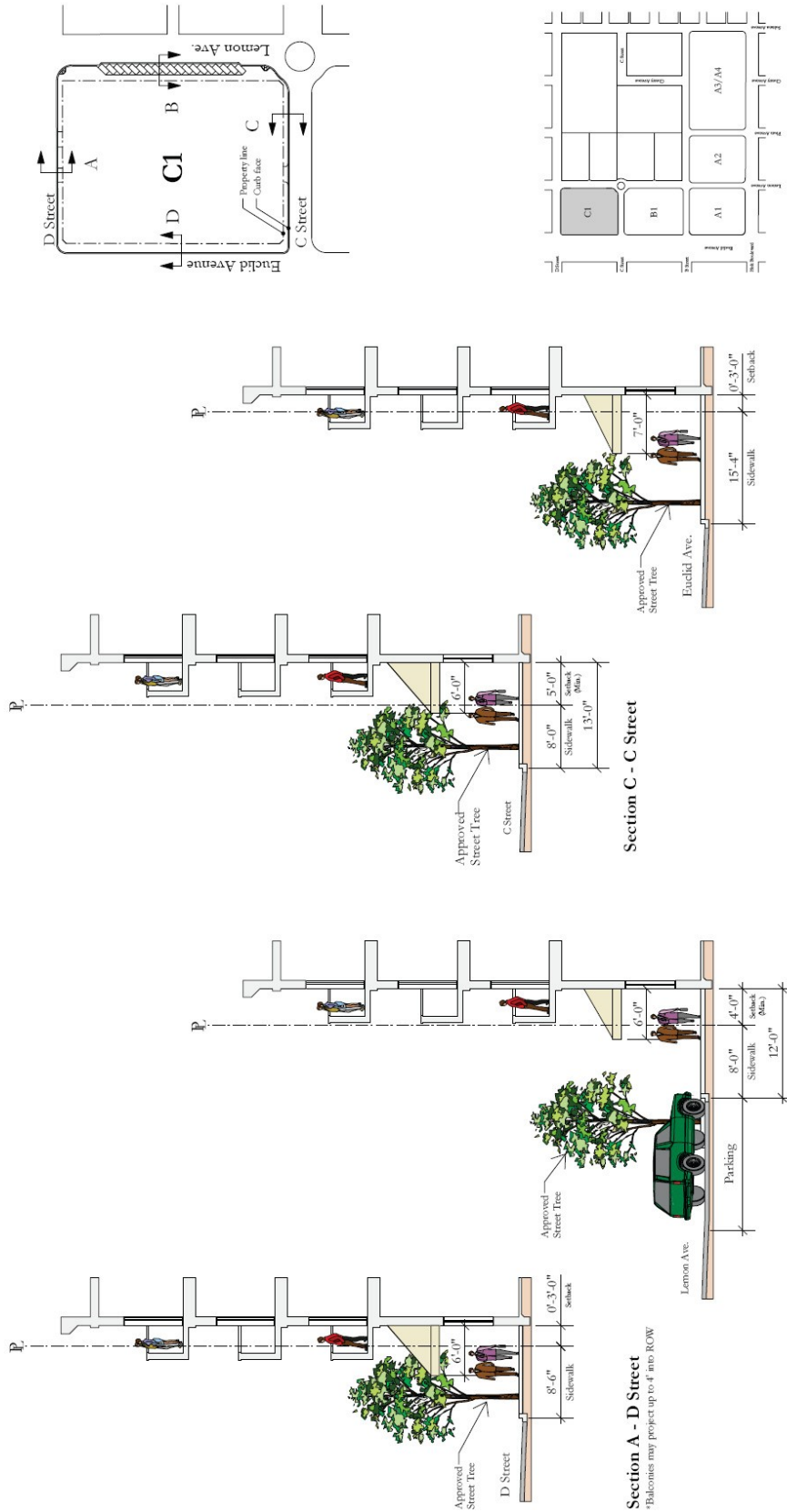
▪ Trash receptacles/recyclable container storage areas may be provided for all commercial and residential uses, within the ground floor parking structure area. Commercial trash/recyclable receptacles that are located within the ground floor parking structure area shall be partially enclosed and residential receptacle/storage areas shall be fully enclosed, to house the outlet of the trash chute system required for the condominium levels. Such receptacle/storage area may receive trash from commercial and residential sources. Locations, sizes and design of such areas shall be readily accessible by trash disposal company equipment and work crews, as approved by the Planning and Public Works Directors, in consultation with the trash disposal vendor.

3.1.6 SIGNS

▪ A Master Sign Program for each block shall be submitted for approval by the Planning Director, prior to issuance of a building permit. The sign program shall be consistent with the Downtown Ontario Design Guidelines, and shall include tenant signs, pedestrian signs, directional signs, historical markers, etc.

3.1.7 LANDSCAPE PLAN

▪ A detailed landscape plan shall be submitted with the Development Plan, specifying all landscape and hardscape elements of the common open space area, as well as the perimeter yards abutting the street rights-of-way and all street trees. The final plan shall also show the location of all ground level utility boxes and mechanical equipment, along with methods of screening such devices from the public right-of-way and adjacent residences, where possible. The landscape plan shall incorporate the master plant palette for this PUD, set forth in Table 4.1 herein.



Section D - Euclid Avenue
 *Balconies may project up to 4' into ROW

Section B - Lemon Avenue

Section A - D Street
 *Balconies may project up to 4' into ROW

Exhibit
Block C-1 Setbacks and Projections
 Downtown Civic Center P.U.D.

3-1

not to scale

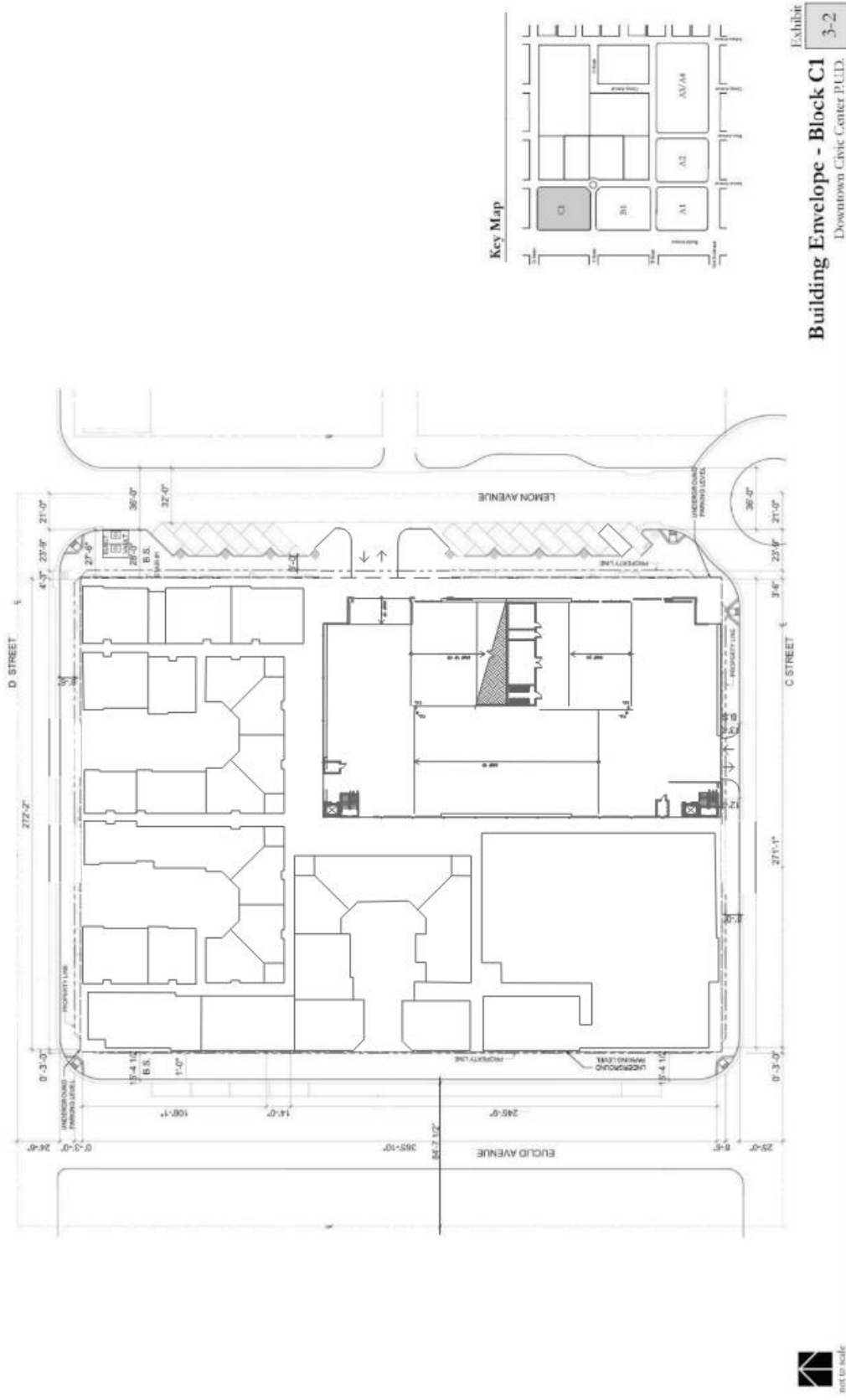


Exhibit
Building Envelope - Block C1
 Downtown Civic Center P.U.D. 3-2



3.2 CIVIC PLAZA (BLOCK B1)

3.2.1 BUILDING ENVELOPE (COMMERCIAL PADS ONLY)

- Buildings may be one or 2-story, not to exceed 35 feet high, measured from highest adjacent existing sidewalk elevation. Architectural projections may extend up to 45 feet high.
- Buildings may be built up to the Euclid Avenue, B Street and C Street rights-of-way. Any building shoring shall not extend into the public right-of-way.

3.2.2 VEHICULAR ACCESS AND PARKING FACILITIES

- Vehicular access shall be limited to no more than 2 locations on Lemon Avenue and one on B Street, as approved by the City Traffic Engineer.
- Vehicle parking on site shall be located along Lemon Avenue
- Parking shall cover no more than 25 percent of the net site area

3.2.3 PARKING SUPPLY AND DESIGN

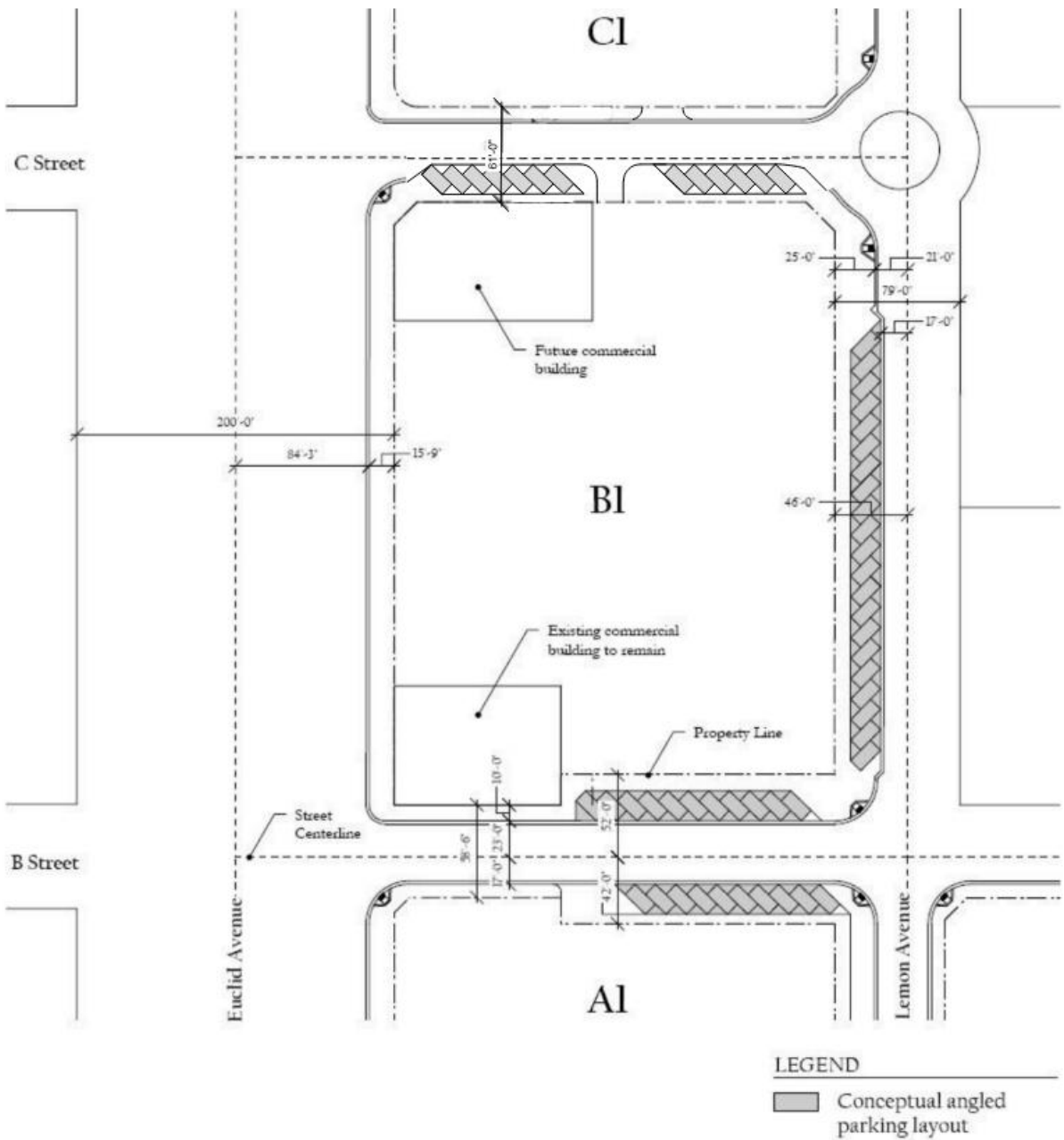
- The number of vehicle parking spaces provided on site shall be determined in a parking demand/supply study included as a supporting document for the Development Plan submitted for Block B1. Space and aisle dimensions shall be in accordance with the standards set forth in the Ontario Development Code. Canopy trees shall be provided within the parking area, as approved by the City's Landscape Planner.

3.2.4 SERVICE FACILITIES

- Loading/unloading for the commercial sites shall be limited to the B and C Street frontages. Location, signage and any hourly restrictions for such areas shall be approved by the Planning and Public Works Directors.
- A decorative trash enclosure, with 4 sides and a partially enclosed roof element, shall be provided within the vehicle parking area. This shall be sized to provide sufficient trash receptacle/recyclable container storage for both commercial buildings. Materials, style and colors shall be compatible with the architectural elements of the 2 commercial buildings and consistent with the traditional downtown theme for the downtown area. The trash enclosure design shall be approved by the Planning and Public Works Directors.

3.2.5 SIGNS

- A master sign program for all private and public signs shall be submitted for approval by the Planning Director, prior to issuance of a building permit. All signs shall be consistent with the Downtown Ontario Design Guidelines.



Conceptual Building Envelope - Block BI
Downtown Civic Center P.U.D.

Exhibit
3-3

3.3 APARTMENTS (BLOCK A2)

3.3.1 BUILDING ENVELOPE

- Structured parking shall be built in a semi subterranean manner, to minimize the exposure of concrete exteriors along adjacent streets
- Up to 3 levels of residential over structured parking, provided that total maximum building height, measured from the highest sidewalk elevation to the main roof line, does not exceed 50 feet. Architectural projections such as elevator towers or other focal elements may extend up to 60 feet high. Any rooftop mechanical equipment shall be built within or otherwise screened by the roof structure, so that they do not project above the roofline of the building and are not visible from any public rights-of-way or neighboring buildings. Temporary construction and/or maintenance machinery shall not exceed a height of 122 feet from existing grade.
- Building setbacks shall be in conformance with the specifications illustrated on Exhibit 3-3 and 3-4, and as set forth in Table 3-1. Ground-level mechanical equipment, utility closets, etc., may be placed within the required setback area; provided, they are fully screened in accordance with Section 4.3.1 herein. Corner elements may extend closer to the right-of-way line as specified in Table 3.2.
- All perimeter yards abutting street rights-of-way shall be landscaped with a uniform palette of materials, in accordance with the master plant list set forth in Section 4.1 herein.
- Above-ground utility closets, electrical transformers, backflow prevention devices, fire department connections and similar items may be placed anywhere within a perimeter yard area, and may require screening measures, as approved by the Planning Director. Underground utility enclosures, such as electrical transformer vaults, may be placed within the street right-of-way, subject to approval by the City Engineer.

3.3.2 VEHICULAR ACCESS AND PARKING FACILITIES

- Vehicular access shall be limited to one location on Lemon Avenue and one on Plum Avenue, as approved by the City Traffic Engineer, (see Exhibit 3-10).
- All resident parking required to meet the quantitative standards set forth in Section 3.3.4 shall be provided on site. Inoperable vehicles shall not be stored in any parking area.
- Resident parking shall be provided in a semi-subterranean parking structure, with secured access to prevent use by non-residents. Guest/visitor parking is not required on site; adjacent on-street parking is considered sufficient. Parking along Holt Boulevard shall be prohibited.

3.3.3 OUTDOOR RECREATION AND YARDS

- Private outdoor open space shall be provided for each apartment, with a minimum dimension of 5 feet and a minimum area of 50 square feet
- Common open space/recreation amenities shall be provided on the podium level of the project. This may include a combination of passive and active, indoor and outdoor spaces, landscaped and hardscaped ground surfaces, and shall include an outdoor pool. Such common area

does not include walkways and corridors between buildings, areas occupied by utility equipment or any perimeter yards abutting the public right-of-way. Outdoor recreation areas shall comprise a minimum of 80 percent of the total common area.

3.3.4 PARKING SUPPLY AND DESIGN

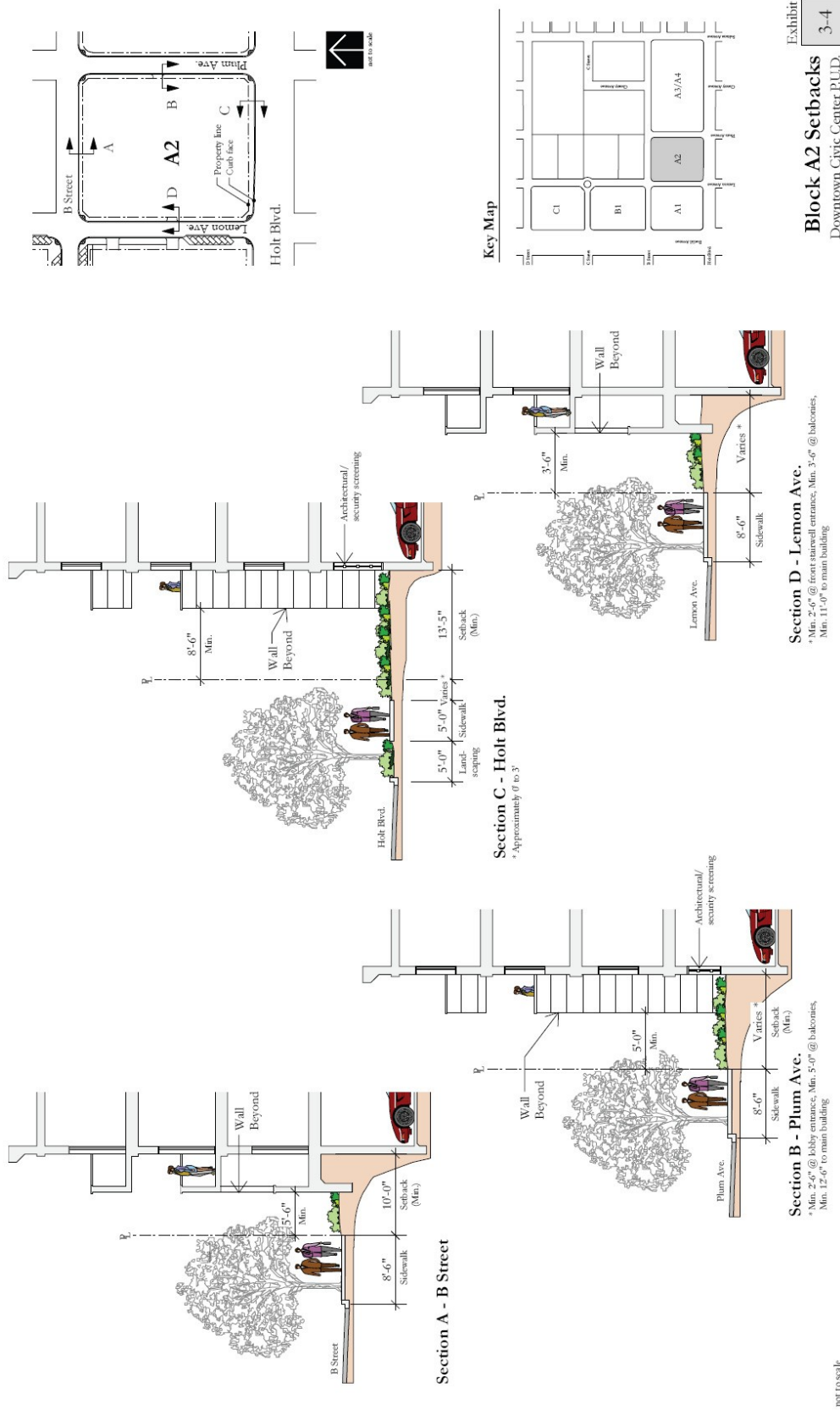
- One and one-half space for each one-bedroom unit and 2 spaces for each 2-bedroom unit, for residents.
- A combination of standard, compact and tandem spaces may be provided. Compact spaces may comprise up to 31 percent of the total spaces, if supported by a parking study approved by the Planning Director. Tandem-parked spaces may comprise up to 28 percent of the total, if supported by a parking study approved by the Planning Director.
- Parking space dimensions, drive aisles, etc. shall generally conform to the design standards set forth in the Ontario Development Code, except as shown in Exhibit 3-10. A tandem space is defined as the smaller of 2 parking spaces, paired end-to-end, which is blocked by the standard-size space (see Exhibit 3-10).
- Each pair of spaces to be parked in tandem shall be assigned to the same apartment unit, as controlled by the property manager.

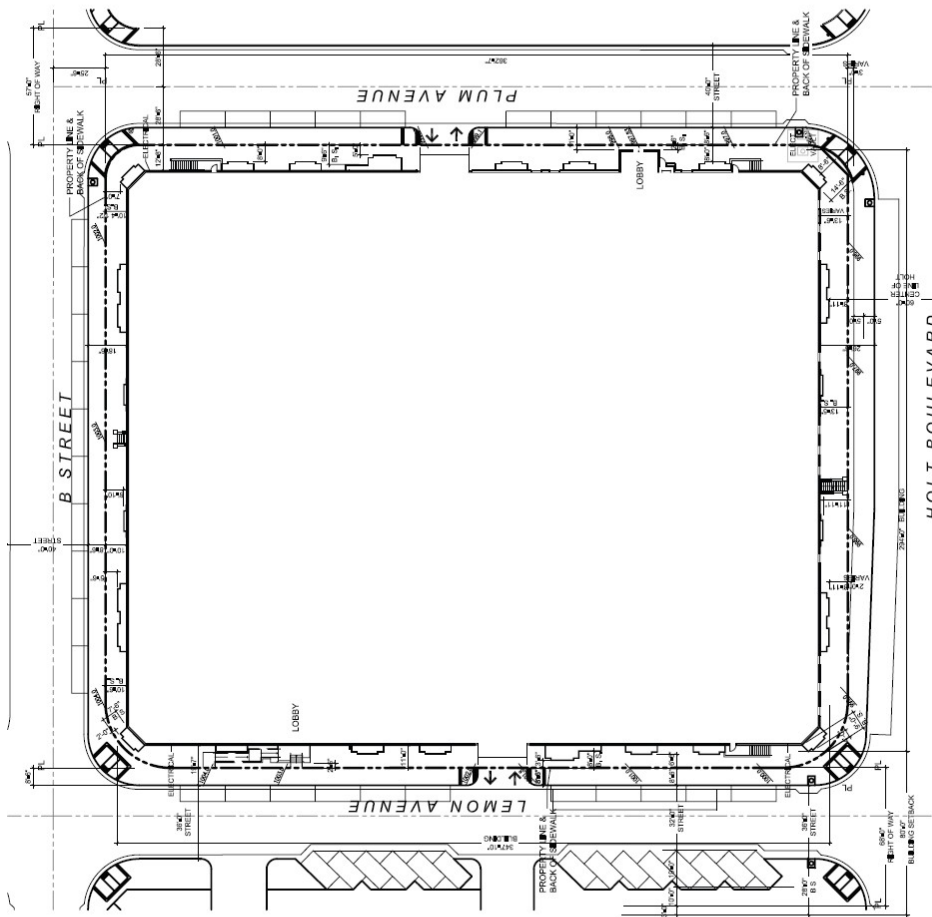
3.3.5 SERVICE FACILITIES

- Loading/unloading for residential uses and trash pickup service shall be limited to adjacent street frontages. Location, signage and any hourly restrictions to be imposed on such areas shall be approved by the Planning Director and the City Engineer
- At least 2 trash /recyclable container storage areas shall be provided within the parking structure area. Such areas shall be fully enclosed and readily accessible by trash disposal company equipment and work crews. Locations, sizes and design of such areas shall be approved by the Planning Director and the City Engineer, in consultation with the trash disposal vendor.

3.3.6 LANDSCAPE PLAN

A detailed landscape plan shall be submitted with the Development Plan for this block, specifying all landscape and hardscape elements of the common open space area, as well as the perimeter yards abutting the street rights-of-way, and all street trees. The final plan shall also show the location of all ground level utility boxes and mechanical equipment, along with methods of screening such devices from the public right-of-way and adjacent residences, where possible. The landscape plan shall incorporate the master plant palette for this PUD, set forth in Table 4.1 herein.





Key Map



Exhibit
Building Envelope - Block A2
 Downtown Civic Center P.U.D.



3.4 TOWNHOMES (BLOCKS A3/A4)

3.4.1 BUILDING ENVELOPE

- Buildings shall not exceed 3 stories, with rooflines reaching a maximum height of 40 feet above the highest existing adjacent sidewalk elevation. Architectural projections, chimneys, etc. may extend up to 45 feet high. Any rooftop mechanical equipment shall be built within or otherwise screened by the roof structure, so that they do not project above the roofline of the building and are not visible from any public rights-of-way or neighboring buildings. Temporary construction and/or maintenance machinery shall not exceed a height of 122 feet from existing grade.

- Building setbacks shall be in conformance with the specifications specified in Table 3-2, and illustrated on Exhibits 3-5 and 3-6. Please note that the minimum setback dimensions shown are not intended as a uniform standard along any street frontage. Corner buildings may have smaller setbacks than mid-block building elements and some undulation in building faces is desired to create a sense of movement, particularly along Holt Boulevard. Residential balconies may project into required setback areas up to 5 feet beyond the main building face. Ground-level mechanical equipment, utility closets, etc., may be placed within the required setback area as specified in Table 3-1, provided they are fully screened in accordance with Section 4.3.2 herein.

- All perimeter yards abutting street rights-of-way shall be landscaped with a uniform palette of materials, in accordance with the master plant palette set forth in Table 4.1 herein.

- Above-ground utility closets, electrical transformers, backflow prevention devices, fire department connections and similar items may be placed anywhere within a perimeter yard area, and may require screening measures, as approved by the Planning Director. Underground utility enclosures, such as electrical transformer vaults, may be placed within the street right-of-way, subject to approval by the Public Works Director.

3.4.2 VEHICULAR ACCESS AND PARKING FACILITIES

- Vehicular access shall be limited to 2 locations on B Street. Drive width, length and locations shall be as approved by the City Traffic Engineer.

- Resident parking shall be provided in fully enclosed garages, directly attached to each dwelling unit. Guest parking is not required on site; adjacent on-street parking is considered sufficient. Parking along Holt Boulevard shall be prohibited. Inoperable vehicles shall not be stored anywhere on premises or on any street.

- Space within each garage shall be provided for trash containers; this shall not encroach into space required for vehicle parking.

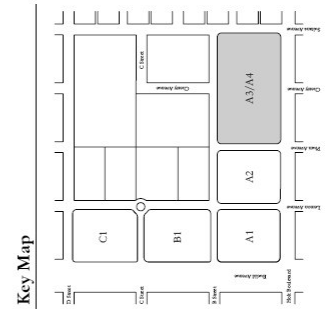
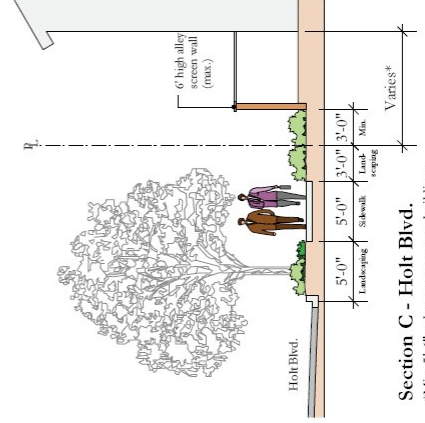
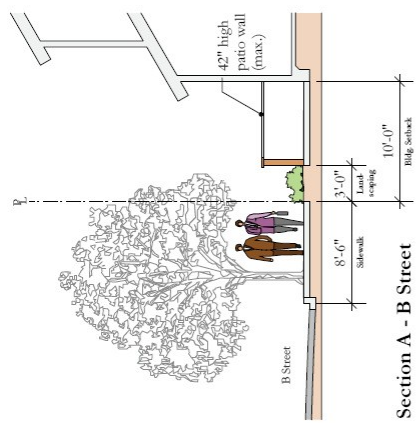
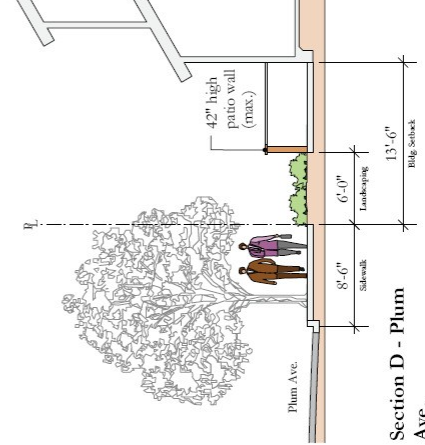
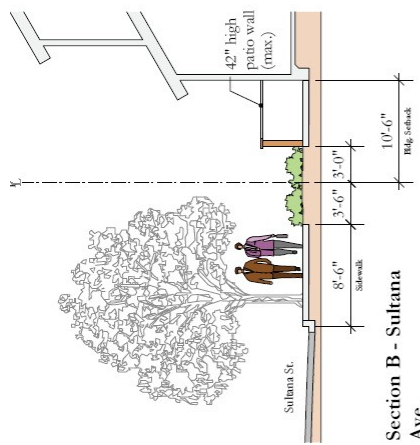
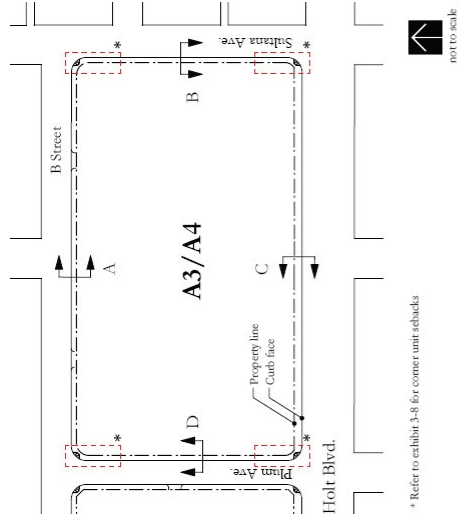


Exhibit 3-6
Block A3/A4 Setbacks
Downtown Civic Center P.U.D.

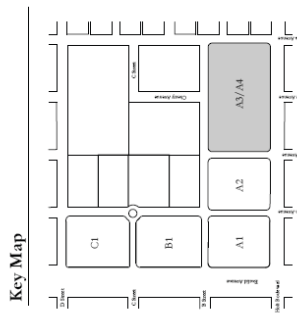
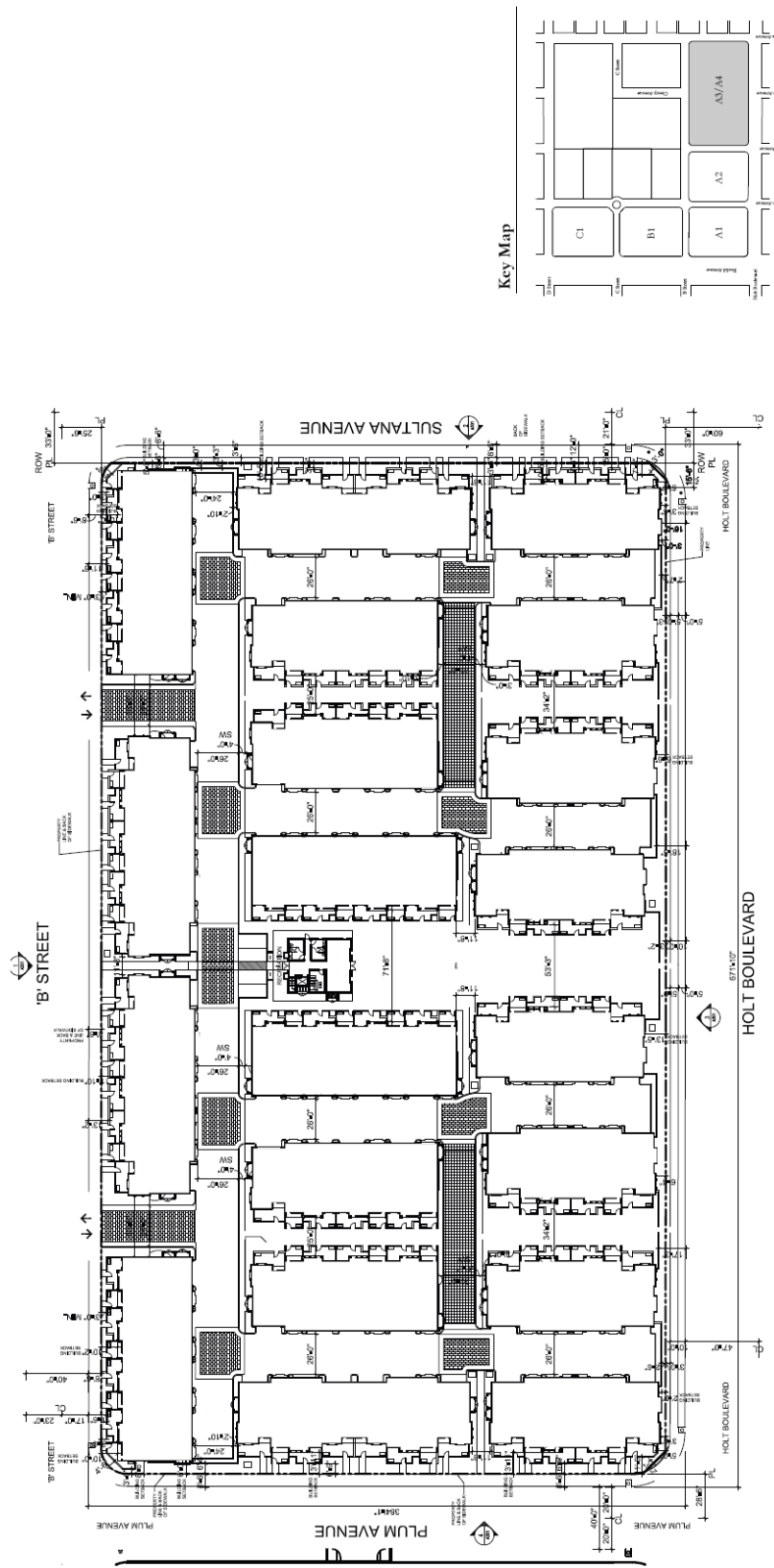
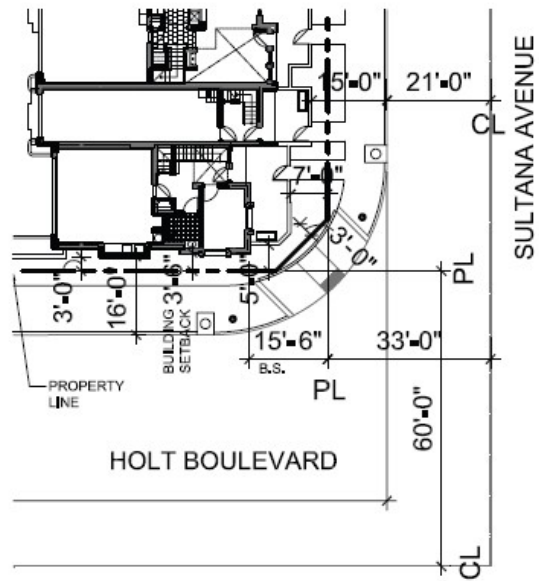
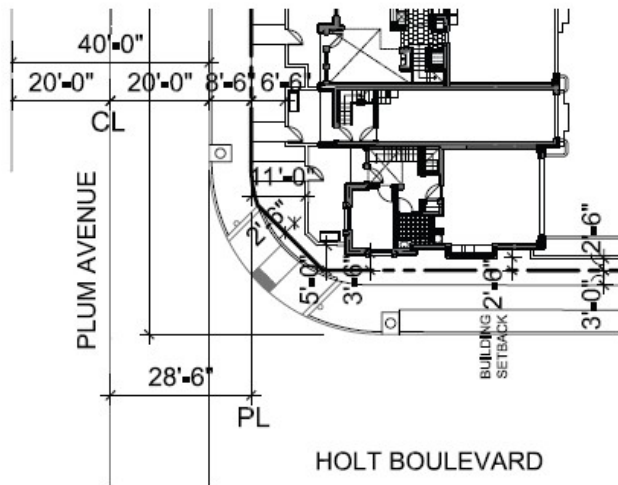
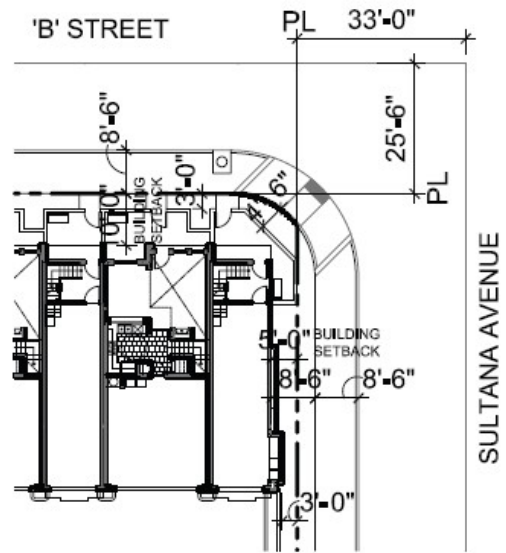
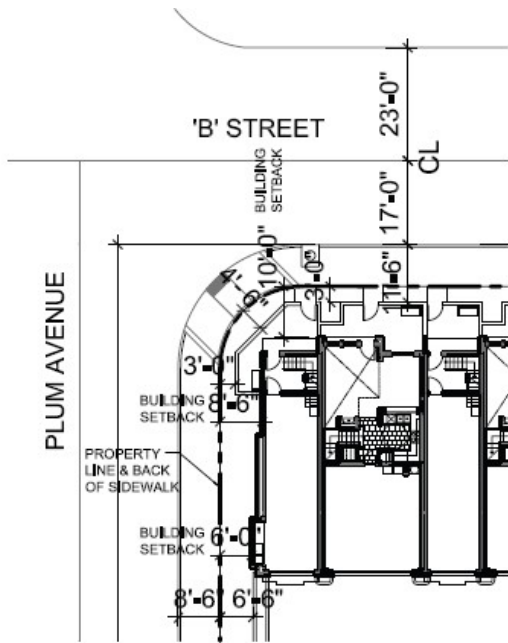


Exhibit
Building Envelope - Block A3/A4
 Downtown Civic Center P.U.D.





Corner Unit Setbacks - Block A3/A4
Downtown Civic Center P.U.D.

Exhibit

3-8

3.4.3 OUTDOOR RECREATION AREA

- A private, fenced, ground level outdoor open space/recreation area shall be provided for each home. Each such space shall have a minimum dimension of 7 feet and a minimum area of 90 square feet, except for the corner units at “B” St./Plum Ave. and “B” St./Sultana Ave, which shall have a minimum area of 50 square feet.
- Common open space/recreation amenities shall be provided in a central location to facilitate access by all residents. Such amenities may be comprised of active and passive, indoor and outdoor spaces, landscaped and hardscaped ground surfaces, and shall include an outdoor pool. Such common area does not include walkways and corridors between buildings, areas occupied by utility equipment or any perimeter yards abutting the public right-of-way. Outdoor spaces shall comprise a minimum of 80 percent of all common recreation area.

3.4.4 PARKING SUPPLY AND DESIGN

- Two spaces per unit
- Standard, side-by-side parked garages shall have a minimum clear floor area of 19’10” wide x 19’10” long. Openings shall be a minimum of 16 feet wide.
- Tandem-style garages (i.e. 2 spaces paired end-to-end) are permitted for up to 39 percent of the total homes, if justified by a parking study approved by the Planning Director. Each such garage shall have a minimum clear floor area of 11’ wide x 36’ long. Openings shall be a minimum of 9 feet wide.

3.4.5 FENCES, WALLS AND OTHER BARRIERS

- 42-inch high block walls with finish materials that are complementary to the materials of the townhomes shall be placed around all ground level patios facing streets.
- Up to 42-inch high brick or masonry walls, with a finish treatment that is complementary to the materials of the townhomes, shall be placed around all round-level patios. Walls may extend up to 6 feet in height between adjoining patios.
- Up to 6-foot high block walls with finish materials that are complementary to the materials of the townhomes shall be placed to screen all interior drives from public view.

3.4.6 SERVICE FACILITIES

- Loading/unloading for residents and trash pick-up services shall occur within the private alleys on site. Maximum time limits for such activities shall be restricted by private Covenants, Conditions and Restrictions to ensure adequate emergency vehicle access can be provided. On-street loading shall be prohibited.

3.4.7 LANDSCAPE AND WALLS PLAN

- A detailed landscape, irrigation and walls plan shall be submitted with the Development Plan for this block, specifying all landscape, hardscape and walls/fencing elements of

the private ground level patios and the common open space area, as well as the perimeter yards and alley screen walls abutting the street rights-of-way. The final plan shall also show the location of all ground level utility boxes and mechanical equipment, along with methods of screening such devices from the public right-of-way and adjacent residences, where possible. The landscape plan shall incorporate appropriate elements of the master plant palette for this PUD, set forth in Table 4-1 herein.

3.5 SUMMARY OF DEVELOPMENT STANDARDS

Tables 3-1 and 3-2, following, provide a summary of the quantitative development standards set forth in Sections 3.1 to 3.4.

3.6 ON-STREET PARKING, LOADING, VEHICULAR ACCESS AND BUS STOPS

On-street parking is provided throughout the project limits, as shown in Exhibit 3-11. This is a significant parking resource, as it will provide a majority of the guest parking opportunities for the apartments and townhomes in Blocks A2 and A3/A4. Loading areas for the condominiums in Blocks A1 and C1 and for the apartments in Block A2 must occur within the public streets, since there will be insufficient vertical clearance and maneuvering space within the parking structures for large trucks and other large delivery vehicles. Loading areas for each of these blocks will be determined by the Director of Public Works, in conjunction with approval of the Development Plan for each block. Potential driveway access locations for each block except B1 are shown on Exhibit 3-10. No driveways will be permitted on Block B1, unless a parking study prepared with the Development Plan for that block indicates that on-site parking is needed. All driveway locations and dimensions will be approved as part of the Development Plan for each block. Bus stops shown on Exhibit 3-10 are in their existing locations and, according to Omnitrans staff, these are expected to remain in these locations for the next several years, after relocation of the downtown transit center. Bus shelters and/or benches at these locations shall be provided in accordance with Omnitrans design criteria and approved as part of the overall Streetscape Master Plan, as required by Section 4.4.2 herein.

Table 3.1: Building Intensities, Access, Parking and Recreation Area Standards

Standards	Block A1	Block A2	Blocks A3/A4	Block B1	Block C1
Intensities	<u>Commercial</u> : 0.4 FAR, max. 36,000 sq. ft. gross floor area. <u>Residential</u> : 45-65 du/ac; max. combined FAR of 2.7.	Max. 65 du/ac; max. 160 apartments	25-35 du/ac; max. 140 town homes	<u>Commercial</u> : max. 10,000 sq. ft. ground floor restaurants, and max. 5,000 sq. ft. second floor mixed commercial space	<u>Commercial</u> : 0.4 FAR, max. 36,000 sq. ft. gross floor area. <u>Residential</u> : 45-65 du/ac; max. combined FAR of 2.7.
Maximum Building Heights (Feet/Stories)1	50 ft/4 stories-main roofline ² 60-ft architectural projections ³	50 ft/4 stories-main roofline ² 60 ft-architectural projections ³	40 ft/3 stories ² 45 ft for architectural projections ³	35 ft/2 stories	50 ft/4 stories-main roofline ² 65 ft-architectural projections ³
Access and Parking Facilities	No more than 2 driveways on Lemon Ave and 2 on B St., as approved by the City Engineer. Commercial, residential and guest parking shall be on-site.	No more than one driveway on Lemon Ave and one on Palm Ave, as approved by the City Engineer. Resident parking on-site, guest parking on street. ⁴	No more than 2 driveways on B Street, as approved by the City Engineer. Resident parking on-site, guest parking on street. ⁴	No driveway access to this block, unless on-site parking required, as determined in parking study submitted with Development Plan.	No more than one driveway on C Street and Lemon Avenue, as approved by the City Engineer. Commercial, residential and guest parking shall be on site.
Parking Supply & Design	<u>Commercial</u> : 1 space/250 sq. ft. GFA. Tandem spaces are prohibited. <u>Residential</u> : 1.2 spaces/bedroom, incl. guest parking. Max. 20 percent tandem spaces. See Exhibit 3-8 for typical parking structure layout and key dimensions.	1.5 spaces/one-bedroom units 2 spaces/2-bedroom unit. Guest parking on street Max. 28 percent total may be tandem spaces. See Exhibit 3-9 for typical parking layout and key dimensions.	2 garage spaces/ unit. Tandem parking allowed in max. 39 percent all homes. Standard Garage 19'10" x 19'10", with 16-foot wide opening Tandem Garage 11'x 36' clear space, with 9-foot wide opening	None required, unless on-site parking is required as determined in parking study submitted with Development Plan. If required, on-site parking spaces shall be designed in accordance with the Ontario Development Code.	<u>Commercial</u> : 1 space/250 sq. ft. GFA. Tandem spaces are prohibited. <u>Residential</u> : 1.2 spaces/bedroom, incl. guest parking. Max. 20 percent tandem spaces. See Exhibits 3-8 for typical parking layout and key dimensions.
Private and Common Recreation Area	<u>Private Outdoor Space</u> : Min. 90 sq. ft./unit, of which up to 20 percent shall have a min. dimension of 3-ft and 80 percent of which shall be at least 7-ft long. Corner Units shall have a min. of 50 sq. ft., with min. 3-ft dimension.	<u>Private Outdoor Space</u> : Min. 50 sq. ft. /unit, with a min. dimension of 5 ft. <u>Common Area on Podium Level</u> : Min. 125 sq. ft./unit. Up to 20 percent of this may be provided by indoor recreation facilities.	<u>Private Outdoor Space</u> : Min. 90 sq. ft./unit at ground level, with a minimum dimension of 7 feet. Corner Units shall have a min. of 50 sq. ft. <u>Common Area</u> : Min. 230 sq. ft./unit. Up to 20 percent of this may be provided by indoor recreation facilities.	None required.	<u>Private Outdoor Space</u> : Min. 90 sq. ft./unit, of which up to 20 percent shall have a min. dimension of 3-ft and a min. of 80 percent of which shall be at least 7-ft long. Corner Units shall have a min. of 50 sq. ft., with min. 3-ft dimension.

Standards	Block A1	Block A2	Blocks A3/A4	Block B1	Block C1
	<u>Common Area on Podium Level:</u> Min. 200 sq. ft./unit. Up to 20 percent of this total common area may be provided by indoor recreation facilities.				<u>Common Area:</u> Min. 200 sq. ft./unit). Up to 20 percent of this total common area may be provided by indoor recreation facilities.

Notes:

- 1 Temporary construction and/or maintenance equipment may extend to 122 feet above the existing ground level, in any block.
- 2 Measured from highest existing sidewalk elevation adjacent to the site.
- 3 Includes elevator towers, cupolas, clocks, and other architectural projections and focal elements.
- 4 Parking is prohibited on Holt Boulevard.

Table 3.2: Building Setbacks and Permitted Projections into Public Rights-of-Way

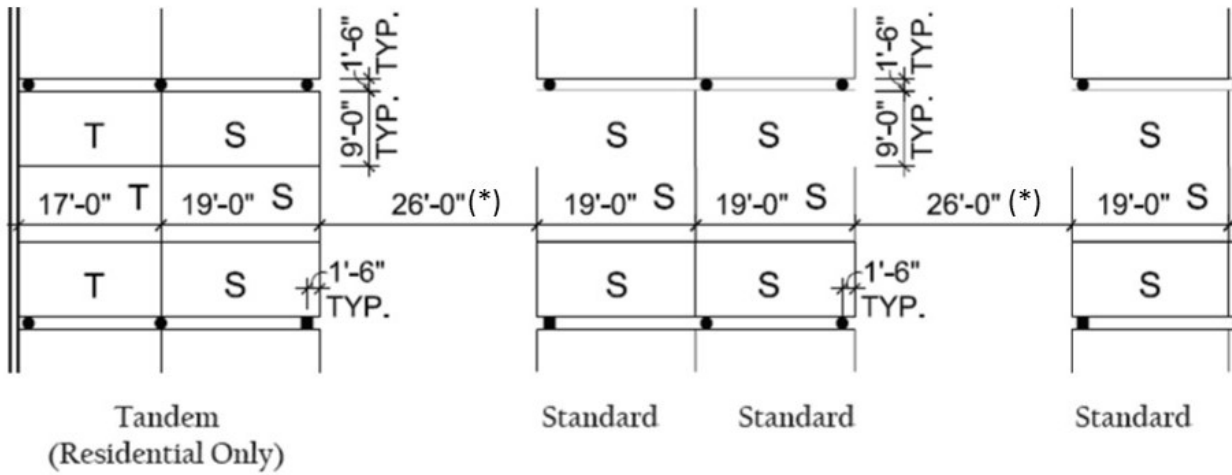
	Block A1	Block A2	Block A3/A4	Block B1	Block C1
Minimum Setbacks To Main Buildings (From Property Line)					
Holt Boulevard	Build to max. 3' from property line*	13'6"	2'6" for corner units and 5'6" for others	N/A	N/A
Euclid Avenue	Build to max. 3' from property line*	N/A	N/A	Build to max. 2' from property line	Build to max. 3' from property line*
B Street			10'	Build to max. 2' from property line	N/A
Lemon Avenue	5' ***	2'6" at lobby entrance, 11' for main building	N/A	N/A	4' ***
C Street	N/A	5' **	10'	Build to max. 2' from property line	4'6" **
Plum Avenue	N/A	2'6" at lobby entrance, 12'6" for main building	6'0" for corner unit, 13'6" for others	N/A	N/A
Sultana Avenue	N/A	N/A	3'0" for corner unit, 10'6" for others	N/A	N/A
D Street	N/A	N/A	N/A	N/A	Build to max. 3' from property line.*
<p>* Residential balconies may extend up to 4 feet into the street right-of-way. ** Pilasters, columns and similar projections may extend 6 inches beyond the building face, for up to 15 percent of the street frontage. *** Pilasters, columns and similar projections may extend 6 inches beyond the building face, for up to 20 percent of the street frontage.</p>					
Awnings, Canopies, Signs And Other Projections Into Public Right-Of-Way					
Euclid Avenue	7'	N/A	N/A	7'	7'
Holt Boulevard	6'	None	None	N/A	
B Street	2'	None	None	N/A	
C Street	N/A	N/A	None	1'0"	2'
D Street	N/A	N/A	N/A	1'6"	2'
Lemon Avenue	1'	None	N/A	2'6"	1'
Plum Avenue	N/A	None	N/A	2'0"	
Sultana Avenue	N/A	N/A	None	N/A	

	Block A1	Block A2	Block A3/A4	Block B1	Block C1
* Residential balconies at Blocks A1 and C1 (2nd floor and up) may extend up to 4 feet into the airspace of the Euclid Avenue and Holt Blvd. rights-of-way. Along other frontages and on other blocks, balconies may extend up to 4 feet into the street setback.					
Minimum Setbacks To Private, Patios, Balconies, And Alley Screen Walls (From Property Line)					
Euclid Avenue	N/A	N/A	N/A	N/A	N/A
Holt Boulevard	N/A	8'6"	Alley walls: 2'6" at corner units, otherwise 3'0"	N/A	N/A
B Street	N/A	5'6"	Patio walls: 3'0"	N/A	N/A
C Street	N/A	N/A	N/A	N/A	N/A
D Street	N/A	N/A	N/A	N/A	N/A
Lemon Avenue	N/A	3'6"	N/A	N/A	N/A
Plum Avenue	N/A	5'0"	Private patio walls: 6'6" Alley wall: 6'6"	N/A	N/A
Sultana Avenue	N/A	N/A	Private patio walls: 3'0" Alley wall: 3'0"	N/A	N/A
Cornices, Eaves and Similar Architectural Features					
Euclid Avenue	30" into ROW	N/A	N/A	30" into ROW	30" into ROW
Holt Boulevard	30" beyond building face	24" beyond building face	24" beyond building face	N/A	N/A
B Street	30" beyond building face	24" beyond building face	24" beyond building face	30" (ROW)	N/A
C Street	N/A	N/A	N/A	30" (ROW)	30" beyond building face
D Street	N/A	N/A	N/A	N/A	30" beyond building face
Lemon Avenue	30" beyond building face	24" beyond building face	N/A	N/A	30" beyond building face
Plum Avenue	N/A	24" beyond building face	24" beyond building face	N/A	N/A
Sultana Avenue	N/A	N/A	24" beyond building face	N/A	N/A

Block A2		Block A3/A4		Block C1	
Corner	Setback	Corner	Setback	Corner	Setback
Minimum Setbacks To Main Building (At Corner Right-Of-Way)*					
Holt/Lemon	9'0"	Holt/Plum	2'6"	Euclid/C Street	3'0"

REGULATIONS FOR DEVELOPMENT

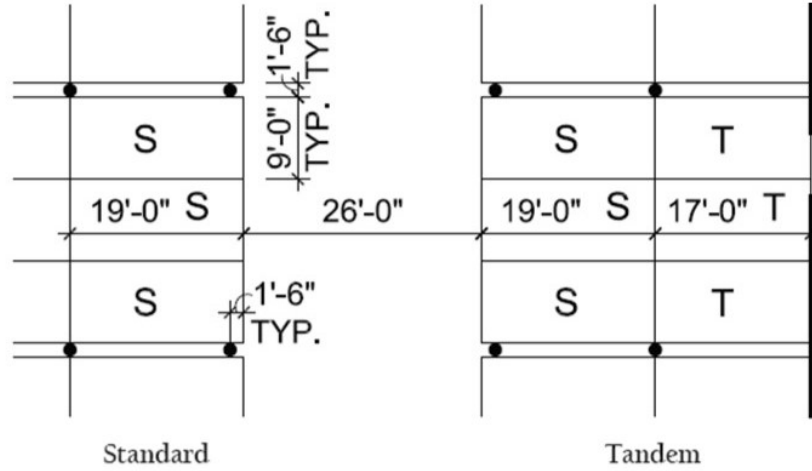
Block A2		Block A3/A4		Block C1	
Corner	Setback	Corner	Setback	Corner	Setback
Holt/Plum	14'6"	Holt/Sultana	3'6"	Euclid/D Street	1'6"
Lemon/B Street	7'6"	B Street/Plum	8'6"	Lemon/C Street	1'6"
B Street/Plum	10'0"	B Street/Sultana	5'0"	Lemon/D Street	1'0"
Minimum Setbacks To Private Patios And Balconies (At Corner Right-Of-Way)*					
Holt/Lemon	4'0"	Holt/Plum	2'6"	Euclid/C Street	3'0"
Holt/Plum	8'6"	Holt/Sultana	3'0"	Euclid/D Street	1'6"
Lemon/B Street	2'0"	B Street/Plum	4'6"	Lemon/C Street	1'6"
B Street/Plum	7'0"	B Street/Sultana	4'6"	Lemon/D Street	1'0"
* To be defined with Development Plans for Blocks A1 and B1					



(*) Drive aisle may be reduced to 24'-0" with Development Advisory Board approval

Standard Structured Parking
 Dimensions for Blocks A1 and C1
 Downtown Civic Center P.U.D.

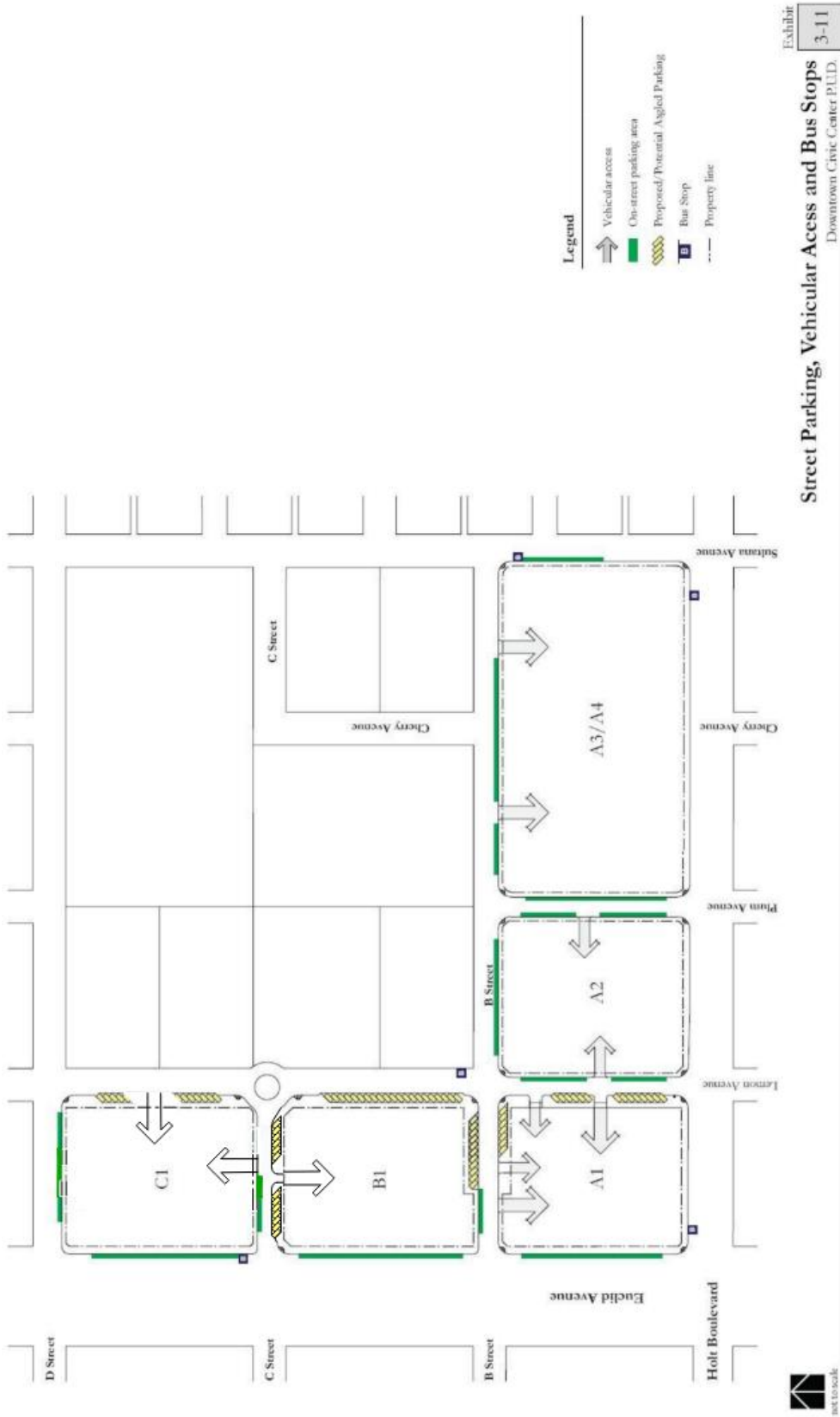
Exhibit
 3-9



(*) Drive aisle may be reduced to 24'-0" with Development Advisory Board approval

Standard Structured Parking
 Dimensions for Block A2
 Downtown Civic Center P.U.D.

Exhibit
 3-10



3.7 HISTORIC PRESERVATION

3.7.1 HISTORIC PROPERTIES

The following properties (see Exhibit 3-11) have been designated by the City as historically significant, and shall be retained in place and restored, the façade retained only, or demolished.

- 112 N. Euclid Ave.: Preserve façade. Cataloging required.
- 122 N. Euclid Ave.: Preserve façade. Cataloging required.
- 128-136 N. Euclid Ave: Preserve façade. Cataloging required.
- 200 N. Euclid Ave.: Retain/Restore
- 208-214 N. Euclid Ave: Tier II--Façade not intact. To be demolished; mitigation fee and cataloging required.
- 224 N. Euclid Ave.: Tier III—To be demolished; mitigation fee required.
- 226 N. Euclid Ave.: Tier III—To be demolished; mitigation fee and cataloging required.
- 230 N. Euclid Ave.: Tier III—To be demolished; mitigation fee and cataloging required.
- 310 N. Euclid Ave.: Tier II—Façade not intact. To be demolished; mitigation fee and cataloging required.
- 318 N. Euclid Ave.: Tier III—To be demolished; mitigation fee and cataloging required.
- 206 E. “B” St.: Tier III—To be demolished; mitigation fee and cataloging required.
- 310 E. “B” St.: Tier III—Previously demolished. Mitigation fee and cataloging complete.
- 330 E. “B” St.: Tier III—Previously demolished. Mitigation fee and cataloging complete.
- 325 E. Holt Blvd.: Tier III—Previously demolished. Mitigation fee and cataloging complete.
- 127 N. Sultana Ave.: Tier III—Previously demolished. Mitigation fee and cataloging complete.

The Ontario Downtown Civic Center Project EIR (October 2004, State Clearinghouse No. 2004051155) reviewed the impacts to historic structures within the Downtown Civic Center Project area. The EIR included a number of mitigation measures to lessen the cultural resource impacts from demolition and/or façade retention of historic structures within the project area, which included documentation (cataloging), salvage of significant features, and payment of a demolition mitigation fee. Despite these mitigation measures, the EIR concluded that demolition and/or façade retention of Tier I and Tier II historic properties were significant impacts that could not be reduced to a level of less than significant and, therefore, the EIR was certified by the City Council on November 16, 2004 with a Statement of Overriding Considerations for Cultural Resources.

The EIR requires preservation in place of Tier I structures and façade retention of Tier II structures including the Tier II – designated Drew Carriage Building (208-214 N. Euclid Ave.) and the Tier II – designated Old Fire House (316 N. Euclid Ave.). Since further analysis revealed that the original façade of the Old Fire House does not exist and the original façade of the Drew Carriage Building has been significantly altered and no longer retains its character-defining features, there is no original fabric left intact to preserve for these 2 buildings.

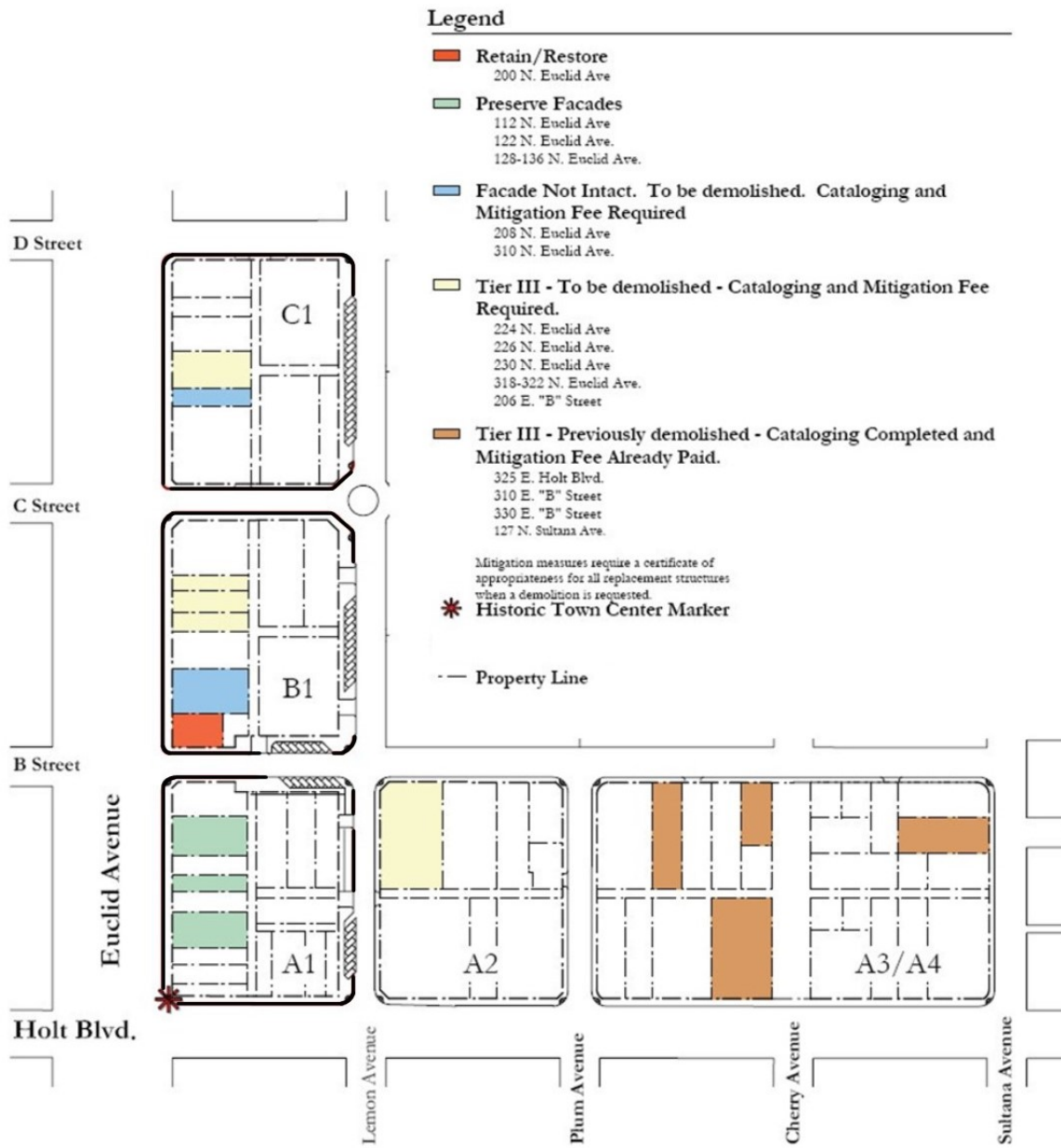
3.7.2 HISTORIC ROCK CURB

The 200-foot wide Euclid Avenue corridor (including the broad center median) is one of the most significant elements of the historic fabric of Ontario, and is listed as a historic district on the National Register of Historic Places.

Portions of the original rock curb face remain in several areas of Ontario’s historic core and are considered essential features that maintain the integrity of the historic character of this corridor (see Exhibit 3-11). Within the subject project limits, no original rock curb face exists along Euclid Avenue, and reconstruction of rock curb within the project area shall not be required.

3.7.3 EUCLID/HOLT HISTORIC MARKER

The intersection of Euclid Avenue and Holt Boulevard is the geographic center of the original Ontario town center and as such is a highly significant location within the historical fabric of the community. To recognize and permanently memorialize this significant site, a historical marker shall be erected within the corner sidewalk area, with the approval of Caltrans. Marker design and precise location shall be approved by the Historical Preservation/Planning Commission as part of the precise Development Plan application.



Historic Features - Plan View
Downtown Civic Center P.U.D.

Exhibit
3-12



Bank of Italy historic photo



Bank of Italy current photo showing south side of building



Commercial Hotel - Historic photo from the 1960's showing front of building



Commercial Hotel - Current photo showing front of building



Citizen's National Bank - Current photo showing front of building (No clear historic photo available)



Lerch Building - Historic Photo



Lerch Building - Current photo showing front of building



not to scale

Photos of Historic Properties
Downtown Civic Center P.U.D.

Exhibit

3-13

3.8 ENVIRONMENTAL PERFORMANCE STANDARDS

All land uses shall adhere to the environmental performance standards set forth in the Ontario Development Code, to avoid and minimize impacts involving noise, vibration, dust and paint, smoke, light, glare and heat, odors and gasses, hazardous and radioactive materials, and electromagnetic interference.

Architectural plans for all residential uses shall be submitted to the City of Ontario Building Department for an acoustical plan check, prior to the issuance of building permits, to assure that construction methods use standard materials that will attenuate 20 dBA of sound from outside to inside, or such that indoor noise does not exceed 45 dBA.

Pursuant to AB 2776 and Mitigation Measure Haz 9 in the Downtown Civic Center Project Final EIR, project developers shall disclose to all home buyers and renters that their homes are in proximity to Ontario International Airport.

Pursuant to Mitigation Measure Util 4 in the Downtown Civic Center Project Final EIR, each project Development Plan should consider and incorporate appropriate sustainable water and energy systems.

The full set of programmatic mitigation measures from the Downtown Civic Center Final EIR that are applicable to this PUD is provided in Appendix B.

4.0 DESIGN GUIDELINES

Purpose and Principles

Located entirely within the central core of the Downtown area, this PUD is considered an integral element of the urban revitalization of the historic retail and civic center areas. A considerable amount of effort has occurred to prepare a comprehensive set of design guidelines for the downtown area, to maintain the historic fabric and achieve excellence and coherence in the quality and character of the ongoing redevelopment of the center city area. Design principles and examples of desired styling elements to guide the redevelopment/revitalization program are set forth in the Downtown Ontario Design Guidelines (“Guidelines”), adopted by the Ontario City Council on August 18, 1998. This section is intended to provide guidance to City staff and private developers in the design of buildings, yards, and streets in a manner that fosters a flexible implementation of the principles and specific techniques described in those Guidelines. As such, some variations from the Guidelines are permitted, provided that the integrity of the urban design and historic preservation principles is not compromised.

A comprehensive assessment of historic properties throughout the downtown area has been completed and for the most part, this PUD does not contain significant buildings, structures or sites to be preserved. While there are a few properties along Euclid Avenue where preservation efforts will be necessary, a majority of this project will involve new construction and redevelopment of existing developed block areas. Ideally, such new infill development will complement and exemplify historic elements, while providing a fresh palette of architectural features that creates a more dynamic visual environment and a more pleasant pedestrian experience.

Because of the increased building intensity of this urban infill project, the relationship of buildings to the streets will be significant and must be treated creatively. It will be essential to provide a sense of movement and rhythm through a variety of articulation methods, materials and colors, to avoid bulky and monotonous elevations. This principle is to be applied to each of the blocks. Street trees and landscaped street-facing yards will provide opportunities to frame the street, create a pleasant pedestrian pathway and to reduce the impact of building bulk from the public rights-of-ways. A Civic Plaza park and a thematic program for street signs, furniture, markers, monumentation, banners, etc. will establish unifying elements that will immediately set the tone for the public spaces of the new downtown.

4.1 EUCLID AVENUE MIXED USE (BLOCKS A1 AND C1)

Design principles and desired architectural features for the mixed-use blocks are derived from the Turn of the Century architectural style and elements set forth in Section 3.5.3 of the Downtown Design Guidelines, with respect to: setbacks, storefront modulation, entrances, roof design, mechanical equipment, building elements (cornices, storefront frame, mid-floor panel, transom windows, bulkheads), awnings, materials and colors. Those guidelines, together with the following, shall be incorporated into the Development Plans for each block, subject to final interpretation by the Planning Commission and City Council.

Building Orientation

- Buildings must provide continuous commercial use and store fronts along Euclid Ave.
- Orient buildings to define the street and sidewalk, with service and parking generally positioned away from public streets, and toward the side and/or rear of buildings; a “building forward” design concept with buildings located near the sidewalk edge is required. Setbacks from the sidewalks are generally discouraged, except for such features as a “welcoming” recessed entry, a useable outdoor gathering space, etc.
- Buildings should be sited so that architectural focal treatments can be provided at strategic locations.
- Buildings located at prominent street corners shall be designed to incorporate architectural features that respond to these special locations, such as corner entries and corner tower elements.
- Incorporate multiple shop entrances and display windows with clear glazing along street facades, as well as facades oriented toward outdoor gathering spaces (i.e. plazas); windows and doors should introduce visual interest along the sidewalk, and create a pedestrian friendly street environment.

Access & Parking Facilities

- Control vehicular access and on-site parking to reduce their visual impact along streets, and protect adjacent land uses; in particular, on-site parking facilities shall be located to the side and/or rear of the building, and driveways shall be located to the side and/or rear of the buildings and minimize curb cuts.
- Parking structures shall not front directly onto Euclid Avenue.
- Parking structures fronting Lemon Street should have attractive facades that are compatible with the surrounding buildings. The design should employ similar colors, materials and details as are used on adjacent facades.

Service Facilities

- Locate service, loading and storage facilities away from public streets, outdoor spaces, and adjacent residential uses; as far as feasible, these facilities should be unobtrusive.
- Prohibit placement of utility transformer boxes within parking structure.
- Electrical equipment shall be mounted on the interior of a building wherever practical. When interior mounting is not practical, electrical equipment shall be enclosed and screened from public streets with walls, fencing and/or landscaping that allows for sufficient ventilation, or buried in vaults.
- No antenna pole or dish shall be placed on any structure so that it is visible from ground level.
- Service areas shall be screened, gated and as unobtrusive as possible.

- No garage type loading doors shall be located on a building façade directly facing an accessible street. All types of loading doors within view of public streets shall be screened with walls, berms or plant material.

Pedestrian Linkages (Sidewalks & Walkways)

- Locate pathways for clear and comfortable on-site pedestrian circulation; sidewalks and pedestrian paths should provide convenient and comfortable connections between storefronts, outdoor spaces, parking, as well as portals to adjacent residential uses. It is especially important to provide multiple, clearly marked and direct pedestrian pathways that connect rear parking areas with sidewalks and street-oriented shop fronts.

- Use Landscape elements to reinforce pedestrian connections; for instance, attractive paving and planting design may identify and highlight these connections by establishing a visual separation between vehicular and pedestrian routes.

- Place site accessories and features so that they do not interrupt connecting walkways and paths, in general, sidewalks and paths should provide for a minimum 6-foot clear pedestrian passage (or min. 4' adjacent to approved outdoor dining areas and street trees).

- Provide adjacent sidewalks with street trees, pedestrian-scaled light fixtures and street furniture.

- Entries to upper floor residential uses shall be from the side or rear of the block, and if on a side street, the visible entry shall be no wider than 15 feet at the street level, to avoid disturbing the rhythm of the retail spaces.

Architectural Character

- Create an architectural character that reflects the various styles within the historic downtown by using similar proportions, building details and building material.

- New buildings should appear to fit in contextually with existing historic buildings by using similar proportions, articulation, building details and building material.

- The overall character should feel like a modern interpretation of a traditional American small town.

- Create compatibility between the mixed-use architecture and the residential architecture within the downtown area; the introduction of unifying elements such as similar materials and colors should be considered; however, “sameness” is discouraged.

- A schematic illustration of an acceptable set of elevations is shown in Exhibit 4-1.

Massing & Scale

- Employ simple, yet varied massing, with wall openings that create shadow line and provide visual relief. Monolithic buildings and extensive blank wall surfaces are not allowed.

- Emphasize massing and height of corner elements

- Building increments should be a maximum of 50 feet in width. Buildings that exceed fifty
- (50) feet of frontage should use various architectural devices to replicate the building increments and pattern of the existing downtown such as:
 - Horizontal breaks and offsets every 25 to 50 feet
 - The upper portion of the buildings should reflect the same increment of articulation as the ground level (storefront level)
 - The cornices and moldings at the top of facades should also reflect the vertical massing (increments) of the building
- Use strong and identifiable building forms to demarcate street intersections for instance, a taller building mass, tower element/clock tower, or cupola may be considered.
- Where adjacent buildings will be of varying heights, they should be sensitively designed to promote a compatible transition in massing and avoid abrupt changes in scale along the streetscape.

Articulation & Fenestration:

- Design buildings with 3-dimensional quality; building facades should be based upon a pleasing set of proportions and a clear pattern of building openings.
- Differentiate between the base, middle and top levels of a building; for instance, street- oriented shops may feature a 1) a bulkhead; 2) display windows; and 3) transom and detailed cornice.
- Avoid monotonous or blank facades on a building; buildings shall be articulated on all sides. Use fenestration (windows and doors), as well as changes in wall plane, material, texture, color, etc. to create shadow lines and articulate building walls.
- Provide highly visible public entrances, preferably oriented to streets and outdoor spaces; shop entrances may be clearly expressed with recesses, overhangs, special materials, and/or detailing.
- Require a high degree of street level transparency, especially along sidewalks, important outdoor spaces and heavily trafficked areas; the use of reflective, opaque, and darkly tinted glass should be restricted.



Lemon Avenue Elevation



D Street Elevation



Encina Avenue Elevation



C Street Elevation

NOT TO SCALE

Exhibit

Mixed Use Architectural Character
Downtown Civic Center P.U.D.

4-1

Architectural Details

- Use architectural details to enhance a building's appearance. Careful detailing is especially appropriate at the base of buildings, along cornices, eaves, parapets or ridge tops, and around entries and windows.
- Incorporate human-scaled details such as canopies and awnings, transparent windows and windows displays. These are required at public entrances and along heavily trafficked outdoor areas.
- Design architectural features and details as an integral part of the building; architectural features, including canopies and awnings, should not appear "tacked-on."

Storefront Guidelines

- Storefronts should provide an individual look for each store or establishment while still blending in with the overall design.
- Storefronts shall incorporate high quality building materials. A durable base material (18 inches high min.) such as brick, tile, exterior cement plaster, or pre-cast architectural concrete shall be used. Storefront glazing and exterior plaster shall not terminate at the ground plane.
- Storefront glazing shall be clear glass to permit clear views into the interior space. The amount of such glass shall provide a balance between high visibility into the store interior and architectural character/quality.
- Entrance doors shall be of a traditional style and transparent as possible; however, an all glass door is prohibited.
- No part of any open doors shall extend into the public right-of-way.
- Storefront openings should not exceed 30 feet in width without the interruption of a vertical building element (column or wall).
- Storefront openings may extend up to 16 feet in height; the height should range from 8 to 16 feet.

Material & Colors

- Use materials that exhibit permanence and quality, and that unify a building's appearance on all sides; materials and colors should accentuate the architectural details of the building and promote visual harmony. Brick, tile, pre-cast architectural concrete, stone veneers or stucco are preferred.
- To create visual interest, use at least 3 different building materials for each block. Materials may be used to highlight a building entrance or distinguish the building base.
- Any awnings shall be comprised of durable, heat, dust and mildew-resistant canvas materials that are designed to withstand the rigors of the Ontario climate. Vinyl or other shiny materials are prohibited.

Roofs/Rooftop Equipment

- Express roofs in a visually interesting manner that complements the composition of the building and surrounding area, (i.e. use sculpted roof forms or a strong and attractively detailed cornice).
- Locate and/or screen rooftop equipment, including ladders and antenna devices, so that it is not visible from streets and adjacent buildings; methods of rooftop screening should be integral to the building's form.
- All mechanical units shall be screened from public view and from adjacent existing buildings (preferably with roof forms).
- When roof decks and mechanical units are visible from adjacent developments, they should be as unobtrusive as possible and painted to match finish roof material.

Outdoor Dining Areas

- Outdoor dining areas adjacent to a street level eating or drinking establishment are encouraged along Euclid Avenue and are permitted along any street frontage. Any such areas shall be designed in accordance with the provisions set forth in Section 2A.7 of the Downtown Ontario Design Guidelines, plus any additional features that may be required as part of a Conditional Use Permit for on-sale alcoholic beverages. Outdoor dining along Euclid will require an agreement with the California Department of Transportation, since this street is within a State right-of-way.

Outdoor Space

- Private balcony enclosures shall be comprised of materials that are complementary to materials and colors used for the main elevations. Each balcony shall be enclosed sufficiently to provide privacy between balconies of adjacent units. Any communications antennas placed within a balcony shall be screened.
- Common outdoor spaces, especially on the podium level, shall provide opportunities for social interaction and passive recreation, with shading elements to counteract the summer sun. An indoor recreation/fitness/community facility shall be provided within this central common area, with architectural style and materials that match or complement the primary building architecture. Landscaping shall be provided to define and enrich courtyards and walkways, and shall include specimen trees with drainage outlets that do not affect usable recreation space. Outdoor seating, tables and umbrellas, gazebos, barbecues, natural lawn area, and/or other place-making features are to be provided, and shall be compatible with the architectural style of the project. Hardscaping shall be composed of decorative paving materials and patterns, rather than a single, continuous surface. Outdoor common areas should be designed to be visible for surveillance by residents and police patrol units, if possible. A detailed plan for the common area open space shall be submitted as part of the Development Plan application for each of the mixed-use blocks.

Signs

- All commercial signs shall be designed in accordance with the criteria set forth in Section 2C of the Downtown Ontario Design Guidelines.

Lighting

- All lighting of facades, decorative fixtures, store window interiors, awnings, and signs shall be designed in accordance with the criteria set forth in Section 2D of the Downtown Ontario Design Guidelines.

4.2 CIVIC PLAZA (BLOCK B1)

Any new construction shall be designed to be compatible with the historical elements of the Bank of Italy Building to be retained at the southwest corner of this block, and shall be consistent with the design guidelines for the Mixed Use blocks, set forth in Section 4.1.

4.3 HOLT BOULEVARD RESIDENTIAL

4.3.1 APARTMENTS (BLOCK A2)

Building Orientation

- The buildings should be oriented to reinforce the urban grid of the street pattern in downtown Ontario. As many units as possible should be oriented to the street to help create an active streetscape. Dwelling units not facing the street should be oriented around a central courtyard. The podium level common area shall contain shared recreational elements (such as pools, spas, barbecues, picnic areas etc) for all of the residents to use.

Architectural Character

- Design buildings that complement the architectural character of the surrounding commercial and residential areas and support a pedestrian environment.
- Buildings should be designed to be viewed from all sides, including, public areas, common open space areas, sidewalks, alleys and streets. It is required that all 4 buildings elevations should share common materials and degrees of articulation.
- A schematic illustration of an acceptable set of elevations is provided as Exhibit 4-2.

Massing and Scale

- A maximum of 3 visible stories of living space and one visible story of parking structure below living space will define this building's block.
- Building heights and mass shall be sufficient to ensure compatibility among the various housing products and the surrounding commercial uses.

- elements
 - Roof lines should be varied through the use of stepped roofs, towers or other roof elements
 - A variety of stacked massing arrangements should be used to create visual interest.
 - Long blank walls should be broken up with architectural elements that add visual interest and enhance the character of the neighborhood. Architectural features such as setbacks, indentations, overhangs, projections, cornices, balconies can be used to create interesting building facades.
- The mass of the building should step down at the corners and entries to provide a greater definition of the building.
- planes.
 - Improve the articulation of the plan forms by offsetting upper floor and wall planes.
 - There shall be no more than 2 adjacent balconies on any single plane.

Articulation and Fenestration:

- A minimum of 15 percent of the entire horizontal length of any building elevation should be articulated by varying the form or footprint and/or by introducing architectural elements and/or projections.
 - Major building entries shall be clearly marked with architectural elements such as porches, arcades, ornamental lighting, landscaping and other elements.
 - Building offsets/setbacks should be a minimum of 12 inches in depth and 12 inches in width.
 - Balconies may be covered or open but should be an integral element of the building using details such as eaves, supports and railings.

Architectural Details

- Introduce architectural treatments at the base of the building to distinguish it from the top; this can be accomplished through the use of textured materials, variations in color and enriched landscaping.
- Architectural details should be consistent with the architectural style, materials and colors of the building.
- Within the appropriate style requirements, group and coordinate windows with other design elements to create a composition and sense of order.
- Multi-Paned windows are encouraged if appropriate to the style.



Holt Boulevard Elevation



Plum Avenue Elevation



B Street Elevation



Lemon Avenue Elevation

Exhibit

4-2

Apartment Architectural Character
Downtown Civic Center P.U.D.

not to scale

Material and Colors

- Employ high quality materials that are durable, lasting and aesthetically appealing.
- The use of complementary colors and changes in materials can add visual interest to a building; however, to avoid a false appearance, texture or color changes should not occur at external corners.

Screening of Parking Structure

- Where parking structure elements are visible from the street they shall be screened with landscape and/or architectural solutions, without conflicting with ventilation requirements set forth in the City's Mechanical Code. The method of screening shall be compatible with the overall architecture and landscaping themes. Any berms employed for screening shall not exceed a 2:1 slope and shall be appropriately landscaped with materials selected from the master plant palette (see Table 4-1). Exhibit 4-3 illustrates a preferred approach to screening of the parking structure.
- Visible parking structure elevation shall be finished in the same style as the primary buildings, with compatible materials. This will help integrate the structure into the overall project design while reducing its visual impact.

Roof Top and Ground Level Mechanical Equipment

- All mechanical equipment, whether mounted on the roof or ground, shall be screened from view. Rooftop equipment shall be screened from public rights-of-way and adjacent, existing buildings. Utility meters and equipment should be placed in locations that are not exposed to view from open areas or they should be suitable screened. All screening devices are to be compatible with the architecture and colors of the nearest main building.

Outdoor Space

- Private balcony enclosures shall be comprised of materials that are complementary to materials and colors used for the main elevations. Each balcony shall be enclosed sufficiently to provide privacy between balconies of adjacent units. Any communications antennae placed within balconies shall be screened.
- Common outdoor spaces shall provide opportunities for social interaction and passive recreation, with shading elements to counteract the summer sun. An indoor recreation/fitness/community facility shall be provided within this central common area, with architectural style and materials that match or complement the primary building architecture. A variety of landscaping elements shall be provided to define and enrich courtyards and walkways. Specimen trees shall be provided, each with drainage outlets that do not affect usable recreation space. Outdoor seating, tables and umbrellas, gazebos, barbeques, natural or lawn area, and/or other place-making features are to be provided, and shall be compatible with the architectural style of the project. Hardscaping shall be composed of decorative paving materials and patterns, rather than a single,

continuous surface. Outdoor common areas should be designed to be visible for surveillance by residents and police patrol units, if possible. A detailed plan for the common area open space shall be submitted as part of the Development Plan application for this block.

Landscaping

- Yards abutting the street frontages shall be landscaped with a uniform mixture of materials selected from Table 4-1 that provides a variety of vertical elements. Selection and placement of trees and shrubs shall ensure that adequate visibility into the parking structure is available for passing police patrol units.
- Any manufactured slopes shall not exceed a 2:1 slope and shall be landscaped with materials selected from the master plant palette set forth in Table 4.1 herein.

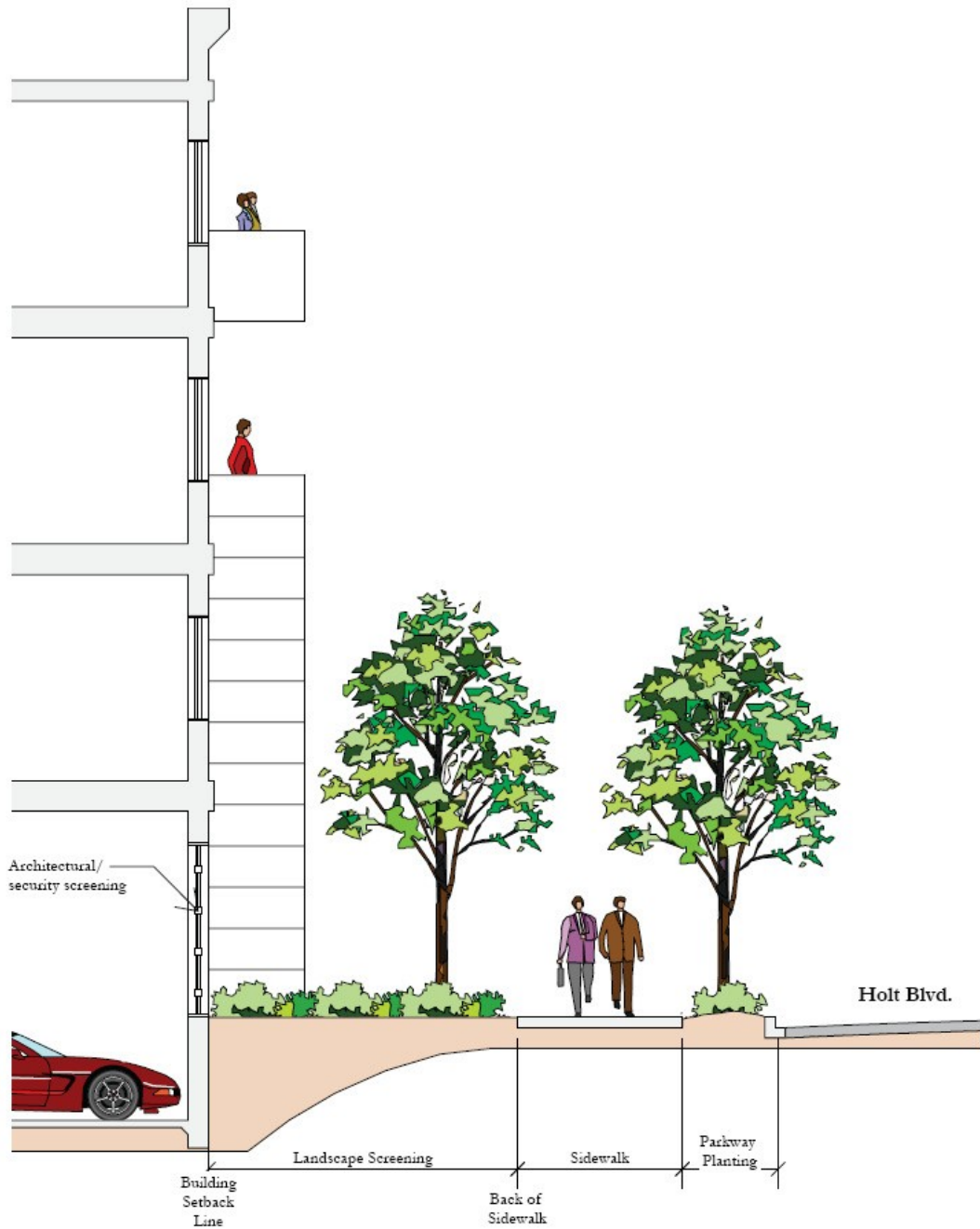
4.3.2 TOWNHOMES (BLOCKS A3/A4)

Building Orientation

- Where possible, townhome units should orient to the surrounding streets or interior open space elements.
- Every unit should have a porch/patio with an entry that overlooks and is directly accessible from the street or interior open space.
- The configuration of buildings, streets, parking, open space and landscaping must balance the needs of the pedestrian and automobile. Parking should not dominate the experience along any required path.

Architectural Character

- Design buildings that complement the architectural character of surrounding commercial and residential areas and support a pedestrian friendly environment.
- Buildings should be designed to be viewed from all sides, including, public areas, common open space areas, sidewalks, alleys and streets.
- Schematic illustrations of acceptable elevations are presented in Exhibit 4-4.



not to scale

Parking Garage Screening at Block A2
Downtown Civic Center P.U.D.

Exhibit
4-3



B' Street Front Elevation



Holt Boulevard Side Elevation



Interior Alley Elevation

Exhibit

Townhome Architectural Character
Downtown Civic Center P.U.D.

4-4

not to scale

Massing and Scale

- Rooflines should be varied through the use of hips, stepped roofs, gables, or other roof elements.
- Long blank walls should be broken up with architectural elements that add visual interest and enhance the character of the neighborhood. Architectural features such as setbacks, indentations, overhangs, projections, cornices, and bay windows should be used to create interesting building facades.
- Each individual dwelling unit should be expressed as an identifiable unit through its use of roof forms, change in building mass, defined entryways and other architectural treatments.
- Buildings along Sultana Avenue should be designed with sensitivity to massing and scale of nearby residential properties.

Articulation and Fenestration

- Building elevations should be articulated every 15 linear feet by varying the form or footprint and/or by introducing architectural elements and/or projections.
- A minimum of 15 percent of the entire horizontal length of any building elevation should be articulated by varying the form or footprint and/or by introducing architectural elements and/or projections.
- Entries shall be clearly marked with architectural elements such as porches, ornamental lighting, landscaping and other elements.
- Entrances from public sidewalks shall be prominent, visible from the street and connected by a walkway to the sidewalk.
- Building offsets or projections should be a minimum of 12 inches in depth and 12 inches in width.
- Windows should be inset from the face of the exterior wall to provide a visual relief and strong shadow line.

Architectural Details

- Introduce architectural treatments at the base of the building to distinguish it from the top; this can be accomplished through the use of textured materials, variations in color and enriched landscaping.
- Architectural details should be consistent with the architectural style, materials and colors of the building.

Material and Colors

- Exterior colors and materials shall be subdued and reflect the character and setting of downtown Ontario.

- Employ high quality materials that are durable, lasting and aesthetically appealing. Stucco and brick are preferred materials.

Alley Character & Entry Points

- Alleys should be designed with the same attention and level of detail as the streets. Landscaping and enhanced paving (pavers, colored concrete), etc. shall be used to facilitate alleyway and drive entrances that are both functional and attractive.
- A variety of massing, roof forms, and architectural massing shall be provided on the alley edge.
- The primary vehicular entries into the residential neighborhoods should provide ample planter space to create colorful and distinctive edges, including flowering perennials and annuals, and rows of trees.
- Each point of entry should be designed to allow for project monumentation and should create a sense of “welcoming” into the community.
- Homes abutting drive entries shall be separated a minimum of 40 feet at the ground level, and separated a minimum of 35 feet at second and third story levels.
- Examples of desired alley characteristics are shown in Exhibit 4-5.

Roof Top and Ground Level Mechanical Equipment:

- All mechanical equipment, whether mounted on the roof or ground, shall be screened from view. Rooftop equipment shall be screened from view from public rights-of-way and from adjacent, existing buildings. Utility meters and equipment should be placed in locations that are not exposed to view from open areas or they should be suitably screened. All screening devices are to be compatible with the architecture and colors of the building.
- Architectural screening elements should be constructed of the same materials and finishes as the primary building.
- If centralized mailbox locations are necessary to serve the residential community they shall be designed to be architecturally compatible with other buildings.

Outdoor Recreation Space

- Private, open ground level patios shall be finished with a combination of landscape and hardscape surfaces. The specific palette of materials shall be identified in a landscape plan to be submitted for approval by the City’s Landscape Planner, prior to issuance of building permits.
- Awnings, trellis structures, patio covers, and similar improvements shall not be attached to the main building, and shall not cover the entire patio area.
- A central common open space area shall provide opportunities for social interaction and passive recreation, with shading elements to counteract the summer sun. An indoor recreation/fitness/community facility shall be provided within this central common area, with architectural style and materials that match or complement the homes. A variety of landscape and

hardscape elements shall be provided to define and enrich courtyards and walkways. Specimen trees shall be provided. Outdoor seating, tables and umbrellas, gazebos, barbeques, natural lawn area, and/or other place-making features are to be provided, and shall be compatible with the architectural style of the project. Hardscaping shall be composed of decorative paving materials and patterns, rather than a single, continuous surface. Outdoor common areas should be designed to be visible for surveillance by residents and police patrol units, if possible. A detailed plan for the common area open space shall be submitted as part of the Development Plan application for the 2 blocks comprising the townhomes project.

Landscaping, Walls and Fences

- Yards abutting the street frontage shall be landscaped with a uniform mixture of plant materials that provide a variety of textures and massing. Each main drive entrance shall be bordered by landscape elements that include ground-level flowering materials, shrubs and a row of specimen trees, with a sidewalk on one side of the driveway.

- Where there are substantial expanses of exposed main building walls along a street frontage, treatments of the perimeter yards in those areas should include appropriate landscaping, fencing and/or other methods of deterring potential vandalism and graffiti.

- Private ground level patios shall be fully enclosed with a wall material that is complementary to the style and materials of the homes. Walls facing streets shall not exceed 42 inches high. Walls along interior common areas may be up to 6 feet high. Six-foot high walls composed of a masonry material that is complementary to the style and materials of the homes shall be placed at the end of alleys, to block views from the street into the alley.

Internal Pedestrian Circulation

- Landscaped walkways shall provide internal pedestrian circulation between Plum and Sultana Avenues and between Holt Boulevard and B Street. Such walkways may be part of common areas that separate opposing rows of homes or may be within a linear pathway between groupings of homes. These walkways are intended to provide convenient access to public sidewalks for project residents and are not intended to operate as public pass-thrus. Connections to public sidewalks along Holt may be gated and locked to limit access to residents only.



Enhanced Paving



Landscape Pockets



Building Undulation

Exhibit
4-5
Townhome Alley Character
Downtown Civic Center P.U.D.

4.4 LANDSCAPE ELEMENTS

Landscaping elements will provide significant contributions to the aesthetic quality and character of the Town Center, with respect to public and private spaces. Table 4-1 describes the master plant palette that will guide the selection of materials for street trees, perimeter trees and shrubs, podium courtyard and common area trees, vines, screening and hedge plants, drive alley shrubs, security/barrier plantings, and ground covers for a variety of applications. Detailed landscape/irrigation plans shall be included in the Development Plan submittals for each block.

Table 4.1: Downtown Civic Center Master Plant Material List

Botanical Name	Common Name	Size	Comments
Street Trees			
C Street			
Hymenosporum flavum	Sweetshade Tree	36" Box	Standard
Podocarpus henkelii	Yew Pine	36" Box	Standard
Washingtonia filifera	California Fan Palm	12' BTH	Standard
B Street			
Trachycarpus fortunei	Windmill Palm	8' BTH	Standard
Lagerstroemia indica	Crape Myrtle	36" Box	Standard
Pittosporum rhombifolium	Queensland Pittosporum	36" Box	Standard
Euclid Avenue			
Ginkgo biloba	Maidenhair Tree	24" Box	Standard
Quercus muehlenbergii	Chinquapin Oak	24" Box	Standard
Schinus molle	California Pepper	24" Box	Standard
Lemon Street			
Hymenosporum Flavum	Sweetshade Tree	36" Box	
Geijera parviflora	Australian Willow	36" Box	
Washingtonia filifera	California Fan Palm	24" Box	
Plum Avenue			
Lagerstroemia Indica	Crape Myrtle	36" Box	
Hymenosporum Flavum	Sweetshade Tree	36" Box	
Washingtonia filifera	California Fan Palm	24" Box	
Sultana Avenue			
Cercis reniformis 'Alba'	Texas White Redbud	36" Box	
Chitalpa tashkentensis	Chitalpa	36" Box	
Lagerstroemia indica	Crape Myrtle	36" Box	
Streetscape Trees – Trees Around Project Perimeter			

Botanical Name	Common Name	Size	Comments
Archontophoenix cunninghamiana	King Palm	12' BTH	
Cercis canadensis 'Forest Pansy'	Eastern Redbud	24" Box	Standard
Cercis reniformis 'Alba'	Texas White Redbud	24" Box	Standard
Hymenosporum flavum	Sweetshade Tree	36" Box	Standard
Lagerstroemia indica	Crape Myrtle	36" Box	Multi
Trachycarpus fortunei	Windmill Palm	8' BTH	
Tristania conferta	Brisbane Box	24" Box	Standard
Podium Courtyard Trees (A1, A2, C1)			
Archontophoenix cunninghamiana	King Palm	12' BTH	
Cercis reniformis 'Alba'	Texas White Redbud	24" Box	Standard
Maytenus boaria	Mayten Tree	24" Box	Standard
Melaleuca quinquinervia	Cajeput Tree	24" Box	Multi
Quercus ilex	Holly Oak	24" Box	Standard
Trachycarpus fortunei	Windmill Palm	8' BTH	
Tristania conferta	Brisbane Box	24" Box	Standard
Common Area Trees (A3/A4)			
Archontophoenix cunninghamiana	King Palm	12' BTH	
Cercis reniformis 'Alba'	Texas White Redbud	24" Box	Standard
Maytenus boaria	Mayten Tree	24" Box	Standard
Melaleuca quinquinervia	Cajeput Tree	24" Box	Multi
Quercus ilex	Holly Oak	24" Box	Standard
Trachycarpus fortunei	Windmill Palm	8' BTH	
Tristania conferta	Brisbane Box	24" Box	Standard
Drive Alley Trees (A3/A4)			
Callisetemon citrinus	Lemon Bottlebrush	24" Box	Standard
Maytenus boaria	Mayten Tree	24" Box	Standard
Phapiolepis 'Majestic Beauty'	India Hawthorn	24" Box	Standard
Tristania laurina	Water Gum	24" Box	Standard
Vines			
Vines – South/West Exposure			
Distictis buccinatoria	Blood Trumpet Vine	5 Gal	
Ficus pumila	Creeping Fig	5 Gal	
Gelsemium sempervirens	Carolina Jessamine	5 Gal	

Botanical Name	Common Name	Size	Comments
Vines – Sun/Part Shade Exposure			
Clytostoma callistegioides	Lavander Trumpet Vine	5 Gal	
Hardenbergia violacea	Lilac Vine	5 Gal	
Parthenocissus tricuspidata	Boston Ivy	5 Gal	
Trachelospermum jasminioides	Star Jasmine	5 Gal	
Shrubs			
Interior Common Area & Podium Courtyard Shrubs			
Agapanthus species	Lily Of The Nile, Queen Anne	1 Gal	
Asparagus syersii	Myers Asparagus	5 Gal	
Carex facca	Blue Sedge	1 Gal	
Buxus M. J. ‘Green Beauty’	Green Beauty Boxwood	5 Gal	
Camellia sasanqua	Camellia	5 Gal	
Ceanothus g.h. ‘Yankee Point’	Ceanothus Yankee Point	5 Gal	
Dianella Species	Flax Lily	15 Gal	
Diets hybrids	Fortnight Lily	1 Gal	
Hemerocallis ‘Bitsy’	Evergreen Bitsy Daylily (Yellow)	1 Gal	
Saliva ‘Bee’s Bliss’	‘Bee’s Bliss’ Saliva	1 Gal	
Limonium perezii	Sea Lavander	1 Gal	
Liriope muscari	Lily Turf	1 Gal	
Mahonia aquifolium	Oregon Grape	5 Gal	
Nandina d. ‘Firepower’	power Heavenly Bamboo	5 Gal	
Festuca mairei	Atlas Fescue	1 Gal	
Philodendron ‘Xanadu’	Xanadu Philodendron	5 Gal	
Phoenix roebelenii	Pigmy Date Palm	15 Gal	
Pittosporum Species	Mock Orange	5 Gal	
Rhapiolepis Species	India Hawthorn	5 Gal	
Rosa ‘Iceberg’	White Rose	5 Gal	
Rumhora adiantiformis	Leatherleaf Fern	5 Gal	
Spathiphyllum ‘Mauna Loa’	Spathiphyllum	5 Gal	
Strelitzia nicolai	Giant Bird of Paradise	15 Gal	
Strelitzia reginae	Bird Of Paradise	5 Gal	
Trachelospermum jasminioides	Star Jasmine	1 Gal	

Botanical Name	Common Name	Size		Comments
Streetscape & Perimeter Shrubs				
Agapanthus Species	Lily Of The Nile, Queen Anne	1 Gal		
Anigozanthos flavidus	Kangaroo Paw	1 Gal		
Buxus M.J. ‘Green Beauty’	Green Beauty Boxwood	5 Gal		
Dianella Species	Flax Lily	15 Gal		
Dietes Hybrids	Fortnight Lily	1 Gal		
Hemerocallis ‘Bitsy’	Evergreen Bitsy Daylilly (Yellow)	1 Gal		
Limonium perezii	Sea Lavander	1 Gal		
Pittosporum Species	Mock Orange	5 Gal		
Rhaphiolepis Species	Indian Hawthorn	5 Gal		
Rosa ‘Iceberg’	White Rose	5 Gal		
Rosmarinius Tuscan Upright	Upright Rosemary	5 Gal		
Salvia greggii ‘Furman’s Red’	Red Salvia Greggii	5 Gal		
Strelitzia reginae	Bird Of Paradise	5 Gal		
Screening & Hedge Plants				
Grewia occidentalis	Lavander Starflower	5 Gal		Espalier
Ligustrum texanum	Wax Leaf Privet	15 Gal		
Prunus caroliniana	Carolina Laurel Cherry	15 Gal		
Xylosma congestum	Xylosma	5 Gal		
Drive Alley Shrubs				
Dianella Species	Flax Lily	5 Gal		
Dietes hybrids	Fortnight Lily	1 Gal		
Hemerocallis hybrids	Evergreen Daylilly	1 Gal		
Nandina domestica	Hevenly Bamboo	5 Gal		
Podocarpus henkelii	Yew Pine	15 Gal		
Strelitzia reginae	Bird Of Paradise	5 Gal		
Security/Barrier Planting				
Bougainvillea Spp	Bougainvella	5 Gal		
Carissa macrocarpa	Natal Palm	5 Gal		
Hakea suaveolens	Sweet Hakea	5 Gal		
Rosa ‘Floribundas’	Rose	5 Gal		
Ground Covers				
Annual Color	Annual Color	4” Pots	8” oc	Color By L.A.

Botanical Name	Common Name	Size		Comments
Dwarf Tall Fescue	Marathon Li	Sod		
Campanula poscharskyana	Bellflower	Flats	12" oc	Triangular Space
Duchesnea indica	Indian Mock Strawberry	Flats	12" oc	Triangular Space
Gazania 'Mitsuwa Yellow'	Yellow Gazania	Flats	13" oc	Triangular Space
Lantana montevidensis 'Alba'	Lantana	Flats	18" oc	Triangular Space
Myoproum parvifolium	Prostrate Myoproum	Flats	12" oc	Triangular Space
Pelagonium 'Balkan Pink' prachelospermum	Pink Ivy Geranium	Flats	12" oc	Triangular Space
Jasminioides	Star Jasmine	Flats	18" oc	Triangular Space
Vinca Minor	Dwarf Periwinkle	Flats	12" oc	Triangular Space

4.5 STREETSAPES

Streetscape elements shall be submitted for approval by the Planning and Public Works Directors. A comprehensive master streetscape plan for the entire project limits shall be submitted prior to issuance of building permits for the first block to be developed, or separate streetscape plans shall be submitted prior to issuance of building permits for each block. Each streetscape plan shall include the elements set forth in Sections 4.5.1 to 4.5.2 herein.

4.5.1 STREET LIGHTING

Human scale and continuation of a modernized, yet traditional American downtown character will govern the selection of streetlights. A combination of Marblelight and "King" standard lighting (City of Ontario Standard Drawing No. 5103) shall be required within the public right-of-way, with LED fixtures along Euclid Avenue and Holt Boulevard. King standard post-top lights shall be placed along all other streets. Light spacing and wattage shall be as determined by the City Engineer.

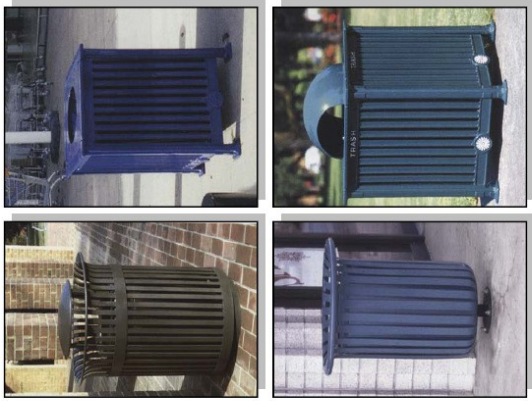
All post-top lamps shall incorporate shielding within the luminaire and/or globe to prevent glare into upper level residential windows. All Marblelight fixtures shall incorporate cut-off shields to prevent glare into upper level residential windows.

4.5.2 STREET FURNITURE AND SIDEWALK ENHANCEMENTS

Street furniture will help to create a special character to reflect the traditional American downtown theme and character. The use of repetitive materials and textures will create a continuity and consistency that is desired throughout the downtown area. A variety of decorative elements will be incorporated into the public sidewalk zone, including bus shelters, benches, tree grates, bike racks, enhanced paving, trash receptacles and possibly bollards. Examples of acceptable materials and styles are illustrated in Exhibit 4-6. Not all elements are required along every street. Selection of appropriate elements shall consider sidewalk widths, size and spacing of street trees, proximity to store entrances, importance of the pedestrian path, and the requirement to maintain a minimum 4 feet clear space for pedestrian movement in all areas. Bus shelters/benches, street furniture and sidewalk enhancements shall be identified on a master streetscape plan to be submitted as part of the Development Plan submitted for each block, or as part of a Development Plan for the overall project. General characteristics to be incorporated into these features include:

- All street furniture shall comply with the Americans with Disabilities Act of 1990
- Benches shall have arms and backs
- A powder-coated, dark finish shall be applied to all fixtures and furniture
- All tree grates shall be iron with 3/8" max slots, with 30" center opening for tree skating and future trunk growth
- Bollards may be placed at entrances to bicycle/pedestrian pathways and where controlled emergency or maintenance access is required or desired. A bottom locking mechanism shall be provided to enable easy removal by authorized City personnel.
- Litter receptacles shall have a fixed top
- Design of bus stop benches and shelters shall satisfy the criteria specified by the local transit agency (currently Omnitrans)
- Bike racks shall be located adjacent to parking areas or bicycle/pedestrian pathways

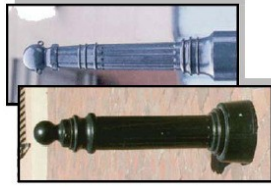
Other street amenities may be placed in appropriate locations, such as kiosks, newspaper racks, drinking fountains and utility covers. Such amenities shall be comprised of the same character and color as the other street furniture elements.



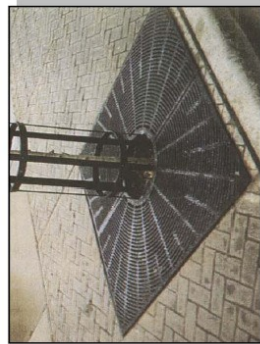
Trash Receptacles



Street Benches



Bollards

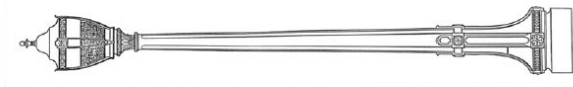


Tree Grates / Enhanced Paving

**Street Furniture &
Sidewalk Enhancement**
Downtown Civic Center P.U.D.

Exhibit

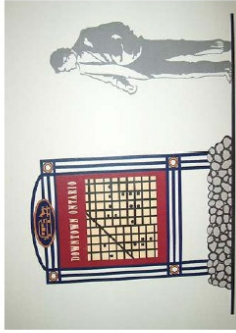
4-6



King French Intermediate Street Lamp



Banners on Light Poles



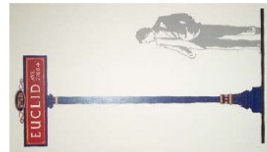
Downtown Map Sign



Interpretive Historic Panel



Downtown Directional Signs



Downtown Street Signs



Street Lighting, Thematic Street Signs & Markers

Exhibit

4-7

Downtown Civic Center P.U.D.

5.0 ADMINISTRATION

5.1 DEVELOPMENT CODE GOVERNS IF NOT ADDRESSED IN PUD

Any terms not defined herein and any issues affecting the design and development of the subject project area that not specifically addressed in this PUD shall be governed by the provisions of the Ontario Development Code.

5.2 DEVELOPMENT APPLICATION PROCESSING

A Development Plan shall be submitted for each block, for approval by the Development Advisory Board and the Ontario Planning Commission, pursuant to the Ontario Development Code..

5.3 OTHER APPLICATIONS TO FACILITATE IMPLEMENTATION OF PUD

5.3.1 CHERRY AVENUE VACATION

Blocks A3 and A4 are to be merged to create a single development site for the proposed townhomes. This will require vacation and abandonment of the existing Cherry Avenue right-of-way that separates these blocks. This street vacation/abandonment shall be completed prior to issuance of a building permit for the townhomes project.

5.3.2 CERTIFICATE OF APPROPRIATENESS

To ensure proper implementation of the historic resource preservation and mitigation measures established in the Final EIR for the Downtown Civic Center Project, an application for a Certificate of Appropriateness ("C of A") shall be submitted and approved in conjunction with each block Development Plan. Application requirements and required findings for approval of a C of A are specified in the Ontario Development Code.

5.4 CEQA COMPLIANCE

The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. The proposed vacation of Cherry Avenue was not addressed in the Final EIR; therefore, an Addendum to the Ontario Downtown Civic Center Project EIR (SCH No. 200405115) was prepared. This Addendum determined that this PUD application would not introduce any new significant environmental impacts. All applicable mitigation measures adopted with the previous environmental assessment will become a condition of project approval. The City of Ontario will continue to be responsible for implementation of the Mitigation Monitoring and Reporting Program adopted for the Downtown Civic Center Project. This may involve application of one or more mitigation measures to implement this PUD, or fair-share proportions thereof.

5.5 ADMINISTRATIVE EXCEPTIONS

Exceptions to a maximum of 10 percent of any of the development standards set forth in Sections 3.1 to 3.5 may be granted by the Zoning Administrator, in accordance with the Ontario Development Code.

APPENDIX A: ORDINANCES ADOPTING PUD
(Appendix A follows this page)

ORDINANCE NO. 3180

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PUD20-002, AN AMENDMENT TO THE DOWNTOWN CIVIC CENTER PLANNED UNIT DEVELOPMENT ("DCCPUD"), AMENDING CERTAIN PROVISIONS TO: [1] BRING THE DCCPUD INTO CONSISTENCY WITH THE POLICY PLAN (GENERAL PLAN) COMPONENT OF THE ONTARIO PLAN AND THE CITY'S DEVELOPMENT CODE; [2] AMEND CERTAIN LAND USE AND DEVELOPMENT STANDARDS APPLICABLE TO BLOCKS A1 AND C1, INCLUDING BUT NOT LIMITED TO ALLOWING FOR GROUND FLOOR RESIDENTIAL LAND USES, AT GRADE STAND-ALONE PARKING STRUCTURES, AND AN INCREASE IN THE MAXIMUM HEIGHT OF BUILDING ELEMENT PROJECTIONS (FROM 60 FEET TO 65 FEET), REVISION TO ALLOW VEHICULAR ACCESS POINTS, AND MODIFY MINIMUM PARKING STALL AND DRIVE AISLE STANDARDS; AND [3] MAKE VARIOUS REVISIONS NECESSARY TO THE INTERNAL CONSISTENCY OF THE DCCPUD AND MAKING FINDINGS IN SUPPORT THEREOF— APNS: 1048-547-29, 1048-547-36, 1048-553-15, 1048-551-13, 1048-552-13, 1048-553-13, 1048-547-33, 1048-547-34, 1048-553-02, 1048-553-04, 1048-547-30, 1048-552-21, 1048-547-32, 1048-551-11, 1048-547-94, 1048-553-11, 1048-547-31, 1048-553-06, 1048-551-10, 1048-547-39, 1048-547-43, 1048-553-09, 1048-553-10, 1048-547-42, 1048-551-12, 1048-553-16, 1048-553-05, 1048-553-14, 1048-552-17, 1048-552-17, 1048-553-17, 1048-547-40, 1048-553-08, 1048-554-11, 1048-553-12, 1048-547-44, 1048-547-37, 1048-553-07, 1048-547-35, 1048-553-01, 1048-553-03, 1048-547-87, 1048-548-30, 1048-547-81, 1048-547-88, 1048-547-69, 1048-547-76, 1048-548-40, 1048-547-47, 1048-547-89, 1048-547-64, 1048-547-85, 1048-547-51, 1048-547-77, 1048-547-38, 1048-547-70, 1048-547-45, 1048-548-12, 1048-547-71, 1048-547-73, 1048-547-56, 1048-547-92, 1048-547-83, 1048-548-02, 1048-547-49, 1048-547-46, 1048-547-48, 1048-547-91, 1048-548-24, 1048-547-41, 1048-548-47, 1048-547-90, 1048-548-54, 1048-548-15, 1048-547-72, 1048-547-82, 1048-547-50, 1048-547-68, 1048-547-84, 1048-547-86, 1048-547-52, 1048-548-48, 1048-547-80, 1048-547-78, 1048-548-32, 1048-547-55, 1048-547-66, 1048-548-21, 1048-548-23, 1048-547-62, 1048-548-06, 1048-547-63, 1048-547-79, 1048-547-61, 1048-547-59, 1048-548-25, 1048-548-31, 1048-548-20, 1048-548-05, 1048-548-46, 1048-548-03, 1048-548-41, 1048-548-42, 1048-548-27, 1048-547-60, 1048-548-34, 1048-548-43, 1048-548-26, 1048-548-01, 1048-547-67, 1048-548-29, 1048-547-65, 1048-548-04, 1048-548-22, 1048-547-54, 1048-548-07, 1048-548-44, 1048-547-75, 1048-547-57, 1048-547-58, 1048-548-45, 1048-547-27, 1048-547-21, 1048-547-23, 1048-547-15, 1048-552-15, 1048-548-08, 1048-548-11, 1048-548-49, 1048-547-16, 1048-548-52, 1048-548-18, 1048-548-13, 1048-548-09, 1048-548-53, 1048-548-28, 1048-547-22, 1048-547-12, 1048-548-39, 1048-552-22, 1048-552-22, 1048-548-36, 1048-548-19,

1048-547-18, 1048-548-33, 1048-548-35, 1048-552-18, 1048-548-16,
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1048-547-14, 1048-548-37, 1048-552-14, 1048-552-14, 1048-548-14,
1048-547-19, 1048-547-11, 1048-547-17, 1048-547-09, 1048-547-10,
1048-547-04, 1048-547-08, 1048-547-07, 1048-547-05, 1048-547-06.

WHEREAS, City of Ontario ("Applicant") has initiated a request for the approval of an amendment to the Downtown Civic Center Planned Unit Development, File No. PUD20-002, as described in the title of this Ordinance (hereinafter referred to as "Application" or "Project"); and

WHEREAS, in June 2006, the City Council adopted Ordinance No. 2829 approving the Downtown Civic Center Planned Unit Development ("DCCPUD"), which governs land use and development within a 6-block area within the heart of the City's original downtown, including 3 blocks along Holt Boulevard, between Lemon Avenue and Sultana Avenue, and 3 blocks along Euclid Avenue, between Holt Boulevard and D Street; and

WHEREAS, in July 2011, the City Council adopted Ordinance No. 2937 approving an amendment to the DCCPUD, which increased the maximum residential dwelling units per acre from 45-50 dwelling units per acre to 45-65 dwelling units per acre and clarifying that "for rent" and "for sale" housing is allowed within the Commercial/Residential Mixed Use District; and

WHEREAS, the properties to the north are located within the MU-1 (Mixed Use Downtown) and Civic zoning districts. Properties to the south, east, and west are located within the MU-1 (Mixed Use Downtown) zoning districts; and

WHEREAS, the surrounding land uses include commercial to the west, across Euclid Avenue, commercial, civic, and residential land uses to the east, retail to the north, and retail and office to the south, across Holt Boulevard; and

WHEREAS, the DCCPUD Amendment primarily applies to the Commercial/Residential Mixed-Use District, affecting A1 block, generally located at the northeast corner of Euclid Avenue and Holt Boulevard, and C1 block, generally located at the southeast corner of Euclid Avenue and D Street; and

WHEREAS, the DCCPUD Amendment will allow for ground floor residential land uses, at grade stand-alone parking structures, and an increase in height of building element projections, such as elevators and towers, from 60 feet to 65 feet. Compact parking stalls will be eliminated for compliance with the Development Code and a reduction in drive aisle width from 26 feet to 24 feet may be allowed on a case-by-case basis. The amendment also proposes on the C1 block to relocate the vehicular access on D Street to Lemon Avenue. Other minor revisions proposed are intended to clarify or update standards which reflect applicable Development Code standards and overall industry best practices; and

WHEREAS, the Project is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an initial study has been prepared to determine possible environmental impacts; and

WHEREAS, the environmental impacts of this project were previously reviewed in conjunction with File No. PUD08-001, an amendment to the DCCPUD for which an Addendum to the Ontario Downtown Civic Center Environmental Impact Report (SCH # 200405115, certified on November 16, 2004), was adopted by the City Council on June 21, 2011. This Project introduces no new significant environmental impacts; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the City Council the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport ("ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (ALUCP), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on February 23, 2021, the Planning Commission of the City of Ontario conducted a public hearing to consider the Project and concluded said hearing on that date, voting to issue Resolution No. PC21-003, recommending the City Council approve the Application; and

WHEREAS, on March 16, 2021, the City Council of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Ordinance have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND ORDAINED by the City Council of the City of Ontario, as follows:

SECTION 1. *Environmental Determination and Findings.* As the decision-making body for the Project, the City Council has reviewed and considered the information contained in the previous Certified Ontario Downtown Civic Center

Environmental Impact Report and supporting documentation. Based upon the facts and information contained in the previous Certified Ontario Downtown Civic Center Environmental Impact Report and supporting documentation, the City Council finds as follows:

(1) The environmental impacts of this project were previously reviewed in conjunction with File No. PUD08-00, a Planned Unit Development for which the Ontario Downtown Civic Center Environmental Impact Report was adopted by the City Council on November 16, 2004; and

(2) The previous Certified Ontario Downtown Civic Center Environmental Impact Report contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous Certified Ontario Downtown Civic Center Environmental Impact Report was completed in compliance with CEQA and the Guidelines promulgated thereunder; and

(4) The previous Certified Ontario Downtown Civic Center Environmental Impact Report reflects the independent judgment of the City Council; and

(5) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the previous Certified Ontario Downtown Civic Center Environmental Impact Report, and all mitigation measures previously adopted with the Certified Ontario Downtown Civic Center Environmental Impact Report, are incorporated herein by this reference.

SECTION 2. *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the City Council finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the Project does not eliminate or reduce the number of dwelling units and density specified in the Available Land Inventory.

SECTION 3. *Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local development regulations, land use plans, and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport ("ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within

the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the City Council has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the City Council, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 4. Concluding Facts and Reasons. Based upon the substantial evidence presented to the City Council during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 3, above, the City Council hereby concludes as follows:

(1) ***The proposed PUD, or amendment thereto, is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The Project is consistent with Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. Section 1.1, The Ontario Plan Objectives of the DCCPUD, describes the manner in which the PUD complies with the Policy Plan goals and policies.

(2) ***The proposed PUD, or amendment thereto, would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City.*** The Ontario Plan and Downtown Civic Center Environmental Impact Report include safeguards and impose certain mitigation measures to ensure that development within the DCCPUD boundary area would not be detrimental to public interest, health, safety, or general welfare of the City.

(3) ***In the case of an application affecting specific property(ies), the proposed PUD, or amendment thereto, will not adversely affect the harmonious relationship with adjacent properties and land uses.*** The DCCPUD area is situated in the heart of Ontario's downtown and is adjacent to residential and commercial land uses and development. The surrounding development supports the objective of the DCCPUD further commercial and residential mixed-use development. The Project does not affect the harmonious relationship with adjacent properties and land use.

(4) ***In the case of an application affecting specific property(ies), the subject site is physically suitable, including, but not limited to, parcel size, shape, access, and availability of utilities, for the request and anticipated development.*** The Project does not propose any changes to the project boundary limits, parcel size, shape, or the availability of utilities that would impact future development.

(5) ***The proposed PUD is superior to that which could be obtained through the application of the Development Code or a specific plan.*** The Project to the DCCPUD does not propose changes any major deviation from the development standards

SECTION 5. City Council Action. Based upon the findings and conclusions set forth in Sections 1 through 4, above, the City Council hereby APPROVES the herein described Amendment to the DCCPUD (File No. PUD20-002), attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 6. Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 7. Severability. If any section, sentence, clause or phrase of this Ordinance or the application thereof to any entity, person or circumstance is held for any reason to be invalid or unconstitutional, such invalidity or unconstitutionality shall not affect other provisions or applications of this Ordinance which can be given effect without the invalid provision or application, and to this end the provisions of this Ordinance are severable. The People of the City of Ontario hereby declare that they would have adopted this Ordinance and each section, sentence, clause or phrase thereof, irrespective of the fact that any one or more section, subsections, sentences, clauses or phrases be declared invalid or unconstitutional.

SECTION 8. Effective Date. This Ordinance shall become effective 30 days following its adoption.

SECTION 9. Publication and Posting. The Mayor shall sign this Ordinance and the City Clerk shall certify as to the adoption and shall cause a summary thereof to be published at least once, in a newspaper of general circulation in the City of Ontario, California within 15 days following the adoption. The City Clerk shall post a certified copy of this ordinance, including the vote for and against the same, in the Office of the City Clerk, in accordance with Government Code Section 36933.

PASSED, APPROVED, AND ADOPTED this 6th day of April 2021.



PAUL S. LEON, MAYOR

ATTEST:



SHEILA MAUTZ, CITY CLERK

APPROVED AS TO FORM:



BEST BEST & KRIEGER, LLP
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Ordinance No. 3180 was duly introduced at a regular meeting of the City Council of the City of Ontario held March 16, 2021 and adopted at the regular meeting held April 6, 2021 by the following roll call vote, to wit:

AYES: MAYOR/COUNCIL MEMBERS: LEON, WAPNER, BOWMAN,
DORST-PORADA AND VALENCIA
NOES: COUNCIL MEMBERS: NONE
ABSENT: COUNCIL MEMBERS: NONE


SHEILA MAUTZ, CITY CLERK

(SEAL)

I hereby certify that the foregoing is the original of Ordinance No. 3180 duly passed and adopted by the Ontario City Council at their regular meeting held April 6, 2021 and that Summaries of the Ordinance were published on March 23, 2021 and April 13, 2021, in the Inland Valley Daily Bulletin newspaper.


SHEILA MAUTZ, CITY CLERK

(SEAL)

APPENDIX B: MITIGATION MONITORING PROGRAM

(Appendix B follows this page)

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MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
Air Quality	Emissions from project construction equipment.	MM Air 1: Maintain equipment and vehicle engines in good condition and in proper tune as per manufacturer's specifications.	Include in construction document specifications to be implemented during Construction	City of Ontario construction document plan checker. Contractor
Air Quality	Emissions from project construction equipment.	MM Air 2: Prohibit all vehicles from idling in excess of ten minutes, both on-site and off-site.	Include in construction document specifications to be implemented during Construction	City of Ontario construction document plan checker. Contractor
Air Quality	Dust emissions during construction activities.	MM Air 3: Water active grading sites at least twice daily. Water unpaved roads or surfaces at least twice daily. Water surfaces before grading.	Include in construction document specifications to be implemented during Construction	City of Ontario construction document plan checker. Contractor
Air Quality	Dust emissions during construction activities.	MM Air 4: Trucks hauling dirt, sand, gravel or soil are to be covered or should maintain at least two feet of freeboard, in accordance with Section 23114 of the California Vehicle Code.	Include in construction document specifications to be implemented during Construction	City of Ontario construction document plan checker. Contractor
Air Quality	Dust emissions during construction activities.	MM Air 5: Reduce on-site vehicle speed to less than 15 mph.	Include in construction document specifications to be implemented during Construction	City of Ontario construction document plan checker. Contractor
Air Quality	Dust emissions during construction activities.	MM Air 6: Sweep nearby or adjacent streets at the end of the day if visible soil material is carried over from construction site.	Include in construction document specifications to be implemented during Construction	City of Ontario construction document plan checker. Contractor
Air Quality	Dust emissions during construction activities.	MM Air 7: Suspend all grading and excavating operations when wind speeds exceed 25 mph.	Include in construction document specifications to be implemented during Construction	City of Ontario construction document plan checker. Contractor
Air Quality	Dust emissions during construction activities.	MM Air 8: Hydroseed or apply soil stabilizers to inactive construction areas left inactive for ten days or more, or replant vegetation in disturbed areas as soon as possible.	Include in construction document specifications to be implemented during Construction	City of Ontario construction document plan checker. Contractor

MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
Air Quality	Impacts to air quality due to long-term emissions.	MM Air 9: The project will participate in the cost of off-site improvements through fair-share payment of the Development Impact fee as established by the City of Ontario. These fees should be collected and utilized as needed by the City to construct the improvements necessary to maintain the required level of service.	Prior to building permits As required to maintain required LOS	Developer/Housing Authority Pays. Engineering implements improvements
Air Quality	Impacts to air quality due to long-term emissions.	MM Air 10: Local transit agencies (Omnitrans and RTD) shall be contacted to determine bus routing in the project area that can accommodate bus stops at the project access points and the project shall provide bus passenger benches and shelters at these project access points. See MM 22.	Prior to site plan approvals for overall project concept for location Prior to issuance of last Certificate of Occupancy for construction	Planning Department, Developer and Omnitrans
Cultural	Impact due to loss of, or significant alteration of an historic resource.	MM Cultural 1: Prior to issuance of building permits, determination of the status of historical designation of each structure within the project area shall be completed by City Planning Department staff and the Historic Preservation Commission, as required in City Development Code. Table III-2-C shall be consulted in order to determine the mitigation measures required based on the status of historical designation. On the vertical axis, Table III-2-C lists the possible “status of historical designation” to which a property could be subject. The horizontal axis shows all the potential actions that could occur to each building in the project area and lists the appropriate mitigation measures required for each.	Prior to the issuance of building permits, the Planning Department shall be consulted, and historical designations verified. Prior to demolition or building permits, Table III-2-C shall be consulted	Planning Department Staff Developer provides proof of completion of mitigation
Cultural	Undocumented cultural/archaeological resources.	MM Cultural 2: Should any cultural and/or archaeological resources be accidentally discovered during construction; construction activities shall be moved to other parts of the project site and a qualified archaeologist shall be contacted to determine the significance of these resources. If the find is determined to be an historical or unique archaeological resource, as defined in Section 15064.5	If found during construction	Contractor and City Staff

MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
		of the CEQA Guidelines, avoidance or other appropriate measures shall be implemented.		
Cultural	Undocumented cultural/archaeological resources.	MM Cultural 3: If paleontological resources are identified during any excavations, construction activities shall be moved to other parts of the project site and a qualified paleontologist shall be contacted to determine the significance of these resources. If the find is determined to be significant, avoidance or other appropriate measures shall be implemented. One appropriate measure would include that a qualified paleontologist shall be permitted to recover and evaluate the find(s) in accordance with current standards and guidelines.	If found during construction	Contractor and City staff
Cultural	Discovery of human remains	MM Cultural 4: In the event of the accidental discovery or recognition of any human remains during excavation/construction, there shall be no further excavation or disturbance of the site or any nearby area reasonably suspected to overlie adjacent human remains until the County Coroner has been contacted and any required investigation or required Native American consultation has been completed.	If found during construction	Contractor and City staff
Geology	Erosion due to wind	MM Geo 1: To reduce impacts associated with erosion due to high winds, prior to construction, all development/ redevelopment plans will apply for and adhere to the permit given by the City of Ontario and enforced by the Building Official found in Title 6, Chapter 12, sections 6-12.01 – 6-12.07. The permit lasts for one (1) year, therefore all construction lasting for a period of more than one calendar year from the date of issue will reapply for the permit and pay the annual fee of \$250 plus \$5 per acre for each acre over ten acres.	Prior to issuance of grading or demolition permits	Building official
Geology	Construction on an unstable geologic unit	MM Geo 2: Prior to approval of all development plans in the Downtown Ontario Civic Center project area, site-specific geotechnical report(s) shall be	Prior to approval of development plans.	Building and Engineering Department

MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
		submitted to the City of Ontario's Building and Engineering Departments for review and approval. The recommendations provided in the geotechnical report shall be incorporated into the design of the project, or portion of the project under construction.		
Hazards	Release of asbestos through demolition of asbestos-containing materials	MM Haz 1: A comprehensive survey for asbestos-containing materials (ACM) that meets the requirements of the South Coast Air Quality Management District's Rule 1403 shall be performed by the City of Ontario on all buildings built prior to 1980 that are proposed to be altered or demolished. This mitigation measure shall apply to properties 2, 5, 8, 12, and 19 referenced in Table III-4-A and other properties listed in Table III-4-B that do not have a reference number. ACM shall be removed by a State-licensed asbestos abatement contractor prior to demolition or burning.	Prior to demolition	Housing Authority and Developer
Hazards	Lead-based paint exposure	MM Haz 2: In order to reduce potential impacts related to lead-based paint exposure and/or disposal, and because it is not certain which buildings will be demolished, if any building identified in an Environmental Site Assessment (ESA) or if constructed in 1978 or earlier, than a lead-based paint survey shall be conducted. Buildings 2, 5, 7, 12 (Table III-4-A) have been identified as having lead-based paint, either through a previous ESA, or through a subsequent lead-based paint survey. Lead abatement and/or proper disposal shall be conducted by a qualified specialist.	Prior to demolition and/or Construction	Housing Authority and Developer
Hazards	Oil-stained concrete pads	MM Haz 3: For oil-stained areas in, and around Richard's Beauty College (200 N. Euclid Avenue) identified in the Phase I Environmental Site Assessment prepared by P & D Environmental Report No. 8 in Table III-4-A (June 18, 2003: Project No. 174717.0043), the City of Ontario shall be responsible for excavation and proper disposal of oil-	Prior to building permits.	Housing Authority and Developer

MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
		stained concrete pads (since it was determined in the Phase II that soil underlying the concrete had not been significantly contaminated, though the stained pads remain).		
Hazards	Undocumented hazardous materials	MM Haz 4: In the event that construction reveals arterial believed to be hazardous waste, as defined in Section 25117 of the California Health & Safety Code, the developer shall contact the City of Ontario Fire Department Hazardous Materials Division and the County of San Bernardino Environmental Health Department. Excavation shall be stopped until the material has been tested and the presence of hazardous waste has been confirmed. If no hazardous waste is present, excavation may continue. If hazardous waste is determined to be present, the California Department of Toxic Substances Control shall be contacted, and the material shall be removed and disposed of pursuant to applicable provisions of California law.	If found during construction	Developer
Hazards	Undocumented hazardous materials	MM Haz 5: In the event that during alteration of an existing building hazardous materials are discovered, and that they are not removed as part of the building's rehabilitation, the building shall be placed on an appropriate hazardous materials database by the City of Ontario.	If found during construction	Building Department and Developer
Hazards	Underground storage tanks	MM Haz 6: The underground tanks used at the old Police Facility have been removed and properly abated. If any underground tanks are discovered during construction, the developer, in coordination with the County Fire Department, shall remove them. If above ground tanks are removed as part of this project, a replacement plan for at least one 500-gallon tank/fueling station to support City operations near the Civic Center should be implemented.	If required.	Developer, County Fire Department for removal issues, City for replacement issue.
Hazards	Potential impacts to evacuation routes and	MM Haz 7: During construction, access from adjacent homes and businesses and two-way traffic	During construction	Contractor

MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
	other streets.	flow must be specifically maintained on Euclid Avenue and Holt Boulevard, which are designated “evacuation routes” with detours and/or flagmen. Access and two-way traffic flow on Sultana Avenue and “D” Street must also be maintained with detours and/or flagmen to the satisfaction of the Ontario City Fire Department.		
Hazards	Potential interference with air traffic, height restrictions.	MM Haz 8: Structures within the project area cannot exceed 122 feet from the site elevation of 980 feet above sea level including temporary structures such as cranes used during construction.	Include in construction document specifications to be implemented during Construction	Building Department, Contractor, Site Inspectors
Hazards	Potential unwanted noise impacts to future landowners or lessees.	MM Haz 9: To disclose to the buyer or lessee of subdivided lands within the Civic Center project of the proximity of this site to the Ontario International Airport as required by AB 2776, the City shall disclose, and ensure that the developer makes such disclosures, as required by law to all future buyers.	Prior to specified filings and sale agreements as stated in AB 2776	Housing Authority and Developer
Hydrology	Violation of water quality or waste discharge requirements.	MM Hydro 1: In order to ensure that construction activities associated with the Ontario Downtown Civic Center project will not cause a violation of any water quality standard or waste discharge requirements, and to assure no substantial degradation of water quality occurs, developments within the project area shall comply with all applicable provisions of the State’s General Permit for Construction Activities (Order No. 99-08- DWQ, or most recent version) during all phases of construction.	Prior to grading and during construction.	Developer and Contractor
Hydrology	Violation of water quality or waste discharge requirements.	MM Hydro 2: In order to ensure that the Ontario Downtown Civic Center project will not cause or contribute to violations of any water quality standard or waste discharge requirements, and to assure no substantial degradation of water quality occurs, the project will complete a Water Quality Management Plan (WQMP) pursuant to the MS4 permit (Order No. 2002-0012) adopted by the City of Ontario. The	Prior to development plan approval	Developer, Planning and Engineering Department

MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
		project shall incorporate Site Design BMPs and Source Control BMPs, and potentially Treatment Control BMPs. See Table III-5-E in EIR.		
Hydrology	Violation of water quality or waste discharge requirements.	MM Hydro 3: To assure that development of the Ontario Downtown Civic Center project will not cause a violation of any water quality standard or waste discharge requirements, including San Bernardino County's MS4 permit issued by the SARWQCB, and to assure that no substantial degradation to water quality occurs after construction, any loading docks present within the office, academic or retail areas specified in the project description will be designed with devices to trap oil and grease, such that these pollutants are not discharged from the site in storm water or non- storm water discharges.	Prior to development plan approval and building	Developer, Public Works, Engineering Department and Building Official
Hydrology	New storm drain facilities	MM Hydro 4: In the event that connections to the existing storm drain system are required, each development within the Ontario Downtown Civic Center Project will be required to pay a drainage impact fee.	Prior to grading permits	Developer and Building Department
Land Uses & Aesthetics	Safety hazards and noise	MM LU 1: To limit exposure to noise from traffic and traffic hazards for children playing along busy streets, no ground floor outdoor residential use areas shall be allowed to front along Holt Boulevard or Euclid Avenue.	Prior to site plan approval	Planning Department
Land Uses & Aesthetics	Land use compatibility and aesthetics related to residential historic neighborhood.	MM LU 2: To address both aesthetic and land use compatibility issues, design of new structures located along 'D' Street and Sultana Avenue shall be sensitive to the mass, scale, and architectural style of the existing residential areas located east and north of the project area.	Prior to site and architectural plan approvals	Planning and Building Departments
Land Uses & Aesthetics	Land use compatibility and aesthetics related to historic commercial	MM LU 3: New construction and adaptive reuse located along and adjacent to Euclid Avenue shall be sensitive to historic structures on- and off-site. (See also mitigation measures in the Cultural Resources	Prior to architectural plan approvals	Planning and Building Departments

MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
	buildings	section of this EIR.)		
Land Uses & Aesthetics	Safety in public parks	MM LU 4: Parks and open spaces shall be designed for ease of resident and police surveillance.	Prior to site plan approvals	Planning and Police Departments
Noise	Construction noise	MM Noi 1: The construction activities of the proposed project shall comply with the City of Ontario noise ordinance that prohibits construction activities on Sundays, Federal holidays, and other days between the hours of 7:00 p.m. and 7:00 a.m.	During construction	Contractor
Noise	Construction noise	MM Noi 2: To the extent possible, the number of graders on-site shall be limited to two, or temporary sound barriers shall be installed adjacent to sensitive receptors for the duration of the grading activities.	During construction	Contractor and City Inspectors
Noise	Construction noise	MM Noi 3: Construction staging areas shall not be located within 150 feet of existing sensitive receptors and construction equipment shall be fitted with properly operating and maintained mufflers.	Prior to grading plan approval	Engineering Department and Contractor
Noise	Indoor noise impacts	MM Noi 4: Architectural plans shall be submitted to the City of Ontario Building Department for an acoustical plan check prior to the issuance of building permits.	Prior to architectural plan approval	Developer and Building Official
Public Services	Impacts to public services	MM Serv 1: The project applicant shall pay police, library and fire service development impact fees in place at the time certificates of occupancy are issued.	Prior to building permits	Developer and Bldg. Official
Public Services	Public Safety	MM Serv 2: The Ontario Police Department shall maintain a substation facility within proximity to service the proposed project area.	Determine appropriate location. Open by 350 th Certificate of Occupancy	Planning and Police Departments
Public Services	Impacts to schools of additional students	MM Serv 3: The project applicant shall pay school fees or otherwise meet project obligations to schools, as required by Ontario-Montclair School District and Chaffey Joint Union High School District.	Prior to building permits	Developer and Building Official
Public Services	Adequate park space	MM Serv 4: The project applicant shall pay park fees in place at the time building permits are issued, dedicate land and/or develop parks (or a combination of these) to the satisfaction of the Public	Prior to site plan approval	Planning and Parks Departments

MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
		Works Department to meet City parkland requirements.		
Opening Year with Preferred Project Scenario				
Traffic	With Preferred Project Scenario, to maintain LOS D or better, comply with CMP standards	MM Trans 1: Install traffic signal and modify the intersection of I-10 WB Off-ramp/ 7 th Street to include the following geometrics: Northbound: One left-turn lane. One shared through and right-turn lane. Southbound: N/A. Eastbound: One left-turn lane and one through lane. Westbound: One through lane and one right-turn lane.	Development Impact Fees and Fair Share Fees to be paid at the time of development plan approval	City Engineering Department SANBAG and Caltrans Building Dept. collects all development impact fees.
Traffic	With Preferred Project Scenario, to maintain LOS D or better, comply with CMP standards	MM Trans 2: Install traffic signal at Euclid Avenue/ E Street intersection.	Development Impact Fees and Fair Share Fees to be paid at the time of development plan approval	City Engineering Department SANBAG and Caltrans Building Dept. collects all development impact fees.
<i>To comply with CMP standards and reduce all potential impacts to LOS E or better, the following mitigation measures shall be implemented as part of the project.</i>				
Build-Out Year with Project (CMP Intersections)				
Traffic	With Preferred Project Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	MM Trans 3: Modify the intersection of Euclid Avenue/ SR-60 East-bound ramps to include the following geometrics: Northbound: Three through lanes. One right-turn lane. Southbound: Two left-turn lanes. Three through lanes. Eastbound: One left-turn lane. One shared left and through lane. One right-turn lane. Westbound: N/A.	Development Impact Fees and Fair Share Fees to be paid at the time of development plan approval	City Engineering Department SANBAG and Caltrans. Building Dept. collects all development impact fees.
Traffic	With Preferred Project Scenario, to comply with CMP standards and reduce all potential	MM Trans 4: Modify the intersection of Euclid Avenue/ SR-60 West-bound ramps to include the following geometrics: Northbound: Two left-turn lanes. Three through	Development Impact Fees and Fair Share Fees to be paid at the time of development plan approval	City Engineering Department SANBAG and Caltrans. Building Dept. collects all

MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
	impacts to LOS E or better	lanes. Southbound: Three through lanes. One right-turn lane. Eastbound: N/A. Westbound: One left-turn lane. One shared left and through lane. One right-turn lane.		development impact fees.
Traffic	With Preferred Project Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	MM Trans 5: Modify the intersection of Euclid Avenue/ Philadelphia Street to include the following geometrics: Northbound: Two left-turn lanes. Three through lanes. One right-turn lane. Southbound: Two left-turn lanes. Three through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Two through lanes. One right-turn lane. Westbound: One left-turn lane. One through lane. One shared through and right-turn lane.	Development Impact Fees and Fair Share Fees to be paid at the time of development plan approval	City Engineering Department SANBAG and Caltrans. Building Dept. collects all development impact fees.
Traffic	With Preferred Project Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	MM Trans 6: Modify the intersection of Euclid Avenue/Mission Boulevard to include the following geometrics: Northbound: One left-turn lane. Three through lanes. One right-turn lane. Southbound: Two left-turn lanes. Three through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Two through lanes. One shared through and right-turn lane. Westbound: Two left-turn lanes. Three through lanes. One right-turn lane.	Development Impact Fees and Fair Share Fees to be paid at the time of development plan approval	City Engineering Department SANBAG and Caltrans. Building Dept. collects all development impact fees.
Traffic	With Preferred Project Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	MM Trans 7: Modify the intersection of Euclid Avenue/Holt Avenue to include the following geometrics: Northbound: Two left-turn lanes. Three through lanes. One right-turn lane. Southbound: One left-turn lane. Two through lanes.	Development Impact Fees and Fair Share Fees to be paid at the time of development plan approval	City Engineering Department SANBAG and Caltrans

MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
		One shared through and right-turn lane. Eastbound: Two left-turn lanes. Three through lanes. One right-turn lane. Westbound: Two left-turn lanes. Two through lanes. One right-turn lane.		
Traffic	With Preferred Project Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	MM Trans 8: Modify the intersection of Euclid Avenue/4 th Street to include the following geometrics: Northbound: One left-turn lane. Three through lanes. One right-turn lane. Southbound: One left-turn lane. Two through lanes. One through and right-turn shared lane. Eastbound: One left-turn lane. Three through lanes. One right-turn lane. Westbound: Two left-turn lanes. Two through lanes. One through and right-turn shared lane.	Development Impact Fees and Fair Share Fees to be paid at the time of development plan approval	City Engineering Department SANBAG and Caltrans. Building Dept. collects all development impact fees.
Traffic	With Preferred Project Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	MM Trans 9: Add 2 nd southbound left-turn lane and 4 th northbound through lane at the intersection of Euclid Avenue/I-10 EB Ramps.	Development Impact Fees and Fair Share Fees to be paid at the time of development plan approval	City Engineering Department SANBAG and Caltrans. Building Dept. collects all development impact fees.
Traffic	With Preferred Project Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	MM Trans 10: Modify the intersection of Campus Avenue/Mission Boulevard to include the following geometrics: Northbound: One left-turn lane. One through lane. One through and right-turn shared lane. Southbound: One left-turn lane. One through lane. One through and right-turn shared lane. Eastbound: One left-turn lane. Three through lanes. One right-turn lane. Westbound: One left-turn lane. Two through lanes. One through and right-turn shared lane.	Development Impact Fees and Fair Share Fees to be paid at the time of development plan approval	City Engineering Department SANBAG and Caltrans. Building Dept. collects all development impact fees.
Traffic	With Preferred Project	MM Trans 11: Modify the intersection of Campus	Development Impact Fees and	City Engineering Department

MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
	Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	Avenue/Holt Boulevard to include the following geometrics: Northbound: One left-turn lane. One through lane. One through and right-turn shared lane. Southbound: One left-turn lane. One through lane. One through and right-turn shared lane. Eastbound: One left-turn lane. Two through lanes. One through and right-turn shared lane. Westbound: One left-turn lane. Two through lanes. One through and right-turn shared lane.	Fair Share Fees to be paid at the time of development plan approval	SANBAG and Caltrans. Building Dept. collects all development impact fees.
Traffic	With Preferred Project Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	MM Trans 12: Modify the intersection of Grove Avenue/Mission Boulevard to include the following geometrics: Northbound: Two left-turn lanes. Three through lanes. One through and right-turn shared lane. Southbound: Two left-turn lanes. Four through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Two through lanes. One through and right-turn shared lane. Westbound: Two left-turn lanes. Four through lanes. One right-turn lane.	Development Impact Fees and Fair Share Fees to be paid at the time of development plan approval	City Engineering Department SANBAG and Caltrans. Building Dept. collects all development impact fees.
Traffic	With Preferred Project Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	MM Trans 13: Modify the intersection of Grove Avenue/Holt Boulevard to include the following geometrics: Northbound: Two left-turn lanes. Three through lanes. One right-turn lane. Southbound: One left-turn lane. Three through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Three through lanes. One right-turn lane. Westbound: Two left-turn lanes. Three through lanes. One right-turn lane.	Development Impact Fees and Fair Share Fees to be paid at the time of development plan approval	City Engineering Department SANBAG and Caltrans Building Dept. collects all development impact fees.

MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
Traffic	With Preferred Project Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	MM Trans 14: Modify the intersection of Vineyard Avenue/Holt Boulevard to include the following geometrics: Northbound: One left-turn lane. Three through lanes. One right-turn lane. Southbound: One left-turn lane. Two through lanes. One through and right-turn shared lane. Eastbound: Two left-turn lanes. Two through lanes. One through and right-turn shared lane. Westbound: Two left-turn lanes. Three through lanes. One right-turn lane.	Development Impact Fees and Fair Share Fees to be paid at the time of development plan approval	City Engineering Department SANBAG and Caltrans. Building Dept. collects all development impact fees.
Traffic	With Preferred Project Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	MM Trans 15: Modify the intersection of Mountain Avenue/Mission Boulevard to include the following geometrics: Northbound: One left-turn lane. Two through lanes. One right-turn lane. Southbound: Two left-turn lanes. Two through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Three through lanes. One right-turn lane. Westbound: Two left-turn lanes. Three through lanes. One right-turn lane.	Development Impact Fees and Fair Share Fees to be paid at the time of development plan approval	City Engineering Department SANBAG and Caltrans. Building Dept. collects all development impact fees.
Traffic	With Preferred Project Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	MM Trans 16: Modify the intersection of Mountain Avenue/Holt Boulevard to include the following geometrics: Northbound: Two left-turn lanes. Three through lanes. One right-turn lane. Southbound: One left-turn lane. Three through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Two through lanes. One through and right-turn shared lane. Westbound: Two left-turn lanes. Two through lanes. One through and right-turn shared lane.	Development Impact Fees and Fair Share Fees to be paid at the time of development plan approval	City Engineering Department SANBAG and Caltrans. Building Dept. collects all development impact fees.
Traffic	With Preferred Project	MM Trans 17: Add 3 rd Eastbound through lane and	Development Impact Fees and	City Engineering Department

MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
	Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	3 rd Westbound through lane at the intersection of San Antonio Avenue/Holt Boulevard.	Fair Share Fees to be paid at the time of development plan approval	SANBAG and Caltrans. Building Dept. collects all development impact fees.
Traffic	With Preferred Project Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	MM Trans 18: The project will participate in the cost of off-site improvements through the payment of the city of Ontario Development Impact “fair share” mitigation fees. These fees shall be collected by the City at the time of issuance of building permits and utilized as needed by the City to construct the above improvements necessary to maintain acceptable levels of services in the project area.	Prior to Building Permits	City Engineering Department SANBAG and Caltrans. Building Dept. collects all development impact fees.
Traffic	With Preferred Project Scenario, to comply with CMP standards and reduce all potential impacts to LOS E or better	MM Trans 19: In addition to the DIF, the developer will pay fair share costs for all off-site roadway improvements that are not included in the existing DIF. Table III-10-I in the Final EIR summarizes these fair share costs that the developer will have to pay in addition to the DIF.	Prior to Building Permits	Developer and Housing Authority
Parking				
Traffic	Inadequate parking	MM Trans 20: All forms of development in the project area must meet City on-site parking code requirements and/or shared parking standards to the satisfaction of the Planning Department.	Prior to site plan approval	Planning Department
Traffic	Inadequate parking	MM Trans 21: As the project is built out in phases, some parking areas may be shared or off-street parking for one block may be provided on the adjacent block in an interim situation. The downtown Parking Model shall be used to analyze any interim or phased conditions to assure that off- street parking demand is met by the project as a whole throughout all phases of build-out.	Prior to site plan approval	Planning Department
<i>To comply with City standards and reduce all potential impacts to alternative transportation, the following mitigation measures shall be implemented:</i>				
Traffic	With Preferred Project	MM Trans 22: The City shall consult with	Location shall be chosen prior to	Planning Department, Omnitrans

MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
	Scenario, To comply with City standards and reduce all potential impacts to alternative transportation	Omnitrans to determine the location and type of transit facilities warranted by the proposed project. The location and type(s) of facility(ies) shall be determined prior to approval of site plans for the first phase of the proposed project. The siting of the facility(ies) shall be within the proposed project boundaries or within 500 feet of the edges of the project. The facility(ies) shall be constructed and adequate transit service shall be operating from the facility(ies) at the time of the last certificate of occupancy for residential units within the project.	site plan approvals Construction shall commence prior to issuance of last Certificate of Occupancy.	
Traffic	With Preferred Project Scenario, To comply with City standards and reduce all potential impacts to alternative transportation	MM Trans 23: The City should encourage the use of public transportation by providing Omnitrans and Metrolink information at public facilities within the project.	Ongoing	City of Ontario, Omnitrans, Metrolink
Traffic	With Preferred Project Scenario, To comply with City standards and reduce all potential impacts to alternative transportation	MM Trans 24: Pedestrian activity and bicycles shall be encouraged within the project site through the provision of sidewalks along all streets, connecting pathways and trails, and bicycle racks near commercial and public buildings and parks.	Prior to site plan approvals	Planning Department
Traffic	With High-Density Project Scenario, to maintain LOS D or better, comply with CMP standards	MM Trans 29: Modify the intersection of Cherry Avenue/Holt Boulevard to allow Right-in/Right-out turning movements only as planned by the City of Ontario.	Prior to Certificates of Occupancy.	Engineering Department and Developer
Traffic	With High-Density Project Scenario, to maintain LOS D or better, comply with CMP standards	MM Trans 30: Install Traffic Signal at the intersection of Plum Avenue/Holt Boulevard and include the following geometrics: Northbound: One shared left, through, and right-turn lane. Southbound: One shared left, through, and right-turn lane.	Prior to Certificates of Occupancy.	Engineering Department and Developer

MITIGATION MEASURES REQUIRED FOR J.H. SNYDER DOWNTOWN CIVIC CENTER PROJECT PUD				
Impact Category	Impact/Issue	Mitigation Measures	Implementation Timing	Responsible Party
		Eastbound: One left-turn lane. One shared through and right-turn lane. Westbound: One left-turn lane. One shared through and right-turn lane.		
Traffic	With High-Density Project Scenario, to maintain LOS D or better, comply with CMP standards	MM Trans 31: Modify the intersection of Lemon Avenue/Holt Boulevard to allow Right-in/Right-out turning movements only as planned by the City of Ontario.	Prior to Certificates of Occupancy.	Engineering Department and Developer
Utilities	Inadequate sewer and/or water pipelines	MM Util 1: All water and sewer pipelines within the project boundary that are identified by the City of Ontario Public Works Department at the time of project approval to require replacement and/or parallel lines shall be provided by the project proponent to the satisfaction of the City.	Prior to first Certificate of Occupancy in affected phase of project.	Public Works
Utilities	Inadequate sewer and/or water pipelines	MM Util 2: The segment of sewer pipeline in Francis Street that is currently surcharged, and/or other surcharged facilities required by the project, shall be constructed and operational by the time the project is constructed. Therefore, prior to obtaining occupancy permit(s) the project proponent shall be required to either replace/construct or pay their fair share for the surcharged segments as required by the City.	Prior to first Certificate of Occupancy in affected phase of project.	Public Works
Utilities	Impacts to existing utility lines from construction activities	MM Util 3: Prior to obtaining grading permit(s), the project proponent shall coordinate with the applicable natural gas, electrical, and telephone utility providers for the project site to ensure that all existing underground and overhead lines are not damaged during project construction.	Prior to grading permits	Public Works
Utilities	To reduce the quantity of energy used and to conserve water resources,	MM Util 4: To reduce the quantity of energy used and to conserve water resources, the project developer and City of Ontario should work to include sustainable systems for use of water and energy within the project design.	Prior to development plan approvals	Planning, Engineering, Public Works and Developer