CITY OF ONTARIO
PLANNING COMMISSION/
HISTORIC PRESERVATION
MEETING AGENDA

July 25, 2017

Ontario City Hall
303 East "B" Street, Ontario, California 91764

6:30 PM

WELCOME to a meeting of the Ontario Planning/Historic Preservation Commission.

All documents for public review are on file in the Planning Department located at 303 E. B Street, Ontario, CA 91764.

- ***Anyone wishing to speak during public comment or on a particular item should fill out a green slip and submit it to the Secretary.***
- ***Comments will be limited to 5 minutes. Speakers will be alerted when their time is up. Speakers are then to return to their seats and no further comments will be permitted.***
- ***In accordance with State Law, remarks during public comment are to be limited to subjects within the Commission's jurisdiction. Remarks on other agenda items will be limited to those items.***
- ***Remarks from those seated or standing in the back of the chambers will not be permitted. All those wishing to speak including Commissioners and Staff need to be recognized by the Chair before speaking.***
- ***The City of Ontario will gladly accommodate disabled persons wishing to communicate at a public meeting. Should you need any type of special equipment or assistance in order to communicate at a public meeting, please inform the Planning Department at (909) 395-2036, a minimum of 72 hours prior to the scheduled meeting.***
- ***Please turn off <u>all</u> communication devices (phones and beepers) or put them on non-audible mode (vibrate) so as not to cause a disruption in the Commission proceedings.***

<u>ROLL CALL</u>

DeDiemar __ Delman __ Downs __ Gage __ Gregorek __ Reyes __ Willoughby __

<u>PLEDGE OF ALLEGIANCE TO THE FLAG</u>

ANNOUNCEMENTS

1) Agenda Items

2) Commissioner Items

PUBLIC COMMENTS

Citizens wishing to address the Planning/Historic Preservation Commission on any matter that is not on the agenda may do so at this time. Please state your name and address clearly for the record and limit your remarks to five minutes.

Please note that while the Planning/Historic Preservation Commission values your comments, the Commission cannot respond nor take action until such time as the matter may appear on the forthcoming agenda.

CONSENT CALENDAR ITEMS

All matters listed under CONSENT CALENDAR will be enacted by one summary motion in the order listed below. There will be no separate discussion on these items prior to the time the Commission votes on them, unless a member of the Commission or public requests a specific item be removed from the Consent Calendar for a separate vote. In that case, the balance of the items on the Consent Calendar will be voted on in summary motion and then those items removed for separate vote will be heard.

A-01. MINUTES APPROVAL

Planning/Historic Preservation Commission Minutes of June 27, 2017, approved as written.

A-02. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV17-009: A Development Plan (File No. PDEV17-009) approval to construct 330 single-family residential dwelling units (104 conventional units and 226 cluster units) on 48.82 acres of land located within the Low Density Residential district of Planning Areas 4, 5 and 6 of the Grand Park Specific Plan, located at the southeast corner of Ontario Ranch Road and Turner Avenue. The environmental impacts of this project were previously analyzed in the EIR (SCH#2012061057) prepared for the Grand Park Specific Plan (File No. PSP12-001) and adopted by the City Council on January 21, 2014. All adopted mitigation measures shall be a condition of approval for the project and are incorporated herein by reference. The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT Airport. (APNs: 0218-241-37, 0218-241-38 and 0218-241-40); **submitted by Lennar Homes of California, Inc.**

A-03. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV17-024: A Development Plan (File No. PDEV17-024) approval to construct 88 single-family residential dwellings on 14.35 acres of land located within the Conventional Medium Lot Residential district of Planning Area 4 of the Subarea 29 Specific Plan, located at the southeast corner of Archibald Avenue and Parkview Street. The environmental impacts of this project were previously analyzed in an addendum to

the Subarea 29 Specific Plan EIR (SCH# 2004011009) that was adopted by the City Council on April 21, 2015. All adopted mitigation measures of the addendum shall be a condition of approval for the project and are incorporated herein by reference. The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and Chino Airport and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plans (ALUCP) for ONT and Chino Airports. (APN: 0218-022-15); **submitted by CalAtlantic Group, Inc.**

PLANNING COMMISSION PUBLIC HEARING ITEMS

For each of the items listed under PUBLIC HEARING ITEMS, the public will be provided an opportunity to speak. After a staff report is provided, the chairperson will open the public hearing. At that time the applicant will be allowed five (5) minutes to make a presentation on the case. Members of the public will then be allowed five (5) minutes each to speak. The Planning Commission may ask the speakers questions relative to the case and the testimony provided. The question period will not count against your time limit. After all persons have spoken, the applicant will be allowed three minutes to summarize or rebut any public testimony. The chairperson will then close the public hearing portion of the hearing and deliberate the matter.

B. ENVIRONMENTAL ASSESSMENT AND TENTATIVE TRACT MAP REVIEW FOR FILE NO. PMTT17-001/TT 20076: A Tentative Tract Map (File No. PMTT17-001/TT 20076) to subdivide 7.65 acres of land into 62 numbered lots and 29 lettered lots within the Low Density Residential (LDR) district of Planning Area 11 of The Avenue Specific Plan, located on the west side of Haven Avenue, approximately 700 feet south of Ontario Ranch Road. The environmental impacts of this project were previously analyzed in an addendum to The Avenue Specific Plan EIR (SCH# 2005071109) that was adopted by the City Council on June 17, 2014. All adopted mitigation measures of the addendum shall be a condition of approval for the project and are incorporated herein by reference. The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plans (ALUCP) for ONT Airport. (APN: 0218-412-02); **submitted by Brookcal Ontario, LLC.**

1. **CEQA Determination**

   No action necessary – use of previous EIR

2. **File No. PMTT17-001** (Tentative Tract Map)

   Motion to Approve/Deny

C. ENVIRONMENTAL ASSESSMENT, TENTATIVE TRACT MAP, DEVELOPMENT PLAN, AND VARIANCE REVIEW FOR FILE NOS. PMTT17-007 (TT17624), PDEV17-022 & PVAR17-007: A Tentative Tract Map (File No. PMTT17-007/TT 17624) to subdivide 3.47 acres of land into 31 single family lots and common areas, in conjunction with a Development Plan (File No. PDEV17-022) to construct a 31 single family homes (Cluster Product) and a Variance (File No. PVAR17-007) to deviate from the minimum building arterial street setback, along Mission Boulevard, from 30 feet to 5 feet and 9 inches. The project is located on the south side of

Mission Boulevard, between San Antonio and Oakland Avenues, within the MDR-11 (Low-Medium Density Residential 5.1 to 11.0 du/ac) zoning district. Staff has determined that the project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Sections 15305 (Minor Alterations in Land Use Limitations) and 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT), and was evaluated and found to be consistent with the policies and criteria of the ONT Airport Land Use Compatibility Plan (APNs: 1049-323-06, 1049-323-07, 1049-323-08, 1049-323-12 & 1049-323-13); **submitted by North by Northwest Capital Inc.**

1. **CEQA Determination**

   No action necessary – Exempt: CEQA Guidelines Sections § 15305 & 15332

2. **File No. PVAR17-007** (Variance)

   Motion to Approve/Deny

3. **File No. PMTT17-007** (Tentative Tract Map)

   Motion to Approve/Deny

4. **File No. PDEV17-022** (Development Plan)

   Motion to Approve/Deny

D. ENVIRONMENTAL ASSESSMENT AND SPECIFIC PLAN REVIEW FOR FILE NO. PSP15-001: A public hearing to consider certification of the Environmental Impact Report, including the adoption of a Statement of Overriding Considerations, for File No. PSP15-001 and a Specific Plan (Colony Commerce Center West) request (File No. PSP15-001**)** to establish land use designations, development standards, design guidelines and infrastructure improvements for approximately 123.17 acres of land, which includes the potential development of 2,951,146 square feet of industrial development. The project site is bounded by Merrill Avenue to the north, Remington Avenue to the south, Carpenter Avenue to the west and the Cucamonga Creek Flood Control Channel to the east. The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and Chino Airport, and was evaluated and found to be consistent with the policies and criteria of both the ONT Airport and Chino Airport Land Use Compatibility Plans (ALUCP). (APNs: 0218-261-24, 0218-292-05, 0218-311-11, 0218-292-12, 0218-292-09, 0218-292-13, 0218-292-10, 0218-292-14); **submitted by Cap Rock-Partners. City Council action is required.**

1. **CEQA Determination**

   Motion to recommend Approval/Denial of a Certification of an EIR, including the adoption of a Statement of Overriding Considerations

2. **File No. PSP15-001** (Specific Plan)

   Motion to recommend Approval/Denial

MATTERS FROM THE PLANNING/HISTORIC PRESERVATION COMMISSION

1) Old Business
   - Reports From Subcommittees
     - Historic Preservation (Standing):

2) New Business

3) Nominations for Special Recognition

DIRECTOR'S REPORT

1) Monthly Activity Report

If you wish to appeal any decision of the Planning/Historic Preservation Commission, you must do so within ten (10) days of the Commission action. Please contact the Planning Department for information regarding the appeal process.

If you challenge any action of the Planning/Historic Preservation Commission in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the Planning/Historic Preservation Commission at, or prior to, the public hearing.

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I, Marci Callejo, Administrative Assistant, of the City of Ontario, or my designee, hereby certify that a true, accurate copy of the foregoing agenda was posted on **Friday, July 21, 2017**, at least 72 hours prior to the meeting per Government Code Section 54954.2 at 303 East "B" Street, Ontario.

Marci Callejo, Secretary Pro Tempore

Scott Murphy, Planning Director
Planning/Historic Preservation
Commission Secretary

CITY OF ONTARIO PLANNING COMMISSION/ HISTORIC PRESERVATION MEETING

MINUTES

June 27, 2017

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CITY OF ONTARIO PLANNING COMMISSION/ HISTORIC PRESERVATION MEETING

MINUTES

June 27, 2017

REGULAR MEETING: City Hall, 303 East B Street
Called to order by Chairman Delman at 6:30 PM

COMMISSIONERS Present: Chairman Delman**,** Vice-Chairman Willoughby, DeDiemar, Gage, Gregorek, and Reyes

Absent: Downs

OTHERS PRESENT: Planning Director Murphy, City Attorney Rice, Principal Planner Zeledon, Senior Planner D. Ayala, Senior Planner Batres, Senior Planner Mercier, Senior Planner Mejia, Senior Planner Noh, Assistant Planner Antuna, Assistant City Engineer Do, and Planning Secretary Callejo

PLEDGE OF ALLEGIANCE TO THE FLAG

The Pledge of Allegiance was led by Commissioner Gregorek.

ANNOUNCEMENTS

Mr. Murphy stated item E had several applications including a Determination of Use, Development Plan and Conditional Use Permit (CUP). He said the applicant requested the application for the CUP including the helistop, be removed from the agenda at this time and should they decide to move forward with it at some future date, a new public hearing will be scheduled.

PUBLIC COMMENTS

Ryan Bachas, 8549 Wilshire Blvd, Beverly Hills, CA came forward to speak regarding medical and recreational cannabis. He stated he was the owner and director for Global Elite Advocacy and interim CEO of California Cannabis Coalition, working with numerous cities, counties, and councils to come up with frameworks and ordinances in allowing businesses in this matter. He gave background of what he felt was very insightful to what was going on upstate and new developments. He had handouts for the Commissioners which included tax break-downs and he hoped they would come back at a later Planning Commission meeting after they reviewed the information. He stated it was his hope they would recommend to the Council further review and workshops using his expertise on this subject. He thanked the Commission and stated he would answer any questions they might have.

CONSENT CALENDAR ITEMS

A-01. MINUTES APPROVAL

Planning/Historic Preservation Commission Minutes of May 23, 2017, approved as written.

It was moved by Willoughby, seconded by Gregorek, to approve the Planning Commission Minutes of May 23, 2017, as written, with Delman Abstaining since he was absent at the last meeting. The motion was carried 5 to 0.

PUBLIC HEARING ITEMS

B. ENVIRONMENTAL ASSESSMENT AND SPECIFIC PLAN AMENDMENT FOR FILE NO. PSPA17-002: An Amendment to the Grove Avenue Specific Plan to: [1] change the land use designation from Business Park to Commercial for approximately one-acre of land located at the northeast corner of Grove Avenue and Philadelphia Street; [2] amend the Commercial District permitted and conditionally permitted uses; and [3] update all applicable specific plan sections to reflect the proposed amendments. The Grove Avenue Specific Plan is generally located on the east and west sides of Grove Avenue and between Mission Boulevard to the north and the I-60 Freeway to the south. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) prepared in conjunction with File No. PGPA06-001, and adopted by City Council on January 27, 2010. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0113-641-13) **submitted by A&E Leasing, LLC. City Council Action is required.**

Senior Planner, Henry Noh, presented the staff report. Mr. Noh gave the location of the vacant project site and its surrounding areas. He shared that in 1993 the Grove Avenue Specific Plan was established and approved with standards, regulations and design guidelines for the development of the site to provide opportunities for the establishment of airport-serving, light industrial land uses, airport-related businesses and offices, and retail and support commercial development aimed at serving the needs within the airport corridor. He stated that it included the 60 FWY to the south and the Ontario International Airport to the north. He said the applicant is requesting the land use be changed from Business Park to Commercial to take advantage of the high traffic volumes at this intersection, as well as it being in keeping with the existing commercial development within the intersection which exists today. He explained that additionally, the Amendment would include: amending the Commercial District permitted and conditionally permitted uses to allow restaurants, conditionally permit restaurants with a drive-thru facility and conditionally permit car wash facilities (full and self-service) with the exception of the Commercial corners of Grove Avenue and Philadelphia Street and Grove Avenue and Francis Street where full and self-service car wash facilities will not be permitted; and update all applicable specific plan sections to reflect the proposed amendments. He stated that staff is recommending the Planning Commission recommend the City Council adopt the use of an Addendum to The Ontario Plan EIR and the approval of File No. PSPA17-002, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval.

No one responded.

PUBLIC TESTIMONY

No one responded.

As there was no one else wishing to speak, Chairman Delman closed the public testimony

There was no Planning Commission deliberation.

PLANNING COMMISSION ACTION

It was moved by Willoughby, seconded by Gage, to recommend adoption of the CEQA Determination and use of an Addendum to a previous EIR. Roll call vote: AYES, DeDiemar, Delman, Gage, Gregorek, Reyes, and Willoughby; NOES, none; RECUSE, none; ABSENT, Downs. The motion was carried 6 to 0.

It was moved by Gage, seconded by Reyes, to recommend adoption of a resolution to approve the Specific Plan Amendment, File No. PSPA17-002, subject to conditions of approval. Roll call vote: AYES, DeDiemar, Delman, Gage, Gregorek, Reyes, and Willoughby; NOES, none; RECUSE, none; ABSENT, Downs. The motion was carried 6 to 0.

C. ENVIRONMENTAL ASSESSMENT, DEVELOPMENT PLAN AND CONDITIONAL USE PERMIT REVIEW FOR FILE NOS. PDEV16-006 & PCUP16-005: A Development Plan (File No. PDEV16-006) to construct a 27,000-square foot industrial building in conjunction with a Conditional Use Permit (File No. PCUP16-005) to establish an architectural and structural metals manufacturing business on 1.96 acres of land located at 535 South Palmetto Avenue, within the IL (Light Industrial) zoning district. Staff has determined that the project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1011-161-01) **submitted by MYWI Fabricators, Inc.**

Senior Planner, Lorena Mejia, presented the staff report. Ms. Mejia gave background on the project site and surrounding site. She stated the project site currently slopes from northeast to southwest, creating a 7-foot differential in grade, and lacks any native flora and presently lacks right-of-way improvements along Palmetto Avenue. She explained there are two components of the project; a 27,000 square foot industrial building that consists of a 7,000 square foot, two-story office building and a 20,000 square foot pre-fabricated metal warehouse and manufacturing building. She stated the proposed industrial building is being constructed for Architectural and Structural Metals Manufacturing, the proposed use of the site. The roll-up doors along the west elevation are needed for the operation of the business. She said ceiling mounted cranes lift metal beams and/or other metal materials from the tractor-trailers into the warehouse. Finished

manufactured products are also placed onto tractor-trailers utilizing the ceiling mounted cranes within the warehouse and exit the building from the northwest roll-up door. Ms. Mejia explained that pursuant to the City of Ontario's Development Code, an "Architectural and Structural Metals Manufacturing" use requires a Conditional Use Permit (CUP) within the IL (Light Industrial) zoning district. She shared the applicant, MYWI Fabricators, has been operating an architectural and structural metals manufacturing business since 1993 within the City of South El Monte and is proposing to relocate to the City of Ontario. The proposed building has been designed to meet their business operational needs. She went over design and architecture features for the proposed building. She stated that staff is recommending the Planning Commission approve File Nos. PDEV16-006 and PCUP16-005, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval.

Mr. Willoughby asked with vehicular traffic going through the building, who oversees air quality issues and are there certain specifications for how exhaust is handled from the diesel fuel trucks driving in the building.

Ms. Mejia stated the Building Department would regulate that and certain occupancy regulations which would regulate the number of trucks. She stated the roll-up doors are expected to remain open during operational hours.

Mr. Willoughby asked if all the doors would be open; not just the drive-in and drive-out doors.

Ms. Mejia said no, her understanding is just the back door and side door would be open due to the operational activity.

Mr. Willoughby stated the topography goes north to south, so there will likely be a lot of run off and rain. He asked if the south planter would catch the water or divert it out to the street.

Ms. Mejia stated it would catch some and then redirect it into the basins located along the street frontage where it heads out to the street.

Mr. Willoughby confirmed there is a catch basin located at the southwest corner.

Ms. Mejia stated yes.

Mr. Reyes asked what the perimeter fencing wall materials were being proposed for the project.

Ms. Mejia stated that currently there are chain link fences and the applicant is looking further into putting wrought iron materials along the interior sides. She said along the front, they are proposing wrought iron fencing similar to what exists and potentially pushing it back and having it continue along the sides. She stated they are working with their neighbors to find the best solution.

Mr. Reyes asked if the chain link would be replaced with either tubular fencing or some

other material.

Ms. Mejia said along the frontage.

Mr. Reyes asked if the sides would be potentially wrought iron or block.

Ms. Mejia stated yes, they would not allow any chain link fencing, not within the visible portions.

Mr. Reyes asked if there were gates being closed over the driveway area, he asked for confirmation.

Ms. Mejia stated yes, that would be to secure the site when the business was non-operational. She said the gates would be open during operating hours and closed when the business was closed.

Mr. Reyes stated that he noticed in the landscape section that there are 15 gallon trees. He had hoped there would be a variety of sizes, like 15, 24 or 36, etc.

Ms. Mejia stated they are still working with the applicant on some of the planter areas, so there still may be a mix on the sizes of trees.

Mr. Reyes asked if they are going to put an outdoor employee lounge area.

Ms. Mejia said she spoke with the applicant and they were open to looking for opportunities to place something which would be appropriate and not impede any ADA requirements.

Mr. Gage asked if there was street parking for this item.

Assistant City Engineer, Mr. Do stated there would be street parking allowed along Palmetto Avenue.

Mr. Gage asked if there would be employee parking on site.

Ms. Mejia stated there would be employee parking provided on the project site.

Mr. Murphy stated parking is meeting the requirement 100% per Development Code standards. He said the requirement is being met on-site and the street parking is considered additional should the need arise, but is not required.

Ms. Mejia stated there are 43 parking spaces provided and 20 employees. There isn't a high demand for additional spaces, so they should be fine.

PUBLIC TESTIMONY

Howard Parcel, the project representative from 4845 Main Street in Yorba Linda, CA appeared and spoke. He said he worked long and hard with Planning staff and have appreciated their input. He said he had read the conditions of approval and agreed with

them. He stated the question about ventilation was a good one and along with the doors being open in the day time, they will also have roof mounted exhaust systems so there will be no accumulation of diesel fumes in the building. He stated that's their goal. He said he would be happy to answer any questions the Commission might have.

As there was no one else wishing to speak, Chairman Delman closed the public testimony

Mr. Reyes stated that he thought the architecture was good for the location in the City and he was glad to hear he worked closely with staff. He thanked staff and the applicant for doing a good job.

Mr. Willoughby wanted to echo Mr. Reyes comments and was glad to see a new business come to Ontario and are always excited to work with them and for their future. With that, he made the motion to approve the project.

Mr. Gage seconded the motion.

Mr. Delman stated before the vote was taken he also really liked the looks of the building and that it was absolutely first class and thanked the applicant for bringing it to the Commission.

PLANNING COMMISSION ACTION

It was moved by Willoughby, seconded by Gage, to adopt resolutions to approve the Conditional Use Permit, File No., PCUP16-005 and Development Plan, File No., PDEV16-006, subject to conditions of approval. Roll call vote: AYES, DeDiemar, Delman, Gage, Gregorek, Reyes, and Willoughby; NOES, none; RECUSE, none; ABSENT, Downs. The motion was carried 6 to 0.

D. ENVIRONMENTAL ASSESSMENT, DEVELOPMENT PLAN, AND TENTATIVE TRACT MAP REVIEW FOR FILE NOS. PDEV16-023 & PMTT16-014: A Development Plan (File No. PDEV16-023) to construct a 36-unit residential condominium development on 1.42 acres of land, and a Tentative Tract Map (File No. PMTT16-014/TM 20028) to subdivide the 1.42-acre project site into a single lot for condominium purposes, located at 1719 East Fourth Street, within the HDR-45 (High Density Residential - 25.1 to 45.0 DUs/Acre) zoning district. Staff has determined that the project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32 In-Fill Development Projects) of the CEQA guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 0108-551-01, 0108-551-34, 0108-551-35) **submitted by Kevin K. Cheung.**

Senior Planner, Luis Batres, presented the staff report. Mr. Batres gave background on the project site and its surrounding area. He explained the project site is bounded on the north and south by multi-family residential apartments, on the east by Corona Elementary School, and on the west by single family homes and showed the proposed project site which consist of 10 buildings. Mr. Batres stated the applicant is requesting approval to

develop a 36-unit residential condominium development, in conjunction with a Tentative Tract Map to subdivide approximately 1.42 acres into a common lot for condominium purposes. He said the project is planned to have two points of access, one on Corona Avenue and the second on Fourth Street. He stated each unit is required to have two parking spaces which will be provided in a subterranean parking garage structure. Mr. Batres stated nine of the ten buildings each have four units and Building 10 will serve as the clubhouse and manager's office. He said each unit will have 3 bedrooms and 2½ baths with living space between 1,300-1,500 square feet. He shared there are four different floorplans prepared for this project and each unit has a private open space in the way of a courtyard or balcony area. He stated common open space and amenities for the project include a courtyard with swimming pool, BBQ, outdoor play area and clubhouse. Mr. Batres explained the contemporary architecture design with Spanish Colonial influences and stated it is to be stooped and elevated and presented images with renderings of the project. He stated the project is being conditioned to have no parking along Corona Avenue and along Fourth Street. He stated that staff is recommending the Planning Commission approve File Nos. PMTT16-014 and PDEV16-023, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval.

Mr. Willoughby stated he had two questions; first he asked if the entrance into Fourth Street was a right turn in and right turn out or would people be able to go east on Fourth Street? He said he didn't believe Fourth Street was divided at that point.

Mr. Murphy stated it was not.

Mr. Batres stated he did not recall and apologized.

Mr. Murphy stated the current design is that there is a striped median. He said he believed the General Plan called for a median long term but when that would exactly go in, we don't know.

Mr. Willoughby confirmed they do have a striped lane to turn into though, rather than right into traffic.

Mr. Murphy stated that was correct.

Mr. Willoughby stated his second question was if the pool equipment was located on the pool level or down at the garage level.

Mr. Batres stated it would be located in the garage area.

Mr. Reyes asked for further explanation of the clubhouse and amenities. He stated he didn't see them clearly on the plan.

Mr. Batres stated Building 10 was being called the clubhouse to give it a definition. He shared it will provide outside showers, restrooms for the swimming pool and will have an office for the manager for the facility. He said that's what will encompass that building. He stated the rest of the amenities will be located outside, which includes the swimming pool, the barbeque area, decorative shade structures, and children play equipment.

Mr. Gage asked if there would be cross walks from to the elementary [Corona] school. Mr. Murphy stated there will be a cross walk at the signalized intersection of Fourth and Corona, but that would be the only one that will be provided.

PUBLIC TESTIMONY

Stanley Ty, from JWDA-MS Architects the architect appeared and spoke. He said they accepted all the conditions and would answer any questions the Commission might have.

As there was no one else wishing to speak, Chairman Delman closed the public testimony

Mr. Gage stated he really liked the project. He said he was really glad they did 360 degree architecture and the windows look really nice. He shared he liked the fact that the parking would be underneath which would allow for no parking on Corona and that's real important since we don't want more cars on Corona where children would go between the parked cars to get over to the school and j-walk. He stated also that the project is being raised up adds to the aesthetics and it really adds a lot. He shared he would be voting yes on this project.

Mr. Reyes said he really appreciated the extensive amenities placed within the open spaces of the units and the balance of adult pool and barbeque with the tot-lots. He said he thinks there will be many places for people to utilize within the project. He would encourage for there to be more benches within the corridors and other places where people can come outside and read a book or work on their laptop, that kind of thing. He said again, he really liked the extensive use of amenities within the project.

Mr. Gregorek said he appreciated the applicant putting thought into the architecture and he was sure staff put in thought as well. He said he looked forward to seeing the project being built and that it would be as good as the renderings. He stated he would be supporting the project.

Mr. Delman said he would like to echo everyone's sentiments and it's a great looking project and it will be great for that area.

Mr. Willoughby confirmed with Mr. Rice that the tract map and development plan could be taken as one action. He also said hats off to staff and the developer. He stated no doubt, this was a challenging piece of property and they did a great job with it. With that, he made the motion to approve the item.

PLANNING COMMISSION ACTION

It was moved by Willoughby, seconded by Gregorek, to adopt resolutions to approve the Tentative Tract Map, File No., PMTT16-014 and Development Plan, File No., PDEV16-023, subject to conditions of approval. Roll call vote: AYES, DeDiemar, Delman, Gage, Gregorek, Reyes, and Willoughby; NOES, none; RECUSE, none; ABSENT, Downs. The motion was carried 6 to 0.

Mr. Gregorek recused himself from item E since his firm had done some work on

the project.

E. **ENVIRONMENTAL ASSESSMENT, DETERMINATION OF USE, DEVELOPMENT PLAN AND CONDITIONAL USE PERMIT REVIEW FOR FILE NO'S. PDET17-002, PDEV17-003 & PCUP17-005:** A Determination of Use (File No. PDET17-002) to allow a heliport use within the Centrelake Specific Plan, in conjunction with a Development Plan (File No. PDEV17-003) to construct a three-story commercial/office building totaling 79,455 square feet, and a Conditional Use Permit (File No. PCUP17-005) to establish a rooftop heliport on 5.05 acres of land located at the southwest corner of Haven Avenue and Guasti Road, within the Office land use district of the Centrelake Specific Plan. Staff is recommending the adoption of a Mitigated Negative Declaration of environmental effects for the project. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0210-551-07) **submitted by HMC Construction, Inc. City Council action is required.**

Senior Planner, Lorena Mejia, presented the staff report. Ms. Mejia explained the Determination of Use application for the heliport use in conjunction with a proposed Development Plan to address land uses typically not listed within the Centrelake Specific Plan. She explained the Development Code currently allows for the zoning of a heliport in high intensity office districts and gave more details as to what that entails. She also stated that staff is recommending the Planning Commission make a determination of the heliport use within the Centrelake Specific Plan subject to a Conditional Use Permit with final approval authority to be given by the City Council. Ms. Mejia shared information regarding the proposed Development Plan and gave background on the project site and its surrounding areas. She said the parking requirements have been exceeded by 72 spaces and the five most southern rows will be covered to accommodate solar panels. She also went over drive aisles, ingress and egress and infrastructure which are already in place for the Centrelake Business Park. Proposed landscape, architecture and elevations were also explained to the Commission. She stated that staff is recommending the Planning Commission adopt the Mitigated Negative Declaration and approve File Nos. PDET17-002 and PDEV17-003, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval.

Mr. Murphy reiterated that the Conditional Use Permit (CUP) application for this item has been removed at the request of the applicant. Before them was a copy of the letter from the applicant stating that request and also proposed language to the resolution adopting the Mitigated Negative Declaration as the CUP is being removed. He stated the Commission's action is now final since this application will no longer be going forward to City Council and this language shows the changes in the resolution.

Mr. Rice also stated that one of the mitigation measures mentioned in the Mitigation Monitoring Program attached to the resolution for the Mitigated Negative Declaration, (Section 5b) is directly related to the CUP for the heliport and should be removed. He said if the applicant should come back at a later time for the CUP, than that condition would apply for the CUP and MND at that time.

Ms. DeDiemar stated she had a question regarding the parking lot from the presented

letter from the applicant. She said it was not clear to her why the association's approval was needed for the parking lot.

Ms. Mejia stated in the shared drive access, half of it lays on the existing project site and the other half on the westerly parcel. She said in order to accommodate their development, a portion of that is reconfiguring that drive entry throat with some of the parking stalls on the western adjacent parcel. She stated that is part of the approval which is being referred to in the letter which was received.

Ms. DeDiemar asked if approval has been received.

Mr. Murphy stated it was his understanding a verbal approval has been received and they [applicant] are waiting for the written approval and there's a condition which requires a letter of approval prior to building permit issuance.

Ms. DeDiemar confirmed that this issue will not impede and factor where they are presently.

Mr. Murphy stated that was correct.

Mr. Reyes asked about the covered parking. He asked if the covered parking was coming now and the panels later or if the covered parking and panels were both coming later.

Ms. Mejia stated it's her understanding that they're moving forward with them now as part of the application.

Mr. Reyes asked if the actual structure will be there and not the panels.

Ms. Mejia said she believed they were moving forward with the panels, they wanted to be solar ready essentially.

Mr. Reyes said he was trying to get at if they were solar ready or just the structure and that's why there was no landscaping. He stated it sounded like the structure was going up.

Ms. Mejia stated the structure was going up.

Mr. Murphy stated that from their point of view if the structure is not going up, they'd rather see the landscaping going in now and if they have to remove trees later, so be it. However, in this situation, I believe they're putting in the structure and panels at the same time as the development.

Mr. Reyes asked if this was when the project is approved.

Mr. Murphy stated yes.

Mr. Gage asked about the landscaping on Guasti [Road] to the north. He asked if that was existing.

Ms. Mejia said the portions within the right-of-way are.

Mr. Gage asked if they look like the portions to the west, which is all part of Centrelake Business Park.

Ms. Mejia showed a slide showing what was existing and stated that was what was to remain in place.

Mr. Gage stated that was already done.

Mr. Murphy stated on both street frontages.

Mr. Gage thanked staff.

Mr. Willoughby wanted to confirm there was no Conditional Use Permit action because that is tied to the heliport and they would only take action on the CEQA, Determination of Use and Development Plan. He also asked for confirmation that no vote would move forward to City Council.

Mr. Murphy stated that was correct and the reason City Council action was envisioned originally was because of the heliport and California Department of Aeronautics requirements. He said in this case since everything is being removed it will stop with the Planning Commission tonight.

PUBLIC TESTIMONY

Aaron Hodgson on behalf of the applicant, HMC Construction and Prime Healthcare appeared and spoke. He stated they didn't have anything more to add to the comments of staff and they've had a good working relationship. He said he would be happy to answer any questions the Commission may have.

Mr. Willoughby asked about the solar panels and if they were all going in at one time.

Mr. Hodgson said it all goes in as one project.

Nicole Ertel, with MarWest Commercial stated she was the Managing Agent for the Centrelake Owners Association and said she wanted to clarify one of the questions the Commission asked regarding the drive aisle. She said it affects a "sub-association" which neighbors the property and that sub-association has approved a modification to the drive aisle. She said it is now with the "Master Association" which she pointed out in the letter, is reviewing the set of plans and not just the parking lot. She stated there was no approval from the Architectural Review Committee but they expect to hear in the next 30 days and she doesn't anticipate any issues. She said she had one question: if the Commission approves the Development Plan, would it include the roof structure ready for the heliport for a later date. She asked for that to be clarified.

Mr. Delman asked Mr. Murphy to correct him if he was wrong, but it will be structurally built to support the heliport if it should come to pass.

Mr. Delman asked why the heliport was a problem for the association.

Ms. Ertel said they (the members) would like to meet with Aaron and find out if there are any concerns. She said whether its noise levels, how often will the helicopter be flying, also any truck issues it might cause being on the corner of Guasti and Haven.

Mr. Delman said as an Aviation Guy all his life, he thinks that's an ideal spot for a heliport if it's inclement weather and an IFR helicopter, he doesn't have to rely on the airport and he can fly under it and land on his heliport.

Ms. Ertel stated she doesn't work there every day and she represents the members and wants their feedback before a decision was made.

As there was no one else wishing to speak, Chairman Delman closed the public testimony

Mr. Gage stated it was a prime corner and Centrelake Business Park is a beautiful development. He said he would be for the project.

Mr. Reyes said he was glad the solar panels would be going in now. He said he liked how many electric vehicle stations were proposed. He thought it was ingenious, the re-entry of the driveway and the curbs.

PLANNING COMMISSION ACTION

It was moved by Gage, seconded by Reyes, to adopt a resolution of the CEQA Determination and Mitigated Negative Declaration with an amendment to remove the language regarding the Conditional Use Permit (section 5b). Roll call vote: AYES, DeDiemar, Delman, Gage, Reyes, and Willoughby; NOES, none; RECUSE, Gregorek; ABSENT, Downs. The motion was carried 5 to 0.

It was moved by Willoughby, seconded by Gage, to adopt resolutions to approve the Determination of Use, File No., PDET17-002 and the Development Plan, File No., File No. PDEV17-003, subject to conditions of approval. Roll call vote: AYES, DeDiemar, Delman, Gage, Reyes, and Willoughby; NOES, none; RECUSE, Gregorek; ABSENT, Downs. The motion was carried 5 to 0.

F. **ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT CODE AMENDMENT REVIEW FOR FILE NO. PDCA17-002:** A Development Code Amendment adding Paragraph 7 to Subsection K of Ontario Development Code Section 8.01.020 (Sign Standards), which authorizes the establishment of an incentive not to place political signs. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15378(b)(4) of the CEQA Guidelines, as the Development Code Amendment does not involve any commitment to a specific project which may result in a potentially significant physical impact on the environment. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); **City Initiated. City Council action is required.**

Senior Planner, Charles Mercier, presented the staff report. Mr. Mercier stated there are often several complaints during campaign time due to the large number of political signs

which are posted to the prior date of installation time and remaining after the required removal date. He explained that complaints are received regarding signs which are illegally placed on public property, as well as within the public right-of-way. He said taking actions to abate these actions takes a substantial amount of staff time, as well as spent a significant amount of money and City resources. Mr. Mercier shared that upon consideration of this issue, the City Administration staff is recommending that the Political Signs regulations, contained in Subsection K of Development Code Section, be amended to allow for the establishment of incentives for candidates to not place political signs within the City. He explained that proposed ordinance would allow candidates to sign a written pledge to not place their political signs in the City. If the candidate complies with the pledge, and in fact refrains from posting signs, the candidate will be refunded the amount of the candidate statement publication fee. He said it is the hope that this incentive would result in a reduction of the number of political signs posted during the election season and reduce the expenditure of staff time and City resources in abating illegally placed political signs. He stated that staff is recommending the Planning Commission recommend the City Council approve File No. PDCA17-002, pursuant to the facts and reasons contained in the staff report and attached resolution.

Mr. Reyes questioned the amount of the fee.

Mr. Mercier stated it's about $2,000.

Mr. Murphy said the $2,000 is what the City charges a candidate when they file all their paperwork. He said if they follow-through with the pledge to not post any signs, they would affectively receive that $2,000 back and it would cost them nothing to register to run as a candidate. Mr. Murphy clarified that if there was some question as if there was an additional fee of $2,000 to run for election; there is not. It is you pay your money regardless and if you elect to follow-through and not post signs, you would receive that money back at the end of the campaign.

Mr. Reyes confirmed that this is a fee that the City currently charges for anybody that wants to run, separate from a county fee. He asked if there is a county fee.

City Attorney, Mr. Rice said he knows the county has fees, but it would cover the City fees which include cost of publication of candidate statements, the legal review of materials and it's a fairly standard fee that's charged and it will remain regardless of what happens tonight. He said it's a fee which is pre-existing and this is a way in which the City would waive that fee for candidates willing to not place signs.

Mr. Reyes stated that if he understood it correctly, it's a fee which is already in place and this is just a way to deter from over-posting signs or any signs. He asked is one sign triggers the fee.

Mr. Murphy stated there certainly will be some enforcement issues that will pop up. He said there have been discussions about if a particular candidate is not liked if signs are posted up with their name on it to "stick it to them". He also mentioned that in the discussions with Code Enforcement when a candidate registers to run for office, they are assigned a number and that is also given to their election committee. Often, that assigned number is placed on their signs and posters. He said they hoped those numbers could be

used to affiliate a certain candidate by their campaign rather than those being put up by others exercising their First Amendment right of free speech. He said typically those signs will not have any identification on them. He said they saw in the last election a group which was active in posting signs in the south [of the City] that did not have the identification number because they were not affiliated with a certain campaign group or political action committee or anything of that nature. He said it was just individuals expressing their desires and concerns. He explained this was the thought how they would go about trying to enforce the signs given the nature of what they are and give candidates the opportunity to sign the pledge.

Mr. Gage asked if the incentive was to stop signs from being in illegal places. He read aloud part of the staff report and proposed resolution. He said that the incentive is not stop illegal signs, but to not have signs period. He asked if that was correct, to not put up signs at all.

Mr. Murphy stated that was correct.

Mr. Gage asked why we, [the City], would not want signs up period. He asked if that wasn't part of our political process.

Mr. Murphy stated it's entirely up to the candidate at that point how they wish to pursue their campaign. He said they are not telling them they can't have signs. He said what they are being told is that if they want to have signs, have all the signs you want. If you decide you want no signs, we're willing to refund your $2,000. He said that's all it says. He said it's either you have signs or you don't. He said, if you have signs, it's the same process we have in place today, you pay your $2,000 to get on the ballot and you place all the signs you want and the city will deal with all the clutter that's out there. He said, but if there's a candidate out there who decides they don't want to put any money into [the process] and put any signs up, that's their choice and they get their $2,000 refunded to them. He reiterated that they are not telling people they can't put their signs up, they the City is just providing an opportunity for somebody [candidate] to get their money back, should they do so.

Mr. Gage asked this was the "incentive" not to put up signs.

Mr. Murphy stated yes, an "incentive" not to put up signs.

Mr. Willoughby said there may need to be some flexibility for signs put up by without authorization by individuals on a case by case to case situation.

Mr. Murphy said that was correct.

Mr. Willoughby said his concern was that he could see some circumstances that are beyond the control of the candidate and he doesn't want to see them penalized because someone else did something, as you [Mr. Murphy] said "stick it to them". He said he wanted to be sure that was being thought of.

Mr. Murphy stated there have been a couple of conversations with Code Enforcement in anticipation this would pass and how to enforce and determine what signs are authorized

by the candidate versus those that were not. He said signs that where posted by someone in their front yard, the candidate would not be penalized. But clearly if they have posted several hundred signs throughout the community, that's pretty obvious.

Mr. Willoughby said he understood this would be an incentive not to post any signs, but he stated it does not seem likely to correct the problem of signs too early and signs staying late. He asked if there were penalties in place. If there were fines in place for that type of situation or is that another issue which needs to be addressed.

Mr. Rice said that as difficult a situation as it is to handle, the Municipal Code does provide that a candidate cannot put signs up more than 45 days before hand and they must be removed within 10 days following. He stated any violation of the Municipal Code is a nuisance by law and is subject to penalties under the City's Municipal Code including administrative citation and other sorts of fines and penalties. He said there are mechanisms to address that. He explained as Mr. Murphy pointed out, it can be tricky to assign responsibility in some cases although the City will continue to enforce those rules.

Ms. DeDiemar asked Mr. Murphy if he had discussed with Code Enforcement the level of effort the current Municipal Code states with what the proposed effort would be. She stated she was not clear that this would save any time or make it any easier.

Mr. Murphy stated he didn't know if it would or not. He said his sense was that you would not see a discernable difference in the number of signs that go up, even if a candidate chooses not to put signs up. He stated especially during a Presidential Election when there are state and national offices congress, senate, assembly, etc. He said the number of candidates is tremendous and the number of signs that get posted is ridiculous. He said when you drive down Holt Boulevard for example, there are very few open space of chain link fence which are left by the time an election rolls around.

Mr. Gregorek asked if the fee was only for a City Office or if individuals who run for water boards are also charged a City fee.

Mr. Murphy said he didn't believe the fees were paid to the City because those are elections which are handled by the county or state. He said it would only apply to local elections.

Mr. Gregorek said those candidates wouldn't benefit from this, only those running for City Office.

Mr. Murphy said yes and that quite frankly, $2,000 for a State Senator or even an Assembly Member is very low.

Mr. Reyes asked if this was only for signs within the right-of-way or was it within the right-of-way and private property.

Mr. Murphy stated it's posting of signs…period.

Mr. Reyes said there's no distinction of land location.

Mr. Murphy stated that if he were a candidate and had 500 signs made and gave them to all his friends in the City and they posted them at their homes. He would not get his $2,000 back. He said that if you, as an individual homeowner posted a sign on your property in favor of your favorite candidate that is not authorized by the candidate, no harm, no fowl.

Mr. Reyes said he believes that every time there's a sign put up by you, the candidate, there's an identification number of some soft on it. He said the problem becomes when people want to grant you or give you signs because they have a printing business. He thought it was just within the right-of-way and he said it's going to be tough for Code Enforcement to decide who made the sign. He stated he didn't think that Code Enforcement was the right body to be the "Sign Police". He stated they aren't trained to necessarily…he said today color copies make pretty good signs. He said it might be difficult to tell the difference between homemade versus not homemade. He said he doesn't know how easily it may be to police it, he said it will be tough. He thanked Mr. Murphy for sharing about the right-of-way versus private.

Mr. Murphy asked if he can summarize and wrap it up for the Commission. He said there are going to be issues that are going to come up. He said maybe Code Enforcement isn't the right entity to handle enforcement and maybe the City Manager has to come in and arbitrate some type of discussion, but let's not lose sight of what we're trying to do. He said it's simply a matter of offering somebody a way of getting their money back should they choose to do so. He said it's a matter of choice that we're giving them, rather than saying "you're paying the $2,000 anyway". He said it's simply an option that they have.

PUBLIC TESTIMONY

No one responded.

As there was no one else wishing to speak, Chairman Delman closed the public testimony

Mr. Gregorek said that since he's lived in Ontario, he's only known of one candidate who has posted no signs and spent no money. He said he thought it would be a good gesture in as often as it might be used, which might be less than 5%. He said that was his own personal opinion. He stated with that, he wanted to make the motion to recommend approval of the Development Code Amendment based upon the staff report.

Mr. Delman thanked Mr. Gregorek and asked for a second to the motion.

Mr. Reyes asked if they could make comment before they voted.

Mr. Delman said he was going to.

Mr. Gage said there was a motion.

Mr. Delman said they have to second the motion. He said he just wants a second and then they can discuss.

Mr. Willoughby made a second to the motion.

Mr. Delman thanked Mr. Willoughby and said they were open for discussion.

Mr. Gage stated he was against this [Development Code Amendment]. He said he thinks the wording of it says there is an incentive for people not to advertise, you'll get your $2,000 if you don't advertise. He said people with money will not have a problem getting the money back and some candidates will have a problem. He said it's worded that the incentive is to "please don't advertise and you'll get your $2,000 back. So, please don't advertise." He stated it just seemed wrong to him that we would have an incentive for people to not to advertise their candidacy. He asked why have an incentive for that? He stated, let them put them signs up. He said that if it's for the beautification and safety of Ontario citizens, then let's address what that real problem is. He said the real problem is the people who put up, and have the $2, 000 and who don't care about the City and put up the signs where they shouldn't and that's what we should be enforcing. He stated that's what makes Ontario less beautiful, not the small candidate who wants to run and have yard signs. He said it's the one that hires the outside agency that puts signs everywhere even where they're not supposed to, on fences, on public right-of ways and everywhere else. He said we should address that problem, not put an incentive for the candidate for less money not to advertise. He stated he thinks that just un-American and wrong. He stated again, he won't be for this.

Mr. Delman called on Mr. Reyes.

Mr. Reyes said these would be his own words. He said he doesn't think its fairness in a way for somebody that doesn't have money and is trying to get back. He said he doesn't get that part, it's not clear to him and it will not solve the sign issue. He said the whole premise of it that we're trying to beautify or make the City look cleaner during election. He stated that there are so many different signs out there that don't belong to only those running for City office; he said there's State Assembly signs, and signs from other areas that should not be in Ontario. He said they post them at the gate of the City, at the north, south, east and west for surrounding cities. He said he doesn't think it will make the City look any cleaner if that's what the whole issue is, since it can look cluttered and littered with a bunch of signs. He stated he doesn't think it's clear by creating this incentive that it gives a person the freedom and the right to post signs. He said it's not a clear distinction for him. He said the big one for him is how does this make it equal for past elections, so people who had past elections and people who have new elections. He asked how does that make it fair? He said the others didn't deal with us and new people [candidates] have to deal with this. He said it isn't fair and it should just be the way it is. He said again it's not fair and that's the problem he's having; the fairness part of past elections versus current elections. Why are we throwing this little glitch in there? He stated he doesn't see it as an incentive, sorry.

Mr. Willoughby said he doesn't believe this will solve the sign problem but he is willing to support it as an initiative that it may lead them down a road that could possibly get them closer to that. He said because he sees that it doesn't hinder a candidate from advertising his or her campaign, it's not an additional fee, they are already paying that fee, and so it really isn't going to change anything as they go forward. He said plus the fact that it only pertains to city offices, so the county offices, state offices, and national offices have no complaints. He stated, as Mr. Rice said, there are things in the Municipal Code to deal with early and late signs, however we can do that, if we can want to. He said

he still believes they need to make it as easy as possible for a local person that wants to run for a local office. He stated he understood what his two fellow Commissioners were saying, but he didn't see that this would put a hamstring to anyone or change anything from the past. He said therefore, he was willing to support it as a possible incentive and let's see what happens. He said they could always revisit the Development Code as they go down this road. He said he doesn't believe for a moment it will change anything that they've been experiencing.

Ms. DeDiemar stated she was opposed this [Development Code Amendment] for several reasons. She said one of them is she agreed with her fellow Commissioners in this will not solve the issue it was intended to solve because it is only a small subset of all the signs that go up during certain campaigns. She stated her main issue was that it would be an embarrassment for the City of Ontario to have something like this. She said if the Commission is divided on this, she could imagine how the citizens and residence of Ontario would feel. She said incentives can be positive or negative and this is an attempt to be a positive incentive and that's wonderful to do that. However, she said the problem is so ubiquitous that it's probably going to take a serious negative incentive to solve the problem and frankly, she said because Ontario only has control over those running for office in Ontario.

Mr. Gage stated that he thought the way this would be looked at is incumbents trying to stop new people from running. He said it's an incentive for them to get money not to advertise. He said it would be looked at if that were the intent or not, it would be looked at as a power-grab by the powers-to-be, incumbents to keep things as they are. He said that's his opinion on what it will look like.

Mr. Reyes said that having run for the water board before, he said the County charges a fee of approximately $2,500-$3,500. He said he understood there are County processing fees and someone has to do, what they have to do and the county has to charge to recoup those costs. He said personally, unfortunately, the City has a fee, and in his opinion, there shouldn't even have a fee and it shouldn't cost anybody in America a dollar to run for office. He said you could be the poorest person on the planet and you should have the right to run regardless if you have shoes on or not. So he said with that, he couldn't support it [the item].

Mr. Murphy asked if he could try one more time to explain the item. He said that if this item were not before them tonight, it would be status quo. A candidate would pay their $2,000 and whether they paid their $2,000 they could put their signs up regardless or not. He said they make a decision whether or not to put signs up, how many signs are put up, whether you plaster the City with them or you don't. He said the City would be left having to deal with that fall out. He stated with the proposal they are presented with, all that is stated is, if a candidate decides not to post signs, they get their money back. It does not say they can't put signs up. He said there is no language in the item anywhere that says they are limited to or eliminates their ability to put signs up or to advertise in any way. He said it is simply giving them an opportunity. He stated they are going to pay $2,000 regardless, that fee is already existing and has been in place and will likely continue to be in place whether we like it or not. This will simply give them an opportunity to get that money back should they choose to do so. He stated he agreed with the comments it will likely not help the visual clutter out there, but he commented to

Commissioner Reyes point. Here's an opportunity for a candidate not to spend a dime. He said a person gets their $2,000 back if they don't put any signs up. He stated he wanted to be sure the Commission was focused on what the true issues are. He said they are not regulating or forcing somebody's behavior. He said they were simply giving somebody an option and whether they choose to take that option is their choice. He stated right now, there is no choice; a candidate pays the $2,000 regardless of how many signs they put up. He thanked Chairman Delman.

Ms. DeDiemar asked if she could respond to Mr. Murphy's comments.

Mr. Delman stated yes.

Ms. DeDiemar stated Mr. Murphy's point was well taken, but she didn't think it was truly a real choice. She said the choice is get your $2,000 back and fail to advertise and if you're seriously running for office you can't fail to advertise. She stated yard signs are just part of the political process. She stated that she didn't think it was a true legitimate choice that you're [the item] giving someone. She said they do have a choice, but they don't have a choice.

Mr. Murphy stated this doesn't change that. He said the Development Code before them does not change that. He said he thinks that's the misconception. He stated that from what he's hearing, he thinks the Commissioners may think the Development Code is changing someone's ability to post signs or not. He said they're not changing their ability to do that.

Ms. DeDiemar said she understood that but practically speaking if you wish to be elected to an office that you're running for, you can't let go of an important part of the campaign like signs. She said, it's not really a true choice. There are consequences for not posting signs. She said she understood that it's a choice, but she's disagreeing that it's a true choice for a true candidate.

Mr. Murphy said that if you're a true candidate, you don't sign the pledge. You pay your money, you raise your money, you run your campaign, just like today.

Ms. DeDiemar stated that in turn this would not solve the problem of campaign signs cluttering the city.

Mr. Murphy stated it would not.

Ms. DeDiemar stated if it doesn't, than what's the point?

Mr. Murphy said it's not supposed to solve the problem. He said all it is doing is offering somebody the ability not to spend any money. He said, like Commissioner Gregorek mentioned, there has been a past candidate who didn't spend a dime on advertising, so he would get his $2,000 back because he didn't spend a dime. He stated nobody else elected to go down that path, so they wouldn't get any money back.

Mr. Gage asked to speak.

Mr. Delman gave him the floor.

Mr. Gage stated that he listens to the comments and he's hearing people have "option". He said however, the wording [in the resolution] used is "incentive" and he finds it troubling. He said he reads that as an incentive not to advertise. He said it doesn't say they have the option to get their money back. He said it's an incentive not to advertise. He asked why would they give someone an incentive not to advertise? He stated he didn't think it was there place to make an incentive to not advertise. He stated that was there he had a problem.

Mr. Gregorek asked for the vote.

Mr. Delman reminded the Commission this was a recommendation to City Council and asked for a roll-call vote.

Ms. DeDiemar asked for the motion to be re-stated so she knew which way to vote.

Planning Secretary Callejo read aloud the motions previously made to recommend approval of the Development Code Amendment to City Council. It was explained to recommend a vote for approval or denial. A roll-call vote was taken beginning with Commissioner DeDiemar, Gage, Gregorek, Reyes, Willoughby and Delman.

PLANNING COMMISSION ACTION

It was moved by Gregorek, seconded by Willoughby, to recommend adoption of a resolution to approve the Development Code Amendment, File No., PDCA17-002. Roll call vote: AYES, Delman, Gregorek, and Willoughby; NOES, DeDiemar, Gage and Reyes; RECUSE, none; ABSENT, Downs. The motion tied 3 to 3.

City Attorney Rice stated the motion failed and can be called up to City Council if so moved by them.

HISTORIC PRESERVATION ITEMS

G. ENVIRONMENTAL ASSESSMENT AND CERTIFICATE OF APPROPRIATENESS REVIEW FOR FILE NO. PHP17-003: A request for a Certificate of Appropriateness to construct a 3,535 square foot, two-story, single-family residence within the Euclid Avenue Historic District, on 0.37 acres of land located at 1521 North Euclid Avenue, within the RE-4 (Residential Estate - 2.1 to 4.0 DU/Acre) and EA (Euclid Avenue) Overlay zoning districts. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15303 (Class 3, New Construction or Conversion of Small Structures). The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1047-251-01) **Submitted by Anthony Lionel Mejia.**

Assistant Planner, Elly Antuna, presented the staff report. Ms. Antuna gave background on the project site which is located within the Euclid Avenue Historic District and explained that the property is a non-contributor to the district. She stated the site is currently vacant and proceeded to go over the proposed architecture and design for the two-story, single-family residence which will be setback 40 feet which is consistent with the other homes in the area. Ms. Antuna stated that on June 8, 2017 the HPSC recommended approval of this application and staff is recommending the Planning Commission approve File No. PHP17-003, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval.

PUBLIC TESTIMONY

No one responded.

As there was no one else wishing to speak, Chairman Delman closed the public testimony

Mr. Gage stated he became active in city government when a proposed home was being built on Euclid Avenue that looked like an Alta Loma tract home with the garage door in front. He said the house was further south than where this project is located but it wasn't compatible with the historic district of Euclid Avenue and he was amazed it was going to be built. He stated they protested and talked so something could be built that looked historical and fit within the neighborhood. He shared a house was built that fit with the neighborhood. He said that was his first exposure to Ontario government. He stated the project had beautiful architecture, Mediterranean-Revival which will fit in great on the lot and on that street. He said he saw a lot of details and it truly is all in the details with construction of what the end product will be. He said he would definitely vote for this.

Mr. Delman stated that was a gorgeous house and as Vice-President of Ontario Heritage he would almost like to designate it as historic from the beginning, but he knows that can't be done. He thanked staff for bringing the project forward to them and stated the turret is absolutely stunning.

Mr. Willoughby stated he echoed Mr. Delman's comments and made a motion of approval for the project.

PLANNING /HISTORIC PRESERVATION COMMISSION ACTION

Acting as the Historic Preservation Commission, it was moved by Willoughby, seconded by Gage, to adopt a resolution to approve the Certificate of Appropriateness, File No. PHP17-003 subject to conditions of approval. Roll call vote: AYES, DeDiemar, Delman, Gage, Gregorek, Reyes, and Willoughby; NOES, none; RECUSE, none; ABSENT, Downs. The motion was carried 6 to 0.

MATTERS FROM THE PLANNING COMMISSION

Old Business Reports From Subcommittees

Historic Preservation (Standing): This subcommittee met on June 8, 2017.

- ***A request for a Certificate of Appropriateness, File No. PHP17-003 was recommended for approval.***
- ***A request to remove 3 single-family residences located at 543, 546 and 547 West Maitland Street from the Ontario Register were approved.***
- ***A request for a Tier I Determination for the Proposed Guasti Village Historic District was approved.***

Development Code Review (Ad-hoc): This subcommittee did not meet.

Zoning General Plan Consistency (Ad-hoc): This subcommittee did not meet.

New Business

NOMINATIONS FOR SPECIAL RECOGNITION

None at this time.

DIRECTOR'S REPORT

Mr. Murphy stated the Monthly Activity Reports were in their packets and Planning Commissions Matter Workshop will be held on July 13, 2017 which will be sponsored by BB&K and Lewis Group of Companies. This will be an opportunity for Commissioners in the Inland Empire area to get together and network. He stated speakers would be Janice Rutherford and Randy Jackson from Placeworks. They would learn about trends and hear about things going on in the area. He reminded them to let him or Planning Secretary Marci Callejo know so the Historic Preservation Subcommittee meeting could be adjusted as needed since the event is on the same night. Mr. Murphy also suggested that the Historic Preservation items be moved to the beginning of the agenda in the future.

ADJOURNMENT

Gregorek motioned to adjourn, seconded by Reyes. The meeting was adjourned at 8:40 PM.

_____
Secretary Pro Tempore

_____
Chairman, Planning Commission

PLANNING COMMISSION STAFF REPORT

July 25, 2017

SUBJECT: A Development Plan (File No. PDEV17-009) approval to construct 330 single-family residential dwelling units (104 conventional units and 226 cluster units) on 48.82 acres of land located within the Low Density Residential district of Planning Areas 4, 5 and 6 of the Grand Park Specific Plan, located at the southeast corner of Ontario Ranch Road and Turner Avenue. (APNs: 0218-241-37, 0218-241-38 and 0218-241-40); **submitted by Lennar Homes of California, Inc.**

PROPERTY OWNER: Lennar Homes of California, Inc.

RECOMMENDED ACTION: That the Planning Commission approve File No. PDEV17-009, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The project site is comprised of 48.82 acres of land located at the southeast corner of Ontario Ranch Road and Turner Avenue, within the Low Density Residential district of Planning Areas 4, 5 and 6 of the Grand Park Specific Plan, and is depicted in ***Figure 1: Project Location***, below. The project site gently slopes from north to south and is currently mass graded. The property to the north of the project site is within the Low Medium Density Residential district of Planning Area 11 of The Avenue Specific Plan and is currently mass graded. The properties to the east, south and west are vacant and are located within the Medium Density Residential and Elementary School Districts of Planning Areas 3 and 9 and the Great Park District and High School District of Planning Area 10 of the Grand Park Specific Plan.

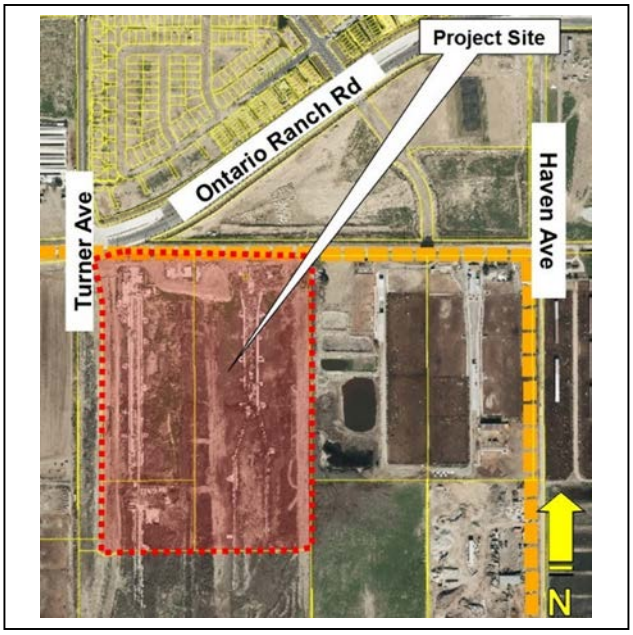

Figure 1: Project Location

Case Planner:	Henry K. Noh
Planning Director Approval:	
Submittal Date:	March 1, 2017
Hearing Deadline:	N/A

Hearing Body	*Date*	*Decision*	*Action*
DAB	July 17, 2017	Approved	Recommend
ZA			
PC	July 25, 2017		Final
CC			

PROJECT ANALYSIS:

[1] Background — The Grand Park Specific Plan and Environmental Impact Report (EIR) were approved by the City Council on January 21, 2014. The Grand Park Specific Plan established the land use designations, development standards, design guidelines and development capacity of 1,327 residential units for the specific plan area. The Specific Plan is comprised of eleven (11) land use districts incorporating eight (8) distinctive neighborhoods, offering a variety of residential products.

On September 23, 2014, the Planning Commission approved Tentative Tract Map 18662 ("B" Map), subdividing 48.82 acres into 330 single-family lots and 62 lettered lots to accommodate a single-family conventional product and 4, 6 and 8-pack cluster products and facilitated the construction of the backbone streets, including the primary access points into the proposed community from Turner Avenue and Grand Park Street, as well as the construction of all the interior neighborhood streets within the subdivision (see ***Exhibit A: Site Plan***). The applicant is now requesting Development Plan approval for construction of 330 single-family dwelling units.

The Applicant, Lennar Homes of California, Inc., has submitted a Development Plan (File No. PDEV17-009) to construct 330 single-family residential dwelling units (104 conventional units and 226 cluster units) (see ***Exhibits B thru D: Typical Plotting and Conceptual Landscaping***) on land located within the Low Density Residential district of Planning Areas 4, 5 and 6 of the Grand Park Specific Plan (see ***Figure 2: Grand Park Specific Plan Land Use Plan,*** below), located at the southeast corner of Ontario Ranch Road and Turner Avenue.

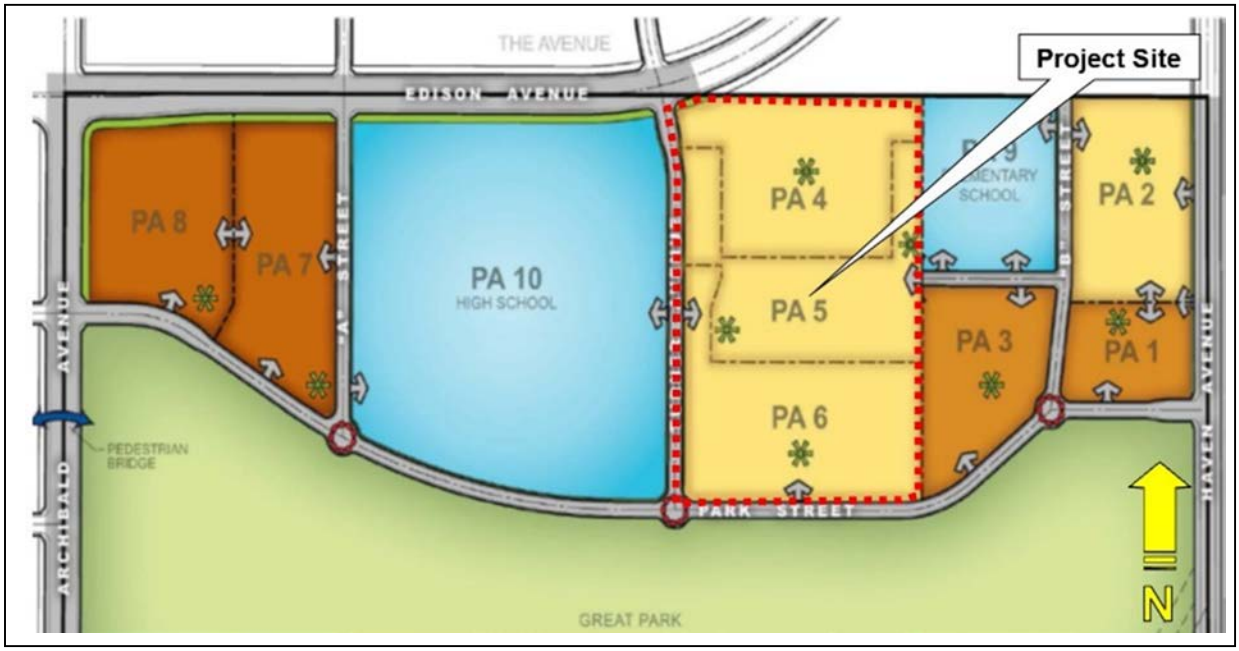

Figure 2: Grand Park Specific Plan Land Use Plan

Site Design/Building Layout — The proposed Development Plan has been designed upon the architectural influences found in Ontario and throughout Southern California. The architectural styles have been selected in order to be reflective of older neighborhoods of historic Ontario, as well as to accommodate innovative transitional architectural influences. The project includes three floor plans for the conventional product and six floor plans for the cluster product with three architectural styles per plan. All plans incorporate various design features, such as single and second-story massing, varied entries, front porches, great rooms, 2nd floor laundry facilities, bonus rooms, lofts and an outdoor California room. In addition, each home will provide a two-car garage and standard driveway. To minimize visual impacts of garages, second story projections above garages, varied first and second-story roof massing and door header trim above the garage are proposed on all elevations.

The three floor plans for the Conventional product include the following:

- Plan 1: 2,277 square feet, 4 bedrooms (option for 5th bedroom) and 3 baths.
- Plan 2: 2,765 square feet, 4 bedrooms and 3 baths.
- Plan 3: 2,967 square feet, 5 bedrooms and 3 baths.

The six floor plans for the 4 and 6-Pack Cluster product include the following:

- Plan 1: 2,142 square feet, 4 bedrooms and 3 baths.
- Plan 1X: 2,563 square feet, 4 bedrooms, bonus room (Option 5th bedroom and 4th bath) and 3 baths.
- Plan 2: 2,309 square feet, 4 bedrooms and 3 baths.
- Plan 2X: 2,731 square feet, 4 bedrooms, bonus room (Option 5th bedroom and 4th bath) and 3 baths.
- Plan 3: 2,496 square feet, 4 bedrooms and 3 baths.
- Plan 3X: 2,920 square feet, 4 bedrooms, bonus room (Option 5th bedroom and 4th bath) and 3 baths.

The six floor plans for the 8-pack Cluster product include the following:

- Plan 1: 1,676 square feet, 3 bedrooms and 2.5 baths.
- Plan 2: 1,794 square feet, 3 bedrooms and 2.5 baths.
- Plan 3: 1,940 square feet, 4 bedrooms and 3 baths.
- Plan 3X: 2,364 square feet, 4 bedrooms, bonus room (Option 5th bedroom and 4th bath) and 3 baths.
- Plan 4: 2,042 square feet, 4 bedrooms and 3 baths.
- Plan 4X: 2,459 square feet, 4 bedrooms, bonus room (Option 5th bedroom and 4th bath) and 3 baths.

[2] Site Access/Circulation — The previously approved Tentative Tract Map 18662 facilitated the construction of the backbone streets along Turner Avenue and Grand Park

Street, including the primary access points into the proposed community, as well as the construction of all the interior neighborhood streets within the subdivision.

[3] Parking — The Development Plan proposes cluster and conventional single-family home products with a variety of lot sizes. The proposed development provides for a two-car garage and a two-car driveway for each dwelling unit, as well as 225 on-street parking spaces for visitors. As demonstrated within Table 1 below, the parking analysis concluded that there will be an average of 4.7 parking spaces per unit, which should be more than adequate to accommodate both resident and visitor parking.

Summary of Parking Analysis							
Product	**Number of Units**	**Garages**	**Driveways Parking**	**On-Street Parking**	**Total Provided**	**Req. Per Unit**	**+/- Parking**
	Parking Per Unit						
SF Conventional	**104**	**2**	**2**	**76**	**492**	**208**	
MEWS (4, 6 and 8-Pack Cluster)	**226**	**2**	**2**	**149**	**1053**	**452**	
Total	**330**			**225**	**1545**	**660**	**+ 885**
					4.7 spaces per unit		

Table 1: Parking Analysis

[4] Landscaping/Open Space — The Development Plan features landscaped parkways and private lanes that are designed with landscape areas to soften the massing of the garages, which provides visual interest and promotes pedestrian mobility (see ***Exhibits B thru D: Typical Plotting and Conceptual Landscaping***).

The related Tentative Tract Map 18662 facilitated the construction of sidewalks, parkways, and open space areas within the project site. The Ontario Plan (TOP) Policy PR1-1 requires new developments to provide a minimum of 2 acres of private park per 1,000 residents. The proposed project is required to provide 2.51 acres of park area to meet the minimum TOP private park requirement. To satisfy this requirement, the applicant is constructing four neighborhood parks totaling 2.56 acres of park land area that are strategically located throughout the project site to provide residents a variety of park options within walking distance from their homes. Additionally, the applicant is proposing to construct a private recreation center that features a pool/spa, BBQ's, cabanas, and a children's splash pad play area. The recreation center is located within the western portion of the project site at the intersection of Rocky Mountain Street and Bryce Canyon Trail. The residents of the development will also have access to the future Grand Park that will be located directly south of the proposed project.

[5] Architecture — The proposed architectural styles include Spanish, Italianate and Craftsman. The styles were chosen to complement one another through the overall scale, massing, proportions, details and the ability to establish an attractive backdrop that will age gracefully over time.

Each architectural style will include the following details (**See *Exhibit E – Floor Plan and Elevations*)**:

Spanish: Varying gable and hipped roofs with concrete "S" roof tile, stucco exterior, square windows openings, arched porch and entryways, decorative barrel tiles below gable ends, wrought-iron elements, wrought-iron pot shelves, shutters and cantilevered elements with corbels.

Spanish (Plan 1: 4 & 6 Pack-Cluster)

Italianate: Varying hipped roofs with concrete "S" roof tile, decorative cornice treatments along the eaves, stucco exterior, smooth accent corner boards, cantilevered elements with corbels; first floor bay windows, shutters and pot shelf accents.

Italianate (Plan 1: 4 & 6 Pack-Cluster)

Craftsman: Varying gable roofs with a flat concrete roof tile, wood outlookers below gable ends, knee braces, vertical siding below gable ends, stucco, horizontal siding and shingle siding, cantilevered elements with corbels; covered porches with either a simple tapered or dual post columns with stone veneer bases, shutters and decorative window framing.

Craftsman (Plan 1: Conventional)

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)
- Ensure the Development of a Well Planned, Balanced, and Self-Sustaining Community in the New Model Colony

[2] Vision.

Distinctive Development:

- Commercial and Residential Development
  - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

[3] Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.

  - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

[4] Policy Plan (General Plan)

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.

  - LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.

  - LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).

- Goal LU2: Compatibility between a wide range of uses.

  - LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

Housing Element:

- Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

  - H2-4 New Model Colony. We support a premier lifestyle community in the New Model Colony distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.
  - H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.

Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.

Community Economics Element:

- Goal CE1: A complete community that provides for all incomes and stages of life.

  - CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

  - CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

  - CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

➢ CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

➢ CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

▪ Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.

➢ S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

▪ Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➢ CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

➢ CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

➢ CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

▪ Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

➢ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and

- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➢ CD2-2 Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the "outdoor living room"), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb.

➢ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➢ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➢ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➢ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➢ CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

➢ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

- Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

➢ CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

- Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

➢ CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.

➢ CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project is consistent with the number of dwelling units (330) and density (6.76 DU/AC) specified in the Available Land Inventory.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The project site is located within the Airport Influence Area of the Ontario International Airport (ONT), and has been found to be consistent with the policies and criteria set forth within the ALUCP for ONT.

ENVIRONMENTAL REVIEW: The environmental impacts of this project were previously reviewed in conjunction with File No. PSP12-001, the Grand Park Specific Plan for which an EIR (SCH# 2012061057) was adopted by the City Council on January 21, 2014. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures shall be a condition of project approval and are incorporated herein by reference.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	Existing Land Use	*General Plan Designation*	*Zoning Designation*	*Specific Plan Land Use*
Site	Mass Graded	Low Density Residential	Grand Park Specific Plan	LDR: Planning Areas 4, 5 and 6
North	Mass Graded	Medium Density Residential	The Avenue Specific Plan	LMDR: Planning Area 11
South	Vacant with Previous Agricultural/Dairy Uses	Open Space- Parkland	Grand Park Specific Plan	Great Park
East	Vacant with Previous Agricultural/Dairy Uses	Public School and Medium Density Residential	Grand Park Specific Plan	MDR and Elementary School: Planning Areas 3 and 9
West	Vacant with Previous Agricultural/Dairy Uses	Public School	Grand Park Specific Plan	High School: Planning Area 10

General Site & Building Statistics

Item	*Required Min./Max.*	*Provided (Ranges)*	*Meets Y/N*
Project area (in acres):	N/A		Y
Maximum project density (dwelling units/ac):	8.23 DU/AC	6.76 DU/AC	Y
Maximum coverage (in %):	60% (Cluster) 55% (SFD)	54% (Cluster) 52% (SFD)	Y
Front yard setback (in FT):	10’ (Both)	10’ (Both)	Y
Side yard setback (in FT):	5’ (Both)	5’ (Both)	Y
Rear yard setback (in FT):	5’ (Cluster) 15’ (SFD)	5’ (Cluster) 15’ (SFD)	Y
Maximum dwelling units/building:	361 DU	330 DU	Y
Maximum height (in FT):	35’ (Both)	28’ (Both)	Y

TOP Private Park Requirement

TOP Park Requirement	*Total Number of Lots*	*Park Acres Proposed*	*TOP Park Requirement Formula*	*Meets Y/N*
Minimum Private Park Requirement of 2 acres of park per 1,000 residents	330	0.82 Acres 0.73 Acres 0.53 Acres 0.48 Acres	2.51 Acres (Based on 3.806 residents per single-family home (lot)).	Y
	Total: 330 Units	Total: 2.56 Acres	Total: 2.51 Acres	Y

Exhibit A—SITE PLAN

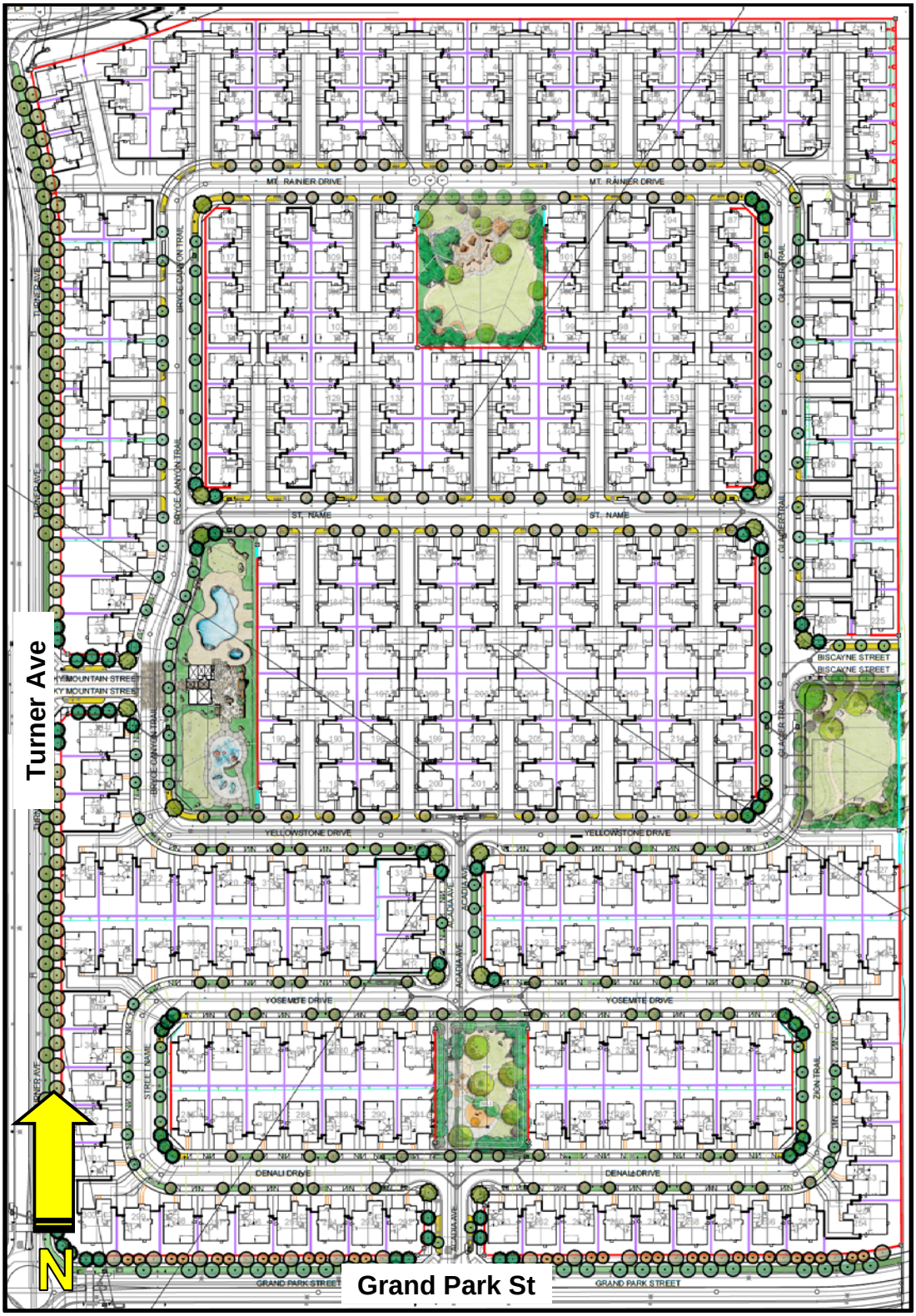

Exhibit B: Typical Plotting and Conceptual Landscaping – Conventional

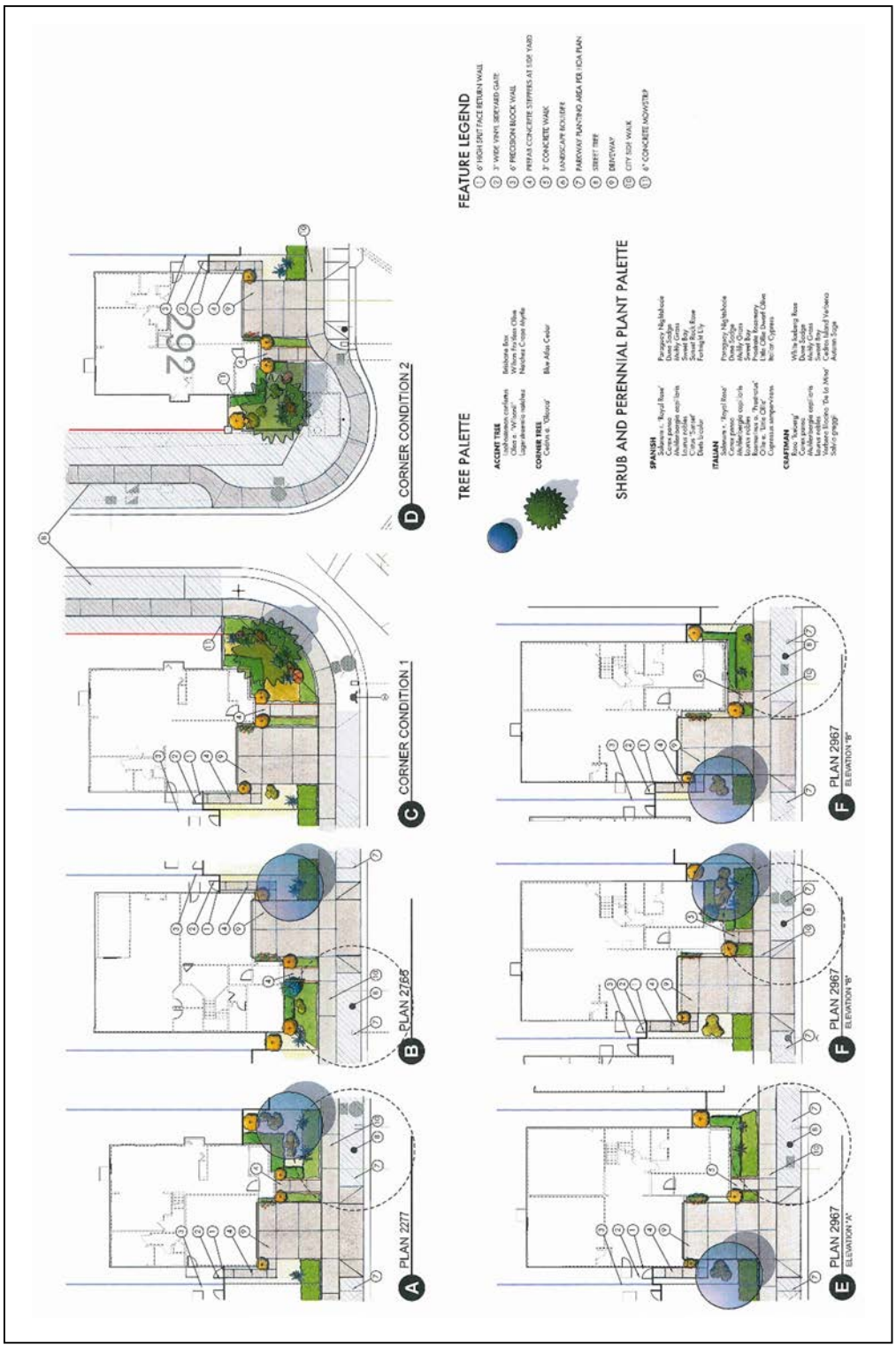

Exhibit C: Typical Plotting and Conceptual Landscaping – 4 and 6-Pack Cluster

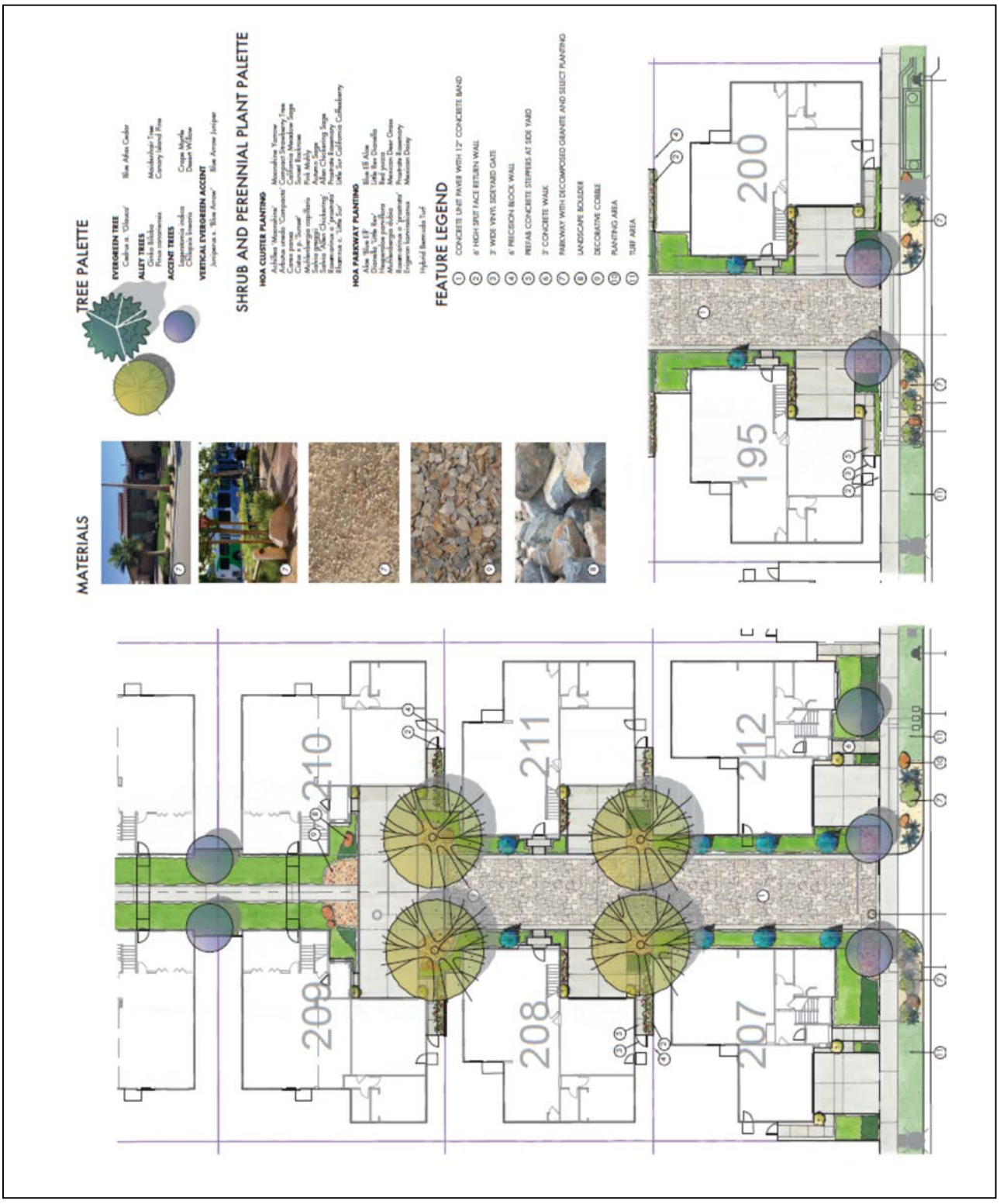

Exhibit D: Typical Plotting and Conceptual Landscaping – 8-Pack Cluster

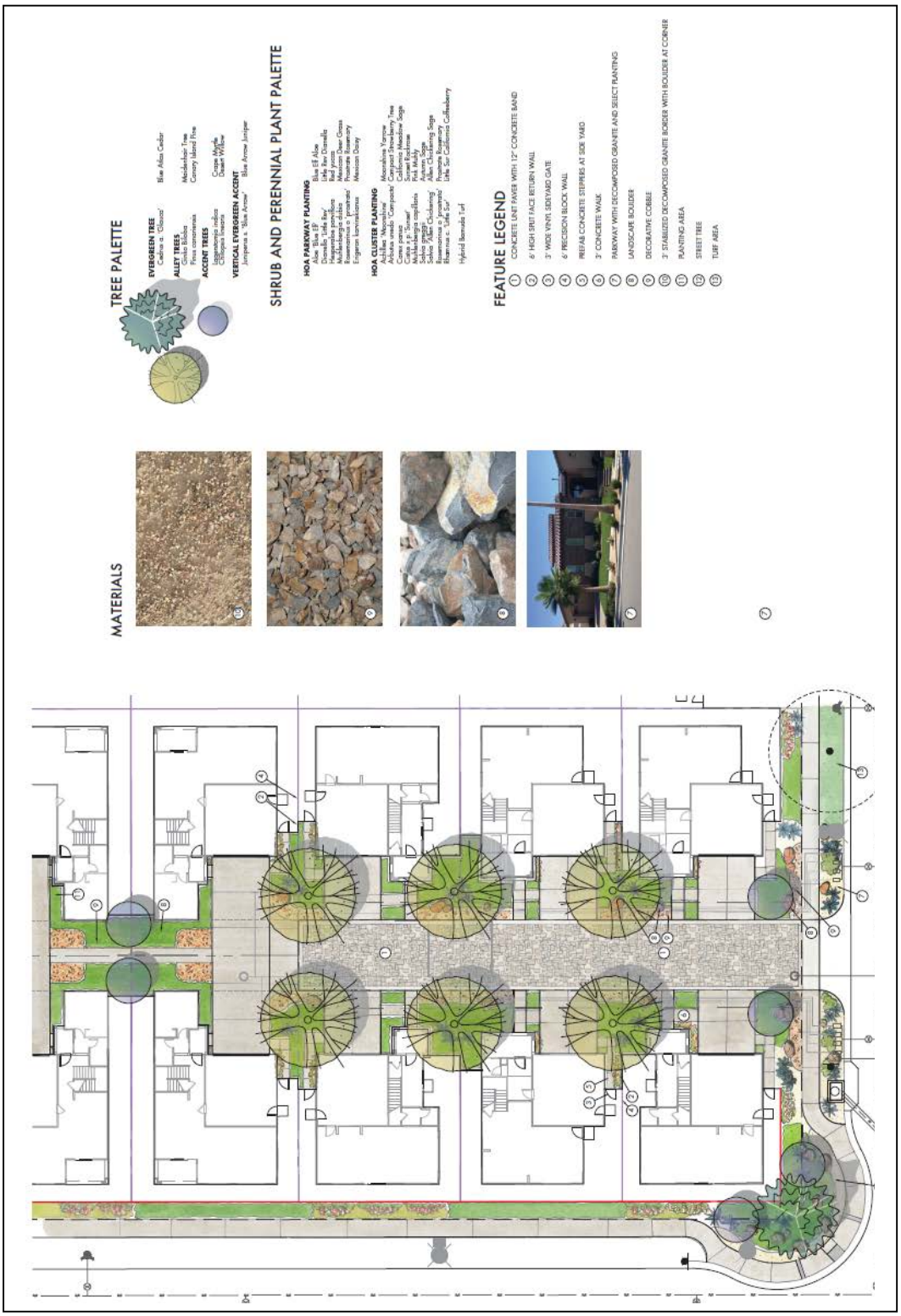

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 1: CONVENTIONAL

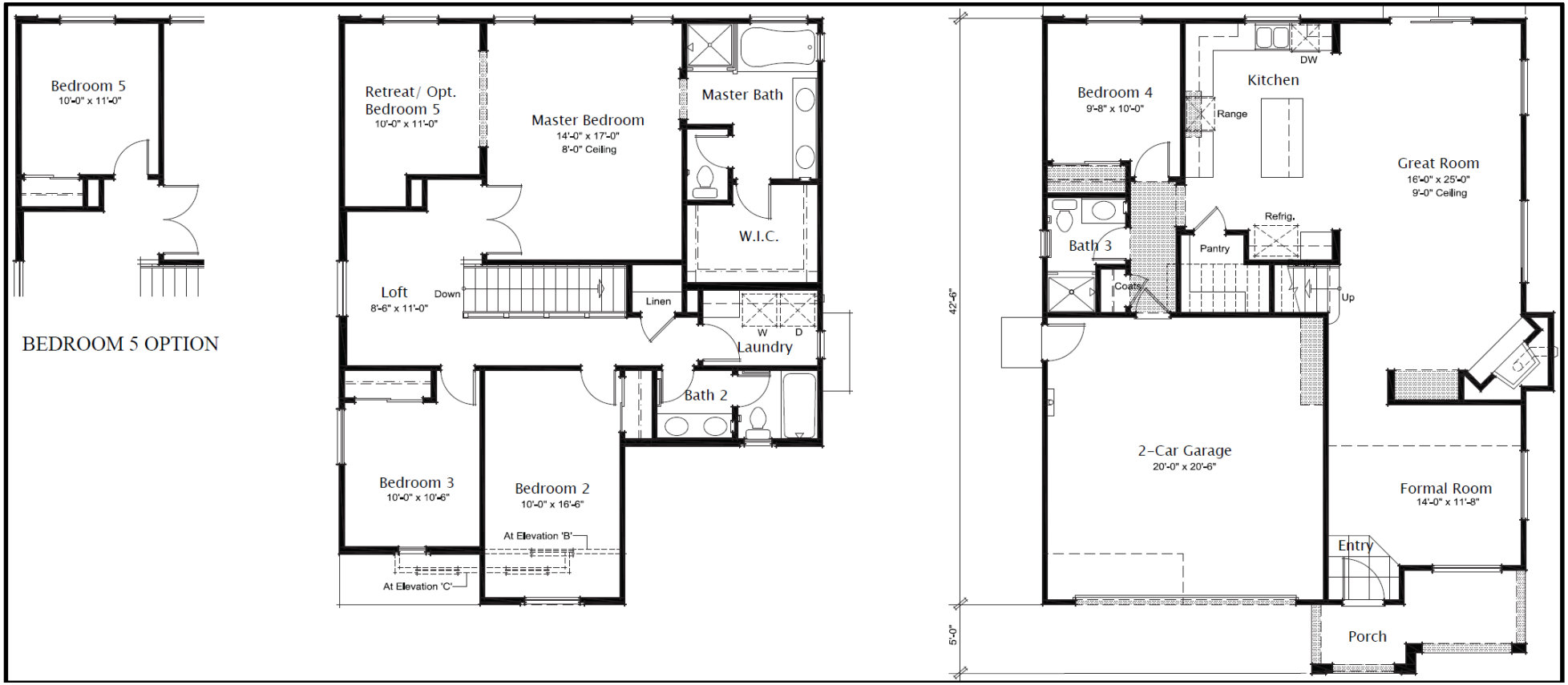

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 1: CONVENTIONAL

Spanish

Italianate

Craftsman

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 2: CONVENTIONAL

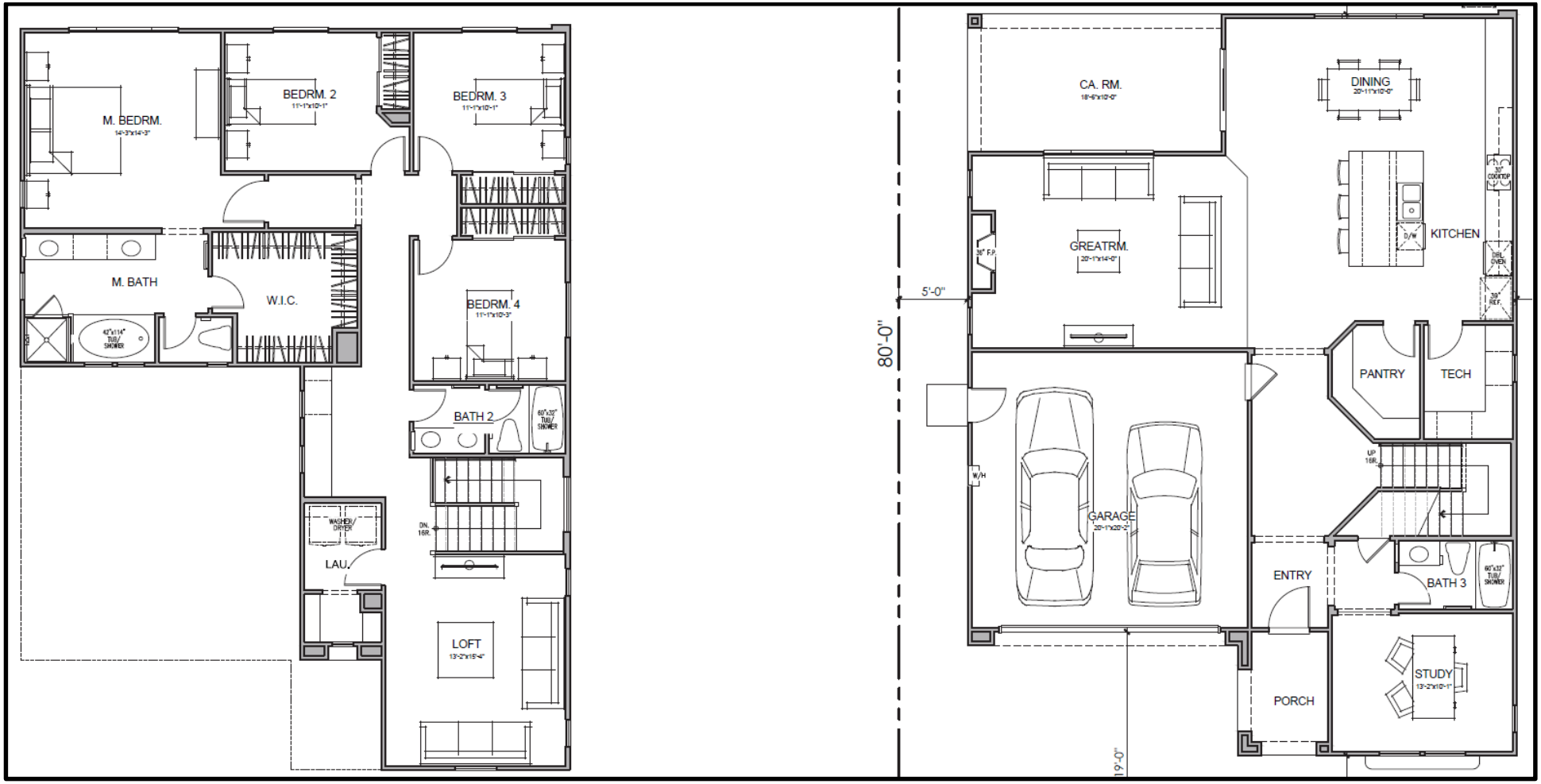

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 2: CONVENTIONAL

Spanish

Italianate

Craftsman

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 3: CONVENTIONAL

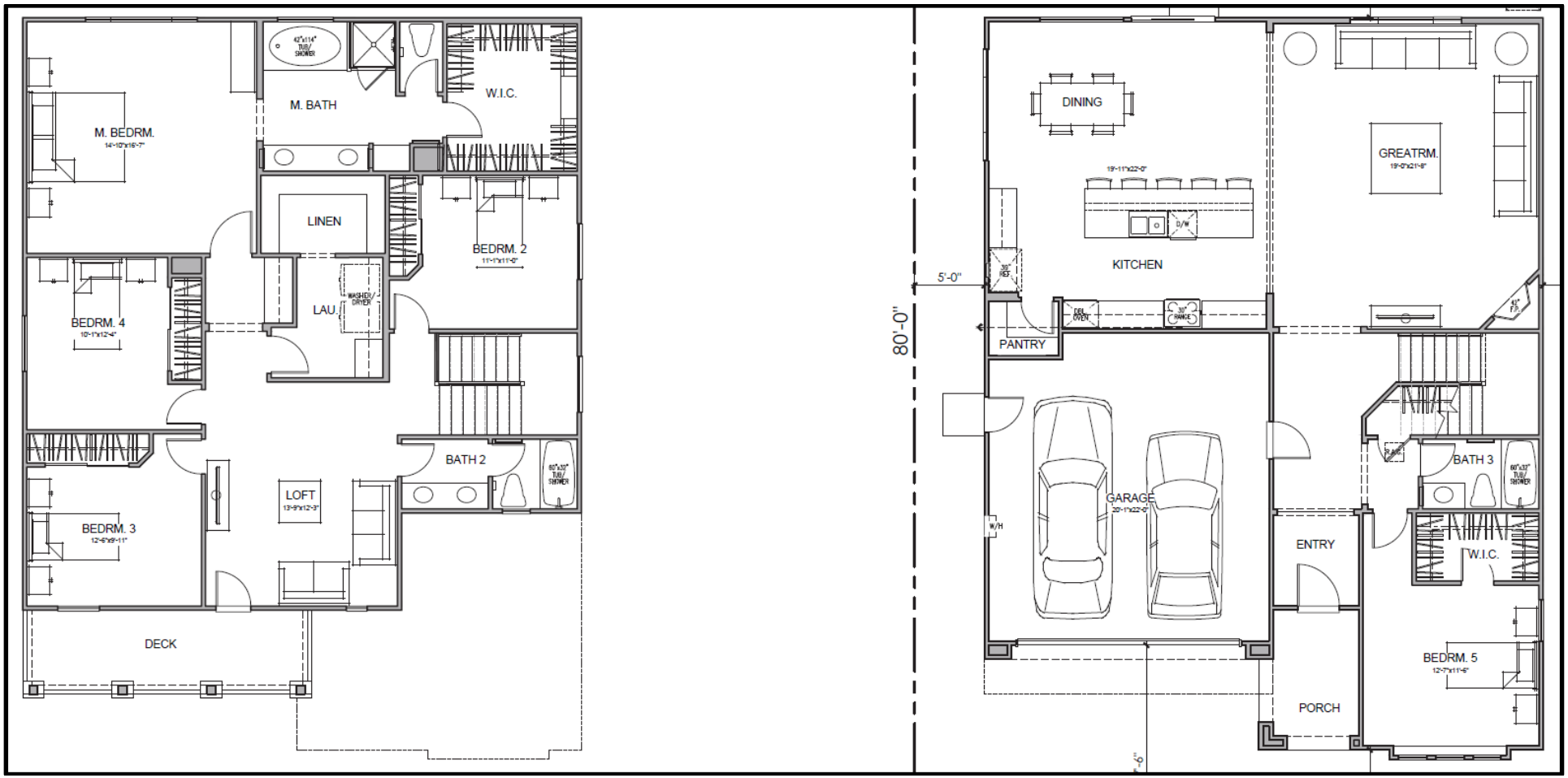

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 3: CONVENTIONAL

Spanish

Italianate

Craftsman

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 1: 4 & 6-PACK CLUSTER

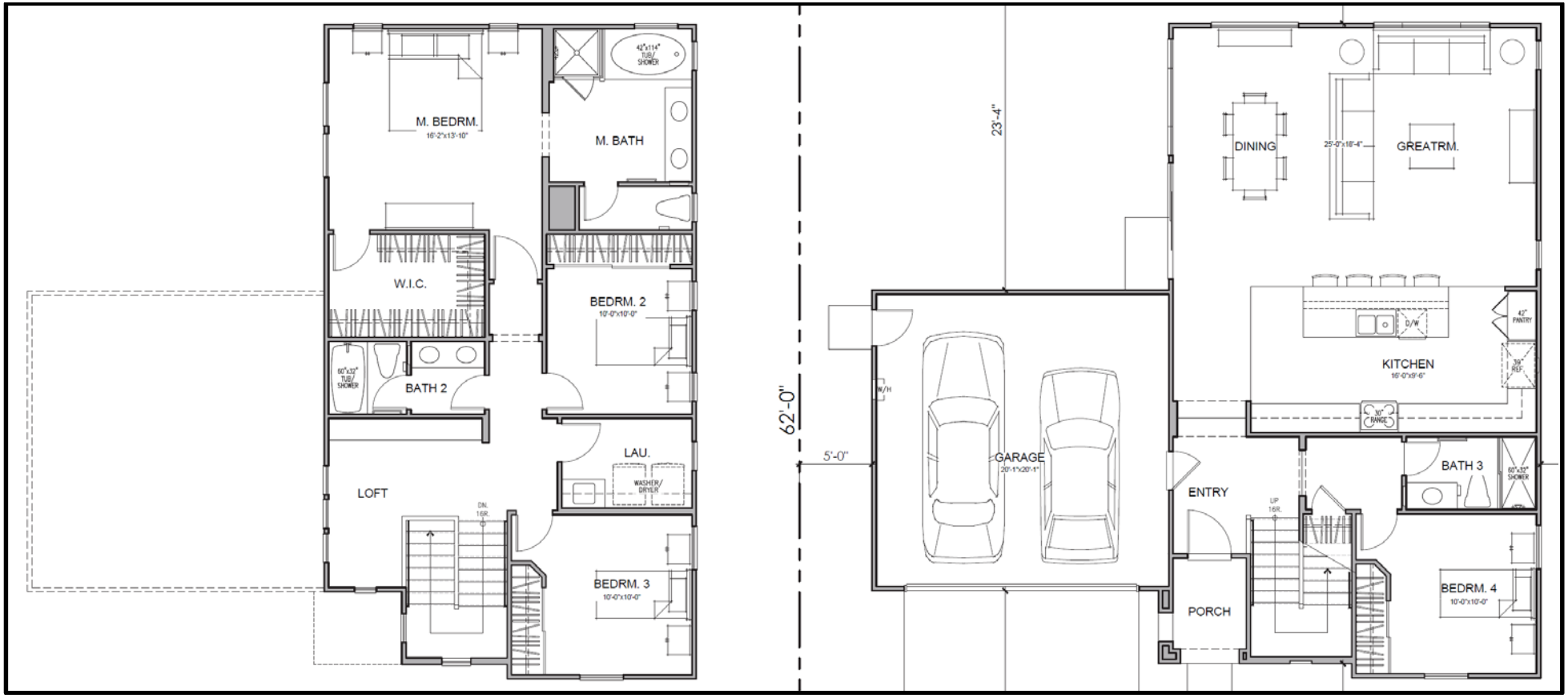

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 1: 4 & 6-PACK CLUSTER

Spanish

Italianate

Craftsman

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 1X: 4 & 6-PACK CLUSTER

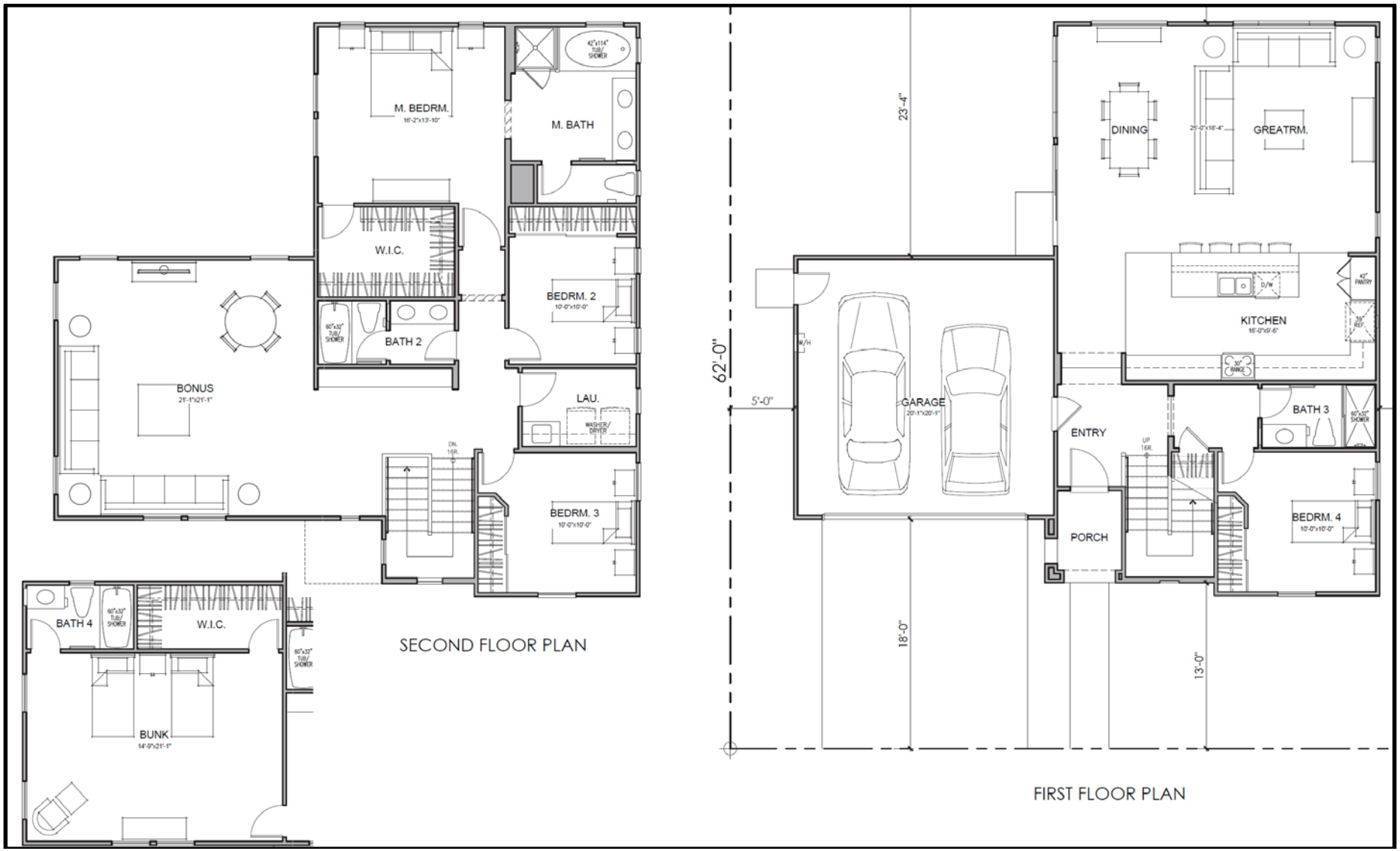

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 1X: 4 & 6-PACK CLUSTER

Spanish

Italianate

Craftsman

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 2: 4 & 6-PACK CLUSTER

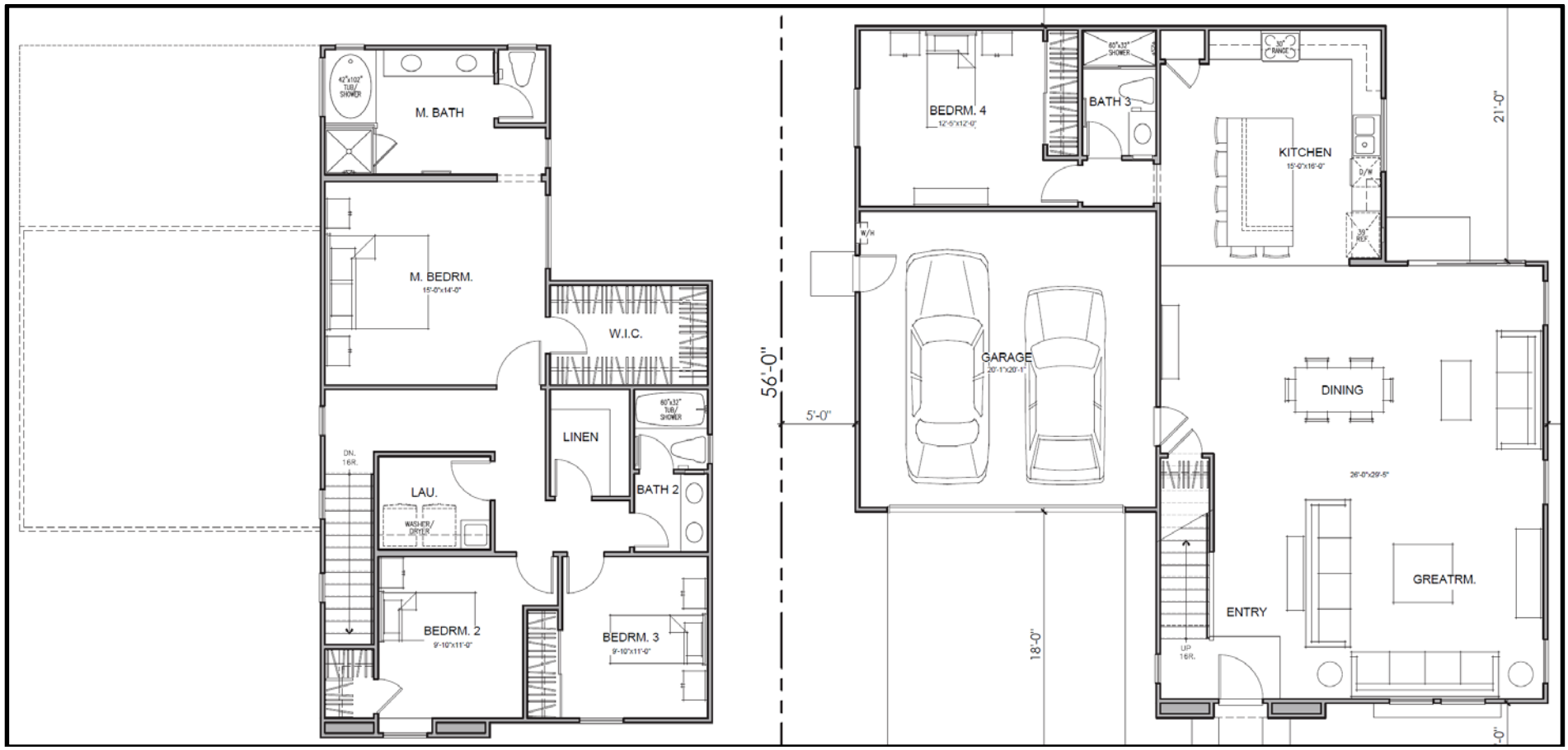

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 2: 4 & 6-PACK CLUSTER

Spanish

Italianate

Craftsman

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 2X: 4 & 6-PACK CLUSTER

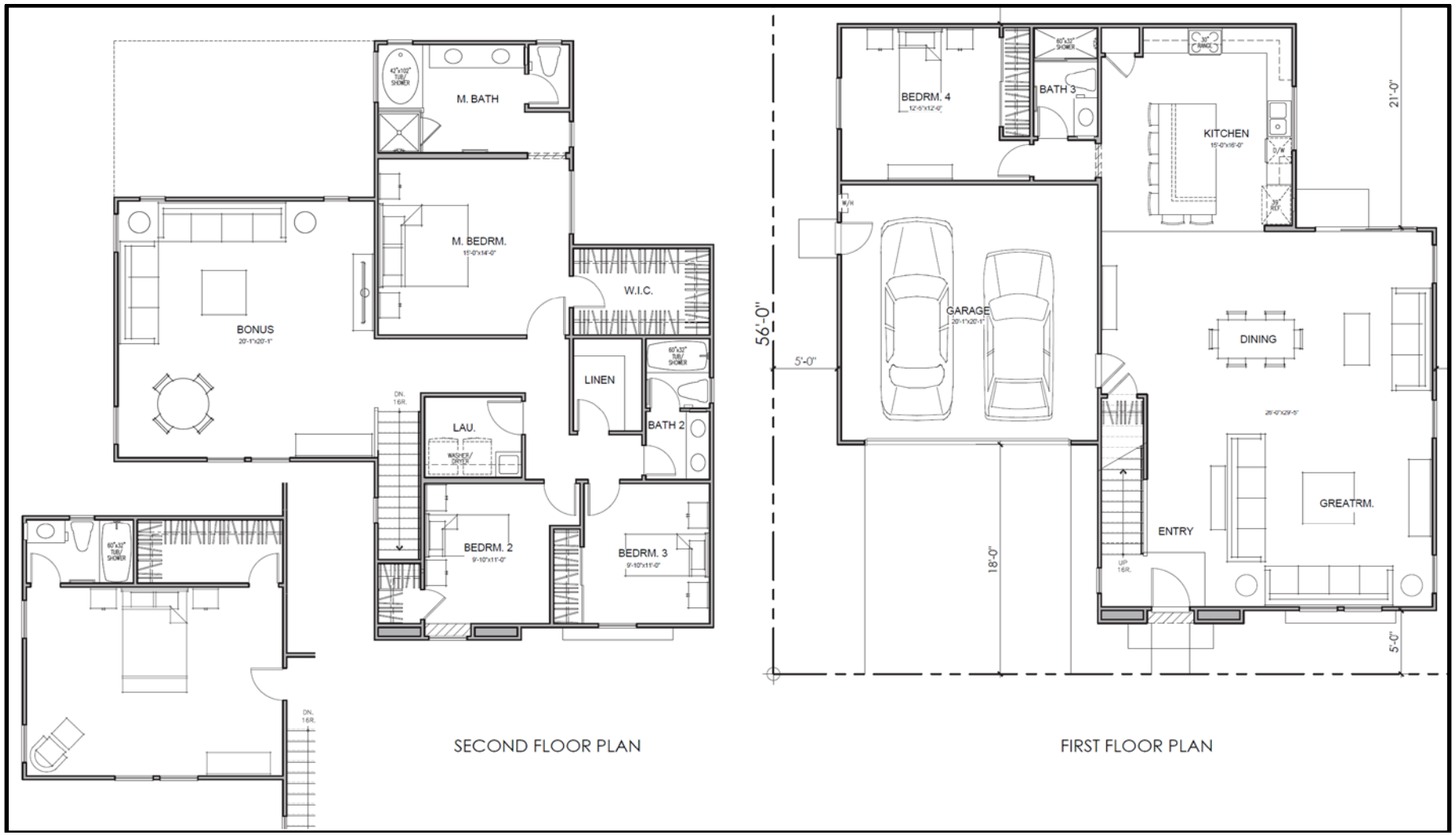

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 2X: 4 & 6-PACK CLUSTER

Spanish

Italianate

Craftsman

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 3: 4 & 6-PACK CLUSTER

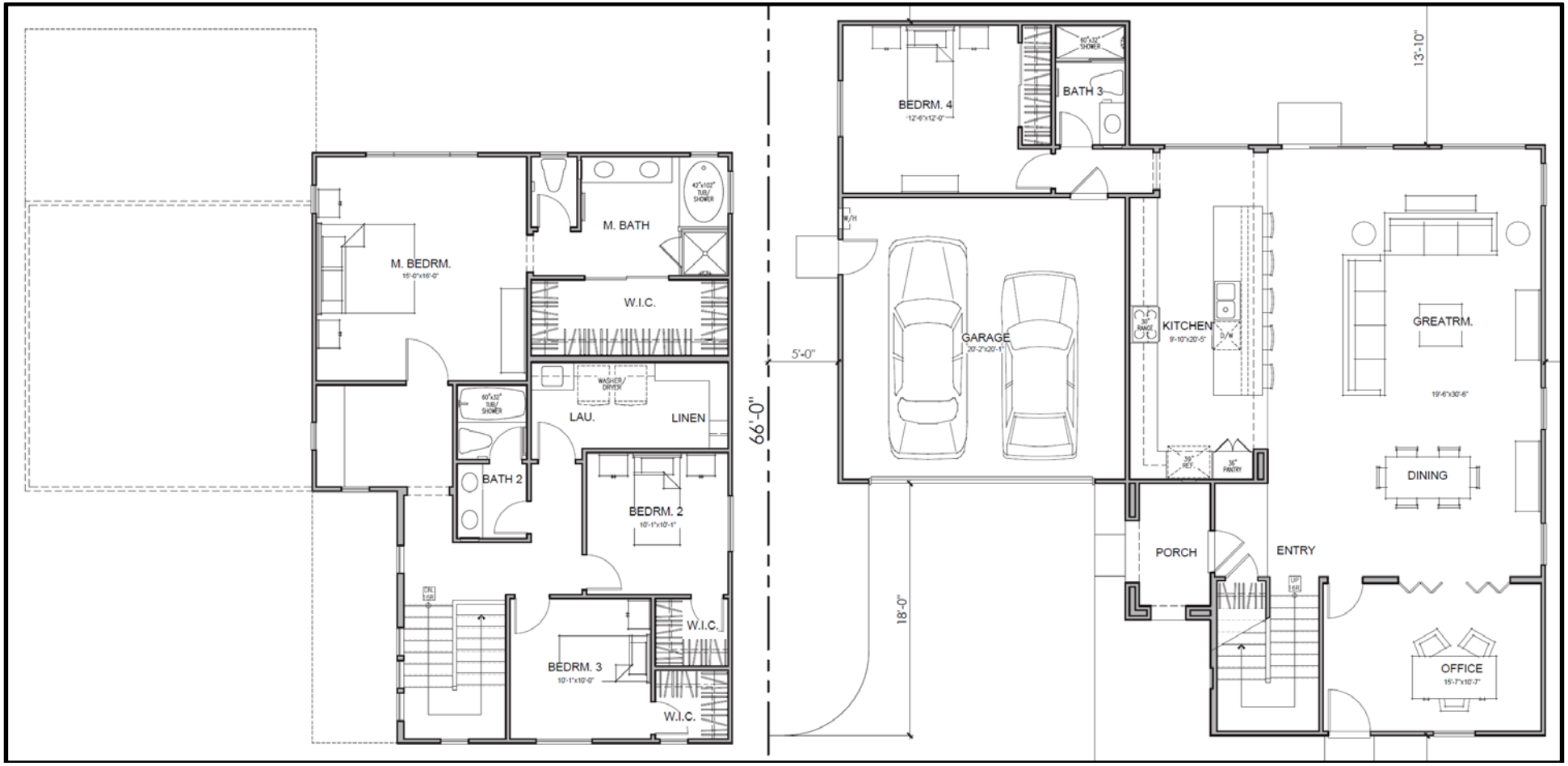

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 3: 4 & 6-PACK CLUSTER

Spanish

Italianate

Craftsman

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 3X: 4 & 6-PACK CLUSTER

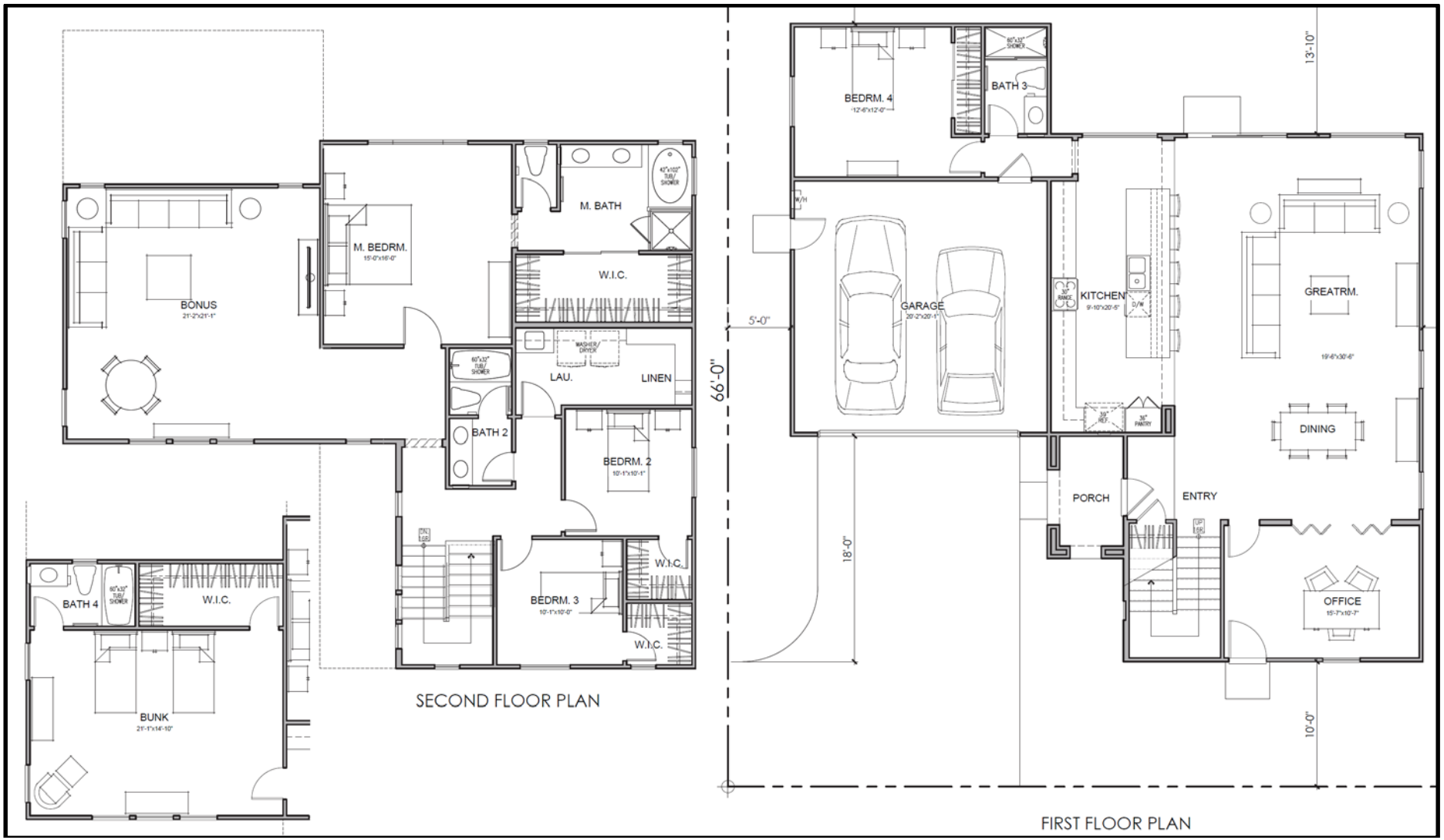

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 3X: 4 & 6-PACK CLUSTER

Spanish

Italianate

Craftsman

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 1: 8-PACK CLUSTER

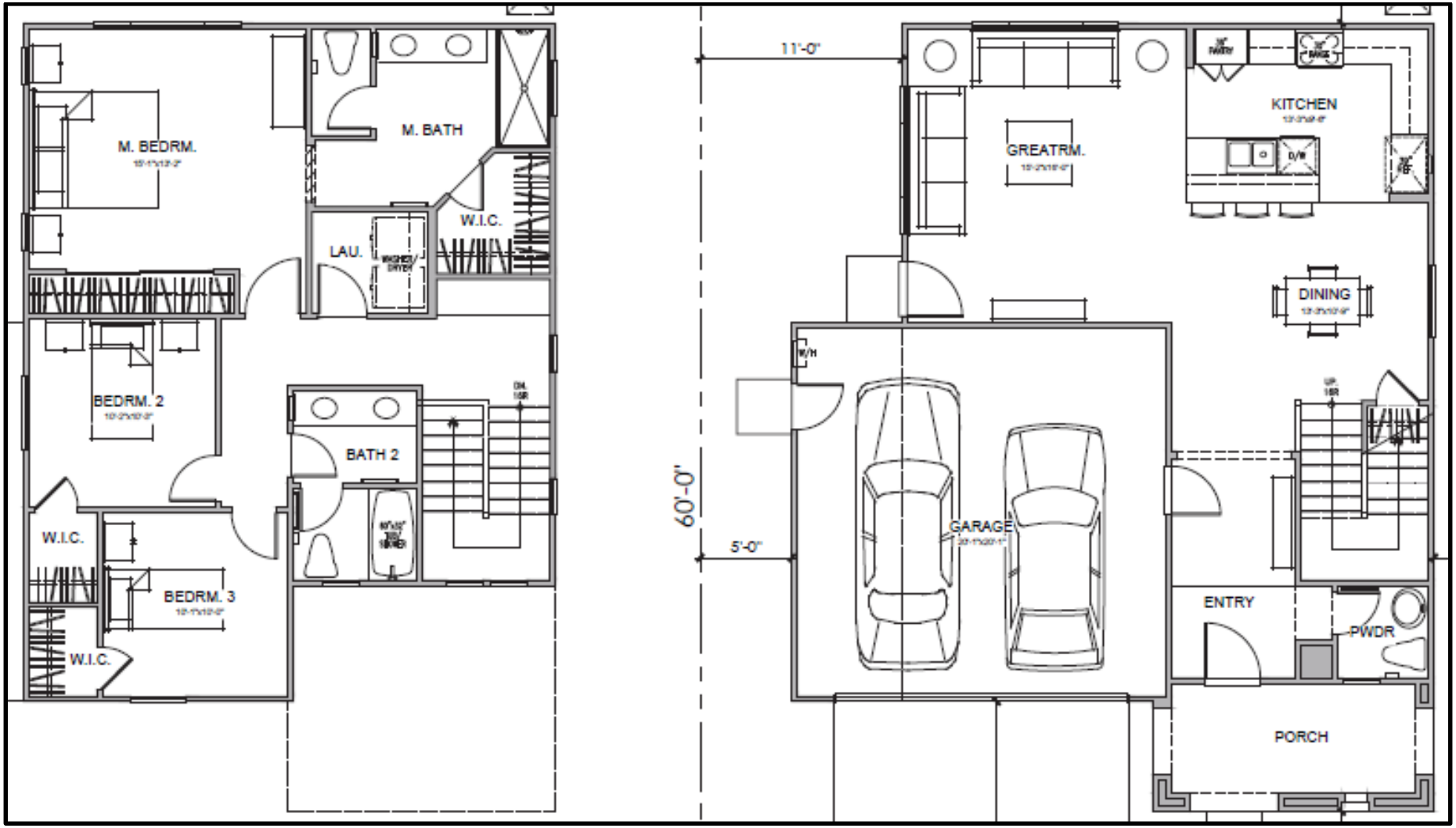

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 1: 8-PACK CLUSTER

Spanish

Italianate

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Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 2: 8-PACK CLUSTER

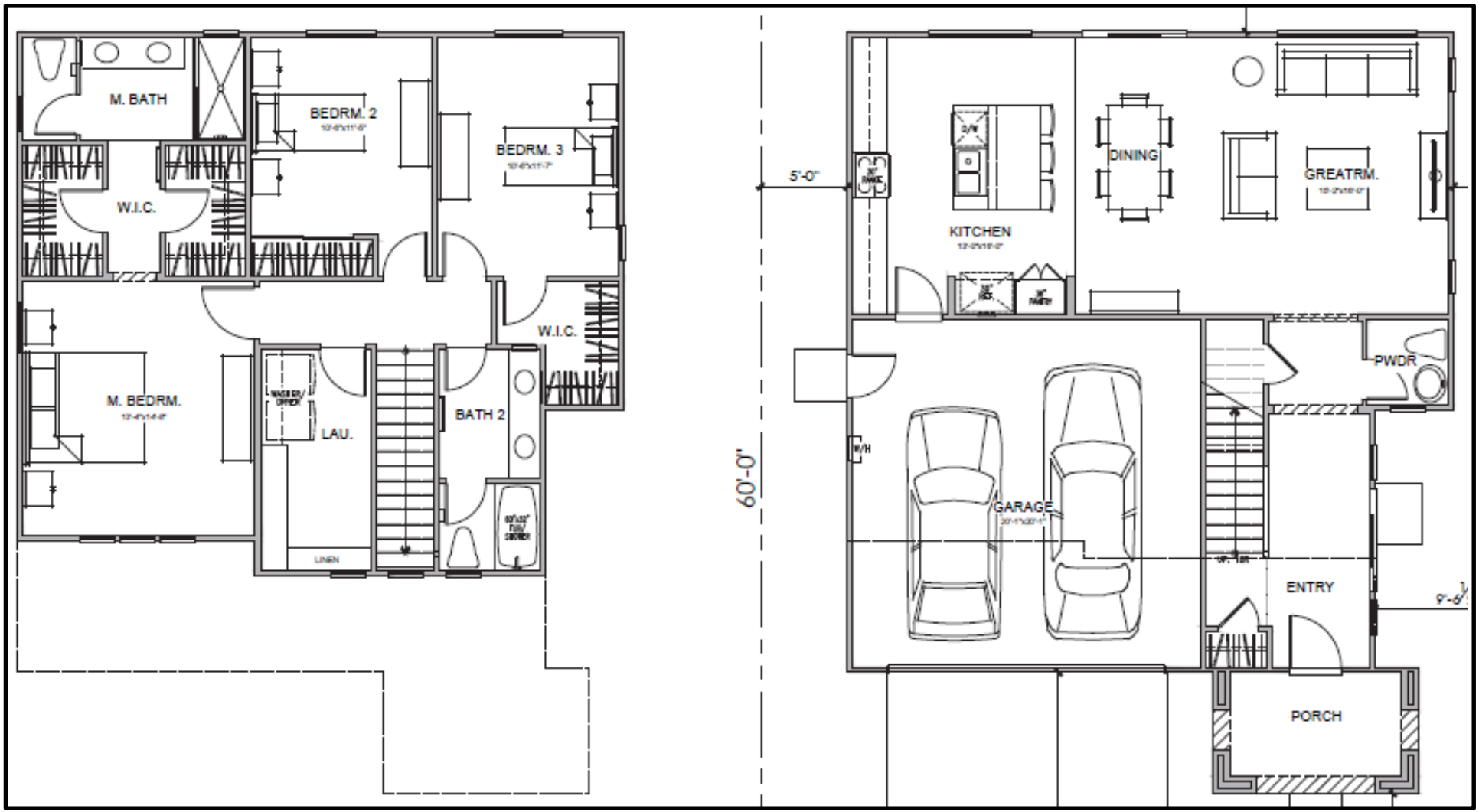

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 2: 8-PACK CLUSTER

Spanish

Italianate

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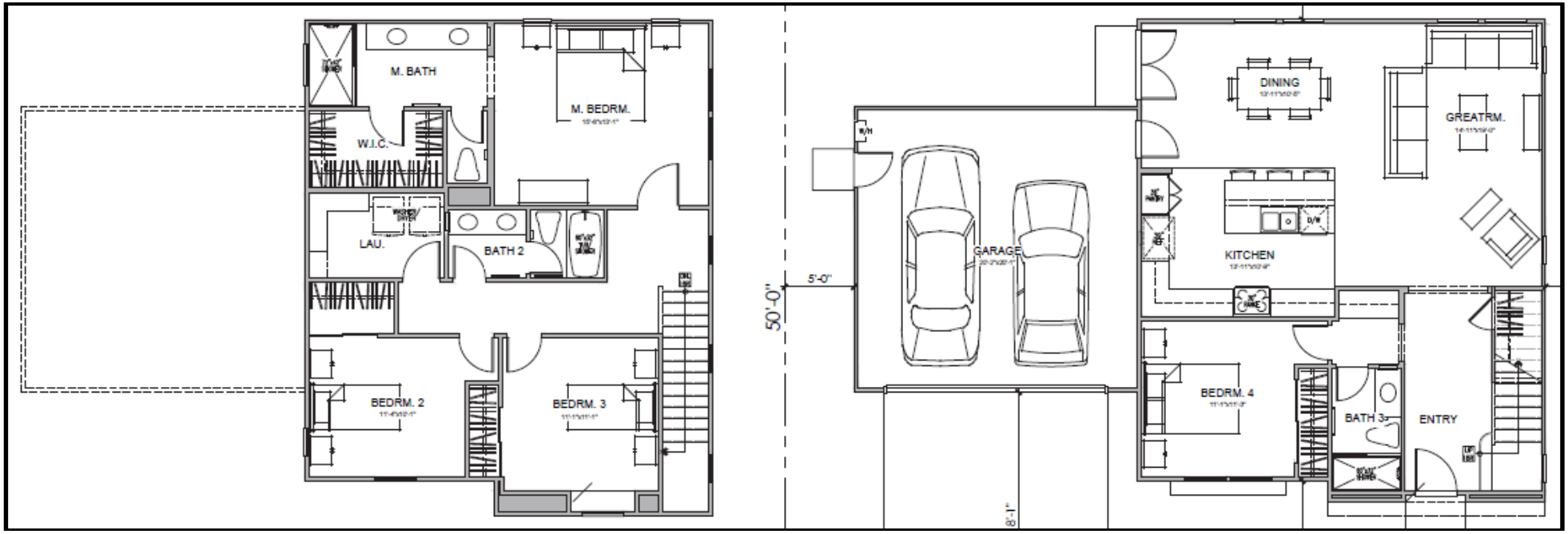

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 3: 8-PACK CLUSTER

Spanish

Italianate

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Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 3X: 8-PACK CLUSTER

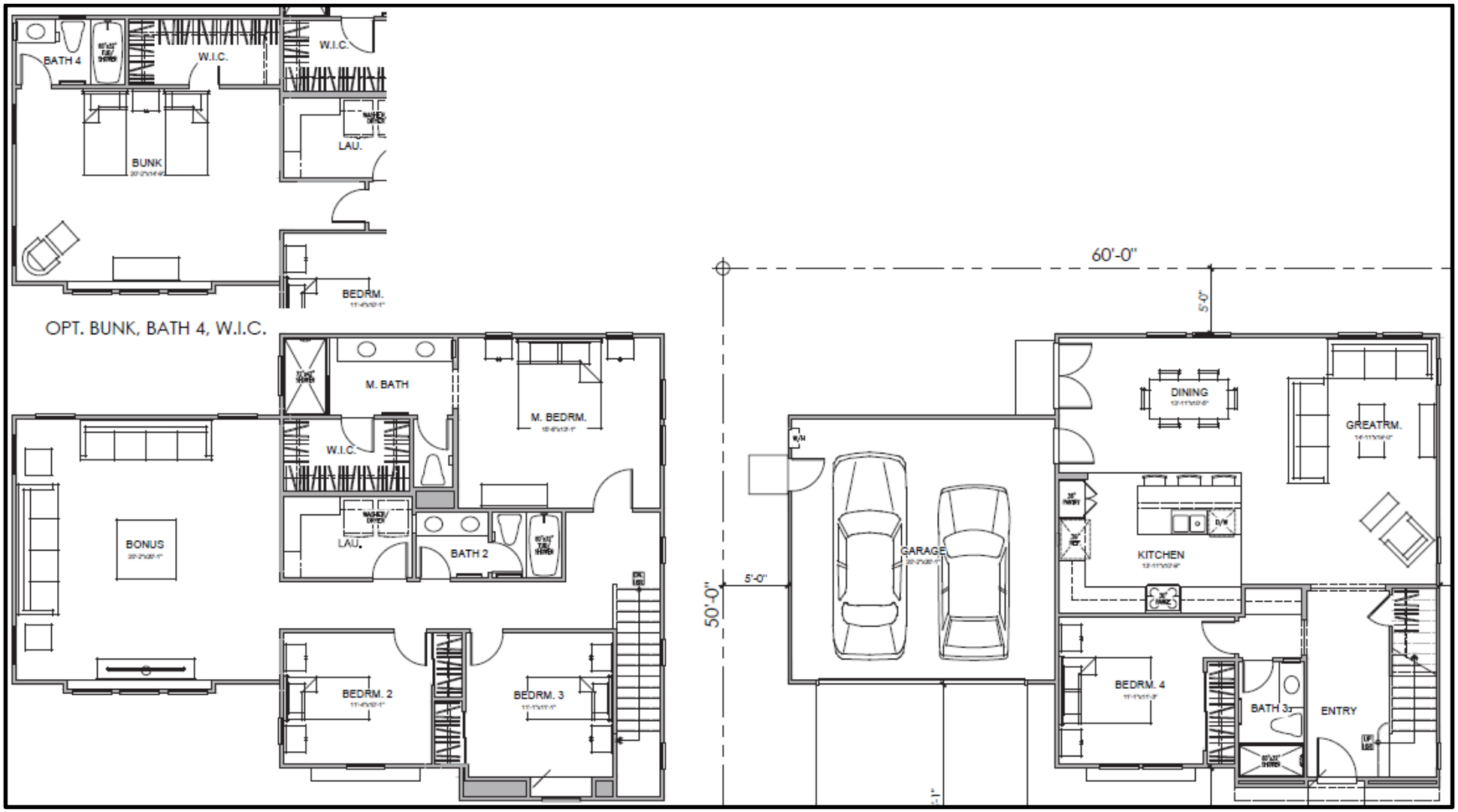

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 3X: 8-PACK CLUSTER

Spanish

Italianate

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Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 4: 8-PACK CLUSTER

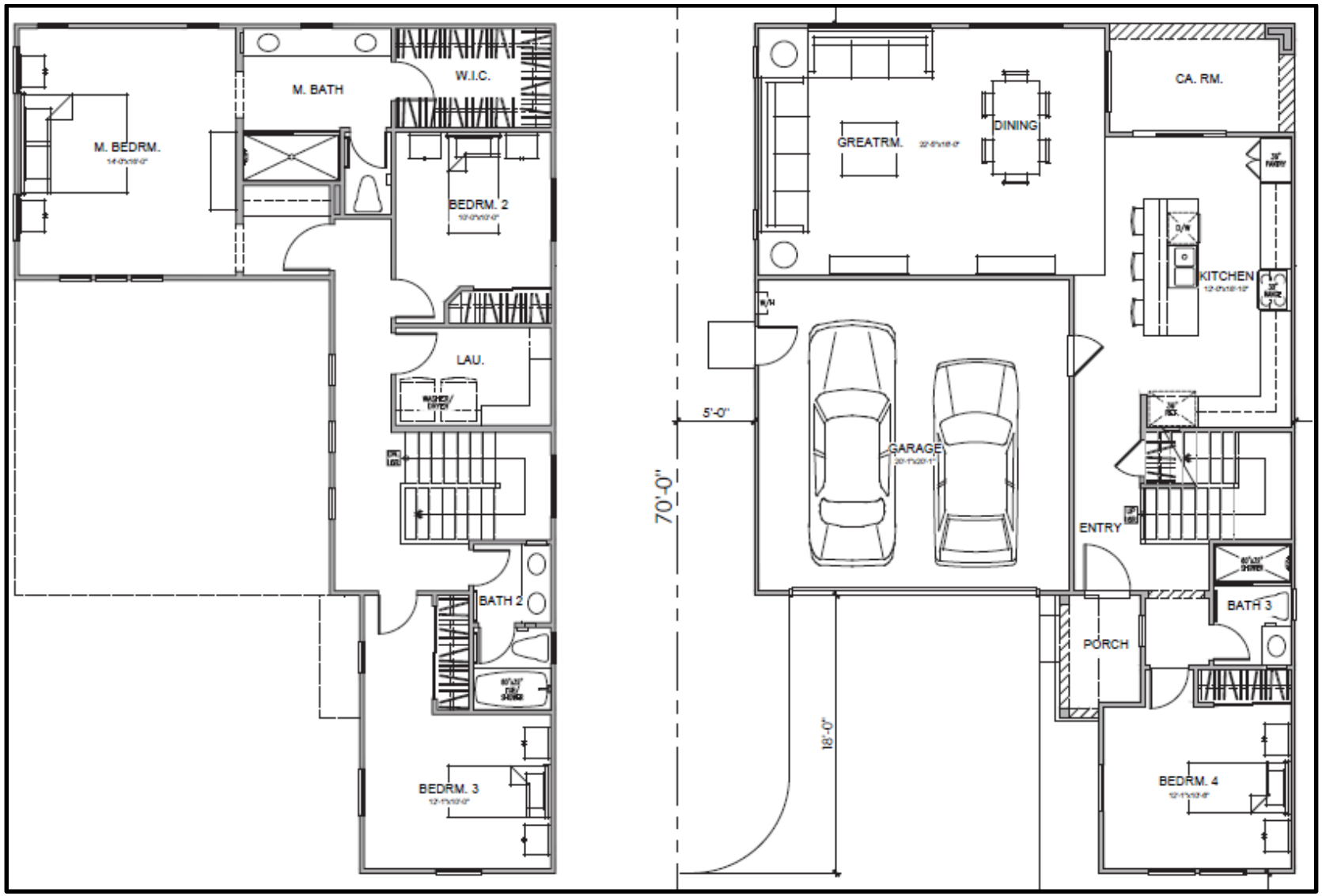

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 4: 8-PACK CLUSTER

Spanish

Italianate

Craftsman

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 4X: 8-PACK CLUSTER

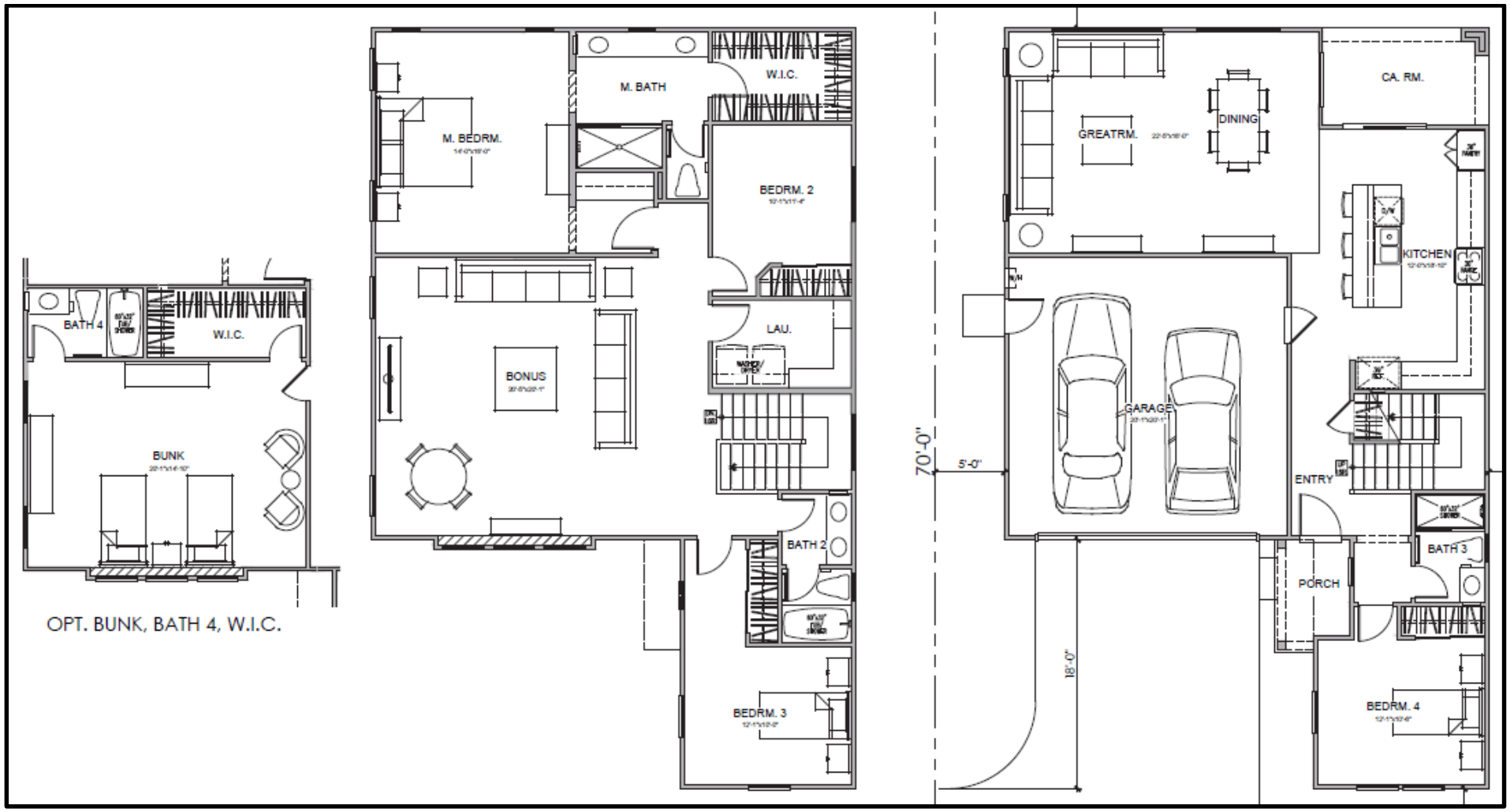

Exhibit E—FLOOR PLAN AND ELEVATIONS – PLAN 4X: 8-PACK CLUSTER

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RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV17-009, A DEVELOPMENT PLAN APPROVAL TO CONSTRUCT 330 SINGLE-FAMILY RESIDENTIAL DWELLING UNITS (104 CONVENTIONAL UNITS AND 226 CLUSTER UNITS) ON 48.82 ACRES OF LAND LOCATED WITHIN THE LOW DENSITY RESIDENTIAL DISTRICT OF PLANNING AREAS 4, 5 AND 6 OF THE GRAND PARK SPECIFIC PLAN, LOCATED AT THE SOUTHEAST CORNER OF ONTARIO RANCH ROAD AND TURNER AVENUE, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 0218-241-37, 0218-241-38 AND 0218-241-40.

WHEREAS, LENNAR HOMES OF CALIFORNIA, INC. ("Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV17-009, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 48.82 acres of land generally located at the southeast corner of Ontario Ranch Road and Turner Avenue, located within the Low Density Residential district of Planning Areas 4, 5 and 6 of the Grand Park Specific Plan, and is presently mass graded; and

WHEREAS, the property to the north of the project site is within the Low Medium Density Residential district of Planning Area 11 of The Avenue Specific Plan and is currently mass graded. The properties to the east, south and west are vacant and are located within the Medium Density Residential and Elementary School Districts of Planning Areas 3 and 9 and the Great Park District and High School District of Planning Area 10 of the Grand Park Specific Plan; and

WHEREAS, the Development Plan proposed is in compliance with the requirements of the Grand Park Specific Plan and is sufficient in size to facilitate and implement the traditional planning concepts for the “Residential Neighborhood” within the Specific Plan; and

WHEREAS, the proposed Development Plan is located within Planning Areas 4, 5 and 6 (SFD Conventional Homes and MEWS Homes Product Types) land use district of the Grand Park Specific Plan, which establishes a minimum lot size of 3,750 square feet for the SFD and 2,700 square feet for the MEWS and a development capacity of 361 dwelling units; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA"); and

WHEREAS, the environmental impacts of this project were previously reviewed in conjunction with File No. PSP12-001, the Grand Park Specific Plan for which an EIR (SCH# 2012061057) was adopted by the City Council on January 21, 2014, and this Application introduces no new significant environmental impacts; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to CEQA (Public Resources Code Section 21000 et seq.), and an initial study has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on September 23, 2014, the Planning Commission of the City of Ontario conducted a hearing and approved the related Tentative Tract Map File No. PMTT13-014 (TT18662); and

WHEREAS, on July 17, 2017, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project and concluded said hearing on that date, voting to issue Decision No. DAB17-035 recommending the Planning Commission approve the Application; and

WHEREAS, on July 25, 2017, the Planning Commission of the City of Ontario conducted a hearing to consider the Project and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: ***Environmental Determination and Findings.*** As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the previous Grand Park Specific Plan EIR (SCH# 2012061057) and supporting documentation. Based upon the facts and information contained in the previous Grand Park Specific Plan EIR (SCH# 2012061057) and supporting documentation, the Planning Commission finds as follows:

(1) The environmental impacts of this project were reviewed in conjunction with the Grand Park Specific Plan EIR (SCH# 2012061057), certified by the City of Ontario City Council on January 21, 2014, in conjunction with File No. PSP12-001; and

(2) The previous Grand Park Specific Plan EIR (SCH# 2012061057) contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous Grand Park Specific Plan EIR (SCH# 2012061057) was completed in compliance with CEQA and the Guidelines promulgated thereunder; and

(4) The previous Grand Park Specific Plan EIR (SCH# 2012061057) reflects the independent judgment of the Planning Commission; and

(5) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the previous Grand Park Specific Plan EIR (SCH# 2012061057), and all mitigation measures previously adopted with the Grand Park Specific Plan EIR (SCH# 2012061057), are incorporated herein by this reference.

SECTION 2: ***Subsequent or Supplemental Environmental Review Not Required.*** Based on the information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the

preparation of a subsequent or supplemental Grand Park Specific Plan EIR (SCH# 2012061057) is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Grand Park Specific Plan EIR (SCH# 2012061057) that will require major revisions to the Grand Park Specific Plan EIR (SCH# 2012061057) due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Grand Park Specific Plan EIR (SCH# 2012061057) was prepared, that will require major revisions to the Grand Park Specific Plan EIR (SCH# 2012061057) due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Grand Park Specific Plan EIR (SCH# 2012061057) was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the Grand Park Specific Plan EIR (SCH# 2012061057); or

(b) Significant effects previously examined will be substantially more severe than shown in the Grand Park Specific Plan EIR (SCH# 2012061057); or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Grand Park Specific Plan EIR (SCH# 2012061057) would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: ***Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the

proposed project is consistent with the number of dwelling units (330) and density (6.76 DU/AC) specified in the Available Land Inventory.

SECTION 4: ***Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport ("ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the Planning Commission, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 5: ***Concluding Facts and Reasons.*** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the Low Density Residential land use district of the Policy Plan Land Use Map, and the Low Density Residential district of Planning Areas 4 thru 6 of the Grand Park Specific Plan. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. The Development Plan has been required to comply with all provisions of SFD Conventional Homes and MEWS Homes Residential Development Standards of the Grand Park Specific Plan. Future neighborhoods within the Grand Park Specific Plan and surrounding area will provide for diverse housing and highly amenitized neighborhoods that will be compatible in design, scale and massing to the proposed development.

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the Low Density Residential (Planning Areas 4 thru 6) land use district of the Grand Park Specific Plan, including standards relative to the particular land use proposed (single-family residential), as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions. The Project has been designed consistent with the requirements of the Grand Park Specific Plan (SFD Conventional Homes and MEWS Homes) land use designations, including standards relative to the particular land use proposed (conventional and cluster single-family residential products), as well as building intensity, building and parking setbacks, building height, number of off-street parking spaces, on-site and off-site landscaping, and fences, walls and obstructions.

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Grand Park Specific Plan are maintained; [ii] the project will not endanger the public health, safety or general welfare; [iii] the project will not result in any significant environmental impacts; [iv] the project will be in harmony with the area in which it is located; and [v] the project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan, and the Grand Park Specific Plan. Additionally, the environmental impacts of this project were previously reviewed in conjunction with the Grand Park Specific Plan Environmental Impact Report (SCH#2012061057). This application is consistent with the previously adopted EIR and introduces no new significant environmental.

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development*****.** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Grand Park Specific Plan that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking, design and landscaping, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (conventional single-family residential). As a result of this review, the Planning Commission has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the Grand Park Specific Plan. Additionally, the Development Plan complies

with all provisions of SFD Conventional Homes and MEWS Homes Residential Development Standards of the Grand Park Specific Plan.

SECTION 6: ***Planning Commission Action.*** Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 7: ***Indemnification.*** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: ***Custodian of Records.*** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: ***Certification to Adoption.*** The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of July 2017, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Richard D. Delman
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Principal Planner / Acting Secretary of Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Marci Callejo, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. PC17-[insert #] was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 25, 2017, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Marci Callejo
Secretary Pro Tempore

ATTACHMENT A:

File No. PDEV17-009
Departmental Conditions of Approval

(Departmental conditions of approval to follow this page)

City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department
Land Development Division
Conditions of Approval

Meeting Date: July 17, 2017

File No: PDEV17-009

Related Files: PMTT13-014

Project Description: A Development Plan (File No. PDEV17-009) approval to construct 330 single-family residential dwelling units (104 conventional units and 226 cluster units) on 48.82 acres of land located within the Low Density Residential district of Planning Areas 4, 5 and 6 of the Grand Park Specific Plan, located at the southeast corner of Ontario Ranch Road and Turner Avenue. (APNs: 0218-241-37, 0218-241-38 and 0218-241-40); **submitted by Lennar Homes of California, Inc.**

Prepared By: Henry K. Noh, Senior Planner
Phone: 909.395.2429 (direct)
Email: hnoh@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2010. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(c) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

2.6 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.7 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.8 Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements.

(a) CC&Rs shall be prepared for the Project and shall be recorded prior to the issuance of a building permit.

(b) The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owners association and the CC&Rs shall be reviewed and approved by the City.

(c) CC&Rs shall ensure reciprocal parking and access between parcels.

(d) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:

(i) Landscaping and irrigation systems within common areas;
(ii) Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02;
(iii) Shared parking facilities and access drives; and
(iv) Utility and drainage easements.

(e) CC&Rs shall include authorization for the City's local law enforcement officers to enforce City and State traffic and penal codes within the project area.

(f) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.

(g) A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

2.9 Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.
(ii) Some of the property adjacent to this tract is zoned for agricultural uses and there could be fly, odor, or related problems due to the proximity of animals.
(iii) The area south of Riverside Drive lies within the San Bernardino County Agricultural Preserve. Dairies currently existing in that area are likely to remain for the foreseeable future.
(iv) This tract is part of a Landscape Maintenance District. The homeowner(s) will be assessed through their property taxes for the continuing maintenance of the district.

2.10 Environmental Review.

(a) The environmental impacts of this project were previously reviewed in conjunction with File No. PSP12-001, the Grand Park Specific Plan for which a(n) EIR (SCH# 2012061057) was adopted by the City Council on January 21, 2014. This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. The previously adopted mitigation measures shall be a condition of project approval, and are incorporated herein by this reference.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is

determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.11 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.12 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.13 Additional Requirements.

(a) Off-Site Subdivision Signs.

The City Council has authorized the Baldy View Chapter of the Building Industry Association to manage a standardized off-site directional sign program on a non-profit basis. The program uses uniform sign structures and individual identification and directional signs for residential development. **No other off-site signing is authorized.** (For additional information, contact the Baldy View Chapter BIA at (909) 945-1884.

(b) The applicant shall contact the Ontario Post Office to determine the size and location of mailboxes for this project. The location of the mailboxes shall be submitted to the Planning Department for review and approval prior to the issuance of building permits.

(c) The applicant (Developer) shall be responsible for providing fiber to each home per City requirements and standards.

(d) Dairy Separation Requirement for Residential Development.

The following separation requirements from existing dairies/feed lots shall apply to new residential development or structures used for public assembly purposes from existing dairies/feed lots.

A minimum 100' separation shall be required between a new residential, commercial or industrial development or structure used for public assembly and an existing animal feed trough, corral/pen or an existing dairy/feed lot including manure stockpiles and related wastewater detention basins. The 100-foot separation requirement may be satisfied by an off-site easement acceptable to the Planning Director with adjacent properties, submitted with the initial final map and recorded prior to or concurrent with the final map.

(e) All applicable conditions of approval of Development Agreement (File No. PDA14-002) shall apply to this tract.

(f) All applicable conditions of approval of the Grand Park Specific Plan shall apply to this tract.

(g) All applicable conditions of approval of the "B" Map TT 18662 (File No. PMTT13-014) shall apply to this Development Plan.

(h) Private Parks (Lots C, D, and E) shall be constructed prior to the issuance of the certificate of occupancy of the 113th home within the cluster development (Lots 1-226). Private Park (Lot F) shall be constructed prior to the issuance of the certificate of occupancy of the 52nd home within the conventional single-family detached homes (Lots 227-330).

(i) All private lanes shall be enhanced with a combination of pavers, colored concrete or similar decorative material subject to the review and approval by the Planning Director.

(j) The Ontario Climate Action Plan (CAP) requires new development to be 25% more efficient. The applicant has elected to utilize the Screening Tables provided in the CAP instead of preparing separate emissions calculations. By electing to utilize the Screening Tables the applicant shall be required to garner a minimum 100 points to be consistent with the reduction quantities outlined in the CAP. The applicant shall identify on the construction plans the items identified in the attached residential Screening Tables.

Table 1: Screening Table for Implementation of GHG Reduction Measures for Residential Development

Feature	Description	Assigned Point Values	Project Points
Reduction Measure PS E1: Residential Energy Efficiency			
Building Envelope			
Insulation	2008 Baseline (walls R-13:, roof/attic: R-30)	0 points	
	Modestly Enhanced Insulation (walls R-13:, roof/attic: R-38)	(12 points)	12
	Enhanced Insulation (rigid wall insulation R-13, roof/attic: R-38)	15 points	
	Greatly Enhanced Insulation (spray foam wall insulated walls R-15 or higher, roof/attic R-38 or higher)	18 points	
Windows	2008 Baseline Windows (0.57 U-factor, 0.4 solar heat gain coefficient (SHGC)	0 points	
	Modestly Enhanced Window Insulation (0.4 U-Factor, 0.32 SHGC)	6 points	
	Enhanced Window Insulation (0.32 U-Factor, 0.25 SHGC)	(7 points)	7
	Greatly Enhanced Window Insulation (0.28 or less U-Factor, 0.22 or less SHGC)	9 points	
Cool Roof	Modest Cool Roof (CRRC Rated 0.15 aged solar reflectance, 0.75 thermal emittance)	(10 points)	
	Enhanced Cool Roof(CRRC Rated 0.2 aged solar reflectance, 0.75 thermal emittance)	12 points	10
	Greatly Enhanced Cool Roof (CRRC Rated 0.35 aged solar reflectance, 0.75 thermal emittance)	14 points	
Air Infiltration	Minimizing leaks in the building envelope is as important as the insulation properties of the building. Insulation does not work effectively if there is excess air leakage.		
	Air barrier applied to exterior walls, calking, and visual inspection such as the HERS Verified Quality Insulation Installation (QII or equivalent)	(10 points)	10
	Blower Door HERS Verified Envelope Leakage or equivalent	8 points	
Thermal Storage of Building	Thermal storage is a design characteristic that helps keep a constant temperature in the building. Common thermal storage devices include strategically placed water filled columns, water storage tanks, and thick masonry walls.		
	Modest Thermal Mass (10% of floor or 10% of walls: 12" or more thick exposed concrete or masonry. No permanently installed floor covering such as carpet, linoleum, wood or other insulating materials)	2 points	
	Enhanced Thermal Mass (20% of floor or 20% of walls: 12" or more thick exposed concrete or masonry. No permanently installed floor covering such as carpet, linoleum, wood or other insulating materials)	4 points	

Energy Consultant

Feature	Description	Assigned Point Values	Project Points
Indoor Space Efficiencies			
Heating/ Cooling Distribution System	Minimum Duct Insulation (R-4.2 required)	0 points	
	Modest Duct Insulation (R-6)	7 points	7
	Enhanced Duct Insulation (R-8)	8 points	
	Distribution loss reduction with Inspection (HERS Verified Duct Leakage or equivalent)	12 points	12
Space Heating/ Cooling Equipment	2008 Minimum HVAC Efficiency (SEER 13/60% AFUE or 7.7 HSPF)	0 points	
	Improved Efficiency HVAC (SEER 14/65% AFUE or 8 HSPF)	4 points	4
	High Efficiency HVAC (SEER 15/72% AFUE or 8.5 HSPF)	7 points	
	Very High Efficiency HVAC (SEER 16/80% AFUE or 9 HSPF)	9 points	
Water Heaters	2008 Minimum Efficiency (0.57 Energy Factor)	0 points	
	Improved Efficiency Water Heater (0.675 Energy Factor)	12 points	
	High Efficiency Water Heater (0.72 Energy Factor)	15 points	
	Very High Efficiency Water Heater (0.92 Energy Factor)	18 points	18
	Solar Pre-heat System (0.2 Net Solar Fraction)	4 points	
	Enhanced Solar Pre-heat System (0.35 Net Solar Fraction)	8 points	
Daylighting	Daylighting is the ability of each room within the building to provide outside light during the day reducing the need for artificial lighting during daylight hours.		
	All peripheral rooms within the living space have at least one window (required)	0 points	
	All rooms within the living space have daylight (through use of windows, solar tubes, skylights, etc.)	1 points	
	All rooms daylighted	2 points	
Artificial Lighting	2008 Minimum (required)	0 points	
	Efficient Lights (25% of in-unit fixtures considered high efficacy. High efficacy is defined as 40 lumens/watt for 15 watt or less fixtures; 50 lumens/watt for 15-40 watt fixtures, 60 lumens/watt for fixtures >40watt)	8 points	
	High Efficiency Lights (50% of in-unit fixtures are high efficacy)	10 points	
	Very High Efficiency Lights (100% of in-unit fixtures are high efficacy)	12 points	12
Appliances	Energy Star Refrigerator (new)	1 points	
	Energy Star Dish Washer (new)	1 points	1
	Energy Star Washing Machine (new)	1 points	

Feature	Description	Assigned Point Values	Project Points
Miscellaneous Residential Building Efficiencies			
Building Placement	North/South alignment of building or other building placement such that the orientation of the buildings optimizes natural heating, cooling, and lighting.	5 point	
Shading	At least 90% of south-facing glazing will be shaded by vegetation or overhangs at noon on Jun 21st.	4 Points	
Energy Star Homes	EPA Energy Star for Homes (version 3 or above)	25 points	
Independent Energy Efficiency Calculations	Provide point values based upon energy efficiency modeling of the Project. Note that engineering data will be required documenting the energy efficiency and point values based upon the proven efficiency beyond Title 24 Energy Efficiency Standards.	TBD	
Other	This allows innovation by the applicant to provide design features that increases the energy efficiency of the project not provided in the table. Note that engineering data will be required documenting the energy efficiency of innovative designs and point values given based upon the proven efficiency beyond Title 24 Energy Efficiency Standards.	TBD	
Existing Residential Retrofits	The applicant may wish to provide energy efficiency retrofit projects to existing residential dwelling units to further the point value of their project. Retrofitting existing residential dwelling units within the City is a key reduction measure that is needed to reach the reduction goal. The potential for an applicant to take advantage of this program will be decided on a case by case basis and must have the approval of the Ontario Planning Department. The decision to allow applicants to ability to particlpate in this program will be evaluated based upon, but not limited to the following; Will the energy efficiency retrofit project benefit low income or disadvantaged residents? Does the energy efficiency retrofit project fit within the overall assumptions in reduction measures associated with existing residential retrofits? Does the energy efficiency retrofit project provide co-benefits important to the City? Point value will be determined based upon engineering and design criteria of the energy efficiency retrofit project.	TBD	
Reduction Measure PS E2: Residential Renewable Energy Generation			
Photovoltaic	Solar Photovoltaic panels installed on individual homes or in collective neighborhood arrangements such that the total power provided augments:		
	Solar Ready Homes (sturdy roof and solar ready service panel)	2 points	
	10 percent of the power needs of the project	10 points	
	20 percent of the power needs of the project	15 points	
	30 percent of the power needs of the project	20 points	
	40 percent of the power needs of the project	28 points	
	50 percent of the power needs of the project	(35 points)	35
	60 percent of the power needs of the project	38 points	
	70 percent of the power needs of the project	42 points	
	80 percent of the power needs of the project	46 points	

Feature	Description	Assigned Point Values	Project Points
	90 percent of the power needs of the project	52 points	
	100 percent of the power needs of the project	58 points	
Wind turbines	Some areas of the City lend themselves to wind turbine applications. Analysis of the area's capability to support wind turbines should be evaluated prior to choosing this feature.		
	Individual wind turbines at homes or collective neighborhood arrangements of wind turbines such that the total power provided augments:		
	10 percent of the power needs of the project	10 points	
	20 percent of the power needs of the project	15 points	
	30 percent of the power needs of the project	20 points	
	40 percent of the power needs of the project	28 points	
	50 percent of the power needs of the project	35 points	
	60 percent of the power needs of the project	38 points	
	70 percent of the power needs of the project	42 points	
	80 percent of the power needs of the project	46 points	
	90 percent of the power needs of the project	52 points	
	100 percent of the power needs of the project	58 points	
Off-site renewable energy project	The applicant may submit a proposal to supply an off-site renewable energy project such as renewable energy retrofits of existing homes that will help implement renewable energy within the City. These off-site renewable energy retrofit project proposals will be determined on a case by case basis and must be accompanied by a detailed plan that documents the quantity of renewable energy the proposal will generate. Point values will be determined based upon the energy generated by the proposal.	TBD	
Other Renewable Energy Generation	The applicant may have innovative designs or unique site circumstances (such as geothermal) that allow the project to generate electricity from renewable energy not provided in the table. The ability to supply other renewable energy and the point values allowed will be decided based upon engineering data documenting the ability to generate electricity.	TBD	
Reduction Measure PS W1: Residential Water Conservation			
Irrigation and Landscaping			
Water Efficient Landscaping	Limit conventional turf to < 50% of required landscape area	0 points	
	Limit conventional turf to < 25% of required landscape area	4 points	
	No conventional turf (warm season turf to < 50% of required landscape area and/or low water using plants are allowed)	6 points	6
	Only California Native Plants that requires no irrigation or some supplemental irrigation	8 points	

Landscape Arch.

Feature	Description	Assigned Point Values	Project Points
Water Efficient irrigation systems	Low precipitation spray heads < .75"/hr or drip irrigation Weather based irrigation control systems or moisture sensors (demonstrate 20% reduced water use)	2 point 3 points	5
Recycled Water	Recycled connections (purple pipe) to irrigation system on site	6 points	Co
Water Reuse	Gray water Reuse System collects Gray water from clothes washers, showers and faucets for irrigation use,	12 points	
Storm water Reuse Systems	Innovative on-site stormwater collection, filtration and reuse systems are being developed that provide supplemental irrigation water and provide vector control. These systems can greatly reduce the irrigation needs of a project. Point values for these types of systems will be determined based upon design and engineering data documenting the water savings.	TBD	
Potable Water			
Showers	Water Efficient Showerheads (2.0 gpm)	3 points	3
Toilets	Water Efficient Toilets (1.5 gpm)	3 points	3
Faucets	Water Efficient faucets (1.28 gpm)	3 points	3
Dishwasher	Water Efficient Dishwasher (6 gallons per cycle or less)	1	
Washing Machine	Water Efficient Washing Machine (Water factor <5.5)	1	
WaterSense	EPA WaterSense Certification	12 points	
Reduction Measure PS T1: Land Use Based Trips and VMT Reduction			
Mixed Use	Mixes of land uses that complement one another in a way that reduces the need for vehicle trips can greatly reduce GHG emissions. The point value of mixed use projects will be determined based upon a Transportation Impact Analysis (TIA) demonstrating trip reductions and/or reductions in vehicle miles traveled. Suggested ranges: Diversity of land uses complementing each other (2-28 points) Increased destination accessibility other than transit (1-18 points) Increased transit accessibility (1-25 points) Infill location that reduces vehicle trips or VMT beyond the measures described above (points TBD based on traffic data).	TBD	
Residential Near Local Retail (Residential only Projects)	Having residential developments within walking and biking distance of local retail helps to reduce vehicle trips and/or vehicle miles traveled. The point value of residential projects in close proximity to local retail will be determined based upon traffic studies that demonstrate trip reductions and/or reductions in vehicle miles traveled (VMT)	TBD	

Landscape Arch

Feature	Description	Assigned Point Values	Project Points
Other Trip Reduction Measures	Other trip or VMT reduction measures not listed above with TIA and/or other traffic data supporting the trip and/or VMT for the project.	TBD	
Reduction Measure PS T2: Bicycle Master Plan			
Bicycle Infrastructure	Ontario's Bicycle Master Plan is extensive and describes the construction on 11.5 miles of Class I bike paths and 23 miles of Class II and Class III bikeways to build upon the current 8 miles of bikeways. Provide bicycle paths within project boundaries. Provide bicycle path linkages between residential and other land uses. Provide bicycle path linkages between residential and transit.	TBD 2 points 5 points	
Reduction Measure PS T3: Neighborhood Electric Vehicle Infrastructure			
Electric Vehicle Recharging	Provide circuit and capacity in garages of residential units for use by an electric vehicle. Charging stations are for on-road electric vehicles legally able to drive on all roadways including Interstate Highways and freeways. Install electric vehicle charging stations in the garages of residential units	1 point 8 points	1
Total Points Earned by Residential Project:			155

Architect

CITY OF ONTARIO
MEMORANDUM

ENGINEERING DEPARTMENT
CONDITIONS OF APPROVAL

(Traffic/Transportation Division and Municipal Utilities Agency, and Environmental Section Conditions incorporated)

DATE: 06/29/17

PROJECT PLANNER: Henry Noh, Planning Department

PROJECT: PDEV17-009– A Development Plan approval to construct 329 single-family dwellings on approximately 79 acres within the Grand Park Specific Plan.

APN: 0238-241-10, 11, 13, 14, 17 & 18

LOCATION: Southeast corner of Ontario Ranch Road and Turner Avenue

PROJECT ENGINEER: Bryan Lirley, P.E., Engineering Department

The following items are the Conditions of Approval for the subject project:

1. The applicant/developer shall be responsible to complete all applicable conditions as specified in the Conditions of Approval for TM 18662 and the Ontario Edison Holdings L.L.C. (Lennar Homes of California, Inc.) Development Agreement.

2. The applicant/developer shall provide fiber optic connection to each home unit per city standards and guidelines.

3. The applicant/developer shall be responsible to pay Development Impact Fees (DIF) to the Building Department.

Bryan Lirley, P.E. — 6/28/17
Senior Associate Civil Engineer — Date

Khoi Do, P.E. — 6-29-17
Assistant City Engineer — Date

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
303 East "B" Street, Ontario, CA 91764

CONDITIONS OF APPROVAL

Sign Off

Carolyn Bell — Carolyn Bell, Sr. Landscape Planner — 6/28/17 Date

Reviewer's Name: **Carolyn Bell, Sr. Landscape Planner**

Phone: **(909) 395-2237**

D.A.B. File No.: PDEV17-009

Case Planner: Henry Noh

Project Name and Location:
300 Single family and Cluster homes, 1 Rec center and 3 Parks
Grand Park Eagle Rock, Pacific Crest, Sierra Park
SEC Turner and Ontario Ranch Rd

Applicant/Representative:
Lennar Homes – Elena Figueroa
980 Montecito Dr ste 300
Corona, CA 92879

☒	**A Preliminary Landscape Plan (dated 3/2/17) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.**
☐	**A Preliminary Landscape Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.**

CORRECTIONS REQUIRED

Civil Plans

1. Correct corner ramps and back of sidewalk design to meet std detail 1213 for max 10' or 13' dimension. Do not align sidewalk edge with right of way line if greater than dimension on detail.
2. Show backflows and transformers on plan, and dimension a min 4' set back from paving.
3. Locate light standards, fire hydrants, water and sewer lines to not conflict with required tree locations (typ. 30' oc). Coordinate civil plans with landscape plans
4. Note for compaction to be no greater than 85% at landscape areas. All finished grades at 1 ½" below finished surfaces. Slopes to be maximum 3:1. Change 2:1 slopes to be 3:1.
5. If any parking areas, dimension all planters to have a minimum 5' wide inside dimension with 12" wide curbs, 12" wide pavers or DG paving with edging where parking spaces are adjacent to planters. Show 1 planter for every 10 parking spaces and at each row end, where occurs.
6. Show letter lots between sidewalk and single family residence side yard wall, to identify HOA maintained landscape and recycled water irrigation.
7. Typical lot drainage shall include a catch basin with gravel sump below each before exiting property, to avoid nuisance water and improve water quality infiltration.

Landscape Plans

8. Show all utilities on the landscape plans. Coordinate with civil to keep utilities clear of required tree locations.
9. Show parkway landscape max 18" high and dimension street trees to be evenly spaced 30' apart.
10. Show planters at rec building to screen trash enclosures and backflow devices.
11. Show appropriate parking lot shade trees where occurs with min 30' canopy at maturity.
12. Call out type of proposed irrigation system and include preliminary MAWA calculation.
13. Show landscape hydrozones to separate low water from moderate water landscape.
14. Note that irrigation plans shall provide separate systems for trees with stream bubblers with pc screens.
15. Verify nursery availability of Rhamnus and Elaeagnus in large quantities prior to construction drawings
16. Replace short lived, high maintenance or poor performing plants: Miscanthus – change to

evergreen instead of deciduous grass to avoid bare spaced during winter, Do not use where screening is required along walls; Geranium- change to small shrub or groundcovers instead of perennial. For trees requiring good drainage (Cedrus) provide a detail with sumps and additional tree pit inspections to construction drawings. Provide min 12' clearance for large trees with low branches from walls or change to an upright canopy tree.

17. Note for agronomical soil testing and include report on landscape construction plans. For phased projects, a new report is required for each phase or a minimum of every 6 homes in residential developments. Note for contractor to provide additional soil reports to verify amendments were installed.
18. Call out all fences and walls, materials proposed and heights.
19. Show concrete mowstrips to identify property lines along open areas or to separate ownership or between maintenance areas (HOA, CFD or private property).
20. Residential projects shall include a stub-out for future back yard irrigation systems with anti-siphon valves.
21. Show minimum on-site tree sizes per the Landscape Development standards: 5% 48" box, 10% 36 box, 30% 24" box, 55% 15 gallon.
22. Show 25% of trees as California native (Platanus racemosa, Quercus agrifolia, Quercus wislizenii, Quercus douglasii, Cercis occidentalis, Sambucus Mexicana, etc.) in appropriate locations.
23. Show all proposed sign locations (on buildings and in landscape) to avoid conflicts with trees or shrubs and verify height of plant material and vertical clearance for sign text.
24. Landscape construction plans shall meet the requirements of the Landscape Development Guidelines. See http://www.ontarioca.gov/landscape-planning/standards
25. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council. Typical fees are:

Plan Check—5 or more acres	$2,326.00
Plan Check—less than 5 acres	$1,301.00
Inspection—Construction (up to 3 inspections)	$278.00
Inspection—Field - additional	$83.00

Electronic plan check sets may be sent to: landscapeplancheck@ontarioca.gov

CITY OF ONTARIO
MEMORANDUM

TO: PLANNING DEPARTMENT, Henry Noh

FROM: BUILDING DEPARTMENT, Kevin Shear

DATE: March 8, 2017

SUBJECT: PDEV17-009

☒ The plan **does** adequately address the departmental concerns at this time.

☐ No comments

☒ Report below.

Conditions of Approval

1. Standard Conditions of Approval apply.

KS:lm

AIRPORT LAND USE COMPATIBILITY PLANNING
CONSISTENCY DETERMINATION REPORT

ONTARIO AIRPORT PLANNING

Project File No.: PDEV17-009

Address: Southeast corner of Turner Ave & Ontario Ranch Road

APN: 218-241-37, 38, 39 & 40

Existing Land Use: Vacant/ Dairy Land

Proposed Land Use: 330 Single Family Homes, recreational center and 3 parks

Site Acreage: 47 Proposed Structure Height: 27 ft

ONT-IAC Project Review: N/A

Airport Influence Area: ONT

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Henry Noh

Date: 4/18/17

CD No.: 2017-020

PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
☐ Zone 1	☐ 75+ dB CNEL	☐ High Terrain Zone	☐ Avigation Easement Dedication
☐ Zone 1A	☐ 70 - 75 dB CNEL	☐ FAA Notification Surfaces	☐ Recorded Overflight Notification
☐ Zone 2	☐ 65 - 70 dB CNEL	☐ Airspace Obstruction Surfaces	☑ Real Estate Transaction Disclosure
☐ Zone 3	☐ 60 - 65 dB CNEL	☐ Airspace Avigation Easement Area	
☐ Zone 4			
☐ Zone 5		Allowable Height: 200 ft +	

The project is impacted by the following Chino ALUCP Safety Zones:

☐ Zone 1 ☐ Zone 2 ☐ Zone 3 ☐ Zone 4 ☐ Zone 5 ☐ Zone 6

Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: ☐ Exempt from the ALUCP ☐ Consistent ● Consistent with Conditions ☐ Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

See Attached.

Airport Planner Signature: Lorena Mejia

Form Updated: March 3, 2016

CD No.: 2017-020
PALU No.:

PROJECT CONDITIONS

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the Airport Land Use Compatibility Plan (ALUCP) for ONT. The applicant is required to meet the Real Estate Transaction Disclosure in accordance with California Codes (Business and Professions Code Section 11010-11024). New residential subdivisions within an Airport Influence Area are required to file an application for a Public Report consisting of a Notice of Intention (NOI) and a completed questionnaire with the Department of Real Estate and include the following language within the NOI:

NOTICE OF AIRPORT IN VICINITY
This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.

TOP-Zoning Consistency Determination

File No.: PDEV17-009

Location: SEC Turner Ave. & Ontario Ranch Rd.

Project Description:

A Development Plan for 330 residential lots with single-family and "cluster" single-family homes, including one recreation center and three parks, on an approximately 47-acre site generally located at the SEC of Turner Avenue and Ontario Ranch Road. Grand Park, Tract #18662.

Prepared By: Clarice Burden

Date: 4/12/17

Signature: Clarice Burden

This project has been reviewed for consistency with The Ontario Plan Zoning Consistency project. The following was found:

☑ The existing TOP land use designation of the property is: Low Density Residential
The existing zoning of the property is: Low Density Residential, Grand Park Specific Plan

☐ A change to the TOP land use designation has been proposed which would change the land use designation of the property to:

This proposed TOP land use change will:

☐ Make the existing zoning of the property consistent with the proposed General Plan Amendment;

☐ Make the proposed project consistent with The Ontario Plan.

☐ The zoning of the property will need to be changed in order to be consistent with The Ontario Plan. Through the TOP-Zoning Consistency effort, the zoning of the property is proposed to be changed to:

This proposed zone change will:

☐ Make the zoning of the property consistent with The Ontario Plan;

☐ Without the Zone Change described above, the proposed project is not consistent with The Ontario Plan. A finding of consistency with The Ontario Plan is required in order to approve this project.

☑ Additional Comments:

The overall Grand Park Specific Plan is consistent with the number of units allowed and analyzed by The Ontario Plan although technically the project site (Planning Areas 4, 5 & 6) does not fall in the density range of 2.1 to 5 dwelling units per acre for Low Density Residential per TOP. No changes are being requested at this time.

CITY OF ONTARIO
MEMORANDUM

TO: **Henry Noh, Senior Planner**
Planning Department

FROM: **Lora L. Gearhart, Fire Protection Analyst**
Fire Department

DATE: **March 21, 2017**

SUBJECT: **PDEV17-009 – A Development Plan for 330 residential lots with single-family and "cluster" single-family homes, including one recreation center and three parks, on an approximately 47-acre site generally located at the SEC of Turner Avenue and Ontario Ranch Road. Grand Park, Tract #18662.**

☒ The plan **does** adequately address Fire Department requirements at this time.

- ☐ No comments.
- ☒ Standard Conditions of Approval apply, as stated below.

SITE AND BUILDING FEATURES:

A. 2013 CBC Type of Construction: Type V-B wood frame

B. Type of Roof Materials: non-rated

C. Ground Floor Area(s): Various

D. Number of Stories: Two Story

E. Total Square Footage: Various

F. 2013 CBC Occupancy Classification(s): R-3, U

CONDITIONS OF APPROVAL:

1.0 GENERAL

☒ 1.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov, click on "Fire Department" and then on "Standards and Forms."

☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty (20) ft. wide. See Standard #B-004.

☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25') inside and forty-five feet (45') outside turning radius per Standard #B-005.

☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150') in length shall have an approved turn-around per Standard #B-002.

3.0 WATER SUPPLY

☒ 3.1 The required fire flow per Fire Department standards, based on the 2013 California Fire Code, Appendix B, is 1500 gallons per minute (g.p.m.) for 2 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.

☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.

☒ 3.4 The public water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 13 D. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with

detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

5.0 BUILDING CONSTRUCTION FEATURES

☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.

☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.

☒ 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.

☒ 5.5 All residential chimneys shall be equipped with an approved spark arrester meeting the requirements of the California Building Code.

CITY OF ONTARIO

MEMORANDUM

TO: "Vacant", Development Director
Scott Murphy, Planning Director (Copy of memo only)
Cathy Wahlstrom, Principal Planner (Copy of memo only)
Charity Hernandez, Economic Development
Kevin Shear, Building Official
Khoi Do, Assistant City Engineer
Carolyn Bell, Landscape Planning Division
Sheldon Yu, Municipal Utility Company
Doug Sorel, Police Department
Art Andres, Deputy Fire Chief/Fire Marshal
Tom Danna, T. E., Traffic/Transportation Manager
Lorena Mejia, Associate Planner, Airport Planning
Steve Wilson, Engineering/NPDES
Bob Gluck, Code Enforcement Director
Jimmy Chang , IT Department
David Simpson, Development/IT (Copy of memo only)

FROM: Henry Noh, Senior Planner

DATE: March 02, 2017

SUBJECT: FILE #: PDEV17-009 Finance Acct#:

The following project has been submitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by **Thursday, March 16, 2017**.

Note:
- ☐ Only DAB action is required
- ☑ Both DAB and Planning Commission actions are required
- ☐ Only Planning Commission action is required
- ☐ DAB, Planning Commission and City Council actions are required
- ☐ Only Zoning Administrator action is required

PROJECT DESCRIPTION: A Development Plan for 330 residential lots with single-family and "cluster" single-family homes, including one recreation center and three parks, on an approximately 47-acre site generally located at the SEC of Turner Avenue and Ontario Ranch Road. Grand Park, Tract #18662.

☒ The plan does adequately address the departmental concerns at this time.
- ☐ No comments
- ☐ Report attached (1 copy and email 1 copy)
- ☒ Standard Conditions of Approval apply

☐ The plan does not adequately address the departmental concerns.
- ☐ The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

Department	Signature	Title	Date
POLICE	DOUGLAS SOREL	MANAGEMENT ANALYST	4/3/17

PLANNING COMMISSION STAFF REPORT

July 25, 2017

SUBJECT: A Development Plan (File No. PDEV17-024) approval to construct 88 single-family residential dwellings on 14.35 acres of land located within the Conventional Medium Lot Residential district of Planning Area 4 of the Subarea 29 Specific Plan, located at the southeast corner of Archibald Avenue and Parkview Street. (APN: 0218-022-15); **submitted by CalAtlantic Group, Inc.**

PROPERTY OWNER: CalAtlantic Group, Inc.

RECOMMENDED ACTION: That the Planning Commission approve File No. PDEV17-024, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The project site is comprised of 14.35 acres of land located at the southeast corner of Archibald Avenue and Parkview Street, within the Conventional Medium Lot Residential district of Planning Area 4 of the Subarea 29 Specific Plan, and is depicted in ***Figure 1: Project Location***, below. The project site gently slopes from north to south and is currently mass graded. The property to the north of the project site is within the Conventional Medium Lot Residential district of Planning Area 3 of the Subarea 29 Specific Plan and is vacant. The property to the east of the project site is within the Conventional Small Lot Residential district of Planning Area 17 of the Subarea 29 Specific Plan and is vacant. The property to the south of the project site is within the Conventional Small Lot Residential district of Planning Area 5 of the Subarea 29 Specific Plan and is being developed with a single-family residential use. The property to the west of the project site is within the Conventional Small Lot Residential district of Planning

Figure 1: Project Location

Case Planner:	Henry K. Noh
Planning Director Approval:	
Submittal Date:	June 6, 2017
Hearing Deadline:	N/A

Hearing Body	*Date*	*Decision*	*Action*
DAB	July 17, 2017	Approved	Recommend
ZA			
PC	July 25, 2017		Final
CC			

Area 1 of the Subarea 29 Specific Plan and is vacant.

PROJECT ANALYSIS:

[1] Background — The Subarea 29 Specific Plan and Environmental Impact Report (EIR) were approved by the City Council on October 17, 2006. The Specific Plan established the land use designations, development standards, and design guidelines for approximately 540 gross acres of land, which included the potential development of 2,293 single-family units and 87,000 square feet of commercial. The Specific Plan is comprised of twenty-five (25) land use districts incorporating twelve (12) distinctive neighborhoods, offering a variety of residential products.

On August 19, 2013, the Planning Commission approved Tentative Tract Map 18913 ("A" Map). The approved "A" Map facilitated the backbone infrastructure improvements (major streets, sewer, water and storm drain facilities) along Archibald Avenue and Merrill Avenue and the construction of Celebration Park, a clubhouse/recreational center, and residential neighborhoods within the southern portion of the Specific Plan area. Additionally, the previously approved Tentative Tract Map 18266 ("B" Map) subdivided 14.35 acres of land into 88 single-family lots and 8 lettered lots to accommodate a single-family conventional product and facilitated the construction of the backbone streets, including the primary access points into the proposed community from Parkview Street and Park Place Avenue, as well as the construction of all the interior neighborhood streets within the subdivision (see ***Exhibit A: Site Plan***). The applicant is now requesting Development Plan approval for construction of 88 single-family dwelling units.

The Applicant, CalAtlantic Group, Inc., has submitted a Development Plan (File No. PDEV17-024) to construct the single-family conventional residential dwelling units (see ***Exhibit B: Typical Plotting and Conceptual Landscaping***) on land located within the Conventional Medium Lot Residential district of Planning Area 4 of the Subarea 29 Specific Plan (see ***Figure 2: Subarea 29 Specific Plan Land Use Plan,*** below), located at the southeast corner of Archibald Avenue and Parkview Street.

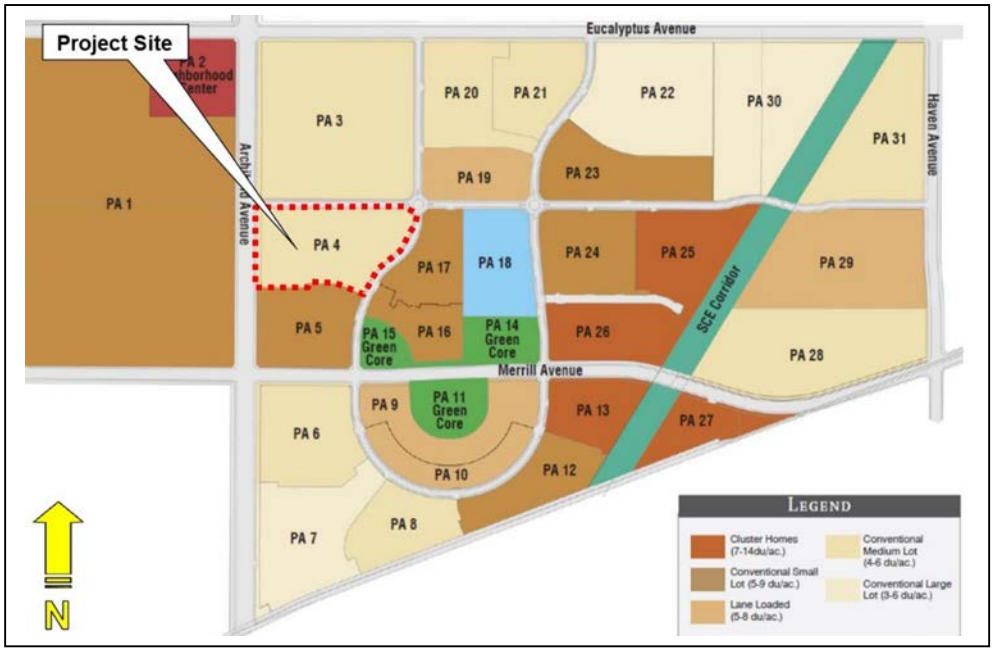

Figure 2: Subarea 29 Specific Plan Land Use Plan

[2] Site Design/Building Layout —The project includes three floor plans and three architectural styles per plan. The three floor plans include the following:

- Plan 1: 2,718 square feet, 5 bedrooms (options for Loft and Office) and 3 baths.
- Plan 2: 2,900 square feet, 5 bedrooms (option for Office) and 3 baths.
- Plan 3: 3,086 square feet, 5 bedrooms (option for Office) and 3 baths.

The proposed Development Plan has been designed to create architecture that reflects quality in design, simplicity in form and contributes charm and appeal to the neighborhoods within the Subarea 29 Specific Plan as a whole. All plans incorporate various design features, such as single and second-story massing, varied entries, front porches, outdoor California rooms, 2nd floor laundry facilities and a great room. In addition, each home will provide a two or three-car garage and standard driveway. Thirty-four percent of the homes will feature a garage forward design, while sixty-six percent of the homes will have a shallow-recessed garage design. To minimize visual impacts of garages, second story projections above garages, varied first and second story roof massing and door header trim above garage are proposed on all elevation.

[3] Site Access/Circulation — The previously approved Tract Maps 18913 (“A” Map) and 18266 (“B” Map), facilitated the construction of the backbone streets including the primary access points into the central portion of the Subarea 29 (Park Place) community from Archibald Avenue and Merrill Avenue, as well as the construction of all the interior neighborhood streets within the subdivision. Primary access into the subdivision will be from Archibald Avenue and Parkview Street.

[4] Parking — The proposed conventional single-family homes will provide either a two or three-car garage and a standard two-car driveway, which meets the Subarea 29 Specific Plan requirements.

[5] Landscaping/Open Space — The Development Plan features landscaped parkways to soften the massing of the garages, provide visual interest and promotes pedestrian mobility (see ***Exhibits B: Typical Plotting and Conceptual Landscaping***).

The related Tentative Tract Map TT18266 facilitated the construction of sidewalks, parkways, and open space areas within the project site. TOP Policy PR1-1 requires new developments to provide a minimum of 2 acres of private park per 1,000 residents. The proposed project is required to provide a 0.67 acre park to meet the minimum TOP private park requirement. To satisfy the park requirement, the applicant is proposing a 0.60-acre neighborhood park that is located within the northwest portion of the project site. However, to satisfy the private park requirements of the Policy Plan, the master developer (SL Ontario Development Company, LLC) was required by the Development Agreement (PDA06-001) to construct a total of 8 acres of private parks within the Park Place community (Phases 1, 2 & 3). Through the various tentative tract map approvals within Phases 1, 2 and 3 of the Park Place community, the applicant has provided 8.16 acres of private parks, which satisfies the Policy Plan private park requirements. Additionally, the master developer has constructed a 2.78-acre private recreation facility, consisting of a 16,000 square foot clubhouse. The recreation facility is located at the northeast corner of Park Place Avenue and Merrill Avenue and features a clubhouse, pool and cabana, tennis courts and playground area. The residents of the subdivision will also have access to Celebration Park.

[6] Architecture —The architectural philosophy of the Subarea 29 Specific Plan is based on architectural styles found in Ontario's historic neighborhoods. The inspiration and design intent is to re-capture the charm and essence of the historic home styles in Ontario and express them in the simple, honest manner. The proposed architectural styles include Spanish Colonial, Craftsman and Cottage. The styles were chosen to complement one another through the overall scale, massing, proportions, details and the ability to establish an attractive backdrop that will age gracefully over time.

Each architectural style will include the following details (**See *Exhibit C – Floor Plans and Elevations*)**:

Spanish Colonial: Varying gable and hipped roofs with "S" type roof tiles, stucco exterior, square and arched windows openings, wrought-iron pot shelves, cantilevered elements with corbels and decorative vents below gables.

Plan 3: Spanish Colonial

Craftsman: Varying gable and cross gable roofs with flat concrete roof tiles, wood outlookers below gables, exposed rafter tails, vertical siding below gable ends, stucco and shingle siding, cantilevered elements with corbels, covered porches with simple tapered columns, and decorative window framing.

Plan 1: Craftsman

Cottage: Varying gable and cross gable roofs with flat concrete roof tiles, a high pitched roof with a sloped roof treatment, horizontal siding and corbels under gable ends, stucco exterior, arched covered entries, cantilevered elements with corbels; first floor bay windows, deep set windows with wood shutters.

Plan 2: Cottage

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)
- Ensure the Development of a Well Planned, Balanced, and Self-Sustaining Community in the New Model Colony

[2] Vision.

Distinctive Development:

- Commercial and Residential Development

➢ Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

[3] Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.

  - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

[4] Policy Plan (General Plan)

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.

  - LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.

  - LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).

- Goal LU2: Compatibility between a wide range of uses.

  - LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

Housing Element:

- Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

  - H2-4 New Model Colony. We support a premier lifestyle community in the New Model Colony distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.

➢ H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.

Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.

Community Economics Element:

- Goal CE1: A complete community that provides for all incomes and stages of life.

➢ CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

➢ CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

➢ CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

➢ CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

➢ CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.

➢ S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

- Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

  - CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

  - CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

  - CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

- Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

  - CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

    - Building volume, massing, and height to provide appropriate scale and proportion;
    - A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
    - Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

  - CD2-2 Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

    - A pattern of smaller, walkable blocks that promote access, activity and safety;
    - Variable setbacks and parcel sizes to accommodate a diversity of housing types;
    - Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
    - Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and

- Landscaped parkways, with sidewalks separated from the curb.

➢ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➢ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➢ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➢ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➢ CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

➢ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

- Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

➢ CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

- Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

- CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.

- CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project is consistent with the number of dwelling units (88) and density (6.13 DU/AC) specified in the Available Land Inventory.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The project site is located within the Airport Influence Area of the Ontario International Airport (ONT), and has been found to be consistent with the policies and criteria set forth within the ALUCP for ONT.

ENVIRONMENTAL REVIEW: The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA14-002, an Amendment to the Subarea 29 Specific Plan for which an addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009) was adopted by the City Council on April 21, 2015. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures shall be a condition of project approval and are incorporated herein by reference.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	Existing Land Use	*General Plan Designation*	*Zoning Designation*	*Specific Plan Land Use*
Site	Mass Graded	Low Density Residential	Subarea 29 Specific Plan	Planning Area 4 (Conventional Medium Lot)
North	Vacant with Previous Agricultural/Dairy Uses	Low Density Residential	Subarea 29 Specific Plan	Planning Area 3 (Conventional Medium Lot)
South	Single-Family Residential	Low Density Residential	Subarea 29 Specific Plan	Planning Area 5 (Conventional Small Lot)
East	Vacant with Previous Agricultural/Dairy Uses	Low Density Residential	Subarea 29 Specific Plan	Planning Area 17 (Conventional Small Lot)
West	Vacant with Previous Agricultural/Dairy Uses	Low Density Residential	Subarea 29 Specific Plan	Planning Area 1 (Conventional Small Lot)

General Site & Building Statistics

Item	*Required Min./Max.*	*Provided (Ranges)*	*Meets Y/N*
Project area (in acres):	N/A	14.35	Y
Maximum project density (dwelling units/ac):	8.7 DU/AC	6.13 DU/AC	Y
Maximum coverage (in %):	50%	45%	Y
Front yard setback (in FT):	12’	12’	Y
Side yard setback (in FT):	5’	5’	Y
Rear yard setback (in FT):	15’	15’	Y
Maximum dwelling units/building:	88 DU	88 DU	Y
Maximum height (in FT):	35’	30’	Y

Exhibit A: Site Plan

Exhibit B: Typical Plotting and Conceptual Landscaping

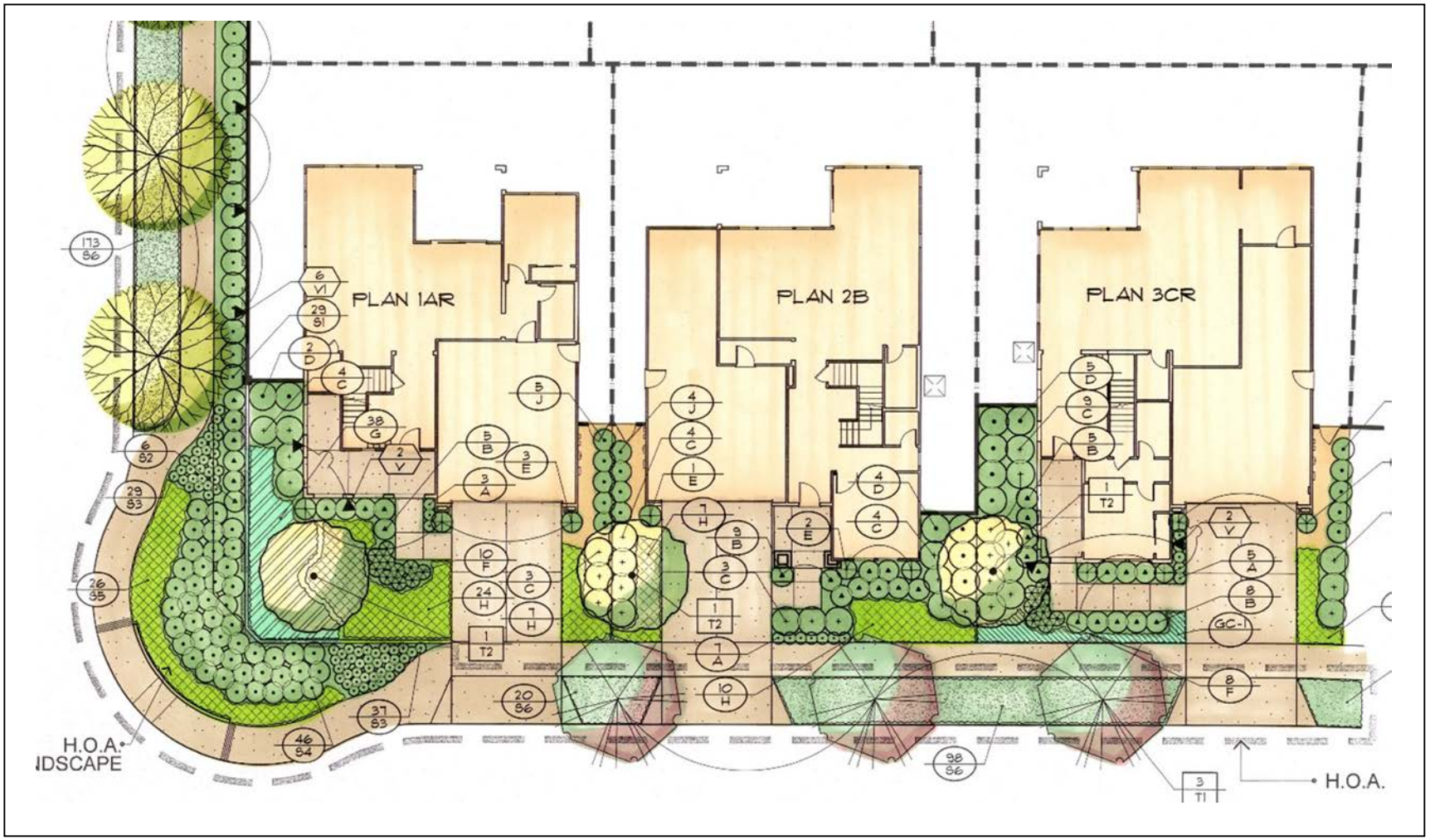

Exhibit C: Floor Plans and Elevations – Plan 1

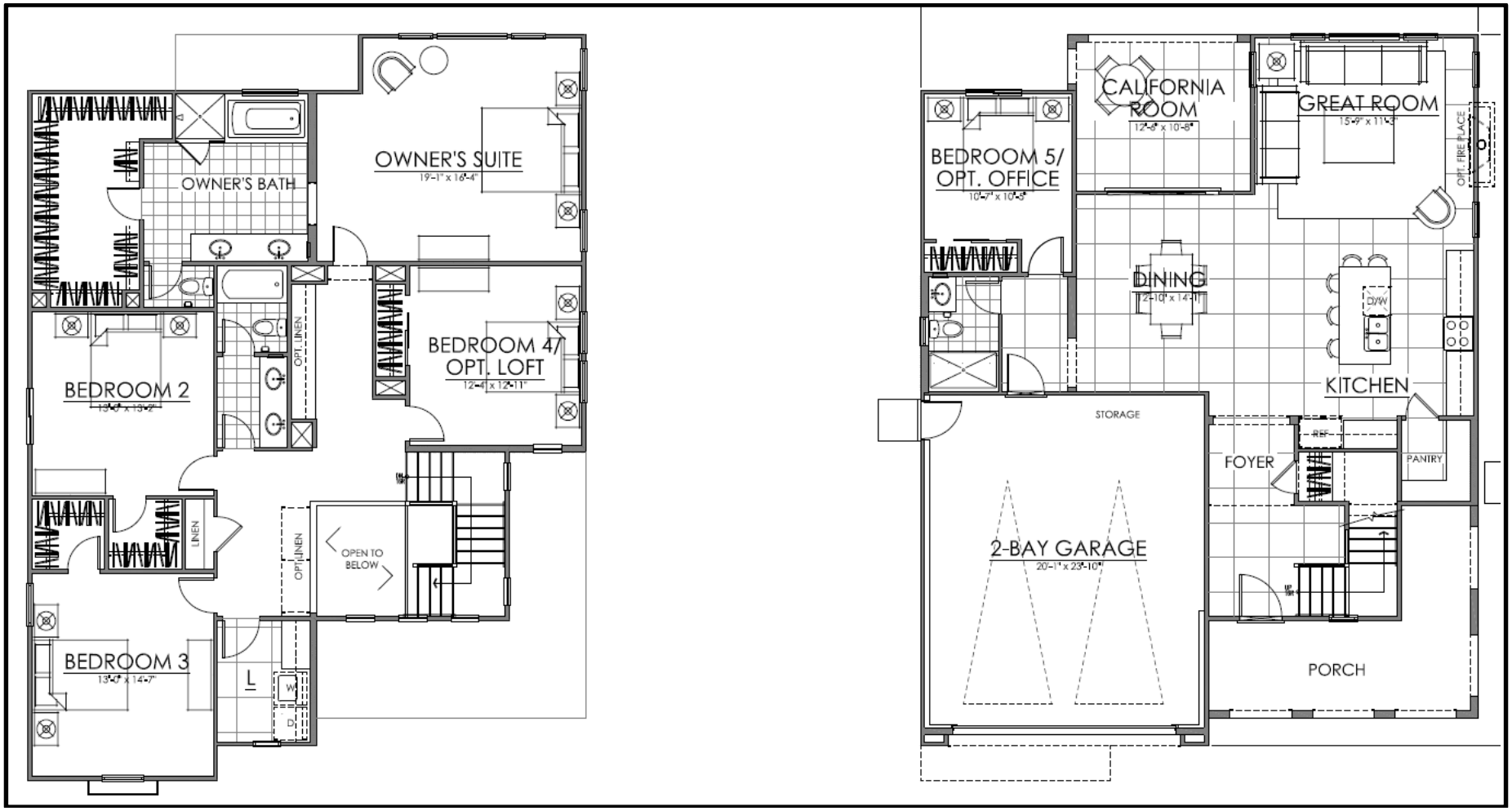

Exhibit C: Floor Plans and Elevations – Plan 1

Exhibit C: Floor Plans and Elevations – Plan 2

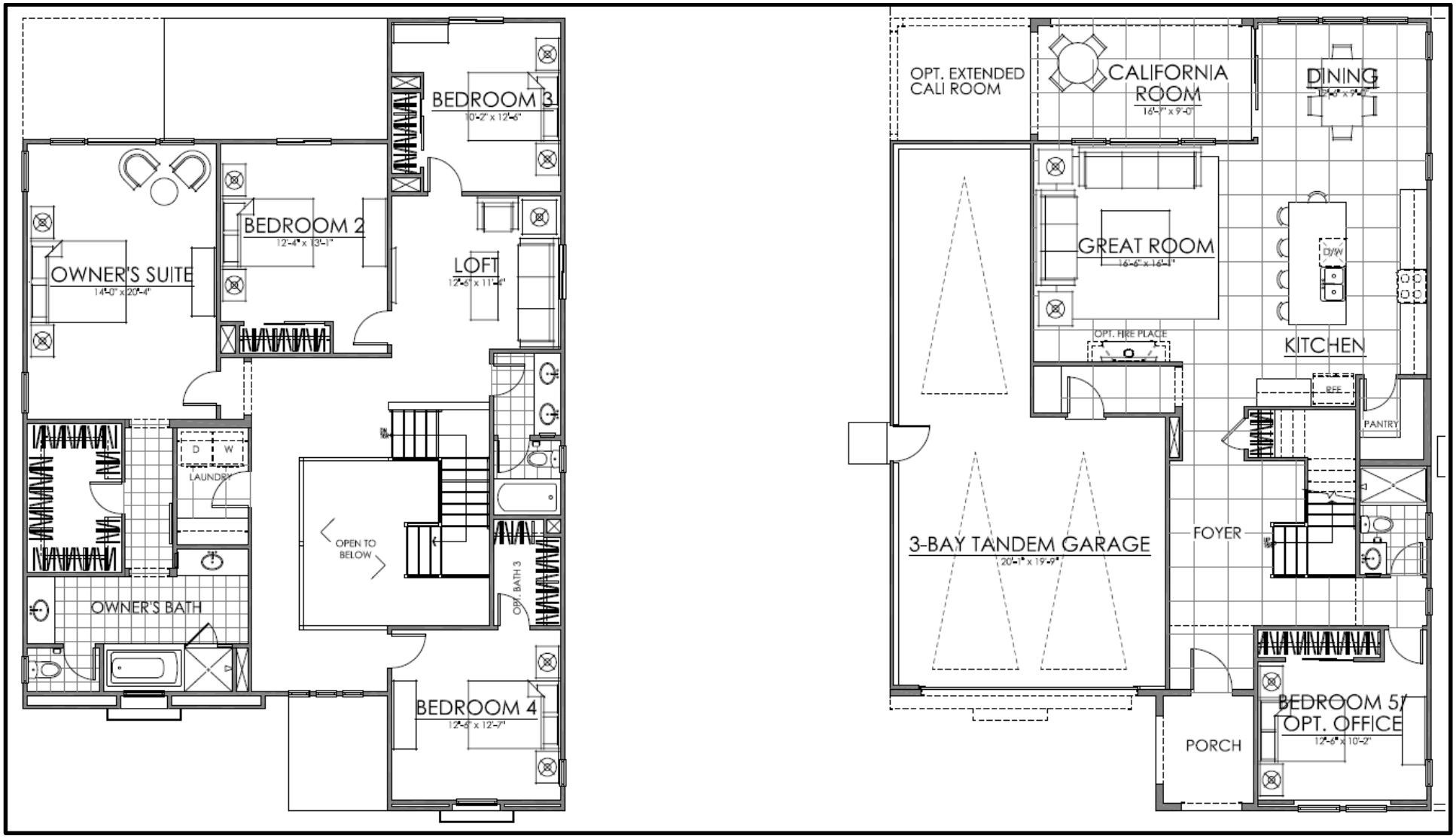

Exhibit C: Floor Plans and Elevations – Plan 2

MATERIAL NOTES
Concrete low profile roof tile
4 x 6 exposed rafter tails
6 x 6 decorative tile
Foam trim @ windows & doors
Decorative wrought iron

ELEVATION A - SPANISH
SCALE: 1/4" = 1'-0"
scheme 2

MATERIAL NOTES
Concrete flat roof tile
4" x 6" exposed rafter tails
8" vertical siding
Shingle siding
Wood trim @ siding
Foam trim @ stucco
Tapered wood column
Stone veneer

ELEVATION B - CRAFTSMAN

MATERIAL NOTES
Concrete flat roof tile
Stucco
Lap siding
Foam trim
Hardiboard wainscot
Metal roof
Decorative shutters

ELEVATION C - COTTAGE
SCALE: 1/4" = 1'-0"
scheme 8

05.26.17

PLAN TWO

woodley architect group,

Exhibit C: Floor Plans and Elevations – Plan 3

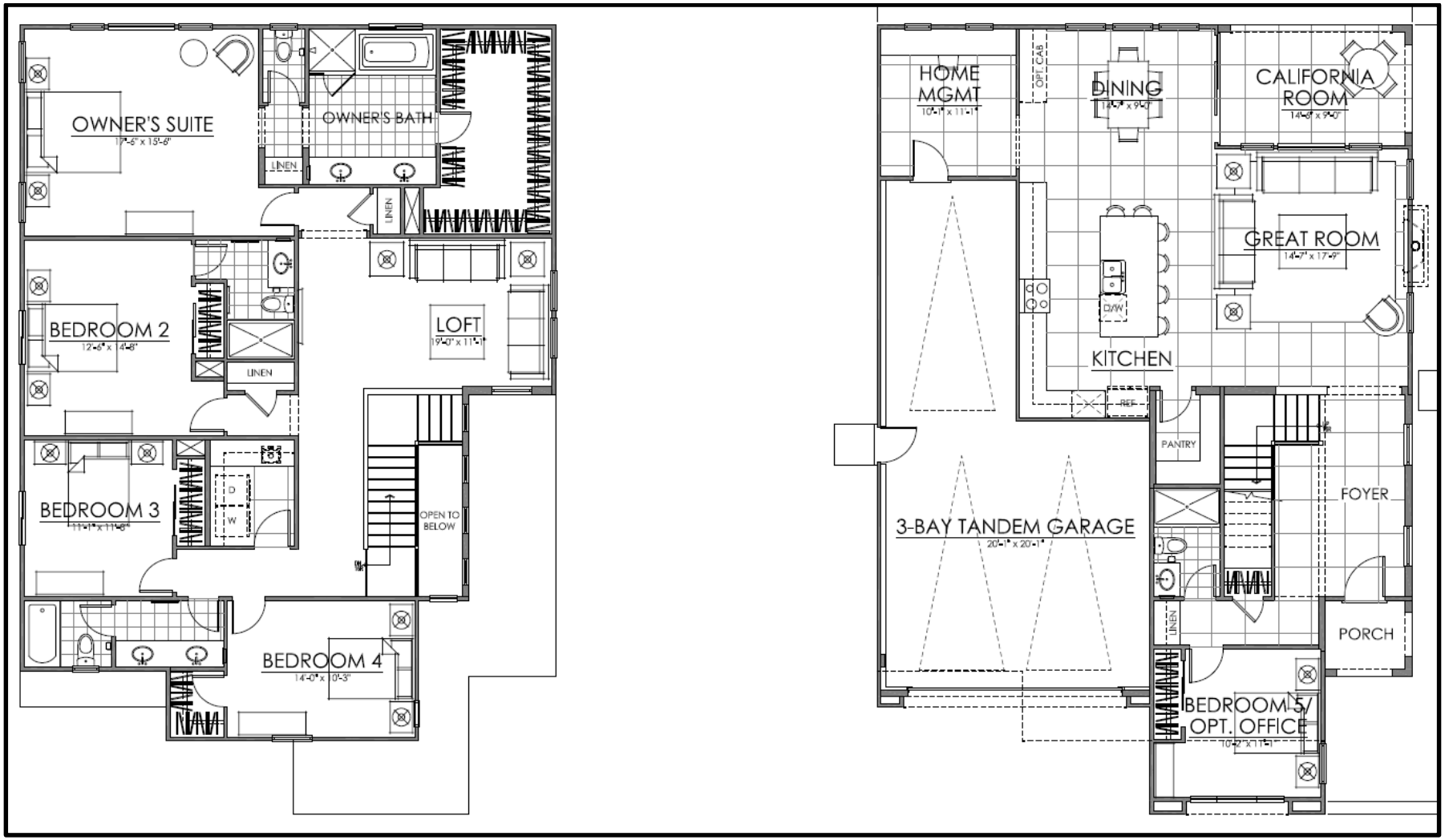

Exhibit C: Floor Plans and Elevations – Plan 3

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV17-024, A DEVELOPMENT PLAN APPROVAL TO CONSTRUCT 88 SINGLE-FAMILY RESIDENTIAL DWELLINGS ON 14.35 ACRES OF LAND LOCATED WITHIN THE CONVENTIONAL MEDIUM LOT RESIDENTIAL DISTRICT OF PLANNING AREA 4 OF THE SUBAREA 29 SPECIFIC PLAN, LOCATED AT THE SOUTHEAST CORNER OF ARCHIBALD AVENUE AND PARKVIEW STREET, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 0218-022-15.

WHEREAS, CalAtlantic Group, Inc. ("Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV17-024, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 14.35 acres of land generally located at the southeast corner of Archibald Avenue and Parkview Street, within the Conventional Medium Lot Residential district of Planning Area 4 of the Subarea 29 Specific Plan, and is presently mass graded; and

WHEREAS, the property to the north of the project site is within the Conventional Medium Lot Residential district of Planning Area 3 of the Subarea 29 Specific Plan and is vacant. The property to the east of the project site is within the Conventional Small Lot Residential district of Planning Area 17 of the Subarea 29 Specific Plan and is vacant. The property to the south of the project site is within the Conventional Small Lot Residential district of Planning Area 5 of the Subarea 29 Specific Plan and is being developed with a single-family residential use. The property to the west of the project site is within the Conventional Small Lot Residential district of Planning Area 1 of the Subarea 29 Specific Plan and is vacant; and

WHEREAS, the Development Plan proposed is in compliance with the requirements of the Subarea 29 Specific Plan and is sufficient in size to facilitate and implement the traditional planning concepts for the “Residential Neighborhood” within the Specific Plan; and

WHEREAS, the proposed Development Plan is located within Planning Area 4 (Conventional Medium Lot Residential Product Type) land use district of the Subarea 29 Specific Plan, which establishes a minimum lot size of 4,000 square feet and a development capacity of 88 dwelling units; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA"); and

WHEREAS, the environmental impacts of this project were previously reviewed in conjunction with File No. PSPA14-002, an Amendment to the Subarea 29 Specific Plan for which an addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009) was adopted by the City Council on April 21, 2015, and this Application introduces no new significant environmental impacts; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to CEQA (Public Resources Code Section 21000 et seq.), and an initial study has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (“ALUCP”), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on July 28, 2015, the Planning Commission of the City of Ontario conducted a hearing and approved the related Tentative Tract Map File No. PMTT14-010 (TT18266); and

WHEREAS, on July 17, 2017, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project and concluded said hearing on that date, voting to issue Decision No. DAB17-036 recommending the Planning Commission approve the Application; and

WHEREAS, on July 25, 2017, the Planning Commission of the City of Ontario conducted a hearing to consider the Project and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: ***Environmental Determination and Findings.*** As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the previous addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009) and supporting documentation. Based upon the facts and information contained in the previous addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009) and supporting documentation, the Planning Commission finds as follows:

(1) The environmental impacts of this project were reviewed in conjunction with an Addendum to Subarea 29 Specific Plan EIR (SCH# 2004011009) Environmental Impact Report, certified by the City of Ontario City Council on April 21, 2015, in conjunction with File No. PSPA14-002; and

(2) The previous addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009) contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009) was completed in compliance with CEQA and the Guidelines promulgated thereunder; and

(4) The previous addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009) reflects the independent judgment of the Planning Commission; and

(5) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the previous addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009), and all mitigation measures previously adopted with the addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009), are incorporated herein by this reference.

SECTION 2: ***Subsequent or Supplemental Environmental Review Not Required.*** Based on the information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009) is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009) that will require major revisions to the addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009) due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009) was prepared, that will require major revisions to the addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009) due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009) was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009); or

(b) Significant effects previously examined will be substantially more severe than shown in the addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009); or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009) would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: ***Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on

the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project is consistent with the number of dwelling units (88) and density (6.13 DU/AC) specified in the Available Land Inventory.

SECTION 4: ***Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport ("ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the Planning Commission, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 5: ***Concluding Facts and Reasons.*** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the Low Density Residential land use district of the Policy Plan Land Use Map, and the Conventional Medium Lot Residential (Planning Area 4) land use district of the Subarea 29 Specific Plan. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. The Development Plan has been required to

comply with all provisions of Conventional Medium Lot Residential Product: Village Homes Residential Development Standards of the Subarea 29 Specific Plan. Future neighborhoods within the Subarea 29 Specific Plan and surrounding area will provide for diverse housing and highly amenitized neighborhoods that will be compatible in design, scale and massing to the proposed development.

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the Conventional Medium Lot Residential (Planning Area 4) land use district of the Subarea 29 Specific Plan, including standards relative to the particular land use proposed (single-family residential), as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions. The Project has been designed consistent with the requirements of the Subarea 29 Specific Plan (Conventional Medium Lot Residential Product: Village Homes) land use designations, including standards relative to the particular land use proposed (conventional single-family residential product), as well as building intensity, building and parking setbacks, building height, number of off-street parking spaces, on-site and off-site landscaping, and fences, walls and obstructions.

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Subarea 29 Specific Plan are maintained; [ii] the project will not endanger the public health, safety or general welfare; [iii] the project will not result in any significant environmental impacts; [iv] the project will be in harmony with the area in which it is located; and [v] the project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan, and the Subarea 29 Specific Plan. Additionally, the environmental impacts of this project were previously reviewed in conjunction with the Subarea 29 Specific Plan Environmental Impact Report (SCH#2004011009). This application is consistent with the previously adopted EIR and introduces no new significant environmental impacts.

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development*****.** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Subarea 29 Specific Plan that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking, design and

landscaping, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (conventional single-family residential). As a result of this review, the Development Advisory Board has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the Subarea 29 Specific Plan. Additionally, the Development Plan complies with all provisions of Conventional Medium Lot Residential Product: Village Homes Residential Development Standards of the Subarea 29 Specific Plan.

SECTION 6: ***Planning Commission Action.*** Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 7: ***Indemnification.*** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: ***Custodian of Records.*** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: ***Certification to Adoption.*** The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of July 2017, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Richard D. Delman
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Principal Planner / Acting Secretary of Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Marci Callejo, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. PC17-[insert #] was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 25, 2017, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Marci Callejo
Secretary Pro Tempore

ATTACHMENT A:

File No. PDEV17-024
Departmental Conditions of Approval

(Departmental conditions of approval to follow this page)

City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department
Land Development Division
Conditions of Approval

Meeting Date: July 17, 2017

File No: PDEV17-024

Related Files: PMTT14-010

Project Description: A Development Plan (File No. PDEV17-024) approval to construct 88 single-family residential dwellings on 14.35 acres of land located within the Conventional Medium Lot Residential district of Planning Area 4 of the Subarea 29 Specific Plan, located at the southeast corner of Archibald Avenue and Parkview Street. (APN: 0218-022-15); **submitted by CalAtlantic Group, Inc.**

Prepared By: Henry K. Noh, Senior Planner
Phone: 909.395.2429 (direct)
Email: hnoh@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2010. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(c) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

2.6 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.7 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.8 Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements.

(a) CC&Rs shall be prepared for the Project and shall be recorded prior to the issuance of a building permit.

(b) The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owners association and the CC&Rs shall be reviewed and approved by the City.

(c) CC&Rs shall ensure reciprocal parking and access between parcels.

(d) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:

(i) Landscaping and irrigation systems within common areas;
(ii) Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02;
(iii) Shared parking facilities and access drives; and
(iv) Utility and drainage easements.

(e) CC&Rs shall include authorization for the City's local law enforcement officers to enforce City and State traffic and penal codes within the project area.

(f) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.

(g) A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

2.9 Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.
(ii) Some of the property adjacent to this tract is zoned for agricultural uses and there could be fly, odor, or related problems due to the proximity of animals.
(iii) The area south of Riverside Drive lies within the San Bernardino County Agricultural Preserve. Dairies currently existing in that area are likely to remain for the foreseeable future.
(iv) This tract is part of a Landscape Maintenance District. The homeowner(s) will be assessed through their property taxes for the continuing maintenance of the district.

2.10 Environmental Review.

(a) The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA14-002, a(n) Amendment to the Subarea 29 Specific Plan for which a(n) addendum to the Subarea 29 Specific Plan EIR (SCH# 2004011009) was adopted by the City Council on April 21, 2015. This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. The previously adopted mitigation measures shall be a condition of project approval, and are incorporated herein by this reference.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.11 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.12 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.13 Additional Requirements.

(a) Off-Site Subdivision Signs.

The City Council has authorized the Baldy View Chapter of the Building Industry Association to manage a standardized off-site directional sign program on a non-profit basis. The program uses uniform sign structures and individual identification and directional signs for residential development. **No other off-site signing is authorized.** (For additional information, contact the Baldy View Chapter BIA at (909) 945-1884.

(b) The applicant shall contact the Ontario Post Office to determine the size and location of mailboxes for this project. The location of the mailboxes shall be submitted to the Planning Department for review and approval prior to the issuance of building permits.

(c) The applicant (Developer) shall be responsible for providing fiber to each home per City requirements and standards.

(d) Dairy Separation Requirement for Residential Development.

The following separation requirements from existing dairies/feed lots shall apply to new residential development or structures used for public assembly purposes from existing dairies/feed lots.

A minimum 100' separation shall be required between a new residential, commercial or industrial development or structure used for public assembly and an existing animal feed trough, corral/pen or an existing dairy/feed lot including manure stockpiles and related wastewater detention basins. The 100-foot separation requirement may be satisfied by an off-site easement acceptable to the

Planning Director with adjacent properties, submitted with the initial final map and recorded prior to or concurrent with the final map.

(e) All applicable conditions of approval of Development Agreement (File No. PDA13-003) shall apply to this tract.

(f) All applicable conditions of approval of the Subarea 29 Specific Plan shall apply to this tract.

(g) All applicable conditions of approval of the "B" Map TT 18266 (File No. PMTT14-010) shall apply to this Development Plan.

(h) The Private Park (Lot A) shall be constructed prior to the issuance of the certificate of occupancy of the 44th home.

CITY OF ONTARIO
MEMORANDUM

ENGINEERING DEPARTMENT
CONDITIONS OF APPROVAL

(Traffic/Transportation Division and Municipal Utilities Agency, and Environmental Section Conditions incorporated)

DATE: June 8, 2017

PROJECT PLANNER: Henry Noh, Senior Planner

PROJECT: PDEV17-024 – A Development Plan to construct 88 single family homes on 14.35 acres within Planning Area 4 of the Subarea 29 Specific Plan (Reference File: TM 18266-PMTT14-010)

APN: 0218-022-15

LOCATION: SEC of Archibald Avenue and Parkview Street

PROJECT ENGINEER: Manoj Hariya, P.E, Sr. Associate Civil Engineer

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The following items are the Conditions of Approval for the subject project:

1. The applicant/developer shall be responsible to complete all applicable conditions as specified in the Conditions of Approval for TM18266, TM18913-2 and Development Agreement between SL Ontario Development Company LLC and City of Ontario for Subarea 29 (Park Place) Specific Plan.

2. The applicant/developer shall provide fiber optic connection to each home per City Standards and guidelines.

3. Prior to Building Permits: Any changes to the already approved Engineering Report (ER), including landscaping plans, due to the proposed re-lotting, shall be amended with City and State. Please coordinate with Cynthia Heredia-Torres 909-395-2647, ctorres@ontarioca.gov) to confirm immediately.

M.B.Hariya 06/12/17

Manoj Hariya , P.E. Date
Senior Associate Civil Engineer

6/12/17

Khoi Do, P.E. Date
Assistant City Engineer

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
303 East "B" Street, Ontario, CA 91764

CONDITIONS OF APPROVAL

Sign Off

Carolyn Bell, Sr. Landscape Planner	6/22/17 Date

eviewer's Name: **Carolyn Bell, Sr. Landscape Planner**

Phone: **(909) 395-2237**

D.A.B. File No.: PDEV17-024

Case Planner: Henry Noh

Project Name and Location:
Park Place Camden II TM 18266
SEC Archibald and Park View St

Applicant/Representative:
Frank Radmacher Assoc. Landscape Architects
14841 4841 Yorba St ste 204
Tustin, CA 92780

☒	**A Preliminary Landscape Plan (dated 5/25/17) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.**
☐	**A Preliminary Landscape Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.**

CORRECTIONS REQUIRED

Civil Plans

1. Locate light standards, fire hydrants, water and sewer lines to not conflict with required tree locations 30' oc and 25' from BCR. Relocate utilities to the minimum clearances. Lot 68, 52, 55, 19-20, 4-5.
2. Note for compaction to be no greater than 85% at landscape areas. All finished grades at 1 ½" below finished surfaces. Slopes to be maximum 3:1.
3. Typical lot drainage shall include a catch basin with gravel sump below each before exiting property, for nuisance water.

Landscape Plans

4. Show all utilities on the landscape plans. Coordinate with civil to keep utilities clear of required tree locations 30' oc and 25' from BCR.
5. Change street trees: Rosemary way – Pistache; Agrarian Street - Magnolia 'Russet'; Millstone Lane – Ulmus 'Drake'; Bountiful Trail – Fraxinus 'Raywood'.
6. Add large corner accent trees to the HOA area instead of homeowners front yard
7. Call out type of proposed irrigation system (drip and pop up tree bubblers) and include preliminary MAWA calculation.
8. Provide a planting list of proposed water efficient plants. Provide separate plant palettes for low water and moderate water hydrozones and sun or shade locations (south and west facing; north or east facing) Can show this in wucols column – ex. shrub A-1 low water sun, A-2 low water shade A-3 medium water shade. Avoid medium water plants in sun area - will not survive well.
9. Replace invasive, high water using, short lived, high maintenance or poor performing plants: Liriope – not for hot dry locations change to sesleria autumnalis; Tulbaghia -offensive odor; Eriobotrya - disease prone, Baccharis -too large and unkempt- change to Fragaria, Erigeron- dies out in heat, invasive if moist – change to Kurapia; Escallonia – not for dry heat. Avoid plants that are too large or require frequent maintenance or hedging.
10. Note for agronomical soil testing and include report on landscape plans. For phased projects, a new report is required for each phase or a minimum of every 7 homes in residential developments. Note contractor to provide testing and reports after amendments installed to verify.
11. Residential projects shall include a stub-out for future back yard irrigation systems with anti-siphon

valves.

12. Show 25% of trees as California native (Platanus racemosa, Quercus agrifolia, Quercus wislizenii, Quercus douglasii, Cercis occidentalis, etc.) in appropriate locations.
13. **Show a min 6' diameter mulch area at each tree**
14. Correct typical plan 1C that shows the AC unit in the front yard instead of behind the fence.
15. L-4 correct text not printing. Change synthetic turf **to sod or a low water groundcover. Show a 5' diameter mulch area at trees. Change pea gravel to max 12" at building wall and shredded bark** mulch. Show concrete mowstrips to separate private property and HOA maintenance areas.
16. W-1 detail E change the drain slot detail to remove the #4 rebar and show a mteal lintel.
17. Landscape construction plans shall meet the requirements of the Landscape Development Guidelines. See http://www.ontarioca.gov/landscape-planning/standards
18. **After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan** check and inspections at a rate established by resolution of the City Council. Typical fees are:

Plan Check—5 or more acres	$2,326.00
Plan Check—less than 5 acres	$1,301.00
Inspection—Construction (up to 3 inspections)	$278.00
Inspection—Field - additional	$83.00

Landscape construction plans with building permit number for plan check may be emailed to: landscapeplancheck@ontarioca.gov

CITY OF ONTARIO
MEMORANDUM

TO: **Henry Noh, Senior Planner**
Planning Department

FROM: **Lora L. Gearhart, Fire Protection Analyst**
Fire Department

DATE: **June 29, 2017**

SUBJECT: **PDEV17-024 – A Development Plan approval to construct 88 single-family dwellings on approximately 14.35 acres of land located at the southeast corner of Archibald Ave and Parkview St within the Planning Area 4 land use district of the Subarea 29 Specific Plan (APN: 0218-022-15). Related File(s): PMTT14-010.**

☒ The plan **does** adequately address Fire Department requirements at this time.

- ☐ No comments.
- ☒ Standard Conditions of Approval apply, as stated below.

SITE AND BUILDING FEATURES:

A. 2013 CBC Type of Construction: Type V-B wood frame

B. Type of Roof Materials: non-rated

C. Ground Floor Area(s): Various

D. Number of Stories: Two Story

E. Total Square Footage: Various

F. 2013 CBC Occupancy Classification(s): R-3, U

CONDITIONS OF APPROVAL:

1.0 GENERAL

☒ 1.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov, click on "Fire Department" and then on "Standards and Forms."

☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty (20) ft. wide. See Standard #B-004.

☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25') inside and forty-five feet (45') outside turning radius per Standard #B-005.

☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150') in length shall have an approved turn-around per Standard #B-002.

3.0 WATER SUPPLY

☒ 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 1500 gallons per minute (g.p.m.) for 2 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.

☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.

☒ 3.4 The public water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 13 D. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with

detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

5.0 BUILDING CONSTRUCTION FEATURES

☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.

☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.

☒ 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.

☒ 5.5 All residential chimneys shall be equipped with an approved spark arrester meeting the requirements of the California Building Code.

CITY OF ONTARIO

MEMORANDUM

TO: "Vacant", Development Director
Scott Murphy, Planning Director (Copy of memo only)
Cathy Wahlstrom, Principal Planner (Copy of memo only)
Charity Hernandez, Economic Development
Kevin Shear, Building Official
Khoi Do, Assistant City Engineer
Carolyn Bell, Landscape Planning Division
Sheldon Yu, Municipal Utility Company
Doug Sorel, Police Department
Art Andres, Deputy Fire Chief/Fire Marshal
Tom Danna, T. E., Traffic/Transportation Manager
Lorena Mejia, Associate Planner, Airport Planning
Steve Wilson, Engineering/NPDES
Bob Gluck, Code Enforcement Director
Jimmy Chang , IT Department
David Simpson, Development/IT (Copy of memo only)

FROM: Henry Noh, Senior Planner

DATE: June 06, 2017

SUBJECT: FILE #: PDEV17-024 Finance Acct#:

The following project has been submitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by **Tuesday, June 20, 2017**.

Note:
- ☐ Only DAB action is required
- ☑ Both DAB and Planning Commission actions are required
- ☐ Only Planning Commission action is required
- ☐ DAB, Planning Commission and City Council actions are required
- ☐ Only Zoning Administrator action is required

29'

PROJECT DESCRIPTION: A Development Plan approval to construct 88 single-family dwellings on approximately 14.35 acres of land located at the southeast corner of Archibald Ave and Parkview St within the Planning Area 4 land use district of the Subarea 29 Specific Plan (APN: 0218-022-15). Related File(s): PMTT14-010.

☑ The plan does adequately address the departmental concerns at this time.
- ☐ No comments
- ☑ Report attached (1 copy and email 1 copy)
- ☐ Standard Conditions of Approval apply

☐ The plan does not adequately address the departmental concerns.
- ☐ The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

Airport + Planning	L Mejia	Senior Planner	7/7/16
Department	Signature	Title	Date

CD 2017-039

AIRPORT LAND USE COMPATIBILITY PLANNING
CONSISTENCY DETERMINATION REPORT

ONTARIO AIRPORT PLANNING

Project File No.: PDEV17-024

Address: SEC Archibald Avenue & Parkview Street

APN: 0218-022-15

Existing Land Use: Vacant

Proposed Land Use: 88 Single Family Residential Units

Site Acreage: 14.35 acres Proposed Structure Height: 29 ft

ONT-IAC Project Review: n/a

Airport Influence Area: ONT Airport

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Henry Noh

Date: 7/7/17

CD No.: 2017-039

PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
☐ Zone 1	☐ 75+ dB CNEL	☐ High Terrain Zone	☐ Avigation Easement Dedication
☐ Zone 1A	☐ 70 - 75 dB CNEL	☐ FAA Notification Surfaces	☐ Recorded Overflight Notification
☐ Zone 2	☐ 65 - 70 dB CNEL	☐ Airspace Obstruction Surfaces	☑ Real Estate Transaction Disclosure
☐ Zone 3	☐ 60 - 65 dB CNEL	☐ Airspace Avigation Easement Area	
☐ Zone 4			
☐ Zone 5		Allowable Height: 200 ft plus	

The project is impacted by the following Chino ALUCP Safety Zones:

☐ Zone 1 ☐ Zone 2 ☐ Zone 3 ☐ Zone 4 ☐ Zone 5 ☐ Zone 6

Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: ☐ Exempt from the ALUCP ☐ Consistent ● Consistent with Conditions ☐ Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT, provided the following condition is met:

Airport Planner Signature: Lorena Mejia

PROJECT CONDITIONS

The applicant is required to meet the Real Estate Transaction Disclosure in accordance with California Codes(Business and Professions Code Section 11010-11024). New residential subdivisions within an Airport Influence Area are required to file an application for a Public Report consisting of a Notice of Intention (NOI) and a completed questionnaire with the Department of Real Estate and include the following language within the NOI:

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.

TOP-Zoning Consistency Determination

File No.: PDEV17-024

Location: Planning Area 4 of the Subarea 29 Specific Plan

Project Description:

A Development Plan approval to construct 88 single-family dwellings on approximately 14.35 acres of land located at the southeast corner of Archibald Ave and Parkview St within the Planning Area 4 land use district of the Subarea 29 Specific Plan (APN: 0218-022-15).

Prepared By: Clarice Burden

Date: 6/19/17

Signature: Clarice Burden

This project has been reviewed for consistency with The Ontario Plan Zoning Consistency project. The following was found:

☑ The existing TOP land use designation of the property is: Low Density Residential (2.1-5 du/ac)
The existing zoning of the property is: Conventionial Large Lot, Subarea 29 SP (3-6 du/ac) PA 4

☐ A change to the TOP land use designation has been proposed which would change the land use designation of the property to:

This proposed TOP land use change will:

☐ Make the existing zoning of the property consistent with the proposed General Plan Amendment;

☐ Make the proposed project consistent with The Ontario Plan.

☐ The zoning of the property will need to be changed in order to be consistent with The Ontario Plan. Through the TOP-Zoning Consistency effort, the zoning of the property is proposed to be changed to:

This proposed zone change will:

☐ Make the zoning of the property consistent with The Ontario Plan;

☐ Without the Zone Change described above, the proposed project is not consistent with The Ontario Plan. A finding of consistency with The Ontario Plan is required in order to approve this project.

☑ Additional Comments:

The density of the project, in conjunction with the balance of the Subarea 29 Specific Plan, falls within the allowed density of the General Plan. No changes in Specific Plan land use or General Plan are required.

CITY OF ONTARIO

MEMORANDUM

TO: "Vacant", Development Director
Scott Murphy, Planning Director (Copy of memo only)
Cathy Wahlstrom, Principal Planner (Copy of memo only)
Charity Hernandez, Economic Development
Kevin Shear, Building Official
Khoi Do, Assistant City Engineer
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Lorena Mejia, Associate Planner, Airport Planning
Steve Wilson, Engineering/NPDES
Bob Gluck, Code Enforcement Director
Jimmy Chang , IT Department
David Simpson, Development/IT (Copy of memo only)

FROM: Henry Noh, Senior Planner

DATE: June 06, 2017

SUBJECT: FILE #: PDEV17-024 Finance Acct#:

The following project has been submitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by **Tuesday, June 20, 2017**.

Note:
- ☐ Only DAB action is required
- ☑ Both DAB and Planning Commission actions are required
- ☐ Only Planning Commission action is required
- ☐ DAB, Planning Commission and City Council actions are required
- ☐ Only Zoning Administrator action is required

PROJECT DESCRIPTION: A Development Plan approval to construct 88 single-family dwellings on approximately 14.35 acres of land located at the southeast corner of Archibald Ave and Parkview St within the Planning Area 4 land use district of the Subarea 29 Specific Plan (APN: 0218-022-15). Related File(s): PMTT14-010.

☐ The plan does adequately address the departmental concerns at this time.
- ☐ No comments
- ☐ Report attached (1 copy and email 1 copy)
- ☒ Standard Conditions of Approval apply

☐ The plan does not adequately address the departmental concerns
- ☐ The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board

Police	DENNIS SOREL	MANAGEMENT ANALYST	6-26-17
Department	Signature	Title	Date

CITY OF ONTARIO
MEMORANDUM

TO: PLANNING DEPARTMENT, Henry Noh

FROM: BUILDING DEPARTMENT, Kevin Shear

DATE: June 7, 2017

SUBJECT: PDEV17-024

☒ The plan **does** adequately address the departmental concerns at this time.

- ☐ No comments
- ☒ Report below.

Conditions of Approval

1. Standard Conditions of Approval apply.

KS:lm

PLANNING COMMISSION STAFF REPORT

July 25, 2017

SUBJECT: A Tentative Tract Map (File No. PMTT17-001/TT 20076) to subdivide 7.65 acres of land into 62 numbered lots and 29 lettered lots within the Low Density Residential (LDR) district of Planning Area 11 of The Avenue Specific Plan, located on the west side of Haven Avenue and approximately 700 feet south of Ontario Ranch Road. (APN: 0218-412-02); **submitted by Brookcal Ontario, LLC.**

PROPERTY OWNER: Brookcal Ontario, LLC

RECOMMENDED ACTION: That the Planning Commission approve File No. PMTT17-001, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The project site is comprised of 7.65 acres of land located on the west side of Haven Avenue, approximately 700 feet south of Ontario Ranch Road, within the Low Density Residential (LDR) district of Planning Area 11 of The Avenue Specific Plan, and is depicted in ***Figure 1: Project Location***, below. The project site slopes gently from north to south and is currently mass graded. The property to the north of the project site is within the Retail district of Planning Area 10B of The Avenue Specific Plan and is vacant. The property to the east is within the Specific Plan (AG) zoning district and is vacant with previous agricultural/dairy uses. The property to the south is within the Low Density Residential district of Planning Area 2 of the Grand Park Specific Plan and has an existing agricultural/dairy use. The property to the west of the project site is within the Low Medium Density Residential district of Planning Area 11 of The Avenue Specific Plan and is mass graded.

Figure 1: Project Location

PROJECT ANALYSIS:

Case Planner:	Henry K. Noh
Planning Director Approval:	
Submittal Date:	January 18, 2017
Hearing Deadline:	N/A

Hearing Body	*Date*	*Decision*	*Action*
DAB	July 17, 2017	Approved	Recommend
ZA			
PC	July 25, 2017		Final
CC			

[1] Background — The Avenue Specific Plan and Environmental Impact Report (EIR) were approved by the City Council on December 19, 2006. The Avenue Specific Plan establishes the land use designations, development standards, and design guidelines for 568 acres, which includes the potential development of 2,875 dwelling units and approximately 131,000 square feet of commercial.

On April 8, 2014, the Planning Commission approved Tentative Tract Map 18922 (referred to as an "A" Map) for Planning Areas 9A and 10A of The Avenue Specific Plan. The approved "A" Map facilitated the backbone infrastructure improvements (major streets, sewer, water and storm drain facilities) and the creation of park/recreational facilities and residential neighborhoods within the eastern portion of the Specific Plan (see ***Figure 2: The Avenue Specific Plan Land Use Plan,*** below).

Figure 2: The Avenue Specific Plan Land Use Map

The Applicant, Brookcal Ontario, LLC, has submitted a Tentative Tract Map to subdivide 7.65 acres of land into 62 numbered lots and 29 lettered lots for the construction of 62 single-family, 6-pack cluster product (see ***Figure 3: Conceptual 6-Pack Cluster Site Plan*****,** below). The development plan for the proposed single-family, 6-pack cluster product will be brought before the Planning Commission at a future date.

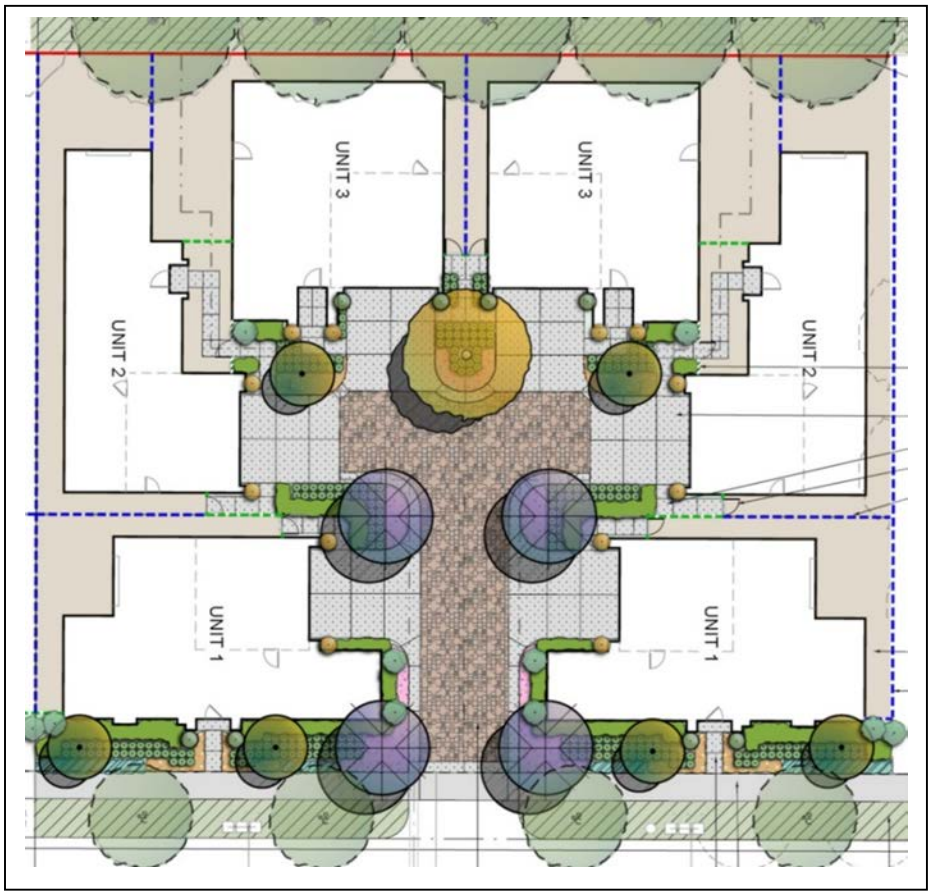


Figure 3: Conceptual 6-Pack Cluster Site Plan

To date there have been six development plans approved for the New Haven community that include:

- Holiday – 259 autocourt units consisting of 19 two-story buildings;
- Summerset - 112 single-family conventional homes (55'x90' lots);
- Waverly – A 6-pack cluster product with 135 single-family homes;
- Marigold - 149 single-family conventional homes (45'x90' lots);
- Poppy – A 6-pack cluster product with 104 single-family homes;
- Arborel – 91 single-family alley loaded homes; and
- Solstice – 93 rowtown units consisting of 16 two-story buildings.

[2] Tract Map Subdivision — The proposed Tentative Tract Map (File No. PMTT17-001/TT 20076) will subdivide 7.65 acres of land into 62 numbered lots and 29 lettered lots, located on the west side of Haven Avenue, approximately 700 feet south of Ontario Ranch Road and within the Low Density Residential (LDR) district of Planning Area 11 of The Avenue Specific Plan, as illustrated in **(*Exhibit A: Tentative Tract Map 20076*)**. The residential lots range in size from 2,854 to 4,541 square feet. The lots proposed exceed the Specific Plan's minimum lot requirement of 2,000 square feet for the cluster product.

[3] Site Access/Circulation — The previously approved related Tentative Tract Map 18922 ("A" Map), facilitated the construction of the backbone streets and primary access points into the existing New Haven Community (Planning Area 10A) of The Avenue Specific Plan from Ontario Ranch Road, Turner Avenue, Schaefer Avenue and Haven Avenue. The project site will have access from New Haven Drive, which runs north and south along the western frontage of the site and has direct access to Ontario Ranch Road. The Tentative Tract Map will also construct the interior tract private drive (loop) that will provide access to the future single-family residential development. Additionally, an emergency access road will be constructed within the southeastern portion of the project site that will connect to Haven Avenue.

[4] Open Space — The Tentative Tract Map will facilitate the construction of sidewalks, parkways, and open space areas within the tract **(See *Exhibit B: Conceptual Landscape Plan*)**. The Ontario Plan (TOP) Policy PR1-1 requires new developments to provide a minimum of 2 acres of private park per 1,000 residents. The proposed project is required to provide a 0.47 acre park to meet the minimum TOP private park requirement. To satisfy the park requirement, the applicant is constructing a 1.75 acre neighborhood park that is centrally located within the adjacent tract (TT 20061) to the west. In total, TT 20061 and TT 20076 are required to construct a 1.71 acre park to meet the minimum TOP private park requirement, therefore the 1.75 acre park would satisfy the minimum TOP private park requirement. In addition, a 6.8 acre park, as part of the related "A" Map (TT18922), was constructed at the center of Planning Area 10A, located north of the project site. The park features an 8,348 square foot club house, two pools and a spa, open lawn area and other recreational amenities. The residents of the development will have access to the parks and all park amenities.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)
- Ensure the Development of a Well Planned, Balanced, and Self-Sustaining Community in the New Model Colony

[2] Vision.

Distinctive Development:

- Commercial and Residential Development

➢ Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

[3] Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.

➢ G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

[4] Policy Plan (General Plan)

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.

➢ LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.

➢ LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).

Housing Element:

- Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

- H2-4 New Model Colony. We support a premier lifestyle community in the New Model Colony distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.

- Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.

Community Economics Element:

- Goal CE1: A complete community that provides for all incomes and stages of life.

  - CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

  - CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

  - CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

  - CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Community Design Element:

- Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

  - CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

  - CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

- Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

➢ CD2-2 Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Landscaped parkways, with sidewalks separated from the curb.

➢ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➢ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➢ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➢ CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

➢ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

▪ Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

➢ CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

- CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.

- CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.

- CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

- Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

- CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project is consistent with the number of dwelling units (62) and density (8.10 DU/AC)specified in the Available Land Inventory.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The project site is located within the Airport Influence Area of the Ontario International Airport (ONT), and has been found to be consistent with the policies and criteria set forth within the ALUCP for ONT.

ENVIRONMENTAL REVIEW: The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA13-003, an amendment to The Avenue Specific Plan for which an addendum to The Avenue Specific Plan EIR (SCH# 2005071109) was adopted by the City Council on June 17, 2014. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures shall be a condition of project approval and are incorporated herein by reference.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	Existing Land Use	*General Plan Designation*	*Zoning Designation*	*Specific Plan Land Use*
Site	Vacant	Low Density Residential	The Avenue Specific Plan	Planning Area 11 – (LDR)
North	Vacant	Neighborhood Commercial	The Avenue Specific Plan	Planning Area 10B – (Retail)
South	Dairy/Agricultural	Medium Density Residential	Grand Park Specific Plan	Planning Area 2 – (LDR)
East	Vacant	Mixed Use – NMC East	SP (AG Overlay)	N/A
West	Vacant	Medium Density Residential	The Avenue Specific Plan	Planning Area 11 – (LMDR)

Tentative Tract Map Summary:

Item	*TT20076*
Total Area Gross (AC)	7.65
Total Area Net (AC)	7.65
Min. Lot Size (Sq. Ft.)	2,854
Max. Lot Size (Sq. Ft.)	4,541
Avg. Lot Size (Sq. Ft.)	3,267
No. of Numbered Lots/Units	62
No. of Lettered Lots	29
Gross Density (du/gross ac)	8.10
Net Density (du/net ac)	8.10

EXHIBIT A – TENTATIVE TRACT MAP 20076

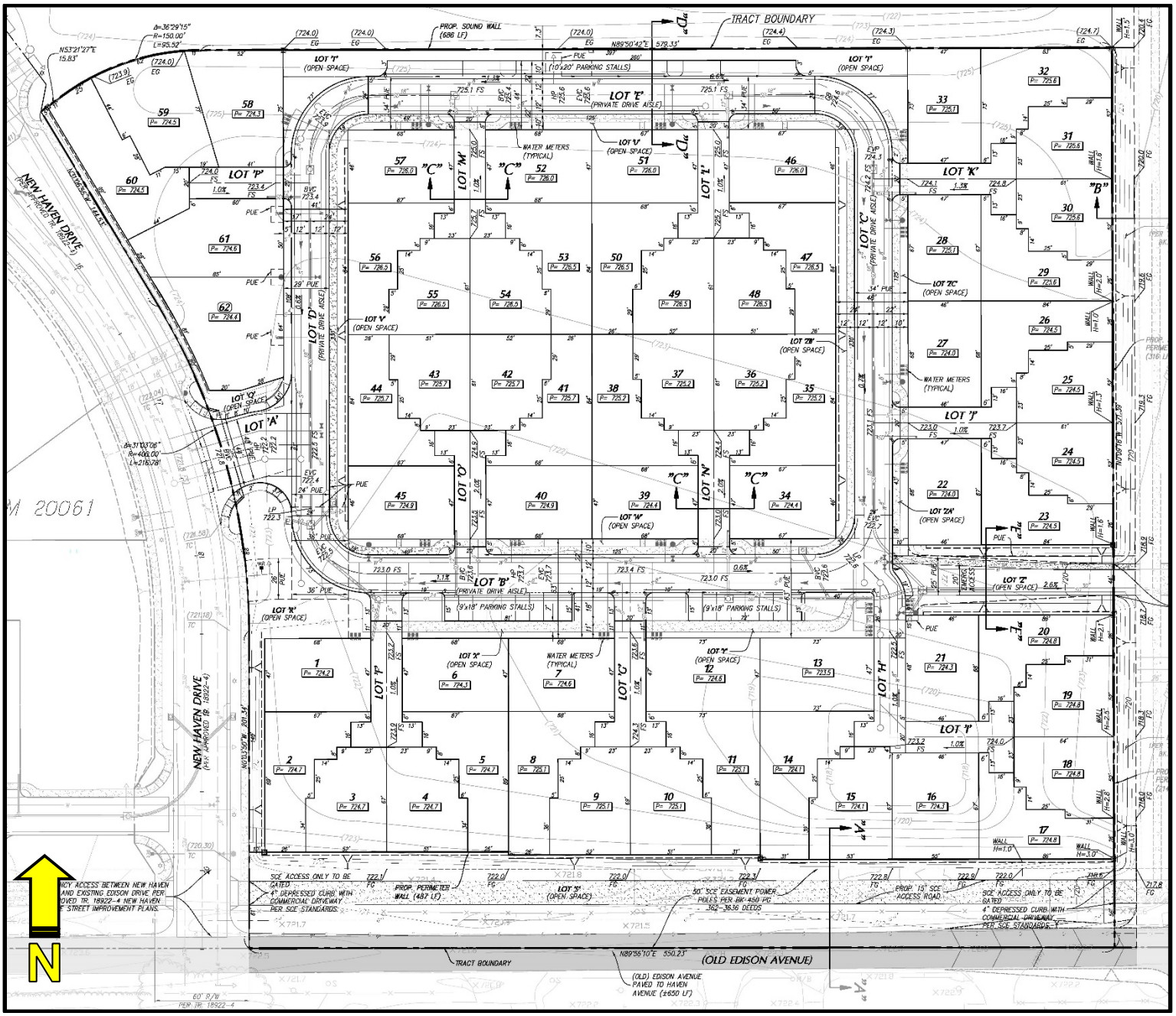
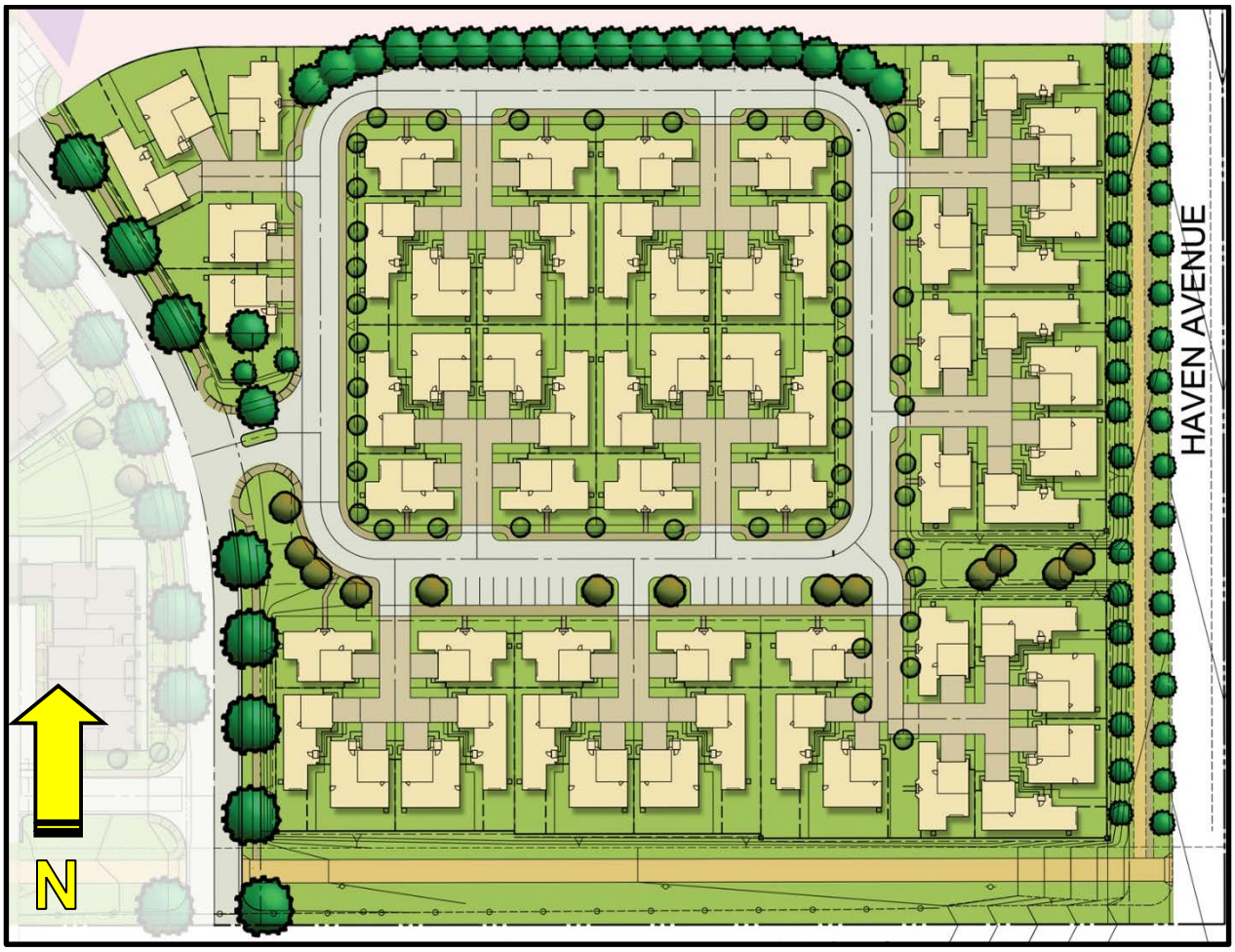


EXHIBIT B — CONCEPTUAL LANDSCAPE PLAN



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PMTT17-001 (TT20076), A TENTATIVE TRACT MAP TO SUBDIVIDE 7.65 ACRES OF LAND INTO 62 NUMBERED LOTS AND 29 LETTERED LOTS WITHIN THE LOW DENSITY RESIDENTIAL (LDR) DISTRICT OF PLANNING AREA 11 OF THE AVENUE SPECIFIC PLAN, LOCATED ON THE WEST SIDE OF HAVEN AVENUE AND APPROXIMATELY 700 FEET SOUTH OF ONTARIO RANCH ROAD, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 0218-412-02.

WHEREAS, BROOKCAL ONTARIO, LLC ("Applicant") has filed an Application for the approval of a Tentative Tract Map, File No. PMTT17-001 (TT20076), as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 7.65 acres of land located on the west side of Haven Avenue and approximately 700 feet south of Ontario Ranch Road, within the Low Density Residential (LDR) district of Planning Area 11 of The Avenue Specific Plan, and is presently mass graded; and

WHEREAS, the property to the north of the project site is within the Retail district of Planning Area 10B of The Avenue Specific Plan and is vacant. The property to the east is within the Specific Plan (AG) zoning district and is vacant. The property to the south is within the Low Density Residential district of Planning Area 2 of the Grand Park Specific Plan and has an existing agricultural/dairy use. The property to the west of the project site is within the Low Medium Density Residential district of Planning Area 11 of The Avenue Specific Plan and is mass graded; and

WHEREAS, the Tentative Tract Map proposed is in compliance with the requirements of The Avenue Specific Plan and is sufficient in size to facilitate and implement the traditional planning concepts for the “Residential Neighborhood” within the Specific Plan; and

WHEREAS, the proposed Tentative Tract Map is located within Planning Area 11 (Low Density Residential) land use district of The Avenue Specific Plan, which establishes a minimum lot size of 2,000 square feet for the cluster product and a development capacity of 225 dwelling units; and

WHEREAS, the proposed Tentative Tract Map will subdivide 7.65 acres of land into 62 numbered residential lots and 29 lettered lots. The residential lots range in size from 2,854 to 4,541 square feet. The lots proposed exceeds the Specific Plan’s minimum lot requirement of 2,000 square feet. The Tentative Tract Map is consistent with The Avenue Specific Plan; and

WHEREAS, TOP Policy PR1-1 requires new developments to provide a minimum of 2 acres of private park per 1,000 residents. The proposed project is required to provide a 0.47 acre park to meet the minimum TOP private park requirement. To satisfy the park requirement, the applicant is constructing a 1.75 acre neighborhood park that is centrally located within the adjacent tract (TT 20061) to the west. In total, TT 20061 and TT 20076 are required to construct a 1.71 acre park to meet the minimum TOP private park requirement, therefore the 1.75 acre park would satisfy the minimum TOP private park requirement. In addition, a 6.8 acre park, as part of the related “A” Map (TT18922), was constructed at the center of Planning Area 10A located north of the project site. The park features an 8,348 square foot club house, two pools and a spa, open lawn area and other recreational amenities. The residents of the development will have access to the parks and all park amenities; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA"); and

WHEREAS, the environmental impacts of this project were previously reviewed in conjunction with File No. PSPA13-003, an amendment to The Avenue Specific Plan for which an addendum to The Avenue Specific Plan EIR (SCH# 2005071109) was adopted by the City Council on June 17, 2014, and this Application introduces no new significant environmental impacts; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to CEQA (Public Resources Code Section 21000 et seq.), and an initial study has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on July 17, 2017, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project and concluded said hearing on that date, voting to issue Decision No. DAB17-034 recommending the Planning Commission approve the Application; and

WHEREAS, on July 25, 2017, the Planning Commission of the City of Ontario conducted a hearing to consider the Project and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: ***Environmental Determination and Findings.*** As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the previous addendum to The Avenue Specific Plan EIR (SCH# 2005071109) and supporting documentation. Based upon the facts and information contained in the previous addendum to The Avenue Specific Plan EIR (SCH# 2005071109) and supporting documentation, the Planning Commission finds as follows:

(1) The environmental impacts of this project were reviewed in conjunction with an Addendum to The Avenue Specific Plan Environmental Impact Report, certified by the City of Ontario City Council on June 17, 2014, in conjunction with File No. PSPA13-003.

(2) The previous addendum to The Avenue Specific Plan EIR (SCH# 2005071109) contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous addendum to The Avenue Specific Plan EIR (SCH# 2005071109) was completed in compliance with CEQA and the Guidelines promulgated thereunder; and

(4) The previous addendum to The Avenue Specific Plan EIR (SCH# 2005071109) reflects the independent judgment of the Planning Commission; and

(5) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the previous addendum to The Avenue Specific Plan EIR (SCH# 2005071109), and all mitigation measures previously adopted with the addendum to The Avenue Specific Plan EIR (SCH# 2005071109), are incorporated herein by this reference.

SECTION 2: ***Subsequent or Supplemental Environmental Review Not Required.*** Based on the information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental addendum to The Avenue Specific Plan EIR (SCH# 2005071109) is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the addendum to The Avenue Specific Plan EIR (SCH# 2005071109) that will require major revisions to the addendum to The Avenue Specific Plan EIR (SCH# 2005071109) due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the addendum to The Avenue Specific Plan EIR (SCH# 2005071109) was prepared, that will require major revisions to the addendum to The Avenue Specific Plan EIR (SCH# 2005071109) due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the addendum to The Avenue Specific Plan EIR (SCH# 2005071109) was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the addendum to The Avenue Specific Plan EIR (SCH# 2005071109); or

(b) Significant effects previously examined will be substantially more severe than shown in the addendum to The Avenue Specific Plan EIR (SCH# 2005071109); or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the addendum to The Avenue Specific Plan EIR (SCH# 2005071109) would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: ***Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project is consistent with the number of dwelling units (62) and density (8.10) specified in the Available Land Inventory.

SECTION 4: ***Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport ("ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the Planning Commission, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 5: ***Concluding Facts and Reasons.*** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing,

and upon the specific findings set forth in Section 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed Tentative Tract/Parcel Map is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable area and specific plans, and planned unit developments.*** The proposed Tentative Tract Map is located within the Low Density Residential land use district of the Policy Plan Land Use Map, and within Planning Area 11 (LDR) district of The Avenue Specific Plan. The proposed subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will contribute to providing "a spectrum of housing types and price ranges that match the jobs in the City, and that make it possible for people to live and work in Ontario and maintain a quality of life" (Goal LU1). Furthermore, the project will promote the City's policy to "incorporate a variety of land uses and building types that contribute to a complete community where residents at all stages of life, employers, workers, and visitors, have a wide spectrum of choices of where they can live, work, shop, and recreate within Ontario" (Policy LU1-6 *Complete Community*). In addition, the Tentative Tract Map meets all minimum size requirements and development standards specified within the Low Density Residential (Planning Area 11 – Product Type 3) land use district of The Avenue Specific Plan, therefore the proposed Tentative Tract Map is consistent with The Ontario Plan and The Avenue Specific Plan.

(2) ***The design or improvement of the proposed Tentative Tract/Parcel Map is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable specific plans and planned unit developments.*** The proposed Tentative Tract Map is located within the Low Density Residential land use district of the Policy Plan Land Use Map, and within Planning Area 11 (LDR) district of The Avenue Specific Plan. The proposed design or improvement of the subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will contribute to providing "[a] high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct" (Goal CD2). Furthermore, the project will promote the City's policy to "create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while

maintaining acceptable fire protection and traffic flows;

- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the "outdoor living room"), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb." (Policy CD2-2 *Neighborhood Design*).

In addition, the Tentative Tract Map meets all minimum size requirements and development standards specified within the Low Density Residential (Planning Area 11 – Product Type 3) land use district of The Avenue Specific Plan, therefore the proposed Tentative Tract Map is consistent with The Ontario Plan and The Avenue Specific Plan.

(3) ***The site is physically suitable for the type of development proposed.*** The Tentative Tract Map proposes to subdivide 7.65 acres of land into 62 numbered lots for the construction of 62 single-family residential within (Planning Area 11) of The Avenue Specific Plan. The proposed lots range in size from 2,854 to 4,541 square feet. The lots proposed exceeds the Specific Plan's minimum lot requirement of 2,000 square feet. The Specific Plan provides for the development of up to 225 residential dwelling units and a density of 10.06 dwelling units per acre within Planning Area 11. The Tentative Tract Map proposes 62 dwelling units at a density of 8.10 dwelling units per acre. The project site meets the minimum lot area and dimensions of the Low Density Residential (Planning Area 11 – Product Type 3) land use district of The Avenue Specific Plan, and is physically suitable for the type of residential development proposed in terms of zoning, land use and development activity proposed, and existing and proposed site conditions.

(4) ***The site is physically suitable for the density/intensity of development proposed.*** The project site is proposed for residential development at a density of 8.10 DUs/acre. The Specific Plan provides for the development of up to 225 residential dwelling units and a density of 10.06 dwelling units per acre within Planning Area 11. The Tentative Tract Map proposes 62 dwelling units at a density of 8.10 dwelling units per acre. The Tentative Tract Map proposes to subdivide 7.65 acres of land into 62 numbered lots for the construction of 62 single-family residential within (Planning Area 11) of The Avenue Specific Plan. The proposed lots range in size from 2,854 to 4,541 square feet. The lots proposed exceeds the Specific Plan's minimum lot requirement of 2,000 square feet. The project site meets the minimum lot area and dimensions of the Low Density Residential (Planning Area 11 – Product Type 3) zoning district, and is physically suitable for this proposed density/intensity of development.

(5) ***The design of the subdivision or the proposed improvements thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.*** The project site is not located in an area that has been identified as containing species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations or by the

California Department of Fish and Wildlife or the U.S. Fish and Wildlife Service, nor does the site contain any riparian habitat or other sensitive natural community, and no wetland habitat is present on site; therefore, the design of the subdivision, or improvements proposed thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat. In addition, the environmental impacts of this project were previously reviewed in conjunction with an addendum to The Avenue Specific Plan EIR (SCH# 2005071109). This application is consistent with the previously adopted EIR and introduces no new significant environmental impacts.

(6) ***The design of the subdivision, or the type of improvements thereon, are not likely to cause serious public health problems.*** The design of the proposed subdivision, and the residential and infrastructure improvements proposed on the project site, are not likely to cause serious public health problems, as the project is not anticipated to involve the transport, use, or disposal of hazardous materials during either construction or project implementation, include the use of hazardous materials or volatile fuels, nor are there any known stationary commercial or industrial land uses within close proximity to the subject site that use/store hazardous materials to the extent that they would pose a significant hazard to visitors or occupants to the project site. In addition, the environmental impacts of this project were previously reviewed in conjunction with the addendum to The Avenue Specific Plan EIR (SCH# 2005071109). This application is consistent with the previously adopted EIR and introduces no new significant environmental impacts.

(7) ***The design of the subdivision, or the type of improvements thereon, will not conflict with easements acquired by the public at large for access through, or use of property within, the proposed subdivision.*** The proposed subdivision has provided for all necessary public easements and dedications for access through, or use of property within, the proposed subdivision. Furthermore, all such public easements and dedications have been designed pursuant to: (a) the requirements of the Policy Plan component of The Ontario Plan and applicable area plans; (b) applicable specific plan; (c) applicable provisions of the City of Ontario Development Code; (d) applicable master plans and design guidelines of the City; and (e) applicable Standard Drawings of the City.

SECTION 6: ***Planning Commission Action.*** Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 7: ***Indemnification.*** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the

applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: ***Custodian of Records.*** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: ***Certification to Adoption.*** The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of July 2017, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Richard D. Delman
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Principal Planner/Acting Secretary of Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Marci Callejo, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. PC17-[insert #] was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 25, 2017, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Marci Callejo
Secretary Pro Tempore

ATTACHMENT A:

File No. PMTT17-001
Departmental Conditions of Approval

(Departmental conditions of approval to follow this page)

City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department
Land Development Division
Conditions of Approval

Meeting Date: July 17, 2017

File No: PMTT17-001

Related Files:

Project Description: A Tentative Tract Map (File No. PMTT17-001/TT 20076) to subdivide 7.65 acres of land into 62 numbered lots and 29 lettered lots within the Low Density Residential (LDR) district of Planning Area 11 of The Avenue Specific Plan, located on the west side of Haven Avenue, approximately 700 feet south of Ontario Ranch Road. (APN: 0218-412-02); **submitted by Brookcal Ontario, LLC.**

Prepared By: Henry K. Noh, Senior Planner
Phone: 909.395.2429 (direct)
Email: hnoh@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2010. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Tentative Tract Map approval shall become null and void 2 years following the effective date of application approval, unless the final parcel/tract map has been recorded, or a time extension has been approved by the Planning Commission pursuant to Development Code Section 2.02.025 (Time Limits and Extensions). This Permit does not supersede any individual time limits specified herein for performance of specific conditions or improvements.

2.2 Subdivision Map.

(a) The Final Tract Map shall be in conformance with the approved Tentative Tract Map on file with the City. Variations from the approved Tentative Tract Map may be reviewed and approved by the Planning Department. A substantial variation from the approved Tentative Tract Map may require review and approval by the Planning Commission, as determined by the Planning Director.

(b) Tentative Tract Map approval shall be subject to all conditions, requirements and recommendations from all other departments/agencies provided on the attached reports/memorandums.

(c) Pursuant to California Government Section 66474.9, the subdivider agrees that it will defend, indemnify, and hold harmless the City of Ontario or its agents, officers and employees from any

claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer of this subdivision, which action is brought within the time period provided for in Government Code Section 66499.37. The City of Ontario shall promptly notify the subdivider of any such claim, action or proceeding and the City of Ontario shall cooperate fully in the defense.

2.3 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.4 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.5 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.6 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(c) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(d) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

2.7 Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

2.8 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.9 Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements.

(a) CC&Rs shall be prepared for the Project and shall be recorded prior to the issuance of a building permit.

(b) The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owners association and the CC&Rs shall be reviewed and approved by the City.

(c) CC&Rs shall ensure reciprocal parking and access between parcels.

(d) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:

(i) Landscaping and irrigation systems within common areas;
(ii) Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02;
(iii) Shared parking facilities and access drives; and
(iv) Utility and drainage easements.

(e) CC&Rs shall include authorization for the City's local law enforcement officers to enforce City and State traffic and penal codes within the project area.

(f) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.

(g) A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

2.10 Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

(ii) Some of the property adjacent to this tract is zoned for agricultural uses and there could be fly, odor, or related problems due to the proximity of animals.

(iii) The area south of Riverside Drive lies within the San Bernardino County Agricultural Preserve. Dairies currently existing in that area are likely to remain for the foreseeable future.

2.11 Environmental Review.

(a) The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA13-003, an amendment to The Avenue Specific Plan for which an addendum to The Avenue Specific Plan EIR (SCH# 2005071109) was adopted by the City Council on June 17, 2014. This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. The previously adopted mitigation measures shall be a condition of project approval, and are incorporated herein by this reference.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.12 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.13 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.14 Additional Requirements.

(a) All applicable conditions of approval of Development Agreement (File No. PDA10-002) shall apply to this tract.

(b) All applicable conditions of approval of The Avenue Specific Plan shall apply to this tract.

(c) All applicable conditions of approval of the "A" Map TT 18922 (File No. PMTT13-010) shall apply to this tract.

(d) Off-Site Subdivision Signs.

The City Council has authorized the Baldy View Chapter of the Building Industry Association to manage a standardized off-site directional sign program on a non-profit basis. The program uses uniform sign structures and individual identification and directional signs for residential development. **No other off-site signing is authorized.** (For additional information, contact the Baldy View Chapter BIA at (909) 945-1884.

(e) The applicant shall contact the Ontario Post Office to determine the size and location of mailboxes for this project. The location of the mailboxes shall be submitted to the Planning Department for review and approval prior to the issuance of building permits.

(f) The applicant (Developer) shall be responsible for providing fiber to each home per City requirements and standards.

(g) Dairy Separation Requirement for Residential Development.

The following separation requirements from existing dairies/feed lots shall apply to new residential development or structures used for public assembly purposes from existing dairies/feed lots.

A minimum 100' separation shall be required between a new residential, commercial or industrial development or structure used for public assembly and an existing animal feed trough, corral/pen or an existing dairy/feed lot including manure stockpiles and related wastewater detention basins. The 100-foot separation requirement may be satisfied by an off-site easement acceptable to the Planning Director with adjacent properties, submitted with the initial final map and recorded prior to or concurrent with the final map.

(h) The applicant (Developer) shall be responsible for providing fiber to each home per City requirements and standards.

(i) Prior to the issuance of the 1st home certificate of occupancy within TT 20076, the Open Space Lot Z (Emergency Access) shall be fully constructed.

(j) The applicant/developer shall coordinate with the Engineering Department to pay in-lieu fees for the ultimate frontage improvements for the portion of existing Edison Avenue between the New Haven Drive and New Haven Avenue since this segment of existing Edison Avenue is not being vacated at the present time.

CITY OF ONTARIO

ENGINEERING DEPARTMENT CONDITIONS OF APPROVAL

(Environmental, Traffic/Transportation Division, Ontario Municipal Utilities Company Information Technology and Management Services Department conditions incorporated herein)

☒ **DEVELOPMENT PLAN** ☐ OTHER	☐ PARCEL MAP ☒ **TRACT MAP** ☐ FOR CONDOMINIUM PURPOSES
PROJECT FILE NO. TM-20076 **RELATED FILE NO(S). TM18922-4**	
☒ **ORIGINAL** ☐ REVISED: __/__/__	

CITY PROJECT ENGINEER & PHONE NO: Naiim Khoury, Associate Engineer NK (909) 395-2152

CITY PROJECT PLANNER & PHONE NO: Henry Noh, Senior Planner (909) 395-2429

DAB MEETING DATE: July 17, 2017

PROJECT NAME / DESCRIPTION: TM-20076, a Tentative Tract Map to subdivide 7.65 acres of land into 62 numbered lots for clustered single family residences

LOCATION: Northwest corner of Haven Avenue and Old Edison Avenue

APPLICANT: BrookCal Ontario, LLC

REVIEWED BY: N. Khoury — Naiim Khoury, Associate Engineer — 6.29.17 Date

APPROVED BY: FOR. — Khoi Do, P.E., Assistant City Engineer — 6/29/17 Date

Last Revised: 6/29/2017

THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2010-021) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT PLUS THE CONDITIONS OF APPROVAL FOR TRACT MAP TM18922 – Phase 4, The Avenue Specific Plan and the DA Agreement.

1. PRIOR TO FINAL MAP APPROVAL, APPLICANT SHALL:

Check When Complete

☐ 1.01 Dedicate to the City of Ontario, the right-of-way, described below: ☐

_____ feet on _____

Property line corner 'cut-back' required at the intersection of _____ and _____.

☒ **1.02 Dedicate to the City of Ontario, the following easement(s):** ☐
Various widths (24-63 feet) Public Easements (PUE's) to the City of Ontario in the private drive aisles (Lots A through E and Lot Z) for sewer, water, and fiber optic purposes, as shown on the tentative tract map.

_____

☐ 1.03 Restrict vehicular access to the site as follows: _____ ☐

☐ 1.04 Vacate the following street(s) and/or easement(s): _____ ☐

☐ 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles. ☐

☒ **1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.** ☐

a) **All drive aisles and alley ways shown on this map are private to be maintained by the HOA. All improvements within these drive aisles and alley ways will be privately maintained except for sewer, water and fiber optic within the PUE limits.**

b) **The public and private improvements constructed within this tentative tract map shall be maintained through the combination of public and private entities as described in Section 5.5 and Table 4, "Maintenance Responsibilities" of The Avenue Specific Plan.**

c) **Improvements within Southern California Edison easements to be maintained by the HOA.**

☐ 1.07 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Management Services Department at (909) 395-2124 regarding this requirement. ☐

(1) _____

(2) _____

☒ 1.08 **Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.** ☐

☒ 1.09 **Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.** ☐

☒ 1.10 **Provide a preliminary title report current to within 30 days.** ☐

☒ 1.11 **File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Management Services at (909) 395-2353 to initiate the CFD application process.** ☐

☒ 1.12 **New Model Colony (NMC) Developments:** ☐

☒ 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.

☒ 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).

☒ 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).

☐ 1.13 Other conditions:_____. ☐

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

A. GENERAL
(Permits includes Grading, Building, Demolition and Encroachment)

☒ 2.01 **Record Tract Map No. 20076 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.** ☐

☒ 2.02 **Submit a duplicate photo mylar of the recorded map to the City Engineer's office.** ☐

☒ 2.03 **Note that the subject parcel is a recognized parcel in the City of Ontario Per Tract Map No. 18922-4** ☐

☐ 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of_____. ☐

☐ 2.05 Apply for a: ☐ Certificate of Compliance with a Record of Survey; ☐ Lot Line Adjustment ☐

☐ Make a Dedication of Easement.

☐ 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. ☐

☐ 2.07 Submit a soils/geology report. ☐

☒ **2.08 Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:** ☐

- ☐ State of California Department of Transportation (Caltrans)
- ☐ San Bernardino County Road Department (SBCRD)
- ☐ San Bernardino County Flood Control District (SBCFCD)
- ☐ Federal Emergency Management Agency (FEMA)
- ☐ Cucamonga Valley Water District (CVWD) for sewer/water service
- ☐ United States Army Corps of Engineers (USACE)
- ☐ California Department of Fish & Game
- ☐ Inland Empire Utilities Agency (IEUA)
- ☒ **Other: Southern California Edison for improvements within their easement along the north side of Edison Avenue.**

☐ 2.09 Dedicate to the City of Ontario the right-of-way described below: ☐

_____ feet on _____

Property line corner 'cut-back' required at the intersection of _____ and _____.

☐ 2.10 Dedicate to the City of Ontario the following easement(s):_____ ☐

_____

☒ **2.11 New Model Colony (NMC) Developments:** ☐

☒ **1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.**

☐ 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.

☒ **3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall.**

☐ 2.12 Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at _____% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements. ☐

☐ 2.13 The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office. ☐

☒ **2.14 Pay all Development Impact Fees (DIF) to the Building Department.** ☐

☒ **2.15 Other conditions:** ☐

a) **That portion of existing Edison Avenue between New Haven Drive and Haven Avenue will not be vacated at the present time and shall maintain a paved roadway to provide access to this tract. However, the applicant/developer shall pay in-lieu fees for the ultimate frontage improvements for the above mentioned Edison Avenue segment and shall be responsible to vacate this street segment in the future when the City is ready to vacate this street.**

B. PUBLIC IMPROVEMENTS
(See attached Exhibit 'A' for plan check submittal requirements.)

☒ 2.16 **Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):**

Improvement	Within Ontario Utility Easement on Lots A through E and Lot Z	New Haven Drive (Improvements to be completed per TM18922-4)	Edison Avenue	Haven Avenue (Improvements to be completed per TM18922-4)
Curb and Gutter	☐ New; ___ ft. from C/L ☐ Replace damaged ☐ Remove and replace	☒ **New/ modification; 18 ft. from C/L at Edison Avenue** ☐ Replace damaged ☐ Remove	☐ New; ___ ft. from C/L ☐ Replace damaged ☐ Remove and replace	☐ New; ___ ft. from C/L ☐ Replace damaged ☐ Remove and replace
AC Pavement (see section 2.17)	☐ Replacement ☐ Widen ____ additional feet along frontage, including pavm't transitions	☐ Replacement ☒ **New/ modification at Edison Avenue including pavm't transitions**	☒ **New plus Grind and overlay and/or replacement, as required** ☒ **New/ modification at New Haven Drive including pavm't transitions**	☐ Replacement ☐ Widen ____ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	☐ New ☐ Modify existing	☐ New ☐ Modify existing	☐ New ☐ Modify existing	☐ New ☐ Modify existing
Drive Approach	☒ **New- private. Shall meet ADA requirements** ☐ Remove and replace replace	☒ **New Entrance to the tract** ☒ **New on both sides for access to SCE easement**	☐ New ☐ Remove and replace replace	☒ **New for access to PUE in Lot Z** ☐ Remove and replace replace
Sidewalk	☒ **New- private. Shall meet ADA requirements** ☐ Remove and replace	☒ **New/ modification; north of Edison Avenue** ☐ Remove and replace	☐ New ☐ Remove and replace	☐ New ☐ Remove and replace
ADA Access Ramp	☒ **New** ☐ Remove and replace	☒ **New at Entrances/exits and driveways** ☐ Remove and replace	☐ New ☐ Remove and replace	☒ **New for ADA access in Lot Z** ☐ Remove and replace

Parkway	☐ Trees ☐ Landscaping (w/irrigation)	☒ **Trees** ☒ **Landscaping (w/irrigation)**	☐ Trees ☐ Landscaping (w/irrigation)	☐ Trees ☐ Landscaping (w/irrigation)
Raised Landscaped Median	☐ New ☐ Remove and replace	☐ New ☐ Remove and replace ☐ Remove and replace	☐ New ☐ Remove and replace	☐ New ☐ Remove and replace
Fire Hydrant	☒ **New** ☐ Relocation	☐ New ☐ Relocation	☒ **New** ☐ Relocation	☐ New ☐ Relocation
Sewer (see Sec. 2.C)	☒ **Main plus Connection to New Haven Avenue sewer main** ☒ **Laterals**	☒ **Main** ☐ Lateral	☐ Main ☐ Lateral	☐ Main ☐ Lateral
Water (see Sec. 2.D)	☒ **Main plus Connection to New Haven Drive water main at lot A plus Main plus Connection to Haven Avenue water main at Lot Z** ☒ **Services**	☒ **Main** ☒ **Service**	☐ Main ☐ Service	☒ **Main Connection in Haven Avenue water main at Lot Z** ☐ Lateral
Recycled Water (see Sec. 2.E)	☐ Main ☐ Service	☐ Main ☒ **Service**	☐ Main ☐ Service	☐ New ☐ Service
Traffic Signal System (see Sec. 2.F)	☐ New ☐ Modify existing	☐ New ☐ Modify existing	☐ New ☐ Modify existing	☐ New ☐ Modify existing
Traffic Signing and Striping (see Sec. 2.F)	☒ **New** ☐ Modify existing	☐ New ☐ Modify existing	☒ **New** ☐ Modify existing	☐ New ☐ Modify existing
Street Light (see Sec. 2.F)	☐ New / Upgrade ☐ Relocation	☐ New / Upgrade ☐ Relocation	☒ **New** ☐ Relocation	☐ New / Upgrade ☐ Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	☐ New ☐ Modify existing	☐ New ☐ Modify existing	☐ New ☐ Modify existing	☐ New ☐ Modify existing

Storm Drain (see Sec. 2G)	☐ Main ☐ Lateral	☐ Main ☐ Lateral	☐ Main ☐ Lateral	☐ Main ☒ **Connection to proposed SD main in Haven Avenue at Lot Z**
Fiber Optics (see Sec. 2K)	☒ **Conduit system and Appurtenances**	☒ **Conduit system and Appurtenances**	☐ Conduit / Appurtenances	☐ Conduit / Appurtenances
Overhead Utilities	☐ Underground per City Ordinance ☐ Relocate	☐ Underground ☐ Relocate	☒ **Underground per City Ordinance** ☐ Relocate	☒ **Underground per City Ordinance** ☐ Relocate
Removal of Improvements	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____ _____ _____
Entrance/Exit	☐ New ☐ Remove and replace replace	☒ **New/ modification; north of Edison Avenue** ☐ Remove and replace replace	☐ New ☐ Remove and replace replace	☐ New ☐ Remove and replace replace

Specific notes for improvements listed in item no. 2.15, above:

a) **No decorative pavers or permanent improvements such as walls, fences, trash enclosures, etc. shall be installed within the City of Ontario utility easements. During the course of maintenance and repairs of public utilities within the public easement, the City will restore trenches per the latest City Standards, as applicable. Restoration of any enhancements above and beyond City standards, including but not limited to architectural paving, hardscape and landscape enhancements shall be the responsibility of the HOA.**

b) **No underground BMP or infiltration basins shall be permitted within the City of Ontario Easements.**

c) **The applicant/developer shall pay in-lieu fees for the ultimate frontage improvements for that portion of existing Edison Avenue between New Haven Drive and Haven Avenue since this segment of Edison Avenue is not being vacated at the present time. The ultimate improvements shall match proposed improvements along Edison Avenue for TM20061. Also, a paved roadway shall be maintained between New Haven Drive and Haven Avenue.**

☒ 2.17 **The Applicant/developer shall be responsible for the construction of the proposed Haven Avenue public improvements between Ontario Ranch Road and Edison Avenue (Currently under review and approval for TM18922-4) including, but not limited to the following: intersection improvements beyond the subject tract limits, construction of a minimum 2" asphalt concrete (AC) grind and overlay and/or replacement of existing AC pavement in Edison Avenue between new pavement and New Haven Drive, slopes and swales.** ☐

☐ 2.18 Reconstruct the full pavement structural section per City of Ontario Standard Drawing number 1011, based on existing pavement condition and approved street section design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter. 'Pothole' ☐

verification of existing pavement section required prior to acceptance/approval of street improvement plan.

☐ 2.19 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide ☐ water service ☐ sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid. ☐

☐ 2.20 Other conditions: _____ ☐

C. SEWER

☒ **2.21 Existing sewer main stub is available for connection to serve this project at Ontario Ranch Road and New Haven Drive (DWG. No. S15326).** ☐

☐ 2.22 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away. ☐

☐ 2.23 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer. ☐

☒ **2.24 Other conditions:** ☐

a) **8-inch sewer main shall be constructed in New Haven Drive, from Ontario Ranch Road to north of Edison Avenue which ultimately connects to the sewer main Ontario Ranch Road.**

b) **A minimum of 8-inch public sewer main shall be constructed in the interior private streets (Lots A through E) and connect to the sewer main in New Haven Avenue. A PUE shall be granted to the City of Ontario for the sewer lines in Lots A to E.**

c) **All improvements in the "Lettered Lots" of the cluster buildings will be privately owned and maintained.**

D. WATER

☒ **2.25 Existing water main stub is available for connection to serve this project at Ontario Ranch Road and New Haven Drive (DWG. No. W15835).** ☐

☐ 2.26 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away. ☐

☒ **2.27 Other conditions:** ☐

a) **12-inch domestic water main shall be constructed in New Haven Drive, between Ontario Ranch Road and Edison Avenue.**

b) **Water mains shall be looped with connection to the water main in New Haven Drive at Lot A and Haven Avenue at Lot Z.**

c) **All improvements in the "Lettered Lots" of the cluster buildings will be privately owned and maintained.**

E. RECYCLED WATER

☒ **2.28 Existing water main stub is available for connection to serve this project at Ontario Ranch Road and New Haven Drive (DWG. No. P11412).** ☐

☐ 2.29 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project. ☐

☐ 2.30 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. Applicant shall be responsible for construction of a connection to the recycled water main for approved uses, when the main becomes available. The cost for connection to the main shall be borne solely by Applicant. ☐

☒ **2.31 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.** ☐

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2687 regarding this requirement.

☒ **2.32 Other conditions:** ☐

a) 8-inch recycled water main shall be constructed in New Haven Drive, between Ontario Ranch Road and Edison Avenue.

b) The applicant/developer shall confirm the necessary meter(s) to service the entire tract's landscaping needs and provide additional easements to the City, if necessary.

c) The proposed project shall use recycled water for landscaping purposes. Provide an exhibit showing all areas using recycled water.

d) Provide two hard copies and the digital files in PDF and AutoCAD format of both on-site and off-site utility plans, including landscape and irrigation.

F. TRAFFIC / TRANSPORTATION

☐ 2.33 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer: ☐

1. On-site and off-site circulation
2. Traffic level of service (LOS) at 'build-out' and future years
3. Impact at specific intersections as selected by the City Engineer

☒ **2.34 Other conditions:** ☐

a) Construct New Haven Drive per TM18922-4, between Ontario Ranch Road and Edison Avenue. All public improvements required herein, shall include, but not be limited to, raised median, concrete curb and gutter, sidewalk, LED street lights, signing and striping, parkway landscaping, and pavement transitions.

b) New Haven Drive and in-tract streets shall be signed "No Parking Anytime". Haven Avenue shall be signed "No Stopping Anytime".

c) "No Parking Anytime" signs shall be installed along the curb side of the interior private streets Lots A through E.

d) All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City of Ontario Standard Drawing No. 1309.

e) The emergency access at Lot Z shall be gated at the P/L, and designed such that it does not appear traversable. The design shall include features such as mountable curb, landscaping, and be to the satisfaction of the City Engineer.

f) The Applicant/Developer shall design and construct a pedestrian access next to the emergency access.

g) **During the development and construction of this Tract, at least two full point of access and one emergency access shall be maintained at all times.**

h) **Applicant/Developer shall construct temporary dead end street guard rail per Standard Drawings 1310 and 1311 along the south side of Edison Avenue at New Haven Drive.**

i) **The Applicant/Developer's engineer-of-record shall meet with City Engineering staff prior to starting signing and striping and/or street lighting design to discuss items such as striping layout and tie-ins to existing or future street light circuits.**

G. DRAINAGE / HYDROLOGY

☐ 2.35 A _____inch storm drain main is available to accept flows from this project in _____. (Ref: Storm Drain plan bar code:_____) ☐

☐ 2.36 Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study. ☐

☐ 2.37 An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans. ☐

☐ 2.38 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project. ☐

☐ 2.39 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program. ☐

☒ **2.40 Pay Storm Drain Development Impact. Fee shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.** ☐

☒ **2.41 Other conditions:** ☐

a) **The proposed storm drain main in Haven Avenue between Ontario Ranch Road and the County Channel at Bellegrave Avenue (per TM18922-4) must be complete and operational and connected to the storm drain system in this tract.**

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

☐ 2.41 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.
If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.
Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130. ☐

☒ **2.42 Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: http://www.sbcounty.gov/dpw/land/npdes.asp.** ☐

☐ 2.43 Other conditions: _____ ☐

J. SPECIAL DISTRICTS

☐ 2.44 File an application, together with an initial payment deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community facilities District Act of 1982. The application and fee shall be submitted a minimum three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact the Management Services Department at (909) 395-2353 to initiate the CFD application process. ☐

☐ 2.45 Other conditions: _____ ☐

K. FIBER OPTIC

☒ **2.46 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole.** ☐

☒ **2.47 For additional information refer to the Fiber Optic Master Plan and the In-tract Fiber Network Design guidelines on the City's website. Contact the Information Technology Department at (909) 395-2000/OntarioNet@ontarioca.gov, regarding this requirement.** ☐

☒ **2.45 Other conditions:** ☐

a) The applicant/developer shall provide fiber optic connection to each home unit per city standards and guidelines.

b) OntarioNet fiber optic plans must be designed and approved at the same time as other improvement plans.

a) Project shall be designed and constructed to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole in the Right-of-Way (ROW) and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole.

b) Contractor is responsible for locating and connecting conduit to existing OntarioNet hand holes on adjacent properties. There should be no "Gaps" in conduit between the contractor's development and the adjacent property. OntarioNet hand holes are typically located in the ROW at the extreme edge of a property.

c) Hand holes - Design and install OntarioNet fiber optic hand hole HH-2 (17x30x24), HH-2A (24x36x30), HH-3 (30x48x36) and/or HH-4 (36x60x36) as needed. Respectively Newbasis Part # PCA-173024-90116, PCA-243630-90064, PCA-304836-90244 and PCA-366036-90146 per City Standard 1316. Conduits sweeping into hand holes shall enter in flush with the cut-out mouse holes aligned parallel to the bottom of the box and come in perpendicular to the wall of the box. Conduits shall not enter at any angle other than parallel. Provide 5 foot minimum clearance from existing/proposed utilities.

d) ROW Conduit – Design and install fiber optic conduit at a minimum depth of 36-inch. Trenching shall be per City Standard 1306. Install (1) 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct and (1) 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange with Black Stripe) duct. Conduit(s) between ROW hand holes and hand holes on private

property shall be 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct.

e) ROW Conduit - Design and Install all Fiber Optic Conduit at a Minimum Depth of 36". Trenching Shall be Per City Standard 1306. (1) 7-way Microduct (Duraline - Orange) 13/16mm tubes and (1) 2" HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct. Conduit(s) between ROW hand holes and hand holes on private property shall be 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct. Conduit(s) between ROW hand holes and hand holes on private property shall be 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct.

f) Building Entrance (Multi-family and Commercial) - Design and install fiber optic conduit at a minimum depth of 36 inches. Trenching shall be per City Standard for Commercial Buildings. (1) 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct. Install locate/tracer wires minimum 12AWG within conduit bank and fiber warning tape 18-inch above the uppermost duct.

g) Building Entrance (Single Family) – Design and install 0.75-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct from hand holes on property or hand holes in the ROW. Consult City's Fiber Team for design assistance.

h) Warning Tape - Contractor shall supply and install an approved non-detectable warning tape 18-inch above the uppermost conduit when backfilling trenches, pits or excavations greater than 10' in length. Warning Tape shall be non-detectable, Orange in color, 4-inch minimum width, 4 mil, 500% minimum elongation, with bold printed black letters "CAUTION - BURIED FIBER OPTIC CABLE BELOW" printed in bold black lettering no less than 2-inch high.

i) All hand holes, conduits, conduit banks, materials and installations are per the City's Fiber Optic Master Plan and City Fiber Optic Cable and Duct Standards. All hand holes, conduits and ducts shall be placed in the public right of way. All hand holes will have ¼-inch galvanized wire between the hand holes and the gravel it is placed on.

j) All unused conduits/ducts/microducts shall be protected with duct plugs that provide a positive seal. Ducts that are occupied shall be protected with industry accepted duct seal compound.

k) Locate/Tracer Wire - Conduit bank requires (1) 12AWG high strength (minimum break load 452#) copper-clad steel with 30mil HDPE orange insulation for locate/tracer wire. Contact City's Fiber Team for tracer wire specifications and see note 8.

l) Multi-family and commercial properties shall terminate conduit in an electrical room adjacent to the wall no less than five inches above the finished floor. A 20" width X length 36" space shall be reserved on the plywood wall for OntarioNet equipment. This space shall labeled "OntarioNet Only". Ontario Conduit shall be labeled "OntarioNet"

m) A minimum 1.5-inch joint use telecommunications conduit with pull-rope from the multi-family or commercial building communal telecomm/electrical room/closet to each multi-family or commercial building unit shall be installed. See Structured Wiring Checklist on City's website for additional details.

n) Developer to install 3 inch SCE conduit stub for future City fiber optic meter pedestal within an 8-foot wide, 5-foot deep reserved area for City fiber optic network cabinet. A 3-foot clearance must be maintained around the cabinet and the meter. HH4 shall be placed near the reserved area for cable entrance to network cabinet. The pedestal and network cabinet will be supplied and installed by the City. The service submittal to SCE will be coordinated by the City.

o) Hand holes, conduits and ducts shall be placed in the public right of way.

p) Multi-family dwellings are considered commercial property.

q) Refer to the In-tract Fiber Network Design guideline on the City's website for additional in-tract conduit guidelines.

r) Please contact City's Fiber Team at OntarioNet@ontarioca.gov for conduit design assistance.

s) For additional information please refer to the City's Fiber Optic Master Plan.

t) Structured Wiring – An integrated structured wiring system (low-voltage wiring) provides infrastructure for today's technology applications and the framework for the future technology advances.

u) Requirements and benefits of a structured wiring system include:
- Residential (single-family and multi-family), commercial and industrial developments shall adhere to the City's Structured Wiring ordinance
- Allows for uniform receipt & distribution of technology services
- Ensures scalability of wiring for future technology advances
- Provides consistent & identical wiring protocols throughout developments
- Enables the property infrastructure to interface efficiently with broadband networks for highest bandwidth capacity
- Adoption of these standards will minimize retrofitting required to ensure new property owners are capable of the latest technologies and services.

L. Solid Waste

☒ 2.48 Onsite solid waste shall be designed in accordance with the City's Solid Waste Manual location at: ☐

http://www.ontarioca.gov/municipal-utilities-company/solid-waste

☒ 2.49 Other conditions: ☐

a) For solid waste servicing, No Parking Anytime" signs shall be installed along the curb side of the interior private streets Lots A through E and this condition to be included in the CC&R document.

3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

☒ 3.01 Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer. ☐

☒ 3.02 Complete all requirements for recycled water usage. ☐

☒ 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.

☒ 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.

☒ 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.

☒ 3.03 The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office. ☐

☒ 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a benchmark if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals. ☐

☒ 3.05 Confirm payment of all Development Impact Fees (DIF) to the Building Department. ☐

☒ 3.06 Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studie and reports (i.e. hydrology, traffic, WQMP, fiber optic,etc.). ☐

☒ 3.07 The required OntarioNet fiber optic installations per the approved plans must be completed. ☐

EXHIBIT 'A'

ENGINEERING DEPARTMENT
First Plan Check Submittal Checklist

Project Number: Tract Map No. 20076

The following items are required to be included with the first plan check submittal:

1. ☒ **A copy of this check list**
2. ☒ **Payment of fee for Plan Checking**
3. ☒ **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. ☒ **One (1) copy of project Conditions of Approval**
5. ☒ **Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).**
6. ☐ Three (3) sets of Public Street improvement plan with street cross-sections
7. ☐ Three (3) sets of Private Street improvement plan with street cross-sections
8. ☒ **Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
9. ☒ **Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)**
10. ☒ **Four (4) sets of Public Sewer improvement plan**
11. ☒ **Five (5) sets of Public Storm Drain improvement plan**
12. ☐ Three (3) sets of Public Street Light improvement plan
13. ☐ Three (3) sets of Signing and Striping improvement plan
14. ☒ **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15. ☐ Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)
16. ☒ **Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.**
17. ☒ **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
18. ☒ **One (1) copy of Hydrology/Drainage study**
19. ☒ **One (1) copy of Soils/Geology report**
20. ☒ **Payment for Final Map/Parcel Map processing fee**

21. ☒ **Three (3) copies of Final Map/Parcel Map**

22. ☒ **One (1) copy of approved Tentative Map**

23. ☒ **One (1) copy of Preliminary Title Report (current within 30 days)**

24. ☒ **One (1) copy of Traverse Closure Calculations**

25. ☒ **One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.**

26. ☒ **Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use**

27. ☐ Other Other: Street vacation application to vacate portion of Edison Avenue that needs to be submitted separately for processing

CITY OF ONTARIO

MEMORANDUM

TO: "Vacant", Development Director
Scott Murphy, Planning Director (Copy of memo only)
Cathy Wahlstrom, Principal Planner (Copy of memo only)
Charity Hernandez, Economic Development
Kevin Shear, Building Official
Khoi Do, Assistant City Engineer
Carolyn Bell, Landscape Planning Division
Sheldon Yu, Municipal Utility Company
Doug Sorel, Police Department
Art Andres, Deputy Fire Chief/Fire Marshal
Tom Danna, T. E., Traffic/Transportation Manager
Lorena Mejia, Associate Planner, Airport Planning
Steve Wilson, Engineering/NPDES
Bob Gluck, Code Enforcement Director
Jimmy Chang , IT Department
David Simpson, Development/IT (Copy of memo only)

FROM: Henry Noh, Senior Planner

DATE: January 23, 2017

SUBJECT: FILE #: PMTT17-001 Finance Acct#:

The following project has been submitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by **Monday, February 6, 2017**.

Note:
- ☐ Only DAB action is required
- ☑ Both DAB and Planning Commission actions are required
- ☐ Only Planning Commission action is required
- ☐ DAB, Planning Commission and City Council actions are required
- ☐ Only Zoning Administrator action is required

PROJECT DESCRIPTION: A Tentative Tract Map (TTM 20076) to subdivide 7.65 acres into 62 numbered lots and 29 lettered lots, located on the northwest corner of Edison Avenue and Haven Avenue, within the Avenue Specific Plan. APN: 0218-412-03

☒ The plan does adequately address the departmental concerns at this time.
- ☐ No comments
- ☑ Report attached (1 copy and email 1 copy)
- ☐ Standard Conditions of Approval apply

☐ The plan does not adequately address the departmental concerns.
- ☐ The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

1/30/17

Landscape Planning Carolyn Bell Sr Landscape Planner

Department Signature Title Date

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
303 East "B" Street, Ontario, CA 91764

DAB CONDITIONS OF APPROVAL

Sign Off	
Carolyn Bell Carolyn Bell, Sr. Landscape Planner	1/30/17 Date

Reviewer's Name:	Phone:
Carolyn Bell, Sr. Landscape Planner	**(909) 395-2237**

D.A.B. File No.:	Related Files:	Case Planner:
PMTT17-001		Henry Noh

Project Name and Location:
The Avenue Specific Plan – TM20076
NW corner of Edison Ave and Haven Ave

Applicant/Representative:
BrookCal Ontario LLC – Derek Barbour
3200 Park Center Dr ste 1000
Costa Mesa, CA 92626

☒	**A Tentative Tract Map (dated 1/23/17) has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.**
☐	**A Tentative Tract Map (dated) has not been approved. Corrections noted below are required prior to DAB approval.**

CORRECTIONS REQUIRED

1. Note on grading plans compaction shall be no greater than 85% at landscape areas.
2. Note all finished grades at 1 ½" below finished surfaces.
3. All landscaped slopes shall be max 3:1 and incorporate erosion control jute mesh or erosion control blankets. If 3:1 slopes cannot be achieved then retaining walls shall be incorporated, (lots C, D and section E).
4. Utility meters shall be in front of the side yard fence and located close to the building corner where possible with landscape screening; AC units shall be located in side yards away from windows; trash storage area shall be accessible by gate with a concrete walkway to front. All utilities including vaults and transformers shall be shown on the landscape plans so that hardscape and fencing may be modified and landscape screening provided.
5. Group utilities together at the minimum spacing to allow a 6' wide clear space for street trees 25-30' apart.
6. Typical lot drainage shall include a catch basin with gravel sump below each before exiting property, if no other water quality infiltration is provided.

Landscape construction plans may be emailed to: landscapeplancheck@ontarioca.gov or uploaded to the city's FTP site https://ontwebdav.ci.ontario.ca.us/

CITY OF ONTARIO
MEMORANDUM

TO: **Henry Noh, Senior Planner**
Planning Department

FROM: **Lora L. Gearhart, Fire Protection Analyst**
Fire Department

DATE: **February 14, 2017**

SUBJECT: **PMTT17-001 - A Tentative Tract Map (TT 20076) to subdivide 7.65 acres of land into 62 numbered lots and 29 lettered lots, located at the northwest corner of Edison Avenue and Haven Avenue, within the Avenue Specific Plan (APN: 0218-412-03).**

☒ The plan **does** adequately address Fire Department requirements at this time.

☐ No comments.

☒ Standard Conditions of Approval apply, as stated below.

CONDITIONS OF APPROVAL:

1.0 GENERAL

☒ 1.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov, click on "Fire Department" and then on "Standards and Forms."

☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways

shall be paved with an all-weather surface and shall be a minimum of twenty (20) ft. wide. See Standard #B-004.

☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25') inside and forty-five feet (45') outside turning radius per Standard #B-005.

☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150') in length shall have an approved turn-around per Standard #B-002.

3.0 WATER SUPPLY

☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.

☒ 3.4 The public water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

AIRPORT LAND USE COMPATIBILITY PLANNING
CONSISTENCY DETERMINATION REPORT

ONTARIO AIRPORT PLANNING

Project File No.: PMTT17-001

Address: NWC Edison Ave. & Haven Ave

APN: 218-412-03

Existing Land Use: Vacant Dairy Land

Proposed Land Use: Residential Subdivision of 62 numbered lots and 29 lettered lots

Site Acreage: 7.65 Proposed Structure Height: N/A

ONT-IAC Project Review: N/A

Airport Influence Area: ONT

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Henry Noh

Date: 2/3/2017

CD No.: 2017-003

PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
☐ Zone 1	☐ 75+ dB CNEL	☐ High Terrain Zone	☐ Avigation Easement Dedication
☐ Zone 1A	☐ 70 - 75 dB CNEL	☑ FAA Notification Surfaces	☐ Recorded Overflight Notification
☐ Zone 2	☐ 65 - 70 dB CNEL	☐ Airspace Obstruction Surfaces	☑ Real Estate Transaction Disclosure
☐ Zone 3	☐ 60 - 65 dB CNEL	☐ Airspace Avigation Easement Area	
☐ Zone 4			
☐ Zone 5		Allowable Height: 200 ft +	

The project is impacted by the following Chino ALUCP Safety Zones:

☐ Zone 1 ☐ Zone 2 ☐ Zone 3 ☐ Zone 4 ☐ Zone 5 ☐ Zone 6

Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: ☐ Exempt from the ALUCP ☐ Consistent ● Consistent with Conditions ☐ Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

See Attached

Airport Planner Signature: Lorena Mejia

PROJECT CONDITIONS

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the Airport Land Use Compatibility Plan (ALUCP) for ONT. The applicant is required to meet the Real Estate Transaction Disclosure in accordance with California Codes (Business and Professions Code Section 11010-11024). New residential subdivisions within an Airport Influence Area are required to file an application for a Public Report consisting of a Notice of Intention (NOI) and a completed questionnaire with the Department of Real Estate and include the following language within the NOI:

NOTICE OF AIRPORT IN VICINITY
This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.

CITY OF ONTARIO

MEMORANDUM

TO: "Vacant", Development Director
Scott Murphy, Planning Director (Copy of memo only)
Cathy Wahlstrom, Principal Planner (Copy of memo only)
Charity Hernandez, Economic Development
Kevin Shear, Building Official
Khoi Do, Assistant City Engineer
Carolyn Bell, Landscape Planning Division
Sheldon Yu, Municipal Utility Company
Doug Sorel, Police Department
Art Andres, Deputy Fire Chief/Fire Marshal
Tom Danna, T. E., Traffic/Transportation Manager
Lorena Mejia, Associate Planner, Airport Planning
Steve Wilson, Engineering/NPDES
Bob Gluck, Code Enforcement Director
Jimmy Chang , IT Department
David Simpson, Development/IT (Copy of memo only)

FROM: Henry Noh, Senior Planner

DATE: January 23, 2017

SUBJECT: FILE #: PMTT17-001 Finance Acct#:

The following project has been submitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by **Monday, February 6, 2017.**

Note:
- ☐ Only DAB action is required
- ☑ Both DAB and Planning Commission actions are required
- ☐ Only Planning Commission action is required
- ☐ DAB, Planning Commission and City Council actions are required
- ☐ Only Zoning Administrator action is required

PROJECT DESCRIPTION: A Tentative Tract Map (TTM 20076) to subdivide 7.65 acres into 62 numbered lots and 29 lettered lots, located on the northwest corner of Edison Avenue and Haven Avenue, within the Avenue Specific Plan. APN: 0218-412-03

☒ The plan does adequately address the departmental concerns at this time.
- ☐ No comments
- ☐ Report attached (1 copy and email 1 copy)
- ☒ Standard Conditions of Approval apply

☐ The plan does not adequately address the departmental concerns.
- ☐ The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

POLICE	DOUGLAS SOREL	MGMT ANALYST	1/30/17
Department	Signature	Title	Date

CITY OF ONTARIO
MEMORANDUM

TO: PLANNING DEPARTMENT, Henry Noh

FROM: BUILDING DEPARTMENT, Kevin Shear

DATE: January 25, 2017

SUBJECT: PMTT17-001

☒ 1. The plan **does** adequately address the departmental concerns at this time.
No comments.

KS:lm

PLANNING COMMISSION STAFF REPORT

July 25, 2017

SUBJECT: A Tentative Tract Map (File No. PMTT17-007/TT17624) to subdivide 3.47 acres of land into 31 single family lots and common areas, in conjunction with a Development Plan (File No. PDEV17-022) to construct a 31 single family homes (Cluster Product) and a Variance (File No. PVAR17-007) to deviate from the minimum building arterial street setback, along Mission Boulevard, from 30 feet to 5 feet. The project is located on the south side of Mission Boulevard, between San Antonio and Oakland Avenues, within the MDR-11 (Low-Medium Density Residential 5.1 to 11.0 du/ac) zoning district: **submitted by North by Northwest Capital Inc.**

PROPERTY OWNER: City of Ontario Housing Authority

RECOMMENDED ACTION: That the Planning Commission approve File Nos. PMTT17-007, PVAR17-007 and PDEV17-022, pursuant to the facts and reasons contained in the staff report and attached resolutions, and subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The project site is comprised of 3.47 acres of land located on the south side of Mission Boulevard, between San Antonio and Oakland Avenues, within the MDR-11 (Low-Medium Density Residential) zoning district, and is depicted in ***Figure 1: Project Location***, below. The project consists of five vacant parcels of land. The project site is surrounded by residential and commercial development to the north (across Mission Boulevard) and single-family residential development on the south, east and west.

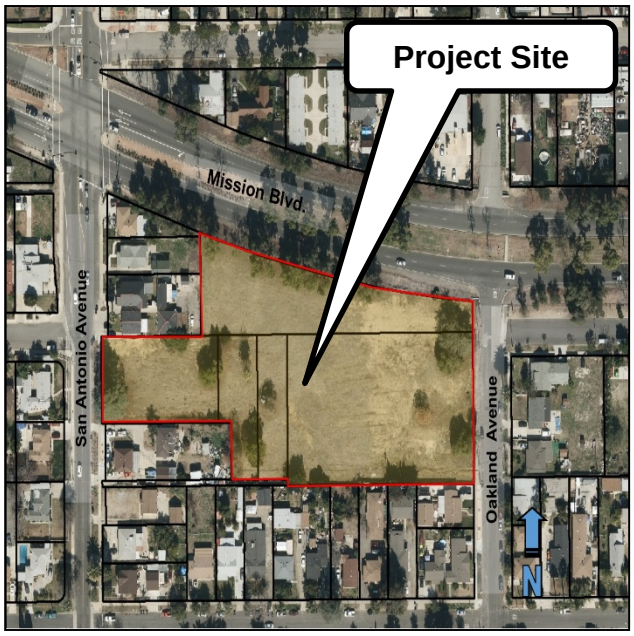


Figure 1: Project Location

PROJECT ANALYSIS:

[1] Background — The project site is owned by the City of Ontario Housing Authority. The Applicant has entered into a Purchase and Sale Agreement with the

Case Planner:	Luis Batres, Senior Planner
Planning Director Approval:	
Submittal Date:	05/12/2017
Hearing Deadline:	11/12/2017

Hearing Body	*Date*	*Decision*	*Action*
DAB	07/17/2017	Approved	Recommend
ZA			
PC	07/25/2017		Final
CC			

Ontario Housing Authority to purchase the property and develop it with market rate, single-family homes. The proposed project was previously approved by the Planning Commission in September of 2005. However, due to the economic downturn shortly after the entitlement approval, construction permits were never acquired and the entitlements expired.

[2] Site Design/Building Layout — The Development Plan (File No. PDEV07-022) proposes to construct 31 single family detached homes. A Tentative Tract Map (File No. PMTT17-007/ TT 17624) to subdivide 3.47 acres of land into 31 single family lots and common area and a Variance application (File No. PVAR17-007) to deviate from the minimum building arterial street setback, along Mission Boulevard, from 30 feet to 5 feet has been filed in conjunction with the Development Plan application.

The Development Plan proposes 21 units configured in a six-pack courtyard cluster design and 10 single family conventional units at a density of 9.0 dwelling units per acre. The six-pack courtyard cluster units are designed with the front of the units facing onto a common courtyard that provides front entry access into each unit via pedestrian walkway. Three of the cluster units will face onto Oakland Avenue with front entry access from a walkway to the public sidewalk. In addition, each cluster courtyard unit will have direct garage access from a private drive at the rear of each unit. The 10 single family conventional units are situated in a more traditional way, with the home's front yard and garage access taken from the main east-west private drive aisle. All units will have front entry access from a common courtyard, the common open space area (park) or by the public sidewalks on San Antonio Avenue and Oakland Avenue.

The project includes three 2-story floor plans with up to four architectural styles per plan. The three floor plans include the following:

- Plan 1: 1,611 square feet, 3 bedrooms and 21/2 baths.
- Plan 2: 1,688 & 1,696 square feet, 3 bedrooms and 21/2 baths.
- Plan 3: 1,969 square feet, 5 bedrooms and 3 baths.

The proposed Development Plan is designed to be pedestrian friendly. This is accomplished through the use of courtyards, pedestrian pathways, landscaping, and common areas, including a tot lot. A series of pathways are provided to accommodate project-wide pedestrian access to the community. Aesthetic consideration to the overall design is shown through key placement and orientation of the dwelling units to emphasize the unique characteristics of each architectural theme.

[3] Site Access/Circulation — Access to the community is provided by a 26-foot wide private drive aisle extending east to west through the site from San Antonio Avenue to Oakland Avenue. Additionally, four north-south private drives (alleyways) will provide garage access to each individual dwelling unit of the cluster courtyard product.

[4] Parking — An enclosed two-car garage is provided for each of the 31 dwelling units. Guest parking is required to be provided at 1 space per every 4 units. As demonstrated in the parking summary table below, the project provides 62 dedicated enclosed garage parking spaces for each unit and 23 spaces allocated for guest parking, which exceeds the minimum parking requirements by 15 parking spaces.

Summary of Parking Table						
Number of Units	**Req. Parking Per unit**	**Req. Guest Parking**	**Total Parking Req.**	**Garage Spaces Provided**	**Guest Parking Provided**	**Total Provided**
31	2 spaces within a Garage	1 space per 4 units	70	62	23	85
Total			**70**	62	23	**85**

[5] Architecture — The proposed architectural design of the homes reflects function and tradition, simplicity in the massing plan and roof forms, and authenticity of homes found within Ontario's historic neighborhoods and newer development in the city. The proposed architectural styles include Spanish, Monterey, and Tuscan. The styles are unique from each other and were chosen to complement one another through the overall scale, massing, proportions, and details. Each architectural style will include the following details:

Spanish: A low and shallow-pitched "S" tile cross gable roof with an intersecting front hipped gable, cement plaster exterior sand finish, square windows, stucco surrounds with detailed window header and sill, arched entry opening, wrought-iron Juliet and details, fabric awnings above key windows, metal spire on gable roof peak pot shelves, and decorative vents below gables.

Spanish Elevation

Monterey: A low and shallow-pitched “S” tile cross hipped roofs, second story balcony on front elevation with detailed wood post and railing, square windows with window header and sill details, wood shutters at key windows, decorative wrought-iron detail on key windows, arched front entry opening and decorative vents below gables.

Monterey Elevation

Tuscan: A low and hipped shallow-pitched cross gable “S” tile roof with an intersecting front hipped or open gable, cement plaster exterior sand finish, square windows with stucco surrounds with detailed window header and sill, wood shutters at key windows, corbels and decorative vents below gables.

Tuscan Elevation

[6] Landscaping — The Development Plan features sidewalks separated by landscaped parkways, which provides visual interest and promotes pedestrian mobility. Mission Boulevard will feature a 5-foot wide curb adjacent sidewalk with 12 wide landscaped parkway. The San Antonio Avenue frontage will be improved with 5-foot sidewalk separated by a 5’ foot parkway. The Oakland Avenue frontage will be improved with a 5-foot sidewalk separated by a 12’ foot parkway. The conventional single family homes and the cluster courtyard products that front onto the private drive aisle or public

street will be provided with front lawn landscaping (lawn, shrubs and trees) and an automatic irrigation system to be installed by the developer and maintained by the Home Owners Association (HOA).The front entry courtyard of the cluster product will feature arbors with decorative pilaster columns at each entry from the sidewalk on the private drive. The courtyard will feature pedestrian walkways to each unit and extensive landscaping (shrubs and a series of accent 24", 36" and 48" box accent trees) and an automatic irrigation system to be maintained by the Home Owners Association. The homeowner will be responsible for side and rear yard landscape improvements. In addition, a 6-foot high decorative slumpstone wall with pilasters is proposed along the northern boundary of the site along Mission Boulevard, the southern interior boundary, and at key areas along the San Antonio and Oakland Avenues.

The private drives that service the cluster court products will be required to be designed to provide a comfortable sense of function and character within the neighborhood. To accomplish this, all private cluster courtyard drive aisles will be required to be enhanced with decorative interlocking pavers, textured and color pigmented concrete or stamped concrete and subject to Planning Department review and approval.

The cluster courtyard drive aisles will be enhanced with 5- foot wide landscaped planters and pockets between garages. Accent tress (24" to 36" box min.) will be planted within the planters and pockets to augment the architecture along the drive aisle. Entries into the private drive aisles will be enhanced with accent trees, consistent within the overall streetscape feel of the community. Lighting will be provided to maintain visibility and greater security for the residents.

The Development Plan also includes the construction of a 0.2-acre (9,144 s. ft.) park located within the southern area of the community. The park features a tot-lot, open turf play area, picnic tables, and a circular walkway around park and turf areas. The park will be landscaped with accent shade trees, shrubs, turf and decomposed granite (DG). Each home will have dedicated private open space, ranging in size from 377 to 1,585 square feet, in the form of backyard yards. A series of common area pathways are provided to accommodate project-wide pedestrian access to the park and each home.

[7] Tentative Tract Map — In conjunction with the Development Plan application, the applicant has submitted a Tentative Tract Map to subdivide 3.47 acres of land into 31 single family lots and common area. The residential lots range in size from 1,529 to 2,607 square feet. The proposed lots are consistent with the MDR-11 (Low-Medium Density Residential) zoning district and the Development Code standards *Table 6.01C: Small Lot Single Family Development Standards – Cluster Subdivisions.* The Tentative Tract Map will facilitate the construction of sidewalks and parkways along Mission Boulevard, San Antonio Avenue and Oakland Avenue. Additionally, all streets within the project area are classified as "Private Drives" will be maintained by the Home Owners Association (HOA). All remaining areas within the tract boundary will be dedicated as common space and be maintained in accordance with the (HOA) agreement.

[8] CC&R's — As a Condition of Approval, staff will require that CC&R's be prepared and recorded with the final map. The CC&R's will outline the maintenance responsibilities for the open space areas, recreation amenities, drive aisles, utilities and upkeep of the entire site to ensure the on-going maintenance of the common areas and facilities.

[9] Utilities (drainage, sewer) — Public utilities (water and sewer) are available to serve the project. The site drains north to south. The Applicant has submitted a Preliminary Water Quality Management Plan (PWQMP), which establishes the project's compliance with storm water discharge/water quality requirements. The project will be required to comply with low impact development (LID) best management practices (BMPs), such as retention and infiltration and evapotranspiration.

[10] Variance — The Applicant is requesting a Variance to deviate from the minimum building arterial street setback along Mission Boulevard from 30 feet to 5 feet. The project site is narrow in depth with elongated angled width and bounded by Mission Avenue to the north, Oakland Avenue to east and San Antonio Avenue to the west. The project site configuration poses a challenge in effectively planning the site and providing proper access. Requiring a 30 foot building setback along the Mission Boulevard would impact the project sites ability to achieve a well planned development and provide a product that is consistent with the density and scale of the surrounding residential development.

Mission Boulevard, which bounds the project site to the north, is a major arterial and access into the project site is not permitted. Restricting access from Mission Boulevard requires the project to provide a 26-foot wide, east-west private drive, extending from San Antonio Avenue to Oakland Avenue, to adequately serve the community. Requiring the buildings along Mission Boulevard to be setback 30 feet from the project property line would require the private drive, serving the site, to be pushed south impacting and eliminating 10 proposed single-family homes. The proposed project, with the Variance request, has demonstrated a very well planned residential community that will provide much needed housing in the City. The proposed Development Plan is compatible with surrounding existing single-family and multi-family residential developments to the east and west of the project site, which have varied setbacks between 5 to 20 feet along Mission Boulevard. The project proposes varied building setbacks of 5 feet to 20.6 feet along Mission Boulevard, with average setback of 12 feet. In addition, there will be a 17-foot landscape buffer and parkway along the north boundary of the site between the property line and the curb line on Mission Boulevard.

Staff believes that the Variance request is consistent with The Ontario Plan (TOP) Goal LU3, which promotes flexibility in order to respond to special conditions and circumstances in order to achieve the Vision of providing housing opportunities for all sectors of our community. In acting on a Variance request, the Planning Commission must consider and clearly establish certain findings of fact, which are prescribed by State

law and the City's Development Code. The following facts and findings have been provided as basis for approval of the requested Variance:

1) ***The strict or literal interpretation and enforcement of the specified regulation would result in practical difficulty or unnecessary physical hardship inconsistent with the objectives of the development regulations contained in this Development Code.*** The project site is narrow in depth with elongated angled width and bounded by Mission Avenue to the north, Oakland Avenue to east and San Antonio Avenue to the west. The project site configuration poses a challenge in effectively planning the site and providing proper access. Requiring a 30 foot building setback along the Mission Boulevard would impact the project sites ability to achieve a well planned development and provide a product that is consistent with the density and scale of the surrounding residential development. Mission Boulevard, which bounds the project site to the north, is a major arterial and, therefore, access into the project is not permitted. Prohibiting access from Mission Boulevard requires the project site to provide a 26-foot wide, east-west private drive extending from San Antonio Avenue to Oakland Avenue to adequately serve the community. Requiring the buildings along Mission Boulevard to be setback 30 feet from the project property line would require the private drive serving the site to be pushed south impacting and eliminating 10 proposed single-family homes. The proposed Development Plan is compatible with surrounding existing single-family and multi-family residential developments to the east and west of the project site, which have varied setbacks between 5 to 20 feet along Mission Boulevard. The project proposes varied building setbacks of 5 feet to 20.6 feet along Mission Boulevard, with average setback of 12 feet. The Variance request is consistent with The Ontario Plan (TOP) Goal LU3, which promotes flexibility in order to respond to special conditions and circumstances in order to achieve the Vision of providing housing opportunities for all sectors of our community. Therefore, the strict or literal interpretation and enforcement of the specified regulation would result in practical difficulty or unnecessary physical hardship inconsistent with the objectives of the development regulations contained in the Development Code; and

2) ***There are exceptional or extraordinary circumstances or conditions applicable to the property involved, or to the intended use of the property, that do not apply generally to other properties in the vicinity and in the same zoning district.*** The project site is narrow in depth with elongated angled width and bounded by Mission Avenue to the north, Oakland Avenue to east and San Antonio Avenue to the west. The project site configuration poses a challenge in effectively planning the site and providing proper access. Requiring a 30 foot building setback along the Mission Boulevard would impact the project sites ability to achieve a well planned development and provide a product that is consistent with the density and scale of the surrounding residential development. The proposed Development Plan is compatible with surrounding existing single-family and multi-family residential developments to the east and west of the project site, which have varied setbacks between 5 to 20 feet along Mission Boulevard. In addition, very few properties within the same MDR-11 zoning district are

not impacted by a major arterial, such as Mission Boulevard, and subject to a 30 foot rear or interior side setback. Therefore a variance is necessary to meet development standards as granted on other properties in the same zone; and

3) ***The strict or literal interpretation and enforcement of the specified regulation would deprive the applicant of privileges enjoyed by the owners of other properties in the same zoning district.*** The requested relief from 30 to 5 feet along Mission Boulevard will allow for greater design flexibility and will serve to equalize development rights between the applicant and owners of property in the same zoning district, located within the area of the project site. In addition, very few properties within the same MDR-11 zoning district are impacted by a major arterial, such as Mission Boulevard, and subject to a 30 foot rear or interior side setback. The setback deviation of 5 feet will allow for the substantial improvement and utilization of the otherwise challenging site. The strict and literal interpretation and enforcement of the specified regulation would deprive the applicant of privileges enjoyed by owners of other properties in the same zone; and

4) ***The granting of the Variance will not be detrimental to the public health, safety or welfare, or be materially injurious to properties or improvements in the vicinity.*** A thorough review and analysis of the proposed Variance and its potential to adversely impact properties surrounding the subject site was completed by staff. As a result of this review, certain design considerations will be incorporated into the project as conditions of approval, to address identified impacts to a less-than-significant level, including the use of upgraded materials, the inclusion of certain architectural design elements on building exteriors, intensified landscape elements, and decorative paving; and

5) ***The proposed Variance is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and the purposes of any applicable specific plan or planned unit development, and the purposes of this Development Code.*** The proposed Project is located with the Low Medium Density Residential (5.1 – 11.0 du/ac) land use district of the Policy Plan Land Use Map and the MDR-11 (Low-Medium Density Residential 5.1 to 11.0 du/ac) zoning district. The development standards and conditions under which the proposed Project will be constructed and maintained is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)
- Ensure the Development of a Well Planned, Balanced, and Self-Sustaining Community in the New Model Colony

[2] Vision.

Distinctive Development:

- Commercial and Residential Development

➢ Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

[3] Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.

➢ G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

[4] Policy Plan (General Plan)

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.

➢ LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.

➢ LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of

choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).

- Goal LU2: Compatibility between a wide range of uses.

  - LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

Housing Element:

- Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

  - H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.

Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.

Community Economics Element:

- Goal CE1: A complete community that provides for all incomes and stages of life.

  - CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

  - CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

  - CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

➢ CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

➢ CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.

➢ S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

- Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➢ CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

➢ CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

➢ CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

- Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

➢ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;

- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➢ CD2-2 Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the "outdoor living room"), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb.

➢ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➢ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➢ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➢ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➢ CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use

areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

- CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

- Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

- CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

- Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

- CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.

- CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The project site is located within the Airport Influence Area of the Ontario International Airport (ONT), and has been found to be consistent with the policies and criteria set forth within the ALUCP for ONT.

ENVIRONMENTAL REVIEW: The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Sections 15305 (Class 5, Minor Alterations in Land Use Limitations) and 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	Existing Land Use	*General Plan Designation*	*Zoning Designation*	*Specific Plan Land Use*
Site:	Vacant	Low Medium Density Residential (5.1 – 11.0 du/ac)	MDR-11 (Low-Medium Density Residential)	N/A
North:	Residential/Commercial Uses	Business Park	Business Park	N/A
South:	Single-Family Residential	Low-Density Residential (2.1-5.0 du/ac)	LDR-5 (Low-Density Residential)	N/A
East:	Single-Family Residential	Low-Density Residential (2.1-5.0 du/ac)	LDR-5 (Low-Density Residential)	N/A
West:	Single-Family Residential	Low Medium Density Residential (5.1 – 11.0 du/ac)	MDR-11 (Low-Medium Density Residential))	N/A

Off-Street Parking:

Summary of Parking Table						
Number of Units	**Req. Parking Per unit**	**Req. Guest Parking**	**Total Parking Req.**	**Garage Spaces Provided**	**Guest Parking Provided**	**Total Provided**
31	2 spaces within a Garage	1 space per 4 units	70	62	23	85
Total			**70**	62	23	**85**

General Site & Building Statistics

Item	*Required Min./Max.*	*Provided (Ranges)*	*Meets Y/N*
Project area (in acres):	1-acre.	3.47-acres	Y
Maximum project density (dwelling units/ac):	5.1 to 11.0	9.0	Y
Maximum coverage (in %):	N/A	N/A	
Minimum lot size (in SF):	N/A	N/A	
Minimum lot depth (in FT):	N/A	N/A	
Minimum lot width (in FT):	N/A	N/A	

Item	*Required Min./Max.*	*Provided (Ranges)*	*Meets Y/N*
Maximum height (in FT):	35’	23’-9”	
Open space – private:	6,200 Sq. Ft.	23,100	Y
Open space – common:	9,300 Sq. Ft.	50,100 Sq. Ft. (Park 9,144 Sq. Ft.)	Y

Dwelling Unit Count:

Item	*Required Min./Max.*	*Provided (Ranges)*	*Meets Y/N*
Total no. of units	17 to 38	31	Y

Dwelling Unit Statistics:

Unit Type	*Size (in SF)*	*No. Bedrooms*	*No. Bathrooms*	*No. Stories*
Plan 1	1.611	3	21/2	2
Plan 2	1,688 & 1,696	3	2/1/2	2
Plan 3	1,969	5	3	2

Tentative Tract Map Summary:

Item	*Proposed*
Project area (acres):	3.47
Number of lots	31
Number of Lettered Lots	4
Minimum Lot Size	1,529 Sq. Ft,
Maximum Lot Size	2,607 Sq. Ft.
Average Lot Size	1,943 Sq. Ft.

Exhibit A- Site Plan

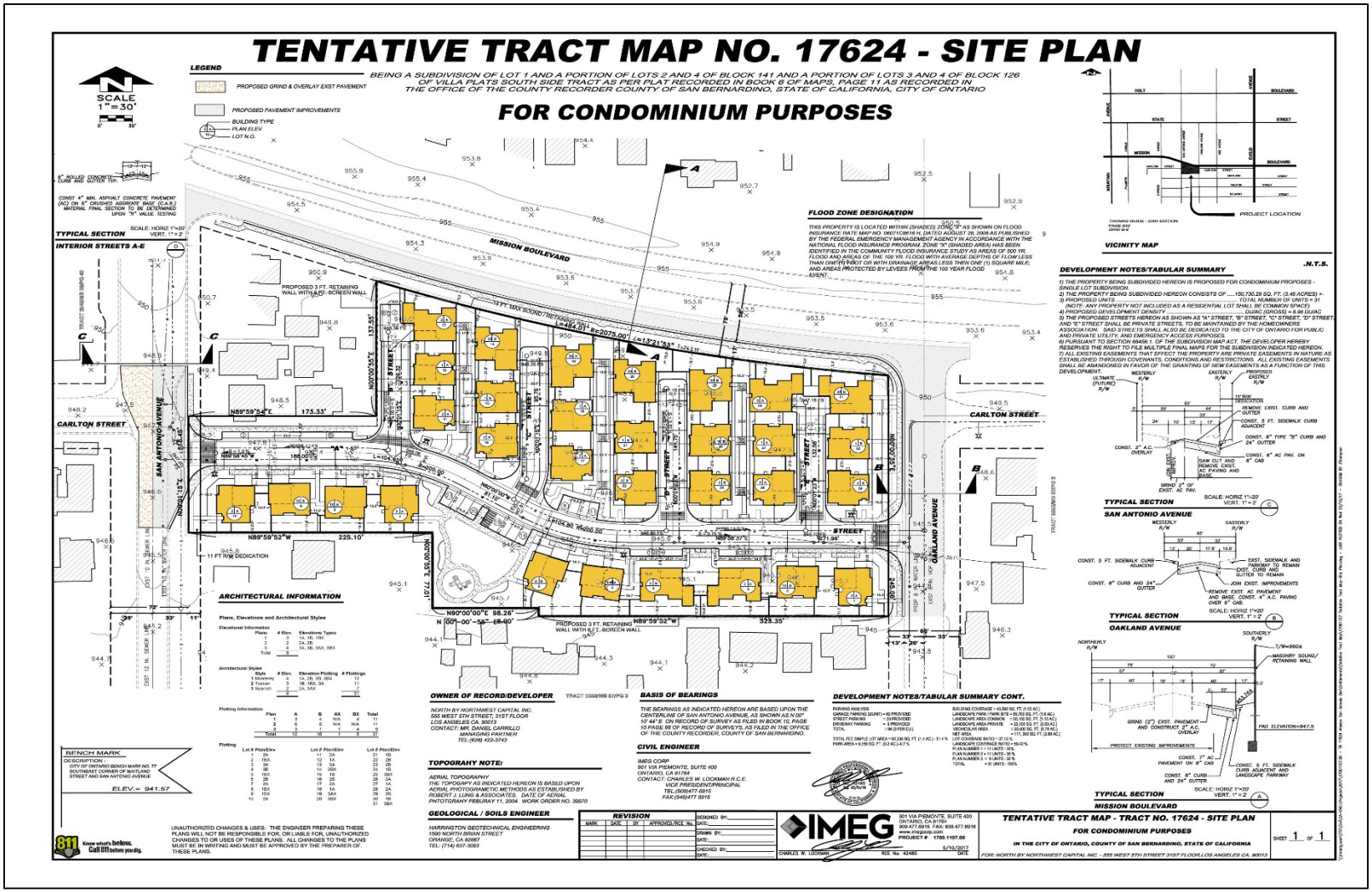


Exhibit A- Tentative Tract Map

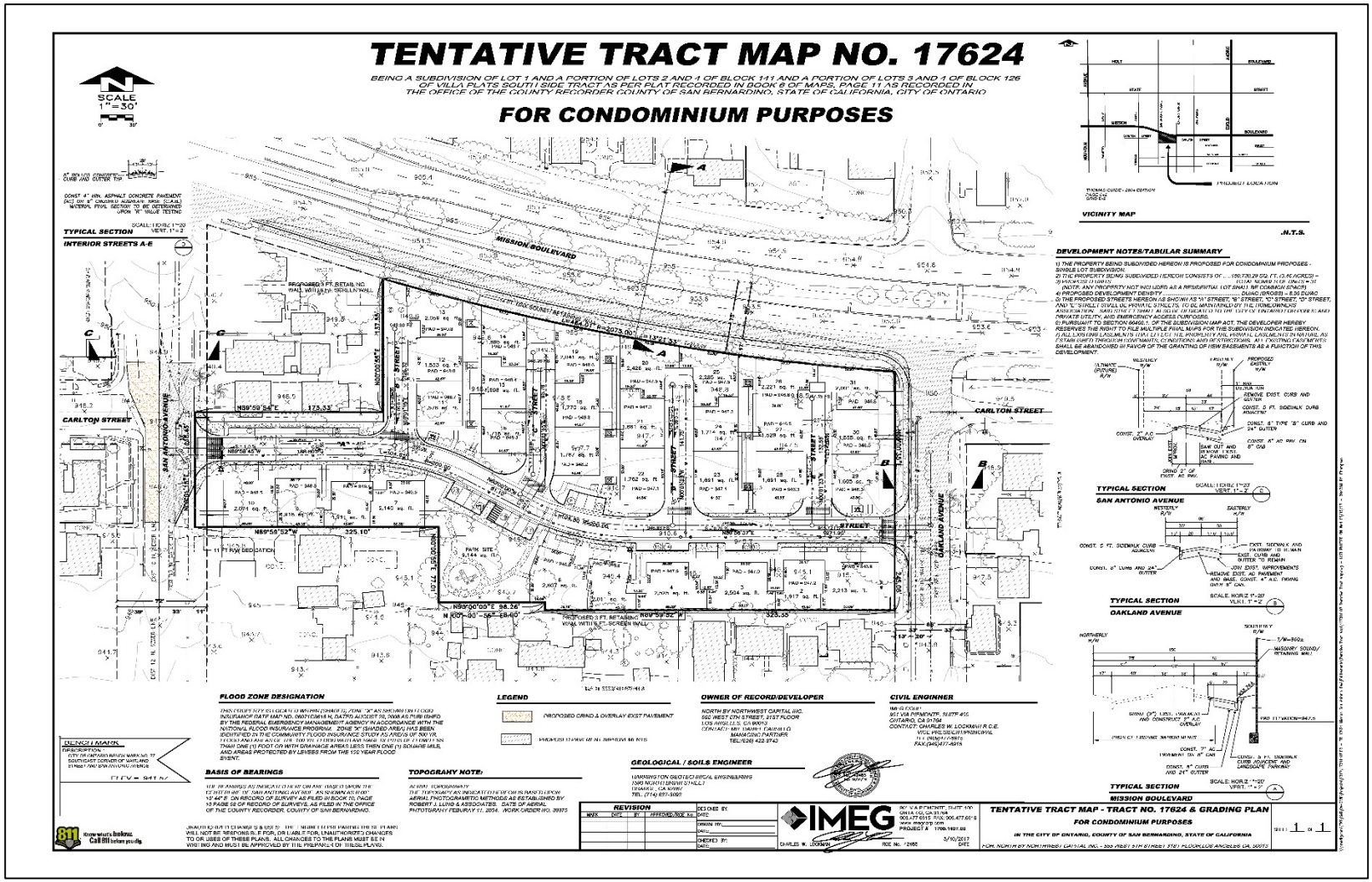
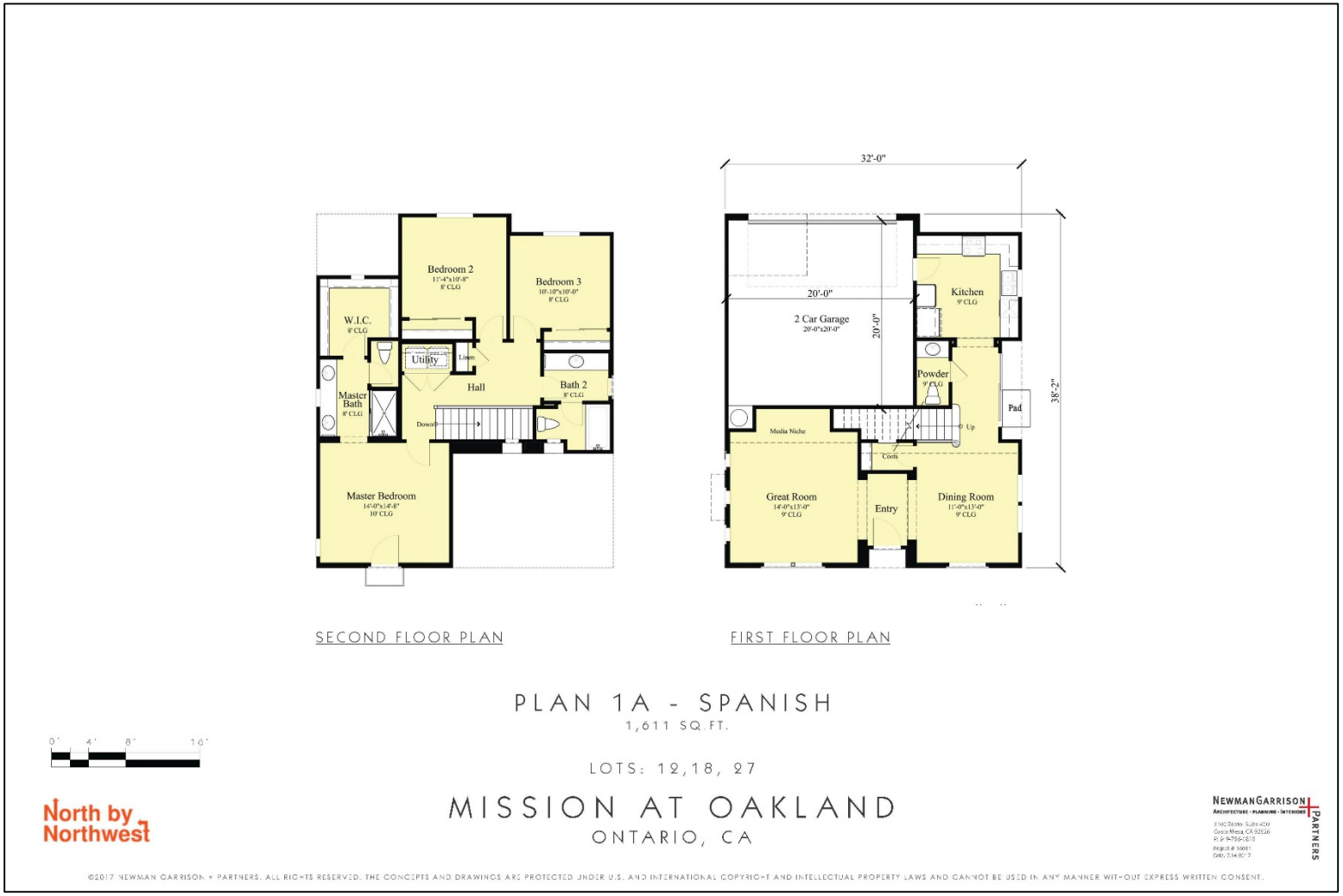


Exhibit C – Landscape Plan

Exhibit D – Floor Plan and Elevations Plan 1



CEILING LINE
FENCE LINE
RIGHT
GABLE END DETAIL
SCALE: N.T.S
REAR
Material Key:
① Concrete 'S' Tile Roof
② 7/8" Cement Plaster with Sand Finish
③ Stucco on Foam Trim
④ Metal Spire
⑤ Decorative Gable End Detail
⑥ Fabric Awning
⑦ Wrought Iron Detail
⑧ Sectional Garage Door
CEILING LINE
8'-0" HDR
FENCE LINE
LEFT
NOTE: THIS ELEVATION LEFT INTENTIONALLY BLANK AS IT FACES THE NEIGHBORING YARD
ROOF PLAN
SCALE: 1/2" = 1'-0"
8'-0" HDR
FRONT
0' 4' 8' 16'
PLAN 1A - SPANISH ELEVATIONS
COLOR SCHEME 1
LOTS: 12, 18, 27
MISSION AT OAKLAND
ONTARIO, CA
North by Northwest
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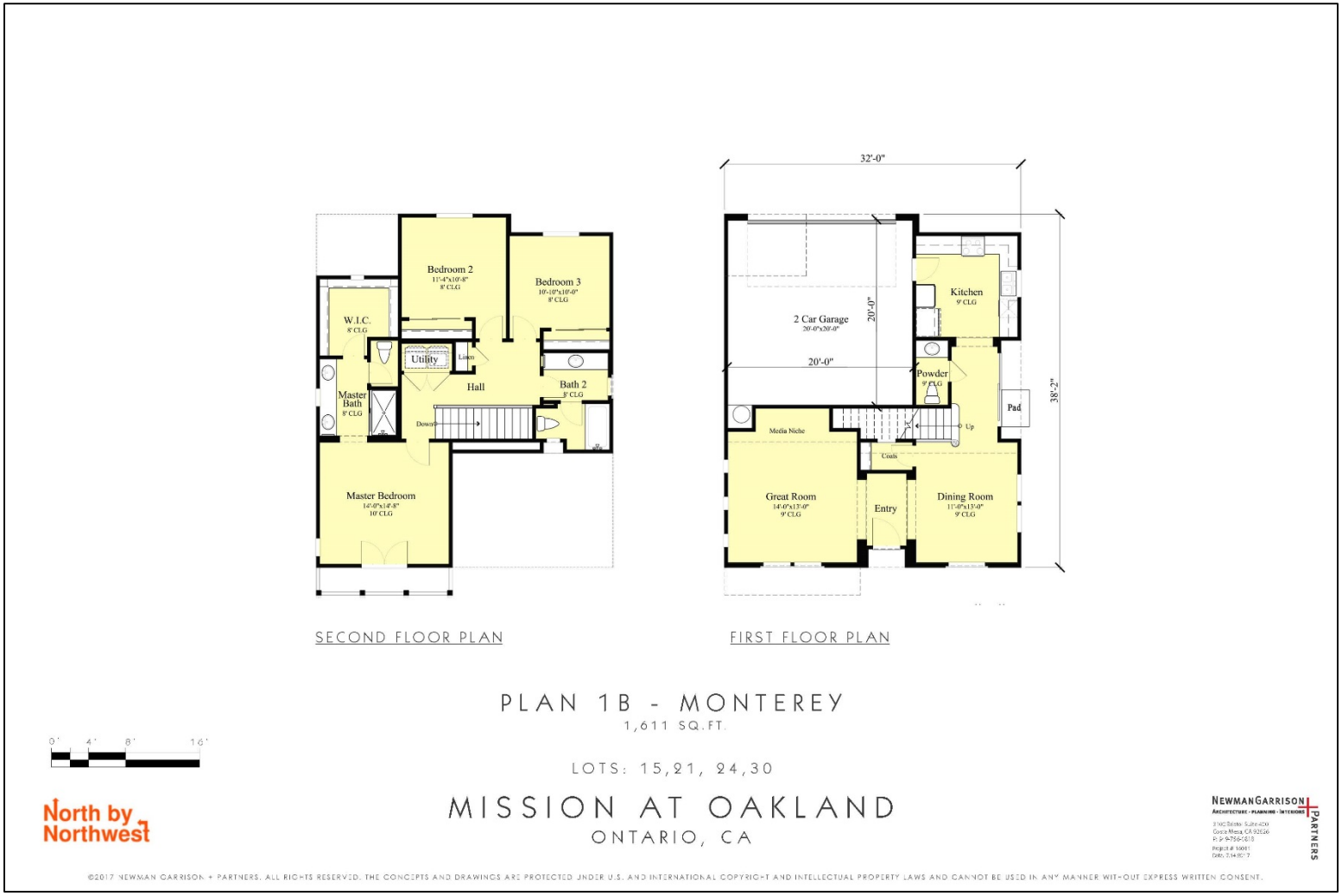
Bedroom 2
11'-4"x10'-8"
8' CLG
Bedroom 3
10'-0"x10'-0"
8' CLG
W.I.C.
8' CLG
Utility
Linen
Hall
Bath 2
8' CLG
Master Bath
8' CLG
Down
Master Bedroom
14'-0"x14'-8"
10' CLG
32'-0"
2 Car Garage
20'-0"x20'-0"
20'-0"
20'-0"
Kitchen
9' CLG
Powder
9' CLG
Pad
38'-2"
Media Niche
Up
Coats
Great Room
14'-0"x13'-0"
9' CLG
Entry
Dining Room
11'-0"x13'-0"
9' CLG
SECOND FLOOR PLAN
FIRST FLOOR PLAN
PLAN 1B - MONTEREY
1,611 SQ.FT.
LOTS: 15,21, 24,30
MISSION AT OAKLAND
ONTARIO, CA
0'
4'
8'
16'
North by Northwest
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RIGHT
FENCE LINE
GABLE END DETAIL
SCALE: N.T.S.
ROOF PLAN
REAR
Material Key:
① Concrete 'Flat' Tile Roof
② 7/8" Cement Plaster with Sand Finish
③ Stucco on Foam Trim
④ Decorative Wood Shutters
⑤ Decorative Gable End Detail
⑥ Decorative Metal Gate
⑦ Sectional Garage Door
⑧ Wood Post & Railing
⑨ Wood Beam
LEFT
FENCE LINE
FRONT
PLAN 1B - MONTEREY ELEVATIONS
COLOR SCHEME 5
LOTS: 15,21,24,30
MISSION AT OAKLAND
ONTARIO, CA
North by Northwest
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SECOND FLOOR PLAN

FIRST FLOOR PLAN

PLAN 2A - TUSCAN

1,696 SQ.FT.

LOTS: 1,7,10,11,17,28

MISSION AT OAKLAND

ONTARIO, CA

0' 4' 8' 16'

North by Northwest

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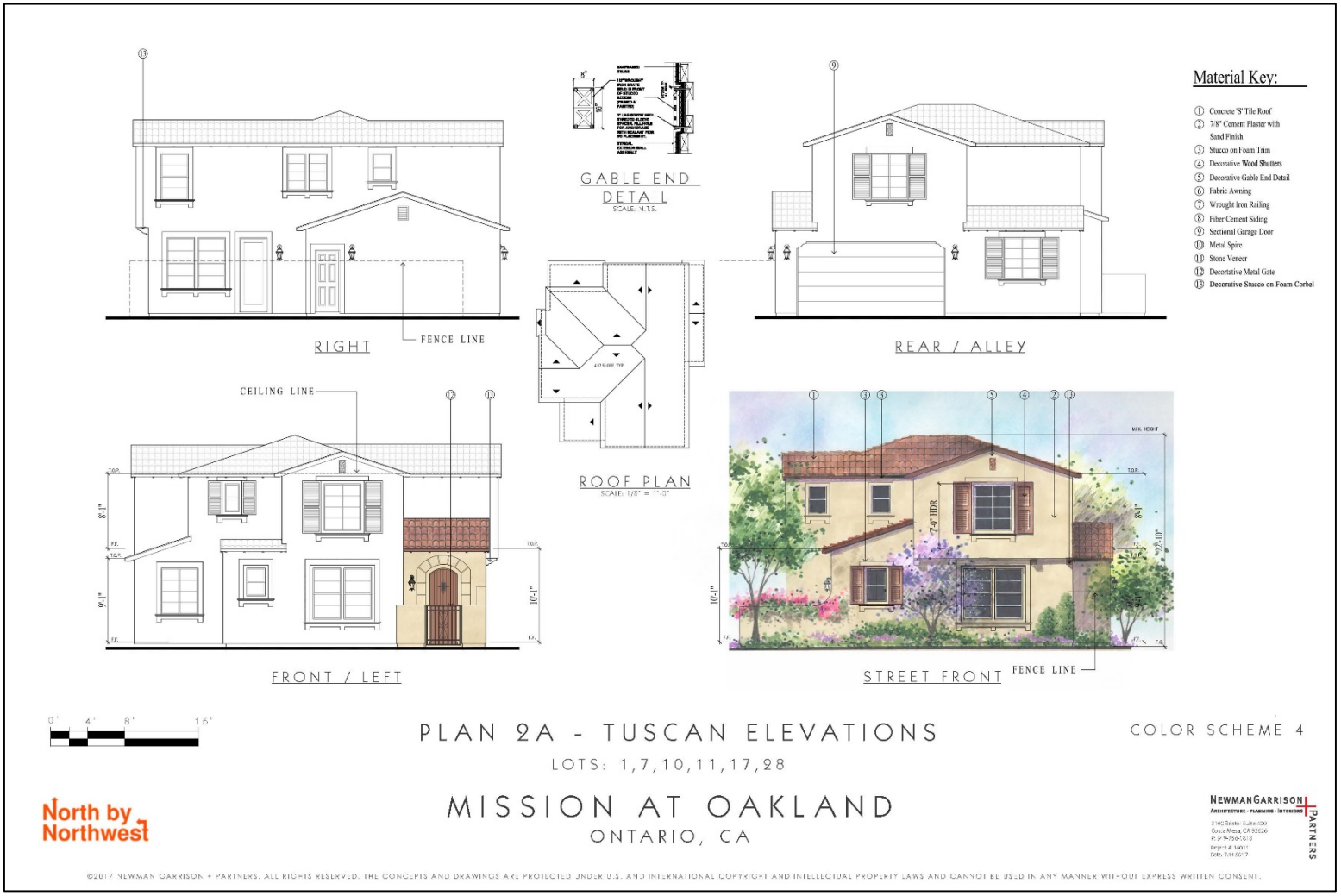
GABLE END DETAIL
SCALE: N.T.S.
RIGHT
FENCE LINE
REAR / ALLEY
ROOF PLAN
CEILING LINE
FRONT / LEFT
STREET FRONT
FENCE LINE
Material Key:
1 Concrete 'S' Tile Roof
2 7/8" Cement Plaster with Sand Finish
3 Stucco on Foam Trim
4 Decorative Wood Shutters
5 Decorative Gable End Detail
6 Fabric Awning
7 Wrought Iron Railing
8 Fiber Cement Siding
9 Sectional Garage Door
10 Metal Spire
11 Stone Veneer
12 Decorative Metal Gate
13 Decorative Stucco on Foam Corbel
0' 4' 8' 16'
PLAN 2A - TUSCAN ELEVATIONS
LOTS: 1,7,10,11,17,28
COLOR SCHEME 4
MISSION AT OAKLAND
ONTARIO, CA
North by Northwest
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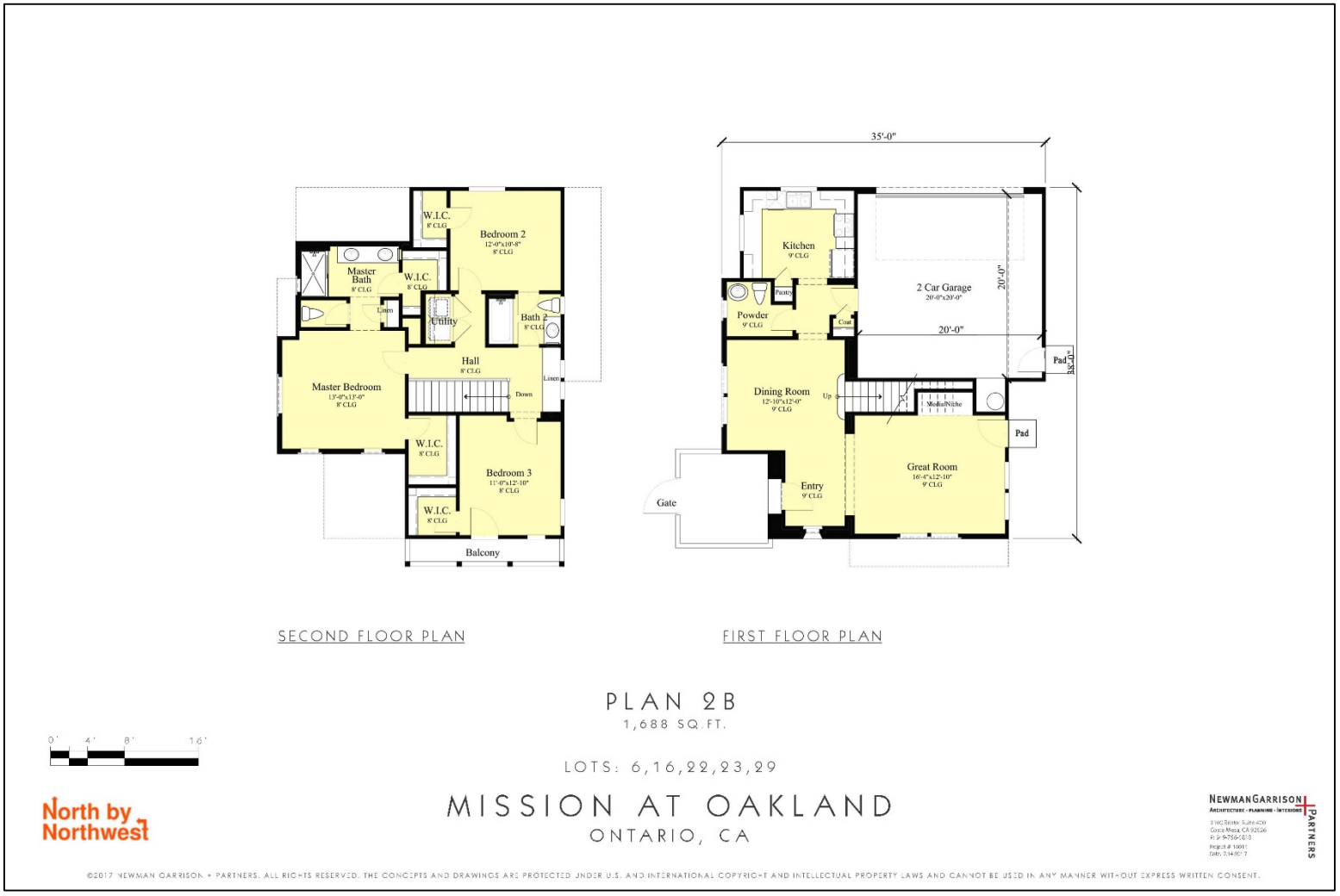
W.I.C.
8' CLG
Bedroom 2
12'-0"x10'-8"
8' CLG
Master Bath
8' CLG
W.I.C.
8' CLG
Linen
Utility
Bath 2
8' CLG
Hall
8' CLG
Master Bedroom
13'-0"x13'-0"
8' CLG
Down
Linen
W.I.C.
8' CLG
Bedroom 3
11'-0"x12'-10"
8' CLG
W.I.C.
8' CLG
Balcony
35'-0"
Kitchen
9' CLG
Pantry
2 Car Garage
20'-0"x20'-0"
20'-0"
Powder
9' CLG
Coat
20'-0"
Pad
Dining Room
12'-10"x12'-0"
9' CLG
Up
Media Niche
Pad
Great Room
16'-4"x12'-10"
9' CLG
Entry
9' CLG
Gate
SECOND FLOOR PLAN
FIRST FLOOR PLAN
PLAN 2B
1,688 SQ.FT.
LOTS: 6,16,22,23,29
MISSION AT OAKLAND
ONTARIO, CA
North by Northwest
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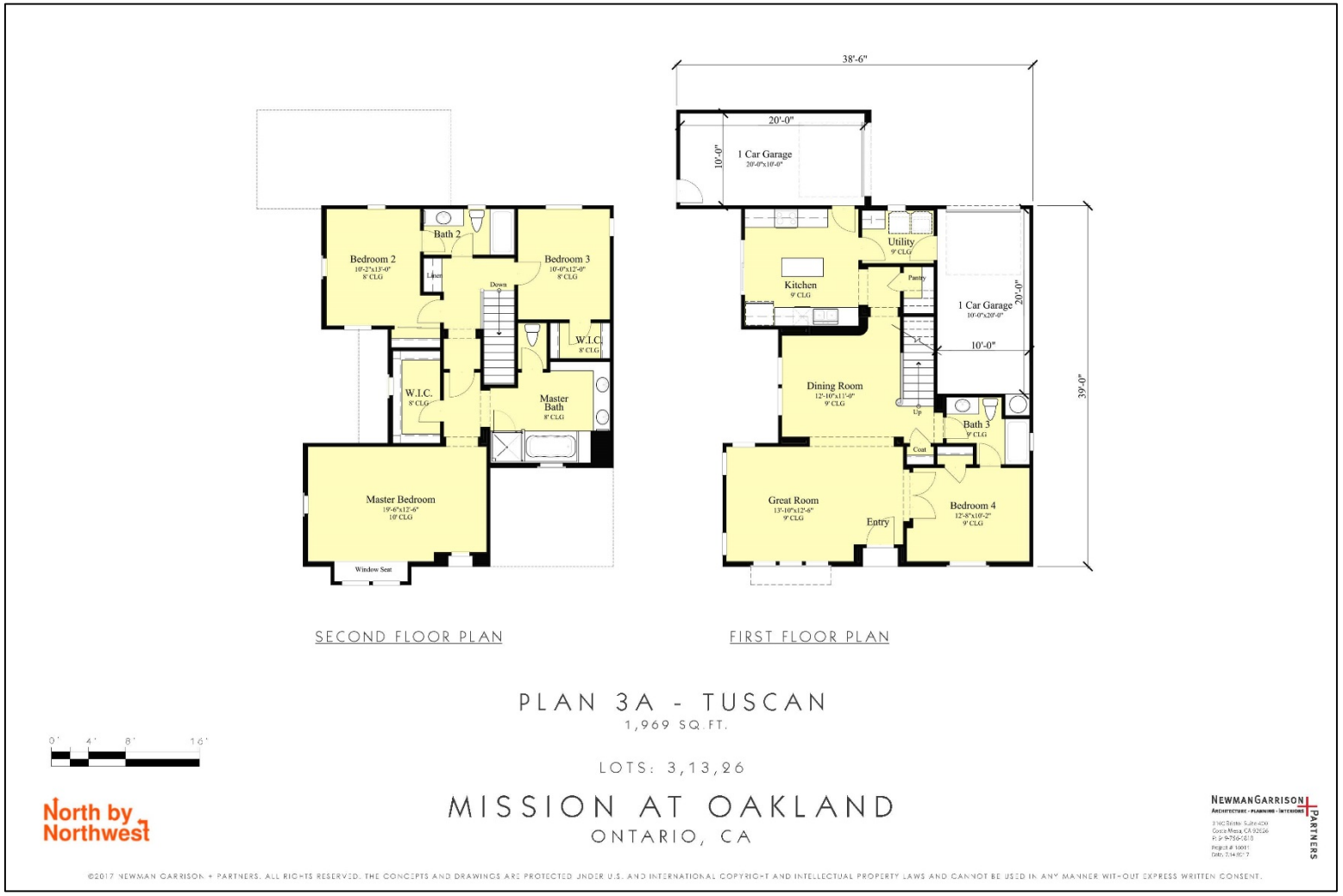
Exhibit F- Plan 3 Floor Plans and Elevations


38'-6"
20'-0"
10'-0"
1 Car Garage
20'-0"x10'-0"
Bedroom 2
10'-2"x13'-0"
8' CLG
Bath 2
Linen
Down
Bedroom 3
10'-0"x12'-0"
8' CLG
W.I.C.
8' CLG
W.I.C.
8' CLG
Master Bath
8' CLG
Master Bedroom
19'-6"x12'-6"
10' CLG
Window Seat
Utility
9' CLG
Kitchen
9' CLG
Pantry
1 Car Garage
10'-0"x20'-0"
20'-0"
10'-0"
Dining Room
12'-10"x11'-0"
9' CLG
Up
Bath 3
9' CLG
Coat
Great Room
13'-10"x12'-6"
9' CLG
Entry
Bedroom 4
12'-8"x10'-2"
9' CLG
39'-0"
SECOND FLOOR PLAN
FIRST FLOOR PLAN
PLAN 3A - TUSCAN
1,969 SQ.FT.
LOTS: 3,13,26
MISSION AT OAKLAND
ONTARIO, CA
North by Northwest
NEWMANGARRISON + PARTNERS
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CEILING LINE
FENCE LINE
RIGHT
NOTE: THIS ELEVATION LEFT INTENTIONALLY BLANK AS IT FACES THE NEIGHBORING YARD
REAR/ ALLEY
Material Key:
① Concrete 'S' Tile Roof
② 7/8" Cement Plaster with Sand Finish
③ Stucco on Foam Trim
④ Decorative Wood Shutters
⑤ Decorative Gable End Detail
⑥ Fabric Awning
⑦ Wrought Iron Railing
⑧ Fiber Cement Siding
⑨ Sectional Garage Door
⑩ Metal Spire
⑪ Stone Veneer
⑫ Decorative Metal Gate
⑬ Wood Corbels
CEILING LINE
FENCE LINE
LEFT
ROOF PLAN
FRONT
0' 4' 8' 16'
PLAN 3A - TUSCAN ELEVATIONS
COLOR SCHEME 3
LOTS: 3,13,26
MISSION AT OAKLAND
ONTARIO, CA
North by Northwest
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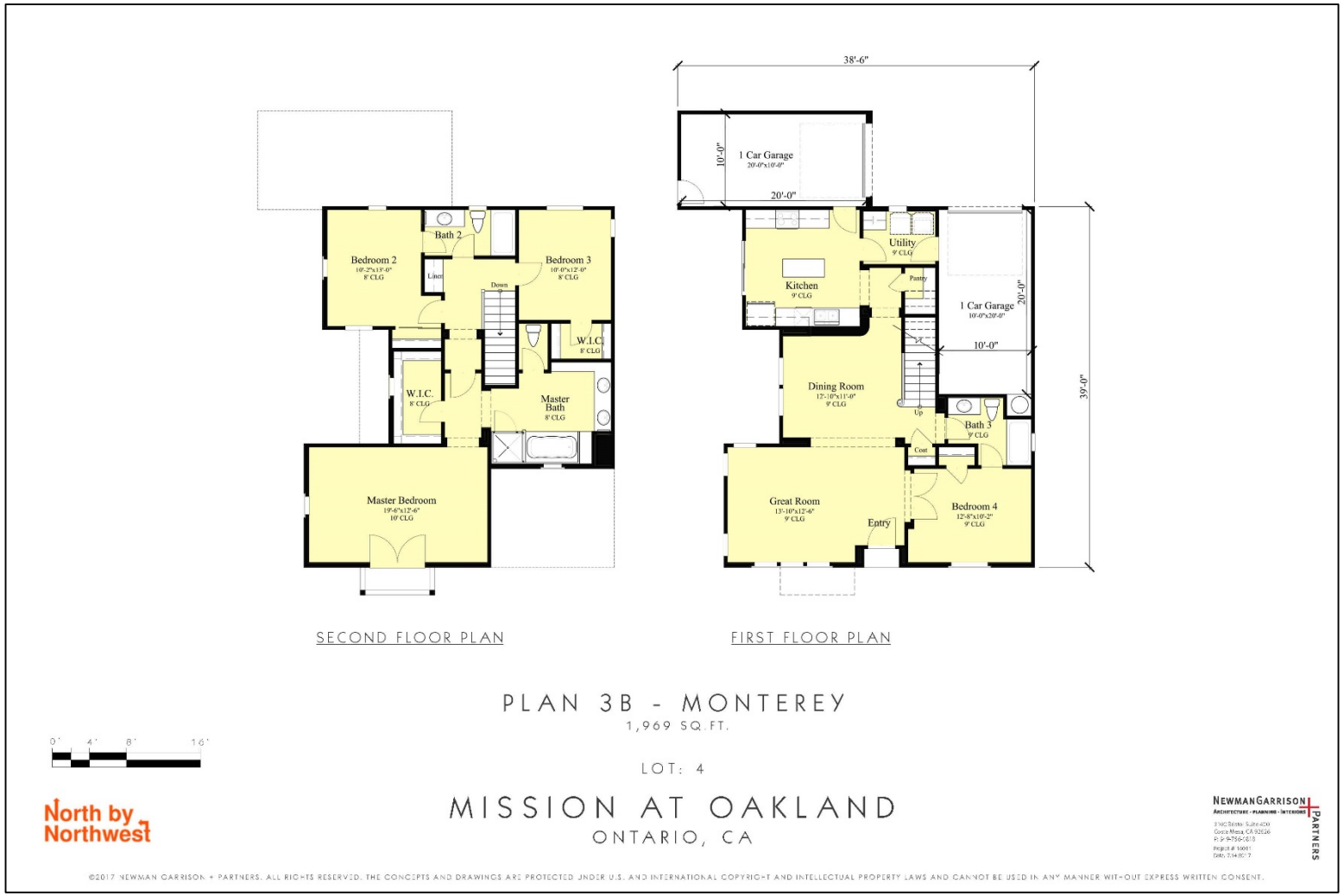
38'-6"
1 Car Garage
20'-0"x10'-0"
10'-0"
20'-0"
Utility
9' CLG
Kitchen
9' CLG
Pantry
1 Car Garage
10'-0"x20'-0"
20'-0"
10'-0"
Dining Room
12'-10"x11'-0"
9' CLG
Up
Bath 3
9' CLG
Coat
Great Room
13'-10"x12'-6"
9' CLG
Entry
Bedroom 4
12'-8"x10'-2"
9' CLG
39'-0"
Bedroom 2
10'-2"x13'-0"
8' CLG
Bath 2
Linen
Down
Bedroom 3
10'-0"x12'-0"
8' CLG
W.I.C.
8' CLG
W.I.C.
8' CLG
Master Bath
8' CLG
Master Bedroom
19'-6"x12'-6"
10' CLG
SECOND FLOOR PLAN
FIRST FLOOR PLAN
PLAN 3B - MONTEREY
1,969 SQ. FT.
LOT: 4
MISSION AT OAKLAND
ONTARIO, CA
0'
4'
8'
16'
North by Northwest
NEWMAN GARRISON + PARTNERS
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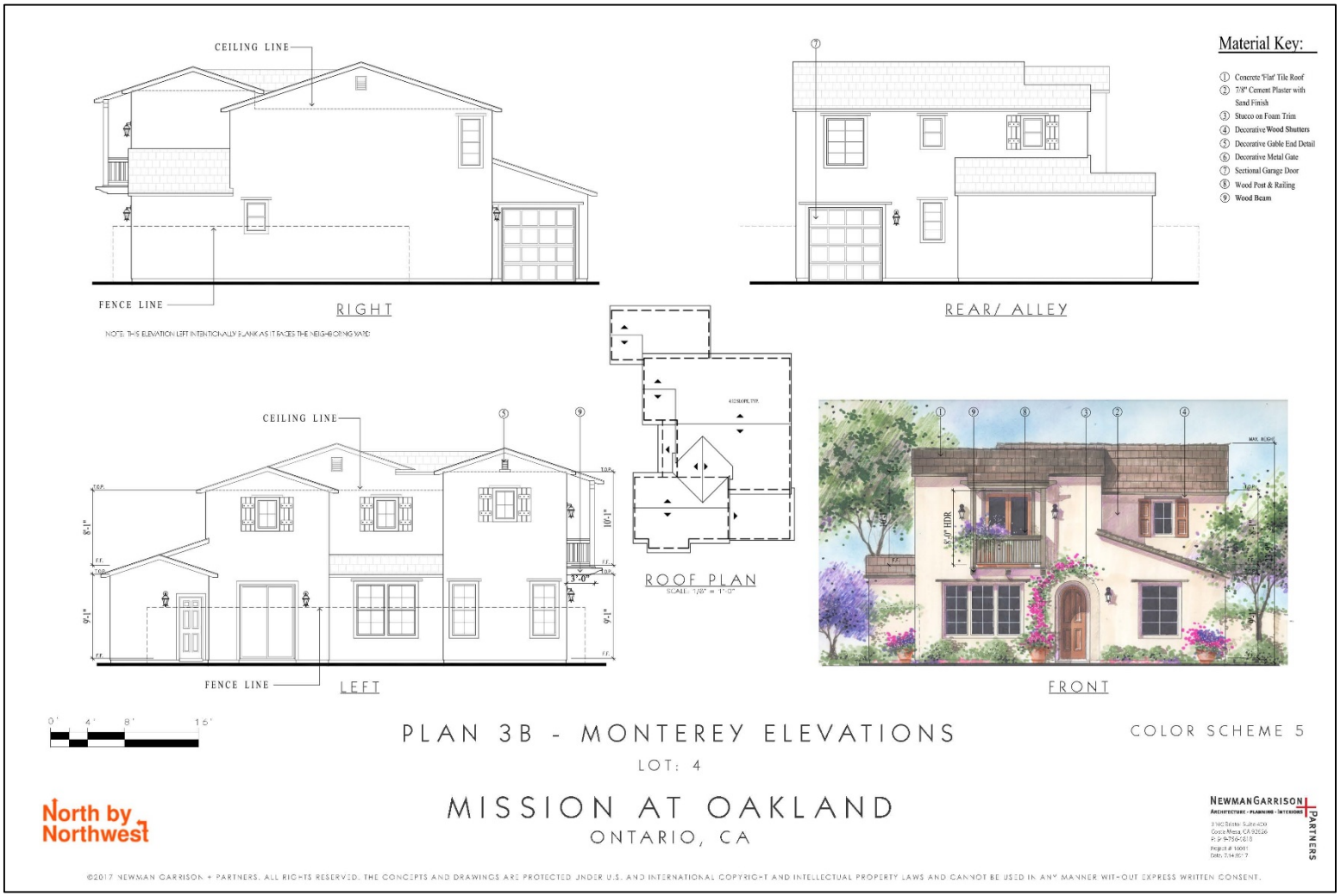
CEILING LINE
FENCE LINE
RIGHT
NOTE: THIS ELEVATION LEFT INTENTIONALLY BLANK AS IT FACES THE NEIGHBORING YARD
REAR/ ALLEY
Material Key:
1 Concrete 'Flat' Tile Roof
2 7/8" Cement Plaster with Sand Finish
3 Stucco on Foam Trim
4 Decorative Wood Shutters
5 Decorative Gable End Detail
6 Decorative Metal Gate
7 Sectional Garage Door
8 Wood Post & Railing
9 Wood Beam
CEILING LINE
FENCE LINE
LEFT
ROOF PLAN
FRONT
0' 4' 8' 16'
PLAN 3B - MONTEREY ELEVATIONS
COLOR SCHEME 5
LOT: 4
MISSION AT OAKLAND
ONTARIO, CA
North by Northwest
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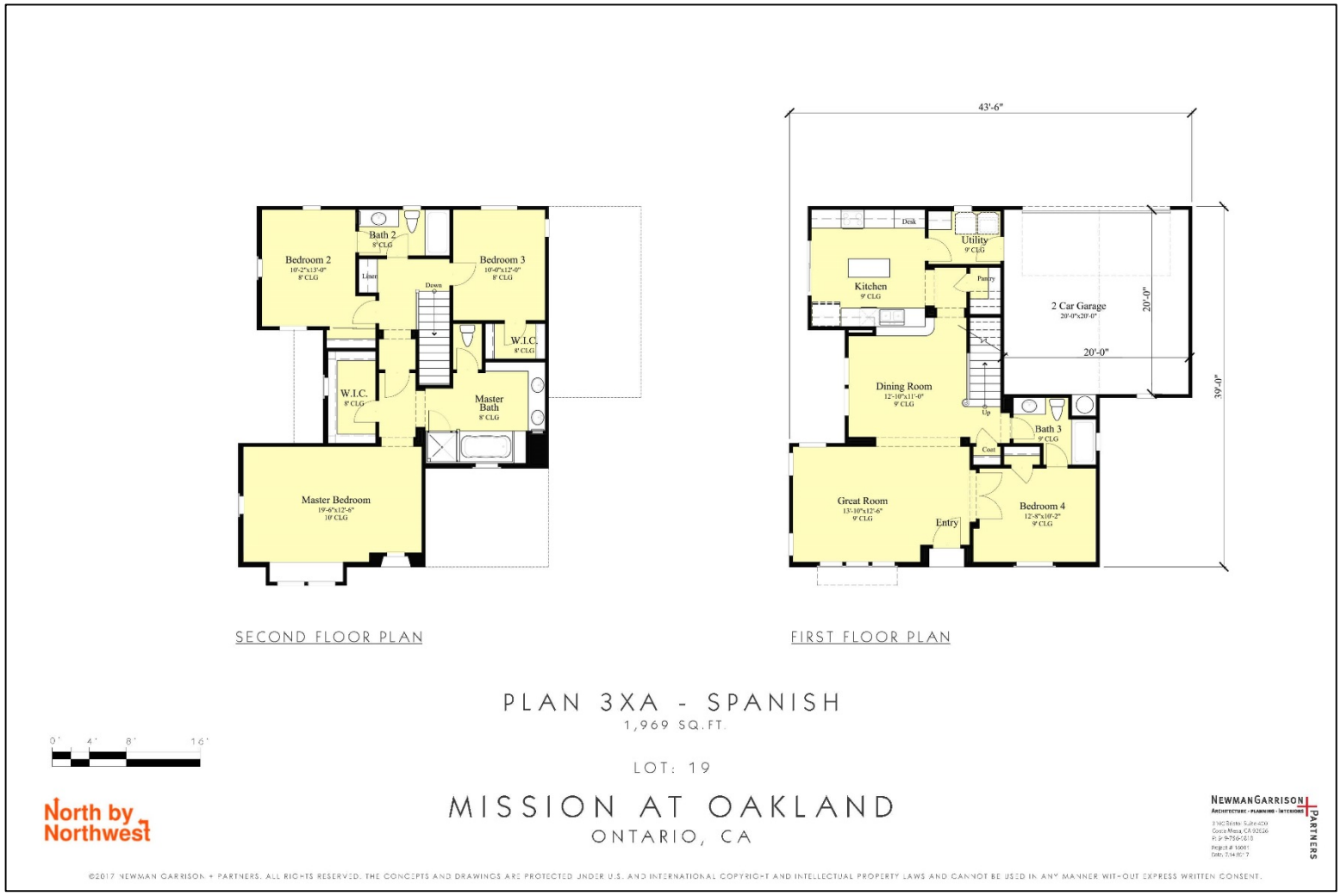
Bedroom 2
10'-2"x13'-0"
8' CLG
Bath 2
8' CLG
Bedroom 3
10'-0"x12'-0"
8' CLG
Linen
Down
W.I.C.
8' CLG
W.I.C.
8' CLG
Master
Bath
8' CLG
Master Bedroom
19'-6"x12'-6"
10' CLG
SECOND FLOOR PLAN
43'-6"
Desk
Utility
9' CLG
Kitchen
9' CLG
Pantry
2 Car Garage
20'-0"x20'-0"
20'-0"
20'-0"
39'-0"
Dining Room
12'-10"x11'-0"
9' CLG
Up
Bath 3
9' CLG
Coat
Great Room
13'-10"x12'-6"
9' CLG
Entry
Bedroom 4
12'-8"x10'-2"
9' CLG
FIRST FLOOR PLAN
PLAN 3XA - SPANISH
1,969 SQ.FT.
LOT: 19
MISSION AT OAKLAND
ONTARIO, CA
0'
4'
8'
16'
North by Northwest
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Material Key:
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③ Stucco on Foam Trim
④ Metal Spire
⑤ Decorative Gable End Detail
⑥ Fabric Awning
⑦ Wrought Iron Detail
⑧ Sectional Garage Door
FENCE LINE
RIGHT
NOTE: THIS ELEVATION LEFT INTENTIONALLY BLANK AS IT FACES THE NEIGHBORING YARD
REAR / ALLEY
ROOF PLAN
Scale: 1/8" = 1'-0"
ROOF PLAN
FENCE LINE
LEFT
FRONT
0' 4' 8' 16'
PLAN 3XA - SPANISH ELEVATIONS
COLOR SCHEME 1
LOT: 19
MISSION AT OAKLAND
ONTARIO, CA
North by Northwest
NEWMANGARRISON + PARTNERS
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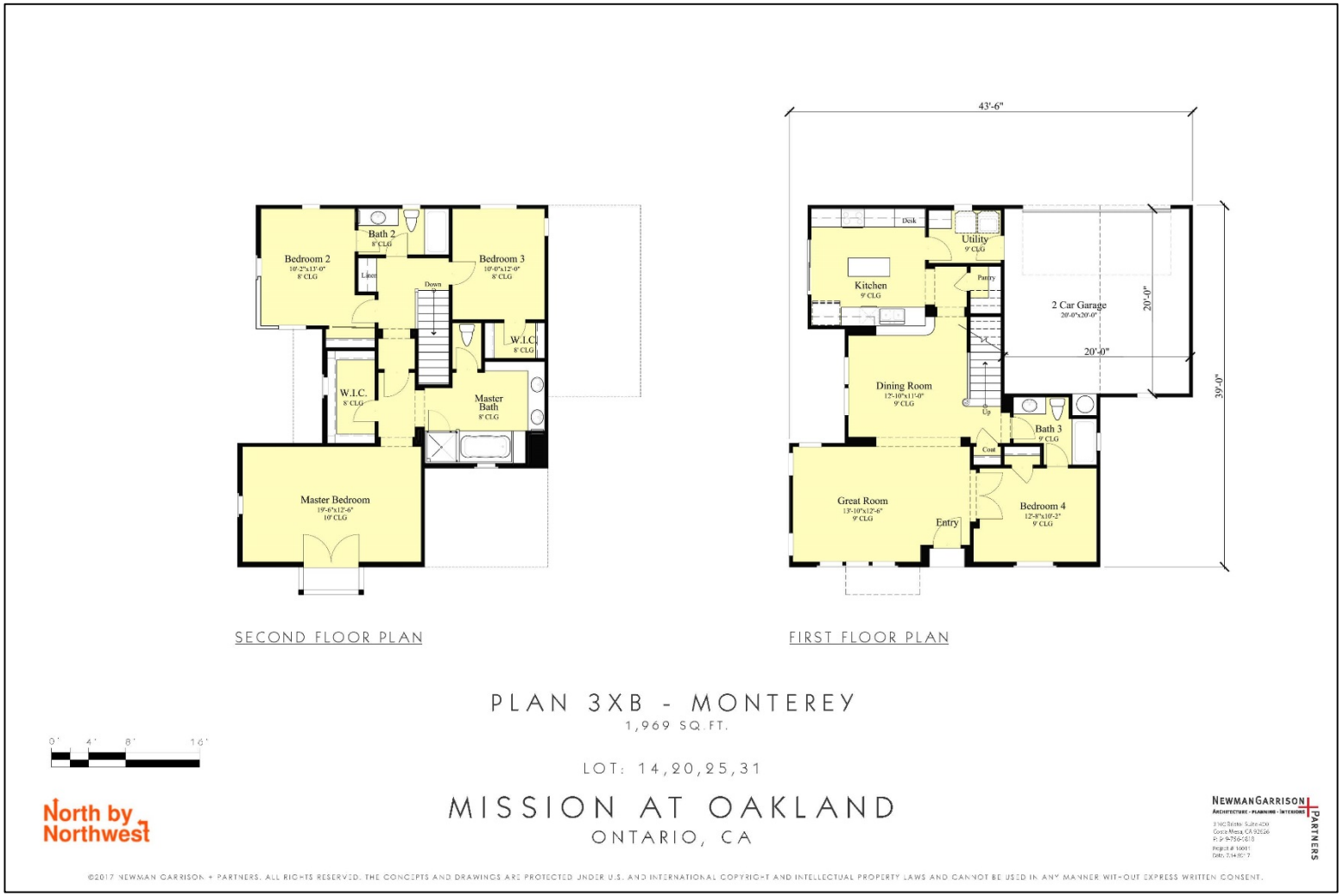
Bedroom 2
10'-2"x13'-0"
8' CLG
Bath 2
8' CLG
Linen
Down
Bedroom 3
10'-0"x12'-0"
8' CLG
W.I.C.
8' CLG
W.I.C.
8' CLG
Master Bath
8' CLG
Master Bedroom
19'-6"x12'-6"
10' CLG
SECOND FLOOR PLAN
43'-6"
Desk
Utility
9' CLG
Kitchen
9' CLG
Pantry
2 Car Garage
20'-0"x20'-0"
20'-0"
20'-0"
39'-0"
Dining Room
12'-10"x11'-0"
9' CLG
Up
Bath 3
9' CLG
Conf
Great Room
13'-10"x12'-6"
9' CLG
Entry
Bedroom 4
12'-8"x10'-2"
9' CLG
FIRST FLOOR PLAN
PLAN 3XB - MONTEREY
1,969 SQ. FT.
LOT: 14,20,25,31
MISSION AT OAKLAND
ONTARIO, CA
0'
4'
8'
16'
North by Northwest
NEWMANGARRISON PARTNERS
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CEILING LINE
FENCE LINE
RIGHT
NOTE: THIS ELEVATION LEFT INTENTIONALLY BLANK AS IT FACES THE NEIGHBORING YARD
REAR / ALLEY
Material Key:
① Concrete 'Flat' Tile Roof
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⑤ Decorative Gable End Detail
⑥ Decorative Metal Gate
⑦ Sectional Garage Door
⑧ Wood Post & Railing
⑨ Wood Beam
CEILING LINE
FENCE LINE
LEFT
ROOF PLAN
SCALE: 1/8" = 1'-0"
FRONT
0' 4' 8' 16'
PLAN 3XB - MONTEREY ELEVATIONS
COLOR SCHEME 2
LOTS: 14,20,25,31
MISSION AT OAKLAND
ONTARIO, CA
North by Northwest
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RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PVAR17-007, A VARIANCE REQUEST TO DEVIATE FROM THE MINIMUM BUILDING ARTERIAL STREET SETBACK ALONG MISSION BOULEVARD, FROM 30 FEET TO 5 FEET, LOCATED ON THE SOUTH SIDE OF MISSION BOULEVARD, BETWEEN SAN ANTONIO AND OAKLAND AVENUES, WITHIN THE MDR-11 (LOW-MEDIUM DENSITY RESIDENTIAL 5.1 TO 11.0 DU/AC) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 1049-323-06, 1049-323-07, 1049-323-08, 1049-323-12 & 1049-323-13.

WHEREAS, North by Northwest Capital, Inc. ("Applicant") has filed an Application for the approval of a Variance, File No. PVAR17-007, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 3.47 acres of land generally located on the south side of Mission Boulevard, between San Antonio and Oakland Avenues, within the MDR-11 (Low-Medium Density Residential 5.1 to 11.0 du/ac), and is presently vacant; and

WHEREAS, the property to the north of the Project site is within the Business Park zoning district, and is developed with residential and commercial uses. The property to the east is within the LDR-5 (Low-Density Residential) zoning district, and is developed with single-family homes. The property to the south is within the LDR-5 (Low-Density Residential) zoning district, and is developed with single-family homes. The property to the west is within the MDR-11 (Low-Medium Density Residential) zoning district, and is developed with single-family homes; and

WHEREAS, the Variance proposes to deviate from the minimum building arterial street setback along Mission Boulevard, from 30 feet to 5 feet. The project site is narrow in depth with an elongated angled width and bounded by Mission Avenue to the north, Oakland Avenue to east and San Antonio Avenue to the west. The project site configuration poses a challenge in effectively planning the site and providing proper access. Requiring a 30 foot building setback along the Mission Boulevard would impact the project sites ability to achieve a well planned development and provide a product that is consistent with the density and scale of the surrounding residential development. In addition, Mission Boulevard, which bounds the project site to the north, is a major arterial and access into the project site is not permitted. Prohibiting access from Mission Boulevard requires the project to provide a 26-foot wide east-west private drive, extending from San Antonio Avenue to Oakland Avenue, to adequately serve the community. Requiring the buildings along Mission Boulevard to be setback 30 feet from the project property line would require the private drive, serving the site, to be pushed south

impacting and eliminating 10 proposed single-family homes. The proposed project, with the Variance request, has demonstrated a very well planned residential community that will provide much needed housing in the City; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed;

WHERARAS, a Development Plan (File No.PDEV17-022) to construct 31 single family detached homes and a Tentative Tract Map application (File No. PMTT17-007(TT17624)) to subdivide the 3.47 acre project site into 31 single family lots and common areas has been filed in conjunction with the Variance application; and

WHEREAS, on July 17, 2017, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project and concluded said hearing on that

date, voting to issue Decision No. DAB17-037 recommending the Planning Commission approve the Application; and

WHEREAS, on July 25, 2017, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: ***Environmental Determination and Findings.*** As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from environmental review pursuant to Section 15305 (Class 5, Minor Alterations in Land Use Limitations) of the CEQA Guidelines; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2: ***Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 3: ***Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance.*** The California State Aeronautics Act (Public Utilities Code

Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport ("ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the Planning Commission, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 4: ***Concluding Facts and Reasons.*** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The strict or literal interpretation and enforcement of the specified regulation would result in practical difficulty or unnecessary physical hardship inconsistent with the objectives of the development regulations contained in this Development Code.*** The project site is narrow in depth with an elongated angled width and bounded by Mission Avenue to the north, Oakland Avenue to east and San Antonio Avenue to the west. The project site configuration poses a challenge in effectively planning the site and providing proper access. Requiring a 30 foot building setback along the Mission Boulevard would impact the project sites ability to achieve a well planned development and provide a product that is consistent with the density and scale of the surrounding residential development. Mission Boulevard, which bounds the project site to the north, is a major arterial and access into the project is not permitted. Prohibiting access from Mission Boulevard requires the project site to provide a 26-foot wide east-west private drive, extending from San Antonio Avenue to Oakland Avenue, to adequately serve the community. Requiring the buildings along Mission Boulevard to be setback 30 feet from the project property line would require the private drive serving the site to be pushed south impacting and eliminating 10 proposed single-family homes. The proposed Development Plan is compatible with surrounding existing single-family and multi-family residential developments to the east and west of the project site, which have varied setbacks between 5 to 20 feet along Mission Boulevard. The project proposes varied

building setbacks of 5 feet to 20.6 feet, along Mission Boulevard, with average setback of 12 feet. The Variance request is consistent with The Ontario Plan (TOP) Goal LU3, which promotes flexibility in order to respond to special conditions and circumstances in order to achieve the Vision of providing housing opportunities for all sectors of our community. Therefore, the strict or literal interpretation and enforcement of the specified regulation would result in practical difficulty or unnecessary physical hardship inconsistent with the objectives of the development regulations contained in the Development Code.

(2) ***There are exceptional or extraordinary circumstances or conditions applicable to the property involved, or to the intended use of the property, that do not apply generally to other properties in the vicinity and in the same zoning district.*** The project site is narrow in depth with an elongated angled width and bounded by Mission Avenue to the north, Oakland Avenue to east and San Antonio Avenue to the west. The project site configuration poses a challenge in effectively planning the site and providing proper access. Requiring a 30 foot building setback along the Mission Boulevard would impact the project sites ability to achieve a well planned development and provide a product that is consistent with the density and scale of the surrounding residential development. The proposed Development Plan is compatible with surrounding existing single-family and multi-family residential developments to the east and west of the project site, which have varied setbacks between 5 to 20 feet along Mission Boulevard. In addition, very few properties within the same MDR-11 zoning district are not impacted by a major arterial, such as Mission Boulevard, and subject to a 30 foot rear or interior side setback. Therefore a variance is necessary to meet development standards as granted on other properties in the same zone.

(3) ***The strict or literal interpretation and enforcement of the specified regulation would deprive the applicant of privileges enjoyed by the owners of other properties in the same zoning district.*** The requested relief from 30 to 5 feet along Mission Boulevard will allow for greater design flexibility and will serve to equalize development rights between the applicant and owners of property in the same zoning district, located within the area of the project site. In addition, very few properties within the same MDR-11 zoning district are impacted by a major arterial, such as Mission Boulevard, and subject to a 30 foot rear or interior side setback. The setback deviation to 5 feet, will allow for the substantial improvement and utilization of the otherwise challenging site. The strict and literal interpretation and enforcement of the specified regulation would deprive the applicant of privileges enjoyed by owners of other properties in the same zone.

(4) ***The granting of the Variance will not be detrimental to the public health, safety or welfare, or be materially injurious to properties or improvements in the vicinity.*** A thorough review and analysis of the proposed Variance and its potential to adversely impact properties surrounding the subject site was completed by staff. As a result of this review, certain design considerations will be incorporated into the project as

conditions of approval to address identified impacts to an acceptable level, including the use of upgraded materials, the inclusion of certain architectural design elements on building exteriors, intensified landscape elements, and decorative paving.

(5) ***The proposed Variance is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and the purposes of any applicable specific plan or planned unit development, and the purposes of this Development Code.*** The proposed Project is located with the Low Medium Density Residential (2.1 – 11.0 du/ac) land use district of the Policy Plan Land Use Map, and the MDR-11 (Low-Medium Density Residential 5.1 to 11.0 du/ac) zoning district. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.

SECTION 5: ***Planning Commission Action.*** Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 6: ***Indemnification.*** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 7: ***Custodian of Records.*** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 8: ***Certification to Adoption.*** The Secretary shall certify to the adoption of the Resolution.

- - - - - - - - - - - - - -

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of July, 2017, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Richard D. Delman
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Principal Planner/Acting Secretary of the
Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Marci Callejo, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. PC17-[insert #] was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 25, 2017, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Marci Callejo
Secretary Pro Tempore

ATTACHMENT A:

File No. PVAR17-007
Departmental Conditions of Approval

(Departmental conditions of approval to follow this page)

City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department
Land Development Division
Conditions of Approval

Meeting Date: July 17, 2017

File No: PVAR17-007

Related Files: PDEV17-022 and PMTT17-007(TT17624)

Project Description: A Variance (File No. PVAR17-007) to deviate from the minimum building arterial street setback, along Mission Boulevard, from 30 feet to 5 feet, located on the south side of Mission Boulevard, between San Antonio and Oakland Avenues, within the MDR-11 (Low-Medium Density Residential 5.1 to 11.0 du/ac) zoning district. (APNs: 1049-323-06, 1049-323-07, 1049-323-08, 1049-323-12 & 1049-323-13); **submitted by North by Northwest Capital Inc.**

Prepared By: Rudy Zeledon, Principal Planner
Phone: 909.395.2422 (direct)
Email: rzeledon@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2010. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Variance approval shall become null and void one year following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director, except that a Variance approved in conjunction with a Development Plan shall have the same time limits as said Development Plan. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading,

utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

(d) The project is contingent upon Planning Commission approval of the related Development Plan (File No. PDEV17-022) and Variance applications (File No. PVAR 17-007). All applicable Conditions of Approval of the related Applications, shall apply to the Variance application.

(e) All applicable City Departmental Conditions of Approval of the related Development Plan (File No. PDEV17-022) and Variance applications (File No. PVAR 17-007), shall apply to this Variance Application.

2.3 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.4 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PMTT17-077 (TT17624)), A TENTATIVE TRACT MAP (FILE NO. PMTT17-007/TT 17624) TO SUBDIVIDE 3.47 ACRES OF LAND INTO 31 SINGLE FAMILY LOTS AND COMMON AREA, LOCATED ON THE SOUTH SIDE OF MISSION BOULEVARD, BETWEEN SAN ANTONIO AND OAKLAND AVENUES, WITHIN THE MDR-11 (LOW-MEDIUM DENSITY RESIDENTIAL 5.1 TO 11.0 DU/AC) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 1049-323-06, 1049-323-07, 1049-323-08, 1049-323-12 & 1049-323-13.

WHEREAS, North by Northwest Capital, Inc. ("Applicant") has filed an Application for the approval of a Tentative Tract Map, File No. PMTT17-007, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 3.47 acres of land generally located on the south side of Mission Boulevard, between San Antonio and Oakland Avenues, a within the MDR-11 (Low-Medium Density Residential 5.1 to 11.0 du/ac), and is presently vacant; and

WHEREAS, the property to the north of the Project site is within the Business Park zoning district, and is developed with residential and commercial uses. The property to the east is within the LDR-5 (Low-Density Residential) zoning district, and is developed with single-family homes. The property to the south is within the LDR-5 (Low-Density Residential) zoning district, and is developed with single-family homes. The property to the west is within the MDR-11 (Low-Medium Density Residential) zoning district, and is developed with single-family homes; and

WHEREAS, the Tentative Tract Map proposes to subdivide 3.47 acres of land into 31 single family lots and common area. The residential lots range in size from 1,529 to 2,607 square feet. The proposed lots are consistent with the MDR-11 (Low-Medium Density Residential) zoning district and the Development Code standards *Table 6.01C: Small Lot Single Family Development Standards – Cluster Subdivisions*; and

WHEREAS, the Tentative Tract Map will facilitate the construction of sidewalks and parkways along Mission Boulevard, San Antonio Avenue and Oakland Avenue. Additionally, all streets within the project area are classified as “Private Drives” will be maintained by the Home Owners Association (HOA). All remainder areas, within the tract boundary will be dedicated as common space and be maintained in accordance with the (HOA); and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, a Development Plan (File No. PDEV17-022) to construct 31 single family detached homes and a Variance application (File No. PVAR17-007) to deviate from the minimum building arterial street setback, along Mission Boulevard, from 30 feet to 5 feet inches has been filed in conjunction with the Tentative Tract Map application; and

WHEREAS, on July 17, 2017, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project and concluded said hearing on that date, voting to issue Decision No. DAB17-038 recommending the Planning Commission approve the Application; and

WHEREAS, on July 25, 2017, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: ***Environmental Determination and Findings.*** As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from environmental review pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2: ***Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 3: ***Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of

Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport ("ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the Planning Commission, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 4: ***Concluding Facts and Reasons.*** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed Tentative Tract/Parcel Map is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable area and specific plans, and planned unit developments.*** The proposed Tentative Tract is located within the Low Medium Density (5.1 to 11.0 dc/ac) land use district of the Policy Plan Land Use Map, and the MDR- 11(Low Medium Density Residential) zoning district. The proposed subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will contribute to providing "a spectrum of housing types and price ranges that match the jobs in the City, and that make it possible for people to live and work in Ontario and maintain a quality of life" (Goal LU1). Furthermore, the project will promote the City's policy to "incorporate a variety of land uses and building types that contribute to a complete community where residents at all stages of life, employers, workers, and visitors, have a wide spectrum of choices of where they can live, work, shop, and recreate within Ontario" (Policy LU1-6 *Complete Community*).

(2) ***The design or improvement of the proposed Tentative Tract/Parcel Map is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable specific plans and planned unit developments.*** The proposed Tentative Tract is located within the Low Medium Density (5.1 to 11.0 dc/ac) land use district of the Policy Plan Land Use Map, and the MDR-11(Low Medium Density Residential) zoning district. The proposed design or improvement of the subdivision is

consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will contribute to providing "[a] high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct" (Goal CD2). Furthermore, the project will promote the City's policy to "create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the "outdoor living room"), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb." (Policy CD2-2 *Neighborhood Design*).

(3) ***The site is physically suitable for the type of development proposed.*** The project site meets the minimum lot area and dimensions of the MDR- 11(Low Medium Density Residential) zoning district, and is physically suitable for the type of single-family development proposed in terms of zoning, land use and development activity proposed, and existing and proposed site conditions.

(4) ***The site is physically suitable for the density/intensity of development proposed.*** The project site is proposed for residential development at 9.0 DUs/acre. The project site meets the minimum lot area and dimensions of the MDR- 11(Low Medium Density Residential) zoning district, and is physically suitable for this proposed density / intensity of development.

(5) ***The design of the subdivision or the proposed improvements thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.*** The project site is not located in an area that has been identified as containing species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations or by the California Department of Fish and Wildlife or the U.S. Fish and Wildlife Service, nor does the site contain any riparian habitat or other sensitive natural community, and no wetland habitat is present on site; therefore, the design of the subdivision, or improvements proposed thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.

(6) ***The design of the subdivision, or the type of improvements thereon, are not likely to cause serious public health problems.*** The design of the proposed subdivision, and the infrastructure improvements existing or proposed on the project site, are not likely to cause serious public health problems, as The project is not anticipated to involve the transport, use, or disposal of hazardous materials during either construction or project implementation, include the use of hazardous materials or volatile fuels, nor are there any known stationary commercial or industrial land uses within close proximity to the subject site that use/store hazardous materials to the extent that they would pose a significant hazard to visitors or occupants to the project site.

(7) ***The design of the subdivision, or the type of improvements thereon, will not conflict with easements acquired by the public at large for access through, or use of property within, the proposed subdivision.*** The proposed subdivision has provided for all necessary public easements and dedications for access through, or use of property within, the proposed subdivision. Furthermore, all such public easements and dedications have been designed pursuant to: (a) the requirements of the Policy Plan component of The Ontario Plan and applicable area plans; (b) applicable specific plans or planned unit developments; (c) applicable provisions of the City of Ontario Development Code; (d) applicable master plans and design guidelines of the City; and (e) applicable Standard Drawings of the City.

SECTION 5: ***Planning Commission Action.*** Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 6: ***Indemnification.*** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 7: ***Custodian of Records.*** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 8: ***Certification to Adoption.*** The Secretary shall certify to the adoption of the Resolution.

- - - - - - - - - - - - - -

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of July, 2017, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Richard D. Delman
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Principal Planner/Acting Secretary of the
Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Marci Callejo, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. PC17-[insert #] was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 25, 2017, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Marci Callejo
Secretary Pro Tempore

ATTACHMENT A:

File No. PMTT17-007 (TT 17624)
Departmental Conditions of Approval

(Departmental conditions of approval to follow this page)

City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department
Land Development Division
Conditions of Approval

Meeting Date: July 17, 2017

File No: PMTT17-007 (TT17624)

Related Files: PDEV17-022 and PVAR 17-007

Project Description: A Tentative Tract Map (File No. PMTT17-007/TT17624) to subdivide 3.47 acres of land into 31 single family lots and common areas, located on the south side of Mission Boulevard, between San Antonio and Oakland Avenues, within the MDR-11 (Low-Medium Density Residential 5.1 to 11.0 du/ac) zoning district. (APNs: 1049-323-06, 1049-323-07, 1049-323-08, 1049-323-12 & 1049-323-13); **submitted by North by Northwest Capital Inc.**

Prepared By: Rudy Zeledon, Principal Planner
Phone: 909.395.2422 (direct)
Email: rzeledon@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2010. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Tentative Parcel/Tract Map approval shall become null and void 2 years following the effective date of application approval, unless the final parcel/tract map has been recorded, or a time extension has been approved by the Planning Commission pursuant to Development Code Section 2.02.025 (Time Limits and Extensions). This Permit does not supersede any individual time limits specified herein for performance of specific conditions or improvements.

2.2 Subdivision Map.

(a) The Final Tract/Parcel Map shall be in conformance with the approved Tentative Tract/Parcel Map on file with the City. Variations rom the approved Tentative Tract/Parcel Map may be reviewed and approved by the Planning Department. A substantial variation from the approved Tentative Tract/Parcel Map may require review and approval by the Planning Commission, as determined by the Planning Director.

(b) Tentative Tract/Parcel Map approval shall be subject to all conditions, requirements and recommendations from all other departments/agencies provided on the attached reports/memorandums.

(c) The subject Tentative Tract/Parcel Map for condominium purposes shall require the recordation of a condominium plan concurrent with the recordation of the Final Tract/Parcel Map and CC&Rs.

(d) Pursuant to California Government Section 66474.9, the subdivider agrees that it will defend, indemnify, and hold harmless the City of Ontario or its agents, officers and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer of this subdivision, which action is brought within the time period provided for in Government Code Section 66499.37. The City of Ontario shall promptly notify the subdivider of any such claim, action or proceeding and the City of Ontario shall cooperate fully in the defense.

2.3 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

(d) The project is contingent upon Planning Commission approval of the related Development Plan (File No. PDEV17-022) and Variance applications (File No. PVAR 17-007).

(e) All applicable City Departmental Conditions of Approval of the related Development Plan (File No. PDEV17-022) and Variance applications (File No. PVAR 17-007), shall apply to this Tentative Tract Map Application.

2.4 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.5 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

(a) Decorative block wall (split-face block, slump stone or stucco, etc.) shall be constructed along north and south boundaries of the project site at a minimum height of 6 feet. Vinyl or wood fencing will not be permitted.

2.6 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

2.7 Mechanical Equipment.

(a) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

2.8 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.9 Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements.

(a) CC&Rs shall be prepared for the Project and shall be recorded prior to the issuance of a building permit.

(b) The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owners association and the CC&Rs shall be reviewed and approved by the City.

(c) CC&Rs shall ensure reciprocal parking and access between parcels.

(d) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:

(i) Landscaping and irrigation systems within common areas;

(ii) Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02;

(iii) Shared parking facilities and access drives; and

(iv) Utility and drainage easements.

(e) CC&Rs shall include authorization for the City's local law enforcement officers to enforce City and State traffic and penal codes within the project area.

(f) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.

(g) A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

2.10 Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

2.11 Environmental Review.

(a) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines.

(b) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to **Section 15332 (Class 32, In-Fill Development Projects)** of the CEQA Guidelines, meeting the following conditions:

(i) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations;
(ii) The proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses;
(iii) The project site has no value as habitat for endangered, rare, or threatened species;
(iv) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and
(v) The Project site can be adequately served by all required utilities and public services.

2.12 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.13 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

ENGINEERING DEPARTMENT
CONDITIONS OF APPROVAL

(Environmental, Traffic/Transportation Division, Ontario Municipal Utilities Company Information Technology and Management Services Department conditions incorporated herein)

☒ **DEVELOPMENT PLAN** ☐ OTHER	☐ PARCEL MAP ☒ **TRACT MAP** ☒ **FOR CONDOMINIUM PURPOSES**
PROJECT FILE NO. TM-17624 **RELATED FILE NO(S). PDEV17-022**	
☒ **ORIGINAL** ☐ REVISED: __/__/__	

CITY PROJECT ENGINEER & PHONE NO:	**Miguel Sotomayor**	**(909) 395-2108** MS
CITY PROJECT PLANNER & PHONE NO:	**Luis Batres**	**(909) 395-2431**
DAB MEETING DATE:	**July 17th, 2017**	
PROJECT NAME / DESCRIPTION:	**TM-17624, a Tentative Tract Map for condominium purposes and Development Plan to subdivide 3.47 acres into 31 lots and construct 31 single family detached homes within the MDR 11 (Low-Medium Density Residential)**	
LOCATION:	**SWC Mission Boulevard and Oakland Avenue**	
APPLICANT:	**North by Northwest Capital, Inc.**	
REVIEWED BY:	**Bryan Lirley, P.E.** **Principal Engineer**	7/10/17 **Date**
APPROVED BY:	**Khoi Do, P.E.** **Assistant City Engineer**	7-12-17 **Date**

THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2010-021) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO FINAL MAP APPROVAL, APPLICANT SHALL:

Check When Complete

☒ **1.01** **Dedicate to the City of Ontario, the right-of-way, described below:** ☐

1. **11 feet on San Antonio Avenue**
2. **Property line corner 'cut-back' required at the intersection of Mission Boulevard and Oakland Avenue.**

☒ **1.02** **Dedicate to the City of Ontario, the following easement(s):** ☐

1. **Public Utilities Easement along "A" Street shown on Tentative Tract Map No. 17624**
2. **Emergency Access Easement "A" through "E" Streets.**

☐ 1.03 Restrict vehicular access to the site as follows: _____ ☐

☐ 1.04 Vacate the following street(s) and/or easement(s): _____ ☐

☐ 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles. ☐

☒ **1.06** **Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.** ☐

☐ 1.07 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Management Services Department at (909) 395-2124 regarding this requirement. ☐

(1) _____

(2) _____

☒ **1.08** **Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.** ☐

☒ **1.09** **Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.** ☐

☒ **1.10** **Provide a preliminary title report current to within 30 days.** ☐

☐ 1.11 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Management Services at (909) 395-2353 to initiate the CFD application process. ☐

☐ 1.12 New Model Colony (NMC) Developments: ☐

☐ 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.

☐ 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).

☐ 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).

☐ 1.13 Other conditions: _____ ☐

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

A. GENERAL
(Permits includes Grading, Building, Demolition and Encroachment)

☒ **2.01 Record Tract Map No. 17624 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.** ☐

☒ **2.02 Submit a duplicate photo mylar of the recorded map to the City Engineer's office.** ☐

☒ **2.03 Note that the subject parcel will be a recognized parcel in the City of Ontario Upon the recordation of TM-17624.** ☐

☐ 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of_____. ☐

☐ 2.05 Apply for a: ☐ Certificate of Compliance with a Record of Survey; ☐ Lot Line Adjustment ☐

☐ Make a Dedication of Easement.

☐ 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. ☐

☐ 2.07 Submit a soils/geology report. ☐

☐ 2.08 Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies: ☐

- ☐ State of California Department of Transportation (Caltrans)
- ☐ San Bernardino County Road Department (SBCRD)
- ☐ San Bernardino County Flood Control District (SBCFCD)

- ☐ Federal Emergency Management Agency (FEMA)
- ☐ Cucamonga Valley Water District (CVWD) for sewer/water service
- ☐ United States Army Corps of Engineers (USACE)
- ☐ California Department of Fish & Game
- ☐ Inland Empire Utilities Agency (IEUA)
- ☐ Other: _____

☐ 2.09 Dedicate to the City of Ontario the right-of-way described below: ☐

_____ feet on _____

Property line corner 'cut-back' required at the intersection of _____ and _____.

☐ 2.10 Dedicate to the City of Ontario the following easement(s):_____ ☐

_____

☐ 2.11 New Model Colony (NMC) Developments: ☐

☐ 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.

☐ 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.

☐ 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall.

☐ 2.12 Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at _____% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements. ☐

☐ 2.13 The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office. ☐

☒ **2.14 Pay all Development Impact Fees (DIF) to the Building Department.** ☐

☐ 2.15 Other conditions: _____ ☐

B. PUBLIC IMPROVEMENTS
(See attached Exhibit 'A' for plan check submittal requirements.)

☒ 2.16 **Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):** ☐

Improvement	San Antonio Ave.	Mission Blvd.	Oakland Ave.	
Curb and Gutter	☒ **New; 32 ft. from C/L** ☐ Replace damaged ☒ **Remove and replace**	☒ **New; 40 ft. From median curb** ☐ Replace damaged ☐ Remove and replace	☒ **New; 20 ft. from C/L** ☐ Replace damaged ☐ Remove and replace	☐ New; ___ ft. from C/L ☐ Replace damaged ☐ Remove and replace
AC Pavement[1]	☒ **Replacement** ☒ **Widen 17 additional feet along frontage, including pavm't transitions**	☐ Replacement ☒ **Widen 20 additional feet along frontage, including pavm't transitions**	☒ **Replacement** ☐ Widen ____ additional feet along frontage, including pavm't transitions	☐ Replacement ☐ Widen ____ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	☐ New ☐ Modify existing	☐ New ☐ Modify existing	☐ New ☐ Modify existing	☐ New ☐ Modify existing
Drive Approach	☒ **New** ☐ Remove and replace replace	☐ New ☐ Remove and replace replace	☒ **New** ☐ Remove and replace replace	☐ New ☐ Remove and replace replace
Sidewalk [2]	☒ **New** ☐ Remove and replace	☒ **New** ☐ Remove and replace	☒ **New** ☐ Remove and replace	☐ New ☐ Remove and replace
ADA Access Ramp [3]	☐ New ☐ Remove and replace	☒ **New** ☐ Remove and replace	☒ **New** ☐ Remove and replace	☐ New ☐ Remove and replace
Parkway	☒ **Trees** ☒ **Landscaping (w/irrigation)**	☒ **Trees** ☒ **Landscaping (w/irrigation)**	☒ **Trees** ☒ **Landscaping (w/irrigation)**	☐ Trees ☐ Landscaping (w/irrigation)
Raised Landscaped Median	☐ New ☐ Remove and replace	☐ New ☐ Remove and replace	☐ New ☐ Remove and replace	☐ New ☐ Remove and replace

Fire Hydrant	☒ **New / Upgrade** ☒ **Relocation**	☐ New / Upgrade ☐ Relocation	☐ New / Upgrade ☐ Relocation	☐ New / Upgrade ☐ Relocation
Sewer (see Sec. 2.C)	☐ Main ☐ Lateral	☐ Main ☐ Lateral	☐ Main ☐ Lateral	☐ Main ☐ Lateral
Water (see Sec. 2.D)	☐ Main ☐ Service	☐ Main ☐ Service	☒ **Main** ☐ Service	☐ Main ☐ Service
Recycled Water (see Sec. 2.E)	☐ Main ☐ Service	☐ Main ☐ Service	☐ Main ☐ Service	☐ Main ☐ Service
Traffic Signal System (see Sec. 2.F)	☐ New ☐ Modify existing	☐ New ☐ Modify existing	☐ New ☐ Modify existing	☐ New ☐ Modify existing
Traffic Signing and Striping (see Sec. 2.F)	☒ **New** ☐ Modify existing	☒ **New** ☐ Modify existing	☒ **New** ☐ Modify existing	☐ New ☐ Modify existing
Street Light (see Sec. 2.F)	☒ **New / Upgrade** ☐ Relocation	☒ **New / Upgrade** ☐ Relocation	☒ **New / Upgrade** ☐ Relocation	☐ New / Upgrade ☐ Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	☐ New ☐ Modify existing	☐ New ☐ Modify existing	☐ New ☐ Modify existing	☐ New ☐ Modify existing
Storm Drain (see Sec. 2G)(4)	☐ Main ☐ Lateral	☐ Main ☐ Lateral	☒ **Main** ☐ Lateral	☐ Main ☐ Lateral
Fiber Optics (see Sec. 2K)	☐ Conduit / Appurtenances	☒ **Conduit / Appurtenances**	☒ **Conduit / Appurtenances**	☐ Conduit / Appurtenances
Overhead Utilities	☐ Underground ☐ Relocate	☐ Underground ☐ Relocate	☐ Underground ☐ Relocate	☐ Underground ☐ Relocate
Removal of Improvements	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____ _____ _____
Other Improvements(5)	_____ _____ _____	**Modify existing drainage outlet**	_____ _____ _____	_____ _____ _____

Specific notes for improvements listed in item no. 2.15, above:

1. **Remove and replace full AC section on San Antonio Avenue from centerline to easterly existing gutter (from southerly tract limit to 50' north of northerly tract limit). Remove and replace full AC section on Oakland Avenue curb to curb (from**

Mission Boulevard to the southerly tract limit).

2. **The applicant/developer shall ensure sidewalk behind proposed ingress/egress on San Antonio Avenue and Oakland Avenue are ADA compliant.**

3. **Access ramp shall be constructed on the southwest corner of Mission Boulevard and Oakland Avenue per City Standards.**

4. **See 2.41 for Storm Drain on Oakland Avenue.**

5. **The applicant/developer shall design and modify the existing drainage outlet located on the southwest corner of Mission Boulevard and Oakland Avenue.**

☒ **2.17** **Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s): Along San Antonio Avenue centerline to westerly gutter from southerly tract limit to 50' north of northerly tract limit.** ☐

☐ 2.18 Reconstruct the full pavement structural section per City of Ontario Standard Drawing number 1011, based on existing pavement condition and approved street section design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter. 'Pothole' verification of existing pavement section required prior to acceptance/approval of street improvement plan. ☐

☐ 2.19 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide ☐ water service ☐ sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid. ☐

☐ 2.20 Other conditions: _____ ☐

C. SEWER

☒ **2.21** **A 8 inch sewer main is available for connection by this project in San Antonio Avenue and Oakland Avenue.**
(Ref: Sewer plan bar code: S12825 and S12391) ☐

☐ 2.22 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away. ☐

☐ 2.23 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer. ☐

☒ **2.24** **Other conditions:** ☐

1. **Proposed in tract sewer main in "A" Street from San Antonio Avenue to Oakland Avenue shall be a public sewer main. Proposed sewer mains in "B", "C", "D" and "E" Streets shall be privately owned and maintained. Existing approved sewer improvement plans shall be revised (S13928-S13930).**

2. **Install back flow valve on sewer lateral for lot number 16-18 and 26-28.**

D. WATER

☒ **2.25** **A 10 inch water main is available for connection by this project in San Antonio Avenue.**
(Ref: Water plan bar code: W11929) ☐

☐ 2.26 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away. ☐

☒ **2.27 Other conditions:** ☐

1. **Design and construct an 8" water main in Oakland Avenue from Carlton Street (connect to existing 8" main) to the southerly property line (connect to existing 8" water main).**

2. **Proposed in tract water main in "A" Street from San Antonio Avenue to Oakland Avenue shall be a public water main. Proposed water mains in "B", "C", "D" and "E" Streets shall be privately owned and maintained. Existing approved water improvement plans shall be revised (W13310-W13313).**

E. RECYCLED WATER

☐ 2.28 A _____inch recycled water main is available for connection by this project in _____. ☐
(Ref: Recycled Water plan bar code:_____)

☐ 2.29 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project. ☐

☐ 2.30 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. Applicant shall be responsible for construction of a connection to the recycled water main for approved uses, when the main becomes available. The cost for connection to the main shall be borne solely by Applicant. ☐

☐ 2.31 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval. ☐

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2687 regarding this requirement.

☐ 2.32 Other conditions: _____ ☐

F. TRAFFIC / TRANSPORTATION

☐ 2.33 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer: ☐
1. On-site and off-site circulation
2. Traffic level of service (LOS) at 'build-out' and future years
3. Impact at specific intersections as selected by the City Engineer

☒ **2.34 Other conditions:** ☐

1. **The Applicant/Developer shall be responsible to design and construct in-fill public street lights along the property frontage of Mission Boulevard, San Antonio Avenue, and Oakland Avenue, in accordance with the Traffic and Transportation Design Guidelines Section 1.4 Street Light Plans, City of Ontario Standard Drawing No. 5101, and to the satisfaction of the City Engineer.**

2. **Mission Boulevard shall be signed "No Stopping Anytime". No Parking shall be permitted within 20 feet of any project driveway (as measured from the ECR/BCR.)**

3. **No entry gates shall be permitted.**

4. **The Applicant/Developer's engineer-of-record shall meet with City Engineering staff prior to starting signing and striping and street lighting design.**

G. DRAINAGE / HYDROLOGY

☐ 2.35 A _____inch storm drain main is available to accept flows from this project in _____. ☐
(Ref: Storm Drain plan bar code:_____)

☒ **2.36** **Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.** ☐

☒ **2.37** **An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.** ☐

☐ 2.38 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project. ☐

☐ 2.39 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program. ☐

☒ **2.40** **Pay Storm Drain Development Impact Fee. Fee shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.** ☐

☒ **2.41** **Other conditions:** ☐

- **The applicant/developer shall pay an in-lieu fee in the amount of $48,597.92 for the future construction of the 54" storm drain in Oakland Avenue per the Master Plan of Drainage.**

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

☐ 2.41 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels. ☐
If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.
Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.

☒ **2.43** **Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: http://www.sbcounty.gov/dpw/land/npdes.asp.** ☐

☐ 2.44 Other conditions: _____ ☐

J. SPECIAL DISTRICTS

☐ 2.45 File an application, together with an initial payment deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community facilities District Act of 1982. The application and fee shall be submitted a minimum three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact the Management Services Department at (909) 395-2353 to initiate the CFD application process. ☐

☐ 2.46 Other conditions: _____ ☐

K. FIBER OPTIC

☒ **2.47 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole (see Fiber Optic Exhibit herein).** ☐

☒ **2.48 Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Information Technology Department at (909) 395-2000, regarding this requirement.** ☐

L. Solid Waste

☒ **2.49 Onsite solid waste shall be designed in accordance with the City's Solid Waste Manual location at:** ☐

http://www.ontarioca.gov/municipal-utilities-company/solid-waste

☐ 2.50 Other conditions: _____ ☐

3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

☒ **3.01** **Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.** ☐

☐ 3.02 Complete all requirements for recycled water usage. ☐

☐ 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.

☐ 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.

☐ 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.

☒ **3.03** **The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.** ☐

☐ 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a benchmark if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals. ☐

☒ **3.05** **Confirm payment of all Development Impact Fees (DIF) to the Building Department.** ☐

☒ **3.06** **Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studie and reports (i.e. hydrology, traffic, WQMP, etc.).** ☐

EXHIBIT 'A'

ENGINEERING DEPARTMENT
First Plan Check Submittal Checklist

Project Number: PDEV17-022, and/or Tract Map No. 17624

The following items are required to be included with the first plan check submittal:

1. ☒ **A copy of this check list**
2. ☒ **Payment of fee for Plan Checking**
3. ☒ **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. ☒ **One (1) copy of project Conditions of Approval**
5. ☐ Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).
6. ☒ **Three (3) sets of Public Street improvement plan with street cross-sections**
7. ☐ Three (3) sets of Private Street improvement plan with street cross-sections
8. ☒ **Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size) (Revise existing)**
9. ☐ Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10. ☒ **Four (4) sets of Public Sewer improvement plan (revise existing)**
11. ☐ Five (5) sets of Public Storm Drain improvement plan
12. ☒ **Three (3) sets of Public Street Light improvement plan**
13. ☒ **Three (3) sets of Signing and Striping improvement plan**
14. ☒ **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15. ☒ **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
16. ☐ Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
17. ☒ **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
18. ☒ **One (1) copy of Hydrology/Drainage study**
19. ☐ One (1) copy of Soils/Geology report
20. ☒ **Payment for Final Map/Parcel Map processing fee**
21. ☒ **Three (3) copies of Final Map/Parcel Map**
22. ☒ **One (1) copy of approved Tentative Map**

23. ☒ **One (1) copy of Preliminary Title Report (current within 30 days)**

24. ☒ **One (1) copy of Traverse Closure Calculations**

25. ☒ **One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.**

26. ☐ Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use

27. ☐ Other: _____

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV17-022, A DEVELOPMENT PLAN (FILE NO. PDEV17-022) TO CONSTRUCT 31 SINGLE FAMILY HOMES (CLUSTER PRODUCT) ON 3.47 ACRES OF LAND, LOCATED ON THE SOUTH SIDE OF MISSION BOULEVARD, BETWEEN SAN ANTONIO AND OAKLAND AVENUES, WITHIN THE MDR-11 (LOW-MEDIUM DENSITY RESIDENTIAL 5.1 TO 11.0 DU/AC) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 1049-323-06, 1049-323-07, 1049-323-08, 1049-323-12 & 1049-323-13.

WHEREAS, North by Northwest Capital, Inc. ("Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV17-022, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 3.47 acres of land generally located on the south side of Mission Boulevard, between San Antonio and Oakland Avenues, a within the MDR-11 (Low-Medium Density Residential 5.1 to 11.0 du/ac), and is presently vacant; and

WHEREAS, the property to the north of the Project site is within the Business Park zoning district and is developed with residential and commercial uses. The property to the east is within the LDR-5 (Low-Density Residential) zoning district and is developed with single-family homes. The property to the south is within the LDR-5 (Low-Density Residential) zoning district and is developed with single-family homes. The property to the west is within the MDR-11 (Low-Medium Density Residential) zoning district and is developed with single-family homes; and

WHEREAS, The Development Plan proposes 21 units configured in a six-pack courtyard cluster design and 10 single family conventional units at a density of 9.0 dwelling units per acre; and

WHEREAS, The Development Plan proposes three 2-story floor plans with two or four architectural styles per plan. The three floor plans include the following:

- Plan 1: 1,611 square feet, 3 bedrooms and 21/2 baths.
- Plan 2: 1,688 & 1,696 square feet, 3 bedrooms and 21/2 baths.
- Plan 3: 1,969 square feet, 5 bedrooms and 3 baths; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, a Tentative Tract Map application (File No. PMTT17-007 (TT17624)) to subdivide the 3.47 acre project site into 31 single family lots and common areas and a Variance application (File No. PVAR17-007) to deviate from the minimum building arterial street setback, along Mission Boulevard, from 30 feet to 5 feet has been filed in conjunction with the Development Plan application; and

WHEREAS, on July 17, 2017, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB17-039 recommending the Planning Commission approve the Application; and

WHEREAS, on July 25, 2017, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: ***Environmental Determination and Findings.*** As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from environmental review pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2: ***Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 3: ***Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport ("ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport

Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the Planning Commission, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 4: ***Concluding Facts and Reasons.*** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the Low Medium Density (5.1 to 11.0 dc/ac) land use district of the Policy Plan Land Use Map, and the MDR- 11(Low Medium Density Residential) zoning district. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the MDR- 11(Low Medium Density Residential) zoning district, including standards relative to the particular land use proposed (single family residential), as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions.

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the MDR- 11(Low Medium Density Residential) zoning district, including standards relative to the particular land use proposed (single-family residential), as-well-as building intensity, building and parking

setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions.

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development*****.** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Development Code that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (single family residential). As a result of this review, the Planning Commission has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the Development Code.

SECTION 5: ***Planning Commission Action.*** Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 6: ***Indemnification.*** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 7: ***Custodian of Records.*** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 8: ***Certification to Adoption.*** The Secretary shall certify to the adoption of the Resolution.

- - - - - - - - - - - - - -

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of July, 2017, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Richard D. Delman
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Principal Planner/Acting Secretary of the
Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Marci Callejo, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. PC17-[insert #] was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 25, 2017, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Marci Callejo
Secretary Pro Tempore

ATTACHMENT A:

File No. PDEV17-022
Departmental Conditions of Approval

(Departmental conditions of approval to follow this page)

City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department
Land Development Division
Conditions of Approval

Meeting Date: July 17, 2017

File No: PDEV17-022

Related Files: PMTT17-007 (TT17624) and PVAR17-007

Project Description: A Development Plan (File No. PDEV17-022) to construct a 31 single family homes (Cluster Product) and a 3.47 acres of land, located on the south side of Mission Boulevard, between San Antonio and Oakland Avenues, within the MDR-11 (Low-Medium Density Residential 5.1 to 11.0 du/ac) zoning district. (APNs: 1049-323-06, 1049-323-07, 1049-323-08, 1049-323-12 & 1049-323-13); **submitted by North by Northwest Capital Inc.**

Prepared By: Rudy Zeledon, Principal Planner
Phone: 909.395.2422 (direct)
Email: rzeledon@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2010. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

(d) The project is contingent upon Planning Commission approval of the related Tract Map (File No. PMTT17-007) and Variance applications (File No. PVAR 17-007).

(e) All applicable City Departmental Conditions of Approval from the related Tract Map (File No. PMTT17-007) and Variance applications (File No. PVAR 17-007), shall apply to this Development Plan Application.

(f)

2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

(a) A 6-FT high decorative masonry block wall (split-face block, slump stone or stucco, etc.), with a decorative cap, shall be constructed at the following location(s):

1. Along the perimeter of all new residential developments, including all interior side and rear project boundaries, and street frontages without front-on units.

2. Along all street side and interior side yard property lines, and connecting between dwellings, with appropriate gates for rear yard access. (Note: Within the front yard setback walls shall be reduced to 3 FT in height.)

3. Along all rear property lines, except that on through-lots, the wall shall be setback 5 FT behind the rear property line.

4. Vinyl or wood fencing will not be permitted.

(b) Long expanses of fence or wall (50 or more FT in length) adjacent to a public right-of-way shall have offset areas (decorative pilasters or a jog in the wall) along its length, and shall be architecturally designed to prevent monotony.

(c) The height of a wall or fence shall be measured on the exterior side, at the highest point of the natural ground or finished grade at the base of the fence or wall, to the top of the fence or wall above the same base point.

(d) Development plans and construction drawings shall indicate materials, colors, and height of proposed and existing walls/fences, and shall include a cross-section of walls/fences indicating adjacent grades. Walls shall be designed as an integral part of the architecture for the development.

2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

2.6 Mechanical Equipment.

(a) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

2.7 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.8 Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

2.9 Environmental Review.

(a) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines.

(b) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to **Section 15332 (Class 32, In-Fill Development Projects)** of the CEQA Guidelines, meeting the following conditions:

(i) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations;

(ii) The proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses;

(iii) The project site has no value as habitat for endangered, rare, or threatened species;

(iv) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and

(v) The Project site can be adequately served by all required utilities and public services.

2.10 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.11 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.12 Additional Requirements.

2.13 Drive approaches serving a development project of 5 or more dwellings shall be delineated with enhanced paving treatment, such as interlocking pavers, textured and color pigmented concrete, or stamped concrete. Such treatment shall extend from the back of the drive approach to the first intersecting drive aisle or parking space. Please revise site plan and landscape plan to include.

2.14 Pedestrian pathways that cross driveways and drive aisles shall be delineated by enhanced paving treatments, such as interlocking pavers, and textured and/or color pigmented concrete. Please add to project. Will also need to include on the color and material board.

2.15 The private drive aisles (alleyways), providing garage access to each cluster courtyard unit, shall be enhanced with decorative interlocking pavers, textured and color pigmented concrete, or stamped concrete and subject to Planning Department review and approval.

2.16 The minimum interior dimensions for a single car garage shall be 10 FT wide by 20 FT long and 20 FT by 20 FT for a two-car garage.

2.17 Along pedestrian movement corridors, the use of low mounted bollard light standards, which reinforce pedestrian scale, shall be used. Steps, ramps and seatwalls should be illuminated wherever possible, with built-in light fixtures. Please incorporate along A Street and within all the common open space and open parking areas.

2.18 The design of light fixtures and their structural supports should be architecturally compatible with the main structures on the site. Light fixtures should be architecturally integrated into the design of a structure.

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
303 East "B" Street, Ontario, CA 91764

PRELIMINARY PLAN CORRECTIONS

Sign Off

Carolyn Bell, Sr. Landscape Planner	7/10/17
	Date

eviewer's Name:	Phone:
Carolyn Bell, Sr. Landscape Planner	**(909) 395-2237**

D.A.B. File No.:	Case Planner:
PDEV17-022 Rev 1	Luis Batres

Project Name and Location:
San Antonio Estates
S of Mission E of San Antonio, W of Oakland Aves

Applicant/Representative:
Louie Rodrigues, Busy Bee Development
8525 Enramada Ave
Whittier, CA 90605

☒	**A Preliminary Landscape Plan (dated 5/12/16) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.**
☐	**A Preliminary Landscape Plan ( ) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.**

CORRECTIONS REQUIRED

Civil Plans

1. Show existing trees (including all tree trunks) in good condition to remain and note trees proposed to be removed. Include existing trees within 15' of adjacent property that would be affected by new walls, footings or on-site tree planting. Add tree protection notes on construction and demo plans.
2. Show storm water infiltration devices, basins or swales.
3. Dimension basins and swales to be no greater than 50% of the on-site landscape area to allow for ornamental landscape. Provide a level grade minimum 4' from pedestrian paving for safety and min 5' along parking lots for hedge row and trees.
4. Show backflows and transformers on plan, and dimension a 4' set back from paving.
5. Locate light standards, fire hydrants, water and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans
6. Show corner ramp and sidewalk per city standard drawing 1213 – max 10' dimension for accessible ramp and sidewalk behind ramp.
7. Show any easements and identify.
8. Move tot lot out of the corner and toward the parking area for better visibility and distance from property line.
9. Note for compaction to be no greater than 85% at landscape areas. All finished grades at 1 ½" below finished surfaces. Slopes to be maximum 3:1.

Landscape Plans

10. Provide a tree inventory for existing trees include genus, species, trunk diameter, canopy width and condition. Show and note existing trees in good condition to remain and note trees proposed to be removed. Include existing trees within 15' of adjacent property that would be affected by new walls, footings or on-site tree planting. Show replacement trees for tree removed per the Development Code. Add tree protection notes on construction and demo plans.
11. Show all utilities on the landscape plans. Keep utilities clear of required tree locations.
12. Show Mission, San Antonio and Oakland Ave parkway landscape and street trees spaced 30' apart. The Designated street tree on San Antonio is: Koelreuteria panniculata; on Mission: Schinus molle; on Oakland; Geijera parviflora.
13. Show appropriate street or parking lot shade trees with min 30' canopy at maturity.

14. Call out type of proposed irrigation system (low water and efficient, drip line or similar) and include preliminary MAWA calculation.
15. Show landscape hydrozones to separate low water from moderate water landscape.
16. Note that irrigation plans shall provide separate systems for tree stream bubblers with pc screens.
17. Provide a planting list of proposed water efficient plants. Use high water turf for recreation areas only. Proposed water use must meet water budget.
18. Replace invasive, high water using, short lived, high maintenance or poor performing plants
19. Provide an appropriate hydroseed plant mix for water quality basins and swales.
20. Note for agronomical soil testing and include report on landscape plans. For phased projects, a new report is required for each phase or a minimum of every 6 homes in residential developments.
21. Call out all fences and walls, materials proposed and heights.
22. Call out proposed amenity, manufacturer, and model number. Include fall zones for play equip.
23. Show concrete mowstrips to identify property lines along open areas or to separate ownership or between maintenance areas.
24. Residential projects shall include a stub-out for future back yard irrigation systems with anti-siphon valves. All single family and multi-family residential front yards shall have landscape and irrigation.
25. Show minimum on-site tree sizes per the Landscape Development standards, see the Landscape **Planning website. 5% 48" box, 10% 36 box, 30% 24" box, 55% 15 gallon.** Use larger tree sizes for the corner accent trees.
26. Show 25% of trees as California native (Platanus racemosa, Quercus agrifolia, Quercus wislizenii, Quercus douglasii, Cercis occidentalis, Sambucus Mexicana, etc.) in appropriate locations.
27. Show all proposed sign locations (on buildings and in landscape) to avoid conflicts with trees, shrubs or basin areas.
28. Construction plans shall be designed and signed by a licensed landscape architect.
29. Landscape construction plans shall meet the requirements of the Landscape Development Guidelines. See http://www.ontarioca.gov/landscape-planning/standards, Development and Municipal codes.
30. **After a project's entitlement approval, the applicant shall pay all applicable fees for landscape** plan check and inspections at a rate established by resolution of the City Council. Typical fees are:

Plan Check—5 or more acres	$2,326.00
Plan Check—less than 5 acres	$1,301.00
Inspection—Construction (up to 3 inspections)	$278.00
Inspection—Field - additional	$83.00

Landscape construction plans with building permit number for plan check may be emailed to: landscapeplancheck@ontarioca.gov

AIRPORT LAND USE COMPATIBILITY PLANNING
CONSISTENCY DETERMINATION REPORT

ONTARIO AIRPORT PLANNING

Project File No.: PDEV17-022 & PMTT7-007

Address: SWC Oakland & Mission Blvd

APN: 1049-323-06, 07, 08, 12 & 13

Existing Land Use: Vacant

Proposed Land Use: 31 single family detached homes

Site Acreage: 3.47 Proposed Structure Height: 26 ft

ONT-IAC Project Review: n/a

Airport Influence Area: ONT

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Luis Batres

Date: 5/26/17

CD No.: 2017-038

PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
☐ Zone 1	☐ 75+ dB CNEL	☐ High Terrain Zone	☑ Avigation Easement Dedication
☐ Zone 1A	☐ 70 - 75 dB CNEL	☑ FAA Notification Surfaces	☐ Recorded Overflight Notification
☐ Zone 2	☑ 65 - 70 dB CNEL	☑ Airspace Obstruction Surfaces	☐ Real Estate Transaction Disclosure
☐ Zone 3	☐ 60 - 65 dB CNEL	☐ Airspace Avigation Easement Area	
☐ Zone 4		Allowable Height: 170 ft	
☐ Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

☐ Zone 1 ☐ Zone 2 ☐ Zone 3 ☐ Zone 4 ☐ Zone 5 ☐ Zone 6

Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: ☐ Exempt from the ALUCP ☐ Consistent ● Consistent with Conditions ☐ Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT provided the following conditions are met:

Airport Planner Signature: Lorena Mejia

CD No.: 2017-038
PALU No.: _____

PROJECT CONDITIONS

1. The proposed project shall maintain a minimum density of 8 dwelling units per acre.

2. An avigation easement shall be recorded prior to building permit issuance.

3. New Residential land uses are required to have a Recorded Overflight Notification appearing on the Property Deed and Title incorporating the following language:

(NOTICE OF AIRPORT IN VICINITY: This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.)

CITY OF ONTARIO
MEMORANDUM

TO: PLANNING DEPARTMENT, Luis Batres

FROM: BUILDING DEPARTMENT, Pedro Rico

DATE: May 25, 2017

SUBJECT: PDEV17-022

☒ The plan **does** adequately address the departmental concerns at this time.

☐ No comments

☒ Report below.

Conditions of Approval

1. Per the proposed tract map, PMTT17-007, the exterior walls of the dwelling units will have the fire separation distances to the property lines to 5' or less. Fire-resistance construction is to be provided on the projections, openings, penetrations, and construction of exterior walls in accordance with the building codes.

PR:lm

CITY OF ONTARIO
MEMORANDUM

TO: **Luis Batres, Planning Department**

FROM: **Douglas Sorel, Police Department**

DATE: **June 8, 2017**

SUBJECT: **PDEV17-022 – A DEVELOPMENT PLAN TO CONSTRUCT 31 SINGLE FAMILY DETACHED HOMES SOUTH OF MISSION BOULEVARD AND BETWEEN SAN ANTONIO AND OAKLAND AVENUES**

The "Standard Conditions of Approval" contained in Resolution No. 2010-021 apply. The applicant shall read and be thoroughly familiar with these conditions, including, but not limited to, the requirements below.

- Required lighting for walkways, driveways, doorways, parking lots, and other areas used by the public shall be provided. Lights shall operate via photosensor. Photometrics shall be provided to the Police Department and include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.

The Applicant is invited to contact Douglas Sorel at (909) 395-2873 with any questions or concerns regarding these conditions.

CITY OF ONTARIO
MEMORANDUM

TO: **Luis Batres, Senior Planner**
Planning Department

FROM: **Lora L. Gearhart, Fire Protection Analyst**
Fire Department

DATE: **June 20, 2017**

SUBJECT: **PDEV17-022 – A Development Plan to construct 31 single-family cluster dwellings on 3.47 acres of land, located on the south side of Mission Boulevard, between San Antonio and Oakland Avenues, within the MDR-11 (Low-Medium Density Residential ¿ 5.1 to 11.0 DU/acre) zoning district (APNs: 1049-323-06, 1049-323-07, 1049-323-08, 1049-323-12 & 1049-323-13). Related File: PMTT17-007 (TT 17624).**

☒ The plan **does** adequately address Fire Department requirements at this time.

- ☐ No comments.
- ☒ Standard Conditions of Approval apply, as stated below.

SITE AND BUILDING FEATURES:

A. 2013 CBC Type of Construction: Type V-B wood frame

B. Type of Roof Materials: non-rated

C. Ground Floor Area(s): Various

D. Number of Stories: Two Story

E. Total Square Footage: Various

F. 2013 CBC Occupancy Classification(s): R-3, U

CONDITIONS OF APPROVAL:

1.0 GENERAL

☒ 1.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov, click on "Fire Department" and then on "Standards and Forms."

☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty (20) ft. wide. See Standard #B-004.

☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25') inside and forty-five feet (45') outside turning radius per Standard #B-005.

☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150') in length shall have an approved turn-around per Standard #B-002.

3.0 WATER SUPPLY

☒ 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 1500 gallons per minute (g.p.m.) for 2 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.

☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.

☒ 3.4 The public water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 13 D. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more

shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

5.0 BUILDING CONSTRUCTION FEATURES

☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.

☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.

☒ 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.

☒ 5.5 All residential chimneys shall be equipped with an approved spark arrester meeting the requirements of the California Building Code.

PLANNING COMMISSION STAFF REPORT

July 25, 2017

SUBJECT: A Specific Plan (Colony Commerce Center West) request (File No. PSP15-001**)** to establish land use designations, development standards, design guidelines and infrastructure improvements for approximately 123 acres of land, which includes the potential development of 2,951,146 square feet of industrial development. The project site is bounded by Merrill Avenue to the north, Remington Avenue to the south, Carpenter Avenue to the west and the Cucamonga Creek Flood Control Channel to the east; **submitted by Cap Rock-Partners.**

PROPERTY OWNER: CLDFI Remington LLC.

RECOMMENDED ACTION: That the Planning Commission recommend that the City Council adopt and certify an Environmental Impact Report (EIR) including the adoption of a Statement of Overriding Consideration for **File No. PSP15-001** and approve the Colony Commerce Center West Specific Plan **(File No. PSP15-001)**, pursuant to the facts and reasons contained in the staff report and attached resolution(s), and subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The project site is made up of eight separate parcels comprising 123.17 acres of land. The site is bounded by Merrill Avenue to the north, Remington Avenue to the south, Carpenter Avenue to the west and the Cucamonga Creek Flood Control Channel to the east, within the Ontario Ranch Area of the City, as depicted in ***Figure 1: Project Location***. The project site currently slopes from north to south at just over 1 percent. The site is bounded to the east by the Cucamonga Creek Flood Control Channel, the City of Eastvale to the southeast, and the City of Chino to the west and southwest.

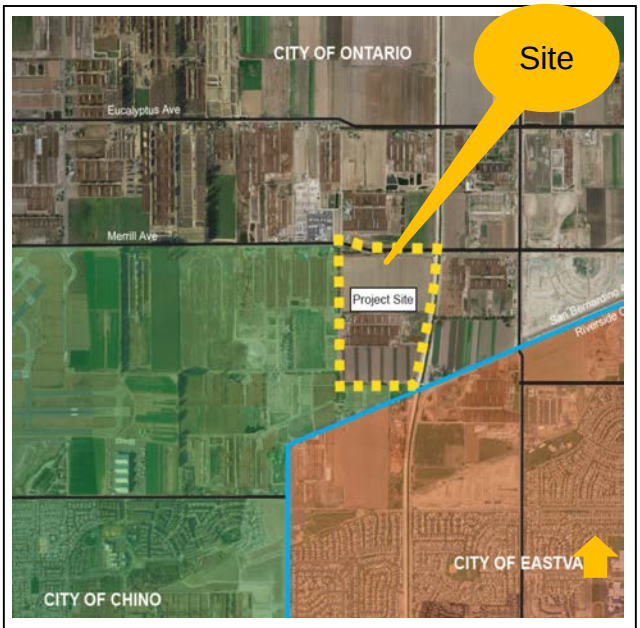

Figure 1: Project Location

Case Planner:	Luis E. Batres	*Hearing Body*	*Date*	*Decision*	*Action*
Planning Director Approval:		*DAB*			
		ZA			
Submittal Date:	January 14, 2015	*PC*	7-25-17		Recommend
Hearing Deadline:	n/a	*CC*	9-5-17		Final

PROJECT ANALYSIS:

[1] Background — The Ontario Plan (TOP) Policy Plan (General Plan) provides the basic framework for development within the 8,200-acre area commonly referred to as Ontario Ranch. The Policy Plan requires City Council approval of a Specific Plan for new developments within Ontario Ranch. Specific Plans are required to ensure that sufficient land area is included to achieve cohesive, unified districts and neighborhoods. Specific Plans are required to incorporate a development framework for detailed land use, circulation, infrastructure (including drainage, sewer, and water facilities), provision for public services (including parks and schools), and urban design and landscape plans.

[2] Specific Plan — The Colony Commerce Center West Specific Plan (File No. PSP15-001) serves to implement the City's Policy Plan for the project site and provides zoning regulations for development of the project site by establishing permitted land use, development standards, infrastructure requirements, and implementation requirements for the development of 123.17 acres within the Specific Plan boundaries. The Specific Plan establishes a comprehensive set of design guidelines and development regulations to guide and regulate site planning, landscape, and architectural character, and ensuring that excellence in community design is achieved during project development. The Colony Commerce Center West Specific Plan establishes the procedures and requirements to approve new development within the project site to ensure TOP goals and policies are achieved.

The overall land use concept for the Colony Commerce Center West Specific Plan takes advantage of the site's proximity to airports and regional freeway access. The land use concept provides for a range of industrial uses, while offering a variety of development, employment opportunities and opportunities for a broad range of industries to accommodate an ever-changing business and industrial environment.

The Specific Plan identifies the land use intensity anticipated in the two planning areas (see ***Figure 2: Land Use Plan***)**.** The maximum Floor Area Ratio (FAR) permitted in each Planning Area conforms to the maximum 0.55 FAR permitted in the Policy Plan (General Plan) Land Use Plan for industrial designations.

Planning Area 1, located along the northern portion of the Specific Plan area, is 57.58 acres in size and can potentially be developed with 1,379,501 square feet of industrial development. Planning Area 2, located along the southern portion of the Specific Plan is 65.60 acres in size and can potentially be developed with 1,571,645 square feet of industrial development **(**see ***Figure 3: Land Use Summary Table***).

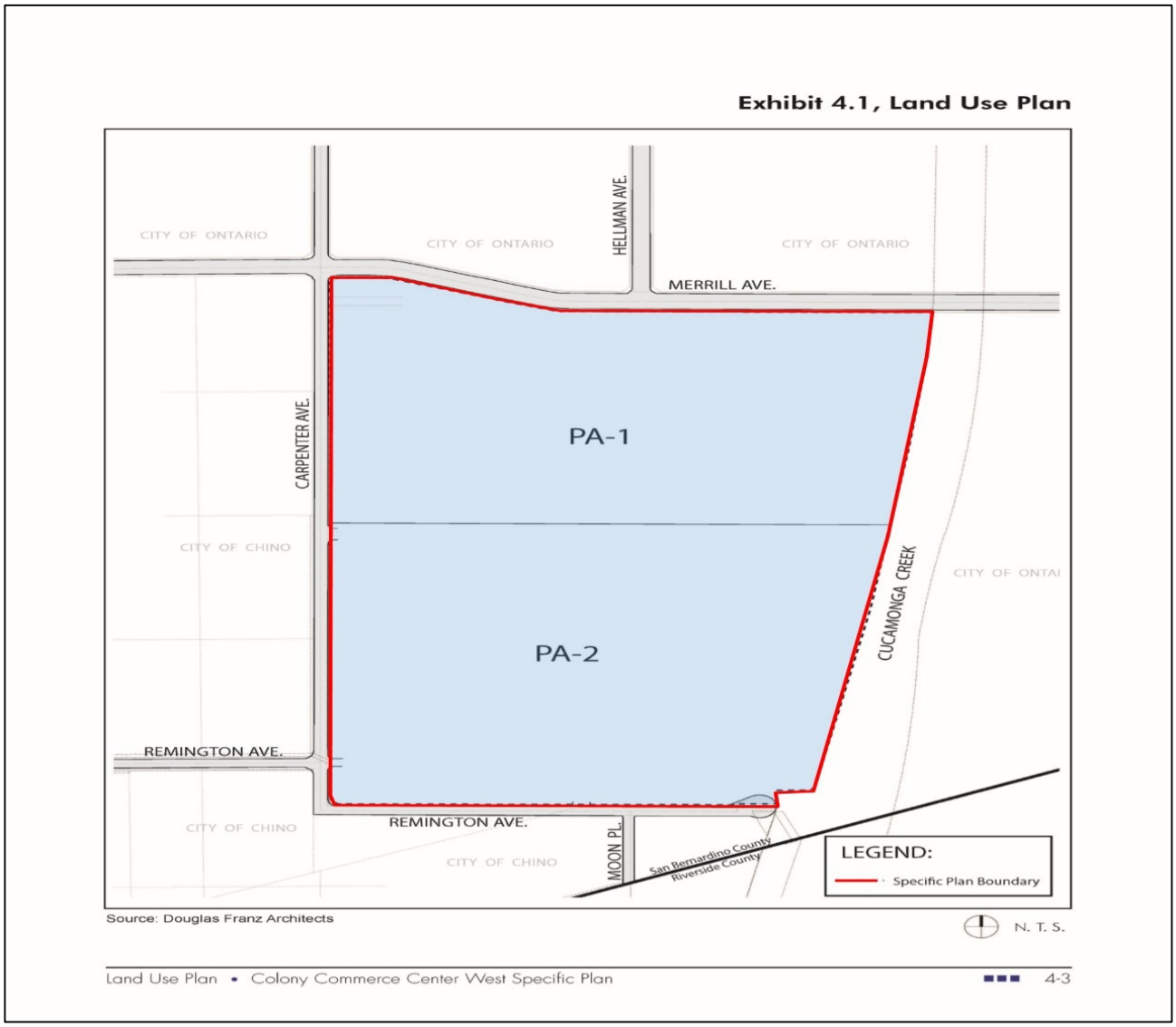

Figure 2: Land Use Plan

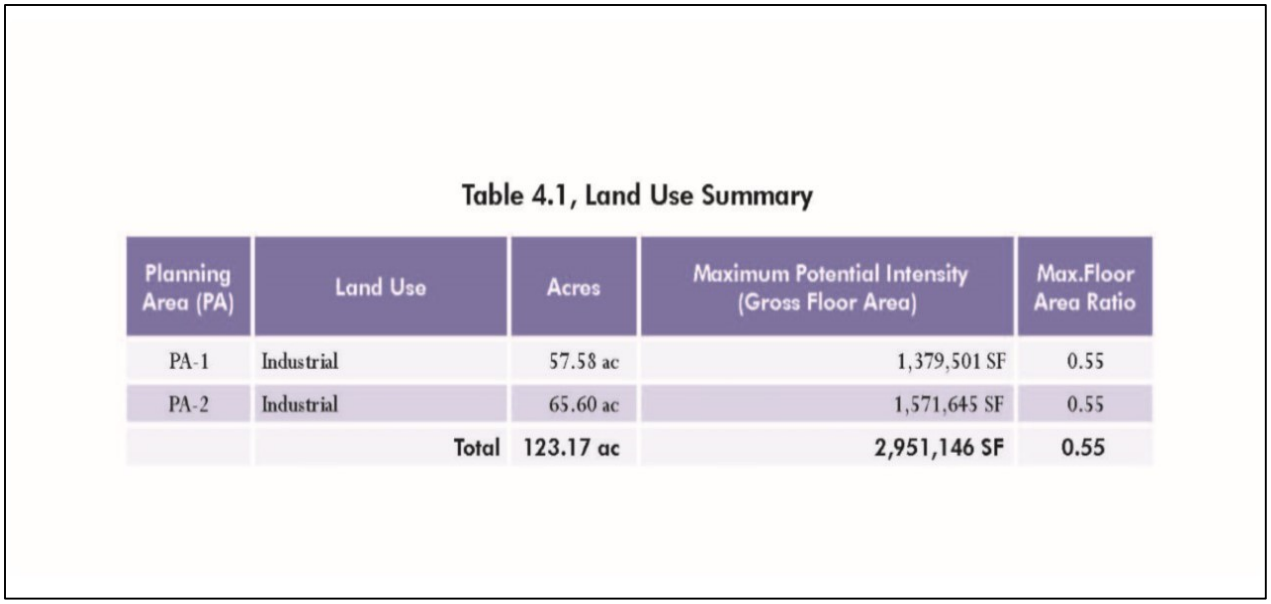

Table 4.1, Land Use Summary

Planning Area (PA)	Land Use	Acres	Maximum Potential Intensity (Gross Floor Area)	Max.Floor Area Ratio
PA-1	Industrial	57.58 ac	1,379,501 SF	0.55
PA-2	Industrial	65.60 ac	1,571,645 SF	0.55
	Total	123.17 ac	2,951,146 SF	0.55

Figure 3: Land Use Summary Table

Specific Plan Design Concept — The design theme and site design of the Colony Commerce Center West Specific Plan was created to ensure that the Specific Plan creates an environment that reflects the vision embodied for the industrial community in the following areas:

- Develop a quality, cohesive design concept and identity for the Colony Commerce Center West area.

- Establish development standards that ensure lasting value for the industrial developments.

- The architectural image of the Specific Plan will be perceived primarily from the public realm. Therefore, building massing, scale and roof forms, as the primary design components require articulation in their architectural expression as they relate to the public view.

- A theme wall/entry monument may be installed at the major project entries at the discretion of the builder or project developer.

- Site design should facilitate the intended functions of developed and open space areas and provide for appropriate interactions between buildings and activity areas, good movement, vehicular access and parking, and pedestrian and bicycle travel.

- Buildings should be oriented to define the streetscene and provide for an aesthetically pleasing streetscape.

- Major vehicular and pedestrian entries to the site from the public street system should be readily visible.

The Colony Commerce Center West Specific Plan Design Guidelines have been established to promote high-quality architecture as required by the Ontario Development Code and The Ontario Plan (TOP). The architecture theme of the Specific Plan will be a tilt-up Contemporary Style.

The design guidelines of the Specific Plan will require buildings to be built with a recognizable base, body, roofline and entries. In addition, all buildings will be required to provide substantial window glazing along the storefronts office areas, incorporate material changes such as stone or metal, wall and roof articulation and rich detailing. Buildings will be further enhanced with decorative lighting and plaza areas, employee outdoor plaza/patio areas that will feature decorative paving, accent plants, decorative lighting and specimen trees.

Architecture — The Colony Commerce Center West Specific Plan Design Guidelines have been established to promote high-quality architecture as required by the Ontario Development Code and The Ontario Plan (TOP). Since it is envisioned that the site will be developed with industrial development, the architecture style will be a tilt-up Contemporary Style. The architecture style will be similar to what has been developed within the Meredith Specific Plan at the southeast corner of Fourth Street and Vineyard Avenue (see ***Figure 4: QVC Building***). All buildings will be required to provide a recognizable base, body, roofline and entry. ***Figure 5*** & ***Figure 6*** below illustrate conceptual building concepts of what is envisioned to be constructed within Specific Plan.

Figure 4: QVC Building Example

Figure 5: Building perspective at the Southwest Corner of Carpenter and Remington Avenue

Figure 6: Building(s) perspective along Carpenter Avenue

Circulation Concept — The circulation plan for Colony Commerce Center Specific Plan reinforces the objective of moving vehicles, pedestrians, cyclists, and public transit safely and efficiently through and around the project. The Specific Plan establishes the hierarchy and general location of roadways within Colony Commerce Center West (**See Figure 7: Circulation Plan**). Primary access into the Specific Plan Area will be from Merrill Avenue on the north, Carpenter Avenue on west and Remington Avenue on the south.

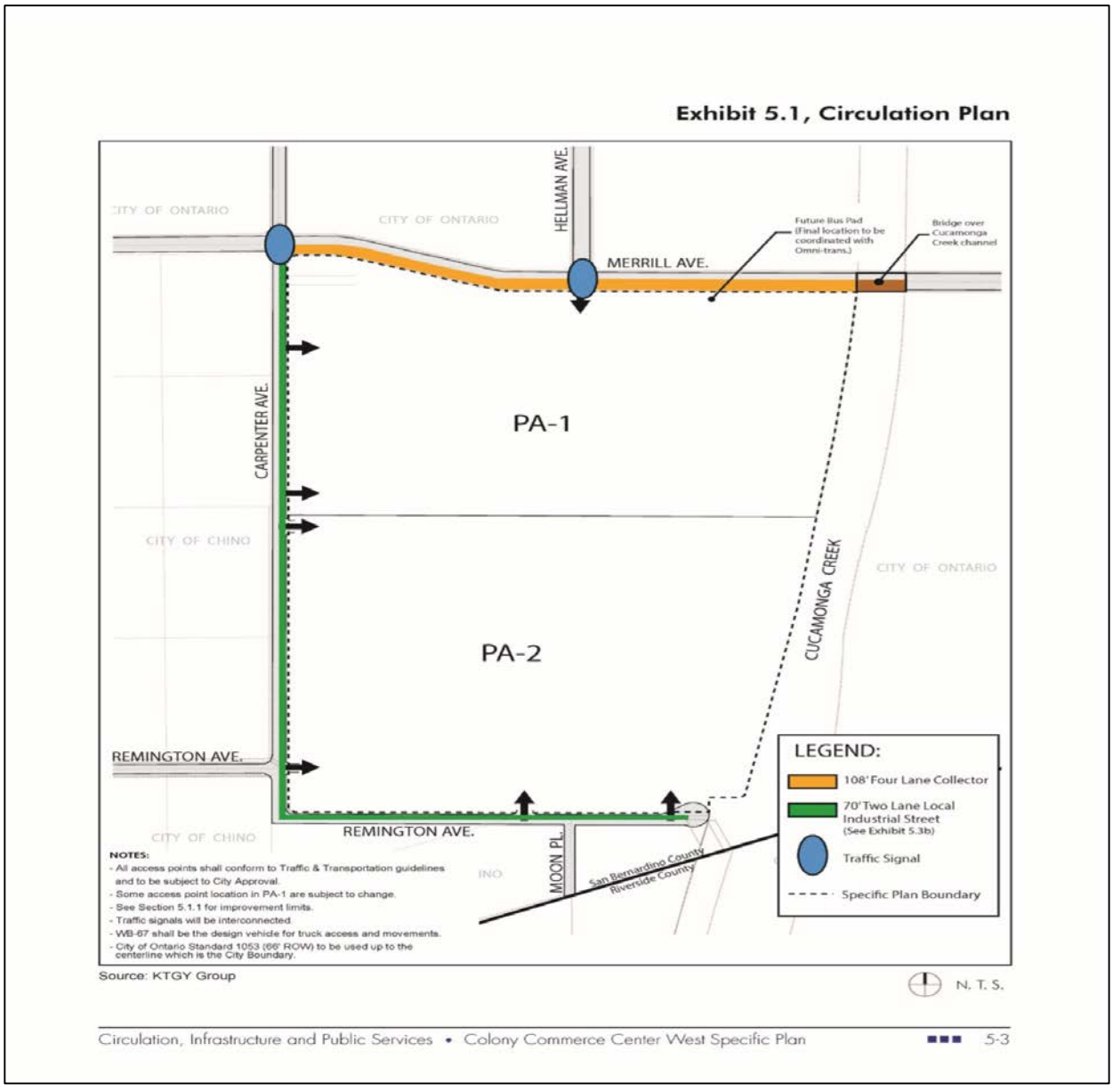

Figure 7: Circulation Plan

Merrill Avenue will be designed to be widened to a four (4) lane (108') collector street, and Carpenter and Remington Avenues designed and widened to two-lane, local industrial streets (70-foot right-of-way). The easterly end of Remington Avenue will be designed to terminate with a cul-de-sac.

Merrill Avenue will also be improved with a 7-foot wide curb adjacent landscape parkway, a 5-foot wide sidewalk, and a 23-foot landscape edge. Carpenter and Remington Avenues will each be improved with a 4-foot wide curb adjacent landscape parkway, and a 5-foot wide sidewalk.

Planning Area 1 has been designed to provide two points of access along Carpenter Avenue and one along Merrill Avenue. Planning Area 2 has been designed to provide two access points along Carpenter Avenue and two along Remington Avenue. The Merrill and Carpenter Avenue intersection is proposed to be a signalized intersection and improvements will be installed when the first development on the site occurs. The intersection at Merrill Avenue and Hellman Avenue is also proposed to be signalized, however, the signal improvement will not be completed until development along the north side of Merrill Avenue occurs.

Landscaping Design — The landscape design theme for the Specific Plan includes a plant palette (Table 7.1 of the Specific Plan) that outlines plant materials and trees to be used in parking lots, street parkways, sign monument areas, and adjacent to buildings.

Development within the Specific Plan will be required to provide a minimum landscape coverage of 10%. Merrill Avenue will be required to provide a 23-foot landscape setback. Carpenter and Remington Avenues will each provide a 10-foot landscape setback, and along the Cucamonga Creek Flood Control Channel, a 5-foot landscape setback will be provided. In addition, Merrill Avenue will be designed with a 7-foot wide curb adjacent landscape parkway, a 5-foot wide sidewalk, and a 23-foot landscape edge. Carpenter Avenue and Remington Avenue, each will be designed with a 4-foot wide curb adjacent landscape parkway, and a 5-foot wide sidewalk (see **Figure 8, 9 & 10: Typical Landscape Street Cross Sections**).

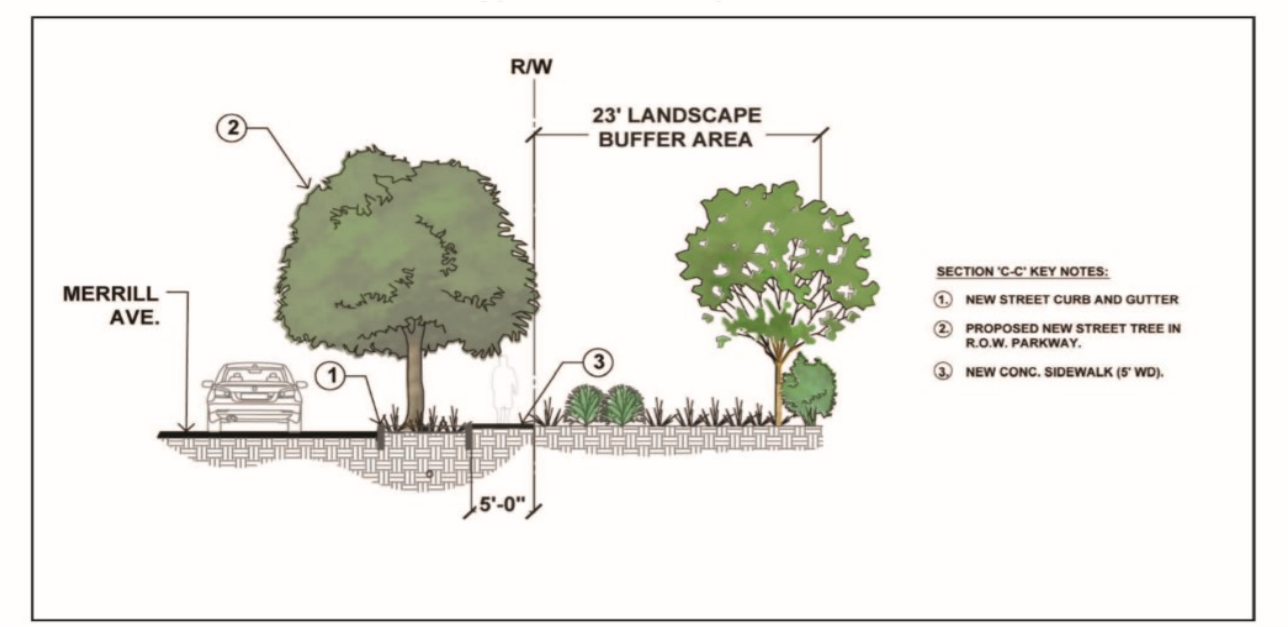

Figure 8: Merrill Avenue Cross Section

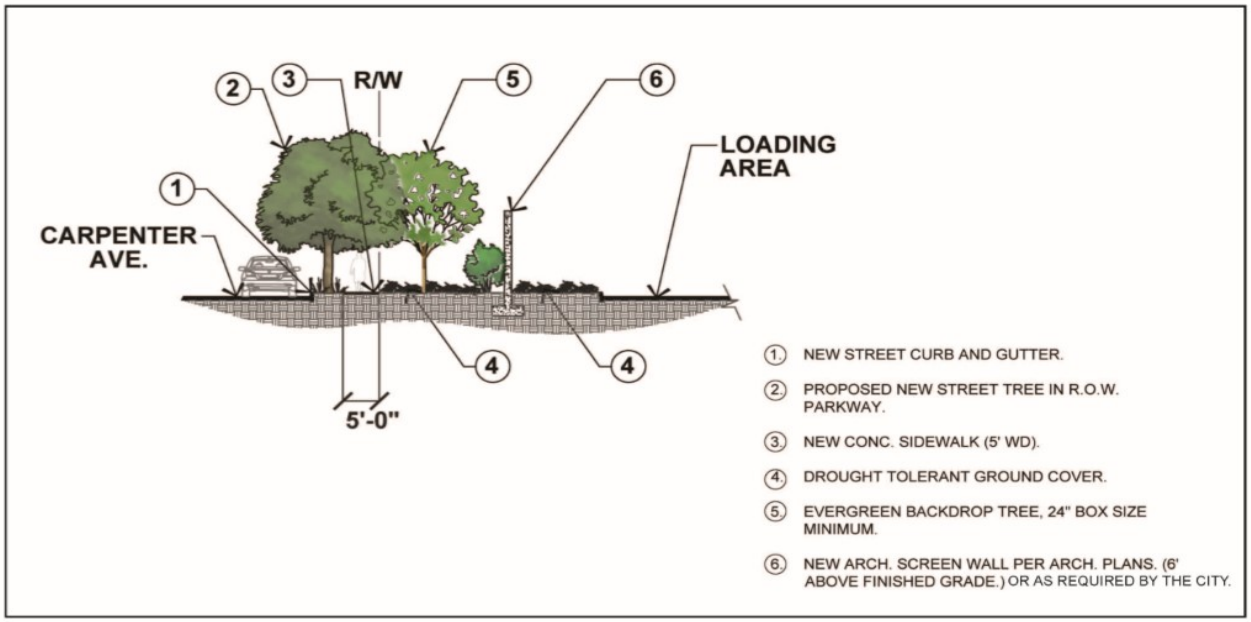

Figure 9: Carpenter Avenue Cross Section

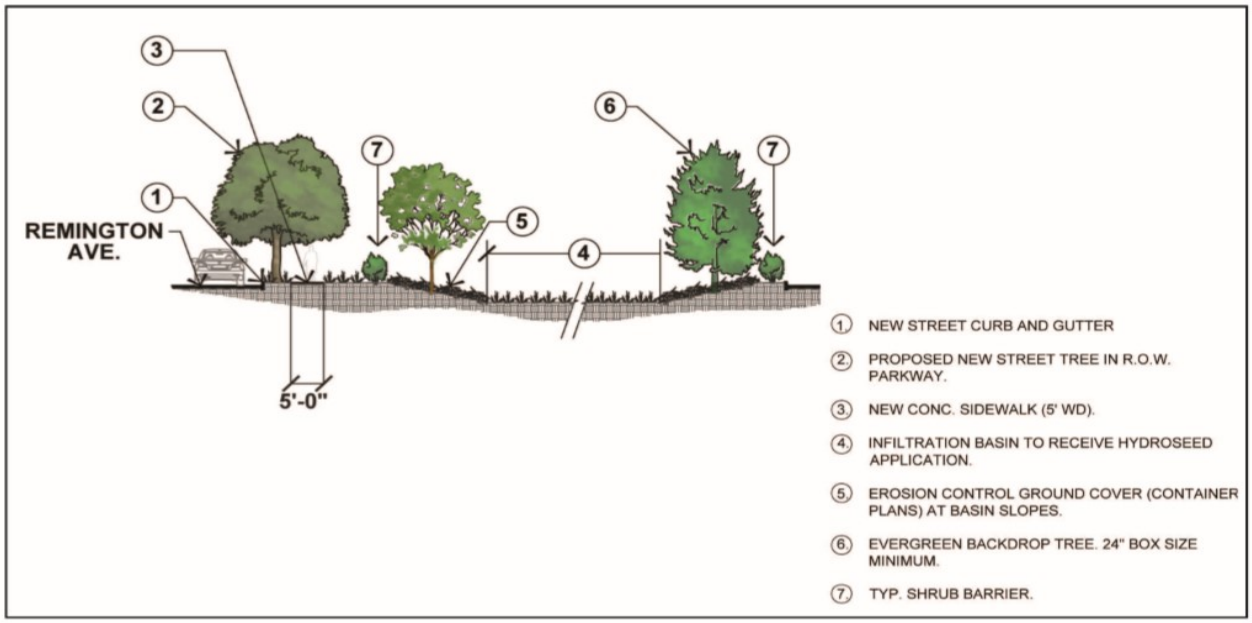

Figure 10: Remington Avenue Cross Section

Infrastructure and Services — Backbone infrastructure to serve all areas of Specific Plan will be installed by the developer(s) in accordance with the Ontario Ranch (New Model Colony) Master Plans for streets, water (including recycled water), sewer, storm drain, and fiber optic facilities. Natural gas will be provided by The Gas Company and electricity by SCE. Development of the project requires the installation by the developer of all infrastructure necessary to serve the project as a standalone development.

Specific Plan Phasing— Development phasing within the Specific Plan will be determined by the various developers, based upon the real estate market conditions. Specific infrastructure, community facilities and open space dedications will be provided/conditioned with individual tract maps and/or development plans.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP).

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits the adoption and administration of specific plans as an implementation tool for elements contained in the local general plan. Specific plans must demonstrate consistency in regulations, guidelines, and programs with the goals and policies set forth in the general plan. The Colony Commerce West Specific Plan has been prepared in conformance with the goals and policies of the City of Ontario Policy Plan (General Plan). The policy analysis in *Appendix "Policy Plan (General Plan) Consistency,"* of the Specific

Plan describes the manner in which the Colony Commerce West Specific Plan complies with the Policy Plan goals and policies applicable to the Colony Commerce West Specific Plan. In addition, the Specific Plan more specifically, implements the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)
- Ensure the Development of a Well Planned, Balanced, and Self-Sustaining Community in the New Model Colony

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The project site is located within the Airport Influence Area of the Ontario International Airport (ONT) and has been found to be consistent with the policies and criteria set forth within the ALUCP for ONT. The project site is also located within the Airport Influence of Chino Airport and is consistent with policies and criteria set forth within the 2011 California Airport Land Use Planning Handbook published by the California Department of Transportation, Division of Aeronautics.

ENVIRONMENTAL REVIEW: The Specific Plan is located in the City of Ontario in what is part of the approximately 8,200-acre area within the City of Ontario Sphere of Influence (SOI). On January 7, 1998, the City of Ontario adopted the New Model Colony (NMC) General Plan Amendment (GPA) setting forth a comprehensive strategy for the future development of the SOI. The NMC is bound by Riverside Drive to the north, Milliken Avenue to the east, Euclid Avenue to the West and Merrell Avenue/Bellegrave to the south.

On January 27, 2010, the City adopted The Ontario Plan (TOP) and certified the accompanying EIR. TOP serves as the City's new General Plan for the entire City, including the NMC (now referred to Ontario Ranch). TOP identified many areas that might have a potentially significant impact on the environment. These areas included: 1) Aesthetics; 2) Biological Resources; 3) Geology and Soils; 4) Hazards and Hazardous Materials; 5) Hydrology and Water Quality; 6) Land Use and Planning; 7) Mineral Resources; 8) Population and Housing; 9) Public Services; 10) Recreation; and 11) Utilities and Service Systems. Through the EIR process these potential impacts were analyzed, revisions were incorporated into the plan and/or mitigation measures were

identified that reduced the potential environmental impacts to a level that was less than significant.

TOP also identified several potential impacts that, even with revisions and/or mitigation measures, could not be reduced to a level of less than significant. These areas included:

- *Agriculture Resources* –

  Impact 5.2-1. Buildout of TOP would convert 3,269.3 acres of California Resource Agency designated Prime Farmland, Unique Farmland, and Farmland of Statewide Importance to residential, commercial, mixed-use, and industrial land uses. Consequently, Impact 5.2-1 would remain significant and unavoidable and a Statement of Overriding Considerations would be required.

  Impact 5.2-2. There are a number of Williamson Act contracts within the City that have yet to expire. Buildout of TOP would most likely require the cancellation or nonrenewal of these contracts. The current use of these contracts would slow the rate of conversion from agricultural to nonagricultural land but it would not impede the conversion. Since there are some Williamson Act contracts still active in the New Model Colony, implementation of the proposed land use plan for The Ontario Plan would conflict with these contracts and cause a significant impact. Consequently, Impact 5.2-2 would remain significant and unavoidable and a Statement of Overriding Considerations would be required.

  Impact 5.2-3. Development of the City in accordance with TOP would increase the amount of nonagricultural land uses. When nonagricultural land uses are placed near agricultural uses, the odors, noises, and other hazards related to agriculture conflict with the activities and the quality of life of the people living and working in the surrounding areas. Consequently, conversion of agricultural uses in the city may cause farms and agricultural land uses outside the City to be converted to nonagricultural uses because of the nuisances related to agriculture. Impact 5.2-3 would remain significant and unavoidable. Therefore, a Statement of Overriding Considerations would be required.

- *Air Quality* –

  Impact 5.3-1. The project would not be consistent with the Air Quality Management Plan (AQMP) because air pollutant emissions associated with buildout of the City of Ontario would cumulatively contribute to the nonattainment designations in the South Coast Air Basin (SoCAB). Furthermore, buildout of the Proposed Land Use Plan would exceed current estimates of population, employment, and vehicle miles traveled for Ontario and therefore these emissions are not included in the current regional emissions inventory for the SoCAB. As both criteria must be met in order for a project to be considered consistent with the AQMP, the project would be

considered inconsistent with the AQMP. Consequently, Impact 5.3-1 would remain significant and unavoidable and a Statement of Overriding Considerations would be required.

Impact 5.3-2. Construction activities associated with buildout of TOP would generate short-term emissions that exceed the South Coast Air Quality Management District's (SCAQMD) regional significance thresholds; cumulatively contribute to the SoCAB's nonattainment designations for O_3, PM_{10}, and $PM_{2.5}$; and potentially elevate concentrations of air pollutants at sensitive receptors. Consequently, Impact 5.3-2 would remain significant and unavoidable and a Statement of Overriding Considerations would be required.

Impact 5.3-3. Buildout of TOP would generate long-term emissions that would exceed SCAQMD'S regional significance thresholds and cumulatively contribute to the SoCAB nonattainment designations for O_3, PM_{10}, and $PM_{2.5}$. Consequently, Impact 5.3-3 would remain significant and unavoidable and a Statement of Overriding Considerations would be required.

Impact 5.3-5. Approval of residential and other sensitive land uses within 500 feet of I-10, I-15, or SR-60 would result in exposure of persons to substantial concentrations of diesel particulate matter. Consequently, Impact 5.3-5 would remain significant and unavoidable and a Statement of Overriding Considerations would be required.

Impact 5.3-6. Conversion of agricultural land to nonagricultural uses would temporarily expose residents to objectionable odors. Consequently, Impact 5.3-6 would remain significant and unavoidable and a Statement of Overriding Considerations would be required.

- *Cultural Resources* –

Impact 5.5-1. Although protective regulations are in place and preservation policies are included in TOP, implementation of the Proposed Land Use Plan, especially within growth focus areas, has the potential to impact Tier III historic resources. Mitigation Measure 5-1 would require a historical evaluation for properties within historic resources in the Focus Areas under the City's ordinance. However, the ordinance does not provide a high level of protection for Tier III resources. As a result, historical resources categorized under the Ordinance as Tier III could potentially be impacts with implementation of the Proposed Land Use Plan. Consequently, Impact 5.5-1 would remain significant and unavoidable and a Statement of Overriding Considerations would be required.

- *Global Climate Change* –

  Impact 5.6-1. Buildout of the City of Ontario would generate greenhouse gas emissions that would significantly contribute to global climate change impacts in California. GHG emissions generated in the City would significantly contribute to climate change impacts in California as a result of the growth in population and employment in the City and scale of development activity associated with buildout of the Proposed Land Use Plan. Consequently, Impact 5.6-1 would remain significant and unavoidable and a Statement of Overriding Considerations would be required.

- *Noise* –

  Impact 5.12-1. Buildout of the Proposed Land Use Plan would result in an increase in traffic on local roadways in the City of Ontario, which would substantially increase noise levels. Consequently, Impact 5.12-1 would remain significant and unavoidable and a Statement of Overriding Considerations would be required.

  Impact 5.12-2. Noise-sensitive uses could be exposed to elevated noise levels from transportation sources. Any siting of new sensitive land uses within a noise environment that exceeds the normally acceptable land use compatibility criterion would result in a potentially significant impact and would require a separate noise study through the development review process to determine the level of impacts and required mitigation. Consequently, Impact 5.12-2 would remain significant and unavoidable and a Statement of Overriding Considerations would be required.

  Impact 5.12-3. Construction activities associated with buildout of the individual land uses associated with the Proposed Land Use Plan would expose sensitive uses to strong levels of groundborne vibration. Consequently, Impact 5.12-3 would remain significant and unavoidable and a Statement of Overriding Considerations would be required.

  Impact 5.12-5. Construction activities associated with buildout of the individual land uses associated with the Proposed Land Use Plan would substantially elevate noise levels in the vicinity of sensitive land uses. Consequently, Impact 5.12-5 would remain significant and unavoidable and a Statement of Overriding Considerations would be required.

  Impact 5.12-6. Noise-sensitive land uses within the 65 dBA CNEL contour of the Los Angeles/Ontario International Airport would be exposed to substantial levels of airport-related noise. Consequently, Impact 5.12-6 would remain significant and unavoidable and a Statement of Overriding Considerations would be required.

- *Transportation and Traffic* –

  Impact 5.15-1. Buildout of the Proposed Land Use Plan would result in additional traffic volume that would significantly cumulatively contribute to main-line freeway segment impacts. The City's development impact fees cannot be used for improvements to roadway facilities under Caltrans jurisdiction. Consequently, impacts to freeway segments within the City under Impact 5.16-1 would be significant and unavoidable and a Statement of Overriding Considerations would be required.

While these impacts will be significant and unavoidable, the City determined that the benefits of the Ontario Ranch development outweigh the potential unavoidable, adverse impacts of the plan. As a result, the City adopted a Statement of Overriding Considerations for those impacts that could not be fully mitigated to a level of less than significant.

Even though an EIR was prepared for TOP, the analyses focused on the program or "big picture" impacts associated with development. With the submittal of the Colony Commerce Center Specific Plan, staff is charged with evaluating the potential impacts of development at the project level. Staff completed an Initial Study for the project and determined that an EIR should be prepared for the Colony Commerce Center Specific Plan. Through the Initial Study preparation and scoping meeting discussion, an EIR was prepared addressing the following issues:

- Aesthetics
- Agricultural Resources
- Air Quality and Greenhouse Gas
- Biological Resources
- Cultural Resources and Historic Resources
- Geology and Soils
- Hazards and Hazardous Materials
- Hydrology and Water Quality
- Land Use and Planning
- Population and Housing
- Noise
- Public Services
- Transportation and Circulation
- Utilities and Service Systems

The Colony Commerce Center Specific Plan EIR evaluates each of these various areas and identifies mitigation measures and/or revisions to the plan to lessen the level of significance. With the implementation of the various mitigation measures, many of the potential adverse impacts can be reduced to a level of less than significant. Of the 14

areas considered by the EIR, all but three of the impact areas were mitigated a level of less than significant. The three remaining impact areas, even with the mitigation measures, could not be reduced to less than significant, resulting in some impacts remaining potentially significant and unavoidable. These areas include:

- Air Quality **-** Impacts related to a net increase in criteria pollutants would remain significant and unavoidable with the implementation of recommended mitigation measures.

- Agricultural Resources **-** Project-specific impacts and cumulative impacts would remain significant and unavoidable.

- Transportation and Traffic – Level of service (LOS) impacts related to intersections are projected to be cumulatively significant and unavoidable.

While mitigation of all potential impacts to a level of less than significant is desirable, the fact that three areas will remain significant and unavoidable is not unexpected. The identification of these areas as significant and unavoidable validates the work previously completed for TOP. Staff believes that the benefits of the proposed development outweigh the potential impacts associated with it. Therefore, staff recommends the Planning Commission recommend certification of the EIR to the City Council and that a Statement of Overriding Considerations be adopted for the project.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	Existing Land Use	*General Plan Designation*	*Zoning Designation*	*Specific Plan Land Use*
Site	Vacant/Agriculture	Industrial	SP (AG)	Industrial
North	Agriculture/Dairy	Industrial	SP (AG)	Industrial
South	Vacant/Agriculture (City of Chino)	Agriculture & Industrial	Agriculture & Light Industrial	n/a
East	Vacant/Agriculture	Business Park & Industrial	SP (AG)	n/a
West	Vacant/Industrial (City of Chino)	Industrial	Industrial	n/a

Colony Commerce Center West Environmental Impact Report

(provided under separate cover)

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL CERTIFY THE PROJECT ENVIRONMENTAL IMPACT REPORT (SCH# 2015061023) AND ADOPT FINDINGS OF OVERRIDING CONSIDERATIONS FOR THE COLONY COMMERCE CENTER WEST SPECIFIC PLAN (FILE NO. PSP15-001), LOCATED WITHIN THE ONTARIO RANCH AND BOUNDED BY MERRILL AVENUE TO THE NORTH, REMINGTON AVENUE TO THE SOUTH, CARPENTER AVENUE TO THE WEST, AND THE CUCAMONGA CREEK FLOOD CONTROL CHANNEL TO THE EAST, AND MAKING FINDINGS IN SUPPORT THEREOF – APNS: 0218-261-24, 0218-292-05, 0218-292-09, 0218-292-10, 0218-292-12, 0218-292-13, 0218-292-14, and 0218-311-11.

WHEREAS, the Project Environmental Impact Report (EIR) for the Colony Commerce Center Specific Plan (File No. PSP15-001) (SCH# 2015061023) has been prepared in accord with the California Environmental Quality Act (CEQA), the State CEQA Guidelines and the City of Ontario Guidelines for implementation of CEQA; and

WHEREAS, the EIR for File No. PSP15-001 consists of the Draft EIR and the comments and responses to comments made on the Draft EIR; and

WHEREAS, the EIR for File No. PSP15-001 was circulated for a 45-day public review period and a notice of its availability was published in a local newspaper and posted in the Office of the Clerk of the Board of Supervisors of San Bernardino County; and

WHEREAS, copies of the EIR were distributed to the Planning Commission, City departments, and federal, state, regional, local, and other agencies and individuals; and

WHEREAS, the EIR for File No. PSP15-001 has been prepared to address the environmental effects of a Specific Plan (Colony Commerce Center) to establish land use designations, development standards, and design guidelines for approximately 123 acres of land within the Ontario Ranch, generally located north of Remington Avenue, south of Merrill Avenue, east of Carpenter Avenue, and west of the Cucamonga Creek flood control channel; and

WHEREAS, on July 25, 2017, the Planning Commission of the City of Ontario conducted a duly noticed public hearing on the EIR at which time all persons wishing to testify were heard and the EIR was fully studied; and

WHEREAS, all other legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: ***Environmental Determination and Findings.*** As the recommending body for the Project, the Planning Commission has reviewed and considered the information contained in the Environmental Impact Report (EIR) and supporting documentation. Based upon the facts and information contained in the EIR (SCH# 2015061023) and supporting documentation, the Planning Commission finds as follows:

(1) The EIR contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(2) The EIR was completed in compliance with CEQA and the Guidelines promulgated thereunder; and

(3) The EIR reflects the independent judgment of the Planning Commission; and

SECTION 2: ***Planning Commission Action.*** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing and upon the specific findings set forth in Section 1 above, the Planning Commission hereby concludes as follows:

(1) The Project EIR analyzed the environmental impacts-associated with the implementation of the Colony Commerce Center Specific Plan, and finds that, if the Specific Plan is adopted and development occurs as proposed by this plan, and with implementation of proposed mitigation measures, the following impacts will still be significant and unavoidable:

(a) Air Quality **-** Impacts related to a net increase in criteria pollutants would remain significant and unavoidable with the implementation of recommended mitigation measures; and

(b) Agricultural Resources **-** Project-specific impacts and cumulative impacts would remain significant and unavoidable.

(c) Transportation and Traffic – Impacts related to intersections are projected to be cumulatively significant and unavoidable.

SECTION 3: ***Recommendation.*** Based upon the findings and conclusions set forth in Sections 1 and 2 above, the Planning Commission hereby recommends that the City Council CERTIFY the Project EIR, ADOPT a Statement of Overriding

Considerations, and that the associated Mitigation Monitoring Program also be approved by the City Council.

SECTION 4: ***Indemnification.*** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 5: ***Custodian of Records.*** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 6: ***Certification to Adoption.*** The Secretary shall certify to the adoption of the Resolution.

- - - - - - - - - - - - - -

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of July, 2017, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Richard D. Delman
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Principal Planner/Acting Secretary of the
Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Marci Callejo, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. PC17-[insert #] was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 25, 2017, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Marci Callejo
Secretary Pro Tempore

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL APPROVE THE COLONY COMMERCE CENTER WEST SPECIFIC PLAN (FILE NO. PSP15-001), TO ESTABLISH LAND USE DESIGNATIONS, DEVELOPMENT STANDARDS, DESIGN GUIDELINES AND INFRASTRUCTURE IMPROVEMENTS FOR 123.17 ACRES OF LAND, WHICH INCLUDES THE POTENTIAL DEVELOPMENT OF 2,951,146 SQUARE FEET OF INDUSTRIAL DEVELOPMENT. THE PROJECT SITE IS BOUNDED BY MERRILL AVENUE TO THE NORTH, REMINGTON AVENUE TO THE SOUTH, CARPENTER AVENUE TO THE WEST AND THE CUCAMONGA CREEK FLOOD CONTROL CHANNEL TO THE EAST, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 0218-292-05, 0218-292-09, 0218-292-10, 0218-311-11, 0218-292-12, 0218-292-13, 0218-292-14, 0218-261-24,

WHEREAS, CAP ROCK-PARTNERS ("Applicant") has filed an Application for the approval of a Specific Plan, File No. PSP15-001, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 123.17 acres of land, bounded by Merrill Avenue to the north, Remington Avenue to the south, Carpenter Avenue to the west and the Cucamonga Creek Flood Control Channel to the east, within the SP (AG) land use designation, and is presently improved with agriculture and farm related uses; and

WHEREAS, the property to the north of the Project site is within the SP (AG) zoning district and is developed with agriculture and dairy land uses. The property to the east is within the SP (AG) zoning district and is developed with the agriculture and vacant land. The property to the south is within the agriculture and light industrial zoning district located within the City of Chino and is developed with agriculture and industrial land uses. The property to the west is within the industrial zoning district located within the City of Chino and is developed with industrial land uses and contains vacant land; and

WHEREAS, the Colony Commerce Center West Specific Plan establishes a comprehensive set of design guidelines and development regulations to guide and regulate site planning, landscape, and architectural character, and ensuring that excellence in community design is achieved during project development. In addition, the Specific Plan will establish the procedures and requirements to approve new development within the project site to ensure TOP goals and policies are achieved; and

WHEREAS, the Colony Commerce Center West Specific Plan consists of 123.17 acres of land, which includes the potential development of up to 2,951,146 square feet of industrial development; and

WHEREAS, the land use intensity of the Colony Commerce Center West Specific Plan anticipated in the two planning areas is consistent with The Ontario Plan (TOP). The maximum Floor Area Ratio (FAR) permitted in each Planning Area conforms to the maximum 0.55 FAR permitted in the Policy Plan (General Plan) Land Use Plan for industrial. Planning Area 1, located along the northern portion of the Specific Plan area, is 57.58 acres in size and can potentially be developed with 1,379,501 square feet of industrial development. Planning Area 2, located along the southern portion of the Specific Plan is 65.60 acres in size and can potentially be developed with 1,571,645 square feet of industrial development; and

WHEREAS, the Colony Commerce West Specific Plan has been prepared in conformance with the goals and policies of the City of Ontario Policy Plan (General Plan). The policy (General Plan) analysis in the *Appendix "Policy Plan (General Plan) Consistency,"* of the Specific Plan describes the manner in which the Colony Commerce West Specific Plan complies with the Policy Plan goals and policies applicable to the Colony Commerce West Specific Plan; and

WHEREAS, the Specific Plan does not conflict with the Land Use Policies of the General Plan (TOP) and will provide for development, in a manner consistent with the General Plan. The policy (General Plan) analysis in the *Appendix "Policy Plan (General Plan) Consistency,"* of the Specific Plan describes the manner in which the Colony Commerce West Specific Plan complies with the Policy Plan goals and policies applicable to the Colony Commerce West Specific Plan; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA"); and

WHEREAS, an Environmental Impact Report (EIR) (SCH#2015061023) has been prepared in accord with the California Environmental Quality (CEQA), the State CEQA Guidelines and the City of Ontario Guidelines to address the environmental effects of the Specific Plan (Colony Commerce Center West); and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make a recommendation on the subject Application; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, the project site is also located within the Airport Influence of Chino Airport and must be consistent with policies and criteria set forth within the 2011 California Airport Land Use Planning Handbook published by the California Department of Transportation, Division of Aeronautics, which addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on July 25, 2017, the Planning Commission of the City of Ontario conducted a duly noticed public hearing to consider the Project and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: ***Environmental Determination and Findings.*** As the recommending body for the Project, the Planning Commission has reviewed and considered the information contained in the Environmental Impact Report (EIR) prepared for the project and supporting documentation. Based upon the facts and information contained in the EIR (SCH# 2015061023) and supporting documentation, the Planning Commission finds as follows:

(1) The Colony Commerce West Specific Plan EIR contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(2) The Colony Commerce West Specific Plan EIR was completed in compliance with CEQA and the Guidelines promulgated thereunder; and

(3) The Colony Commerce West Specific Plan EIR reflects the independent judgment of the Planning Commission; and

SECTION 2: ***Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance.*** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility

Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport ("ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). The project site is also located within the Airport Influence of Chino Airport and is consistent with policies and criteria set forth within the 2011 California Airport Land Use Planning Handbook published by the California Department of Transportation, Division of Aeronautics. As a result, the Planning Commission, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 3: ***Concluding Facts and Reasons.*** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 and 2, above, the Planning Commission hereby concludes as follows:

(1) The 123.17 acre Colony Commerce Center West Specific Plan is suitable for industrial development, uses permitted in the proposed district in terms of access, size, and compatibility with existing land use in the surrounding area; and

(2) The proposed Colony Commerce Center West Specific Plan is in conformance with the Land Use Policies and Goals of the Policy Plan and will provide for development, within the district, in a manner consistent with the Policy Plan and with related development; and

(3) During the Colony Commerce Center West Specific Plan review, opportunities for the involvement of citizens, California Native American Indian tribes (Government Code Section 65352.3.), public agencies, public utility companies, and civic, education, and other community groups, through public hearings or other means were implemented consistent with California Government Code Section 65351; and

(4) The proposed project is consistent with the adopted Housing Element. The Project site is not one of the properties (areas) listed in the Available Land Inventory in the Housing Element.

SECTION 4: ***Planning Commission Action.*** Based upon the findings and conclusions set forth in Sections 1 through 3, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL APPROVE the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 5: ***Indemnification.*** The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 6: ***Custodian of Records.*** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 7: ***Certification to Adoption.*** The Secretary shall certify to the adoption of the Resolution.

- - - - - - - - - - - - - -

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of July, 2017, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Richard D. Delman
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Principal Planner/Acting Secretary of the
Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Marci Callejo, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. PC17-[insert #] was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 25, 2017, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Marci Callejo
Secretary Pro Tempore

ATTACHMENT A:

File No. PSP15-001
Colony Commerce Center West Specific Plan

(Specific Plan to follow this page)

Colony Commerce Center West Specific Plan

Draft | March 2017

COLONY COMMERCE CENTER WEST SPECIFIC PLAN

City of Ontario
303 East 'B' Street
Ontario, California 91764

CapRock Partners
2050 Main Street, Suite #240
Irvine, California 92614

March 2017

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COLONY COMMERCE CENTER WEST SPECIFIC PLAN

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1 EXECUTIVE SUMMARY

1.1 Summary

The Colony Commerce Center West Specific Plan includes approximately 123.17 gross acres located in the southern portion of the City, near to the San Bernardino/Riverside County boundary. The master plan for project will provide for development of industrial buildings offering a variety of uses.

The project site is generally located north of Remington Avenue, south of Merrill Avenue, east of Carpenter Avenue and west of the Cucamonga Creek flood control channel in the City of Ontario, San Bernardino County, California.

The site is also located within Ontario Ranch area which comprises a portion of the former San Bernardino County Agricultural Preserve annexed by the City in 1999. The recently incorporated City of Eastvale (October 2010) is located southeast of Ontario in the County of Riverside, while the City of Chino is located to the west in San Bernardino County.

The relationship of the project site to the surrounding region is depicted in Exhibit 1.1, Regional Context Map. Exhibit 1.2, Vicinity Map, shows the relationship of the site to adjacent land uses. Exhibit 1.3 depicts the development plan for the site.

The City of Ontario Sphere of Influence area, commonly referred to as the "Ag Preserve" was the last significant underdeveloped area in the San Bernardino Valley. In 1993, the San Bernardino Board of Supervisors voted to consider dissolving the Ag Preserve status, thus paving the way for the transition of agricultural uses to other locations and the ultimate development of the area within an urban setting.

In 1998, the City of Ontario prepared and adopted the Sphere of Influence General Plan Amendment, an amendment to the General Plan of the City of Ontario. Planning for the 8,069 acre Ontario Ranch area is the single most important development issue facing the City of Ontario today. The General Plan for the Ontario Ranch intends to provide the long term vision to create a high quality environment where residents can live, work, and play with a sense of individual neighborhoods rather than engulfed in the Ontario Ranch.

The Sphere of Influence annexation, dedicated as Ontario Ranch was annexed by the City of Ontario on November 30, 1999. The Colony Commerce Center West Specific Plan area is situated within the boundaries of the Ontario Ranch area.

On January 26th, 2010, the City of Ontario adopted The Ontario Plan (TOP) which serves as the City's new business plan and includes a long term Vision and a principle based Policy Plan (General Plan). The city's Policy Plan, which acts as the City's General Plan, designates (Policy Plan Exhibit LU-1-Land Use Plan) the project site for development of industrial uses at a maximum 0.55 floor area ratio (FAR) as illustrated in Exhibit 2.2 Policy Plan (General Plan) Land Use Plan.

1.2 Governing Documents

Development of Colony Commerce Center West will be governed by the following:

» The City of Ontario General Plan (January 1998), as amended, which establishes policies governing land use, circulation, housing, conservation and open space, noise, safety, and public facilities within the Colony Commerce Center West Specific Plan area.

» The Colony Commerce Center West Specific Plan which includes a Land Use Plan, Infrastructure Plan, Design Guidelines, and Development Regulations. Where the Colony Commerce Center West Specific Plan is silent, the City of Ontario Development Code shall govern.

» The Aiport Land Use Compatibility Plan for Ontario International Airport Land Use Planning Handbook published by Caltrans Division of Aeronautics.

» A development agreement to include methods for financing, acquisition, and construction of infrastructure.

1.3 Specific Plan Components

The Colony Commerce Center West Specific Plan is organized into the following sections in addition to Section 1, Executive Summary.

1.3.1 (Section 2) Introduction

The Introduction serves to acquaint the reader with:

» Community vision and objectives,
» The project setting,
» A general description of the project proposal,
» The goals and policies of the Colony Commerce Center West Specific Plan,
» The entitlements to accompany the Colony Commerce Center West Specific Plan; and
» The relationship of the Colony Commerce Center West Specific Plan to the City of Ontario General Plan, and the City of Ontario Development Code.

1.3.2 (Section 3) Existing Conditions

The physical setting for Colony Commerce Center West is described in this section outlining the existing physical conditions on and around the Specific Plan area.

1.3.3 (Section 4) Land Use Plan

The Land Use Section describes industrial planning areas and allocations of industrial building sizes per planning area.

1.3.4 (Section 5) Infrastructure and Public Services

This section provides information on circulation improvements, planned backbone water, sewer, and storm drain systems, the grading concept for the development of the project, and a discussion of public utilities and services to serve the Specific Plan.

1.3.5 (Section 6) Development Regulations

Development Regulations established in this section will govern the permitted uses and the standards regulating the development of various industrial uses within the Colony Commerce Center West Specific Plan area. The relationship of the Colony Commerce Center West Specific Plan development regulations to the City of Ontario Development Code is also provided. The policies and procedures for the City's review and approval of specific development proposals within Colony Commerce Center West are presented in this section as well as the methods and procedures for interpreting and amending the Colony Commerce Center West Specific Plan as necessary.

1.3.6 (Section 7) Implementation and Administration

The policies and procedures for the City's review and approval of specific development proposals, within Colony Commerce Center West, are presented in this section. This section provides the methods and procedures for interpreting and amending the Colony Commerce Center West Specific Plan as necessary. A summary of project financing and project maintenance responsibilities for new development within the Specific Plan area is provided in this section.

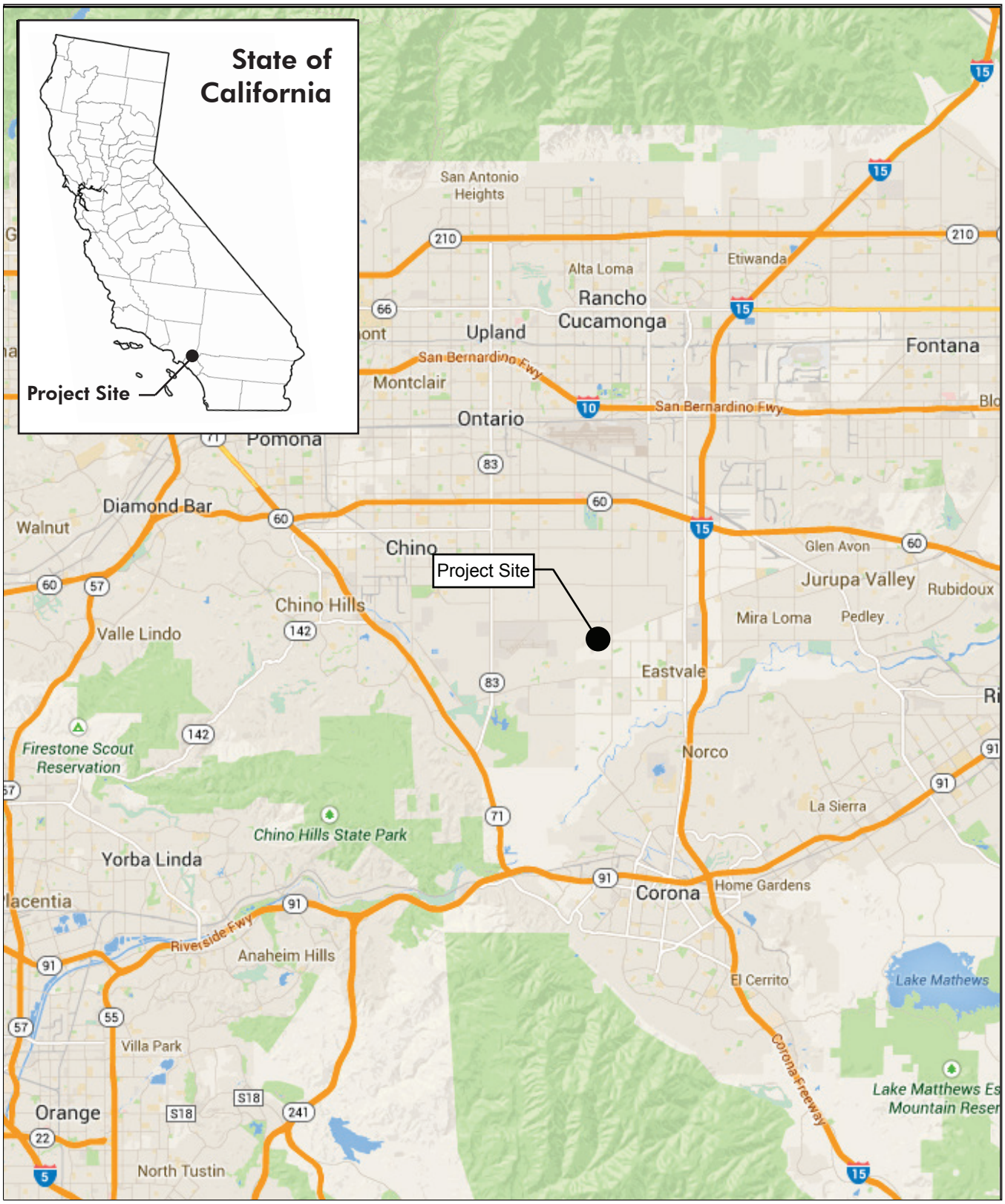
1.3.7 (Section 8) Design Guidelines

The Colony Commerce Center West Design Guidelines are intended to direct the site planning, landscaping, and architectural quality of the development. Streetscapes, entries, edge treatments, walls and fencing, lighting, signage, and architectural design are some of the features to be addressed in the Design Guidelines.

1.3.8 (Section 9) General Plan Consistency

This section includes the City of Ontario General Plan consistency matrix describing the relationship of the Colony Commerce Center West Specific Plan to each policy of the NMC General Plan.

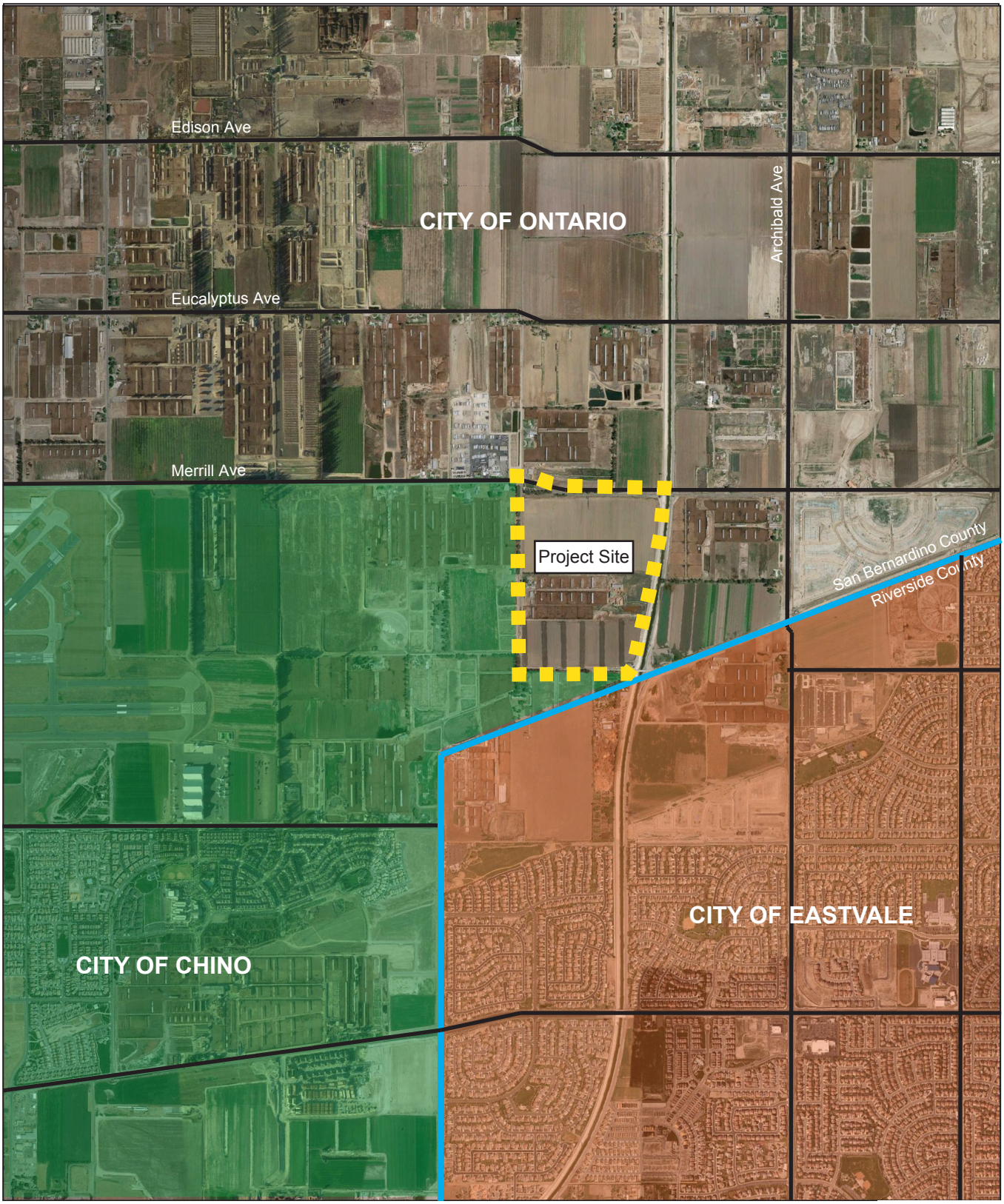
Exhibit 1.1, Regional Context Map



Source: Google Maps

N. T. S.

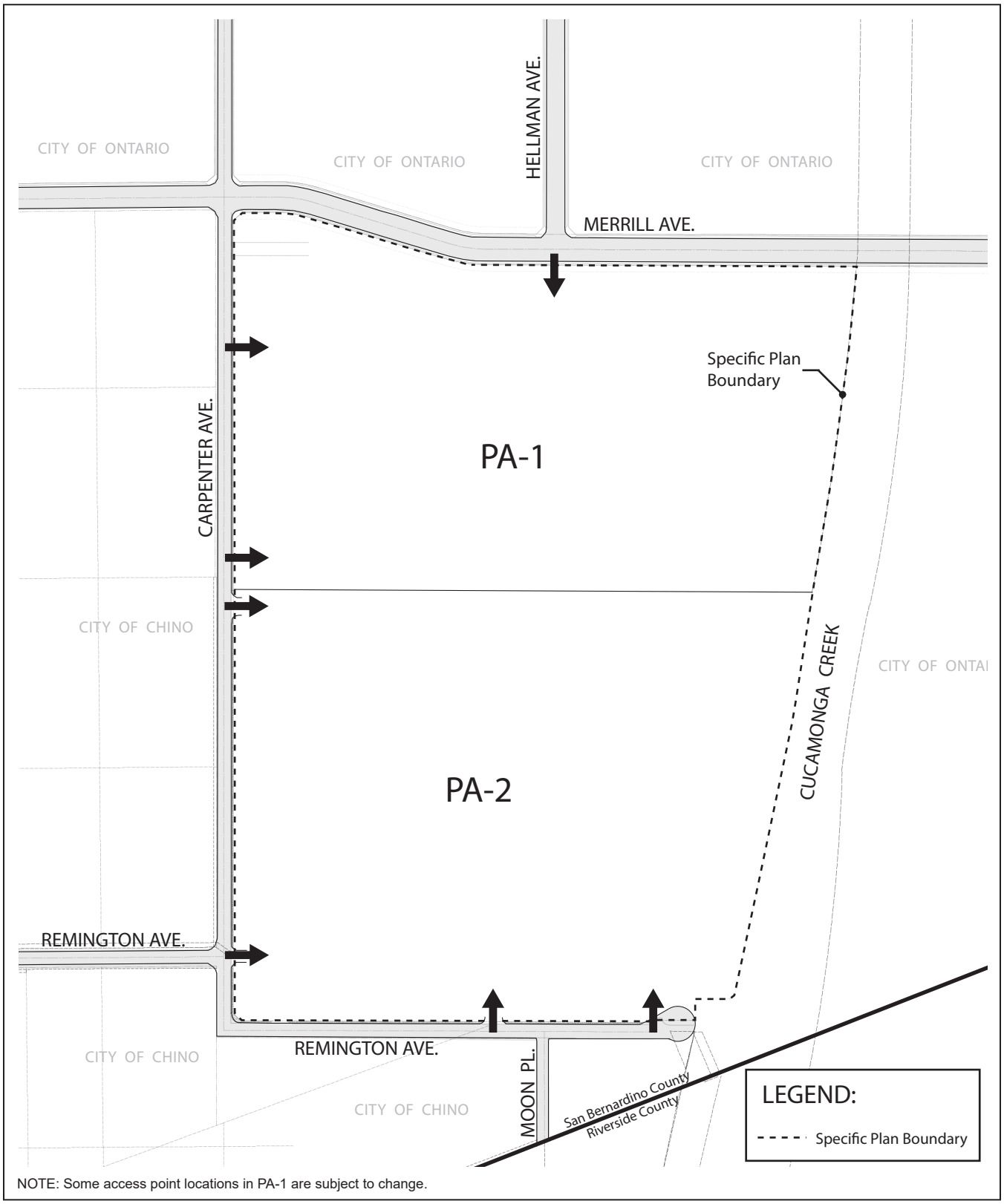
Exhibit 1.2, Vicinity Map



Source: Google Maps

N. T. S.

Exhibit 1.3, Specific Plan Area



Source: KTGY Group

N. T. S.

Table 1.1, Land Use Summary

Planning Area (PA)	Land Use	Acres	Maximum Potential Intensity (Gross Floor Area)	Max.Floor Area Ratio
PA-1	Industrial	57.58 ac	1,379,501 SF	0.55
PA-2	Industrial	65.60 ac	1,571,645 SF	0.55
	Total	**123.17 ac**	**2,951,146 SF**	**0.55**

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2 INTRODUCTION

2.1 Specific Plan Purpose

The Ontario Plan (TOP) Policy Plan includes requirements for subsequent approval by the City of a Specific Plan and an Area Plan for development within the area of the City known as the Ontario Ranch.

Specific Plans are required to ensure that sufficient land area is included to achieve unified districts and neighborhoods. Specific Plans shall incorporate a development framework for detailed land use, circulation, infrastructure including drainage, sewer, and water facilities, provision for public services including parks and schools, and urban design and landscape plans. The Area Plan shall provide additional policy-level guidance and is considered part of the Policy Plan.

The Area Plan for the Ontario Ranch will be initiated by the City of Ontario at a future time. However, until the Area Plan for the Ontario Ranch is adopted, the Policy Plan provides that new specific plans may proceed consistent with the Goals and Policies of the Policy Plan.

The Colony Commerce Center West Specific Plan serves to implement the City's Policy Plan for the project site and provides zoning regulations for development of the project site by establishing permitted land use, development standards, infrastructure-requirements, and implementation requirements for development.

A comprehensive set of design guidelines and development regulations are included to guide and regulate site planning, landscape, and architectural character within the community ensuring that excellence in community design is achieved during project development. The Colony Commerce Center West Specific Plan establishes the procedures and requirements to approve new development within the project site.

2.2 Authority

California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457 et seq grants local planning agencies the authority to prepare Specific Plans for any area covered by a General Plan for the purpose of establishing systematic methods of implementation of the General Plan.

A Specific Plan is designed to address site specific issues such as existing on-site conditions relative to topography and existing environmental concerns, site design and layout, including setbacks and visual appearance, as well as circulation, utility provisions and infrastructure financing alternatives.

The California Government Code establishes the authority and procedures to adopt a specific plan; identifies the required contents of a specific plan; mandates consistency with the General Plan; and also mandates consistency of any future projects or zoning ordinance amendments with a specific plan. Section 9-1.200 of Title 9 of the City of Ontario's Municipal Code states the purpose and intent of specific plans.

The City's Municipal Code will act as a supplement for those areas and issues not covered by this Specific Plan regulations for administration review procedures, environmental review, and others.

2.3 State Requirements

Section 65451 of the Government Code mandates what a Specific Plan shall contain. A Specific Plan shall include a text and diagram or diagrams which specify all the following in detail:

- The distribution, location, and text of the uses of land, including open space, within the area covered by the plan.
- The proposed distribution, location and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other facilities proposed to be located within the area covered by the plan and needed to support the land uses describe in the plan.
- Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.
- A program of implementation measures including regulations, programs, and financing measures necessary to carry out the Colony Commerce Center West West project.
- The Specific Plan shall include a statement of its relationship to the General Plan.

2.4 Severability

If any section, subsection, sentence, clause, phrase, or portion of the Specific Plan, or any future amendment(s) or addition(s) hereto, is for any reason held to be invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of the Specific Plan, or any future amendments or additions hereto. The City hereby declares that it would have adopted these requirements and each sentence, subsection, clause, phrase, or portion or any future amendments or additions thereto, irrespective of the fact that any one or more sections, subsections, clauses, phrases, portions or any future amendments or additions thereto may be declared invalid or unconstitutional.

2.5 Project Objectives

The Colony Commerce Center West Specific Plan is designed to implement a series of project-specific objectives that have been carefully crafted to ensure the project develops with a quality industrial development. The project objectives have been refined throughout the planning and design process for the project. They are identified below:

- To provide for the development of industrial facilities which utilize the site's prime location in proximity to Ontario Airport.
- To create a high quality industrial development that attracts an array of industrial businesses and provides employment opportunities to area residents.
- To provide industrial uses within the project boundaries which are compatible with surrounding uses.
- To develop a flexible plan that meets the needs of an ever-changing business market, while assuring compliance with high development standards.
- To provide a plan for roadways, infrastructure, and utilities to support on-site land uses as the project evolves.
- Promote opportunities for water efficiency in the project architecture and project landscaping to promote water conservation.

2.6 Specific Plan Summary

The Colony Commerce Center West Specific Plan creates a master-plan comprised of industrial development. The project consists of two planning areas:

- PA-1, Approximately 57.58 gross acres of industrial development on the north portion of the site allowing for a total development up to 1,379,501 SF at a Floor Area Ratio (FAR) of .55.

- PA-2, Approximately 65.60 gross acres of industrial development on the south portion of the site allowing for a total development up to 1,571,645 SF at a .55 FAR.

Assessor's parcel numbers within the Colony Commerce Center West Specific Plan are:

- 0218-261-24
- 0218-292-05
- 0218-311-11
- 0218-292-09
- 0218-292-13
- 0218-292-10
- 0218-292-14
- 0218-292-12

Exhibit 2.1 shows the assessor's parcel numbers within the Specific Plan area.

2.7 Discretionary Actions and Approvals

2.7.1 The Ontario Plan

The Ontario Plan (TOP) establishes the direction and vision for the City of Ontario providing a single guidance system that will shape the Ontario community for the future. The Plan provides for policies to accommodate change over a 30 year period commencing in 2010, the beginning of the planning period. The Ontario Plan consists of a six part Component Framework: 1) Vision, 2) Governance Manual, 3) Policy Plan, 4) City Council Priorities, 5) Implementation, and 6) Tracking and Feedback.

2.7.2 Specific Plan

The Policy Plan requires the approval of a Specific Plan for development of the project site to ensure that sufficient land area is included to achieve unified districts and neighborhoods. The City of Ontario has zoned the project site as AG-Specific Plan as illustrated in Exhibit 2.3 Ontario Zoning Map.

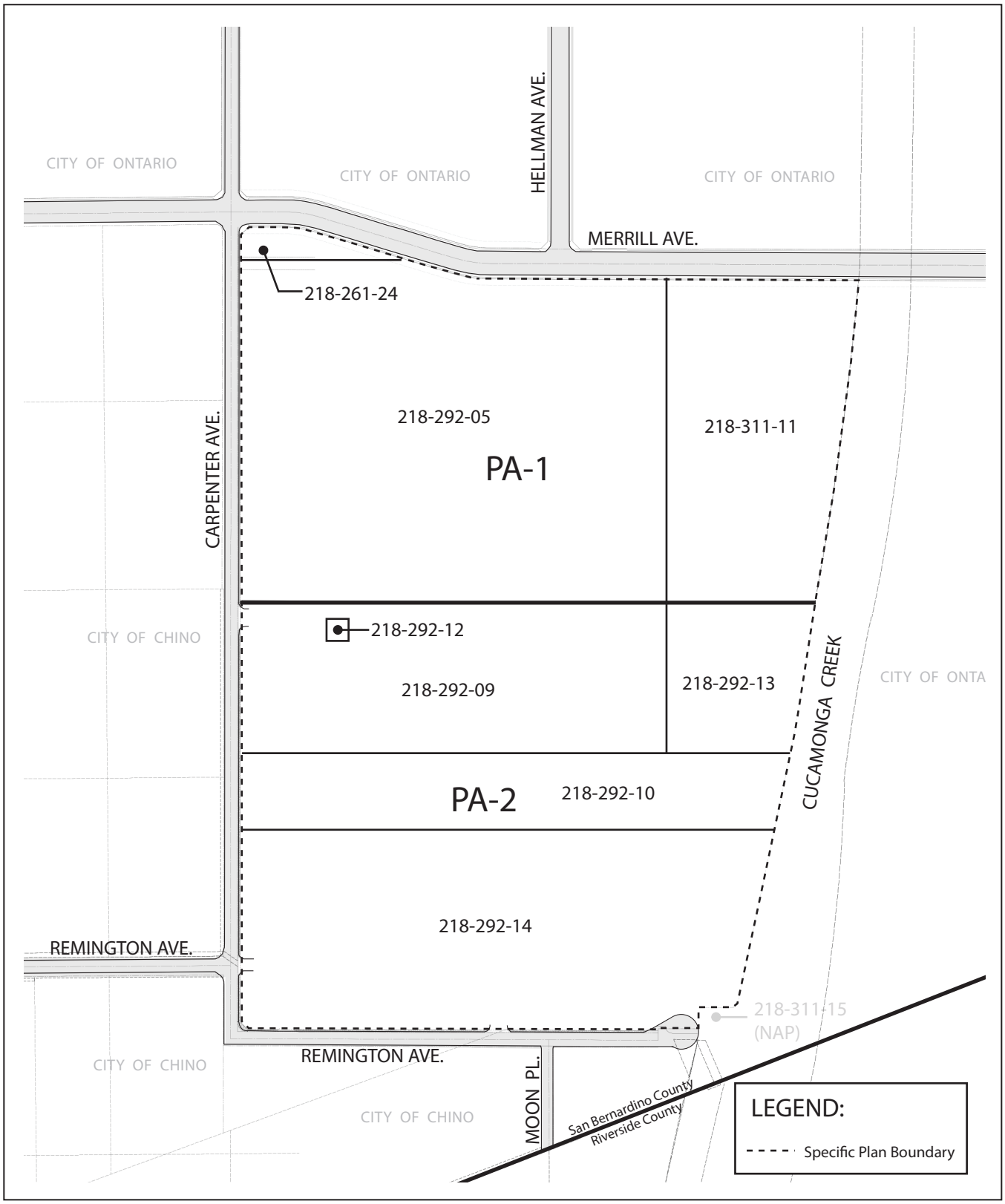
The zoning designation of AG-Specific Plan requires that a Specific Plan be approved to guide development of the project site and to implement the goals and policies of the Policy Plan. Pursuant to adoption by the City of Ontario of the Colony Commerce Center West Specific Plan by the ordinance, the Specific Plan shall take precedence over the City of Ontario Development Code. In instances where the Specific Plan is silent, the City of Ontario Development Code shall prevail.

2.7.3 Development Agreement

Unless developed in a coordinated manner and with adequate fiscal planning, development projects within the City are likely to present a challenge in their implementation because of the lack of existing public facilities including streets, sewerage, transportation, drinking water, schools, and utility facilities. California law establishes a mechanism for ensuring the adequate provision of such facilities while providing assurances to applicants that, upon project approval, applicants can proceed with their projects.

Approval of the Colony Commerce Center West Specific Plan is accompanied by an application for approval of a development agreement to encourage investment in and commitment to comprehensive planning as envisioned by the City, which seeks to take maximum efficient utilization of resources at the least economic cost to the public. A statutory development agreement, authorized pursuant to California Government Code Sections 65864 et seq., shall be required as part of the approval of the Colony Commerce Center West Specific Plan.

Exhibit 2.1, Assessor's Parcels



N. T. S.

The development agreement shall include, but not be limited to, methods for financing acquisition and construction of infrastructure, acquisition and development of adequate levels of parkland and schools, as well as the provision of adequate housing opportunities for various segments of the community consistent with the City's regional housing needs assessments. The Colony Commerce Center West development agreement shall be fully approved before the issuance of the first building permits for the project.

2.7.4 Subdivison Maps

Tentative tract maps will be approved by the City of Ontario for the project indicating the approximate boundaries and dimensions of lots and streets and the proposed grading for the project site. Following approval by the City of tentative tract maps, final maps will be prepared for City approval. Following recordation, final maps become the legal documents defining parcels that can be developed.

2.7.5 Development Plan Review

All development proposals for individual Planning Areas within the Colony Commerce Center West Specific Plan shall be subject to Development Plan Review pursuant Division 4.02 Discretionary Permits and Action of the City's Development Code.

2.7.6 CEQA Compliance

A Project Level Environmental Impact Report (EIR) prepared by the City of Ontario for the Colony Commerce Center West Specific Plan in accordance with the California Environmental Quality Act (CEQA), analyzes impacts associated with the implementation of the Specific Plan and subdivision maps.

The EIR is prepared as a basis for the environmental review of all subsequent discretionary and ministerial actions within the Colony Commerce Center West Specific Plan.

2.8 Subsequent Actions and Approvals

Following adoption of the Colony Commerce Center West Specific Plan, subsequent actions and approvals will be required, which are identified below:

» Approval of Subsequent Tentative Maps: Implementing Tentative Maps will be prepared and processed through the City of Ontario in accordance with the requirements of Title 9, Article 4, Tentative Maps, of the Ontario Municipal Code and in accordance with the Subdivision Map Act.

» Approval of Grading and Improvement Plans: After approval of the Tentative Map, the City of Ontario will process the corresponding Grading and Improvement Plans (e.g., water plans, wastewater plans, drainage plans, grading plans, street improvement plans, final maps, etc.).

2.9 Airport Land Use Compatibility Planning Consistency

All development proposals of Specific Plan Amendments are required to be consistent with the Airport Land Uses Compatibility Plans of Chino Airport and Ontario International Airport.

2.10 General Plan and Zoning Designations

The Colony Commerce Center West Specific Plan area is designated as Industrial as shown on the City of Ontario General Plan Land Use Map (see Exhibit 2.2, Existing General Plan Land Use Designation). No changes in land use categories proposed.

The project site is currently zoned as Agriculture Specific Plan (see Exhibit 2.3, Existing Zoning Designation). Upon adoption of the Colony Commerce Center West Specific Plan, the zoning designation for the site will not need to change; it will remain as Specific Plan.

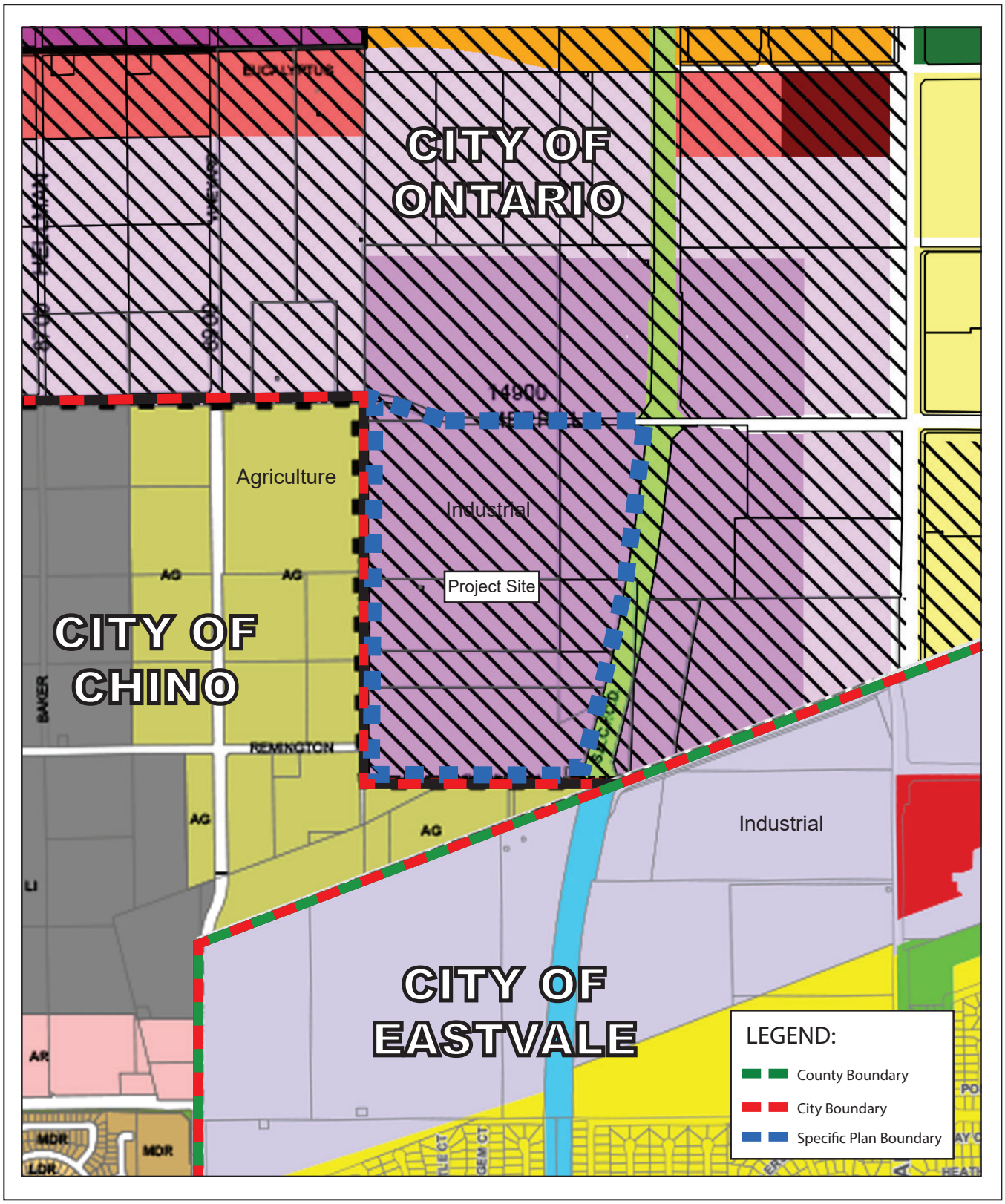
The City of Ontario Zoning Code states that specific plans are created to enable land to be planned and developed as coordinated, comprehensive projects providing for the systematic implementation of the Ontario General Plan. The Colony Commerce Center West Specific Plan will implement the Ontario General Plan as it relates to the Specific Plan area.

2.11 General Plan Consistency

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits the adoption and administration of specific plans as an implementation tool for elements contained in the local general plan. Policy plans must demonstrate consistency in regulations, guidelines, and programs with the goals and policies set forth in the general plan.

The Colony Commerce Center West Specific Plan has been prepared in conformance with the goals and policies of the City of Ontario Policy Plan. The policy analysis listed in Appendix A1 describes the manner in which the Colony Commerce Center West Specific Plan complies with the Policy Plan policies applicable to the project.

Exhibit 2.2, Existing General Plan Land Use Designation



Source: Exhibit LU-01 (Land Use Plan) 2010

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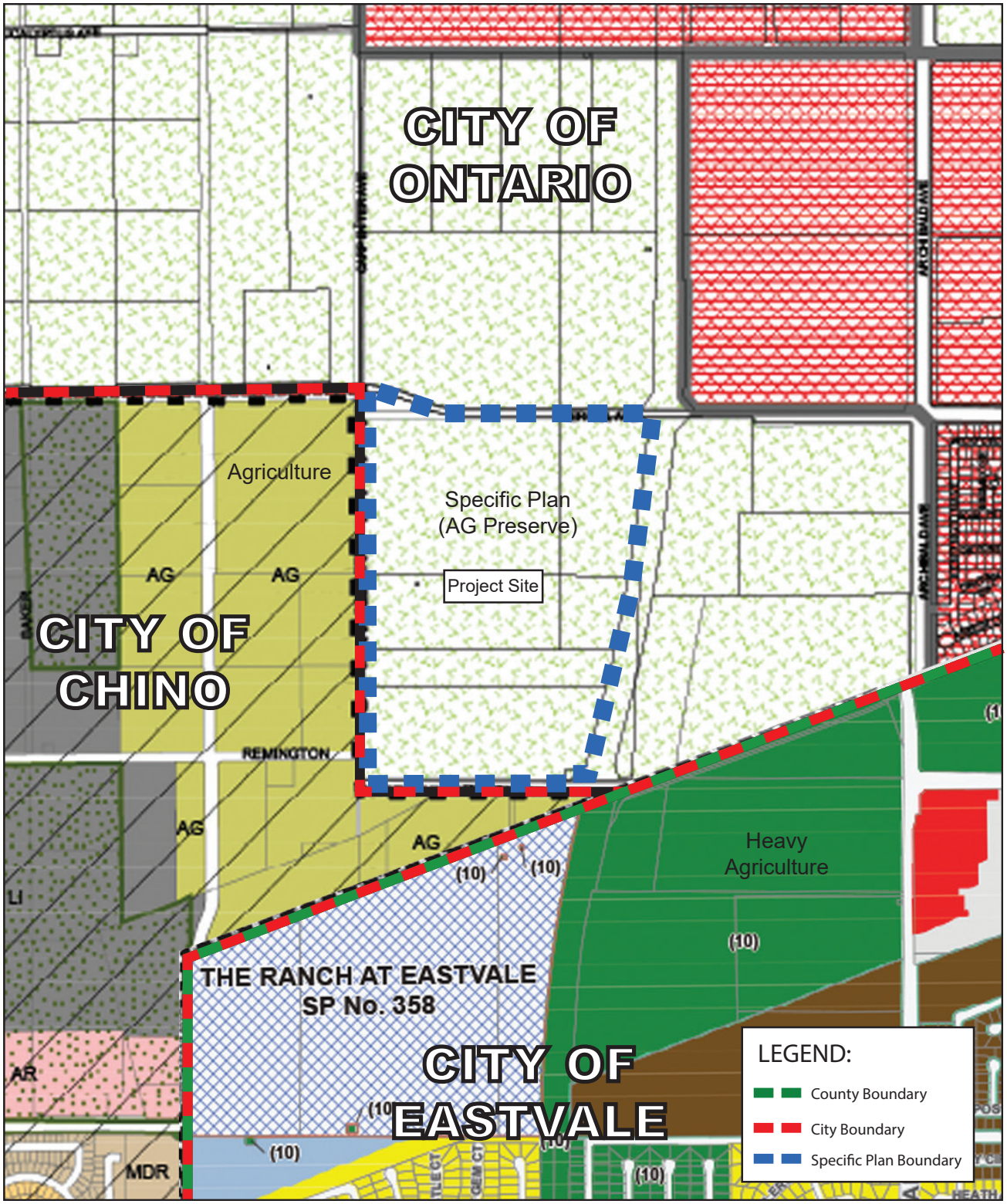
Exhibit 2.3, Existing Zoning Designation



Source: City of Ontario Zoning Map

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3 EXISTING CONDITIONS

3.1 Existing Land Use

The project site has historically been used for agricultural purposes, primarily for dairy and field crop farming. The project site is mostly undeveloped with existing agricultural operations scattered throughout the area. Rural residential housing, farm buildings, and other ancillary facilities occupy those areas not in active agricultural production. Exhibit 2.1, depicts the current aerial photgraphy of the specific plan area.

3.2 Surrounding Land Uses

Current agriculture uses such as dairy and field crop farms are located directly adjacent to the Colony Commerce Center West Specific Plan area. In the City of Ontario General Plan, these areas are designated for Industrial and Business Park uses.

Directly south of the project, in the City of Chino, agricultural uses exist that are incorporated within an airport overlay. The Chino Airport Overlay also covers the Colony Commerce Center West Specific Plan area. See Exhibit 2.2, Surrounding Land Uses, for the various land uses that surround the Colony Commerce Center West Specific Plan Area.

3.3 Policy Plan and Zoning

The City's General Plan designates the project site for the following land use:

» Industrial (0.55 FAR) - Approximately 123.17 gross acres of industrial uses on the site allowing for a total development up to 2.95 million square feet at a Floor Area Ratio (FAR) of 0.55.

The project site is zoned AG-Specific Plan. A specific plan is required by the City in order to comprehensively plan for development of industrial uses within the project site.

3.4 Airport Land Use Compatibility Plan (ALUCP) Consistency

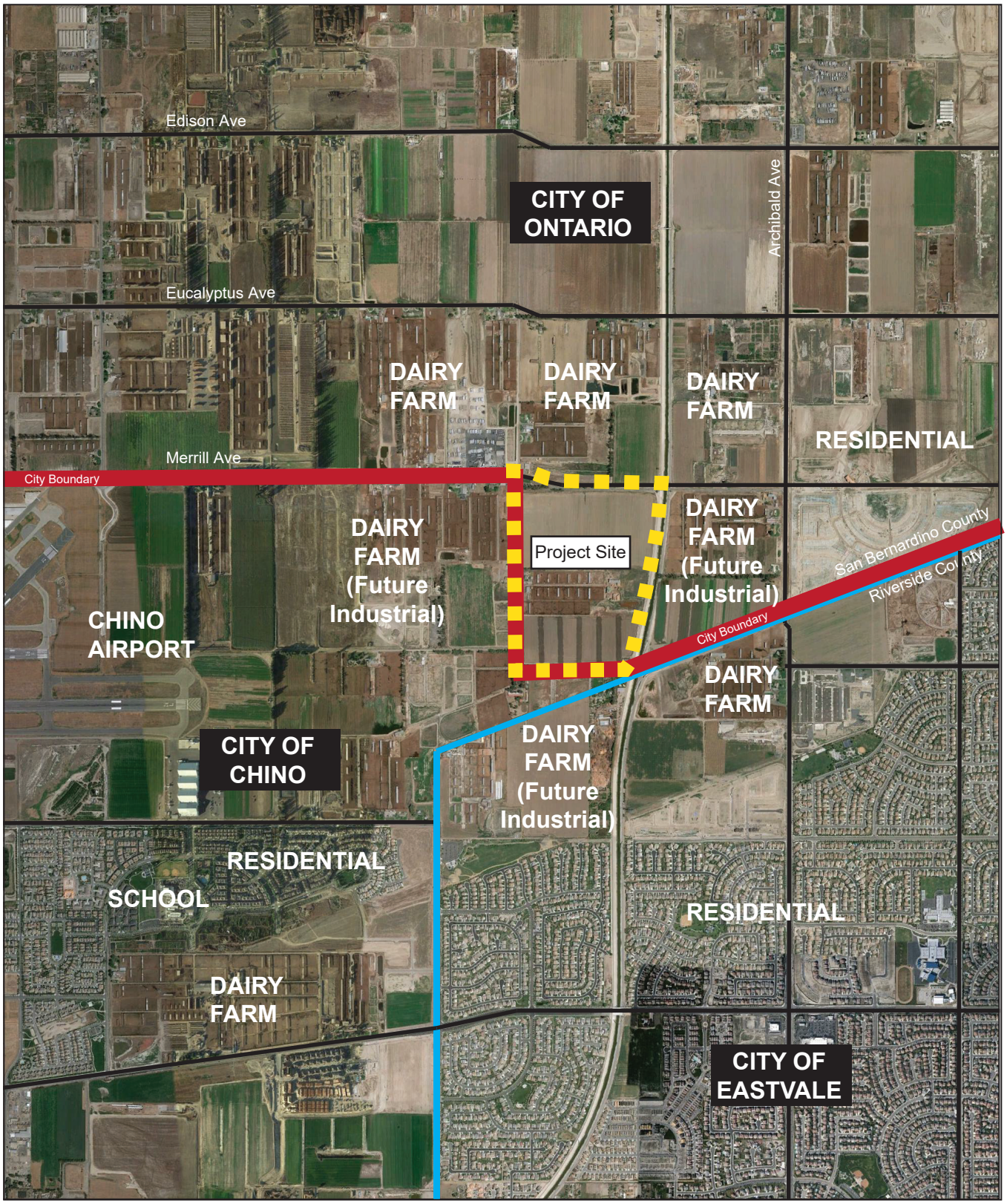
The Project Site is located within the Airport Influence Areas of Chino Airport and Ontario International Airport (ONT). The EIR prepared for the Colony Commerce Center West Specific Plan identifies potential impacts from Chino Airport and ONT and includes criteria for addressing any potential impacts.

Exhibit 3.1, Aerial Photograph

Source: Google Maps

N. T. S.

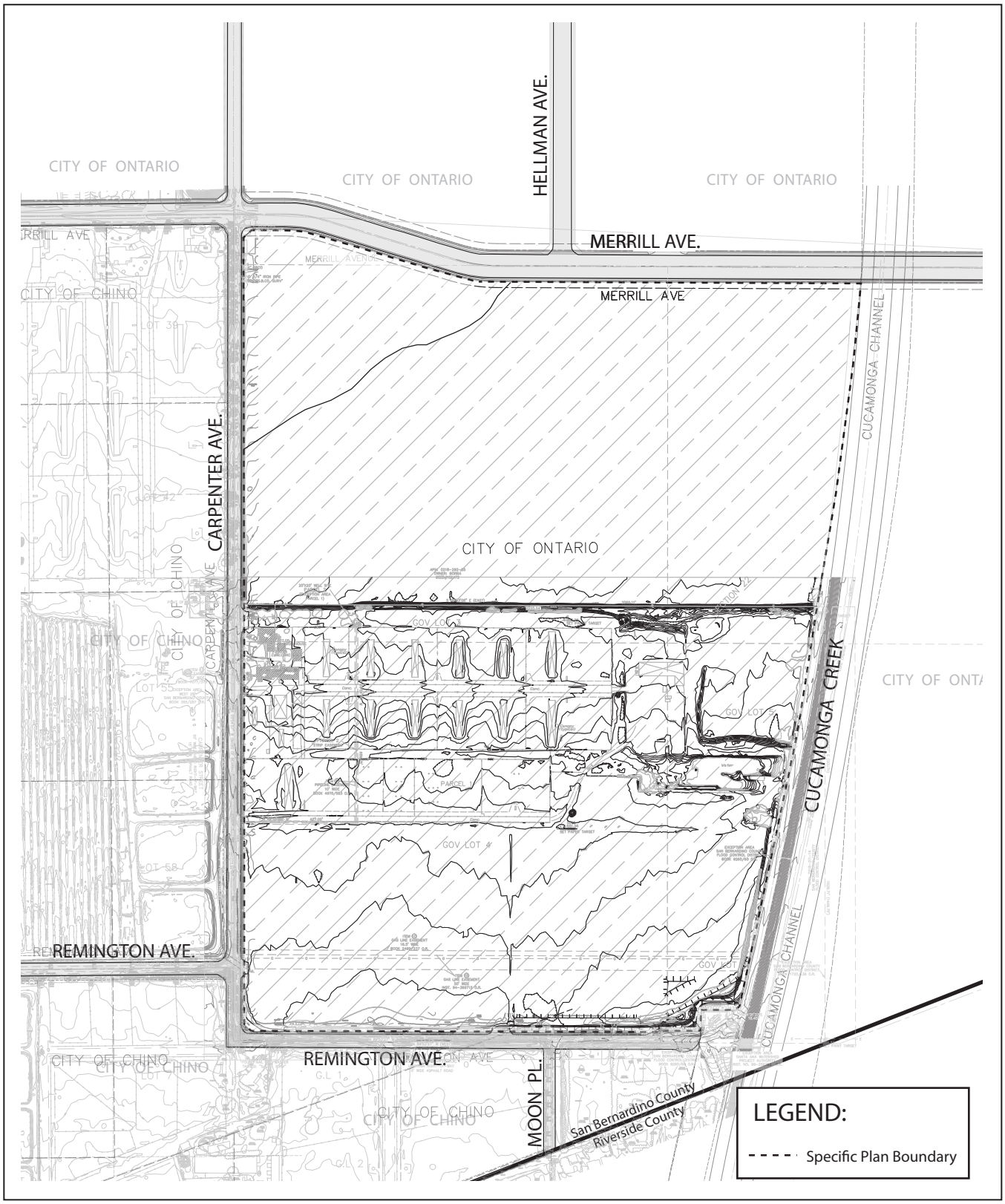
Exhibit 3.2, Surrounding Land Uses



Source: Google Maps

N. T. S.

Exhibit 3.3, Existing Site Topography



Source: David Evans & Associates

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3.5 Topography

The project site is relatively flat and gently falls to the south at an average gradient of approximately 1.0% to 2.0%. The existing topographic conditions for the Specific Plan area are illustrated on Exhibit 3.3, Existing Site Topography.

3.6 Hydrology

Since most of the project site has been in agricultural use, only a limited portion of the site is now covered with impervious surfaces. Normal rainfall to the area is able to percolate through on-site soils and does not result in high volumes of surface runoff, as is typically associated with urban use.

During periods of heavy rainfall, when ground surfaces are saturated, surface runoff is collected in the existing storm drains, culverts, and retention basins located within the project site.

The existing storm drain system throughout the project site is generally unimproved and consists primarily of open earthen swales along area roadways or curbed roadway surfaces. The EIR prepared for the Colony Commerce Center West Plan includes additional hydrology information for the project site.

3.7 Biology

The project site has been extensively used for agricultural operations including dairy and field crop uses. Those limited areas not in active agricultural production are occupied by rural residential housing or are vacant. The natural vegetation and the project site as a whole have been significantly altered through agricultural use, leaving little to no native vegetation. The EIR prepared for the Colony Commerce Center West Specific Plan includes an evaluation of vegetation and biological resources.

3.8 Existing Circulation and Access

3.8.1 Regional Circulation

Interstate 15 (I-15) is located approximately 3.25 miles east of the project site. Access from the project site to the I-15 exists at Limonite Avenue within Riverside County. State Route 60 (SR-60) is located approximately 3.25 miles north of the project site.

Access to the project site from SR-60 exists from Archibald Avenue, which connects to Merrill Avenue abutting the project site on the north. State Route 83 (SR-83/Euclid Avenue) is located approximately 2.5 miles west of the project site. Access from the project site to SR-83 exists from Merrill Avenue which abuts the project site on the north.

3.8.2 Local Circulation

Local access to the project site is provided from Carpenter Avenue, Merrill Avenue, and Remington Avenue. Merrill Avenue abuts the project site on the north and provides two paved travel lanes. The General Plan designates Merrill Avenue as a 4-lane Collector Street. Carpenter Avenue abuts the project site on the west with two travel lanes. Remington Avenue abuts the project site on the south with two travel lanes.

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4 LAND USE PLAN

4.1 Introduction

The overall land use concept for the Colony Commerce Center West Specific Plan takes ad-vantage of the site's proximity to airports and regional freeway access.

The land use concept provides for a range of industrial uses, while offering a variety of development and employment opportunities. The land use in this area also provides opportunities for a broad range of industries to accommodate an ever-changing business and industrial environment.

4.2 Land Use Plan

The circulation patterns, utility systems and overall design of the plan can meet these changes in demand. This is an important concept in a region that is experiencing rapid growth (see Exhibit 4.1, Land Use Plan).

The planned industrial area will include wholesale and distribution, light manufacturing and businesses with high-value, time-sensitive merchandise that would benefit from proximity to an airport.

The land use regulations for the Colony Commerce Center West Specific Plan will allow some flexibility in the location, mix and intensity of industrial uses so that as market demands change and as businesses expand or contract over time, the Specific Plan can respond and adapt to meet those needs. An illustrative site plan is shown on Exhibit 4.1, Land Use Plan.

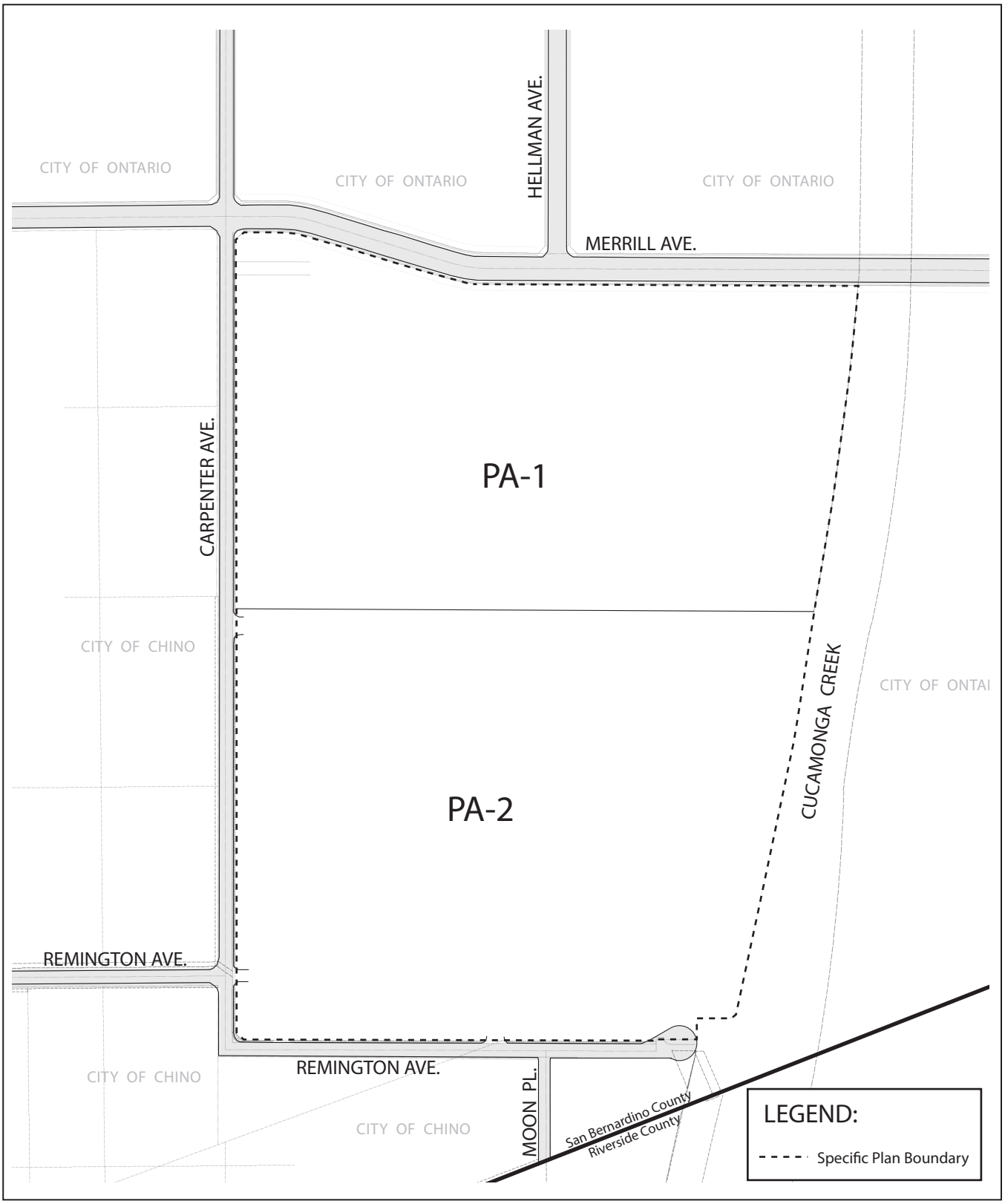
The land use intensity anticipated in the two planning areas is shown on Table 4.1, Land Use Summary. The maximum Floor Area Ratio (FAR) permitted in each Planning Area conforms to the maximum FAR permitted in the Ontario General Plan.

Table 4.1 identifies the anticipated build out of the Specific Plan area. Specific uses may be developed as identified as permitted in Table 6.3, Permitted Uses.

Table 4.1, Land Use Summary

Planning Area (PA)	Land Use	Acres	Maximum Potential Intensity (Gross Floor Area)	Max.Floor Area Ratio
PA-1	Industrial	57.58 ac	1,379,501 SF	0.55
PA-2	Industrial	65.60 ac	1,571,645 SF	0.55
	Total	**123.17 ac**	**2,951,146 SF**	**0.55**

Exhibit 4.1, Land Use Plan



Source: Douglas Franz Architects

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5 CIRCULATION, INFRASTRUCTURE AND PUBLIC SERVICES

The infrastructure, utilities, and public services to be provided as part of the development of the Colony Commerce Center West Specific Plan are discussed in this section.

5.1 Circulation

The circulation plan for Colony Commerce Center reinforces the objective of moving vehicles, pedestrians, cyclists, and public transit safely and efficiently through and around the project. Exhibit 5.1, Circulation Plan establishes the hierarchy and general location of roadways within Colony Commerce Center West.

The minimum design speeds to be used for center line curve radii, super elevation, corner and approach site distances, vertical and horizontal alignment, and sight distances for the Master Plan of Streets will comply with City Standards below:

- Merrill Avenue: 45 mph
- Carpenter Avenue: 40 mph
- Remington Avenue: 40 mph

5.1.1 Master Plan Roadways

The project site is bounded on the north by Merrill Avenue, a City of Ontario Collector Roadway as identified in Exhibit 5.2, Functional Roadway Classification Plan, providing access to and from the site.

Carpenter Avenue bounds the project site on the west; Remington Avenue bounds the project site on the south.

The Cucamonga Creek Channel, a non-vehicle open space area bounds the project site to the east.

A traffic study prepared as part of the project's EIR may identify the need for additional rights-of-way at critical intersections to accommodate lanes for left and right turn movements.

The developer shall be responsible for those improvements as determined by the City Engineer and pursuant to the mitigation measures identified in the EIR and/or Conditions of Approval established on the approved tentative maps for the project.

Phasing and construction of the improvements shall be implemented as required by the City Engineer and pursuant to the mitigation measures identified in the EIR and the conditions of approval adopted with the approval of tentative maps for the project. The locations and construction of bus turnouts may be required within the project to the satisfaction of the City of Ontario and Omnitrans.

5.1.2 Merrill Avenue

The Mobility Element of the Policy Plan (Figure M-2 Functional Roadway Classification Plan) designates Merrill Avenue as a 4-Lane Collector Street with a Class II bikeways and multipurpose trails. Merrill Avenue will provide east/west access to Colony Commerce Center West at the northern boundary of the project site. The proposed improvement to Merrill Avenue

are illustrated in Exhibit 5.3a, Merrill Avenue. Parking is prohibited along Merrill Avenue.

The southern half of the existing Merrill Avenue bridge crossing over Cucamonga Creek will be designed and constructed in accordance to the Ontario Master Plan of Streets and Highways.

5.1.3 Carpenter Avenue

Carpenter Avenue bounds the project site to the west and will provide north/south access to and from the Colony Commerce Center West Specific Plan area. Carpenter Avenue is designated as a 2-Lane Local Industrial Street. Exhibit 5.3b, Carpenter Avenue illustrates the ultimate improvements to Carpenter Avenue.

On-street parking is not allowed on Carpenter Avenue. Intersections and driveways shall be shown/designed in accordance to the Ontario Master Plan of Streets and Highways and coordinated with the City of Chino if necessary.

5.1.4 Remington Avenue

Remington Avenue bounds the project site to the south and will provide east/west access to and from the Colony Commerce Center West Specific Plan area. Remington Avenue is designated as a 2-Lane Local Industrial Street. Exhibit 5.3b, Remington Avenue illustrates the ultimate improvements to Remington Avenue by this project. No on-street parking shall be permitted on Remington Avenue.

Intersections and driveways shall be shown/designed in accordance to the Ontario Master Plan of Streets and Highways and coordinated with the City of Chino if necessary.

5.1.5 Pedestrian Circulation

In addition to vehicular circulation, a pedestrian circulation system utilizing the sidewalks will be provided within the Colony Commerce Center West Specific Plan.

Sidewalks will be provided along all streets abutting the Specific Plan area, and will be a minimum of five (5') feet in width. Sidewalks shall be constructed of concrete as part of the adjacent roadway improvements.

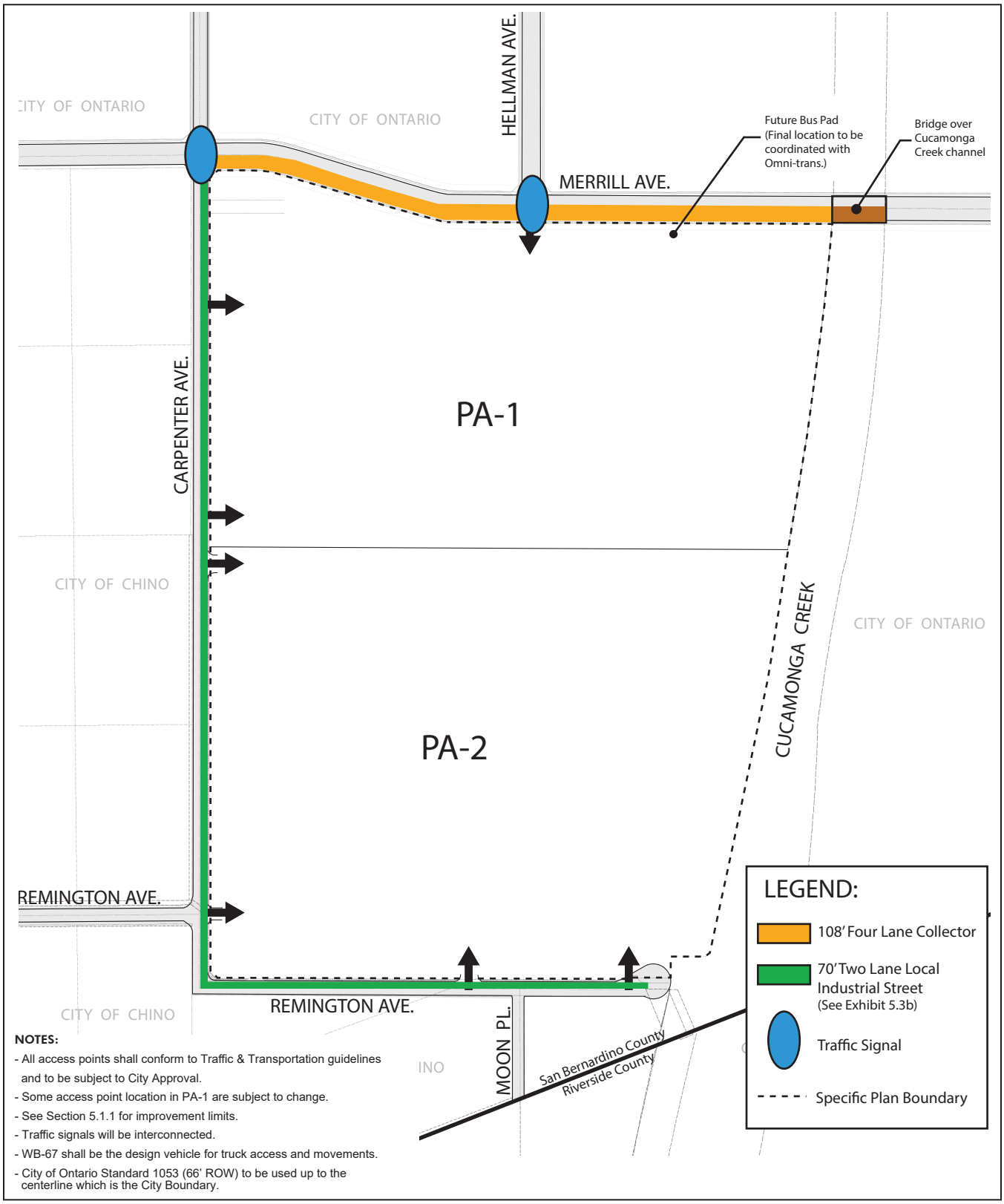
5.1.6 Bicycle Circulation

Bicycle trails are an integral element in creating accessibility and mobility within the Specific Plan. A Class I bikeway will be provided within the Cucamonga Creek Channel as illustrated in Exhibit 5.3c. The Specific Plan will construct trail connections to link the bikeway along Cucamonga Creek Channel with the on-street bicycle system.

The Mobility Element of the Policy Plan (Figure M-1 Mobility Element System) designates a Class II Bikeway & Multi-purpose Trail along Merrill Avenue. These bike paths will provide linkages to the City's master planned bike paths system. General timing and responsibility will be discussed in the Development Agreement.

Refer to Exhibit 5.4, Pedestrian and Bicycle Circulation Plan, for locations of these paths and trails. Refer to Exhibit 5.5, City of Ontario Trails & Bikeway Plan to see how the Specific Plan connects to the larger network.

Exhibit 5.1, Circulation Plan



Source: KTGY Group

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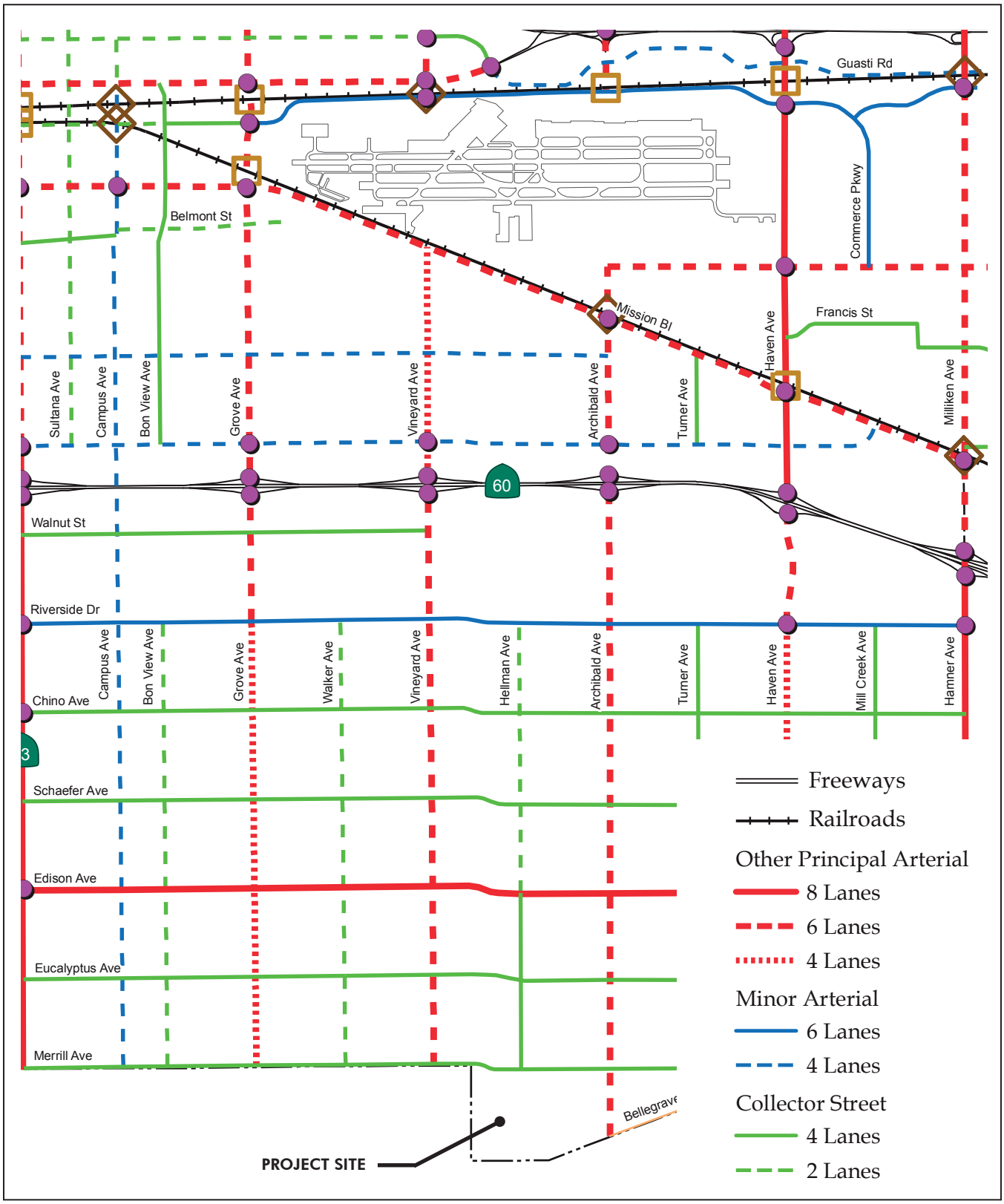
Exhibit 5.2, City of Ontario Roadway Classification Plan



Source: City of Ontario, Figure M-2 Functional Roadway Classification Plan, August 19, 2014
NOTE: Reference the City's most current Master Plan for the latest designations.

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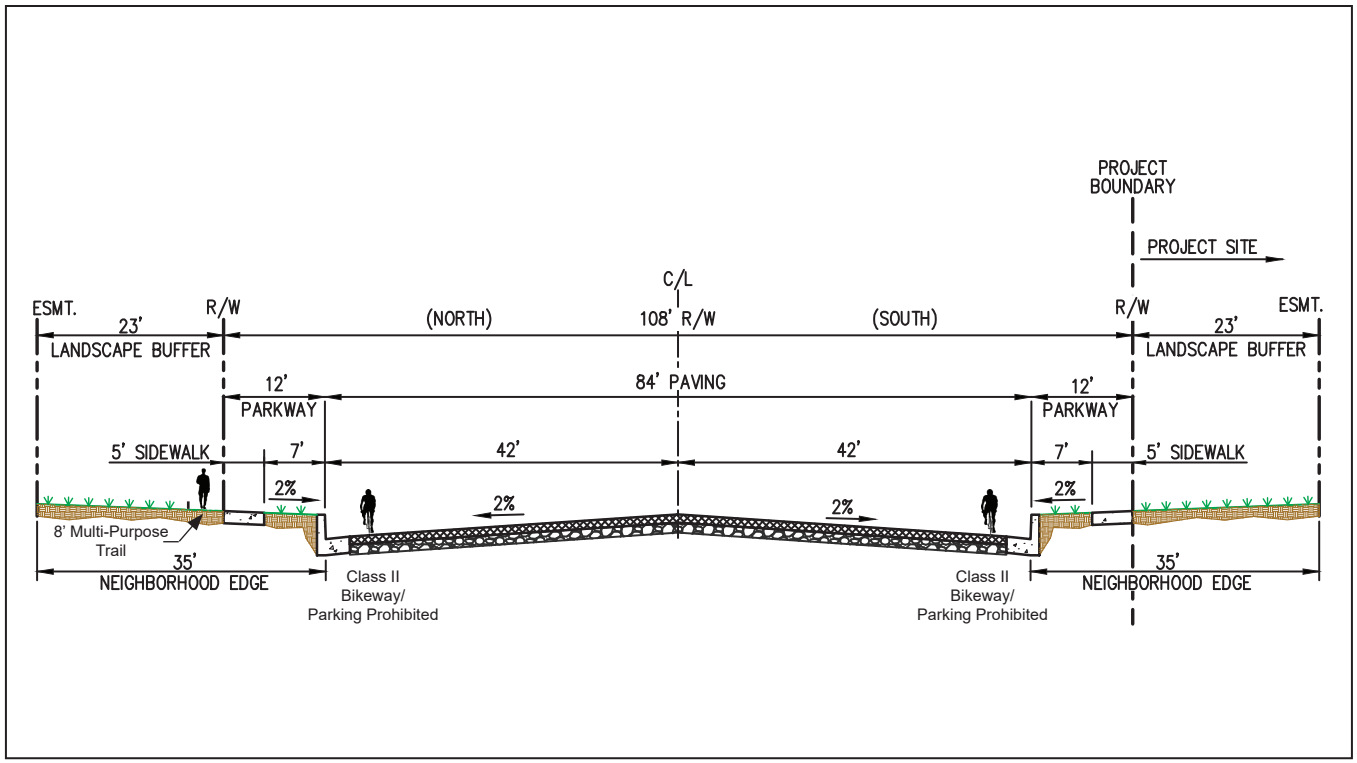
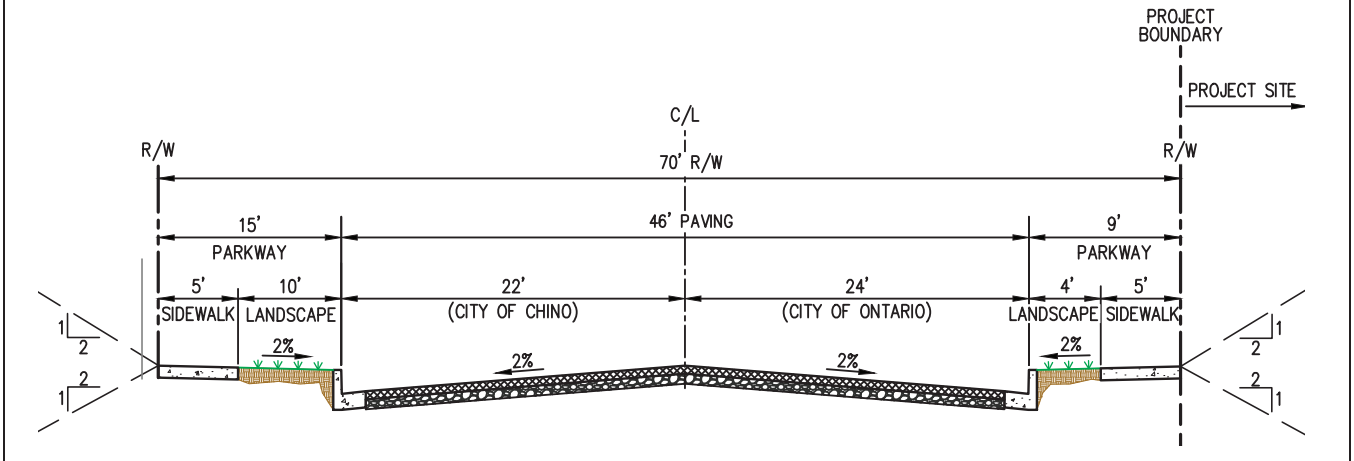
Exhibit 5.3a, Typical Street Cross Section - Merrill Avenue (108' ROW)



Source: David Evans & Associates

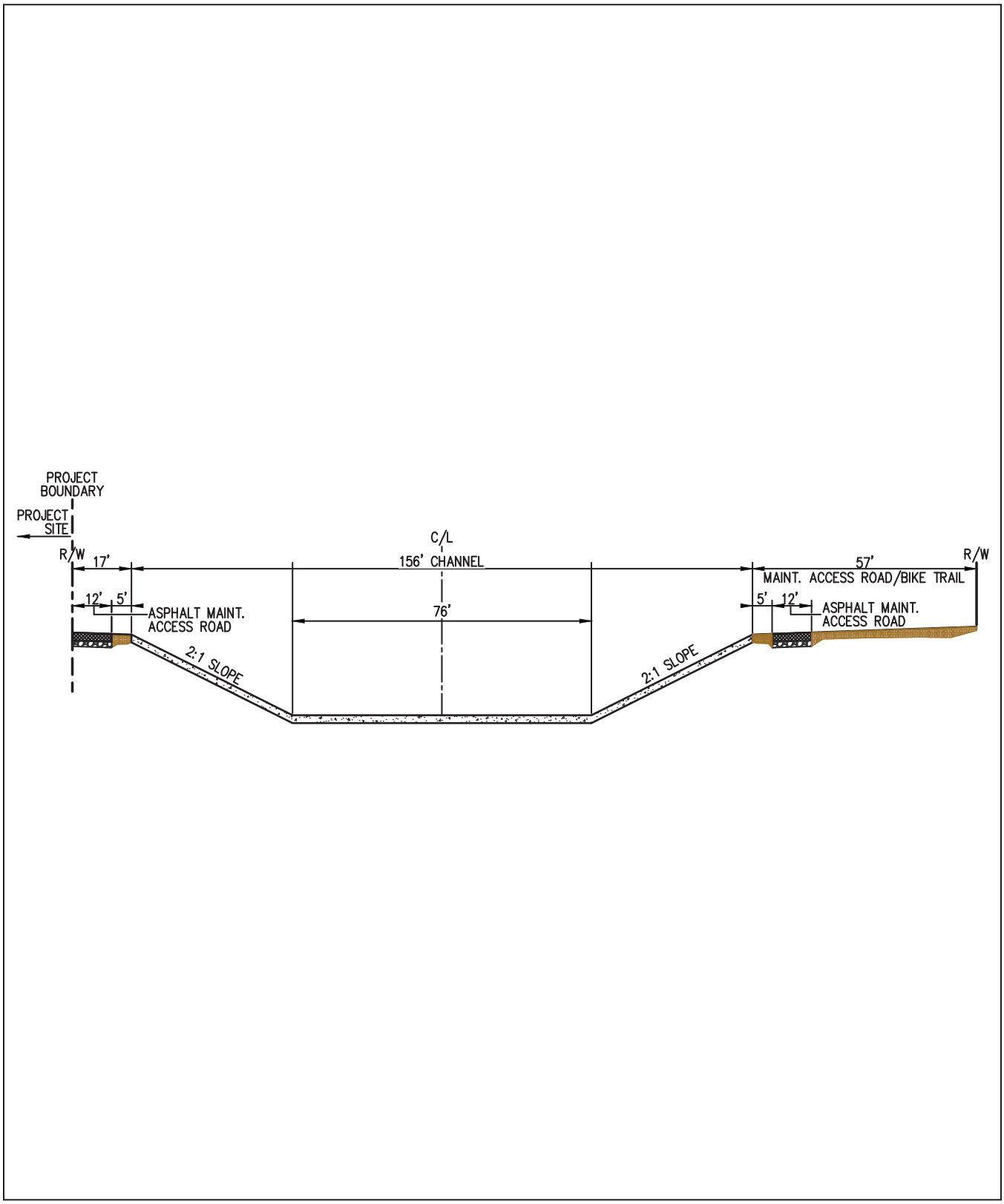
Exhibit 5.3b, Typical Street Cross Section - Carpenter Avenue & Remington Avenue (70' ROW)



NOTE: City of Ontario Standard 1053 (66' ROW) to be used up to the centerline which is the City Boundary. Parking shall be prohibited.

Source: David Evans & Associates

Exhibit 5.3c, Cucamonga Creek Channel



Source: David Evans & Associates

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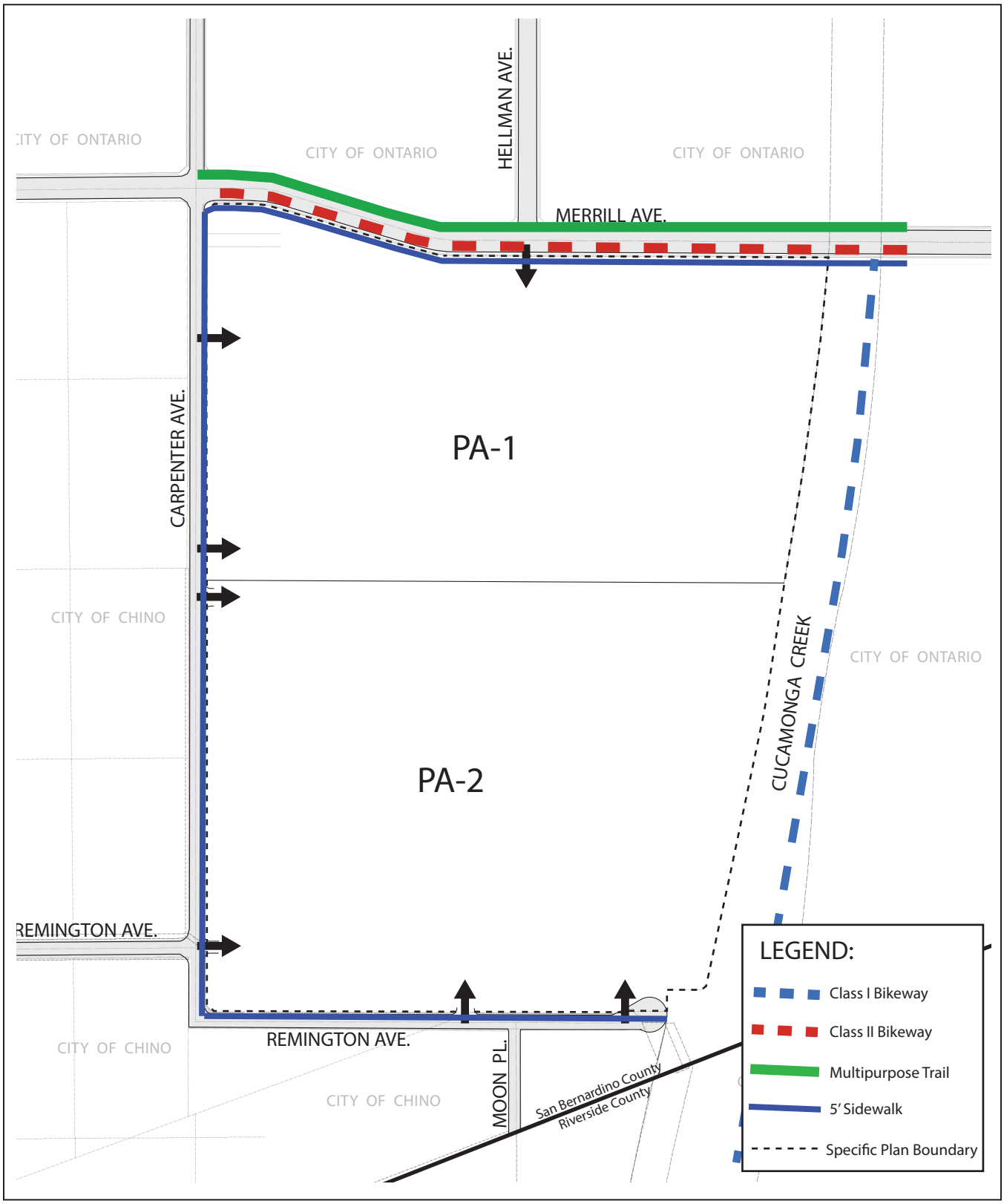
Exhibit 5.4, Pedestrian and Bicycle Circulation



Source: KTGY Group

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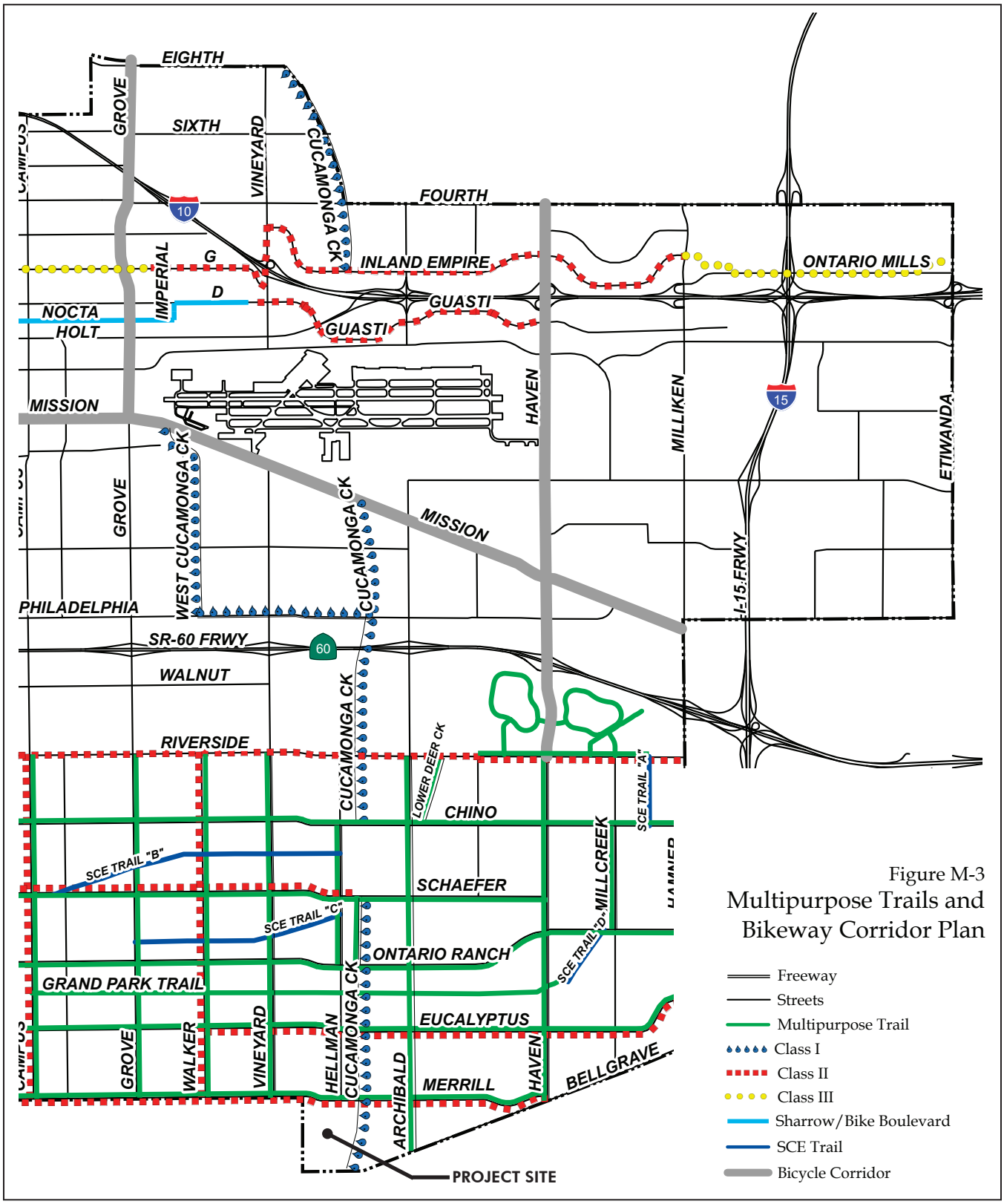
Exhibit 5.5, City of Ontario Trails & Bikeway Plan



Source: City of Ontario, Figure M-3 Multipurpose Trails and Bikeway Corridor Plan (Sept. 2016)
NOTE: Reference the City's most current Master Plan for the lastest route.

N. T. S.

5.2 Water Master Plan

Domestic water will be provided by the City of Ontario. The City's Water Master Plan identifies new water facilities to serve the Ontario Ranch area, which will need to be constructed prior to or concurrent with on-site water improvements.

All private agricultural wells located within each Tract shall be destroyed per Cal Department of Water Resources prior to the issuance of a certificate of occupancy for any construction activity. Well destruction requires a permit from County Health Department. A copy of such permit shall be provided to Engineering and OMUC prior to issuance of certificate of occupancy.

Note: Reference the City's most current Master Plan for sizing/alignment.

5.2.1 Master Planned Domestic Water System

The project site lies within the 925' Pressure Zone as depicted on Exhibit 5.6, City of Ontario Ultimate Water System. The ultimate improvements for domestic water will include a 12" Master Plan water main in Merrill Avenue, from Archibald Avenue to Carpenter Avenue, and a 12" water main in Carpenter Avenue, from Merrill Avenue to Remington Avenue. The water main continues east on Remington Avenue, crosses the Cucamonga Creek Channel and connects to the existing 12" water main in Archibald Avenue as depicted on Exhibit 5.7, Domestic Water System.

Within the project site, a network of 8" and 10" water lines will be installed for the private fire system. The on-site water system includes connections to the main in Carpenter Avenue or the main in Merrill Avenue. The proposed on-site water system sizing is subject to the recommendations of the City Building Department.

Existing Phase 1 water supply infrastructure for the 925' Zone has been recently constructed and are generally located within the eastern portion of Ontario Ranch. Water supply infrastructure (production, storage, transmission) required for this Specific Plan will also need to incorporate the following:

a) The future Phase 2 backbone water infrastructure for the 925' Zone generally consisting of transmission mains, wells, and reservoir as depicted in Exhibit 5.6.

b) A minimum of two points of connection to the backbone transmission main(s) (Phase 1 and/or Phase 2) to provide for looped water service.

Note: Reference the City's most current Master Plan for sizing/alignment.

5.2.2 Master Planned Recycled Water System

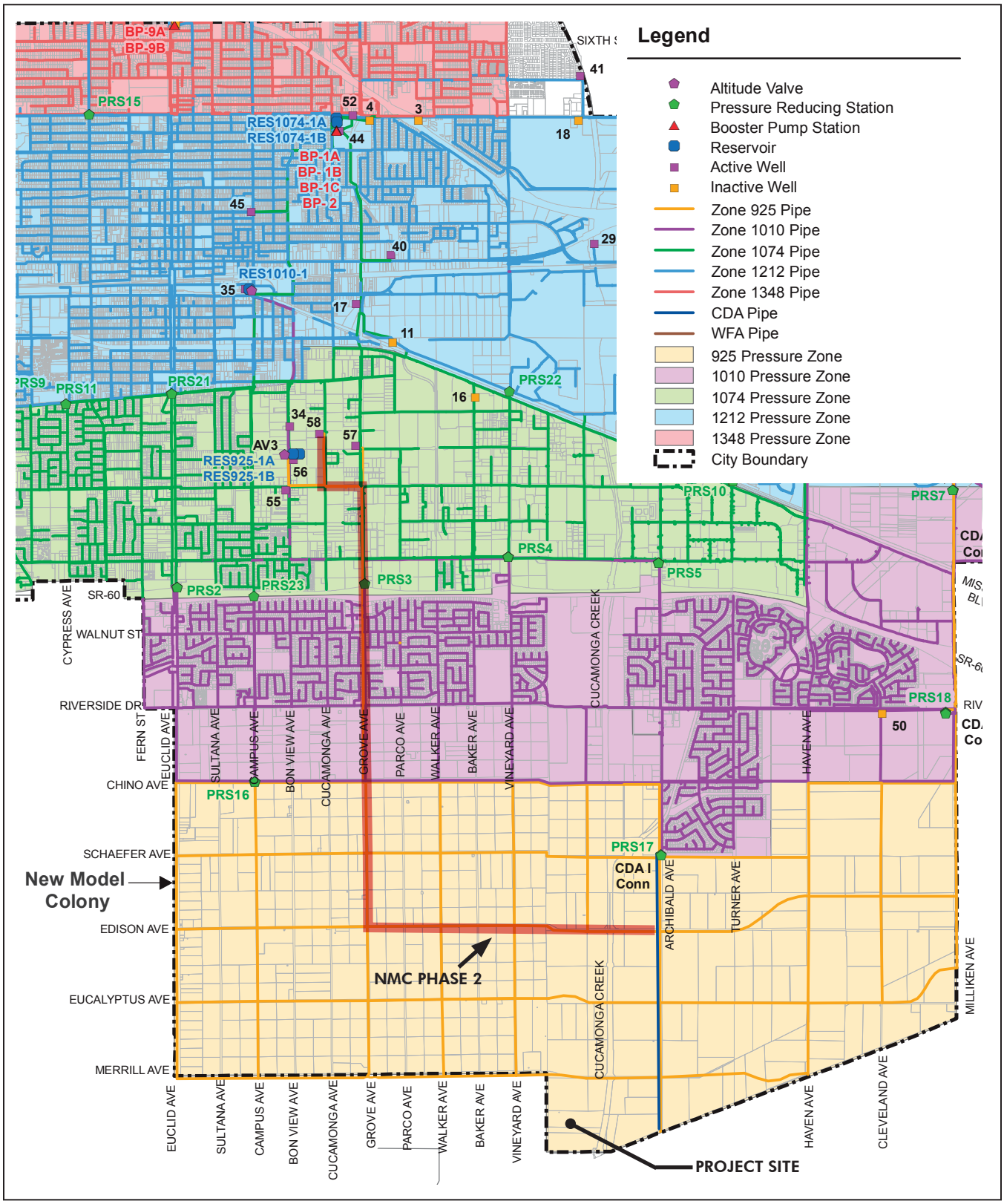
The City of Ontario will ultimately provide recycled water from IEUA's RP-1 and RP-1 outfall parallel located in Carpenter Avenue and via City of Ontario recycled water improvements as presented in the City's Recycled Water Master Plan (see Exhibit 5.8).

The 930' Pressure Zone recycled water system has an existing Inland Empire Utilities Agency (IEUA) 30" line in Carpenter Avenue that runs along the project frontage from Remington Avenue to Merrill Avenue. This project will construct a 12" City of Ontario line along Merrill Avenue from Carpenter Avenue to Archibald Avenue that will serve Colony Commerce Center West as illustrated on Exhibit 5.9, Recycled Water System.

The developer of Colony Commerce Center West will utilize the existing recycled water laterals that stub into both Planning Areas and enhance where required to serve the project. The Colony Commerce Center West Specific Plan shall comply with City Ordinance 2689 and make use of recycled water for all approved uses, including but not limited to the irrigation of street landscaping, and common areas.

The developer shall prepare and secure approval of an Engineering Report from the City of Ontario and State Water Resources Control Board (SWRCB) prior to the use of recycled water. Sizing of the on-site system

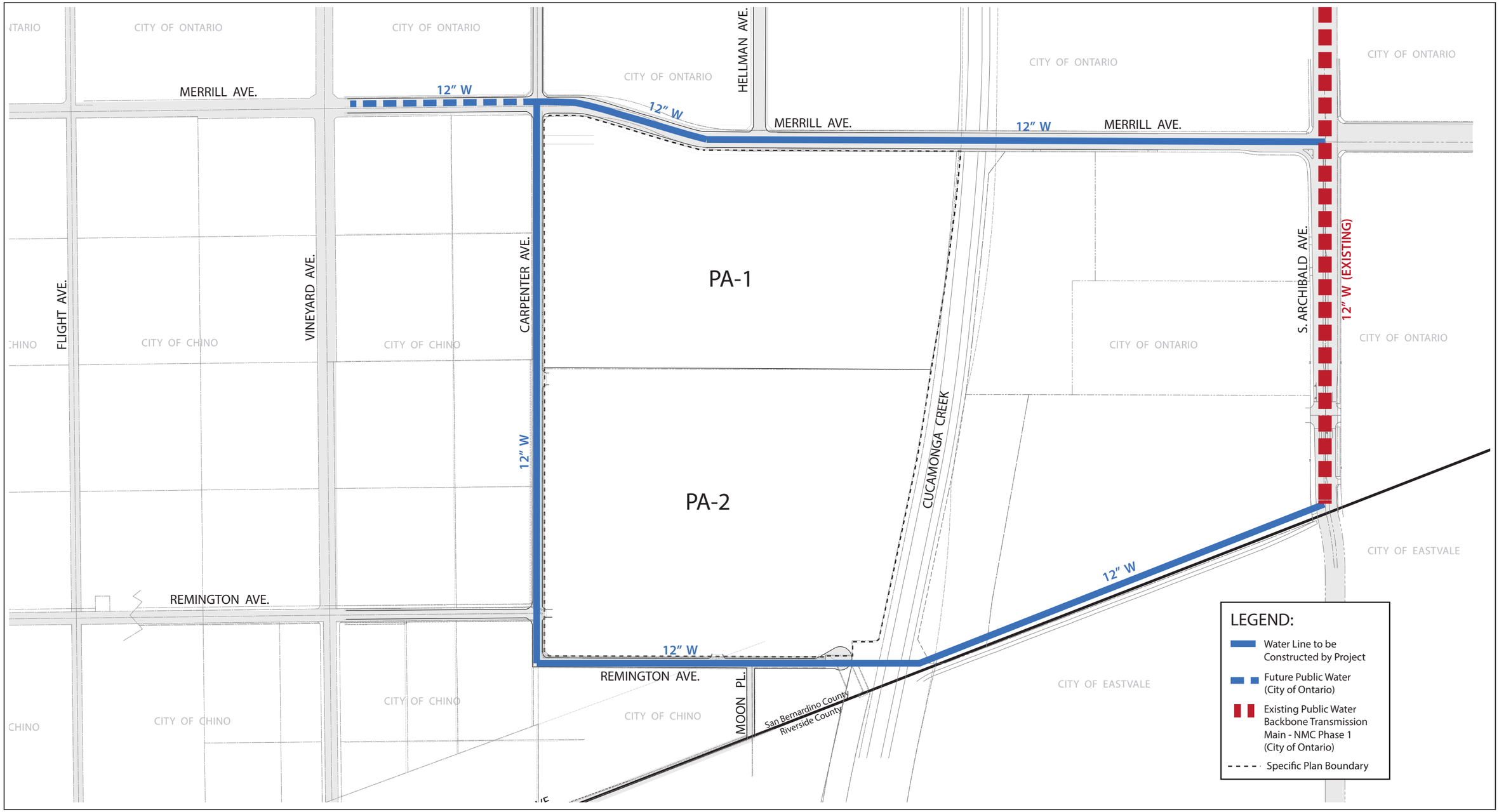
Exhibit 5.6, City of Ontario Ultimate Water System



Source: City of Ontario, Ultimate Water System (Figure 10-1) October 2011
NOTE: Reference the City's most current Master Plan for sizing/alignment.

N. T. S.

Exhibit 5.7, Domestic Water System



Source: David Evans & Associates
NOTE: Reference the City's most current Master Plan for sizing/alignment.

N. T. S.

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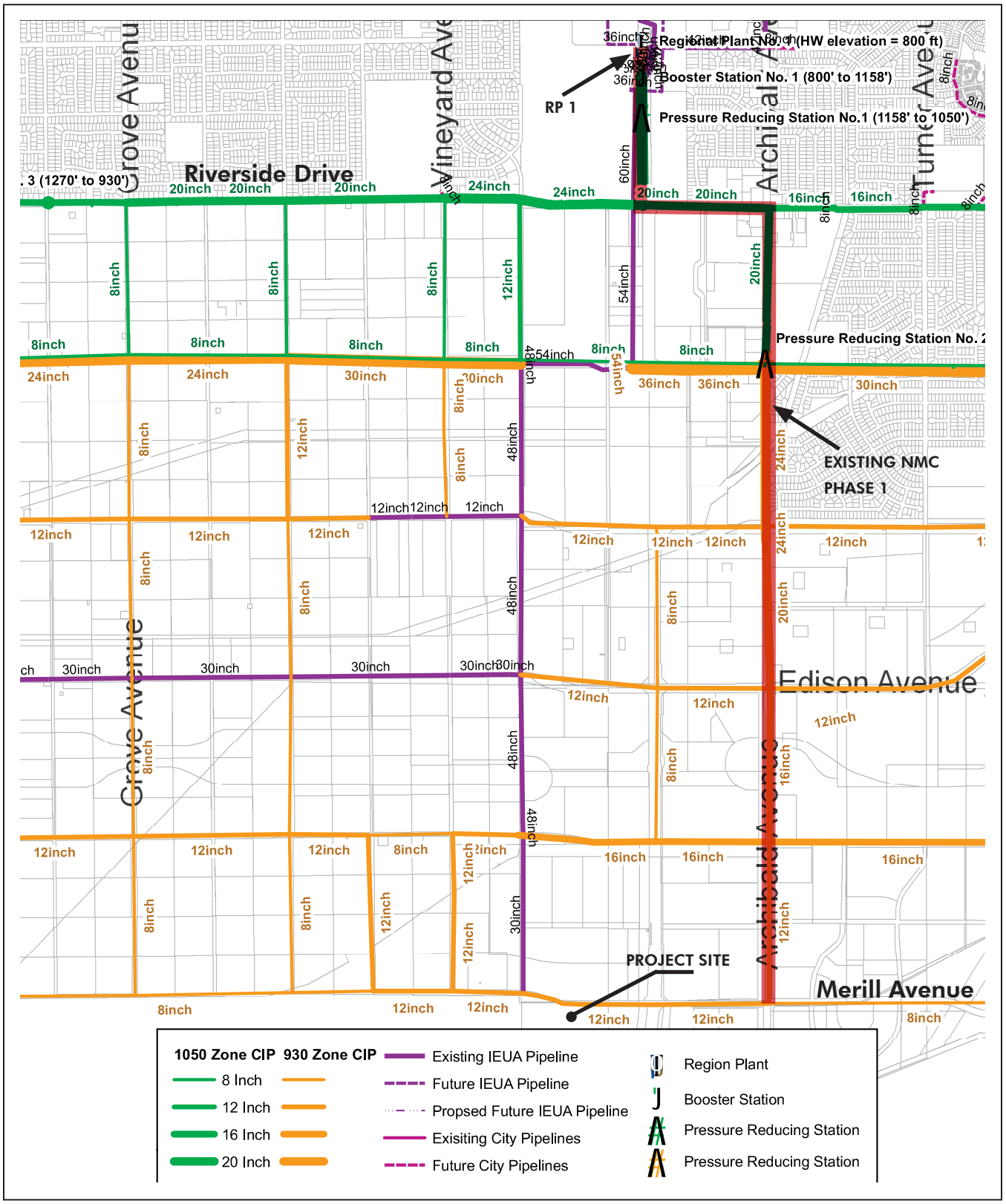
Exhibit 5.8, (Partial) Ontario Ranch Recycled Water Master Plan



Source: City of Ontario, Recycled Water Master Plan, Figure 6-2 (October 2011)
NOTE: Reference the City's most current Master Plan for sizing/alignment.

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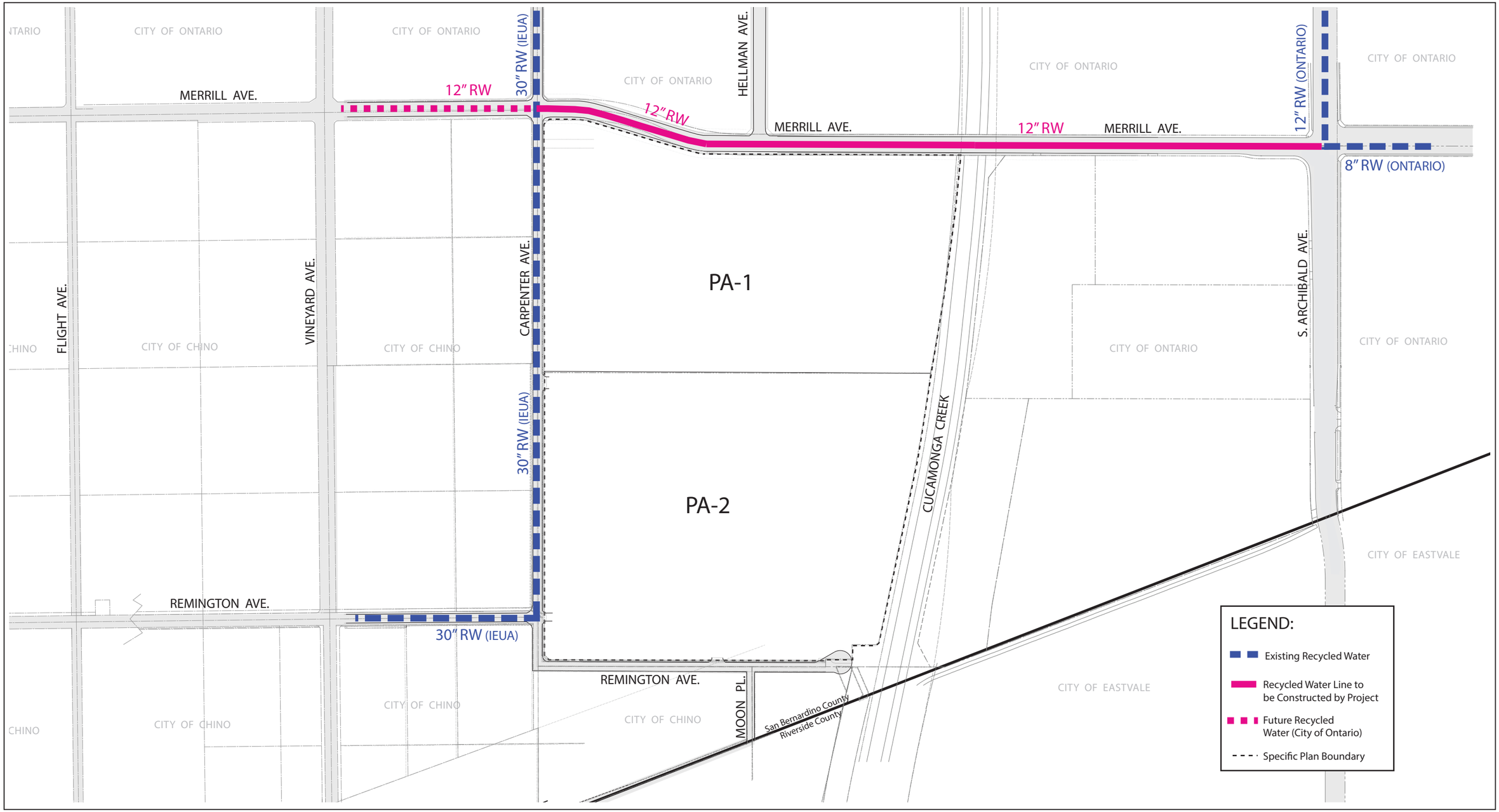
Exhibit 5.9, Recycled Water System



Source: David Evans & Associates
NOTE: Reference the City's most current Master Plan for sizing/alignment.

N. T. S.

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is subject to the City approved hydraulic analysis and minimum requirements of the City. Interim connection to potable water is not allowed.

Note: Reference the City's most current Master Plan for sizing/alignment.

5.3 Sewer Master Plan

Sewer service for Colony Commerce Center West will be provided by the City of Ontario. The City of Ontario Master Plan of Sewer as depicted on Exhibit 5.10, proposes an 18"Sewer Trunk Line in Carpenter Avenue from the Eastern Trunk Sewer north to Merrill Avenue. The latest design of this master planned trunk sewer line, performed by MDS Consulting, requires the upsizing of this 18" line to a 24" line between the Eastern Trunk Sewer and Merrill Avenue.

The Colony Commerce Center West Specific Plan proposes a revised alignment for this 24" Sewer Trunk Line. The new alignment will run eastwardly in Remington Avenue from the southerly extension of Carpenter Avenue and southwardly on Moon Place where it will connect to the Eastern Trunk Sewer approximately 1,000 feet northeast of the original connection point. The reason for this proposed revision to the connection point (See Exhibit 5.11, Sewer Master Plan) is the lack of right-of-way or easement availability from the landowner south of Remington Avenue.

The revised alignment will allow the 24" Sewer Trunk Line to be constructed within Moon Place, which is a publicly dedicated street. It should be noted that this alternative alignment will require the City to amend its Sewer Master Plan.

The size and location of the on-site private sewer system required to service the buildings will be engineered during preparation of the final on-site construction documents, per Building Department requirements..

Note: Reference the City's most current Master Plan for sizing/alignment.

5.4 Drainage

The City of Ontario Storm Drain Master Plan identifies storm drain improvements to serve the project site. Completion of these Master Plan improvements will provide storm water drainage for the properties within this specific plan.

The runoff that leaves the site drains to the lower reach of Cucamonga Creek a tributary of the Santa Ana River at Prado Reservoir. The site is a part of the 74 square mile drainage area that is tributary to the Creek. The total drainage area is bounded by the San Gabriel Mountains on the north, San Antonio Creek drainage area on the west, Day Canyon drainage area on the east and Prado Reservoir on the south. The channel has an approximate gradient of 40 feet per mile and is designed to carry 45,000 cfs at the Confluence with the County Line Channel just downstream of the project site.

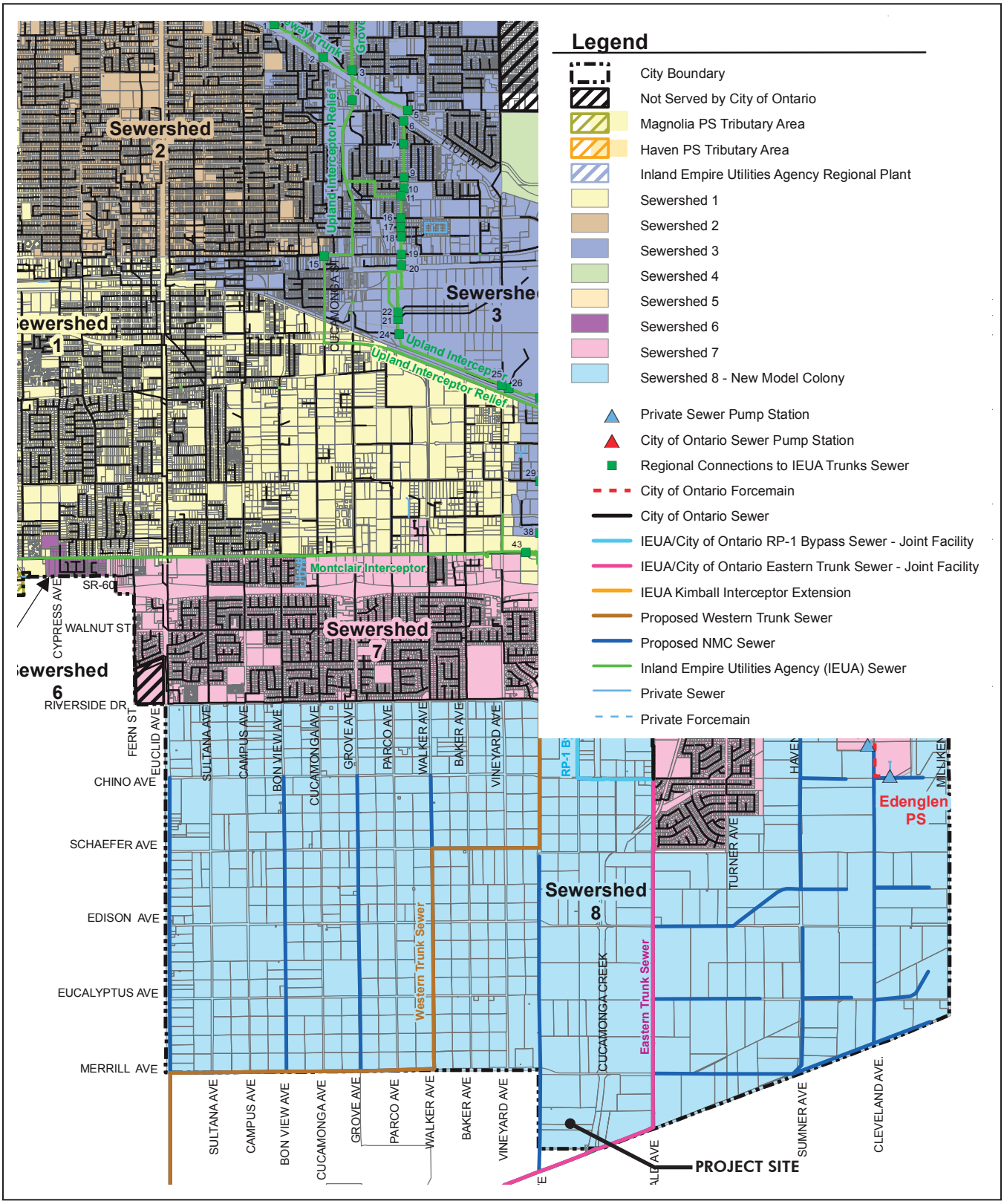
The existing Storm Drain Master Plan indicates the properties within this specific plan as tributary to the Walker Storm Drain System and calls out a double 10-foot by 10-foot box culvert (Double 10x10 Box) serving Area XII as depicted on the City of Ontario's Drainage Area Map, see Exhibit 5.12. That alignment depicts the 10x10 Box beginning northerly on Walker Avenue and continuing south past Merrill Avenue to Remington Avenue, then east along Remington Avenue, connecting into Cucamonga Creek.

The property owner south of Merrill Avenue and West of Carpenter Avenue, in City of Chino, does not need to connect to this Double 10x10 Box and does not want it constructed within their property.

Planning Areas 1 & 2 will drain to the existing 60" storm drain connection into Cucamonga Channel at Remington Avenue that will provide proper drainage capacity to serve the properties within this specific plan.

This specific plan proposes modifying the ultimate alignment of the Double 10x10 Box to turn east at Merrill Avenue and connect into the Cucamonga Channel at the intersection of Merrill Avenue. Due

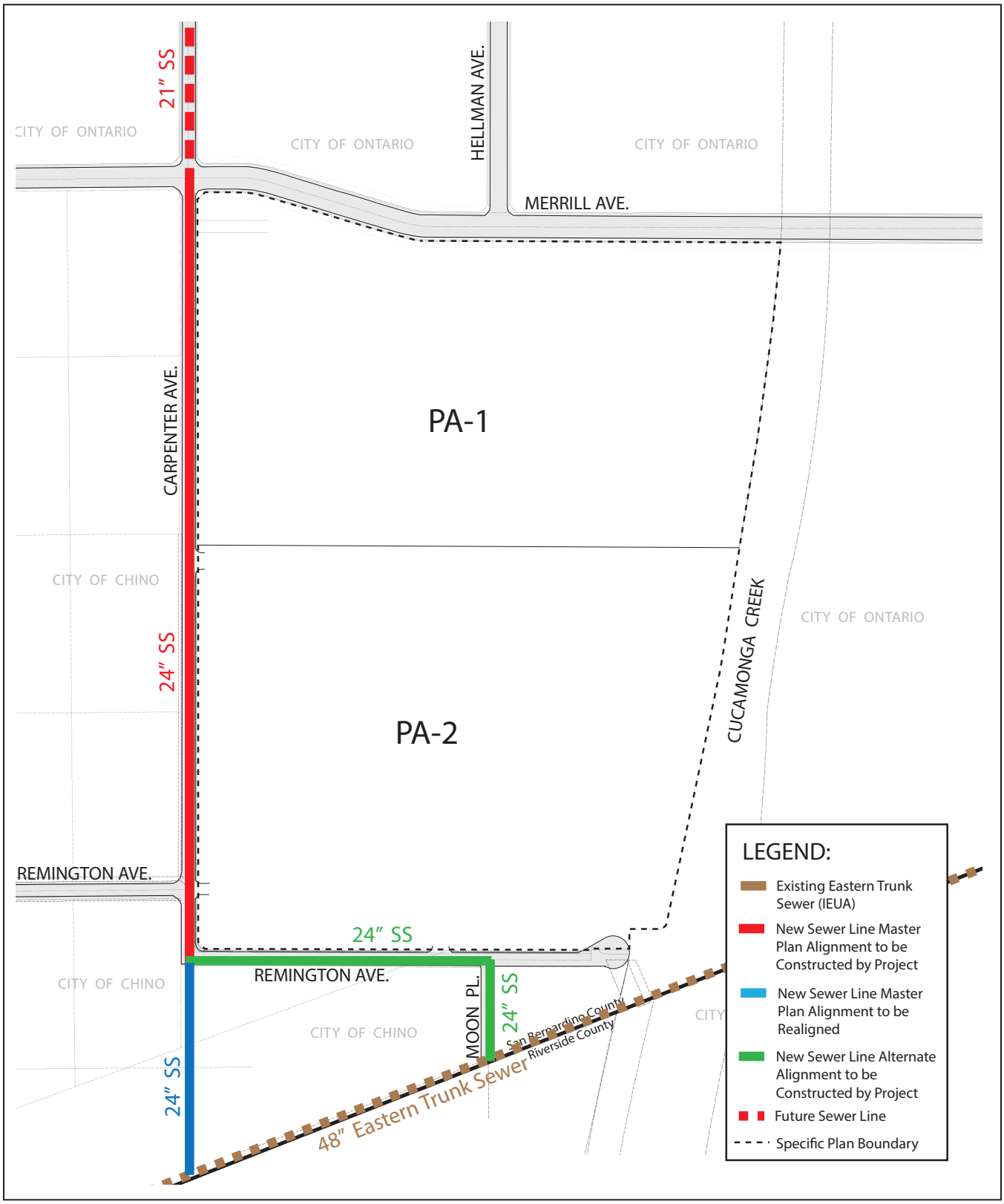
Exhibit 5.10, City of Ontario Ultimate Sewer System



Source: City of Ontario, Ultimate Sewer System (Figure 6-1) October 2011
NOTE: Reference the City's most current Master Plan for sizing/alignment.

N. T. S.

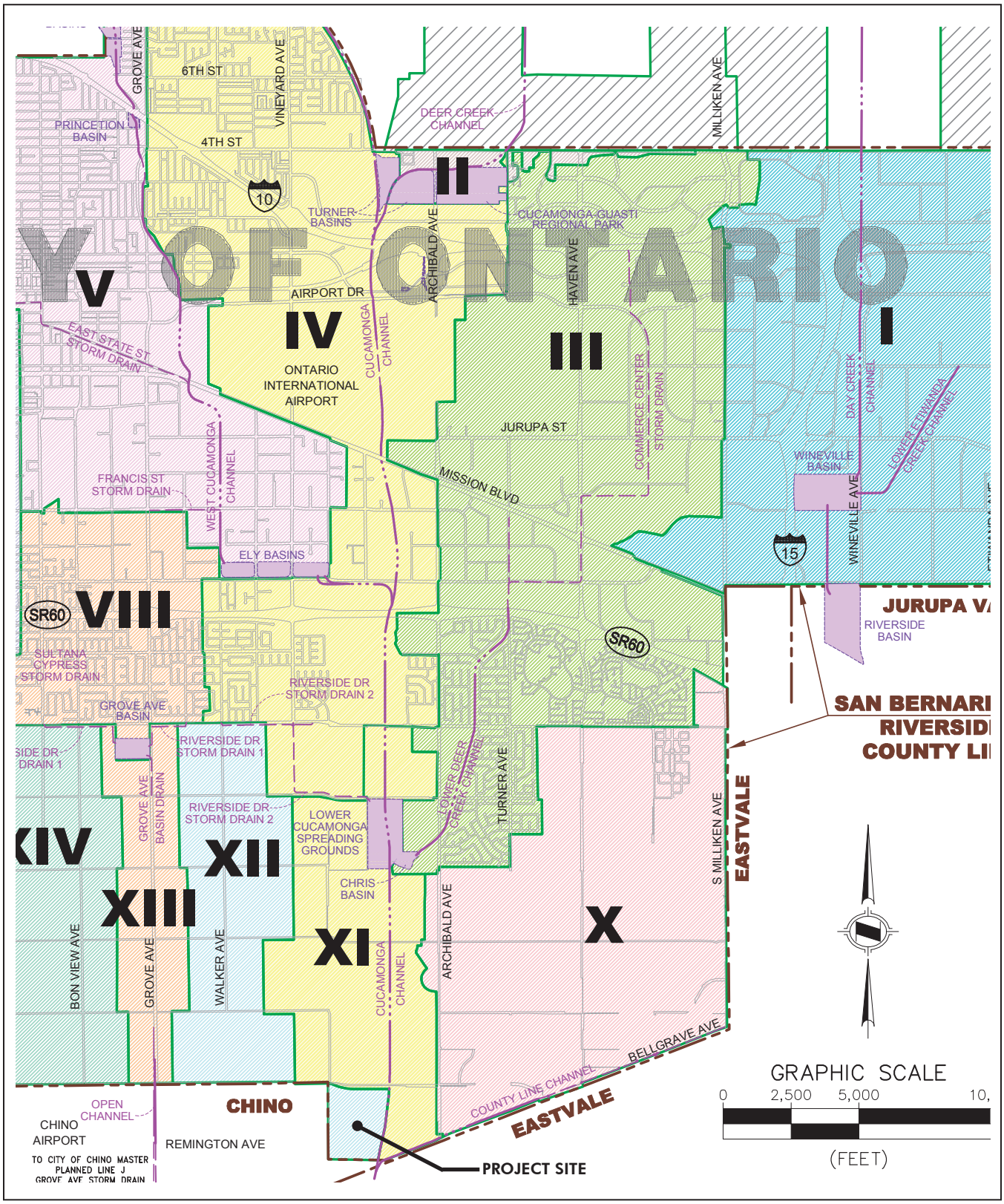
Exhibit 5.11, Sewer Master Plan



Source: David Evans & Associates
NOTE: Reference the City's most current Master Plan for sizing/alignment.

N. T. S.

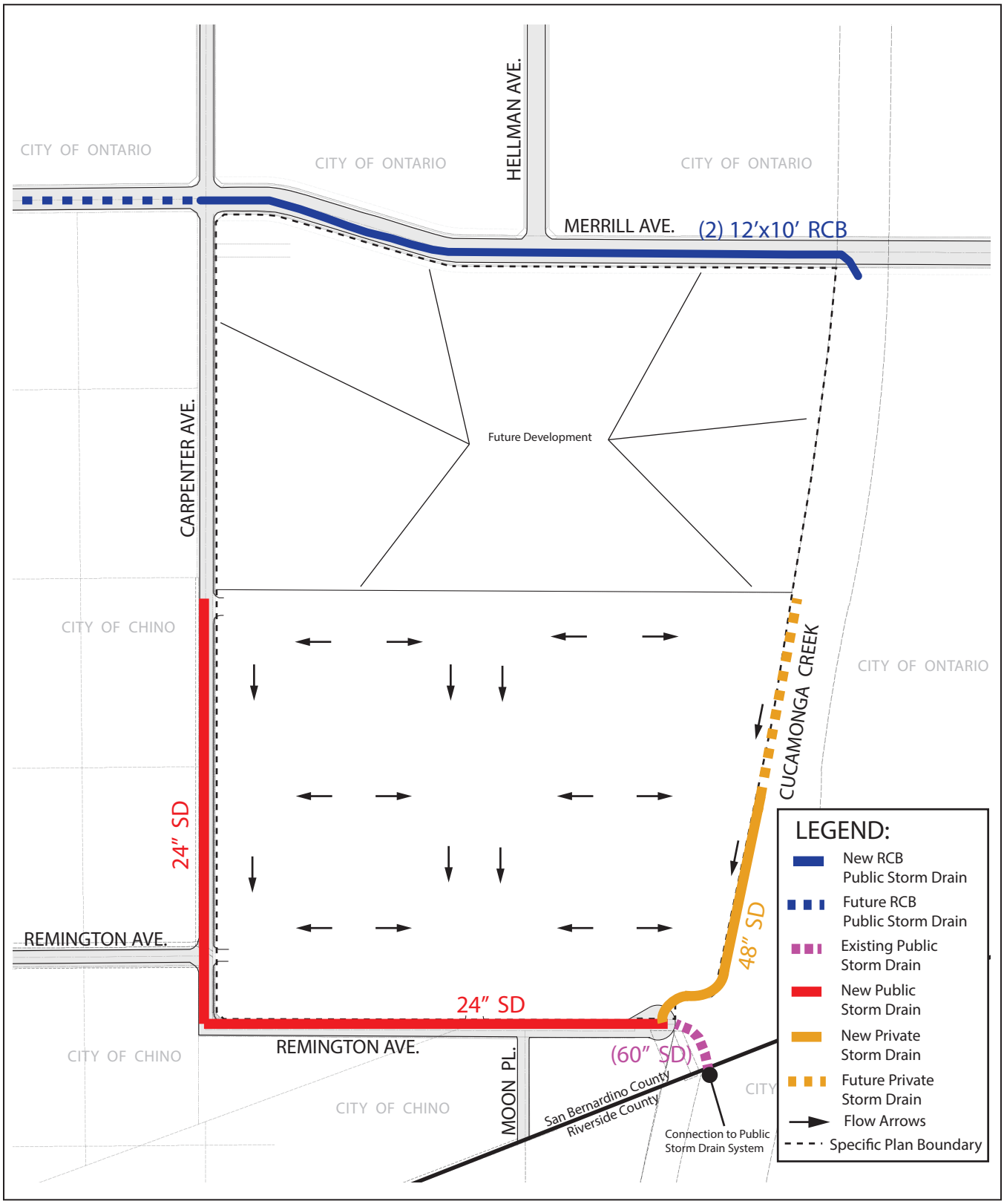
Exhibit 5.12, City of Ontario Drainage Area Map



Source: City of Ontario's Drainage Area Map (Exhibit 7), March 10, 2012
NOTE: Reference the City's most current Master Plan for sizing/alignment.

N. T. S.

Exhibit 5.13, Drainage Plan / Hydrology



Source: David Evans & Associates
NOTE: Reference the City's most current Master Plan for sizing/alignment.

N. T. S.

to the new alignment and hydraulic grade line (HGL), the 10x10 box needs to be upsized to a 12x10. This alignment will reduce the total linear footage of the Double 12x10 Box , provide a straighter alignment & single point of connection, while providing the same storm water drainage capacity for the properties to the north and west that drain to it.

The proposed modification to the Master Plan of drainage for Colony Commerce Center West is illustrated in Exhibit 5.13 Drainage / Hydrology.

5.4.1 NPDES Compliance

The grading and drainage of the Specific Plan Area shall be designed to detain, filter, and treat surface runoff in a manner and combination which is practical, to comply with the requirements of the San Bernardino County NPDES Storm Water Program's current Water Quality Management Plan (WQMP) for new development projects.

The objective of the WQMP for the project is to minimize the detrimental effects of urbanization on the beneficial uses of receiving waters, including effects caused by increased pollutants and changes in hydrology. These effects shall be minimized through the implementation of on-site and off-site Low Impact Development (LID) Site Design Best Management Practices (BMP's) that retain/infilter or biotreat 85th percentile storm event runoff from the project.

In addition, non structural and structural Source Control BMP's shall also be implemented and documented in the projects approved Water Quality Management Plan(s) to reduce pollutant generation and transport from the project site.

Participation in an alternative regional or watershed-based Treatment Control BMP , such as, the Mill Creek Wetlands Project, is regulated by the requirements of the San Bernardino County Stormwater SB County MS4 Permit and the Water Quality Management Plan Technical Guidance Document.

Prior to the issuance of grading or construction permits for any parcel map or area that disturbs 1 acre or more of land, within the Colony Commerce Center West Specific Plan area shall be required to obtain coverage, Erosion/Sediment Control Plans and Storm Water Pollution Prevention Plans (SWPPP) shall be prepared. The SWPPP shall be prepared to comply with California State Water Resources Control Board's (State Water Board) current "General Permit to Discharge Storm Water Associated with Construction Activity" and current "Area Wide Urban Storm Water Runoff (Regional NPDES) Permit."

The SWPPP shall identify and detail all appropriate Best Management Practices (BMP's) to be implemented or installed during construction of the project.

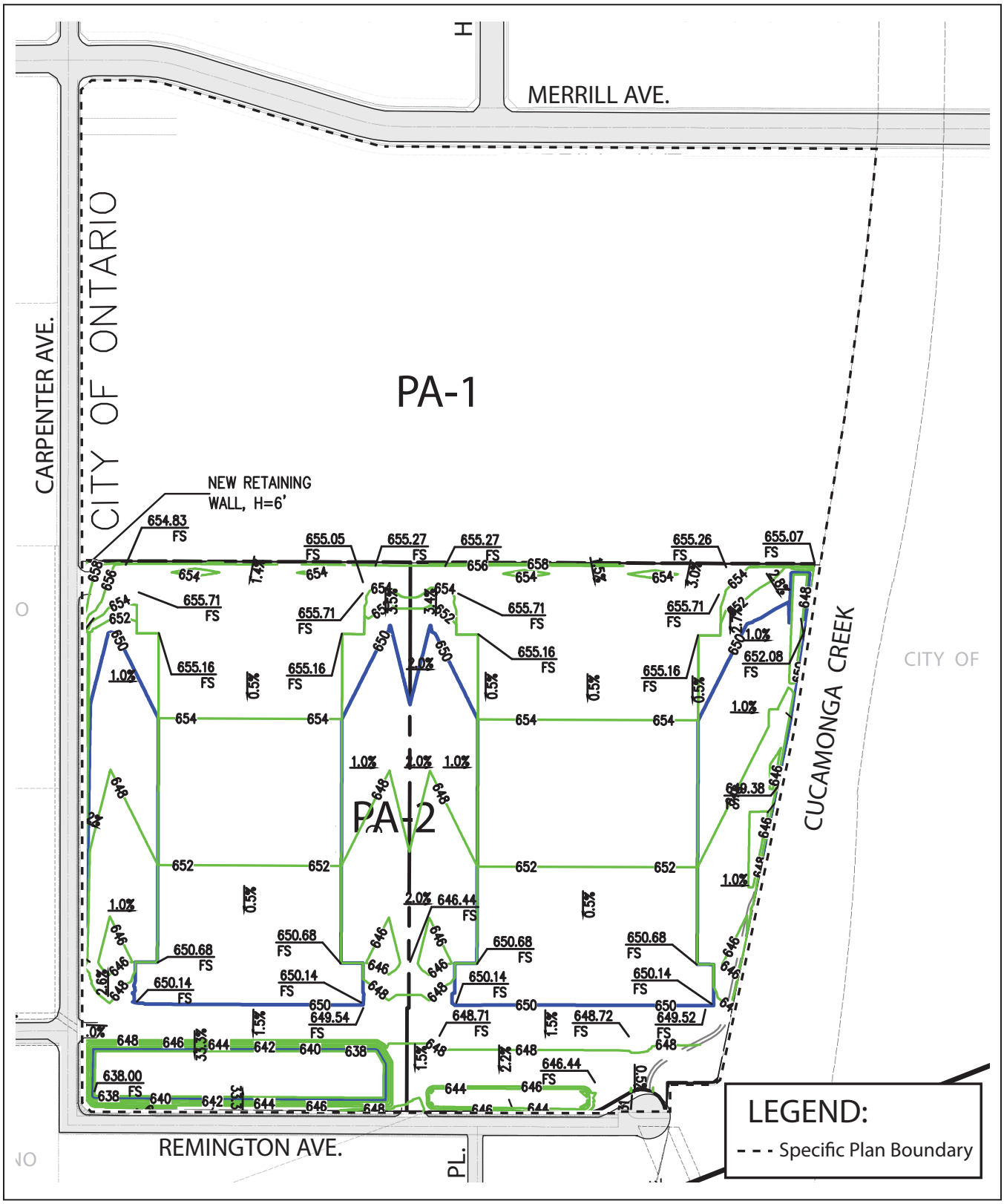
5.5 Grading Concept

The project site generally slopes to the south at approximately 1.0% to 2.0%. The grading activities for Colony Commerce Center West will generally consist of clearing and grubbing, demolition of existing structures, and moving surface soils to construct building pads and streets. Where slope conditions are present, the project lot line shall be located at the top of a slope.

The Conceptual Grading Plan, as illustrated in Exhibit 5.14, Conceptual Grading Plan, provides a balance of cut/fills for the project. Grading plans for each tract within the project shall be reviewed and approved by the City of Ontario Building, Planning, and Engineering Departments prior to the issuance of grading permits. All grading plans and activities shall conform to the City's grading ordinance and dust and erosion control requirements.

All landscape areas, adjacent to streets, including medians, parkways and neighborhood edges, in the Specific Plan Area, shall be finish graded, at a minimum of 1 ½" below top-of-curb or sidewalk finish surface, for conservation of irrigation water and increased retention of rainwater runoff.

Exhibit 5.14, Conceptual Grading Plan



Source: David Evans & Associates

N. T. S.

Wherever practicable, landscaped areas within the project shall be graded as swales and designed to accept runoff water from impervious surfaces. Where necessary, a 5' wide level pad area shall be provided for utilities adjacent to slopes, at each side of detention basins or swales adjacent to paving for pedestrian safety and for screening shrubs.

5.6 Dry Utilities

Utility services provided to the site consist of natural gas, electricity, and communications systems. Utility lines will be installed underground in accordance with City of Ontario guidelines.

5.6.1 Communication Systems

The proposed backbone street fiber optics (conduits, hand holes, tracer wire, and fiber) will be placed underground within a duct and structure system to be installed by the Master Developer in a joint trench, as Illustrated in Exhibit 5.15. In-tract fiber and conduit shall be installed by the Developers per the in-tract fiber optic design guidelines. Maintenance of the installed system will be the responsibility of the City/Special District. Development of the Project requires the installation by the Developers of all fiber optic infrastructure and peripheral equipment necessary to service the Project as a stand-alone development.

5.6.2 Natural Gas

The Gas Company will provide natural gas to the Specific Plan area. The Gas Company will install gas mains to the Specific Plan area as necessary.

5.6.3 Electricity

Southern California Edison Company (SCE) currently provides electrical service in the area. All new lines and all existing lines within the Specific Plan area shall be installed according to City of Ontario requirements.

There are existing power poles that run along the west side of Carpenter Avenue from Merrill Avenue in the north to west extension of Remington Avenue in the south. From the west extension of Remington Avenue, the power poles diagonally cross Carpenter Avenue and run south until the east extension of Remington Avenue. From there the power poles run east along the north side of Remington Avenue and cross the Cucamonga Creek bridge. The existing overhead lines along the project frontage will be relocated underground per the City's Municipal Code.

5.7 Public Facilities and Services

Public services and facilities play an essential role in providing support services to create viable, sustainable, healthy and cohesive communities.

5.7.1 Police

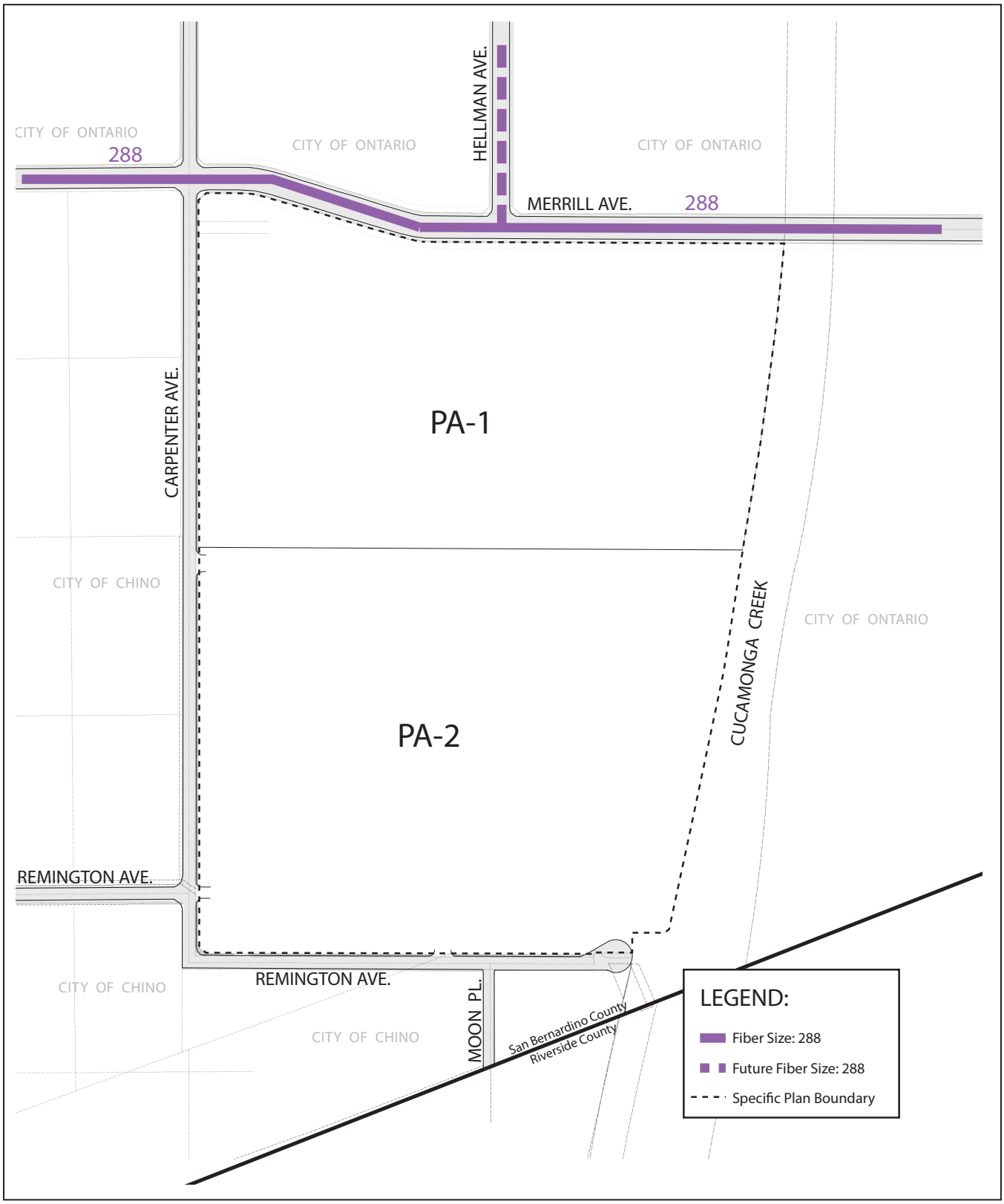
The Ontario Police Department will provide law enforcement to the Colony Commerce Center West area. The Ontario Police Department's mission statement is as follows: "The mission of the Ontario Police Department is to protect life and property, solve neighborhood problems, and enhance the quality of life in our community. We do this by providing superior police services while fostering successful community partnerships."

5.7.2 Fire

The Ontario Fire Department will provide fire protection, paramedic, and emergency response services to the Specific Plan Area. The closest operational fire station is Station 6 located at 2931 E. Philadelphia Avenue. The Ontario Fire Department currently has eight stations, which are comprised of eight 4-man paramedic engine companies and two 4-man truck companies.

The City is in the process of developing 13 square miles in the Ontario Ranch where the Ontario Fire Department will shortly begin construction of Fire Station Number Nine located at 2661 E. Park Vista Drive.

Exhibit 5.15, Fiber Optic Master Plan



Source: City of Ontario, Figure 7 (Fiber Size and Footages)
NOTE: Reference the City's most current Master Plan for sizing/alignment.

N. T. S.

5.7.3 Solid Waste Disposal

The Ontario Municipal Utilities Company is committed to providing reliable, timely, safe, and affordable refuse collection services to the residents and businesses in the city limits. Solid waste requirements shall follow the approved "Solid Waste Department Refuse and Recycling Planning Manual."

5.8 Infrastructure Phasing Plan

The primary intent of the phasing of the project is to ensure that complete and adequate public facilities and services are in place and available to the Specific Plan area as needed.

The phasing program for Colony Commerce Center West will be executed to provide the services and infrastructure required for each of the development planning areas. The phasing set forth in this Specific Plan shall be conditioned on the approval of tentative tract maps. It should be noted that the ultimate pace and phasing of the development is dependent on a number of internal and external factors and is subject to change. See Exhibit 5.16, Conceptual Phasing Plan.

Not all planned development within a given phase may be completed prior to the initiation of the next phase. In cases where development within a new phase is to begin prior to the completion of a phase in progress, all infrastructure improvements shall be funded and designed for the phase in progress before any new phase may begin.

5.8.1 Planning Areas and Streets

The project will be developed in three or more phases. These phases may occur sequentially or concurrently with one another. Build out of the project is undetermined at this time.

Public streets within and abutting the Specific Plan area shall be improved in accordance with approved development agreement. Traffic Impact Analysis may recommend additional improvements, including those potentially outside the limits of the Specific Plan Area, prior to or concurrent with specific development milestones.

5.8.2 Water, Sewer and Recycled Water

Water and sewer services will be provided for each of the planning areas.

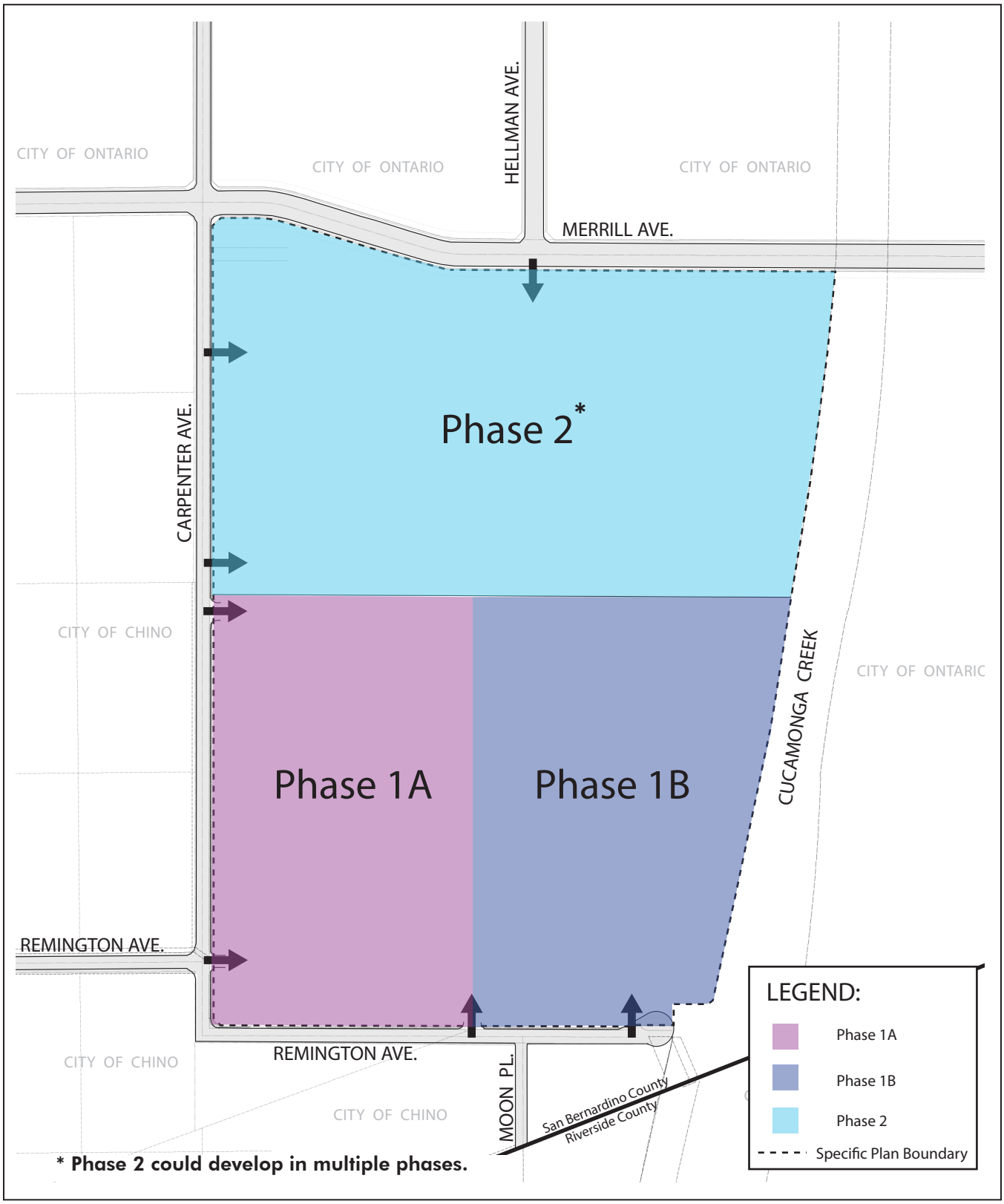
Domestic Water: Phases 1A and 1B require the construction of the 12" Master Plan water line in Merrill Avenue from Archibald Avenue to Carpenter Avenue, in Carpenter Avenue from Merrill Avenue to Remington Avenue, and eastward in Remington Avenue to Archibald Avenue. Due to the location of Phase 1A and 1B, the 12" water line will also be able to serve Phase 2.

Recycled Water: The developer of Colony Commerce Center West will utilize the existing recycled water laterals that stub into both Planning Areas and enhance where required to serve the project. This project will also construct a 12" City of Ontario line along Merrill Avenue from Carpenter Avenue to Archibald Avenue. The recycled water improvements will not be phased and connection to the system is required prior to occupancy.

Sewer: The City of Ontario Master Plan of Sewer proposes an 18" Sewer Trunk Line in Carpenter Avenue from the Eastern Trunk Sewer north to Merrill Avenue. The latest design of this master planned trunk sewer line, performed by MDS Consulting, requires the upsizing of this 18" line to a 24" line between the Eastern Trunk Sewer and Merrill Avenue.
To provide sewer for all phases of development, this project proposes a realignment of this 24" line. The new alignment will run eastwardly in Remington Avenue from the southerly extension of Carpenter Avenue and southwardly on Moon Place where it will connect to the Eastern Trunk Sewer approximately 1,000 feet northeast of the original connection point.

5.8.3 Drainage

Exhibit 5.16, Conceptual Phasing Plan



Source: KTGY Group

N. T. S.

Phase 1a & 1b improvements include the construction of a 60" storm drain line from the NWC of Phase 1a & 1b in Carpenter Avenue, down Carpenter Avenue to Remington Avenue (East), where it heads east and ties into the existing 60" storm drain outlet at Cucamonga Channel.

It should be noted that the ultimate phasing of the development is dependent on a number of internal and external factors. Not all planned development within a given phase may be completed prior to the initiation of the next phase.

5.9 Infrastructure Plan and Phasing Adjustments

The Board of Zoning Adjustment shall have the authority to hear and decide applications for modifications to the infrastructure phasing plans. The Board shall be required to make the following findings:

- That modification is consistent with the General Plan;
- That the proposed changes will not adversely affect the implementation of the Specific Plan;
- That it will not be detrimental to the public health, safety, and general welfare; and
- That the proposed modification will not delay the construction of the master plan improvements necessary to serve the development.

6 DEVELOPMENT REGULATIONS

6.1 Introduction

The provisions contained herein shall regulate design and development within the Colony Commerce Center West Specific Plan. The regulations contained herein establish the minimum standards and requirements for development.

6.2 Definition of Terms

The meaning and construction of words, phrases, titles, and terms shall be the same as provided in the City of Ontario Development Code Article 2, "Definitions," unless otherwise specifically provided for herein.

The definition of architectural and design terms shall be the same as those provided in the City of Ontario Glossary of Design Terms which follows the City of Ontario Development Code, as amended through June 2003.

6.3 Applicability

The development regulations contained herein provide specific land use development standards for the project. Regulations address industrial development and provide for general landscaping regulations. Application of the following regulations is intended to encourage the most appropriate use of the land, ensure the highest quality of development, and protect the public health, safety, and general welfare.

Whenever the provisions and development standards contained herein conflict with those contained in the City of Ontario Development Code, the provisions of the Colony Commerce Center West Specific Plan shall take precedence. Where the Colony Commerce Center West Specific Plan is silent, City codes shall apply. These regulations shall reinforce specific site planning, architectural design, and landscape design guidelines contained in Chapter 7, "Design Guidelines" of the Colony Commerce Center West Specific Plan.

All architectural and landscape improvements shall be consistent with the Design Guidelines contained in Chapter 7, of the Colony Commerce Center West Specific Plan, "Design Guidelines." All architectural and landscape plans shall be submitted to the City of Ontario for approval.

6.4 Administration

The Colony Commerce Center West Specific Plan is adopted by ordinance and serves to implement the Policy Plan Land Use Plan (Policy Plan Exhibit LU-01) as well as the zoning for the Specific Plan Area. The Colony Commerce Center West Specific Plan addresses general provisions, permitted uses, development standards, and design guidelines.

The Colony Commerce Center West Specific Plan Development Regulations address general provisions, permitted uses, and development standards for the community. The Specific Plan has been prepared in conformance with the Goals and Polices of the Policy Plan as outlined Chapter 3 "Plan Conformance".

6.5 General Site Development Criteria

The following general site development criteria shall apply to all development projects within Colony Commerce Center West.

- Gross Acres – Except as otherwise indicated, gross acres for all development areas are measured to the center line of streets.

- Grading – Development within the project site shall utilize grading techniques as approved by the City of Ontario. Grading concepts shall respond to the design guidelines included in the Colony Commerce Center West Specific Plan.

- Building Modification – Building additions and/or alterations permitted by the Colony Commerce Center West Specific Plan shall match the architectural style of the primary unit and shall be constructed of the same materials, details, and colors as the primary unit.

- Utilities – All new and existing public utility distribution lines of 34.5 kV or less shall be subsurface throughout the project.

- Technology – All businesses shall accommodate modern telecommunications as defined by the Fiber Optic Master Plan and in accordance with the City of Ontario Structured Wiring Standards (Ontario Municipal Code, Title 8, Chapter 16).

- Solid Waste/Recycling – Development within the project shall comply with City of Ontario requirements for the provision and placement of solid waste and recycling receptacles.

- Traffic – All traffic-controlled signs, whether on public or private property, shall conform to the California MUTCD.

6.6 Industrial Development Standards

This section includes the development of industrial uses. The development standards for industrial uses establish the minimum criteria for the development of land use types on individual lots within the Planning Areas specified within the Colony Commerce Center West Specific Plan. Specific standards for the industrial land uses are described on Table 6.1. Refer to the Colony Commerce Center West Specific Plan EIR and ONT ALUCP for additional development criteria and policies that may affect but not be limited to the restriction of allowable land uses, the allowable Floor Area Ratio (FAR), overall site design, building heights and so on.

6.7 Permitted Uses

Table 6.2 establishes the uses which are permitted within the two planning areas of Colony Commerce Center West Specific Plan. The following symbols used in the table represent the following:

P	Permitted Use
C	Conditional Use Permit required
A	Ancillary Use (allowed in conjunction with another permitted use)

Accessory uses will be reviewed concurrently with each land use proposal.

Table 6.1, Development Standards

SITE REQUIREMENTS	
Minimum Site Area:	1 Acre (43,560 SF)
Floor Area Ratio:	0.55 (Max. Allowed)
Minimum Landscape Coverage:	10%
BUILDING REQUIREMENTS	
Minimum Building Setbacks: (1,2)	
• From Merrill Avenue	23’
• From Carpenter Avenue	10’
• From Remington Avenue	10’
• Interior\Rear Property Lines	10’
• From Cucamonga Creek Channel	10’
Parking & Drive Aisle Setbacks:	
• From Merrill Avenue	23’
• From Carpenter Avenue	10’
• From Remington Avenue	10’
• From Cucamonga Creek Channel	5’
• Interior\Rear Property Lines	5’
• Adjacent to Building Office Elements	10’
• Adjacent to Solid Building Wall	5’
• Private Street and/or Drive Aisle to Building	5’
Maximum Building Height:	
• Main Structure	55’
• Architectural Projections and Focal Elements Such As Towers, Cupolas, and other Appurtenances. (3)	65’
Walls, Fences, and Hedges	Please see Ontario Development Code.

(1) - All setback areas shall be landscaped.

(2) - All setbacks are measured to habitable area not architectural appurtenance or projection. An architectural projection is defined as an element that articulates the building elevation such as eaves, window and door popout surrounds, bay windows, pot shelves, chimneys, enhanced window sills, shutter details, window trim, balconies, pedestrian colonnades and other similar elements. Such elements may project a maximum of 3 feet into required setback areas.

(3) - Architectural element only not to be used for signage, subject to Planning Director approval.

(4) - General Note, refer to the Colony Commerce Center West Specific Plan EIR and ONT ALUCP for additional development criteria and policies that may affect building heights, allowable FAR, and allowable land uses.

Table 6.2, Permitted Uses

Land Use Types	Industrial Area
AGRICULTURAL USES	
Commercial Growing Establishment - Activities typically include, but not are not limited to the commercial growing of produce by row, field, tree, and crop production. Also included is agricultural research.	P
Wholesale and Retail Plant Nurseries - Activities typically include, but are not limited to, sales of indoor and outdoor plants, including, but not limited to, trees, shrubs, groundcovers, and grass sod, as well as seeds, pots and potting supplies, and growing supplies.	P
RESIDENTIAL USES	
Caretaker's Unit - Area devoted to use not to exceed 1,000 square feet.	A/C
COMMERCIAL USES	
Alcohol Beverage Sales - Activities typically include the sale, subject to required license for the sale of alcoholic beverages.	C
Auto Repair (Minor) - Activities include, but are not limited to automotive and light truck repair; retail sales of goods and services for automobiles and light trucks; and the cleaning and washing of automobiles and light trucks. Uses typically include, but are not limited to, repair of brakes, tires, electrical, etc. and car washes.	P
Auto Repair (Major) - In addition to the types of repair operations included as part of Automobile and Light Truck Repair - Major, activities typically include, but are not limited to, automotive body work, painting, and installation of major accessories; automobile customizing; engine and transmission repair/rebuild and towing facilities.	C
Car Wash - Full service activities typically include the washing and polishing of automobiles. Uses typically include automobile laundries; car washes, excluding self-service washes.	A
COMMUNICATION USES	
Radio and Television Broadcasting Studios. Activities typically include, but are not limited to, broadcasting and other information relay services accomplished primarily through the use of electronic and telephonic mechanisms. Uses typically include, but are not limited to, television and radio studios.	P
EATING AND DRINKING PLACES & FOOD SERVICES	
Eating Establishments - Activities typically include, but are not limited to, the retail sale from the premises of food or beverages prepared for on-premises consumption. Uses typically include, but are not limited to:	
• Full-service restaurants, serving ready-to-eat food and beverages for on-site consumption.	P
• Fast-food restaurants, serving ready-to-eat food and beverages for on-site or off-site consumption, without drive-through facilities.	P

Land Use Types	Industrial Area
MANUFACTURING	
Light Manufacturing - Activities typically include, but are not limited to, the mechanical or chemical transformation of raw or semi-finished materials or substances into new products, including manufacture of products, assembly of component parts (including required packaging for retail sale), and treatment and fabrication operation. Light manufacturing activities do not produce odors, noise, vibration, or particulates which would adversely affect uses within the same structure or on the same site. Activities include the following:	
• Apparel Manufacturing	P
• Computer and Home Electronic Manufacturing	P
• Bakery (Industrial)	P
• Electrical Components	P
• Furniture and Related Products Manufacturing	P
• Home Appliance and Equipment Manufacturing	P
• Instrument Manufacturing (Navigational, Measuring, etc.)	P
• Leather Product Manufacturing (excluding tanning and finishing)	P
MACHINERY MANUFACTURING	
Machinery Manufacturing - Activities typically include, but are not limited to, the mechanical or chemical transformation of raw or semi-finished materials or substances into new products, including manufacture of products; assembly of component parts (including required packaging for retail sale); blending of materials such as lubricating oils, plastics, and resins; and treatment and fabrication operations. Examples of activities include the following:	
• Miscellaneous Manufacturing (jewelry, office supplies, sporting goods, toys, etc.)	P
• Printing and Related Activities	P
WAREHOUSE/STORAGE & TRANSPORTATION	
Warehouse/Distribution Facility - Activities typically include, but are not limited to, warehousing, storage, freight handling, shipping, trucking services; storage	P
OTHER	
Trailers for the use of construction	P
Any use deemed similar by the Planning Director	P

General Note, refer to the Colony Commerce Center West Specific Plan EIR and ONT ALUCP for additional development criteria and policies that may affect building heights, allowable FAR, and allowable land uses.

6.8 Signage

All signage within the boundaries of the Colony Commerce Center West Specific Plan shall conform to the Article 31, *Signs*, of Chapter 1 of the City's Development Code.

6.9 Lighting

The design of lighting fixtures shall be approved by the City as part of the City's Development Plan Review.

6.10 Required Number of Parking and Loading Spaces

Off-street parking facilities are to be provided for each use on Table 6.3 Parking and Loading Requirements in this Specific Plan.

Table 6.3, Parking and Loading Requirements

REQUIREMENTS	
Manufacturing:	
• Manufacturing (assumes 10% max. GFA for office)	1.85 space per 1,000 SF of GFA; plus 1 tractor trailer space per 4 dock high doors
Warehousing/Storage & Transportation Services:	
• Warehouse / Distribution Facility (assumes 10% max. GFA for office)	1 space per 1,000 SF of GFA for the first 20,000 SF; 1 space per 2,000 SF of GFA for that portion over 20,001 SF; plus 1 tractor-trailer space per 4 dock-high doors
General Industrial:	
• Speculative buildings (assumes 10% max. GFA for office)	1.85 space per 1,000 SF of GFA for the first 50,000 SF; 1 space per 1,000 SF of GFA for that portion between 50,001 SF and 100,000 SF; 0.5 space per 1,000 SF of GFA for that portion over 100,000 SF; plus 1 tractor-trailer space per 4 dock-high doors
Office:	
Office Area	1 space per 250 SF of GFA

7 DESIGN GUIDELINES

7.1 Purpose and Intent

The following Design Guidelines have been developed to ensure a quality, cohesive design structure for the Colony Commerce Center West development. Objectives of these design guidelines are:

» To provide the City with the necessary assurances that the Specific Plan area will develop in accordance with the design quality and character proposed herein;

» To serve as design criteria for developers, builders, engineers, architects, landscape architects and other professionals in preparing plans for construction; and

» To lend guidance to City staff, Planning Commission and City Council in the review and evaluation of future development projects in the Specific Plan area.

Certain key design elements will contribute significantly to the visual order and consistency of the entire Specific Plan area and provide a quality development. The fundamental elements of these common features; site planning, architecture, landscape, and architecture design details are established by these Design Guidelines.

The design guidelines are intended to be flexible and illustrative in nature, with the capability of responding to unanticipated conditions, changes in buyer preferences, the market and design trends.

Photo 7.1 - Example of Industrial building with corner office area

Creativity and innovation, as well as consistent, and quality, are encouraged in the implementation of these guidelines.

7.2 Industrial Theme and Character

These Design Guidelines will ensure that the Specific Plan community is an environment that reflects the vision embodied in the following concepts:

» Develop a quality, cohesive design concept and identity for the Colony Commerce Center West area.

» Establish development standards that ensure lasting value for the industrial developments.

» The architectural image of the Specific Plan will be perceived primarily from the public realm. Therefore, building massing, scale and roof forms, as the primary design components, require articulation in their architectural expression as they relate to the public realm.

» A theme wall/entry monument may be installed at the major project entries at the discretion of the builder or project developer.

7.3 Site Design

The following concepts are intended to facilitate design quality and compatibility between industrial uses within the Colony Commerce Center West Specific Plan.

» Site design should facilitate the intended functions of developed and open space areas, and provide for appropriate interactions between buildings and activity areas, good movement, vehicular access and parking, and pedestrian and bicycle travel.

» Buildings should be oriented to define the streetscene and provide for an aesthetically pleasing streetscape.

» Major vehicular and pedestrian entries to the site from the public street system should be readily visible. Major entries to planning areas, other than

Photo 7.2 - Example of Industrial building

Photo 7.3 - Example of Industrial building

truck entries should be marked by accent pavement with accent trees and other landscape features.

» Typical ground-mounted equipment (such as transformers and heating units) should be screened by landscaping where they would otherwise be within public view.

» Where long, linear walls or fences are needed, a combination of wall/fence with dense landscaping is encouraged.

» The mass of new structures, as visible from public views, should be softened by landscaping or lessened by small-scale elements such as windows, panels, entrances, and other detail features to avoid monotony in design.

» Parking spaces adjacent to planters shall have a 12" wide curb for ease in stepping out from vehicles.

» Provide parking lot trees in planter islands at the ratio of one tree for every 10 parking spaces.

7.4 Parking/Loading Facilities

The following concepts are intended to facilitate design quality and compatibility between industrial uses within the Colony Commerce Center West Specific Plan.

» Site entries shall compliment the architectural development by utilizing enhanced pavement treatment in vehicular areas, accent trees, and color planting. Enhanced paving shall extend from the back of the approach apron, into the site, to the first interesecting drive aisle or parking space.

» No required parking or loading facilities shall be located in any required landscape setback.

» All outdoor refuse collection areas shall be decorative and should be visually screened.

» All loading areas shall be screened from public view by buildings or by eight foot high wall (minimum). A line of sight study will determine the final height of the wall. Landscaping should be incorporated to visually soften the appearance of walls.

» Driveways and parking areas should be separated from adjacent sidewalks or landscaped areas by a curb not less than six inches high.

» Development shall provide trees within the vehicular use areas at a ratio of one tree for every 10 parking stalls. The trees shall consist of 24" and 36" box sized trees. See Section 7.7.1 for percentages of tree sizes.

7.5 Walls and Fences

» Walls at loading areas shall be at least six feet in height, or as approved by the City in response to screening loading activities from off-site views from the adjacent public right-of-way.

» Chain link fencing shall be permitted for use in interior truck courts, in non-public viewing areas. Chain link fencing may not be used along public views.

» Walls fronting on streets may be constructed of concrete tilt up or masonry materials such as split face or slump stone.

» Tubular Steel fencing shall be permitted along the Cucamonga Creek Channel if areas are not required to be screened from public views.

7.6 Site Lighting

The following section addresses illumination of on-site areas for purposes of safety, security, and nighttime ambience, including lighting for parking areas, pedestrian walkways, graphics and signage, architectural and landscape features, shipping and loading areas, and any additional exterior areas.

Streetlights shall conform, both in type and location, to the Standards of the City of Ontario at the time of installation.

» A comprehensive lighting plan shall be prepared and approved in conjunction with the site plans submitted for approval to the DAB. In addition, all plans shall be reviewed and approved by the Ontario Police Department.

Photo 7.4 - Example of typical screen wall with landscaping

» Exterior lighting should be located and designed to minimize direct glare beyond the parking lot.

» The design of lighting fixtures shall be consistent throughout individual planning areas, and shall be compatible with the architectural style of the building within each development.

» Lighting sources shall be shielded, or diffused in order to avoid glare to pedestrians and motorists. Lighting fixtures should be selected and located to confine the area of illumination to within the site boundaries.

» Architectural lighting of building facades is encouraged to enhance and emphasize the buildings identity.

7.7 Landscape

This section describes the minimum landscape requirements that shall be followed in the design of all public and private improvements within the Specific Plan. Landscaping shall promote the aesthetic character and value of the Colony Commerce Center West Specific Plan area.

7.7.1 General Provisions

» The landscape design shall meet the requirements of the City of Ontario Landscape Development Standards.

» The landscape design shall incorporate a mix of container size trees and shall comply with the following minimum percentages: 5% of trees shall be 48" box size. 10% shall be 36" box size. 30% of trees shall be 24" box size.

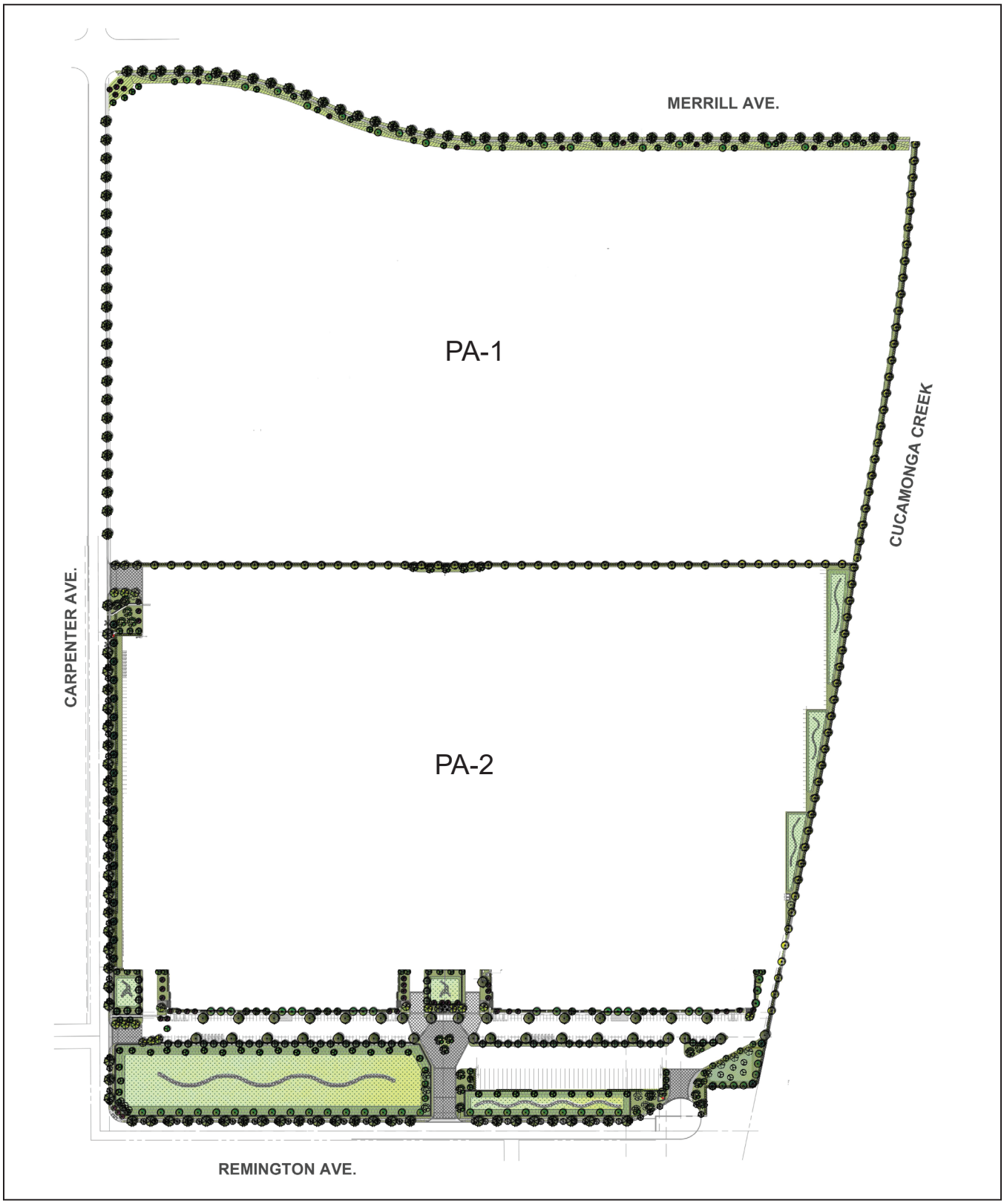
» The use of drought tolerant plants is strongly encouraged.

» Plants shall be grouped into designated 'hydrozones' with similar irrigation requirements.

» All detention basins shall receive container plants and a hydroseed application of low water using plants that can also tolerate seasonal water inundation.

Photo 7.5 - Example of Industrial lighting at office entry

Exhibit 7.1, Conceptual Landscape Master Plan



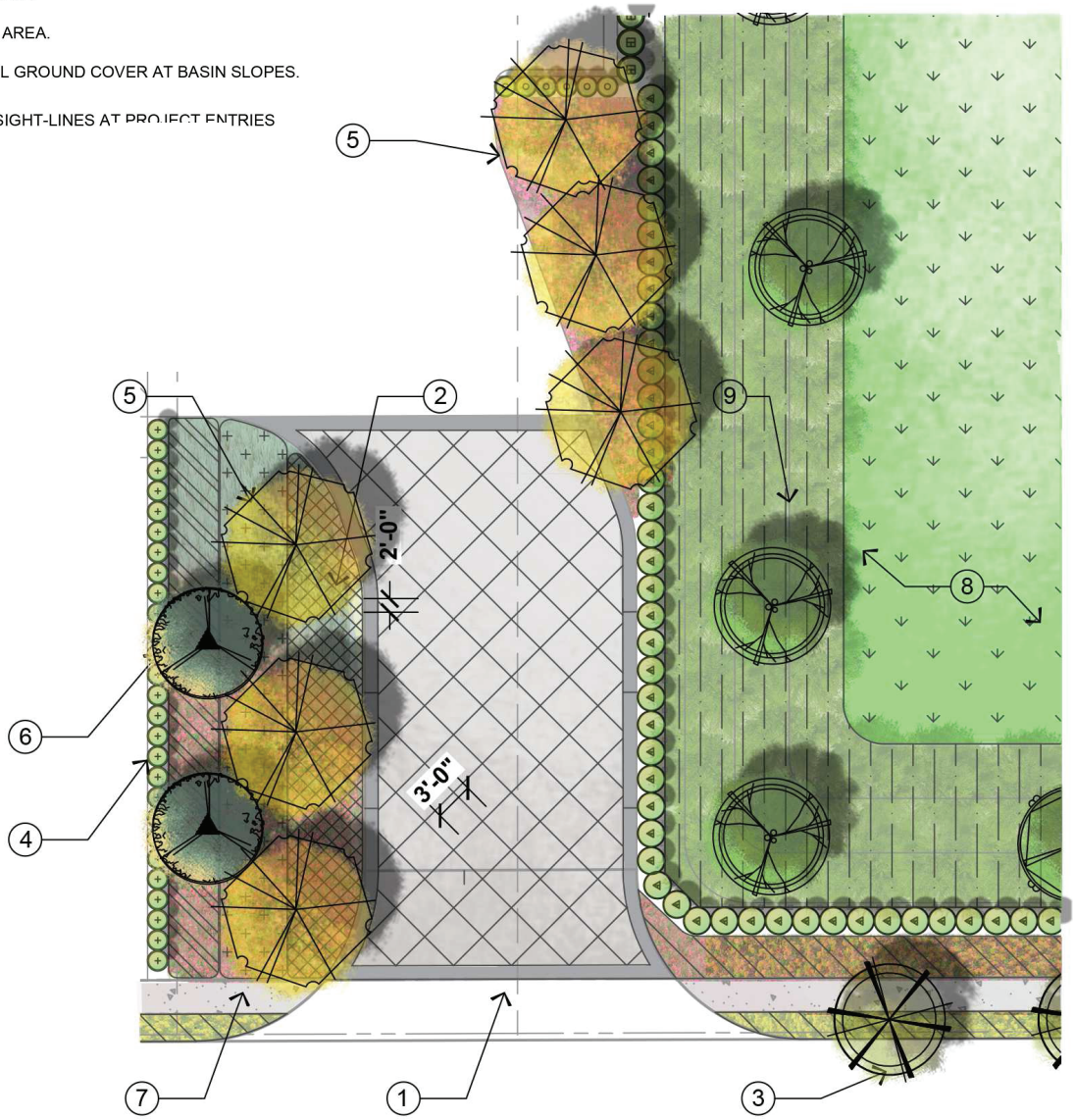
Source: Scott Peterson Landscape Architect

N. T. S.

Exhibit 7.2, Example Project Entry Drives

1. ENHANCED VEHICULAR PAVING AT PROJECT ENTRY DRIVES CONSISTING OF 24" WD. PERIMETER BAND (NATURAL GRAY COLOR) WITH COLORED CONC. INFIELD (36" SQ. GRID PATTERN AT 45° ANGLE).
2. LOW WATER USE LANDSCAPE AREA ADJACENT TO DRIVEWAY ENTRIES. TIER/LAYER DROUGHT TOLERANT GROUND COVER AND SHRUB MATERIAL.
3. NEW STREET TREE.
4. PROPOSED DROUGHT TOLERANT SHRUBS.
5. LARGE FLOWERING ACCENT TREES AT PROJECT ENTRIES.
6. EVERGREEN BACKDROP TREES.
7. TYP. CONC. SIDEWALK.
8. DETENTION BASIN AREA.
9. EROSION CONTROL GROUND COVER AT BASIN SLOPES.

NOTE: ALL REQUIRED SIGHT-LINES AT PROJECT ENTRIES SHALL BE OBSERVED.



Source: Scott Peterson Landscape Architect

» Rock riprap material shall be installed where stormwater drain lines connect to infiltration areas or wherever paved area drainage surface flows directly into depressed landscape areas, via curb cuts or other surface conveyances.

» Trees and landscape design for Master Planned streets such as Merrill Avenue shall meet the requirements of the Ontario Ranch Streetscape Master Plan.

» All utility equipment such as backflow units, electrical transformers, fire detector checks, and fire check valves shall be screened with evergreen shrubs and should be painted a dark green color.

» Compacted decomposed granite (DG) material may be incorporated at accent areas such as project entry drives and other focal areas, but limited to a max of 5% of the landscape area. Large accent boulders may be incorporated into DG areas.

» Low water type of plants including California natives and succulents that thrive in the area's micro-climate shall be incorporated.

» Project entry drives and corner intersection areas shall receive an "intensified" landscape treatment consisting of, but not limited to colorful ground cover and shrubs, and flowering accent trees.

» Parking stalls facing public streets shall include a 36" high hedge adjacent to parking area.

» Landscape shall be irrigated with automatic irrigation systems.

» Irrigation systems shall incorporate smart weather-based or moisture sensor irrigation controller(s) for water conservation.

» Design of low flow drip irrigation systems, where appropriate.

» Irrigation backflow units shall be specified in a theft proof lockable protective steel cage enclosures.

Photo 7.6 - Example of various landscape treatments

» Irrigation controllers shall be in a theft proof enclosure or inside the buildings electrical room.

7.7.2 Landscape Standards

» All landscape areas shall have a minimum inside dimension of 5' feet wide.

» All 2:1 slopes and greater shall be installed with permanent rolled erosion control product (RECP netting), typical.

» A layer of mulch top dressing within all landscaped areas shall be provided to retain soil moisture and mitigate soil erosion. Compacted decomposed granite material is an acceptable alternative if Southern California native plants (Coastal Sage Scrub or Chaparral plant communities) are used to a maximum of 5% of the landscape area. Planting plans shall show plant spacing no greater than the maximum mature width.

» All slopes 3:1 or greater shall be stabilized with spreading erosion control ground cover.

» Foundation shrubs shall be incorporated at base of building to minimize scale of building (min. 5 gal. size at 36" max. spacing).

» Project entry drives shall incorporate enhanced vehicular paving, which may consist of colored concrete with a stamped pattern or scoreline grid pattern at 45 degree angle or similar.

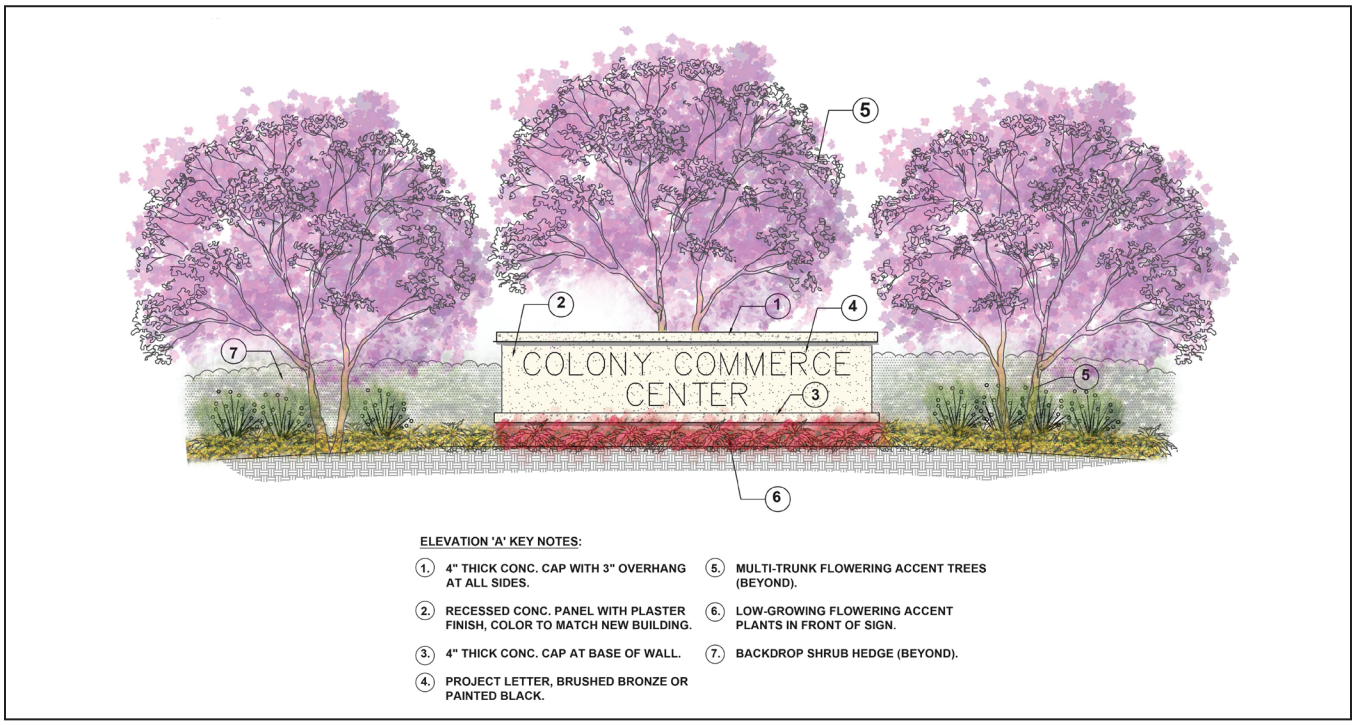
» A 24" clearance from back of parking lot curb to parking lot screen hedge shall be provided for car bumper overhang. Mulch over weed abatement filter fabric shall be provided within this area.

» Chain link fencing shall be permitted for use in interior truck courts, in non-public viewing areas.

» Provide durable perimeter screening trees for shade and windbreaks.

Photo 7.7 - Example of fully landscaped drainage area

Exhibit 7.3, Example Project Entry Monument



Source: Scott Peterson Landscape Architect

» Provide 36" high strappy leaf shrubs to screen utilities such as backflow devices. Use taller evergreen shrubs to screen the sides of transformer units and include maximum 12" high groundcovers in areas to access utilities.

» Landscape shall define and accent entries, pedestrian walkways and architectural features. Landscape shall be attractive and appropriate to define and complement the space and use.

» Entry monuments shall be designed in accordance with City of Ontario Traffic and Transportation Guidelines for monument placement.

» The Landscaping Plan shall comply with City Standard drawings and Traffic and Transportation Guidelines for sight-distance.

» All proposed entry gates shall be reviewed by the Traffic and Transportation Division, and permitted only if approved.

7.7.3 Plant Palette

The Plant Palette on Table 7.1, was selected to complement and enhance the thematic setting for the Ontario community, appropriateness to climatic and soil conditions, ease of maintenance and water conservation.

Table 7.1, Plant Palette

Use	Botanical Name	Common Name
Parking Lot Trees	*Koelreuteria Bipinnata*	Chinese Flame Tree
	Koelreuteria Paniculata	Golden Rain Tree
	Magnolia Grandiflora	Southern Magnolia
	Pistachia Chinensis	Chinese Pistache
	Platanus Acerifolia	London Plane Tree
	Platanus Racemosa	California Sycamore
	Podocarpus Gracilior	Fern Pine
	Quercus Agrifolia	Coast Live Oak
	Quercus Ilex	Holly Oak
	Quercus Engelmanii	Mesa Oak
	Tipuana Tipu	Tipu Tree
	Tristania Conferta	Brisbane Box
	Ulmus Parvifolia	Evergreen Elm
Street Trees (min. 24" box size)	*Quercus Agrifolia*	Coast Live Oak
	Quercus Ilex	Holly Oak
Evergreen Screen Trees	*Eucalyptus species*	Eucalyptus
	Pinus Eldarica	Mondell Pine
	Quercus Agrifolia	Coast Live Oak
	Quercus Ilex	Holly Oak
	Tristania Conferta	Brisbane Box
Trees Adjacent to Buildings	*Callistemon Viminalis*	Weeping Bottlebrush
	Cercis Occidentalis	Western Redbud
	Cupressus Sempervirens	Italian Cypress
	Geijera Parviflora	Australian Willow
	Koelreuteria Bipinnata	Chinese Flame Tree
	Koelreuteria Paniculata	Golden Rain Tree
	Lagerstroemia Indica	Crape Myrtle
	Laurus Nobilis	Sweet Bay Tree
	Melaleuca Quinquinervia	Cajeput tree
	Olea Europaea 'Swan Hill'	Small Fruitless Olive

Use	Botanical Name	Common Name
	Pinus Canariensis	Canary Island Pine
	Pinus Eldarica	Mondell Pine
	Podocarpus Gracilior	Fern Pine
	Podocarpus Macrophyllus	Yew Pine
	Tristania Conferta	Brisbane Box
Tall Shrubs	*Callistemon Viminalis 'Little John'*	Dwarf Bottle Brush
	Cistus Spp.	Rockrose
	Dodonaea Viscosa	Hopseed Bush
	Heteromeles Arbutifolia	Toyon
	Juniperus Chinensis x Pfitzeriana	Pfitzer Juniper
	Lantana Camara	Bush Lantana
	Leptospermum Laevigatum	Australian Tea Tree
	Leucophyllum Candidum	Violet Silverleaf
	Leucophyllum Frutescens	Texas Ranger
	Leucophyllum Laevigatum	Chihahuan Rain Sage
	Leucophyllum Pruinosum	Sierra Bouquet
	Ligustrum Texanum	Texas Privet
	Pittosporum Tobira Variegata	Mock Orange
	Prunus Caroliniana 'Compacta'	Dwarf Cherry Laurel
	Rhamnus Californica	Coffeeberry
	Rhaphiolepis Springtime	Indian Hawthorn
	Rhaphiolepis 'Pink Lady	Indian hawthorn
	Rosa Sp. 'Iceberg Rose'	White Rose
	Rosmarinus O. 'Tuscan Blue'	Bush Rosemary
	Salvia Clevelandii	Chaparral Sage
	Salvia Greggii,	Autumn Sage
	Tecoma Stans	Yellow Trumpet Flower
	Viburnum Japonicum	Viburum
	Westingia Fruticosa	Coast Rosemary
	Xylosma Congestum	Shiny Leaf Xylosma
Low Shrubs / Groundcover	*Acacia Redolens 'Prostrata'*	Prostrate Acacia
	Baccharis x 'Centennial'	Prostrate Desert Broom
	Baccharis Pilularis 'Twin Peaks'	Dwarf Coyote Bush

Use	Botanical Name	Common Name
	Carex Divulsa	Berkley Sedge
	Carex Pansa	California Meadow Sedge
	Carex Praegracilis	Clustered Field Sedge
	Carissa 'Green Carpet'	Prostrate Natal Plum
	Ceanothus Griseus Horizontalis	Caramel Creeper
	Cotoneaster Horizontalis	Rock Contoneaster
	Dalea Gregii	Trailing Indigo Bush
	Dietes Bicolor	Fortnight Lily
	Juniper Horizontalis 'Varieties'	Trailing Juniper Varieties
	Lantana Montevidensis	Trailing Lantana
	Leymus Arenarius	Lyme Grass
	Lomandra Longifolia	Nyalla
	Lonicera Japonica	Hall's Honeysuckle
	Mahonia Repens	Creeping Mahonia
	Muhlenbergia Capllaris	Pink Muhly
	Muhlenbergia Rigens	Deer Grass
	Myoporum Pacificum	Creeping Myoporum
	Pittosporum Tobira 'Wheelers Dwarf'	Wheelers Dwarf Pittosporum
	Rosmarinus Officinalis	Rosemary
	Rosa Floribunda 'Carpet Rose'	Carpet Rose
	Salvia Apiana	White Sage
	Salvia Mellifera	Black Sage
	Senecio Mandraliscae	Senecio
	Trachelospermum Jasminioides	Star Jasmine
	Yucca Aloifolia	Spanish Bayonet
	Yucca Baccata	Banana Yucca
	Yucca Elata	Soaptree Yucca
	Yucca Gloriosa	Spanish Dagger
	Yucca Rigida	Blue Yucca
	Yucca Whipplei	Our Lord's Candle
Palm Trees	*Phoenix Canariensis*	Canary Island Palm
	Phoenix Dactylifera,	Senegal Date Palm
	Washingtonia Filifera	California Fan Palm

7.8 Perimeter Streetscape Design

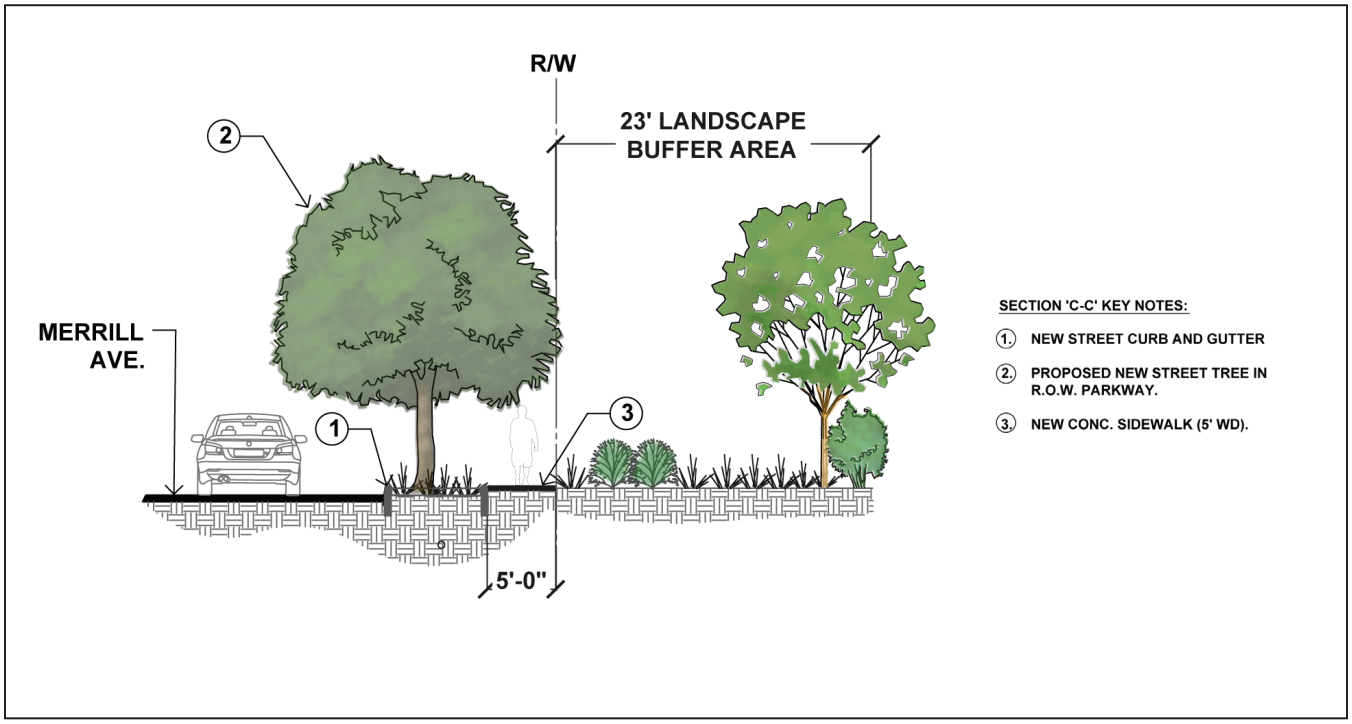
Streetscape design guidelines establish a hierarchy for the landscape development along the surrounding roadways, as well as establish a framework for consistency of design. Three roadways surround the project site as follows:

- Merrill Avenue to the North
- Remington Avenue to the South
- Carpenter Avenue to the West

Merrill Avenue, Carpenter Avenue, and Remington Avenue shall be designed with Low Impact Development Site Design BMP's to retain/infilter or biotreat 85th percentile storm event runoff from newly-widened portions of these streets, per the requirements of the current San Bernardino County Water Quality Management Plan.

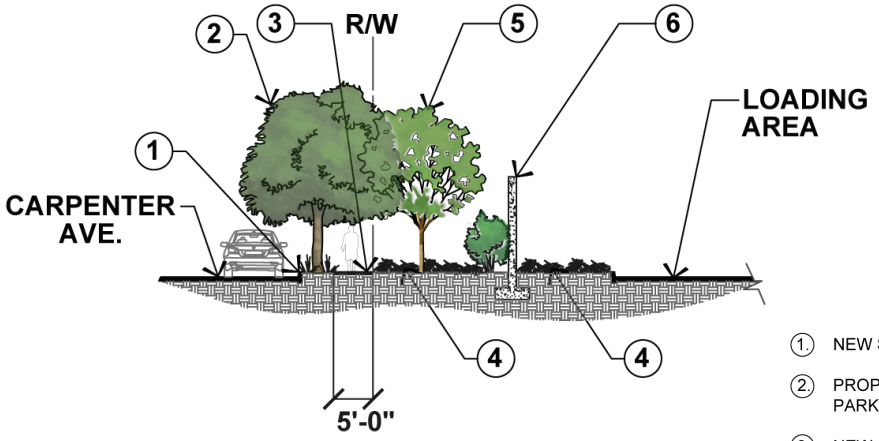
Landscape development surrounding this project will help to set the character, while maintaining consistency with the City of Ontario's pedestrian pathway system as illustrated in the "Trails and Open Space System" section of the Ontario Ranch Streetscape Master Plan. Streetscape sections described below are located on Exhibit 7.4a, 7.4b, and 7.4c, "Typical Landscape Cross Sections."

Exhibit 7.4a, Typical Landscape Cross Section - Merrill Avenue



Source: Scott Peterson Landscape Architect

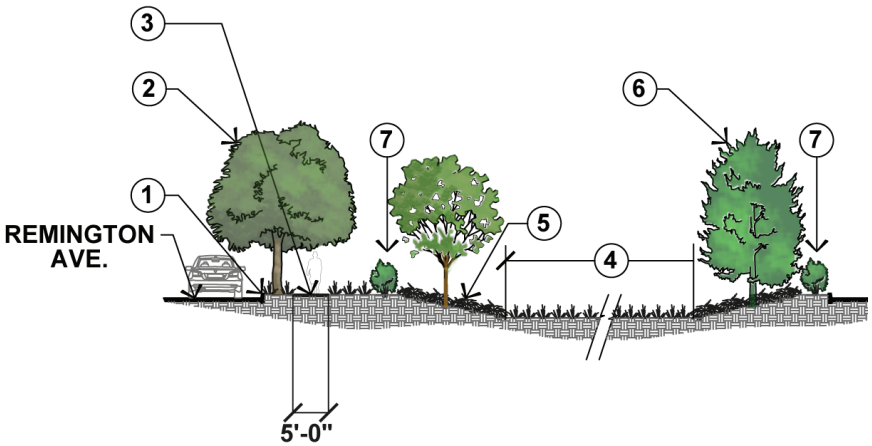
Exhibit 7.4b, Typical Landscape Cross Section - Carpenter Avenue



1. NEW STREET CURB AND GUTTER.
2. PROPOSED NEW STREET TREE IN R.O.W. PARKWAY.
3. NEW CONC. SIDEWALK (5' WD).
4. DROUGHT TOLERANT GROUND COVER.
5. EVERGREEN BACKDROP TREE, 24" BOX SIZE MINIMUM.
6. NEW ARCH. SCREEN WALL PER ARCH. PLANS. (6' ABOVE FINISHED GRADE.) OR AS REQUIRED BY THE CITY.

Source: Scott Peterson Landscape Architect

Exhibit 7.4c, Typical Landscape Cross Section - Remington Avenue



1. NEW STREET CURB AND GUTTER
2. PROPOSED NEW STREET TREE IN R.O.W. PARKWAY.
3. NEW CONC. SIDEWALK (5' WD).
4. INFILTRATION BASIN TO RECEIVE HYDROSEED APPLICATION.
5. EROSION CONTROL GROUND COVER (CONTAINER PLANS) AT BASIN SLOPES.
6. EVERGREEN BACKDROP TREE. 24" BOX SIZE MINIMUM.
7. TYP. SHRUB BARRIER.

Source: Scott Peterson Landscape Architect

7.9 Sustainable Design Strategies

Sustainable practices can lessen the environmental impacts of development in many ways through the use of certain design techniques. These techniques can include reduced pervious surfaces, improved water detention and conservation, preservation of habitat areas, water-efficient irrigation, and improved pedestrian and bicycle amenities which reduce reliance on smog-generating vehicles. This Specific Plan encourages the implementation of sustainable design strategies referenced below and in Appendix B1, with the goal to reduce Greenhouse Gas Emissions.

7.9.1 Site Planning

» Incorporate "green" practices in developing buildings and infrastructure.

» Wherever possible, design and grade the project to direct 2-year storm event runoff from building roofs and paved areas, into swaled landscape areas for capture and retention/infiltration. In particular, open space, parks, landscaped setback areas and trails are to be used for this purpose. Include deciduous trees to shade paved areas and building walls on south and west.

» Stabilize slopes to limit erosion as part of the Stormwater Management Plan and erosion control plan.

7.9.2 Energy Efficiency

Where feasible and appropriate, the following energy conservation strategies are encouraged:

» Passive design strategies can dramatically affect building energy performance. These measures include building shape and orientation, passive solar design, and the use of natural lighting.

» Develop strategies to provide natural lighting to reduce reliance on artificial lighting.

» Install high-efficiency lighting systems with advanced lighting controls.

» Use a properly sized and energy-efficient heat/cooling system in conjunction with a thermally efficient building shell.

» Promote the use of light colored roofing with a high solar reflectance in order to reduce the heat island effect from roofs.

» Include deciduous trees to shade paved areas and building walls on the south and west sides.

7.9.3 Materials Efficiency

» Sustainable construction materials and products are encouraged to have characteristics such as reused and recycled content, zero or low off gassing of harmful air emissions, zero or low toxicity, sustainably harvested materials, high recyclability, durability, longevity, and local production. Such products promote resource conservation and efficiency. Using recycled-content products also helps develop markets for recycled materials that are being diverted from California's landfills, as mandated by the Integrated Waste Management Act.

» Encourage the use of low VOC paints and wallpapers.

» Encourage the use of low VOC Green Label carpet.

» Encourage the use of dimensional planning and other material efficiency strategies. These strategies reduce the amount of building materials needed and cut construction costs. Consider designing rooms on four foot multiples to conform to standard-sized wallboard and plywood sheets.

» Consider using recycle base, crushed concrete base, recycle content asphalt, shredded tires in base and asphalt in roads, parking areas and drive aisles, if feasible and economically viable.

» Design with adequate space to facilitate recycling collection and to incorporate a solid waste management program that prevents waste generation.

» Encourage the use of building materials or products that have been extracted, harvested or recovered, as well as manufactured, within 500 miles of the project.

» Encourage the use of rapidly renewable building materials and products (made from plants that are typically harvested within a ten-year cycle or shorter). Examples of materials that could achieve this goal include, but are not limited to, bamboo, wool, cotton insulation, agrifiber, linoleum, wheatboard, strawboard and cork.

7.9.4 Water Efficiency

» Strive to minimize wastewater by using ultra low-flush toilets, low-flow shower heads and other water conserving fixtures.

» Encourage the use of recirculating systems for centralized hot water distribution.

» Smart irrigation controller which automatically adjusts the frequency and/or duration of irrigation events in response to changing weather conditions for all landscaped areas are required.

» Drip irrigation, bubblers, micro-irrigation or other low precipation irrigation or water conserving technology shall supply water for irrigitaion.

» Encourage the use of recycled water to irrigate landscape areas throughout the project. The non-potable irrigation system shall be designed to meet all applicable standards of the California Regional Water Quality Control Board, California Department of Health, San Bernardino County Health Department, City of Ontario Department of Water and Power, and Ontario Municipal Code.

7.9.5 Occupant Health and Safety

» Choose construction materials and interior finish products with zero or low emissions to improve indoor air quality as feasible.

» Provide adequate ventilation and a high-efficiency, in-duct filtration system. Heating and cooling systems that ensure adequate ventilation and proper filtration can have a dramatic and positive impact on indoor air quality.

» Provide effective drainage from the roof and surrounding landscape.

» Encourage building systems to control humidity.

» Provide one outdoor employee break area per building with shade structure or shade trees on the west and south sides as feasible.

7.9.6 Landscape Design

» Use low or medium water use and native plant materials where appropriate. Minimize turf areas in order to promote water conservation. Limit the use of turf to areas which experience high functional use and are needed to accommodate outdoor activities. Only use warm-season turf varieties which are suited to the climate.

» Provide plant materials that are well suited to the solar orientation and shading of buildings.

» Group plants according to water use, slope aspect and sun/shade requirements. Irrigate each hydrozone on a separate valve using high-efficiency irrigation techniques.

» Use organic wood or shredded bark mulch and soil amendments to retain soil moisture.

» Incorporate native vegetation into the plant palette for Colony Commerce Center West.

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8 IMPLEMENTATION

The Colony Commerce Center West Specific Plan serves to implement the City's Policy Plan policies applicable to the project site and provide for orderly development of the project site. Tentative tract maps and parcel maps, once approved, shall establish the legal lots, public dedications, and easements within for the project.

8.1 Methods and Interpretation

Development within the Colony Commerce Center West Specific Plan shall be implemented through the City approval of tentative and final tract maps and parcel maps and through the Development Plan Review process as established in the City of Ontario Development Code.

The implementation process described herein provides the mechanisms for review and approval of development projects within the Colony Commerce Center West.

8.2 Applicability

All development proposals within the project shall be subject to the implementation procedures established herein. Whenever the provisions and development standards contained herein conflict with those contained in the City of Ontario Development Code, the provisions of the Specific Plan shall take precedence.

In instances where the Specific Plan is silent, the City of Ontario Development Code shall prevail.

8.3 Interpretation

Unless otherwise provided, any ambiguity concerning the content or application of the Specific Plan shall be resolved by the City of Ontario Planning Director, or his/her designee, in a manner consistent with the goals, policies, purpose and intent established in this Specific Plan.

8.4 Implementation of Design Guidelines

Adoption of the Specific Plan by the City includes adoption of the design guidelines contained herein, which shall be the design criteria by which development within the project shall be reviewed during Development Plan Review. The design guidelines are intended to be flexible in nature while establishing basic evaluation criteria for the review of development projects as part of Development Plan Review.

8.5 Development Review Process

8.5.1 Subdivision Maps

Approval of tentative subdivision maps may occur concurrently with the adoption of the Specific Plan. All tentative and final subdivision maps shall be reviewed and approved pursuant to applicable provisions of the City of Ontario Subdivision Ordinance and consistent with the applicable provisions of the Land Use, Infrastructure, Design Guidelines, and Development Regulations adopted as part of this Specific Plan.

8.5.2 Development Plan

All development projects within the confines of the Colony Commerce Center West Specific Plan shall be subject to the Development Plan Review process as established in Article 8 of the City's Development Code. Pursuant to these provisions, Development Plan Review constitutes a design review of project architecture, site plans, and landscape plans.

Adoption of the Specific Plan by the City includes adoption of the design guidelines contained within the Specific Plan which provide direction for the design of development projects within Colony Commerce Center West. Where the Specific Plan development regulations and design guidelines are silent, the applicable development regulations and design guidelines contained within the City's Development Code shall apply.

The design guidelines are intended to be flexible in nature while establishing basic evaluation criteria for the review of development projects by the City.

8.5.3 Development Agreement

Approval of statutory Development Agreements, per individual property owner, authorized pursuant to California Government Code Sections 65864 et seq., is required as part of the approval of the Specific Plan and prior to approval of the first Final Map.

The Development Agreements shall include, but not be limited to, methods for financing, acquisition, and construction of infrastructure. The Colony Commerce Center West Development Agreement shall be fully executed prior to the issuance of the first building permits for the project.

8.6 Specific Plan Modifications and Amendments

8.6.1 Minor Modifications

The following constitute minor modifications to the Specific Plan, and do not require a Specific Plan Amendment and are subject to review and approval by the Planning Director. The Planning Director shall have the discretion to refer any such request for modification to the Planning Commission or the City Council.

- Change in utility and/or public service provider.
- Collector roadway alignment when the change results in a center line shift of less than 250 feet.
- An increase of up to ten percent (10%) in square footage of floor area subject to approval of the Planning Director and agreement of the property owner, provided the total square footage of floor area number for the entire Specific Plan area does not exceed that established by this Specific Plan.
- Adjustment of a Planning Area boundary or acreage designated for a Planning Area provided the total acreage of the affected planning area does not increase by more than ten percent (10%).
- Minor changes to landscape materials, wall materials, wall alignment, entry design, and streetscape design which are consistent with the conceptual design set forth in the design guidelines contained within the Specific Plan.
- Minor changes to the design guidelines, which are intended to be conceptual in nature, and are intended to be flexible in implementation.
- Minor changes of up to ten percent (10%) of any quantifiable development standard or design guideline subject to approval of the Planning Director.
- Other modifications of a similar nature to those listed above, which are deemed minor by the Planning Director, which are in keeping with the purpose and intent of the approved Specific Plan and which are in conformance with the Policy Plan.

8.6.2 Specific Plan Amendments

Amendments to the Specific Plan may be requested by the applicant or the City pursuant to Section 65453(a) of the Government Code. Amendments shall be processed pursuant to the provisions of the Government Code for Specific Plan Amendments.

In the event that the proposed amendment requires supplemental environmental analysis pursuant to the California Environmental Quality Act (CEQA), the applicant(s) is/are responsible for preparing the necessary CEQA documentation.

8.7 Variances

Variances and Administrative Exceptions to the development regulations contained in the Specific Plan with respect to landscaping, screening, site area, site dimensions, yards and projects into yards, heights of structures, distances between buildings, open space and off-street parking and loading shall be reviewed pursuant to Article 10, "Variances and Administrative Exceptions" of the City of Ontario Development Code.

8.8 Conditional Use Permits

Uses specified as conditionally permitted uses within Table 6.3 of Chapter 6, "Development Regulations," of the Specific Plan shall be reviewed and approved by the City pursuant to the requirements of Article 9, "Conditional Use Permits" of the Ontario Development Code.

8.9 Compliance with Mitigation Monitoring Plan

Certification of an Environmental Impact Report (EIR) shall be required prior to approval of the Specific Plan. Development within the project site shall comply with all approved mitigation measures as described in the Mitigation Monitoring Program included as part of the EIR.

8.10 Project Phasing

Phasing of development within the Specific Plan shall meet the following objectives:

- Orderly build-out of the project based upon market and economic conditions.
- Provision of adequate infrastructure and public facilities as determined and deemed necessary by the City concurrent with development of each phase.
- Protection of public health, safety and welfare.

8.11 Infrastructure Phasing

Backbone infrastructure within the Colony Commerce Center West shall be installed by the project developer in accordance with this Specific Plan and the approved project Development Agreement or approved by the City.

Grading and installation of infrastructure to serve the Colony Commerce Center West is anticipated to be completed in two phases – Planning Area 2 (Phase 1) and Planning Area 1 (Phase 2). These phases may be developed as subphases (e.g., Phases 1A and 1B) and may occur either sequentially or concurrently with one another.

8.12 Appeals

Appeals from any determination of the City Planning Director, Zoning Administrator or the Planning Commission, may be made by the applicant or any other aggrieved party by filing an application on forms provided by the City of Ontario and accompanied by the appropriate filing fee within ten (10) days following the final date of action for which an appeal is made. Appeals shall be processed consistent with the provisions of Article 5, "Appeals" of the City of Ontario Development Code.

8.13 Project Financing

The financing of construction, operation, and maintenance of public improvements and facilities (the "facilities"), and public services shall include funding through a combination of financing mechanisms. Final determination as to the facilities to be constructed and as to maintenance responsibilities, whether publicly or privately maintained, shall be made prior to recordation of final maps.

In order to implement the project, financing options including, but not limited to, the following shall be considered:

8.13.1 Facilities and Services

- Private capital investment for the construction of facilities.

- Community Facilities District (CFD) established pursuant to the Mello-Roos Community Facilities District Act of 1982, or other special district, to provide funding for the construction of a variety of public facilities and the provision of public services.

8.13.2 Operation and Maintenance

- By individual private property owner.

- By private Property Owners Association.

- By Community Facilities District (CFD) established pursuant to the Mello-Roos Community Facilities District Act of 1982, or other special district. City Council approval is a prerequisite for the implementation of any and all special district-financing mechanisms. The use of the Mello-Roos Community Facilities District Act of 1982 (the "Act") to finance public facilities and services shall be at the City's sole discretion. Moreover, the use of the Act shall be consistent with the City's adopted goals and policies concerning the use of the Act.

8.14 Maintenance Plan

The public and private improvements constructed within the Colony Commerce Center West shall be maintained through a combination of public and private entities as described below and in Table 8.1, "Maintenance Responsibilities."

8.14.1 Public Maintenance

- All Master Plan streets, and sidewalks serving the industrial Planning Areas shall be dedicated as public streets to the City of Ontario.

- Landscape improvements within the public right-of-way of Master Plan streets and public street lights within the Colony Commerce Center West shall be maintained through a landscape and lighting district or other community financed district established by the City.

- All on-site water, sewer, and storm drains within the public streets or easements dedicated to the City shall be constructed by the developer and, upon acceptance, shall be maintained by the City.

- Off-site infrastructure improvements such as water, sewer and storm drain facilities shall be maintained by the City. Permanent on-site water quality basins, trenches, swales and biotreatment filters required by San Bernardino County MS4 Permit and Water Quality Management Plan and constructed within Colony Commerce Center West shall be maintained by the Property Owner's Association in accordance with Table 8.1.

8.14.2 Property Owners Association

A Property Owners Association (POA) shall be established for the maintenance of common area landscape improvements and private roadways within areas of Colony Commerce Center West. Improvements to be maintained by the POA include:

- Designated private drives, alleys, and adjacent landscaping.

- Designated private streets and landscaping. During the course of maintenance of public utilities within public streets, private streets, private drive aisles, or alleys, the City will restore the streets to City standards for trench backfill, pavement repair, and hardscape or landscape, as applicable and to the original quality. Restoration of any enhancements above and beyond City standards, including but not limited to architectural paving, hardscape and landscape enhancements shall be the responsibility of the POA or other entity maintaining those enhancements.

- Courts, parkways and landscaping within the industrial areas.

- Parkways of Interior Local Streets including sidewalks, landscaping and street lights.

- Maintenance of interior local street landscaping and associated architectural monument elements required to restore these areas to their condition as originally installed.

- Internal slopes fronting streets and slope areas.

- All internal open spaces, and common areas.

- NPDES facilities within private streets and/or common areas.

Table 8.1 Maintenance Responsibilities

	City and/ or CFD	Private Owners Association	Utility Entity
Street Lights and Traffic Signals	●		
Master plan roadways (Merrill Avenue)	●		
Interior (collector) public streets - curb-to-curb (primary entry streets, secondary entry streets)	●		
Interior public streets - Behind the curb improvements (sidewalk, parkway, and monument Signs)(3)		●	
Parkways and neighborhood edges of master plan roadways		●	
Off-site and on-site public water, sewer, and storm drain improvements (excluding laterals)(1)	●		
Community Trail (Cucamonga Creek)	●		
Front yard and all on-site landscaping and irrigation		●	
Private interior yard walls		●	
Project theme wall or fence (outside face for graffiti removal and paint)		●	
Interior project graffiti removal		●	
Neighborhood edge on all non-master plan roadways(3)		●	
Parkways of all interior project streets (including landscaping, medians, and sidewalks)(2)(3)		●	
Monument signs within tract entry		●	
Electricity and natural gas			●
Communications systems			●
Police and Fire	●		
Fiber optic conduit (in streets)	●		
Fiber optic conduit (in-tract streets)	●		
NPDES facilities on private property		●	

(1) Only those facilities in public roads or easements
(2) Only those facilities on private property
(3) Outside public right-of-way

A1 APPENDIX GENERAL PLAN CONSISTENCY

A1.1 General Plan Consistency

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits the adoption and administration of specific plans as an implementation tool for elements contained in the local general plan. Policy plans must demonstrate consistency in regulations, guidelines, and programs with the goals and policies set forth in the general plan.

The Colony Commerce Center West Specific Plan has been prepared in conformance with the goals and policies of the City of Ontario Policy Plan. The policy analysis listed in this Appendix describes the manner in which the Colony Commerce Center West Specific Plan complies with the Policy Plan policies applicable to the project.

Plan Policy	Specific Plan Consistency
Land Use (LU) Element	
Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.	
Policy LU1-1: Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.	***Consistent.*** *The Colony Commerce Center West site location falls within planned infrastructure improvements designated by the City of Ontario.*
Policy LU1-2: Sustainable Community Strategy. We integrate state, regional and local Sustainable Community/ Smart Growth principles into the development and entitlement process.	***Consistent.*** *The Colony Commerce Center West design guidelines encourages all new construction to utilize design features, fixtures, appliances, and heating and cooling controls to conserve energy and water. The landscape concept for Colony Commerce Center West incorporates a plant palette of drought tolerant materials and includes requirements that the development implement planting and irrigation systems designed to conserve water.*
Policy LU1-3: Adequate Capacity. We require adequate infrastructure and services for all development.	***Consistent.*** *The Colony Commerce Center West Specific Plan establishes an infrastructure and public facilities plan to ensure that adequate roadways and public utilities including sewer, water, and drainage facilities, along with other public facilities, are provided to serve the project.*
Policy LU1-4: Mobility. We require development and urban design, where appropriate, that reduces reliance on the automobile and capitalizes on multi-modal transportation opportunities.	***Not Applicable.***
Policy LU1-5: Jobs-Housing Balance. We coordinate land use, infrastructure, and transportation planning and analysis with regional, county and other local agencies to further regional and subregional goals for jobs-housing balance.	***Consistent.*** *The industrial uses planned for on the Colony Commerce Center West will have the ability to generate jobs for City of Ontario residents.*
Policy LU1-6: Complete Community. We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario.	***Consistent.*** *The industrial uses planned for on the Colony Commerce Center West will have the ability to generate jobs for City of Ontario residents.*

Plan Policy	Specific Plan Consistency
Policy LU1-7: Revenues and Costs. We require future amendments to our Land Use Plan to be accompanied by analyses of fiscal impacts.	***Not Applicable.***
Goal LU2: Compatibility between a wide range of uses.	
Policy LU2-1: Land Use Decisions. We minimize adverse impacts on adjacent properties when considering land use and zoning requests.	***Consistent.*** *Many of the adjacent properties to the Colony Commerce Center West have transitioned to more industrials uses.*
Policy LU2-2: Buffers. We require new uses to provide mitigation or buffers between existing uses where potential adverse impacts could occur.	***Not Applicable.***
Policy LU2-3: Hazardous Uses. We regulate the development of industrial and similar uses that use, store, produce or transport toxic substances, air emissions, other pollutants or hazardous materials.	***Consistent.*** *The project will comply with all local and state requirements for using, storing, producing, or transporting toxic substances, air emissions, other pollutants, or hazardous materials.*
Policy LU2-4: Regulation of Nuisances. We regulate the location, concentration and operations of potential nuisances.	***Not Applicable.***
Policy LU2-5: Regulation of Uses. We regulate the location, concentration and operations of uses that have impacts on surrounding land uses.	***Consistent.*** *Many of the adjacent properties to the Colony Commerce Center West have transitioned to more industrials uses.*
Policy LU2-6: Infrastructure Compatibility. We require infrastructure to be aesthetically pleasing and in context with the community character.	***Consistent.*** *Streets within and adjacent to the Specific Plan area will be landscaped in an aesthetically pleasing manner with 4-7 foot wide landscaped parkways on each side of the street. Decorative project monuments will be constructed at key project entries providing project identification and establishing a sense of arrival.*
Policy LU2-7: Inter-jurisdictional Coordination. We maintain an ongoing liaison with IEUA, LAWA, Caltrans, Public Utilities Commission, the railroads and other agencies to help minimize impacts and improve the operations and aesthetics of their facilities.	***Not Applicable.***
Policy LU2-8: Transitional Areas. We require development in transitional areas to protect the quality of life of current residents.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy LU2-9: Methane Gas Sites. We require sensitive land uses and new uses on former dairy farms or other methane-producing sites be designed to minimize health risks.	***Consistent.*** *If necessary, the project will comply with appropriate mitigation measures identified in the project EIR for soil remediation and proper venting to address the potential existence of methane gases within the project.*
Goal LU3: Staff, regulations and processes that support and allow flexible response to conditions and circumstances in order to achieve the Vision.	
Policy LU3-1: Development Standards. We maintain clear development standards which allow flexibility to achieve our Vision.	***Consistent.*** *This Specific Plan includes development standards that allow for flexibility to achieve the City's vision.*
Policy LU3-2: Design Incentives. We offer design incentives to help projects achieve the Vision.	***Not Applicable.***
Policy LU3-3: Land Use Flexibility. We consider uses not typically permitted within a land use category if doing so improves livability, reduces vehicular trips, creates community gathering places and activity nodes, and helps create identity.	***Not Applicable.***
Goal LU4: Development that provides short-term value only when the opportunity to achieve our Vision can be preserved.	
Policy LU4-1: Commitment to Vision. We are committed to achieving our Vision but realize that it may take time and several interim steps to get there.	***Not Applicable.***
Policy LU4-2: Interim Development. We allow development in growth areas that is not immediately reflective of our ultimate Vision provided it can be modified or replaced when circumstances are right. We will not allow development that impedes, precludes or compromises our ability to achieve our Vision.	***Not Applicable.***
Policy LU4-3: Infrastructure Timing. We require that the necessary infrastructure and services be in place prior to or concurrently with development.	***Consistent.*** *Approval of the Colony Commerce Center West Specific Plan is accompanied by an application for approval of a development agreement. The development agreement shall include, but not be limited to, methods for financing, acquisition, and construction of infrastructure.*

Plan Policy	Specific Plan Consistency
Goal LU5: Integrated airport systems and facilities that minimize negative impacts to the community and maximize economic benefits.	
Policy LU5-1: Coordination with Airport Authorities. We collaborate with FAA, Caltrans Division of Aeronautics, airport owners, neighboring jurisdictions, and other shareholders in the preparation, update and maintenance of airport-related plans.	***Not Applicable.***
Policy LU5-2: Airport Planning Consistency. We coordinate with airport authorities to ensure The Ontario Plan is consistent with state law, federal regulations and/or adopted master plans and land use compatibility plans for the ONT and Chino Airport.	***Consistent.*** *The Specific Plan will comply with the ALUCP requirements for Ontario Airport (ONT) and Chino Airport as outlined in Colony Commerce Center West Specific Plan Section 3.4.*
Policy LU5-3: Airport Impacts. We work with agencies to maximize resources to mitigate the impacts and hazards related to airport operations.	***Not Applicable.***
Policy LU5-4: ONT Growth Forecast. We support and promote an ONT that accomodates 30 million annual passengers and 1.6 million tons of cargo per year, as long as the impacts associated with that level of operations are planned for and mitigated.	***Not Applicable.***
Policy LU5-5: Airport Compatibility Planning for ONT. We create and maintain the Airport Land Use Compatibility Plan for ONT.	***Not Applicable.***
Policy LU5-6: Alternative Process. We fulfill our responsibilities and comply with state law with regard to the Alternative Process for proper airport land use compatibility planning.	***Not Applicable.***
Policy LU5-7: ALUCP Consistency and Land Use Regulations. We comply with state law that requires general plans, specific plans and all new development be consistent with the policies and criteria set forth within an Airport Land Use Compatibility Plan for any public use airport.	***Consistent.*** *The Specific Plan will comply with the ALUCP requirements for Ontario Airport (ONT) and Chino Airport as outlined in Colony Commerce Center West Specific Plan Section 3.4.*

Plan Policy	Specific Plan Consistency
Policy LU5-8: Chino Airport. We will support the creation and implementation of the Airport Land Use Compatibility Plan for Chino Airport.	***Consistent.*** *The Specific Plan will comply with the ALUCP requirements for Ontario Airport (ONT) and Chino Airport as outlined in Colony Commerce Center West Specific Plan Section 3.4.*

Community Design (CD) Element

Goal CD1:

A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

Policy CD1-1: City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.	***Not Applicable.***
Policy CD1-2: Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.	***Consistent.*** *The Specific Plan includes design guidelines to guide the physical character of all future industrial development and all project related features, including the overall landscape treatment within the project.*
Policy CD1-3: Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.	***Not Applicable.***
Policy CD1-4: Transportation Corridors. We will enhance our major transportation corridors within the City through landscape, hardscape, signage and lighting.	***Not Applicable.***
Policy CD1-5: View Corridors. We require all major north-south streets be designed and redeveloped to feature views of the San Gabriel Mountains, which are part of the City's visual identity and a key to geographic orientation. Such views should be free of visual clutter, including billboards and may be enhanced by framing with trees.	***Consistent.*** *The Specific Plan includes improvements to Carpenter Avenue which is a north-south street and will be designed in accordance to the Master Plan of Streets and Highways.*

Goal CD2:

A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

Plan Policy	Specific Plan Consistency
Policy CD2-1: Quality Architecture. We encourage all development projects to convey visual interest and character through: » Building volume, massing, and height to provide appropriate scale and proportion; » A true industrial style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and » Exterior building materials that are visually interesting, high quality, durable, and appropriate for the industrial style.	***Consistent.*** *The Specific Plan includes design guidelines to guide the construction of the project so that it is implemented in a comprehensive manner.*
Policy CD2-2: Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as: » a pattern of smaller, walkable blocks that promote access, activity and safety; » variable setbacks and parcel sizes to accommodate a diversity of housing types; » traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows; » floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the "outdoor living room"), as appropriate; and » landscaped parkways, with sidewalks separated from the curb.	***Not Applicable.***
Policy CD2-3: Commercial Centers. We desire commercial centers to be distinctive, pedestrian friendly, functional and vibrant with a range of businesses, places to gather, and connectivity to the neighborhoods they serve.	***Not Applicable.***
Policy CD2-4: Mixed Use, Urban Office and Transit Serving Areas. We require mixed use, urban office and transit serving areas to be designed and developed as pedestrian oriented "villages" that promote a vibrant, comfortable and functional environment.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy CD2-5: Streetscapes. We design new and, when necessary, retrofit existing streets to improve walkability, bicycling and transit integration, strengthen connectivity, and enhance community identity through improvements to the public right of way such as sidewalks, street trees, parkways, curbs, street lighting and street furniture.	***Consistent.*** *The Specific Plan is designed with comprehensive street improvements to accommodate the safe and efficient movement of automobiles as well as bicycle and pedestrian mobility and connectivity along the property frontage. The Colony Commerce Center West project will construct the half-width of the appropriate frontage roads as identified in this Specific Plan and the project Development Agreement.*
Policy CD2-6: Connectivity. We promote development of local street patterns and pedestrian networks that create and unify neighborhoods, rather than divide them, and create cohesive and continuous corridors, rather than independent "islands" through the following means (Link to Mobility): » local street patterns that provide access between subdivisions and within neighborhoods and discourage through traffic; » a local street system that is logical and understandable for the user. A grid system is preferred to avoid circuitous and confusing travel paths between internal neighborhood areas and adjacent arterials; and » neighborhoods, centers, public schools, and parks that are linked by pedestrian greenways/open space networks. These may also be used to establish clear boundaries between distinct neighborhoods and/or centers.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy CD2-7: Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.	***Consistent.*** *Sustainable Community/Smart Growth principles are incorporated into the Colony Commerce Center West Land Use Plan. The sustainable goals for the project as stated in the Specific Plan include the following:* » *1. Encourage walking and other non-vehicular modes of travel.* » *2. Provide pedestrian connectivity through the project perimeter.* » *3. Provide shaded outdoor areas for employee break areas.* » *4. Encourage the use of architectural elements designed to reduce interior heat gain.* » *5. Encourage the use of recycled, recyclable, and environmentally friendly building materials.* » *6. Require the use of low energy glass and low water plumbing features.* » *7. Encourage the use of drought tolerant landscaping and water efficient irrigation methods.* *The Colony Commerce Center West design guidelines encourages all new construction to utilize design features, fixtures, and heating and cooling controls to conserve energy and water. The landscape concept for Colony Commerce Center West incorporates a plant palette of drought tolerant materials and requirements that the development implement planting and irrigation systems designed to conserve water.*
Policy CD2-8: Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.	***Not Applicable.***
Policy CD2-9: Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.	***Consistent.*** *The landscape concept for Colony Commerce Center West incorporates the use of durable landscaping materials, a drought tolerant plant palette, and a planting and irrigation system designed to conserve water. Open space areas will include shaded areas, bicycle racks, and other amenity features to encourage pedestrian and other non-vehicular activities. All materials utilized in private and public common areas will be durable landscaping materials.*

Plan Policy	Specific Plan Consistency
Policy CD2-10: Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.	***Not Applicable.***
Policy CD2-11: Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.	***Consistent.*** *Landscaping will be provided at entries within the Colony Commerce Center West. At key entries, a monumentation program may be utilized to help identify the project, as well as convey a sense of arrival and a welcoming feel for both vehicular and pedestrian traffic. These monuments and entries will be designed with durable, lasting materials approved by the City of Ontario.*
Policy CD2-12: Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.	***Consistent.*** *The Specific Plan requires the developer of Colony Commerce Center West to obtain approval by the City of a Sign Program to address project monumentation, building identification and wayfinding/signage within the project.*
Policy CD2-13: Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.	***Not Applicable.***
Policy CD2-14: Availability of Information. We provide easy access to information for developers, builders and the public about design quality, construction quality, and sustainable building practices.	***Not Applicable.***
Policy CD2-15: Leverage Professional and Trade Organizations. We support excellence in design and construction quality through collaboration with trade and professional organizations that provide expertise, resources and programs for developers, builders and the public.	***Not Applicable.***

Goal CD3:

Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

Plan Policy	Specific Plan Consistency
Policy CD3-1: Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.	***Not Applicable.***
Policy CD3-2: Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.	***Not Applicable.***
Policy CD3-3: Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.	***Not Applicable.***
Policy CD3-4: Ground Floor Usage of Commercial Buildings. We create lively pedestrian streetscapes by requiring the location of uses, such as shopping, galleries, restaurants, etc., on ground floors adjacent to sidewalks.	***Not Applicable.***
Policy CD3-5: Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.	***Consistent.*** *The Specific Plan requires that the design and materials used for all road surfaces and sidewalks within the project be subject to approval by the Engineering Department.*
Goal CD4: Historic buildings, streets, landscapes and neighborhoods, as well as the story of Ontario's people, businesses, and social and community organizations, that have been preserved and serve as a focal point for civic pride and identity.	
Policy CD4-1: Cultural Resource Management. We update and maintain an inventory of historic sites and buildings, professional collections, artifacts, manuscripts, photographs, documents, maps and other archives.	***Not Applicable.***
Policy CD4-2: Collaboration with Property Owners and Developers. We educate and collaborate with property owners and developers to implement strategies and best practices that preserve the character of our historic buildings, streetscapes and unique neighborhoods.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy CD4-3: Collaboration with Outside Agencies. We pursue opportunities to team with other agencies, local organizations and non-profits in order to preserve and promote Ontario's heritage.	***Not Applicable.***
Policy CD4-4: Incentives. We use the Mills Act and other federal, state, regional and local programs to assist property owners with the preservation of select properties and structures.	***Not Applicable.***
Policy CD4-5: Adaptive Reuse. We actively promote and support the adaptive reuse of historic sites and buildings to preserve and maintain their viability.	***Not Applicable.***
Policy CD4-6: Promotion of Public Involvement in Preservation. We engage in programs to publicize and promote the City's and the public's involvement in preservation efforts.	***Not Applicable.***
Policy CD4-7: Public Outreach. We provide opportunities for our residents to research and learn about the history of Ontario through the Planning Department, Museum of History and Art, Ontario and the Robert E. Ellingwood Model Colony History Room.	***Not Applicable.***
Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.	
Policy CD5-1: Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.	***Consistent.*** *The Specific Plan includes a Maintenance Responsibility Matrix defining the public, private, and utility entities responsible for maintenance of roadways, parkways, trails, sidewalks, common areas, walls and monuments, traffic signals, infrastructure, and utilities within the project.*
Policy CD5-2: Maintenance of Infrastructure. We require the continual maintenance of infrastructure.	***Consistent.*** *The Specific Plan includes a Maintenance Responsibility Matrix defining the responsible entities for continual maintenance of roadways, sidewalks, traffic signals, off site and on site public water, sewer, and storm drain infrastructure facilities.*

Plan Policy	Specific Plan Consistency
Policy CD5-3: Improvements to Property & Infrastructure. We provide programs to improve property and infrastructure.	***Not Applicable.***
Policy CD5-4: Neighborhood Involvement. We encourage active community involvement to implement programs aimed at the beautification and improvement of neighborhoods.	***Not Applicable.***

Mobility (M) Element

Goal M1:

A system of roadways that meets the mobility needs of a dynamic and prosperous Ontario.

Policy M1-1: Roadway Design and Maintenance. We require our roadways to: » Comply with federal, state and local design and safety standards. » Meet the needs of multiple transportation modes and users. » Handle the capacity envisioned in the Functional Roadway Classification Plan. » Maintain a peak hour Level of Service (LOS) E or better at all intersections. » Be compatible with the streetscape and surrounding land uses. » Be maintained in accordance with best practices and our Right-of-Way Management Plan.	***Consistent.*** *The Specific Plan requires consistency with the requirements of the City's Functional Roadway Classification Plan and the Ontario Ranch Streetscape Master Plan. The roadway system is designed to maintain a peak hour Level of Service (LOS) E or better at all intersections as discussed in the project EIR. Site design, source control for the project are required to be submitted by the developer for approval by the City prior to issuance of permits for the project.*
Policy M1-2: Mitigation of Impacts. We require development to mitigate its traffic impacts.	***Consistent.*** *All mitigation measures, standard conditions, and project design features identified in the project EIR to mitigate traffic impacts of the project will be implemented by the project prior to any occupancy.*
Policy M1-3: Roadway Improvements. We work with Caltrans, SANBAG and others to identify, fund and implement needed improvements to roadways identified in the Functional Roadway Classification Plan.	***Not Applicable.***
Policy M1-4: Adjacent Jurisdictions. We work with neighboring jurisdictions to meet our level of service standards at the City limits.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Goal M2: A system of trails and corridors that facilitate and encourage bicycling and walking.	
Policy M2-1: Bikeway Plan. We maintain our Multipurpose Trails & Bikeway Corridor Plan to create a comprehensive system of on- and off-street bikeways that connect residential areas, businesses, schools, parks, and other key destination points.	***Consistent.*** *The Specific Plan includes a plan for providing connectivity to the multipurpose trail along the Cucamonga Creek Channel. From these connection points, pedestrians will have access to the larger City of Ontario system of trails and bikeways.*
Policy M2-2: Bicycle System. We provide off-street multipurpose trails and Class II bikeways as our primary paths of travel and use the Class III for connectivity in constrained circumstances.	***Consistent.*** *The Specific Plan includes a plan for providing connectivity to the multipurpose trail along the Cucamonga Creek Channel. From these connection points, pedestrians will have access to the larger City of Ontario system of trails and bikeways.*
Policy M2-3: Pedestrian Walkways. We require walkways that promote safe and convenient travel between residential areas, businesses, schools, parks, recreation areas, and other key destination points.	***Consistent.*** *The Specific Plan includes a plan for construction of an off-street pedestrian circulation system comprised of an interconnected, paved sidewalk system within all roadway rights-of-ways, separated from vehicular travel lanes by a landscaped parkway.*
Policy M2-4: Network Opportunities. We explore opportunities to expand the pedestrian and bicycle networks. This includes consideration of utility easements, levees, drainage corridors, road right-of-ways, medians and other potential options.	***Consistent.*** *The Specific Plan includes a plan for construction of an off-street pedestrian circulation system comprised of an interconnected, paved sidewalk system within all roadway rights-of-ways, separated from vehicular travel lanes by a landscaped parkway.*
Goal M3: A public transit system that is a viable alternative to automobile travel and meets basic transportation needs of the transit dependent.	
Policy M3-1: Transit Partners. We maintain a proactive working partnership with transit providers to ensure that adequate public transit service is available.	***Not Applicable.***
Policy M3-2: Transit Facilities at New Development. We require new development to provide transit facilities, such as bus shelters, transit bays and turnouts, as necessary.	***Not Applicable.***
Policy M3-3: Transit-Oriented Development. We may provide additional development-related incentives to those inherent in the Land Use Plan for projects that promote transit use.	***Not Applicable.***
Policy M3-4: Bus Rapid Transit (BRT) Corridors. We work with regional transit agencies to implement BRT service to target destinations and along corridors, as shown in the Transit Plan.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy M3-5: Light Rail. We support extension of the Metro Rail Gold Line to Ontario, and will work to secure station locations adjacent to the Meredith site and at the proposed multimodal transit center.	***Not Applicable.***
Policy M3-6: Metrolink Expansion. We advocate expansion of Metrolink service to include the Downtown and the multimodal transit center.	***Not Applicable.***
Policy M3-7: High Speed Rail. We encourage the development of high-speed rail systems that would enhance regional mobility in Southern California and serve the City of Ontario.	***Not Applicable.***
Policy M3-8: Feeder Systems. We work with regional transit agencies to secure convenient feeder service from the Metrolink station and the proposed multimodal transit center to employment centers in Ontario.	***Not Applicable.***
Policy M3-9: Ontario Airport Metro Center Circulator. We will explore development of a convenient mobility system, including but not limited to shuttle service, people mover, and shared car system, for the Ontario Airport Metro Center.	***Not Applicable.***
Policy M3-10: Multimodal Transit Center. We intend to ensure the development of a multimodal transit center near LAONT airport to serve as a transit hub for local buses, BRT, the Gold Line, high-speed rail, the proposed Ontario Airport Metro Center circulator and other future transit modes.	***Not Applicable.***
Policy M3-11: Transit and Community Facilities. We require the future development of community-wide serving facilities to be sited in transit-ready areas that can be served and made accessible by public transit. Conversely, we plan (and coordinate with other transit agencies to plan) future transit routes to serve existing community facilities.	***Not Applicable.***
Goal M4: An efficient flow of goods through the City that maximizes economic benefits and minimizes negative impacts.	
Policy M4-1: Truck Routes. We designate and maintain a network of City truck routes that provide for the effective transport of goods while minimizing negative impacts on local circulation and noise-sensitive land uses, as shown in the Truck Routes Plan.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy M4-2: Regional Participation. We work with regional and subregional transportation agencies to plan and implement goods movement strategies, including those that improve mobility, deliver goods efficiently and minimize negative environmental impacts	***Not Applicable.***
Policy M4-3: Railroad Grade Separations. We eliminate at-grade rail crossings identified on the Functional Roadway Classification Plan.	***Not Applicable.***
Policy M4-4: Environmental Considerations. We support efforts to reduce/eliminate the negative environmental impacts of goods movement.	***Not Applicable.***
Policy M4-5: Air Cargo. We support and promote a LAONT airport that accommodates 1.6 million tons of cargo per year, as long as the impacts associated with that level of operations are planned for and mitigated.	***Not Applicable.***
Goal M5: A proactive leadership role to help identify and facilitate implementation of strategies that address regional transportation challenges.	
Policy M5-1: Regional Leadership. We maintain a leadership role to help identify and implement potential solutions to long-term regional transportation problems.	***Not Applicable.***
Policy M5-2: Land Use Compatibility with Regional Transportation Facilities. We work with LAWA, railroads, Caltrans, SANBAG, and other transportation agencies to minimize impacts.	***Not Applicable.***
Environmental Resources (ER) Element	
Goal ER1: A reliable and cost effective system that permits the City to manage its diverse water resources and needs.	
Policy ER-1: Local Water Supply. We increase local water supplies to reduce our dependence on imported water.	***Not Applicable.***
Policy ER-2: Matching Supply to Use. We match water supply and quality to the appropriate use.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy ER1-3: Conservation. We require conservation strategies that reduce water usage.	***Consistent.*** *The Specific Plan requires all public and common area landscaping within the project to utilize plant materials listed on the approved Specific Plan Landscape Plant Matrix which is comprised of drought tolerant and California-friendly plant materials. The Specific Plan requires that irrigation systems for both public and private landscaped areas be designed to be as water-efficient as possible and includes the following minimum requirements.* » *All irrigation systems shall have automatic controllers designed to properly water plant materials given the site's soil conditions, and irrigation systems for all public landscapes shall have automatic rain shut-off devices.* » *Drip bubblers or low volume irrigation is required in areas less than 8' wide.* » *Spray systems shall have low volume matched precipitation heads.* » *All CFD areas a*re to be controlled with central *control irrigation systems, and all trees are to be irrigated utilizing a pop up stream bubbler system on a separate valve. All CFD areas shall be designed to City Standard Specifications.*
Policy ER1-4: Supply-Demand Balance. We require that available water supply and demands be balanced.	***Not Applicable.***
Policy ER1-5: Groundwater Management. We protect groundwater quality by incorporating strategies that prevent pollution, require remediation where necessary, capture and treat urban run-off, and recharge the aquifer.	***Consistent.*** *The Specific Plan requires that the developer obtain approval of a StormWater Pollution Prevention Plan (SWPPP) prior to issuance of grading or construction permits. The SWPPP will be prepared to comply with California State Water Resources Control Board's current "General Permit to Discharge StormWater Associated With Construction Activity" and current "Area Wide Urban Storm Water Runoff (Regional NPDES) Permit."The SWPPP will identify and detail all appropriate Best Management Practices (BMP's) to be implemented or installed during construction of the project.* *In addition to the preparation of a SWPPP for construction-related activities, and as part of the approval of any grading plans for the project, the developer is required to submit a Water Quality Management Plan (WQMP) on the regional model form provided by the City. The WQMP shall identify and detail all Site Design BMP's, Source Control BMP's and Treatment Control BMP's to be implemented or installed as part of the project in order to reduce storm water pollutants and site runoff.*

Plan Policy	Specific Plan Consistency
Policy ER1-6: Urban Run-off Quantity. We encourage the use of low impact development strategies to intercept run-off, slow the discharge rate, increase infiltration and ultimately reduce discharge volumes to traditional storm drain systems.	***Consistent.*** *The Specific Plan requires that grading and drainage for the project be designed to detain, filter, and treat surface runoff in a manner which is practical in order to comply with the most recent requirements of the San Bernardino County NPDES Storm Water Program's Quality Management (WQMP) for significant new development projects. Site design for the project is required to incorporate features which will minimize the use of impervious surfaces and maximize on-site infiltration, Source Control Best Management Practices (BMP's) and either on-site Structural Treatment Control BMP's or participation in regional or watershed-based Treatment Control BMP's.*
Policy ER1-7: Urban Run-off Quality. We require the control and management of urban run-off, consistent with Regional Water Quality Control Board regulations.	***Consistent.*** *The Specific Plan requires that the project comply with the most recent requirements of the San Bernardino County NPDES Storm Water Program's Quality Management (WQMP) for significant new development projects. A final WQMP is required to be submitted by the developer for approval by the City prior to the issuance of any grading and construction permits for the project.*
Policy ER1-8: Wastewater Management. We require the management of wastewater discharge and collection consistent with waste discharge requirements adopted by the Regional Water Quality Control Board.	***Consistent.*** *The Specific Plan requires the construction of a wastewater system consistent with City requirements and also requires that the project obtain approval of a WQMP for the project prior to the issuance of any grading or construction permit.*
Goal ER2: A cost effective, integrated waste management system that meets or exceeds state and federal recycling and waste diversion mandates.	
Policy ER2-1: Waste Diversion. We shall meet or exceed AB 939 requirements.	***Consistent.*** *The Specific Plan shall comply with all state and federal regulations for waste diversion.*
Policy ER2-2: Hazardous and Electronic Wastes. We prohibit the disposal of hazardous and electronic waste into the municipal waste stream pursuant to state law.	***Consistent.*** *The Specific Plan shall comply with all state and federal regulations for waste diversion.*
Policy ER2-3: Purchase Products Made from Recycled Materials. We purchase recycled-content products where it is cost effective.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Goal ER3: Cost-effective and reliable energy system sustained through a combination of low impact building, site and neighborhood energy conservation and diverse sources of energy generation that collectively helps to minimize the region's carbon footprint.	
Policy ER3-1: Conservation Strategy. We require conservation as the first strategy to be employed to meet applicable energy-saving standards.	***Consistent.*** *The Specific Plan requires all public and common area landscaping within the project to utilize plant materials listed on the approved Specific Plan Landscape Plant Matrix which is comprised of drought tolerant and California Friendly plant materials. The Specific Plan requires that irrigation systems for both public and private landscaped areas be designed to be as water-efficient as possible. The Specific Plan requires the construction of separate water mains for the use of recycled water in public and common areas of the project. All new construction will utilize fixtures, and heating and cooling controls to conserve water and energy.*
Policy ER3-2: Green Development– Communities. We require the use of best practices identified in green community rating systems to guide the planning and development of all new communities.	***Consistent.*** *The Colony Commerce Center West design guidelines encourages all new construction to utilize design features, fixtures, appliances, and heating and cooling controls to conserve energy and water. The landscape concept for Colony Commerce Center West incorporates a plant palette of drought tolerant materials and requirements that the development implement planting and irrigation systems designed to conserve water.*
Policy ER3-3: Building and Site Design. We require new construction to incorporate energy efficient building and site design strategies, which could include appropriate solar orientation, maximum use of natural daylight, passive solar and natural ventilation.	***Consistent.*** *The Colony Commerce Center West design guidelines encourages all new construction to utilize design features, fixtures, appliances, and heating and cooling controls to conserve energy and water.*
Policy ER3-4: Green Development– Public Buildings. We require all new and substantially renovated City buildings in excess of 10,000 square feet achieve a LEED Silver Certification standard, as determined by the U.S. Green Building Council.	***Not Applicable.***
Policy ER3-5: Fuel Efficient and Alternative Energy Vehicles and Equipment. We purchase and use vehicles and equipment that are fuel efficient and meet or surpass state emissions requirements and/or use renewable sources of energy.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy ER3-6: Generation- Renewable Sources. We promote the use of renewable energy sources to serve public and private sector development.	***Not Applicable.***
Goal ER4: Improved indoor and outdoor air quality and reduced locally generated pollutant emissions.	
Policy ER4-1: Land Use. We reduce GHG and other local pollutant emissions through compact, mixed use, and transit-oriented development and development that improves the regional jobs-housing balance.	***Consistent.*** *The industrial uses planned for on the Colony Commerce Center West will have the ability to generate jobs for City of Ontario residents.*
Policy ER4-2: Sensitive Land Uses. We prohibit the future siting of sensitive land uses, within the distances defined by the California Air Resources Board for specific source categories, without sufficient mitigation.	***Not Applicable.***
Policy ER4-3: Greenhouse Gases (GHG) Emissions Reductions. We will reduce GHG emissions in accordance with regional, state and federal regulations.	***Not Applicable.***
Policy ER4-4: Indoor Air Quality. We will comply with State Green Building Codes relative to indoor air quality.	***Consistent.*** *All development within the Specific Plan will be required to comply with the State Green Building Code as implemented by the City.*
Policy ER4-5: Transportation. We promote mass transit and non-motorized mobility options (e.g. walking, biking) to reduce air pollutant emissions.	***Not Applicable.***
Policy ER4-6: Particulate Matter. We support efforts to reduce particulate matter to meet State and Federal Clean Air Standards.	***Not Applicable.***
Policy ER4-7: Other Agency Collaboration. We collaborate with other agencies within the South Coast Air Basin to improve regional air quality at the emission source.	***Not Applicable.***
Policy ER4-8: Tree Planting. We protect healthy trees within the City and plant new trees to increase carbon sequestration and help the regional/local air quality.	***Not Applicable.***
Goal ER5: Protected high value habitat and farming and mineral resource extraction activities that are compatible with adjacent development.	
Policy ER5-1: Habitat Conservation Areas. We support the protection of biological resources through the establishment, restoration and conservation of high quality habitat areas.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy ER5-2: Entitlement and Permitting Process. We comply with state and federal regulations regarding protected species.	***Consistent.*** *The project will comply with all mitigation measures identified in the project EIR with regard to biological resources.*
Policy ER5-3: Right to Farm. We support the right of existing farms to continue their operations within the Ontario Ranch.	***Consistent.*** *The Specific Plan supports the right of existing farms to continue their operations within the Ontario Ranch.*
Policy ER5-4: Transition of Farms. We protect both existing farms and sensitive uses around them as agricultural areas transition to urban uses.	***Consistent.*** *The Specific Plan supports the right of existing farms to continue their operations in addition to transitioning their properties to other uses within the Ontario Ranch.*
Policy ER5-5: Mining Operations. We prohibit future mining operations where the resource extraction activities are incompatible with existing or proposed adjacent land uses.	***Not Applicable.***

Safety (S) Element

Goal S1:

Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.

Policy S1-1: Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.	***Consistent.*** *All development within the Specific Plan will be required to comply with the State of California Building Code as adopted and implemented by the City.*
Policy S1-2: Entitlement and Permitting Process. We follow state guidelines and the California Building Code to determine when development proposals must conduct geotechnical and geological investigations.	***Consistent.*** *All development within the Specific Plan will be required to comply with the State of California Building Code as adopted and implemented by the City.*
Policy S1-3: Continual Update of Technical Information. We maintain up-to-date California Geological Survey seismic hazard maps.	***Not Applicable.***
Policy S1-4: Seismically Vulnerable Structures. We conform to state law regarding unreinforced masonry structures.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Goal S2: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by flooding and inundation hazards.	
Policy S2-1: Entitlement and Permitting Process. We follow State guidelines and building code to determine when development proposals require hydrological studies prepared by a State-certified engineer to assess the impact that the new development will have on the flooding potential of existing development down-gradient.	***Consistent.*** *All development within the Specific Plan will be required to comply with the State of California Building Code as adopted and implemented by the City.*
Policy S2-2: Flood Insurance. We will limit development in flood plains and participate in the National Flood Insurance Program.	***Not Applicable.***
Policy S2-3: Facilities that Use Hazardous Materials. We comply with state and federal law and do not permit facilities using, storing, or otherwise involved with substantial quantities of onsite hazardous materials to be located in the 100 year flood zone unless all standards of elevation, flood proofing and storage have been implemented to the satisfaction of the Building Department.	***Consistent.*** *All development within the Specific Plan will be required to comply with the State of California Building Code as adopted and implemented by the City.*
Policy S2-4: Prohibited Land Uses. We prohibit the development of new essential and critical facilities in the 100-year floodplain.	***Not Applicable.***
Policy S2-5: Storm Drain System. We maintain and improve the storm drain system to minimize flooding.	***Consistent.*** *The project shall improve the storm drain system has planned by the City of Ontario.*
Policy S2-6: Use of Flood Control Facilities. We encourage joint use of flood control facilities as open space or other types of recreational facilities.	***Not Applicable.***
Goal S3: Reduced risk of death, injury, property damage and economic loss due to fires, accidents and normal everyday occurrences through prompt and capable emergency response.	
Policy S3-1: Prevention Services. We proactively mitigate or reduce the negative effects of fire, hazardous materials release, and structural collapse by implementing the adopted Fire Code.	***Consistent.*** *All development within the Specific Plan will be required to comply with the State of California Building Code as adopted and implemented by the City.*

Plan Policy	Specific Plan Consistency
Policy S3-2: Community Outreach. We provide education to local schools and community groups to promote personal and public safety.	***Not Applicable.***
Policy S3-3: Fire and Emergency Medical Services. We maintain sufficient fire stations, equipment and staffing to respond effectively to emergencies.	***Not Applicable.***
Policy S3-4: Special Team Services. We maintain effective special rescue services.	***Not Applicable.***
Policy S3-5: Emergency Communication Services. We maintain a 9-1-1 emergency communication and dispatch center.	***Not Applicable.***
Policy S3-6: Interagency Cooperation. In order to back up and supplement our capabilities to respond to emergencies, we participate in the California Fire Rescue and Mutual Aid Plan.	***Not Applicable.***
Policy S3-7: Water Supply and System Redundancy. We monitor our water system to manage firefighting water supplies.	***Not Applicable.***
Policy S3-8: Fire Prevention through Environmental Design. We require new development to incorporate fire prevention consideration in the design of streetscapes, sites, open spaces and buildings.	***Consistent.*** *The Specific Plan requires all new development to be reviewed and approved pursuant to the provisions of the City's Subdivision Ordinance and Development Plan Review process which provides for review by the City's Fire Department which may require the development to incorporate fire prevention design elements in streetscapes, sites, open spaces and buildings.*
Policy S3-9: Resource Allocation. We analyze fire data to evaluate the effectiveness of our fire prevention and reduction strategies and allocate resources accordingly.	***Not Applicable.***
Goal S4: An environment where noise does not adversely affect the public's health, safety, and welfare.	
Policy S4-1: Noise Mitigation. We utilize the City's Noise Ordinance, building codes and subdivision and development codes to mitigate noise impacts.	***Consistent.*** *The Specific Plan shall comply with the City's Noise Ordinance and building codes in order to mitigate noise impacts.*

Plan Policy	Specific Plan Consistency
Policy S4-2: Coordination with Transportation Authorities. We collaborate with airport owners, FAA, Caltrans, SANBAG, SCAG, neighboring jurisdictions, and other transportation providers in the preparation and maintenance of, and updates to transportation-related plans to minimize noise impacts and provide appropriate mitigation measures.	***Not Applicable.***
Policy S4-3: Airport Noise Mitigation. We aggressively pursue funding and utilize programs to reduce effects of aircraft noise in impacted areas of our community.	***Not Applicable.***
Policy S4-4: Truck Traffic. We manage truck traffic to minimize noise impacts on sensitive land uses.	***Not Applicable.***
Policy S4-5: Roadway Design. We design streets and highways to minimize noise impacts.	***Not Applicable.***
Policy S4-6: Airport Noise Compatibility. We utilize information from Airport Land Use Compatibility Plans to prevent the construction of new noise sensitive land uses within airport noise impact zones.	***Not Applicable.***
Goal S5: Reduced risk of injury, property damage and economic loss resulting from windstorms and wind-related hazards.	
Policy S5-1: Backup Power in Critical Facilities. We require backup power be maintained in critical facilities.	***Not Applicable.***
Policy S5-2: Dust Control Measures. We require the implementation of Best Management Practices for dust control at all excavation and grading projects.	***Consistent.*** *Construction within the Specific Plan will comply with a City approved construction management plan and all mitigation measures identified in the project EIR with regard to dust control.*
Policy S5-3: Grading in High Winds. We prohibit excavation and grading during strong wind conditions, as defined by the Building Code.	***Not Applicable.***
Goal S6: Reduced potential for hazardous materials exposure and contamination.	
Policy S6-1: Disclosure and Notification. We enforce disclosure laws that require all users, producers, and transporters of hazardous materials and wastes to clearly identify the materials that they store, use or transport.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy S6-2: Response to Hazardous Materials Releases. We respond to hazardous materials incidents and coordinate these services with other jurisdictions.	***Not Applicable.***
Policy S6-3: Safer Alternatives. We minimize our use of hazardous materials by choosing non-toxic alternatives that do not pose a threat to the environment.	***Not Applicable.***
Policy S6-4: Safe Storage and Maintenance Practices. We require that the users of hazardous materials be adequately prepared to prevent and mitigate hazardous materials releases.	***Not Applicable.***
Policy S6-5: Location of Hazardous Material Facilities. We regulate facilities that will be involved in the production, use, storage or disposal of hazardous materials, pursuant to federal, state, county, and local regulations, so that impacts to the environment and sensitive land uses are mitigated.	***Not Applicable.***
Policy S6-6: Location of Sensitive Land Uses. We prohibit new sensitive land uses from locating within airport Safety Zones and near existing sites that use, store, or generate large quantities of hazardous materials.	***Not Applicable.***
Policy S6-7: Household Hazardous Waste. We support the proper disposal of household hazardous substances.	***Not Applicable.***
Policy S6-8: Mitigation and Remediation of Groundwater Contamination. We actively participate in local and regional efforts directed at both mitigating environmental exposure to contaminated groundwater and taking action to clean up contaminated groundwater once exposure occurs.	***Consistent.*** *The project will comply with all mitigation measures identified as part of the project EIR for groundwater remediation and if necessary, proper action to clean up contaminated groundwater within the project.*
Policy S6-9: Remediation of Methane. We require development to assess and mitigate the presence of methane, per regulatory standards and guidelines.	***Consistent.*** *The project will comply with all mitigation measures identified as part of the project EIR for soil remediation and if necessary, proper venting to address the potential existence of methane gases within the project.*
Goal S7: Neighborhoods and commercial and industrial districts that are kept safe through a multi-faceted approach of prevention, suppression, community involvement and a system of continuous monitoring.	
Policy S7-1: Police Unit Response. We respond to calls for service in a timely manner.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy S7-2: Community Oriented Problem Solving (C.O.P.S.). We support and maintain the mission of COPS to identify and resolve community problems.	***Not Applicable.***
Policy S7-3: Prevention Services. We provide crime prevention programs targeted to youth, parents, seniors, businesses, and neighborhoods.	***Not Applicable.***
Policy S7-4: Crime Prevention through Environmental Design (CPTED).We require new development to incorporate CPTED in the design of streetscapes, sites, open spaces and buildings.	***Consistent.*** *The Specific Plan requires all new development to be reviewed and approved pursuant to the provisions of the City's Subdivision Ordinance and Development Plan Review process which provides for review by the City's Police Department which may require the development to incorporate CPTED in the design of streetscapes, sites, open spaces and buildings.*
Policy S7-5: Interdepartmental Coordination. We utilize all City departments to help reduce crime and promote public safety.	***Not Applicable.***
Policy S7-6: Partnerships. We partner with other local, state and federal law enforcement agencies and private security providers to enhance law enforcement service to Ontario.	***Not Applicable.***
Policy S7-7: Resource Allocation. We analyze crime data to evaluate the effectiveness of crime prevention and reduction strategies and allocate resources accordingly.	***Not Applicable.***
Goal S8: Disaster resilient, prepared community through effective emergency/disaster preparedness, response, mitigation and recovery.	
Policy S8-1: State and Federal Mandates. We maintain emergency management programs that meet the requirements of the State of California Standardized Emergency Management System (SEMS) and the National Incident Management System (NIMS).	***Not Applicable.***
Policy S8-2: Emergency Management Plans. We maintain, update and adopt the Emergency Operations Plan (EOP) and the Hazard Mitigation Plan (HMP).	***Not Applicable.***
Policy S8-3: Emergency/Disaster Training Exercises. We conduct training and exercises to prepare for and evaluate emergency/disaster response and recovery procedures.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy S8-4: Interagency Collaboration. We partner with public and private organizations, such as participation in the California Master Mutual Aid Agreement, in order to enhance and compliment our planning and response capabilities.	***Not Applicable.***
Policy S8-5: Interdepartmental Coordination. We utilize all City departments to help support emergency/disaster preparedness, response, mitigation and recovery.	***Not Applicable.***
Policy S8-6: Community Outreach. We provide education to the community to promote personal, family and community emergency preparedness.	***Not Applicable.***

Community Economics (CE) Element

Goal CE1:

A complete community that provides for all incomes and stages of life.

Policy CE1-1: Jobs-Housing Balance. We pursue improvement to the Inland Empire's balance between jobs and housing by promoting job growth that reduces the regional economy's reliance on out-commuting.	***Consistent.*** *The industrial uses planned for on the Colony Commerce Center West will have the ability to generate jobs for City of Ontario residents.*
Policy CE1-2: Jobs and Workforce Skills. We use our economic development resources to: 1) attract jobs suited for the skills and education of current and future City residents; 2) work with regional partners to provide opportunities for the labor force to improve its skills and education; and 3) attract businesses that increase Ontario's stake and participation in growing sectors of the regional and global economy.	***Not Applicable.***
Policy CE1-3: Regional Approach to Workforce Development. We work with our partners to provide workforce training and development services throughout the region recognizing that Ontario employers rely on workers living outside of the City.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy CE1-4: Business Retention and Expansion. We continuously improve two-way communication with the Ontario business community and emphasize customer service to existing businesses as part of our competitive advantage.	***Not Applicable.***
Policy CE1-5: Business Attraction. We proactively attract new and expanding businesses to Ontario in order to increase the City's share of growing sectors of the regional and global economy.	***Consistent.*** *The Colony Commerce Center West allows for the development of regionally serving employment centers accommodating a variety of jobs that can meet short- and long-term market demands. The project is also consistent with regional planning goals such as SCAG's Goods Movement Corridor with consideration to strategies that facilitate goods movement through the area.*
Policy CE1-6: Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.	***Not Applicable.***
Policy CE1-7: Retail Goods and Services. We seek to ensure a mix of retail businesses that provide the full continuum of goods and services for the community.	***Not Applicable.***
Policy CE1-8: Regional Attraction. We encourage the development and programming of regional, cultural, and entertainment destinations in Ontario.	***Not Applicable.***
Policy CE1-9: Regional Leadership. We provide leadership for public, quasi-public, and private-sector partners that help Ontario and its residents and businesses realize our goals and achieve our Vision.	***Not Applicable.***
Policy CE1-10: Life-Long Education. We work with our partners who provide life-long learning to ensure that our residents and workforce have access to education at all stages of life.	***Not Applicable.***
Policy CE1-11: Socioeconomic Trends. We continuously monitor, plan for, and respond to changing socioeconomic trends.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy CE1-12: Circulation. We continuously plan and improve public transit and non-vehicular circulation for the mobility of all, including those with limited or no access to private automobiles.	***Not Applicable.***
Policy CE1-13: Safety and Security. We invest in public safety and communicate our successes because the perception and reality of safety and security are necessary prerequisites for private investment and economic growth.	***Not Applicable.***
Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.	
Policy CE2-1: Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.	***Consistent.*** *The Specific Plan includes design guidelines that will encourage a quality development that adds value to the surrounding area.*
Policy CE2-2: Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.	***Consistent.*** *The Specific Plan requires all new development to be reviewed and approved pursuant to the provisions of the City's Subdivision Ordinance and Development Plan Review process which provides for review by the City's Planning Department which may require the development to demonstrate how the project will create appropriately unique, functional and sustainable places.*
Policy CE2-3: Interim Development. We require interim development that does not reflect the long-term Vision, be limited in scale of development so that the investment can be sufficiently amortized to make Vision-compatible redevelopment financially feasible.	***Not Applicable.***
Policy CE2-4: Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.	***Not Applicable.***
Policy CE2-5: Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.	***Consistent.*** *The Specific Plan includes a Maintenance Responsibility Matrix defining the private responsibilities for maintenance of private roadways, parkways, trails, common areas, parks, yards, walls, and monuments within the project.*

Plan Policy	Specific Plan Consistency
Policy CE2-6: Public Maintenance. We require the establishment and operation of maintenance districts or other vehicles to fund the long-term operation and maintenance of the public realm whether on private land, in rights-of-way, or on publicly-owned property.	***Consistent.*** *The Specific Plan includes a Maintenance Responsibility Matrix defining the responsible public entities, including special districts, for maintenance of roadways, sidewalks, traffic signals, off site and on site public water, sewer, and storm drain infrastructure facilities.*
Housing (H) Element	
Goal H1: Stable neighborhoods of quality housing, ample community services and public facilities, well-maintained infrastructure, and public safety that foster a positive sense of identity.	
Policy H1-1: Housing Rehabilitation. We support the rehabilitation, maintenance, and improvement of single-family, multiple-family, and mobile homes through code compliance, removal of blight where necessary, and provision of rehabilitation assistance where feasible.	***Not Applicable.***
Policy H1-2: Neighborhood Conditions. We direct efforts to improve the long-term sustainability of neighborhoods through comprehensive planning, provision of neighborhood amenities, rehabilitation and maintenance of housing, and community building efforts.	***Not Applicable.***
Policy H1-3: Community Amenities. We shall provide adequate public services, infrastructure, open space, parking and traffic management, pedestrian, bicycle and equestrian routes and public safety for neighborhoods consistent with City master plans and neighborhood plans.	***Not Applicable.***
Policy H1-4: Historical Preservation. We support the preservation and enhancement of residential structures, properties, street designs, lot configurations, and other reminders of Ontario's past that are considered to be local historical or cultural resources.	***Not Applicable.***
Policy H1-5: Neighborhood Identity. We strengthen neighborhood identity through creating parks and recreational outlets, sponsoring neighborhood events and encouraging resident participation in the planning and improvement of their neighborhoods.	***Not Applicable.***
Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.	

Plan Policy	Specific Plan Consistency
Policy H2-1: Corridor Housing. We revitalize transportation corridors by encouraging the production of higher density residential and mixed-uses that are architecturally, functionally and aesthetically suited to corridors.	***Not Applicable.***
Policy H2-2: Historic Downtown. We foster a vibrant historic downtown through facilitating a wide range of housing types and affordability levels for households of all ages, housing preferences, and income levels.	***Not Applicable.***
Policy H2-3: Ontario Airport Metro Center. We foster a vibrant, urban, intense and highly amenitized community in the Ontario Airport Metro Center Area through a mix of residential, entertainment, retail and office-oriented uses.	***Not Applicable.***
Policy H2-4: Ontario Ranch. We support a premier lifestyle community in the Ontario Ranch distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.	***Not Applicable.***
Policy H2-5: Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.	***Not Applicable.***
Policy H2-6: Infill Development. We support the revitalization of neighborhoods through the construction of higher-density residential developments on underutilized residential and commercial sites.	***Not Applicable.***
Goal H3: A City regulatory environment that balances the need for creativity and excellence in residential design, flexibility and predictability in the project approval process, and the provision of an adequate supply and prices of housing.	
Policy H3-1: Incentives. We maintain incentive programs that can be offered to projects that provide benefits to the community such as exceptional design quality, economic advantages, environmental sustainability, or other benefits that would otherwise be unrealized.	***Not Applicable.***
Policy H3-2: Flexible Standards. We allow flexibility in the application of residential and mixed-use development standards in order to gain benefits such as exceptional design quality, economic advantages, sustainability, or other benefits that would otherwise be unrealized.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy H3-3: Development Review. We maintain a residential development review process that provides certainty and transparency for project stakeholders and the public, yet allows for the appropriate review to facilitate quality housing development.	***Not Applicable.***
Policy H3-4: Financial Incentives. We consider financial incentives to facilitate and encourage the production, rehabilitation or improvement of housing, or provision of services where such activity furthers housing and community-wide goals.	***Not Applicable.***
Goal H4: Increased opportunities for low and moderate income households and families to afford and maintain quality ownership and rental housing opportunities, including move-up opportunities.	
Policy H4-1: Preservation of Affordable Apartments. We strive to facilitate the preservation of the affordability of publicly assisted apartments for lower income households through financial assistance, technical assistance, rehabilitation, and collaborative partnerships.	***Not Applicable.***
Policy H4-2: Homeownership Opportunities. We increase and expand homeownership rates for lower and moderate income households by offering financial assistance, low-interest loans and educational resources, and by working in collaboration with partnerships.	***Not Applicable.***
Policy H4-3: Rental Assistance. We support the provision of rental assistance for individuals and families earning extremely low, very low, and low income with funding from the state and federal government.	***Not Applicable.***
Policy H4-4: Mixed-income Housing. We encourage the integration of affordable housing in the Ontario Ranch, Ontario Airport Metro Center Area, and existing neighborhoods.	***Not Applicable.***
Policy H4-5: Collaborative Partnerships. We support collaborative partnerships of nonprofit organizations, affordable housing developers, major employers, and for-profit developers to produce affordable housing.	***Not Applicable.***
Policy H4-6: Fair Housing. We further fair housing by prohibiting discrimination in the housing market and providing education, support, and enforcement services to address discriminatory practices.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.	
Policy H5-1: Senior Housing. We support the development of accessible and affordable senior housing and provide financial assistance for seniors to maintain and improve their homes.	***Not Applicable.***
Policy H5-2: mily Housing. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation and other amenities.	***Not Applicable.***
Policy H5-3: Disabled People. We increase the supply of permanent, affordable and accessible housing for people with disabilities, and provide assistance to allow them to maintain and improve their homes.	***Not Applicable.***
Policy H5-4: Homeless People. We partner with non-profit partners to provide emergency shelters, transitional housing, permanent supportive housing, and supportive services for people who are homeless.	***Not Applicable.***
Policy H5-5: Supportive Services. We financially support organizations, as feasible, that provide support services that meet the needs of those with special needs and further the greatest level of independence.	***Not Applicable.***
Policy H5-6: Partnerships. We collaborate with non-profit organizations, private developers, employers, government agencies and other interested parties to develop affordable housing and provide support services.	***Not Applicable.***
Parks & Rec (PR) Element	
Goal PR1: A system of safe and accessible parks that meets the needs of the community.	
Policy PR1-1: Access to Parks. We strive to provide a park and/or recreational facility within walking distance (¼ mile) of every residence.	***Not Applicable.***
Policy PR1-2: Adjacency to Schools. We examine locating parks adjacent to school sites to promote joint-use opportunities.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy PR1-3: Funding. We shall seek outside, one-time sources of funding for capital improvements and reserve ongoing City funds primarily for operations and maintenance.	***Not Applicable.***
Policy PR1-4: Joint-use Opportunities. In areas where there is a need but no City recreational facility, we explore joint-use opportunities. (e.g., school sites).	***Not Applicable.***
Policy PR1-5: Acreage Standard. We strive to provide 5 acres of parkland (public and private) per 1,000 residents.	***Not Applicable.***
Policy PR1-6: Private Parks. We expect development to provide a minimum of 2 acres of developed private park space per 1,000 residents.	***Not Applicable.***
Policy PR1-7: Special Needs/Universal Design. We attempt to provide recreational opportunities at parks for people of all ages and abilities.	***Not Applicable.***
Policy PR1-8: Renovation. We examine renovating existing facilities prior to building replacement facilities.	***Not Applicable.***
Policy PR1-9: Phased Development. We require parks be built in new communities before a significant proportion of residents move in.	***Not Applicable.***
Policy PR1-10: Master Plans for Individual Park Facilities. We require an individual park master plan for parks in excess of 10 acres.	***Not Applicable.***
Policy PR1-11: Environmental Function of Parks. We require new parks to meet environmental management objectives.	***Not Applicable.***
Policy PR1-12: Trails. We promote connections between parks and local trails including those managed by other public agencies.	***Not Applicable.***
Policy PR1-13: Equestrian Trails. We require the design, construction and maintenance of equestrian trails in Rural Residential designated areas.	***Not Applicable.***
Policy PR1-14: Multi-family Residential Developments. We require that new multi-family residential developments of five or more units provide recreational facilities or open space, in addition to paying adopted impact fees.	***Not Applicable.***
Policy PR1-15: Trail Connectivity. We strengthen and improve equestrian, bike and multipurpose trail connections within the City and work to improve trail connections into adjacent jurisdictions.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy PR1-16: Equestrian Master Plan. We use Homer Briggs Park as the primary focal point for the development of a Master Plan of Equestrian Trails in the Rural Residential area.	***Not Applicable.***
Goal PR2: A range of recreational programs provided by public, private and non-profit organizations that meet the needs of the community's varied interests, age groups and abilities.	
Policy PR2-1: Participation. We program park facilities to maximize utilization and participation, while considering park size, location and population served.	***Not Applicable.***
Policy PR2-2: Needs Assessment. We track the needs and priorities for recreational programming and look for ways to meet demand.	***Not Applicable.***
Policy PR2-3: Community Involvement. We involve the local community in planning programs for neighborhood and community park facilities.	***Not Applicable.***
Policy PR2-4: Access to Programs. We provide a range of program opportunities for residents of all income levels.	***Not Applicable.***
Policy PR2-5: Partnerships. We partner with local and regional agencies, non-profit organizations and the private sector to provide a comprehensive range of recreational programs.	***Not Applicable.***
Policy PR2-6: Crime Deterrents. We promote and participate in recreational programming as part of our crime prevention effort.	***Not Applicable.***
Social Resources (SR) Element	
Goal SR1: A community where residents have access to information, services and goods that improve their health and well being.	
Policy SR1-1: Partnering for Healthcare. We work with healthcare providers, and local, regional, state and federal agencies to attract and retain a diversity of affordable, quality healthcare and facilities for the entire community.	***Not Applicable.***
Policy SR1-2: Nutrition Choices. We support the promotion of healthy nutritional food choices in the community.	***Not Applicable.***
Policy SR1-3: Health Education. We promote health education, including disease prevention, mental health, nutrition and physical fitness.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy SR1-4: Physical Activity. We encourage activities and community design that improve the physical fitness of our community members.	***Not Applicable.***
Goal SR2: A range of educational and training opportunities for residents and workers of all ages and abilities that improves their life choices and provides a skilled workforce for our businesses.	
Policy SR2-1: Educational Partners. We partner with educational institutions throughout the region in order to expand the range and quality of educational offerings available to the community.	***Not Applicable.***
Policy SR2-2: Workforce Training. We will work with industrial organizations, businesses and educational institutions to create opportunities for workforce training.	***Not Applicable.***
Policy SR2-3: Joint Use of Facilities. We partner with public and private educational institutions to jointly use facilities for both City and educational purposes.	***Not Applicable.***
Policy SR2-4: Access to Schools. We work with local and regional partners to improve the safety in and around schools and to improve access for citizens of all ages and abilities to schools and community services, such as after school and other programs.	***Not Applicable.***
Policy SR2-5: School Facilities. We plan and coordinate with school districts for designing and locating school facilites to meet the City's goals, such as for health, walkability, and safety and to minimize impacts to existing neighborhoods.	***Not Applicable.***
Goal SR3: A range of community and leisure programs and activities provided by public, private and non-profit organizations that meet the needs of the community's varied interests, age groups and abilities.	
Policy SR3-1: Partnerships. We partner with local and regional agencies, non-profit organizations and the private sector to provide a comprehensive range of community activities and events to citizens.	***Not Applicable.***
Policy SR3-2: Needs Assessment. We track the needs and priorities for community services and look for ways to meet demands and avoid duplication of offerings.	***Not Applicable.***
Policy SR3-3: Program Outreach. We promote information about leisure activities, classes, special events and other services and activities to our community.	***Not Applicable.***
Policy SR3-4: Community Events. We plan and actively participate in regularly scheduled community events and seasonal or yearly citywide events.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy SR3-5: Community Activities as Crime Deterrents. We promote and participate in community activities as part of our crime prevention efforts.	***Not Applicable.***
Goal SR4: City libraries that connect community members of all ages and abilities to a broad range of programs, communication and informational resources.	
Policy SR4-1: Community Needs. We identify and monitor community needs for library services, technologies and facilities, and tailor them to effectively meet those needs.	***Not Applicable.***
Policy SR4-2: Interagency Coordination. We leverage relationships with outside agencies, educational institutions and neighboring jurisdictions to share library resources to the benefit of Ontario residents.	***Not Applicable.***
Policy SR4-3: Library Outreach. We outreach to the community to increase the patronage of the library.	***Not Applicable.***
Policy SR4-4: Coordination with Other Community Services. We coordinate library programs with other recreational and community programs and facilities.	***Not Applicable.***
Policy SR4-5: Focal Points of the Community. We design and program Ontario's libraries as focal points for community engagement, including public outreach and community events.	***Not Applicable.***
Policy SR4-6: Robert E. Ellingwood Model Colony History Room. We work with the Museum of History and Art, Ontario in order to collect, preserve and display artifacts and images from Ontario's heritage and connect the City's past to the present through the History Room.	***Not Applicable.***
Goal SR5: Local heritage, entertainment and cultural experiences that enrich the lives of Ontario's residents, workers, and visitors and serve to attract residents and businesses to the City.	
Policy SR5-1: Provision of Entertainment and Culture. We support a range of entertainment and cultural experiences such as public art, exhibitions and performances.	***Not Applicable.***
Policy SR5-2: Local Heritage Education. We partner with educational providers to promote culture and heritage.	***Not Applicable.***
Policy SR5-3: Public Art. We encourage public art in buildings, parks, open spaces and other public and private spaces.	***Not Applicable.***

Plan Policy	Specific Plan Consistency
Policy SR5-4: Private-Public Sector Events. We partner with private and nonprofit sectors to provide and promote participation in cultural activities including fairs, festivals and other events geared to neighborhoods, the City as a whole and the region.	***Not Applicable.***
Policy SR5-5: Promotion of Ontario Artists and Musicians. We promote awareness of entertainment and culture produced in Ontario.	***Not Applicable.***

B1 APPENDIX GREENHOUSE GAS EMISSIONS

B1.1 CEQA Thresholds and Screening Tables

The Ontario Climate Action Plan (CAP) includes reducing 39,769 Metric Tons of Carbon Dioxide Equivalents per year from new development by 2020 as compared to the 2020 unmitigated conditions. This requires new development to be 25% more efficient. Reductions related to transportation, water, solid waste, energy, and renewable energy sources all play a part in gaining this level of efficiency within new development.

The purpose of this Screening Table is to provide preliminary guidance for the Colony Commerce Center Specific Plan in measuring the reduction of greenhouse gas emissions. The actual design features, choices, and construction measures to be incorporated into the development projects will be presented during the Development Plan submittal process to the City.

The Screening Table assigns points for each option incorporated into a project as mitigation or a project design feature (collectively referred to as "feature"). The point values correspond to the minimum emissions reduction expected from each feature. The menu of features allows maximum flexibility and options for how development within the Colony Commerce Center Specific Plan can implement the GHG reduction measures.

The point levels are based upon improvements compared to 2008 emission levels of efficiency. Projects within the Specific Plan that garner at least 100 points will be consistent with the reduction quantities anticipated in the City's CAP.

As such, those projects that garner a total of 100 points or greater would not require quantification of project specific GHG emissions. Consistent with CEQA Guidelines, such projects would be determined to have a less than significant individual and cumulative impact for GHG emissions.

Table 2: Screening Table for Implementation of GHG Reduction Measures for Commercial/Industrial Development

Feature	Description	Assigned Point Values	Project Points
Reduction Measure PS E3: Commercial/Industrial Energy Efficiency Development			
Building Envelope			
Insulation	2008 baseline (walls R-13; roof/attic R-30)	0 points	**15**
	Modestly Enhanced Insulation (walls R-13, roof/attic R-38))	15 points	
	Enhanced Insulation (rigid wall insulation R-13, roof/attic R-38)	18 points	
	Greatly Enhanced Insulation (spray foam insulated walls R-15 or higher, roof/attic R-38 or higher)	20 points	
	(Applies to the conditioned space, defined as those areas within the building that have air conditioning and heating.)		
Windows	2008 Baseline Windows (0.57 U-factor, 0.4 solar heat gain coefficient [SHGC])	0 points	**8**
	Modestly Enhanced Window Insulation (0.4 U-factor, 0.32 SHGC)	7 points	
	Enhanced Window Insulation (0.32 U-factor, 0.25 SHGC)	8 points	
	Greatly Enhanced Window Insulation (0.28 or less U-factor, 0.22 or less SHGC)	12 points	
	(Applies to the conditioned space, defined as those areas within the building that have air conditioning and heating.)		
Cool Roof	Modest Cool Roof (CRRC Rated 0.15 aged solar reflectance, 0.75 thermal emittance)	12 points	**12**
	Enhanced Cool Roof (CRRC Rated 0.2 aged solar reflectance, 0.75 thermal emittance)	14 points	
	Greatly Enhanced Cool Roof (CRRC Rated 0.35 aged solar reflectance, 0.75 thermal emittance)	16 points	
Air Infiltration	Minimizing leaks in the building envelope is as important as the insulation properties of the building. Insulation does not work effectively if there is excess air leakage.		-
	Air barrier applied to exterior walls, calking, and visual inspection such as the HERS Verified Quality Insulation Installation (QII or equivalent)	12 points	
	Blower Door HERS Verified Envelope Leakage or equivalent	10 points	
	(Applies to the conditioned space, defined as those areas within the building that have air conditioning and heating.)		
Thermal Storage of Building	Thermal storage is a design characteristic that helps keep a constant temperature in the building. Common thermal storage devices include strategically placed water filled columns, water storage tanks, and thick masonry walls.		

Feature	Description	Assigned Point Values	Project Points
	Modest Thermal Mass (10% of floor or 10% of walls 12" or more thick exposed concrete or masonry with no permanently installed floor covering such as carpet, linoleum, wood or other insulating materials)	4 points	-
	Enhanced Thermal Mass (20% of floor or 20% of walls 12" or more thick exposed concrete or masonry with no permanently installed floor covering such as carpet, linoleum, wood or other insulating materials)	6 points	
	Enhanced Thermal Mass (80% of floor or 80% of walls 12" or more thick exposed concrete or masonry with no permanently installed floor covering such as carpet, linoleum, wood or other insulating materials)	24 points	
Indoor Space Efficiencies			
Heating/ Cooling Distribution System	Minimum Duct Insulation (R-4.2 required)	0 points	**14**
	Modest Duct insulation (R-6)	8 points	
	Enhanced Duct Insulation (R-8)	10 points	
	Distribution loss reduction with inspection (HERS Verified Duct Leakage or equivalent)	14 points	
	(Applies to the conditioned space, defined as those areas within the building that have air conditioning and heating.)		
Space Heating/ Cooling Equipment	2008 Minimum HVAC Efficiency (EER 13/60% AFUE or 7.7 HSPF)	0 points	-
	Improved Efficiency HVAC (EER 14/65% AFUE or 8 HSPF)	7 points	
	High Efficiency HVAC (EER 15/72% AFUE or 8.5 HSPF)	8 points	
	Very High Efficiency HVAC (EER 16/80% AFUE or 9 HSPF)	12 points	
	(Applies to the conditioned space, defined as those areas within the building that have air conditioning and heating.)		
Commercial Heat Recovery Systems	Heat recovery strategies employed with commercial laundry, cooking equipment, and other commercial heat sources for reuse in HVAC air intake or other appropriate heat recovery technology. Point values for these types of systems will be determined based upon design and engineering data documenting the energy savings.	TBD	-
Water Heaters	2008 Minimum Efficiency (0.57 Energy Factor)	0 points	-
	Improved Efficiency Water Heater (0.675 Energy Factor)	14 points	
	High Efficiency Water Heater (0.72 Energy Factor)	16 points	
	Very High Efficiency Water Heater (0.92 Energy Factor)	19 points	
	Solar Pre-heat System (0.2 Net Solar Fraction)	4 points	
	Enhanced Solar Pre-heat System (0.35 Net Solar Fraction)	8 points	
Daylighting	Daylighting is the ability of each room within the building to provide outside light during the day reducing the need for artificial lighting during daylight hours.		

Feature	Description	Assigned Point Values	Project Points
	All peripheral rooms within building have at least one window or skylight	1 points	
	All rooms within building have daylight (through use of windows, solar tubes, skylights, etc.)	5 points	7
	All rooms daylighted	7 points	
Artificial Lighting	2008 Minimum (required)	0 points	
	Efficient Lights (25% of in-unit fixtures considered high efficacy. High efficacy is defined as 40 lumens/watt for 15 watt or less fixtures; 50 lumens/watt for 15-40 watt fixtures, 60 lumens/watt for fixtures >40watt)	9 points	9
	High Efficiency Lights (50% of in-unit fixtures are high efficacy)	12 points	
	Very High Efficiency Lights (100% of in-unit fixtures are high efficacy)	14 points	
Appliances	Energy Star Commercial Refrigerator (new)	4 points	
	Energy Star Commercial Dish Washer (new)	4 points	8
	Energy Star Commercial Cloths Washing	4 points	
Miscellaneous Commercial/Industrial Building Efficiencies			
Building Placement	North/South alignment of building or other building placement such that the orientation of the buildings optimizes conditions for natural heating, cooling, and lighting.	6 point	6
Shading	At least 90% of south-facing glazing will be shaded by vegetation or overhangs at noon on June 21st.	6 Points	-
Other	This allows innovation by the applicant to provide design features that increases the energy efficiency of the project not provided in the table. Note that engineering data will be required documenting the energy efficiency of innovative designs and point values given based upon the proven efficiency beyond Title 24 Energy Efficiency Standards.	TBD	-
Existing Commercial building Retrofits	The applicant may wish to provide energy efficiency retrofit projects to existing commercial buildings to further the point value of their project. Retrofitting existing commercial buildings within the City is a key reduction measure that is needed to reach the reduction goal. The potential for an applicant to take advantage of this program will be decided on a case by case basis and must have the approval of the Ontario Planning Department. The decision to allow applicants the ability to participate in this program will be evaluated based upon, but not limited to the following:	TBD	-

Feature	Description	Assigned Point Values	Project Points
	Will the energy efficiency retrofit project benefit low income or disadvantaged communities? Does the energy efficiency retrofit project fit within the overall assumptions in the reduction measure associated with commercial building energy efficiency retrofits? Does the energy efficiency retrofit project provide co-benefits important to the City? Point value will be determined based upon engineering and design criteria of the energy efficiency retrofit project.		
Reduction Measure PS E4: Commercial/Industrial Renewable Energy			
Photovoltaic	Solar Photovoltaic panels installed on commercial buildings or in collective arrangements within a commercial development such that the total power provided augments:		
	Solar Ready Roofs (sturdy roof and electric hookups)	2 points	
	10 percent of the power needs of the project	8 points	
	20 percent of the power needs of the project	14 points	
	30 percent of the power needs of the project	20 points	-
	40 percent of the power needs of the project	26 points	
	50 percent of the power needs of the project	32 points	
	60 percent of the power needs of the project	38 points	
	70 percent of the power needs of the project	44 points	
	80 percent of the power needs of the project	50 points	
	90 percent of the power needs of the project	56 points	
	100 percent of the power needs of the project	60 points	
Wind turbines	Some areas of the City lend themselves to wind turbine applications. Analysis of the areas capability to support wind turbines should be evaluated prior to choosing this feature. Wind turbines as part of the commercial development such that the total power provided augments:		
	10 percent of the power needs of the project	8 points	
	20 percent of the power needs of the project	14 points	-
	30 percent of the power needs of the project	20 points	
	40 percent of the power needs of the project	26 points	
	50 percent of the power needs of the project	32 points	
	60 percent of the power needs of the project	38 points	
	70 percent of the power needs of the project	44 points	

Feature	Description	Assigned Point Values	Project Points
	80 percent of the power needs of the project 90 percent of the power needs of the project 100 percent of the power needs of the project	50 points 56 points 60 points	
Off-site renewable energy project	The applicant may submit a proposal to supply an off-site renewable energy project such as renewable energy retrofits of existing commercial/industrial that will help implement reduction measures associated with existing buildings. These off-site renewable energy retrofit project proposals will be determined on a case by case basis accompanied by a detailed plan documenting the quantity of renewable energy the proposal will generate. Point values will be based upon the energy generated by the proposal.	TBD	-
Other Renewable Energy Generation	The applicant may have innovative designs or unique site circumstances (such as geothermal) that allow the project to generate electricity from renewable energy not provided in the table. The ability to supply other renewable energy and the point values allowed will be decided based upon engineering data documenting the ability to generate electricity.	TBD	-
Reduction Measure PS W2: Commercial/Industrial Water Conservation			
Irrigation and Landscaping			
Water Efficient Landscaping	Eliminate conventional turf from landscaping Only moderate water using plants Only low water using plants Only California Native landscape that requires no or only supplemental irrigation	0 points 3 points 4 points 8 points	**4**
Trees	Increase tree planting in parking areas 50% beyond City Code requirements	TBD	-
Water Efficient irrigation systems	Low precipitation spray heads< .75"/hr or drip irrigation Weather based irrigation control systems combined with drip irrigation (demonstrate 20 reduced water use)	1 point 5 points	**1**
Recycled Water	Recycled water connection (purple pipe)to irrigation system on site	5 points	**5**
Storm water Reuse Systems	Innovative on-site stormwater collection, filtration and reuse systems are being developed that provide supplemental irrigation water and provide vector control. These systems can greatly reduce the irrigation needs of a project. Point values for these types of systems will be determined based upon design and engineering data documenting the water savings.	TBD	-

Feature	Description	Assigned Point Values	Project Points
Potable Water			
Showers	Water Efficient Showerheads (2.0 gpm)	3 points	-
Toilets	Water Efficient Toilets/Urinals (1.5gpm) Waterless Urinals (note that commercial buildings having both waterless urinals and high efficiency toilets will have a combined point value of 6 points)	3 points 4 points	-
Faucets	Water Efficient faucets (1.28gpm)	3 points	-
Commercial Dishwashers	Water Efficient dishwashers (20% water savings)	4 points	**4**
Commercial Laundry Washers	Water Efficient laundry (15% water savings) High Efficiency laundry Equipment that captures and reuses rinse water (30% water savings)	3 points 6 points	-
Commercial Water Operations Program	Establish an operational program to reduce water loss from pools, water features, etc., by covering pools, adjusting fountain operational hours, and using water treatment to reduce draw down and replacement of water. Point values for these types of plans will be determined based upon design and engineering data documenting the water savings.	TBD	-
Reduction Measure PS T1: Land Use Based Trips and VMT Reduction			
Mixed Use	Mixes of land uses that complement one another in a way that reduces the need for vehicle trips can greatly reduce GHG emissions. The point value of mixed use projects will be determined based upon traffic studies that demonstrate trip reductions and/or reductions in vehicle miles traveled	TBD	-
Local Retail Near Residential (Commercial only Projects)	Having residential developments within walking and biking distance of local retail helps to reduce vehicle trips and/or vehicle miles traveled. The point value of residential projects in close proximity to local retail will be determined based upon traffic studies that demonstrate trip reductions and/or reductions in vehicle miles traveled	TBD	-
Reduction Measure PS T2: Bicycle Master Plan			
Bicycle Infrastructure	Ontario's Bicycle Master Plan is extensive and describes the construction on 11.5 miles of Class I bike paths and 23 miles of Class II and Class III bikeways to build upon the current 8 miles of bikeways. Provide bicycle paths within project boundaries. Provide bicycle path linkages between project site and other land uses. Provide bicycle path linkages between project site and transit.	TBD TBD 2 points 5 points	-

Feature	Description	Assigned Point Values	Project Points
Reduction Measure PS T3: Electric Vehicle Infrastructure			
Electric Vehicles	Provide public charging station for use by an electric vehicle. *(ten points for each charging station within the facility)*	10 points	**10**
Reduction Measure PS T4: Employee Based Trip &VMT Reduction Policy			
Compressed Work Week	Reduce the number of days per week that employees need to be on site will reduce the number of vehicle trips associated with commercial/industrial development. Compressed work week such that full time employees are on site: 5 days per week 4 days per week on site 3 days per week on site	TBD	-
Car/Vanpools	Car/vanpool program Car/vanpool program with preferred parking Car/vanpool with guaranteed ride home program Subsidized employee incentive car/vanpool program Combination of all the above	TBD	-
Employee Bicycle/ Pedestrian Programs	Complete sidewalk to residential within ½ mile Complete bike path to residential within 3 miles Bike lockers and secure racks Showers and changing facilities Subsidized employee walk/bike program (Note combine all applicable points for total value)	TBD	-
Shuttle/Transit Programs	Local transit within ¼ mile Light rail transit within ½ mile Shuttle service to light rail transit station Guaranteed ride home program Subsidized Transit passes Note combine all applicable points for total value	TBD	-
CRT	Employer based Commute Trip Reduction (CRT). CRTs apply to commercial, offices, or industrial projects that include a reduction of vehicle trip or VMT goal using a variety of employee commutes trip reduction methods. The point value will be determined based upon a TIA that demonstrates the trip/VMT reductions. Suggested point ranges: Incentive based CRT Programs (1-8 points) Mandatory CRT programs (5-20 points)	TBD	-
Other Trip Reductions	Other trip or VMT reduction measures not listed above with TIA and/or other traffic data supporting the trip and/or VMT for the project.	TBD	-
Total Points from Commercial/Industrial Project:			**103**

Colony Commerce Center West

Specific Plan

Draft | March 2017

ATTACHMENT B:

File No. PSP15-001
Departmental Conditions of Approval

(Departmental conditions of approval to follow this page)

City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department
Land Development Division
Conditions of Approval

Meeting Date: July 25, 2017

File No: PSP15-001

Related Files: N/A

Project Description: A Specific Plan (Colony Commerce Center West) request (File No. PSP15-001) to establish land use designations, development standards, design guidelines and infrastructure improvements for approximately 123.17 acres of land, which includes the potential development of 2,951,146 square feet of industrial development. The project site is bounded by Merrill Avenue to the north, Remington Avenue to the south, Carpenter Avenue to the west and the Cucamonga Creek Flood Control Channel to the east; **submitted by Cap Rock-Partners.** (APN(s): 0218-292-05, 0218-292-09, 0218-292-10, 0218-311-11, 0218-292-12, 0218-292-13, 0218-292-14, 0218-261-24).

Prepared By: Luis E. Batres, Senior Planner
Phone: 909.395.2431 (direct)
Email: Lbatres@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2010. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Specific Plan/Specific Plan Amendment. The following shall be submitted to the Planning Department within 30 days following City Council approval of the Specific Plan/Specific Plan Amendment:

(a) Fifteen copies of the final Specific Plan document;

(b) One complete, unbound copy of the final Specific Plan document;

(c) One CD containing a complete Microsoft Word copy of the final Specific Plan document, including all required revisions;

(d) Five CDs, each containing a complete PDF copy of the final Specific Plan document, including all required revisions; and

(e) One CD containing a complete electronic website version of the final Specific Plan document, including all required revisions.

2.2 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.3 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

CITY OF ONTARIO
MEMORANDUM

TO: **Chairman and Members of the Planning Commission**

FROM: **Scott Murphy, Planning Director**

DATE: **July 25, 2017**

SUBJECT: **MONTHLY PLANNING DEPARTMENT ACTIVITY REPORT; MONTH OF JUNE 2017**

Attached, you will find the Planning Department Monthly Activity Report for the month of June 2017. The report describes all new applications received by the Planning Department and actions taken on applications during the month. Please contact me if you have any questions regarding this information.

The attached reports, along with reports from past months, may also be viewed on the City's web site. New applications may be viewed at http://www.ontarioca.gov/planning/reports/monthly-activity-reports-applications, and actions taken on applications may be viewed at http://www.ontarioca.gov/planning/reports/monthly-activity-reports-actions.

Monthly Activity Report—New Applications

Month of June 2017

PDEV17-024: **Submitted by CalAtlantic Group, Inc.**

A Development Plan to construct 88 single-family dwellings on 14.35 acres of land located at the southeast corner of Archibald Avenue and Parkview Street, within Planning Area 4 of the Subarea 29 Specific Plan (APN: 0218-022-15). Related File: PMTT14-010.

PDEV17-025: **Submitted by Christopher Homes**

A Development Plan to construct 102 single-family dwellings on 8.24 acres of land located at the northeast corner of Merrill and Celebration Avenues, within Planning Area 29 of the Subarea 29 Specific Plan (APNs: 0218-033-03, 0218-033-04, 0218-003-05, and 0218-003-06). Related File: PMTT14-019 (TT18998).

PDEV17-026: **Submitted by LD King**

A Development Plan to construct 55 single-family dwellings on 6.11 acres of land, located at 2041 East Fourth Street, within the MDR-11 (Medium Density Residential – 5.1 to 11.0 DU/Acre) zoning district (APN: 0110-441-10). Related File: PMTT17-008.

PDEV17-027: **Submitted by The Heaton Company**

A Development Plan to construct a 23,570 square foot industrial building on 1.27 acres of land located at 930 South Grove Avenue, within the Business Park land use district of the Grove Avenue Specific Plan (APNs: 1049-384-09).

PHP-17-010: **Submitted by City of Ontario**

A Tier Determination for the Proposed Guasti Village Historic District generally located on the south sides of Guasti Road, between Archibald and Turner Avenues, and at 250 North Turner Avenue (San Secondo d'Asti Catholic Church), within the Guasti Plaza Specific Plan (APNs: 0210-192-11 and 0210-551-02).

PHP-17-011: **Submitted by Leah A & Keith L Trust**

A Local Landmark designation for a multiple-family residence located at 406 East I Street, within the LDR-5 (Low Density Residential – 2.1 to 5.0 DU/Acre) zoning district (APN: 1048-251-19). Related File: PHP-17-012.

PHP-17-012: **Submitted by Daniel & Jerilyn Marin**

A Mills Act Contract for a multiple-family residence located at 406 East I Street, within the LDR-5 (Low Density Residential – 2.1 to 5.0 DU/Acre) zoning district (APN: 1048-251-19). Related File: PHP-17-011.

PHP-17-013: **Submitted by Jason Smith**

A Mills Act Contract for a single-family dwelling located at 206 West Armsley Square, within the RE-4 (Residential Estate – 2.1 to 4.0 DU/Acre) zoning district, and the Armsley Square Historic District (APN: 1047-343-08).

PHP-17-014: **Submitted by Amor Architectural Corporation**

A Certificate of Appropriateness for the construction of a 10,504-square foot multiple-tenant commercial building on 0.88 acres of land generally located at the northwest corner of Francis Street and Euclid Avenue, within the CN (Neighborhood Commercial) zoning district and EA (Euclid Avenue) Overlay district, (APN: 1050-281-01, 1050-281-02 and 1050-281-03). Related Files: PDEV17-008 and PVAR17-003.

PHP-17-015: **Submitted by Gregory Del Fante**

A Mills Act Contract for a single-family dwelling located at 227 East G Street, within the LDR-5 (Low Density Residential – 2.1 to 5.0 DU/Acre) zoning district and a Contributor within the El Morado Court Historic District (APN: 1048-243-20).

PHP-17-016: **Submitted by Daniel R. Garcia**

A Mills Act Contract for a single-family dwelling located at 128 East El Morado Court, within the LDR-5 (Low Density Residential – 2.1 to 5.0 DU/Acre) zoning district and a Contributor within the El Morado Court Historic District (APN: 1048-242-03).

PMTT17-008: **Submitted by LD King**

A Tentative Tract Map (TT 18984) to subdivide 6.11 acres of land located at 2041 East Fourth Street, into 55 single-family residential lots within the MDR-11 (Medium Density Residential – 5.1 to 11.0 DU/Acre) zoning district (APN: 0110-441-10). Related File: PDEV17-026.

PSGN17-059: **Submitted by BIS**

A Sign Plan for the installation of one tenant identification wall sign for DIXON HUGHES GOODMAN, located at 3175 East Sedona Court, Building A, within the Wagner Specific Plan (APN: 0210-571-09).

PSGN17-060: **Submitted by Starr Sign Design**

A Sign Plan for the installation of a non-illuminated, channel-cut wall sign with logo for KRONES TRANS-MARKET, located at 3491 East Concours Street, Suite 101, within the Urban Commercial land use district of the Ontario Center Specific Plan.

PSGN17-061: Submitted by Black Coffee Fabricators

A Sign Plan for the installation of an internally illuminated wall sign, with logo, for WALLNER EXPAC, located at 1274 South Slater Court, within the Light Industrial land use district of the California Commerce Center Specific Plan.

PSGN17-062: Submitted by Electricore Signs

A Sign Plan for the installation of an internally illuminated wall sign for STEREO'S R US, located at 761 West Holt Boulevard, within the CC (Community Commercial) zoning district.

PSGN17-063: Submitted by Sign Specialists Corp

A Sign Plan for the installation of two internally illuminated wall signs for TWG - The Wheel Group, located at 1050 North Vineyard Avenue, within the Industrial land use district of the Meredith International Centre.

PSGN17-064: Submitted by American Heritage University of Southern California

A Sign Plan for the installation of an internally illuminated wall sign for AMERICAN HERITAGE UNIVERSITY, located at 1802 East G Street, within the CC (Community Commercial) zoning district.

PSGN17-065: Submitted by Swain Sign, Inc.

A Sign Plan for the installation of a new secondary wall sign for WALMART, on the south elevation, to read “Pickup,” in place of an existing sign reading "Outdoor Living" on the east elevation, which is to be removed, located at 1333 North Mountain Avenue, within the Main Street District of the Mountain Village Specific Plan.

PSGN17-066: Submitted by America's Signs

A Sign Plan for the installation of a nonilluminated wall sign for CTDI, located at 821 South Rockefeller Avenue, within the Light Industrial land use district of the California Commerce Center Specific Plan.

PSGN17-067: Submitted by Pop Warner Football

A Sign Plan for POP WARNER FOOTBALL, to install two canvas signs (one on the back of the scoreboard and one on the equipment storage container) within the Ontario Motor Speedway Park, located at 915 North Center Avenue.

PSGN17-068: Submitted by Encore Signs

A Sign Plan for the installation of a new monument sign (and remove the existing monument sign) for NEW INDY (33 SF), located at 5100 East Jurupa Street, within the IH (Heavy Industrial) zoning District.

PSGN17-069: Submitted by Williams Sign Co.

A Sign Plan for the installation of a new monument sign for RESIDENCE INN BY MARRIOTT ONTARIO AIRPORT (and remove the existing monument sign), located at 2025 East Convention Center Way, within the CCS (Convention Center Support Commercial) zoning district.

PSGN17-070: Submitted by Design UA

A Sign Plan for the installation of a new drive-thru menu boards and a new pre-order board for MCDONALD'S, located at 2455 South Archibald Avenue, within the CN (Neighborhood Commercial) zoning district.

PSGN17-071: Submitted by Electricore Signs

A Sign Plan for the installation of an internally illuminated wall sign (20 SF) for GROOMATORIUM, INC., located at 521 North Euclid Avenue, within the MU-1 (Downtown Mixed-Use) zoning district.

PSGP17-004: Submitted by Reliable Properties

A Sign Program for 1305 East Fourth Street, located at the northeast corner of Fourth Street and Grove Avenue, within the CN (Neighborhood Commercial) zoning district.

PSPA17-003: Submitted by The New Home Company Southern California, LLC

An Amendment to the Subarea 29 Specific Plan, adding a new Conventional Small Lot Residential Product within the Planning Area 24 land use district (APNs: 0218-033-01 through 04).

PTUP17-026: Submitted by Church of God of Prophecy

A Temporary Use Permit to conduct a carwash fundraiser, to be held at Church of God of Prophecy, located at 1130 South Campus Avenue. To be held on 6/10/2017.

PTUP17-027: Submitted by CBBA Arena

A Temporary Use Permit to conduct a Preconcert Def Leppard Event within the Citizen's Business Bank Arena parking lot area, located at 4000 East Ontario Center Parkway. To be held on 6/14/2017.

PTUP17-028: Submitted by Mountain Motorsports

A Temporary Use Permit to conduct a customer appreciation event in conjunction with a retail sales/bike night event, including outdoor DJ and taco stand, with promotions and retail sales occurring indoors, located at 1025 North Mountain Avenue. To be held on 6/15/2017.

PTUP17-029: Submitted by Bierbuzz Events

A Temporary Use Permit to conduct a fundraising event ("Harvest Festival"), with beer/wine and music, at Guasti Park, 800 North Archibald Avenue. To be held on 8/19/2017.

PTUP17-030: Submitted by Ontario Convention Center

A Temporary Use Permit to conduct a preconcert event (for age 21 and over) to include the sales of food and alcoholic beverages, and music (local live band), along the east side plaza area of Citizen's Business Bank Arena, located at 4000 East Ontario Center Parkway. To be held on 7/22/2017, 3:00PM to 7:30PM.

PTUP17-031: Submitted by American Cancer Society

A Temporary Use Permit to conduct an American Cancer Society Car show hosted by San Joaquin Valley College, located at 4580 East Ontario Mills Parkway. To be held on 7/29/2017.

PVAR17-005: Submitted by LZCC Holdings, Inc., Brother's Home Trading

A Minor Variance to deviate from the minimum required front yard setback, from 30 FT to 22.5 FT, in conjunction with the construction of two single-family dwellings on approximately 0.14 acres of land located at 519 North Grove Avenue, within the MDR-18 (Medium Density Residential – 11.1 to 18.0 DU/Acre) zoning district (APN: 1048-451-09). Related Files: B201700872 and B201700973.

PVAR17-006: Submitted by The Heaton Company

A Variance to deviate from the Grove Avenue Specific Plan minimum building setback requirement from Grove Avenue, from 40 FT to 30 FT, and from interior property lines, from 10 FT to 0 FT, in conjunction with the construction of a 23,570-square foot industrial building on 1.27 acres of land, located at 930 South Grove Avenue, within the Business Park land use district of the Grove Avenue Specific Plan (APNs: 1049-384-09). Related File: PDEV17-027.

PVER17-038: Submitted by Rexford Industrial

A Zoning Verification for 2002-3072 East Inland Empire Boulevard (APN: 0210-151-16).

PVER17-039: Submitted by Mary Smith

A Zoning Verification for 121 West State Street (APN: 1049-277-02).

PVER17-040: Submitted by AEI Consultants

A Zoning Verification for Marketplace on Grove, located at 1150 East Philadelphia Street (APN: 1051-151-08).

PVER17-041: Submitted by Planning & Zoning Resource Company

A Zoning Verification for 1110 East Philadelphia Street (APN: 1051-151-04).

PVER17-042: **Submitted by Armada Analytics, Inc.**

A Zoning Verification for 1900 South Campus Avenue (APN: 1050-421-03).

DEVELOPMENT ADVISORY BOARD MEETING — June 5, 2017

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV16-020: A revision to a previously approved Development Plan (File No. PDEV16-020), to add 49,912 square feet of warehouse area to an existing 426,406-square foot furniture store and warehouse (Mathis Brothers), for a total building area of 476,318 square feet on approximately 19.23 acres of land located at 4105 East Inland Empire Boulevard, within the Garden Commercial land use district of the Ontario Center Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with Environmental Impact Report No. 88-2, certified by the Ontario City Council on March 19, 1991. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0210-205-12) **submitted by Mathis Properties California, LLC.**
Action: The Development Advisory Board approved the application subject to conditions.

ENVIRONMENTAL ASSESSMENT, DEVELOPMENT PLAN, AND TENTATIVE TRACT MAP REVIEW FOR FILE NOS. PDEV16-023 & PMTT16-014: A Development Plan (File No. PDEV16-023) to construct a 36-Unit residential condominium development on 1.42 acres of land and a Tentative Tract Map (File No. PMTT16-014/TM20028) to subdivide 1.42 acres into a single lot for condominium purposes, for property located at 1719 E. Fourth Street, within the HDR-45 (High Density Residential 25.1 to 45.0 DUs/Acre) zone. Staff has determined that the project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32 In-Fill Development Projects) of the CEQA guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 0108-551-01, 0108-551-34, 0108-551-35) **submitted by Kevin K. Cheung.** Planning Commission action is required.
Action: The Development Advisory Board recommended the Planning Commission approve the application subject to conditions.

ZONING ADMINISTRATOR MEETING — June 5, 2017

Meeting Cancelled

CITY COUNCIL MEETING — June 6, 2017

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT CODE AMENDMENT REVIEW FOR FILE NO. PDCA17-001: A Development Code Amendment proposing various clarifications to the Ontario Development Code, modifying certain provisions of Division 1.02 (Development Code

Interpretation and Enforcement), Division 4.02 (Discretionary Permits and Actions), Division 5.02 (Land Use), Division 5.03 (Standards For Certain Land Uses, Activities and Facilities), Division 6.01 (District Standards and Guidelines), Division 7.01 (Historic Preservation), and Division 9.01 (Definitions). The proposed Development Code Amendment is exempt from the requirements of the California Environmental Quality Act (CEQA) and the guidelines promulgated thereunder, pursuant to Section 15061(b)(3) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); **City Initiated.** The Planning Commission recommended approval of this item on April 25, 2017 with a vote of 6 to 0.
Action: The City Council approved and waived further reading of an ordinance approving the application.

ENVIRONMENTAL ASSESSMENT AND PLANNED UNIT DEVELOPMENT REVIEW FOR FILE NO. PUD17-001: A Planned Unit Development to establish development standards and guidelines to facilitate the future development of a high density residential apartment project at a density of approximately 25.4 dwelling units per acre on approximately 2.95 acres of land bordered by Holt Boulevard on the north, Fern Avenue on the east, Emporia Street on the south, and Vine Avenue on the west, within the MU-1 (Mixed Use Downtown) zoning district. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (SCH No. 2008101140), prepared in conjunction with File No. PGPA06-001, and certified by the City of Ontario City Council on January 27, 2010. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 1049-051-01, 1049-051-02, 1049-051-03, 1049-052-03, 1049-052-04, 1049-052-05, 1049-052-06, 1049-052-07, 1049-052-08, 1049-052-09 and 1049-052-10) **submitted by Related California.** The Planning Commission recommended approval of this item on April 25, 2017 with a vote of 6 to 0.
Action: The City Council approved and waived further reading of an ordinance approving the application.

DEVELOPMENT ADVISORY BOARD MEETING — June 19, 2017

ENVIRONMENTAL ASSESSMENT, DEVELOPMENT PLAN AND CONDITIONAL USE PERMIT REVIEW FOR FILE NOS. PDEV16-006 AND PCUP16-005: A Development Plan (File No. PDEV16-006) to construct a 27,000 square foot industrial building in conjunction with a Conditional Use Permit (File No. PCUP16-005) to establish an architectural and structural metals manufacturing business on 1.96 acres of land, located at 535 South Palmetto Avenue within the IL (Light Industrial) zoning district. Staff has determined that the project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found

to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1011-161-01) **submitted by MYWI Fabricators, Inc.** Planning Commission action is required.
Action: The Development Advisory Board recommended the Planning Commission approve the applications subject to conditions.

ENVIRONMENTAL ASSESSMENT, DETERMINATION OF USE, DEVELOPMENT PLAN AND CONDITIONAL USE PERMIT REVIEW FOR FILE NO'S. PDET17-002, PDEV17-003 & PCUP17-005: A Determination of Use (File No. PDET17-002) to allow a heliport use within the Centrelake Specific Plan, in conjunction with a Development Plan (File No. PDEV17-003) to construct a three-story commercial/office building totaling 79,455 square feet, and a Conditional Use Permit (File No. PCUP17-005) to establish a rooftop heliport on 5.05 acres of land located at the southwest corner of Haven Avenue and Guasti Road, within the Office land use district of the Centrelake Specific Plan. Pursuant to the California Environmental Quality Act, staff is recommending the adoption of a Mitigated Negative Declaration of environmental effects for the project. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0210-551-07) **submitted by HMC Construction, Inc.** Planning Commission and City Council action is required.
Action: The Development Advisory Board recommended the Planning Commission approve the applications subject to conditions.

ZONING ADMINISTRATOR MEETING — June 19, 2017

Meeting Cancelled

CITY COUNCIL MEETING — June 20, 2017

No Planning Department Items Scheduled

PLANNING/HISTORIC PRESERVATION COMMISSION MEETING — June 27, 2017

ENVIRONMENTAL ASSESSMENT AND SPECIFIC PLAN AMENDMENT FOR FILE NO. PSPA17-002: An Amendment to the Grove Avenue Specific Plan to: [1] change the land use designation from Business Park to Commercial for approximately one-acre of land located at the northeast corner of Grove Avenue and Philadelphia Street; [2] amend the Commercial District permitted and conditionally permitted uses; and [3] update all applicable specific plan sections to reflect the proposed amendments. The Grove Avenue Specific Plan is generally located on the east and west

sides of Grove Avenue and between Mission Boulevard to the north and the I-60 Freeway to the south. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) prepared in conjunction with File No. PGPA06-001, and adopted by City Council on January 27, 2010. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0113-641-13) **submitted by A&E Leasing, LLC.** City Council Action is required.
Action: The Planning Commission recommended the City Council approve the application.

ENVIRONMENTAL ASSESSMENT, DEVELOPMENT PLAN AND CONDITIONAL USE PERMIT REVIEW FOR FILE NOS. PDEV16-006 & PCUP16-005: A Development Plan (File No. PDEV16-006) to construct a 27,000-square foot industrial building in conjunction with a Conditional Use Permit (File No. PCUP16-005) to establish an architectural and structural metals manufacturing business on 1.96 acres of land located at 535 South Palmetto Avenue, within the IL (Light Industrial) zoning district. Staff has determined that the project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1011-161-01) **submitted by MYWI Fabricators, Inc.**
Action: The Planning Commission approved the applications subject to conditions.

ENVIRONMENTAL ASSESSMENT, DEVELOPMENT PLAN, AND TENTATIVE TRACT MAP REVIEW FOR FILE NOS. PDEV16-023 & PMTT16-014: A Development Plan (File No. PDEV16-023) to construct a 36-unit residential condominium development on 1.42 acres of land, and a Tentative Tract Map (File No. PMTT16-014/TM 20028) to subdivide the 1.42-acre project site into a single lot for condominium purposes, located at 1719 East Fourth Street, within the HDR-45 (High Density Residential - 25.1 to 45.0 DUs/Acre) zoning district. Staff has determined that the project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32 In-Fill Development Projects) of the CEQA guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 0108-551-01, 0108-551-34, 0108-551-35) **submitted by Kevin K. Cheung.**
Action: The Planning Commission approved the applications subject to conditions.

ENVIRONMENTAL ASSESSMENT, DETERMINATION OF USE, DEVELOPMENT PLAN AND CONDITIONAL USE PERMIT REVIEW FOR FILE NO'S. PDET17-002, PDEV17-003 & PCUP17-005: A Determination of Use (File No. PDET17-002) to allow a heliport use within the Centrelake Specific Plan, in conjunction with a Development Plan (File No. PDEV17-003) to construct a three-story commercial/office building totaling 79,455 square feet, and a Conditional Use Permit (File No.

PCUP17-005) to establish a rooftop heliport on 5.05 acres of land located at the southwest corner of Haven Avenue and Guasti Road, within the Office land use district of the Centrelake Specific Plan. Staff is recommending the adoption of a Mitigated Negative Declaration of environmental effects for the project. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0210-551-07) **submitted by HMC Construction, Inc.**
Action: The Planning Commission approved the Determination of Use and Development Plan subject to conditions and the Conditional Use Permit was continued indefinitely.

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT CODE AMENDMENT REVIEW FOR FILE NO. PDCA17-002: A Development Code Amendment adding Paragraph 7 to Subsection K of Ontario Development Code Section 8.01.020 (Sign Standards), which authorizes the establishment of an incentive not to place political signs. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15378(b)(4) of the CEQA Guidelines, as the Development Code Amendment does not involve any commitment to a specific project which may result in a potentially significant physical impact on the environment. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); **City Initiated.** City Council action is required.
Action: The Planning Commission voted 3-3 and as a result the application did not move forward.

ENVIRONMENTAL ASSESSMENT AND CERTIFICATE OF APPROPRIATENESS REVIEW FOR FILE NO. PHP17-003: A request for a Certificate of Appropriateness to construct a 3,535 square foot, two-story, single-family residence within the Euclid Avenue Historic District, on 0.37 acres of land located at 1521 North Euclid Avenue, within the RE-4 (Residential Estate - 2.1 to 4.0 DU/Acre) and EA (Euclid Avenue) Overlay zoning districts. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15303 (Class 3, New Construction or Conversion of Small Structures). The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1047-251-01) **Submitted by Anthony Lionel Mejia.**
Action: The Historic Preservation Commission approved the application.